

CITY OF SPOKANE



NOTICE

REGARDING CITY COUNCIL MEETINGS

Notice is hereby given that, pursuant to Governor Jay Inslee's **Eleventh** Updated Proclamation **20-28.11**, dated **November 10, 2020**, all public meetings subject to the Open Public Meetings Act, Chapter 42.30 RCW, are to be held remotely and that the in-person attendance requirement in RCW 42.30.030 has been suspended until at least through **December 7, 2020**.

Temporarily and until further notice, the public's ability to attend City Council meetings is by remote access only. In-person attendance is not permitted at this time. The public is encouraged to tune in to the meeting as noted below.

Public comment will be taken virtually on legislative items during the 6:00 p.m. Legislative Session on **November 23, 2020**. Open Forum will not be held and all testimony must be related to the legislative items on the agenda.

The regularly scheduled Spokane City Council 3:30 p.m. Briefing Session and 6:00 p.m. Legislative Session will be held virtually and streamed live online and airing on City Cable 5. Some members of the City Council and City staff will be attending virtually. The public is encouraged to tune in to the meeting live on Channel 5, at <https://my.spokanecity.org/citycable5/live>, or by calling **1-408-418-9388** and entering the access code **966 942 097** for the 3:30 p.m. Briefing Session or **146 134 0020** for the 6:00 p.m. Legislative Session when prompted; meeting password is **0320**.

To participate in virtual public comment:

Sign up to give testimony at <https://forms.gle/RtcIKb2tju6322BB7>. You must sign up in order to be called on to testify. The form will be **open at 5:00 p.m. on Monday, November 16, 2020, and will close at 6:00 p.m.** At 6:00 p.m., you will call in to the meeting using the information above. When it is your turn to testify, Council President will call your name and direct you to hit *3 on your phone to ask to be unmuted. The system will alert you when you have been unmuted and you can begin giving your testimony. When you are done, you will need to hit *3 again.

**CITY COUNCIL MEETINGS
RULES – PUBLIC DECORUM**

Strict adherence to the following rules of decorum by the public will be observed and adhered to during City Council meetings, including open forum, public comment period on legislative items, and Council deliberations:

- 1. No Clapping!**
- 2. No Cheering!**
- 3. No Booing!**
- 4. No public outbursts!**
- 5. Three-minute time limit for comments made during open forum and public testimony on legislative items!**
- 6. No person shall be permitted to speak at the first open forum more often than once per calendar month.**

In addition, please silence your cell phones when entering the Council Chambers!

Further, keep the following City Council Rules in mind:

Rule 2.2 OPEN FORUM

- D. The open forum is a limited public forum; all matters discussed in the open forum shall relate to the affairs of the City and items not currently on the current or advance Council agendas. No person shall be permitted to speak in open forum regarding items on the current or advance agendas, pending hearing items, or initiatives or referenda in a pending election. Individuals speaking during the open forum shall address their comments to the Council President and shall not use profanity, engage in obscene speech, or make personal comment or verbal insults about any individual.
- E. To encourage wider participation in open forum and a broad array of public comment and varied points of view from residents of the City of Spokane, no person shall be permitted to speak at the first open forum more often than once per calendar month. Any person may speak at the second open forum if they have not yet spoken in that meeting's first open forum or concerning any agenda item at that day's meeting, unless the meeting is that person's first address at open forum in that month.. There is no limit on the number of regular legislative agenda items on which a member of the public may testify, such as legislative items, special consideration items, hearing items, and other items before the City Council and requiring Council action that are not adjudicatory or administrative in nature, as specified in Rules 5.3 and 5.4.

Rule 2.7 SERVICE ANIMALS AT CITY COUNCIL MEETINGS

- B. Service animals must, at all times while present in a City Council meeting, be harnessed, leashed, or tethered, unless these devices interfere with the service animal's work or the individual's disability prevents using these devices, in which case, the individual must maintain control of the animal through voice, signal, or other effective controls.

Rule 5.3 PARTICIPATION OF MEMBERS OF THE PUBLIC IN COUNCIL MEETINGS

- A. Members of the public may address the Council regarding items on the Council's legislative agenda, special consideration items, hearing items, and other items before the City Council requiring Council action that are not adjudicatory or administrative in nature. This rule shall not limit the public's right to speak during the open forum.
- B. No member of the public may speak without first being recognized for that purpose by the Chair. Except for named parties to an adjudicative hearing, a person may be required to sign a sign-up sheet and provide their city of residence as a condition of recognition. In order for a Council member to be recognized by the Chair for the purpose of obtaining the floor, the Council member shall either raise a hand or depress the call button on the dais until recognized by the Council President.
- C. Each person speaking at the public microphone shall verbally identify themselves by name, city of residence, and, if appropriate, representative capacity.
- D. Each speaker shall follow all written and verbal instructions so that verbal remarks are electronically recorded and documents submitted for the record are identified and marked by the Clerk.
- E. In order that evidence and expressions of opinion be included in the record and that decorum befitting a deliberative process be maintained, no modes of expression not provided by these rules, including but not limited to demonstrations, banners, signs, applause, profanity, vulgar language, or personal insults will be permitted.
- F. A speaker asserting a statement of fact may be asked to document and identify the sources of the factual datum being asserted.

- G. When addressing the Council, members of the public shall direct all remarks to the Council President and shall confine remarks to the matters that are specifically before the Council at that time.
- H. When any person, including members of the public, City staff, and others, are addressing the Council, Council members shall observe the same decorum and process, as the rules require among the members inter se. That is, a Council member shall not engage the person addressing the Council in colloquy, but shall speak only when granted the floor by the Council President. All persons and/or Council members shall not interrupt one another. The duty of mutual respect set forth in Rule 1.2 and the rules governing debate set forth in Robert's Rules of Order, newly revised, shall extend to all speakers before the City Council. The City Council Policy Advisor and/or City Attorney shall, with the assistance of Council staff, assist the Council President to ensure that all individuals desiring to speak shall be identified, appropriately recognized, and provided the opportunity to speak.

