



Street Design Standards Update

PIES Committee

March 23, 2020



The City of Choice

What are we doing? And Why?

- Update to Chapter 3 of the Street Design Standards
- Used by city staff and the development community
- Adding the latest best practices



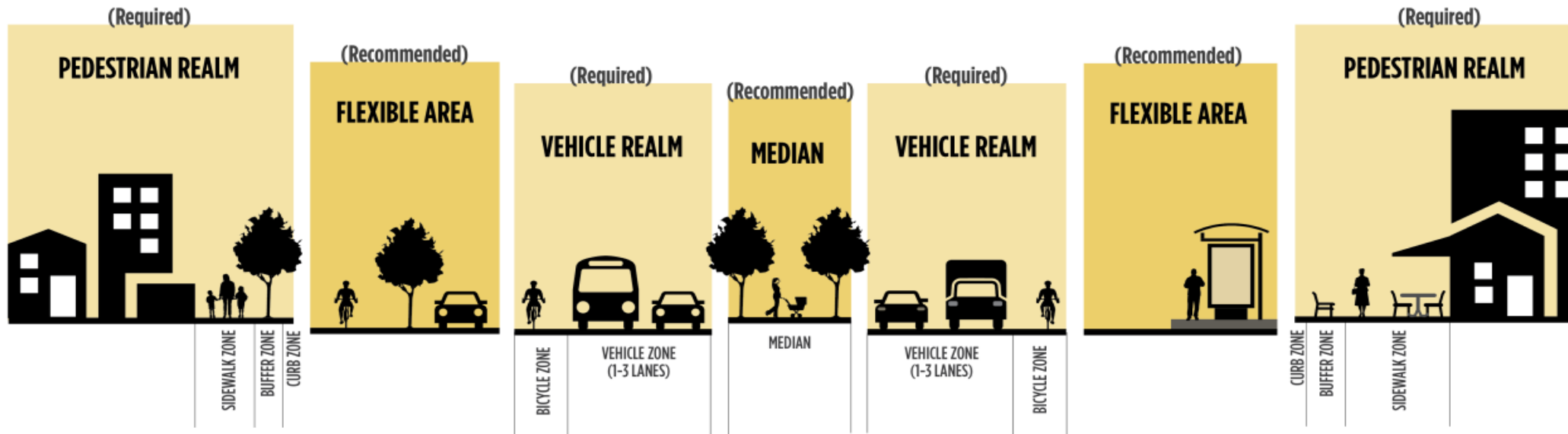
Street Characterization

Characterization = Classification + Context

- Classification
 - Principal, Minor, Major or Minor Collector, Local
- Context
 - Based on Land Use Zoning



Components of the Street



- Sidewalk Zone**
Walkway
Building Frontage
- Buffer Zone**
Street Trees/Grass
Bioswales
Street Furniture
Driveways
- Curb Zone**
Curb

- Curb Extensions**
Bioswales
Parking/Loading
Turn Lanes
Bicycle Facilities
Shared-Use Paths
Bus Bulbs

- Bicycle Zone**
Bicycle Facilities
Bicycle Facility Buffer
- Vehicle Zone**
Transit Lane
Auto Lane

