

May 31, 2019

WCE No. 2018-2178

City of Spokane  
808 W. Spokane Falls Boulevard  
Spokane WA, 99201

Attn: Ali Brast

**Re: The Summit– A Residential Subdivision  
Project Narrative**

Dear Ms. Brast,

This letter is intended to serve as the project development narrative

The project proposes to develop 21.36 acres +/- into a 98-lot subdivision. The project is located at the crest of Cedar Road at the intersection of Cedar Road and Eagle Ridge Boulevard. The property largely consists of a fallow field, and until recently, there was a single-family home and several farm buildings located on the property. The project proposes to extend Eagle Ridge Boulevard into the property and then develop the 21 acres into 98 residential lots in a loop configuration. Also included is a proposed lift station lot that will eventually be abandoned. Additionally, there are three City owned and maintained drainage tracts that will be used to treat and attenuate stormwater. A secondary egress route will be provided onto the existing Oak Road right of way located at the southwest corner of the site.

The site is currently zoned as RSF. The subject property is located in a portion of NE ¼ of the NE ¼ of Section 12, T 24 N., R 42 E., W.M. The parcel number for the project is 24121.0001. The surrounding areas are single family residential land uses to the east, and large lot residential to the north, south, and west.

**Sewer System**

City of Spokane Sewer Department is proposed to serve the project site. Sewer from phase one will gravity flow to the existing manhole located at the intersection of Eagle Ridge Blvd and Cedar Road. The remaining 71 lots in phases 2 and 3 are proposed to gravity flow to a lift station located in the northwest corner of the site. From that lift station it will pump via force main to a manhole located at the intersection of Eagle Ridge Boulevard and Walnut St. If the lift station is to be owned and operated privately, it will be sized to accommodate only this project. Based on 71 lots generating an average of 200 gpd per home (pf 4.0), the peak lift station flow expected to discharge to the existing manhole is approximately 40 gallons per minute. If the lift station is to be publicly owned and operated, it will be sized according to the potential of the contributing sewer basin area. Upon development of the property to the north, it is expected that the lift station will be abandoned and that the gravity sewer will extend north and down the slope to a future regional lift station.

**Water System**

City of Spokane Water Department is proposed to serve the project site. Water is proposed to be connected into the existing system at the 12" line located at the intersection of Cedar Road and Eagle Ridge Boulevard. The site is located within the Eagle Ridge II pressure zone, which according to a City water analysis, has limited capacity. There is adequate storage capacity, but the 12" transmission line in Eagle Ridge Boulevard is undersized compared to the booster pumps feeding it, which results in an inefficient system. In order to remedy these deficiencies, this project proposes to provide redundant supply to the Eagle Ridge II tank via a new booster station located at the Cedar Hills tank site. A water system analysis is anticipated to be completed for this project, and any off-site improvements will be subject to GFC reimbursement.

### **Power and Dry Utilities**

Avista has power and gas service lines on Cedar Road; Comcast and Centurylink also have utilities in and around the development. These utilities are anticipated to be placed in or along all project streets.

### **Stormwater**

For this project stormwater is anticipated to sheet flow from the road surface and the impervious surfaces of each lot with the majority of the stormwater being captured by catch basins and be piped to designated stormwater ponds within several tracts located within the project. Those tracts will be dedicated to the City upon final platting. Within the stormwater ponds the project stormwater will be treated and disposed of per the SRSRM.

### **Comprehensive Plan**

This request is consistent with the following Comprehensive Plan Goals and Policies.

***This project is consistent with the Comprehensive Plan through the implementation of the goals and policies as outlined below.***

- A. Goal LU1. Offer a harmonious blend of opportunities for living, working, recreation, education, shopping, and cultural activities by protecting natural amenities, providing coordinated, efficient, and cost effective public facilities and utility services, carefully managing both residential and non-residential development and design, and proactively reinforcing downtown Spokane's role as a vibrant urban center.
  - a. LU1.1 Neighborhoods. Utilize the neighborhood concept as a unit of design for planning housing, transportation, services, and amenities.

***This project is designed so as to promote neighborhood character. The loop street design concept provides efficient land use while maintaining a cohesive neighborhood concept by discouraging cut through traffic.***
- B. Goal LU2. Encourage the enhancement of the public realm.
  - a. LU- 2.1 Encourage features that improve the appearance of development, paying attention to how projects function to encourage social interaction and relate to and enhance the surrounding urban and natural environment.