Rule 5.4 PUBLIC TESTIMONY REGARDING LEGISLATIVE AGENDA ITEMS – TIME LIMITS

- A. The City Council shall take public testimony on all matters included on its legislative agenda, with those exceptions stated in Rule 5.4(B). Public testimony shall be limited to the final Council action. Public testimony shall be limited to three (3) minutes per speaker, unless, at their discretion, the Chair determines that, because of the number of speakers signed up to testify, less time will be needed for each speaker in order to accommodate all speakers. The Chair may allow additional time if the speaker is asked to respond to questions from the Council.
- B. No public testimony shall be taken on items on the Council's consent agenda, amendments to legislative agenda items, or procedural, parliamentary, or administrative matters of the Council, including amendments to these Rules.
- C. For legislative or hearing items that may affect an identifiable individual, association, or group, the following procedure may be implemented:
 - 1. Following an assessment by the Chair of factors such as complexity of the issue(s), the apparent number of people indicating a desire to testify, representation by designated spokespersons, etc., the Chair shall, in the absence of objection by the majority of the Council present, impose the following procedural time limitations for taking public testimony regarding legislative matters:
 - a. There shall be up to fifteen (15) minutes for staff, board, or commission presentation of background information, if any.
 - b. The designated representative of the proponents of the issue shall speak first and may include within their presentation the testimony of expert witnesses, visual displays, and any other reasonable methods of presenting the case. Up to thirty (30) minutes shall be granted for the proponent's presentation. If there be more than one designated representative, they shall allocate the allotted time between or among themselves.
 - c. Following the presentation of the proponents of the issue, three (3) minutes shall be granted for any other person not associated with the designated representative of the proponents who wishes to speak on behalf of the proponent's position.
 - d. The designated representative, if any, of the opponents of the issue shall speak following the presentation of the testimony of expert witnesses, visual displays, and any other reasonable methods of presenting the case. The designated representative(s) of the opponents shall have the same amount of time which was allotted to the proponents.
 - e. Following the presentation by the opponents of the issue, three (3) minutes shall be granted for any other person not associated with the designated representative of the opponents who wishes to speak on behalf of the opponents' position.
 - f. Up to ten (10) minutes of rebuttal time shall be granted to the designated
 - 2. In the event the party or parties representing one side of an issue has a designated representative and the other side does not, the Chair shall publicly ask the unrepresented side if they wish to designate one or more persons to utilize the time allotted for the designated representative. If no such designation is made, each person wishing to speak on behalf of the unrepresented side shall be granted three (3) minutes to present their position, and no additional compensating time shall be allowed due to the fact that the side has no designated representative.
 - 3. In the event there appears to be more than two groups wishing to advocate their distinct positions on a specific issue, the Chair may grant the same procedural and time allowances to each group or groups, as stated previously.
- D. The time taken for staff or Council member questions and responses there to shall be in addition to the time allotted for any individual or designated representative's testimony.

THE CITY OF SPOKANE



ADVANCE COUNCIL AGENDA

MEETING OF MONDAY, NOVEMBER 23, 2020

MISSION STATEMENT

**TO DELIVER EFFICIENT AND EFFECTIVE SERVICES
THAT FACILITATE ECONOMIC OPPORTUNITY
AND ENHANCE QUALITY OF LIFE.**

MAYOR NADINE WOODWARD

COUNCIL PRESIDENT BREEAN BEGGS

COUNCIL MEMBER KATE BURKE

COUNCIL MEMBER LORI KINNEAR

COUNCIL MEMBER KAREN STRATTON

COUNCIL MEMBER MICHAEL CATHCART

COUNCIL MEMBER CANDACE MUMM

COUNCIL MEMBER BETSY WILKERSON

**CITY COUNCIL CHAMBERS
CITY HALL**

**808 W. SPOKANE FALLS BLVD.
SPOKANE, WA 99201**

CITY COUNCIL BRIEFING SESSION

Council will adopt the Administrative Session Consent Agenda after they have had appropriate discussion. Items may be moved to the 6:00 p.m. Legislative Session for formal consideration by the Council at the request of any Council Member.

SPOKANE CITY COUNCIL BRIEFING SESSIONS (BEGINNING AT 3:30 P.M. EACH MONDAY) AND LEGISLATIVE SESSIONS (BEGINNING AT 6:00 P.M. EACH MONDAY) ARE BROADCAST LIVE ON CITY CABLE CHANNEL FIVE AND STREAMED LIVE ON THE CHANNEL FIVE WEBSITE. THE SESSIONS ARE REPLAYED ON CHANNEL FIVE ON THURSDAYS AT 6:00 P.M. AND FRIDAYS AT 10:00 A.M.

The Briefing Session is open to the public, but will be a workshop meeting. Discussion will be limited to Council Members and appropriate Staff and Counsel. There will be an opportunity for the expression of public views during the Open Forum at the beginning and the conclusion of the Legislative Agenda on any issue not relating to the Current or Advance Agendas, pending hearing items, or initiatives or referenda in a pending election.

ADDRESSING THE COUNCIL

- No member of the public may speak without first being recognized for that purpose by the Chair. Except for named parties to an adjudicative hearing, a person may be required to sign a sign-up sheet and provide their city of residence as a condition of recognition.
- Each person speaking at the public microphone shall verbally identify themselves by name, city of residency and, if appropriate, representative capacity.
- If you are submitting letters or documents to the Council Members, please provide a minimum of ten copies via the City Clerk. The City Clerk is responsible for officially filing and distributing your submittal.
- In order that evidence and expressions of opinion be included in the record and that decorum befitting a deliberative process be maintained, no modes of expression including but not limited to demonstrations, banners, signs, applause, profanity, vulgar language or personal insults will be permitted.
- A speaker asserting a statement of fact may be asked to document and identify the source of the factual datum being asserted.