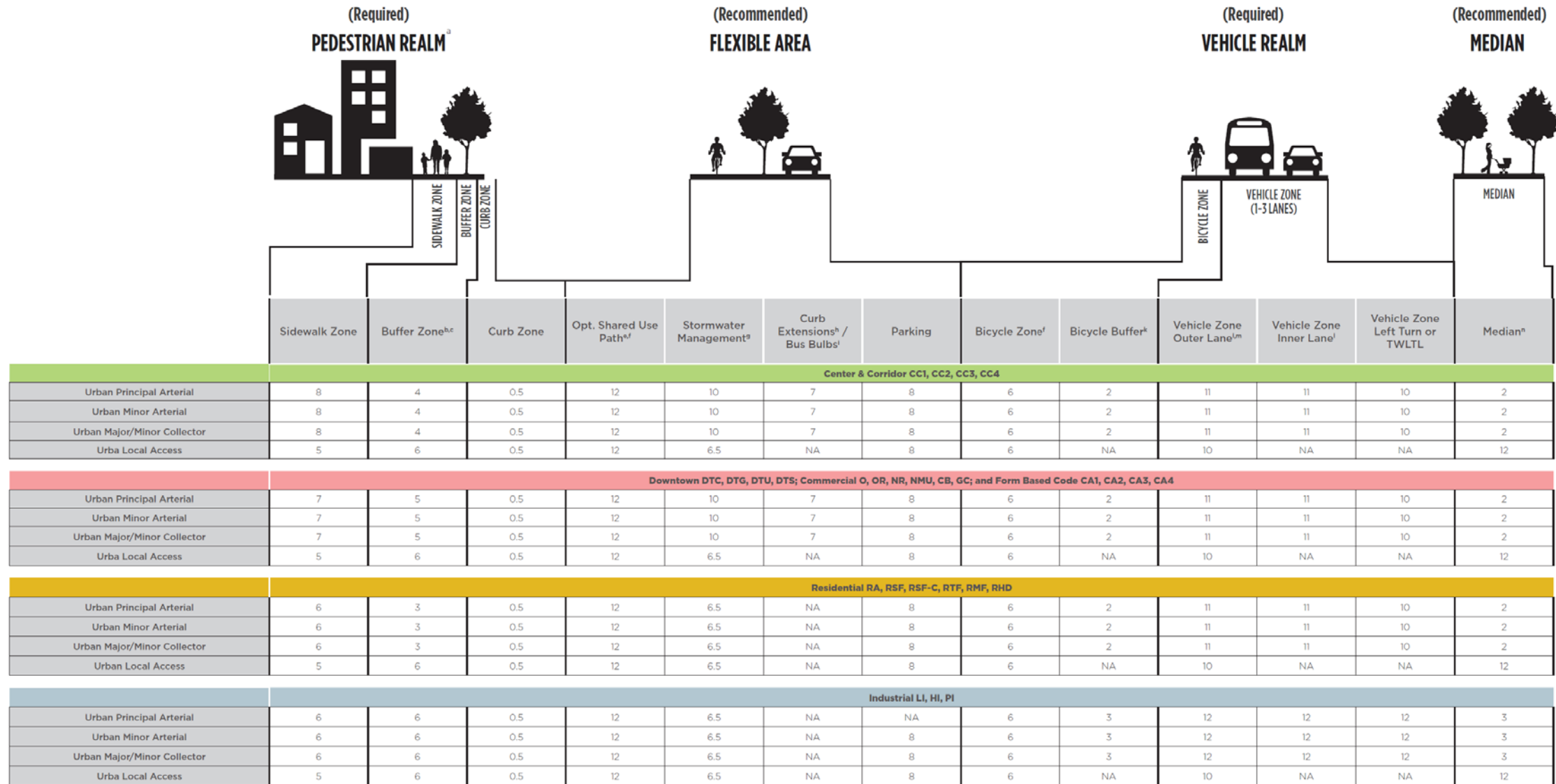
- Median**
Landscaping
Bioswales
Shared-Use Paths
Pedestrian Crossing Refuge
Turn Lanes

- Bicycle Zone**
Bicycle Facilities
Bicycle Facility Buffer
- Vehicle Zone**
Transit Lane
Auto Lane

- Curb Extensions**
Bioswales
Parking/Loading
Turn Lanes
Bicycle Facilities
Shared-Use Paths
Bus Bulbs

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Components of the Street



Feedback from All Ages and Abilities Group

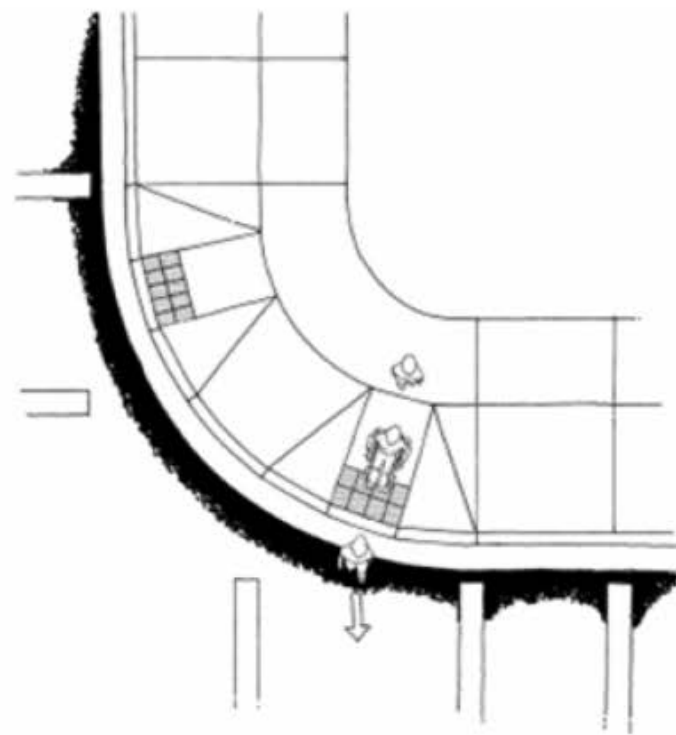
- **Two ramps per corner, not diagonal**
 - Stormwater catch basin conflicts
 - Fire hydrants
 - Power poles
 - Signal poles and cabinets
 - Buildings
- **Desire for Accessible Pedestrian Signal style pushbuttons**