***The addition of single family lots encourages social interaction with the neighbors throughout this project. Tree lined streets with sidewalks throughout the subdivision encourage pedestrian connectivity and the associated social interaction between neighbors. Landscaped front yards provide attractive streetscapes similar to the adjacent residential subdivisions.***

- b. LU-2.2 Employ performance and design standards with sufficient flexibility and appropriate incentives to ensure that development is compatible with surrounding land uses.

***The proposed single family lots are compatible with the lots in adjacent subdivisions, and the homes will be similar in character. It complies with all City performance and design standards associated with the Residential Single Family zoning.***

- B. Goal LU3. Promote the efficient use of land by the use of incentives, density and mixed-use development in proximity to retail businesses, public services, places of work and transportation systems.

- a. LU-3.1 Encourage coordinated and efficient growth and development through infrastructure financing and construction programs, tax and regulatory incentives, and by focusing growth in areas where adequate services and facilities exist or can be economically extended.

***This project is an infill project within the City limits and which will extend city streets and services.***

- C. Goal LU 3.7. Prescribe maximum, as well as minimum, lot size standards to achieve the desired residential density for all areas of the city.

***The project conforms to the lot size and density standards associated with the zoning, which promotes efficient land use within the City's boundaries.***

- D. Goal LU4. Promote a network of safe and cost effective transportation alternatives, including transit, carpooling, bicycling, pedestrian-oriented environments, and more efficient use of the automobile, to recognize the relationship between land use and transportation.

- a. LU-4.1 Coordinate land use and transportation planning to result in an efficient pattern of development that supports alternative transportation modes consistent with the Transportation Chapter and makes significant progress toward reducing sprawl, traffic congestion, and air pollution.

***This project is extending existing roads and provides both vehicular and pedestrian connectivity to adjacent undeveloped lands so as to support efficient expansion of the City's transportation network. The close proximity to Cedar***

***Road, which functions as a collector street, provides for efficient access to the regional transportation system.***

- b. LU-4.4 form a well connected network which provides safe, direct and convenient access for all users, including pedestrians, bicycles, and automobiles, through site design for new development and redevelopment.

***This project is extending existing roads and provides both vehicular and pedestrian connectivity to adjacent undeveloped lands so as to support efficient expansion of the City's transportation network. The connection to Oak Road on the southwest corner of the property will provide secondary emergency egress for that existing rural neighborhood out to Cedar Road. Connectivity has not been provided to the west boundary due to steep topographical constraints.***

- E. Goal LU5. Promote development in a manner that is attractive, complementary, and compatible with other land uses.

- a. LU-5.1 Ensure that developments are sensitive to the built and natural environment (for example, air and water quality, noise, traffic congestion, and public utilities and services), by providing adequate impact mitigation to maintain and enhance quality of life.

***The project will contribute to regional infrastructure improvements such as water system and transportation system improvements through construction and through financial contribution via impact fees so as to mitigate impacts and to maintain and enhance the quality of life in the vicinity.***

- b. LU-5.2 Encourage site locations and design features that enhance environmental quality and compatibility with surrounding land uses.

***The frontage will have street trees to enhance the surrounding area. Natural drainage areas are preserved for stormwater treatment and storage, and development on steep slopes will be avoided.***

- c. LU-5.5 Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

***The project matches the surrounding single-family lots in size and the resulting built environment will appear to be an extension of the surrounding urban environment.***

F. Goal LU6 Ensure the provision and distribution of adequate, public lands and facilities throughout the city.

a. LU-6.9 Ensure the utilization of architectural and site designs of essential public facilities that are compatible with the surrounding area.

***The public infrastructure facilities will be designed so as to blend into the existing natural and built environment to the best extent possible.***

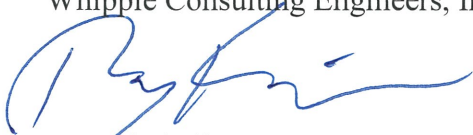
G. Goal LU8. Maintain an Urban Growth Area that includes areas and densities sufficient to accommodate the city's allocated population, housing and employment growth for the succeeding twenty-year period, including the accommodation of the medical, governmental, educational, institutional, commercial, and industrial facilities related to such growth, but that does not exceed the area necessary to accommodate such growth.

a. Goal LU-8.1 Limit urban sprawl by encouraging development in urban areas where adequate public facilities already exist or where such facilities can be more efficiently provided.

***This is an infill project within the urban growth boundary with public facilities located immediately adjacent to the site.***

If you have any questions or comments regarding this letter please feel free to contact me at (509) 893-2617.

Sincerely,  
Whipple Consulting Engineers, Inc.



Ray Kimball, P.E.

Encl:

CC: Sponsor  
File