SPEAKING TIME LIMITS: Unless deemed otherwise by the Chair, each person addressing the Council shall be limited to a three-minute speaking time.

CITY COUNCIL AGENDA: The City Council Advance and Current Agendas may be obtained prior to Council Meetings from the Office of the City Clerk during regular business hours (8 a.m. - 5 p.m.). The Agenda may also be accessed on the City website at www.spokanecity.org. Agenda items are available for public review in the Office of the City Clerk during regular business hours.

AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION: The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. The Spokane City Council Chamber in the lower level of Spokane City Hall, 808 W. Spokane Falls Blvd., is wheelchair accessible and also is equipped with an infrared assistive listening system for persons with hearing loss. Headsets may be checked out (upon presentation of picture I.D.) at the City Cable 5 Production Booth located on the First Floor of the Municipal Building, directly above the Chase Gallery or through the meeting organizer. Individuals requesting reasonable accommodations or further information may call, write, or email Human Resources at 509.625.6383, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or msteinolfson@spokanecity.org. Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.

If you have questions, please call the Agenda Hotline at 625-6350.

BRIEFING SESSION

(3:30 p.m.)

(Council Chambers Lower Level of City Hall)
(No Public Testimony Taken)

Roll Call of Council

Council Reports

Staff Reports

Committee Reports

Advance Agenda Review

Current Agenda Review

ADMINISTRATIVE SESSION

CONSENT AGENDA

REPORTS, CONTRACTS AND CLAIMS

RECOMMENDATION

- | | | |
|--|----------------|------------------------------|
| 1. Spokane Airport Board 2021 Budget.
Dave Armstrong and Larry Krauter | Approve | FIN 2020-0002 |
| 2. Purchases by Fleet Operations for the Wastewater
Department from: | Approve
All | |
| a. Kenworth Sales (Spokane, WA) of a tank truck
using the Sourcewell Contract # 081716-
KTC—\$185,419 (incl. tax). | | OPR 2020-0835 |
| b. Freightliner (Spokane, WA) of a Service
Truck—\$82,287.46 (incl. tax). | | OPR 2020-0836
BID 4394-17 |
| c. Columbia Ford (Longview, WA) of a Service
Truck using Washington State Contract
05916—\$60,499.12 (incl. tax). | | OPR 2020-0837 |

Mike Lowdon

- | | | |
|--|------------------------------------|------------------------------|
| 3. One-year Contract Renewals for Fleet Services with: | Approve
All | |
| a. Racom Corporation for the Installation of Radio and Electrical Equipment—\$150,000. | | OPR 2019-0885
RFP 5153-20 |
| b. Wingfoot Commercial Tire Systems, LLC using Washington State Contract 00519—\$150,000. | | OPR 2019-0855 |
| 4. Local Area A&E Professional Services Consultant Agreements for 2021-20202 Non-Federal Aid Projects with: | Approve | |
| a. Tierra Right of Way, Ltd. (Spokane, WA) for Historic Resource On-call Services—not to exceed \$200,000.(Various Neighborhoods) | | OPR 2020-0838
ENG 2021060 |
| b. Budinger & Associates (Spokane, WA) for Geotechnical Engineering On-call Services—not to exceed \$400,000. (Various Neighborhoods) | | OPR 2020-0839
ENG 2021061 |
| c. CommonStreet Consulting LLC (Spokane, WA) for Real Estate On-call Consulting Services—not to exceed \$200,000. (Various Neighborhoods) | | OPR 2020-0840
ENG 2021062 |
| d. Parametrix, Inc. (Spokane, WA) for On-call Surveying Services—not to exceed \$150,000. (Various Neighborhoods) | | OPR 2020-0841
ENG 2021063 |
| e. Geo Engineers Inc. (Spokane, WA) for Geotechnical Engineering On-call Services—not to exceed \$800,000. (Various Neighborhoods) | | OPR 2020-0842
ENG 2021061 |
| 5. Recommendation to list the Webster Building, 415 West Sprague Avenue, on the Spokane Register of Historic Places. | Approve | OPR 2020-0843 |
| 6. Report of the Mayor of pending claims and payments of previously approved obligations, including those of Parks and Library, through _____, 2020, total \$_____, with Parks and Library claims approved by their respective boards. Warrants excluding Parks and Library total \$_____. | Approve &
Authorize
Payments | CPR 2020-0002 |
| 7. City Council Meeting Minutes: _____, 2020. | Approve
All | CPR 2020-0013 |

EXECUTIVE SESSION

(Closed Session of Council)

(Executive Session may be held or reconvened during the 6:00 p.m. Legislative Session)

CITY COUNCIL SESSION

(May be held or reconvened following the 3:30 p.m. Administrative Session)

(Council Briefing Center)

This session may be held for the purpose of City Council meeting with Mayoral nominees to Boards and/or Commissions. The session is open to the public.

LEGISLATIVE SESSION

(6:00 P.M.)

(Council Reconvenes in Council Chamber)

WORDS OF INSPIRATION

PLEDGE OF ALLEGIANCE

ROLL CALL OF COUNCIL

ANNOUNCEMENTS

(Announcements regarding Changes to the City Council Agenda)

BOARDS AND COMMISSIONS APPOINTMENTS

(Includes Announcements of Boards and Commissions Vacancies)

APPOINTMENTS

RECOMMENDATION

Spokane Airport Board: One Reappointment.	Confirm	CPR 1982-0071
Lodging Tax Advisory Committee: One Reappointment	Confirm	CPR 2000-0031

ADMINISTRATIVE REPORT

COUNCIL COMMITTEE REPORTS

(Committee Reports for Finance, Neighborhoods, Public Safety, Public Works, and Planning/Community and Economic Development Committees and other Boards and Commissions)