Diagonal Ramps are **NOT** ideal



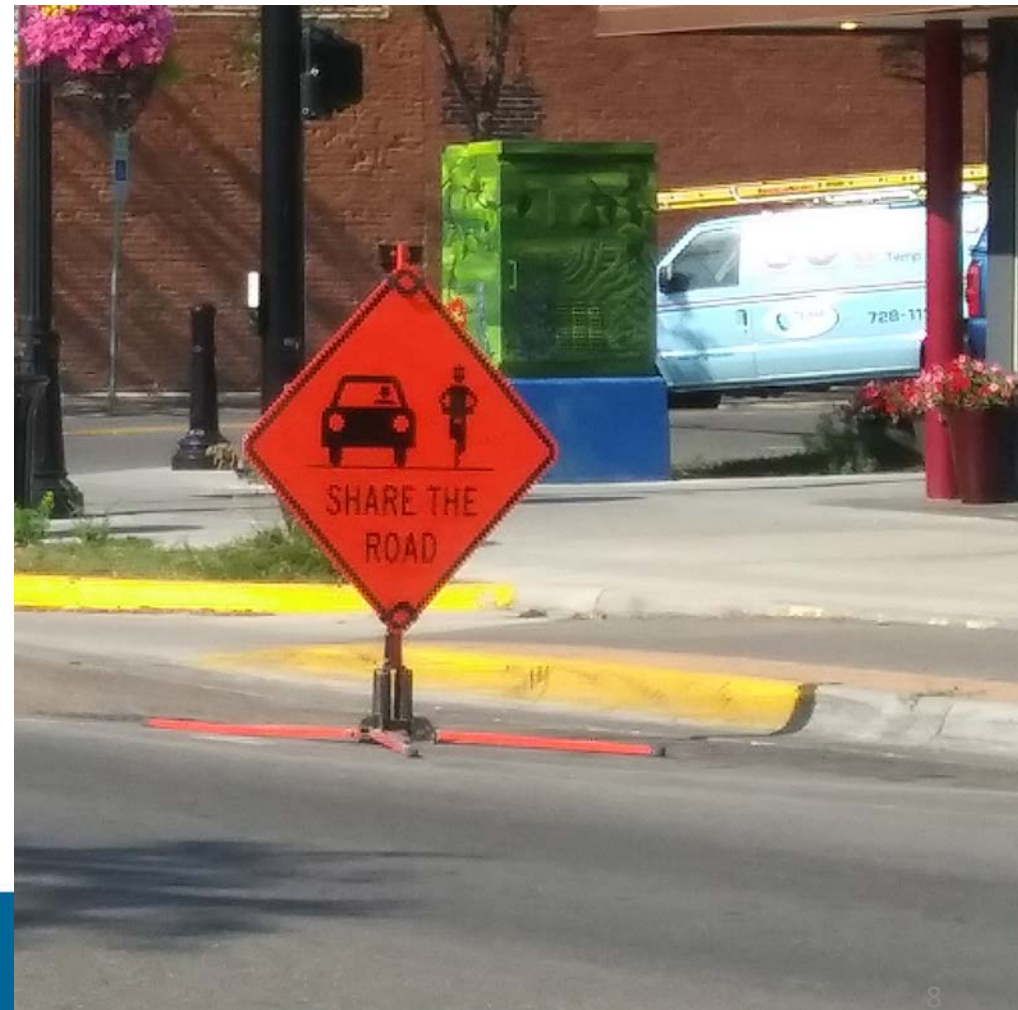
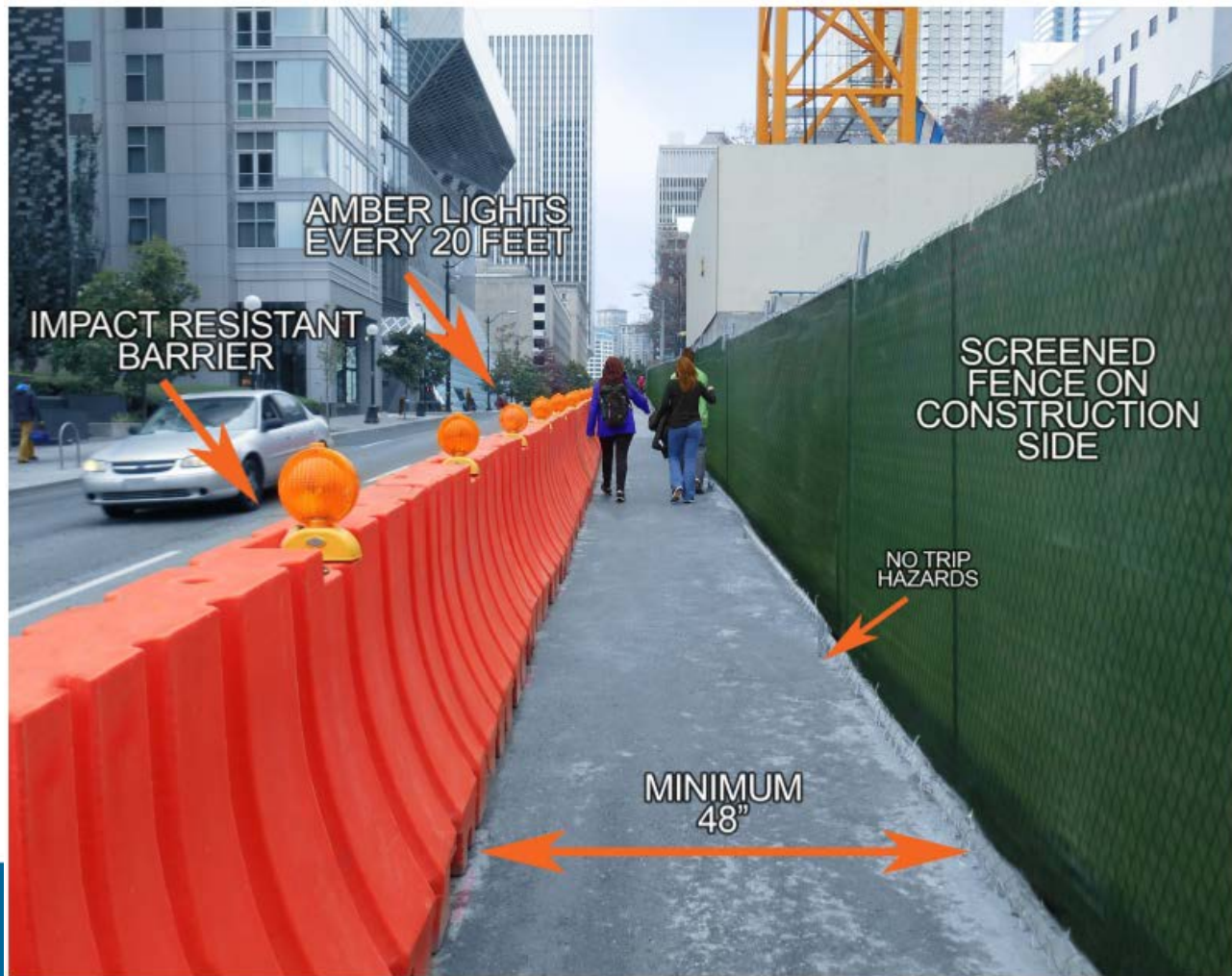
At diagonal curb ramps, wheelchair users cross in different location than other pedestrians.



With 2 separate ramps pedestrians cross at the same location.

Feedback from All Ages and Abilities Group

- Need for pedestrian and bicycle detours



Industrial Streets

The new draft of Street Design Standards includes some basic definitions of what we expect to include when building or rehabilitating streets through industrial land use zones.

These aspects are considered:

- Industrial streets are adjacent to Industrial Land Use Zones, but also along freight routes.
- Wider lanes, particularly approaching intersections and lane change areas.
- Attention to placement of bicycle and pedestrian facilities for visibility and safety. (No curb extensions requirement, wider buffers for bike lanes)



Street Design Standards

Schedule



Schedule for Completion

- March – June: Clean up remaining comments, review SMC's for needed changes
- June-July: Present to Design Review Board and Plan Commission Transportation Subcommittee
- Aug-Oct: Plan Commission and City Council workshops and hearings