OPEN FORUM – WILL NOT BE HELD

LEGISLATIVE AGENDA**NO SPECIAL BUDGET ORDINANCES****NO EMERGENCY ORDINANCES****NO RESOLUTIONS****FINAL READING ORDINANCES**(Require Four Affirmative, Recorded Roll Call Votes)

- ORD C35972** Relating to application file Z19-499COMP amending Map LU 1, Land Use Plan Map, of the City's Comprehensive Plan from "Residential 4-10" to "General Commercial" for approximately 0.85 acres located at 3001, 3011, and 3027 E Liberty Avenue (parcels 35033.1304, 35033.1305, and 35033.1306) and amending the Zoning Map from "Residential Single Family (RSF)" to "General Commercial (GC-70)". (By a vote of 9 to 0, the Plan Commission recommends approval.)
Kevin Freibott
- ORD C35973** Relating to application file Z19-501COMP amending Map LU 1, Land Use Plan Map, of the City's Comprehensive Plan from "Residential 4-10" to "General Commercial" for approximately 0.51 acres located at 6204 Nevada Street and 1015 E Decatur Avenue (parcels 36321.0209 and 36321.0210) and amending the Zoning Map from "Residential Single Family (RSF)" to "Community Business (CB-55)". (By a vote of 9 to 0, the Plan Commission recommends denial.)
Kevin Freibott
- ORD C35974** Relating to application file Z19-502COMP amending Map LU 1, Land Use Plan Map, of the City's Comprehensive Plan from "Residential 4-10" to "Office" for approximately 0.61 acres located at 3207 and 3203 E 29th Avenue and 2820 and 2826 S Ray Street (parcels 35273.0219, 35273.0220, 35273.0305, and 35273.0306) and amending the Zoning Map from "Residential Single Family (RSF)" to "Office (O-35)". (By a vote of 9 to 0, the Plan Commission recommends approval of parcels 35273.0305 and 35273.0306 and denial of parcels 35273.0219 and 35273.0220.)
Kevin Freibott
- ORD C35975** Relating to application file Z19-503COMP amending Map LU 1, Land Use Plan Map, of the City's Comprehensive Plan from "Residential 4-10" to "Residential 15-30" for approximately 10.3 acres located at 3227 E 53rd Avenue and 5106 S Palouse Highway (parcels 34032.9044, 34032.9093, 34032.9094) and amending the Zoning Map from "Residential Single

Family (RSF)” to “Residential Multi-Family (RMF)”. (By a vote of 8 to 1, the Plan Commission recommends approval.)

Kevin Freibott

ORD C35976

Relating to application file Z19-504COMP amending Map LU 1, Land Use Plan Map, of the City’s Comprehensive Plan from “Residential 4-10” to “Residential 15-30” for approximately 2.2 acres located at 3004 W 8th Avenue (parcels 25234.0902 and 25234.6501) and amending the Zoning Map from “Residential Single Family (RSF)” to “Residential Multi-Family (RMF)”. (By a vote of 9 to 0, the Plan Commission recommends approval.)

Kevin Freibott

ORD C35977

Relating to application file Z19-505COMP amending Map LU 1, Land Use Plan Map, of the City’s Comprehensive Plan from “Residential 4-10” to “Residential 15-30” for approximately 0.16 acres located at 1117 W 10th Avenue (parcel 35193.1405) and amending the Zoning Map from “Residential Single Family (RSF)” to “Residential Multi-Family (RMF)”. (By a vote of 7 to 0 and 1 abstention, the Plan Commission recommends denial.)

Kevin Freibott

ORD C35978

Relating to proposal file Z20-019COMP amending Comprehensive Plan Map TR-5, Proposed Bike Network Map, in various locations, and amending the text of Appendix D to the Comprehensive Plan to update terminology relating to protected bike lanes and to update map references. (By a vote of 8 to 0, the Plan Commission recommends approval.)

Kevin Freibott

ORD C35979

Relating to proposal file Z20-042COMP amending Comprehensive Plan Map TR-12, Arterial Network Map, in various locations throughout the city together with corresponding changes to the official Arterial Street Map in SMC 12.08.040. (By a vote of 9 to 0, the Plan Commission recommends approval.)

Kevin Freibott

ORD C35980

Relating to proposal file Z20-045COMP amending the text of Chapter 4, Transportation, of the Comprehensive Plan to discuss safety needs for at-grade railroad crossings. (By a vote of 9 to 0, the Plan Commission recommends approval.)

Kevin Freibott

FIRST READING ORDINANCES

(No Public Testimony Will Be Taken)

ORD C35981

Expanding the number of authorized golf cart zones in Spokane; amending sections 16A.63.010, 16A.63.020, and 16A.63.030 of the Spokane Municipal Code. (Council Sponsor: Council Member Stratton)

Council Member Stratton

ORD C35982

Imposing a sales and use tax for the construction, acquisition, and rehabilitation of attainable housing and for housing-related supportive services; and enacting a new chapter 07.08C of the Spokane Municipal Code. (Counsel Sponsors: Council Members Wilkerson, Stratton and Burke) **Brian McClatchey**

- ORD C35983 Relating to Design Standards; amending SMC sections 17A.020.010, 17A.020.020, 17A.020.030, 17A.020.130, 17A.020.160, 17A.020.190 and 17A.020.200 of the Spokane Municipal Code.
Igna Note
- ORD C35984 Relating to street development standards for the Unified Development Code; amending sections 17C.200.050, 12.01.010, and 12.02.0202 of the Spokane Municipal Code. (Council Sponsor: Council Member Mumm)
Igna Note
- ORD C35985 Relating to Design Standards, amending SMC sections 17H.010.030, 17H.010.050, 17H.010.060, 17H.010.070, 17H.010.120, 17H.010.140, 17H.010.160, 17H.010.180, 17H.010.190, 17H.010.200, 17H.010.210, 17H.010.220, 17H.010.230, 17H.010.240, 17H.010.250 and 17H.010.260 of the Spokane Municipal Code. (Council Sponsor: Council Member Mumm)
Igna Note
- ORD C35986 Revising the Design Standards Chapter 3 – Streets, Alleys, and Bikeways for the City of Spokane (and rescinding the Clear Zone Policy No. 0370-0804 included in Appendix F of the Design Standards). (Council Sponsor: Council Member Mumm)
Igna Note
- ORD C35987 Relating to Parks; amending Section 12.06A.040 of the Spokane Municipal Code concerning park rules and regulations.
Giacobbe Byrd

FURTHER ACTION DEFERRED**SPECIAL CONSIDERATIONS**

(If there are items listed you wish to speak on, please sign your name on the sign-up sheets in the Chase Gallery.)

RECOMMENDATION

- | | |
|--|----------------|
| S1. Purchases by Fleet Operations for Police Department using WA State Contract #05916 of: | Approve
All |
| a. 2 Police K8s from Columbia Ford (Longview, WA)—\$97,254.31 (incl. tax). | OPR 2020-0781 |
| b. 2 Police Tahoes from Bud Clary Chevrolet (Longview, WA)—\$94,377.38 (incl. tax). | OPR 2020-0782 |

(Deferred from November 9, 2020, Agenda)

Eric Olsen

HEARINGS

(If there are items listed you wish to speak on, please sign your name on the sign-up sheets in the Chase Gallery.)

RECOMMENDATION

- | | | | |
|-----|---|---|---------------|
| H1. | Continuation of Public Hearing on 2021 Proposed Budget. (Continued from November 9, 2020, Agenda.)
Paul Ingiosi | Hold
Hearing.
then
Continue
to 11/30/20 | FIN 2020-0001 |
|-----|---|---|---------------|

Motion to Approve Advance Agenda for November 23, 2020
(per Council Rule 2.1.2)

OPEN FORUM (CONTINUED) – WILL NOT BE HELD

ADJOURNMENT

The November 23, 2020, Regular Legislative Session of the City Council is adjourned to November 30, 2020.

NOTES



Agenda Sheet for City Council Meeting of:
11/23/2020

Date Rec'd	11/11/2020
Clerk's File #	FIN 2020-0002
Renews #	

Submitting Dept	AIRPORTS	Cross Ref #	
Contact Name/Phone	DAVE ARMSTRONG 455-6448	Project #	
Contact E-Mail	DARMSTRONG@SPOKANEAIRPORTS.NET	Bid #	
Agenda Item Type	Report Item	Requisition #	
Agenda Item Name	SPOKANE AIRPORT BOARD 2021 BUDGET		

Agenda Wording

Spokane Airport Board 2021 Budget.

Summary (Background)

On November 19, 2020 the Spokane Airport Board approved the 2021 Budget for Spokane International Airport, the Airport Business Park and Felts Field. The budget does not require City or County financial support; however, approval is necessary to meet conditions of the Joint Inter-Local Agreement. The 2021 operating budget is \$39,021,022 and the capital budget is \$42,445,000. The total 2021 budget is \$81,466.022.

Lease? NO	Grant related? NO	Public Works? NO
<u>Fiscal Impact</u>		<u>Budget Account</u>
Select \$		#
Select \$		#
Select \$		#
Select \$		#
<u>Approvals</u>		<u>Council Notifications</u>
<u>Dept Head</u>	ARMSTRONG, DAVE	<u>Study Session\Other</u>
<u>Division Director</u>		<u>Council Sponsor</u>
<u>Finance</u>	BUSTOS, KIM	<u>Distribution List</u>
<u>Legal</u>	PICCOLO, MIKE	lkrauter@spokaneairports.net
<u>For the Mayor</u>	ORMSBY, MICHAEL	darmstrong@spokaneairports.net
<u>Additional Approvals</u>		thart@spokaneairports.net
<u>Purchasing</u>		

Spokane Airports 2021 Budget

Spokane International Airport / Airport Business Park/ Felts Field

WHEN IT'S TIME TO FLY



Now Offering **Nonstop Service** to
16 DESTINATIONS Throughout the U.S.

Alaska

American Airlines

DELTA

FRONTIER

Southwest

UNITED





Spokane Airport Board

Spokane International Airport / Airport Business Park / Felts Field

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Spokane International Airport
GEG – GENERATING ECONOMIC GROWTH

AIRPORT BUSINESS PARK



Spokane Airport Board

Ezra Eckhardt (Chair)
K. Collins Sprague (Vice Chair)
Al French (Secretary)
Breean Beggs
Max Kuney
Nancy Vorhees
Jennifer West

Lawrence J. Krauter, CEO
Brian Werst, General Counsel



Section II. Introduction

The SPOKANE AIRPORT BOARD (Board), operates Spokane International Airport (GEG), the Airport Business Park (ABP), and Felts Field (SFF), collectively referred to as the SPOKANE AIRPORTS (Airport), under and pursuant to the Constitution and Laws of the State of Washington, including Ch. 14.08 RCW, RCW 14.08.200 that certain Joint Resolution and Operating Agreement of Spokane County and the City of Spokane dated October 7, 2019.

The agreement intends the expenses of operating Spokane International Airport, Airport Business Park, and Felts Field shall be paid, *to the maximum extent possible*, from the operating revenues of each area (emphasis added).

The Airport does not receive any programmed general fund appropriations from either the County or City of Spokane, nor does it receive any local tax revenues. Operating funds come from user fees, tenant rents in varying forms along with airline landing fees. Capital funds come from loans, federal and state grants and agreements, facility charges through airlines and rental car agencies along with airport funds generated through operations.

As of December 31, 2019, the Airport employed approximately 160 full and part-time employees responsible for providing access to the global air service network by managing, developing, maintaining and promoting the Airport. Due to the negative impacts of the COVID-19 Pandemic on the aviation industry, staffing levels have been reduced to approximately 125 full and part-time employees. Employees work diligently to provide quality facilities and services along with a high quality customer experience.

The 2021 budget provides funds to accomplish goals while keeping the cost to airlines competitive, funding important capital projects that will improve efficiency, safety and security, maintaining aging facilities while designing new facilities and improving customer service. Through these initiatives, the budget provides funds for activities that provide economic impact and stimulus by supporting existing, and creating new, jobs in the region.

AIRLINE OPERATING AGREEMENT (AOA)

This 2021 Budget is prepared based on the airline agreement with signatory airlines originally dated January 1, 2010. This agreement was extended, mutually by the Airport and the airlines through December 31, 2015. The Airport and airlines extended the agreement again, with minor modifications, for additional years to provide time to continue negotiating a modernized AOA. The agreement, as amended, now expires on December 31, 2021.

The Airport Airline Affairs Committee (AAAC) convened on October 26, 2020 to discuss the 2021 rates and charges. The AAAC is comprised of Airport staff members along with representatives of the Signatory Airlines and the major Cargo Carriers. The Airline members of

the AAAC do not specifically approve the full Airport Budget, however they do provide input for consideration in development of rates and charges.

The amended continuing agreement, on which the 2021 Budgets is based, utilizes a residual rate setting method. This approach is common, but not universal, among US airport operators. A pure residual rate setting method effectively applies all operating revenues and expenses into the models that set rates charged to airlines.

During discussions and consultations, it was agreed the Airport would continue to apply a portion of operating net revenue, rather than all net operating revenue, effectively allowing the Airport to retain a larger amount of generated revenue to fund capital projects.

In 2020, there were significant decreases in passenger activity and aircraft operations due to the actions of federal, state, and local governments in response to the global COVID-19 pandemic. Activity is slowly improving, with a projection to end 2020 at approximately 50% of 2019 levels. In developing the 2021 budget, the goal is to stabilize and position the Airport for a pandemic recovery while continuing as an attractive location for adding new airline service and maintaining current destinations.

THE BUDGET PROCESS

The budget process begins each year in July as the staff reviews the expenditures of the first six months of the current fiscal year. A budget packet was distributed including a budget calendar and general directions for each department's budget submittal. The Airport utilizes a budget process in which each expenditure line item is evaluated on its own merit each year. Specific department budgets are then developed to identify resources necessary to meet the daily functions of operating the airports and implementing the necessary airport improvement projects.

The Airport Finance Department works with a third party consultant to develop the Airport's Rates and Charges structure and educate staff on department allocations in preparation for modernizing the AOA as the current extended agreement approaches expiration.

Departments submitted their budgets which were loaded into the Rates and Charges Model (see Section V) to calculate the upcoming year's Terminal Rental Rates and Landing Fees. Each department's initiatives are reviewed before the proposed draft is completed and submitted for review and comment by the Airport Board.

The Capital Improvement Program is also evaluated to examine its effect on rates and charges. Future period major capital projects are added to the modeling as most of these projects and expenditures affect budget periods beyond the current period under examination.

FINANCIAL SUMMARY

The table below summarizes the 2021 Consolidated Spokane Airport Board Budget. Section X, beginning page 24, and various exhibits throughout this presentation will focus on the individual details of the summary shown.

SPOKANE AIRPORT BOARD 2021 BUDGET SUMMARY			
<u>2021</u>	<u>Operating</u>	<u>Capital</u>	<u>Total</u>
GEG	\$36,130,435	\$40,455,000	\$76,585,435
ABP	1,146,695	1,500,000	2,646,695
Felts Field	<u>1,743,892</u>	<u>490,000</u>	<u>2,233,892</u>
2021 Consolidated	<u>\$39,021,022</u>	<u>\$42,445,000</u>	<u>\$81,466,022</u>
% Change vs 2020	-15.7%	-0.9%	-8.5%

<u>2020</u>			
GEG	\$43,479,547	\$40,919,000	\$84,398,547
ABP	1,206,695	620,000	1,826,695
Felts Field	<u>1,577,180</u>	<u>1,278,000</u>	<u>2,855,180</u>
2020 Consolidated	<u>\$46,263,422</u>	<u>\$42,817,000</u>	<u>\$89,080,422</u>

The Total Spokane Airport Board 2021 Budget presented for approval is \$81,466,022 representing an 8.5% decrease from the 2020 Budget. This change is due to a 15.7% decrease in the Operating Budget and a 0.9% decrease in the Capital Budget. The decrease in the Operating Budget is directly related to the reduction in available revenues, and therefore expenses, due to the reduction in passenger activity in response to the global pandemic. Fluctuations in the Capital Budget occur due to the timing of Federal and Local funding streams and the timing of construction progress of individual projects.

Consolidated Operating Revenues are forecast to decrease by 15.7% from the 2020 budget to \$39.0 million. Included in operating revenues in all three areas are estimated reimbursements from the Coronavirus Aid, Relief, and Economic Security (CARES) Act. SIA operating revenues are forecast to decrease by 14.8% from the 2020 budget, due to a decrease in passenger activity which affects revenues from airline landing fees, concession and rental car activity. Included in the 2021 revenue decrease is an 8.6% decrease in rental rates for terminal tenants, and a 6.3% decrease in the 2021 landing fee rate. The fifth year of refining cost tracking and allocations of indirect costs across direct cost centers (see Section IV, page 9) is showing positive results in obtaining the proper inputs to the airline rate structure.

The **Parking / Ground Transportation** cost center has the largest impact due to the decline in passenger activity in 2020 and 2021. 2020 is forecast to decrease by 50% compared to budget, and will be followed by a 44% reduction in 2021 as compared to the 2020 budget.

Included in the 2021 Budget is a rate structure adjustment due to the temporary closing of certain surface parking lots. The per-hour rate remains the same at \$2 per hour up to the daily maximum for each available lot. Page 31 shows the 2021 vehicle parking lot rates.

An annual adjustment to the trip fee rate in Ground Transportation was memorialized in 2017 for the years 2018 – 2021. The rate for 2021 is set at \$1.75 per trip which is the same per trip rate of 2020.

Consolidated Operating Expenses, including depreciation and debt service, are projected to decrease 15.7% to \$39.0 million. Operating expenses, not including depreciation and debt service, are projected to decrease 21.9% over the 2020 budget to \$27.7 million. The budget model is forecasting passenger growth over projected 2020 levels, and utilizes a conservative increase of 3.2% in 2021 over the estimated total activity of 2020.

The 2021 **Capital Program** represents a 0.9% decrease from the 2020 budget. The total planned capital expenditures in 2021 are \$42.4 million.

Capital sources of funds are somewhat dependent on the Federal Grant process through the Airport Improvement Program (AIP), other funds obtained through the Department of Transportation (DOT), and the Passenger Facility Charge (PFC) Application process. The majority of the 2021 projects scheduled are funded for the upcoming year through AIP Grant applications, a DOT BUILD grant, pre-collected PFCs and Customer Facility Charges (CFCs) on approved projects. The Airport also uses funds from general operations to the extent expenditures do not reduce the reserve amount below a targeted level.

2021 capital projects at SIA include construction of a Transload rail-truck facility, Airport Drive Realignment (Phase 2), certain terminal rehabilitation projects which will enable future terminal projects, replacement of in-line Explosive Detection Screening (EDS) devices, construction of a non-leased aircraft parking ramp, passenger boarding bridge refurbishments, EV charging stations, public Wi-Fi rehabilitation, and Rental Car Facility improvements. Airport Business Park capital projects are planned to include rehabilitation of roadways. Felts Field planned projects include a continuing Master Plan update. As noted above, these projects are funded by DOT and AIP grants, PFCs, a Transportation Security Administration (TSA) agreement, CFCs, and Airport general, including CARES, funds. The DOT and AIP grants contain a matching provision from the Airport's cash. Also planned are replacement of aging equipment and vehicles to be funded with current year operating funds or those funds earmarked in prior years for projects that will be carried over to 2021.

CUSTOMER SERVICE INITIATIVES

As a service to the Airport's travelers, the past few years have seen significant construction and general maintenance in parking lots. Revenue control systems have been recently upgraded to provide secure, contactless, transaction processing for travelers.

Other terminal modifications have been enacted to enhance safe and sanitary conditions for travelers along with physical distancing markers and acrylic barriers throughout the facility.

Construction of a convenience store pad along with a return-to-terminal circulation roadway was completed in the fall of 2018. The convenience store pad is able to house a national branded convenience store and gas station.

A privately owned hotel near the terminal building opened in the spring of 2020 providing an additional lodging option on Airport property.

The Airport continues to provide access to TSA Pre-check in the enrollment center located near the Airport Police offices in the C-Concourse.



Section III. Airline Activity Forecast

PASSENGER TRAFFIC

The Airport has seen enplanement and total passenger increases each year since 2013 to 2019. Through September 2020, enplanements are 52.7% below the same period in 2019. For budgetary purposes, 2020 enplaned passengers are projected to decrease 52.6% from 2019; and increase 3.2% over the projected 2020 total.

The following table shows actual passenger activity by airline for 2019 with estimates for 2020 and 2021, which are incorporated into the 2021 Budget. The table also shows the current breakdown of passengers traveling out of the two terminals.

Traffic Activity

Spokane International Airport

(for the 12 months ending December 31; numbers in thousands)

	Actual 2019	Projected 2020	Budget 2021
Enplaned Passengers			
Alaska	706	332	343
Allegiant (non-sig)	0	-	-
Delta	501	214	221
Frontier (non-sig)	35	17	18
Southwest	464	255	263
American	123	68	70
United	229	91	94
Others	3	0	0
Total	2,061	978	1,009
Enplaned Passengers	2,061	978	1,009
% Change	3.1%	-52.6%	3.2%
By Terminal			
Terminal C	828	400	413
Terminal A&B	1,233	578	596
Total	2,061	978	1,009

AIRCRAFT LANDED WEIGHT FORECAST

The Airport collects landing fees from landing air carriers based on an aircraft's Maximum Gross Landed Weight (MGLW). The costs of operating the airfield are recovered through the Landing Fee Rate multiplied by the MGLW of each aircraft. The accuracy of the landed weight forecast of each air and cargo carrier has an impact on the Landing Fee Rate. Section IV Cost Centers, Allocations, Rate Making and CPE on page 9 examines the methodology of rate making under a Residual Airline Agreement such as the agreement at GEG. Simply stated, the higher the landed weight estimate, the lower the budgeted unit cost rate for the upcoming year.

The following table shows 2019 actual passenger and cargo landed weight along with 2020 and 2021 estimates incorporated into the 2021 Budget.

Traffic Activity

Spokane International Airport

(for the 12 months ending December 31; numbers in thousands)

	Actual 2019	Projected 2020	Budget 2021
Landed Weight			
Passenger Airlines			
Alaska	741	471	542
Allegiant (non-sig)	1	-	-
Delta	554	350	403
Frontier (non-sig)	33	24	23
Southwest	513	416	395
American	142	99	113
United	247	130	124
Others	7	2	2
Cargo			
Signatory	429	385	405
Non-signatory	66	123	48
Total	2,733	2,000	2,055
% Change	1.0%	-26.8%	2.7%



Section IV. Cost Centers, Allocations, Rate Making and CPE

COST CENTERS

The Airport is currently organized with seven direct cost centers with a goal to continue to refine the cost centers and allocations to them each year to provide better forecasts for rate making. The direct cost centers are Airfield (including Operations and the Fuel Facility), Parking and Ground Transportation, Other Buildings and Grounds, GEG Terminal, Rental Car Facilities, the ABP and Felts Field. There are five indirect cost centers in Planning and Engineering, Airport Rescue and Fire Fighting (ARFF), Information Technology, Police / Dispatch, and Airport Administration.

ALLOCATIONS

To develop rates, the expenses from four of the indirect costs centers, excluding administration, are allocated to the direct costs centers based on an analysis of the staff hours dedicated to a center along with the budgeted costs within each indirect cost center. After those indirect costs are allocated to the direct cost centers, administration is allocated based on the total actual direct and indirect costs for each direct cost center. The allocation percentages for 2020 and 2021 are shown below. These allocation rates are reviewed annually and potentially revised at the end of each review period to reflect actual operations and maintenance for all of the facilities.

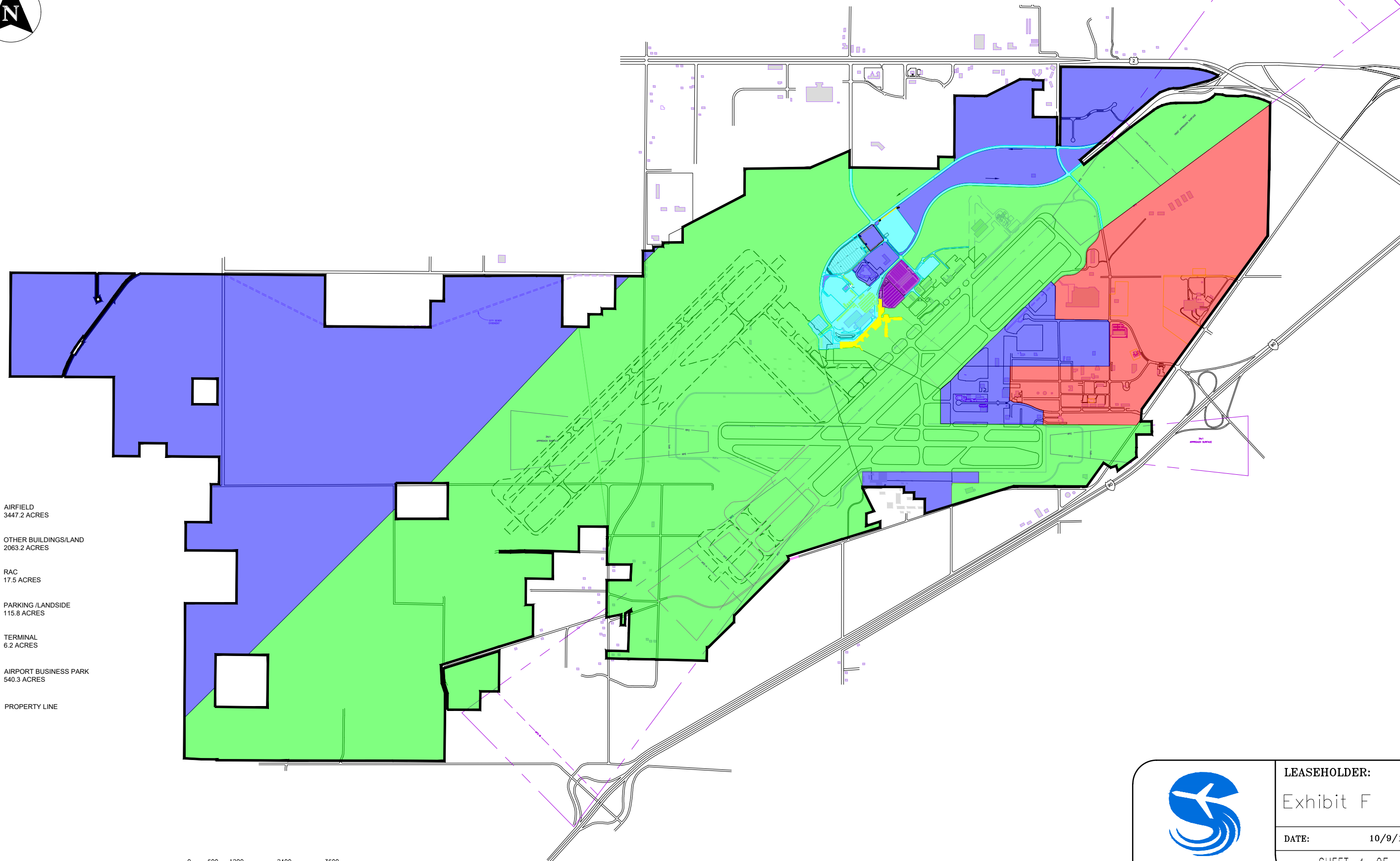
Indirect Allocation Percentages Spokane Airport Board

	Airfield	Fuel	Terminal	Landside	OB & G	ABP	Felts
Indirect Allocations							
Airfield Maintenance	65.0%			20.0%	10.0%		5.0%
Terminal Maintenance	0.0%		80.0%	20.0%			
Operations	90.0%		2.5%	2.5%			5.0%
Fuel	100.0%						
Engineering	40.0%	1.0%	40.0%	10.0%	1.0%	1.0%	7.0%
IT	25.0%		25.0%	25.0%	18.0%	2.0%	5.0%
ARFF/Fire	73.3%	3.2%	15.9%	0.9%	4.6%	2.1%	
Police	1.0%		80.0%	15.0%	1.5%	2.5%	
Communications	1.0%		80.0%	15.0%	1.5%	2.5%	
Airport Business Park						100.0%	
Felts Field							100.0%



LEGEND:

- AIRFIELD
3447.2 ACRES
- OTHER BUILDINGS/LAND
2063.2 ACRES
- RAC
17.5 ACRES
- PARKING /LANDSIDE
115.8 ACRES
- TERMINAL
6.2 ACRES
- AIRPORT BUSINESS PARK
540.3 ACRES
- PROPERTY LINE



0 600 1200 2400 3600

SCALE: 1" = 1200'

NOTE: SCALE SHOWN IS FOR 22 X 34 EXHIBITS, IF PLOTTED
ON 11 X 17 SCALE IS HALF SIZE.



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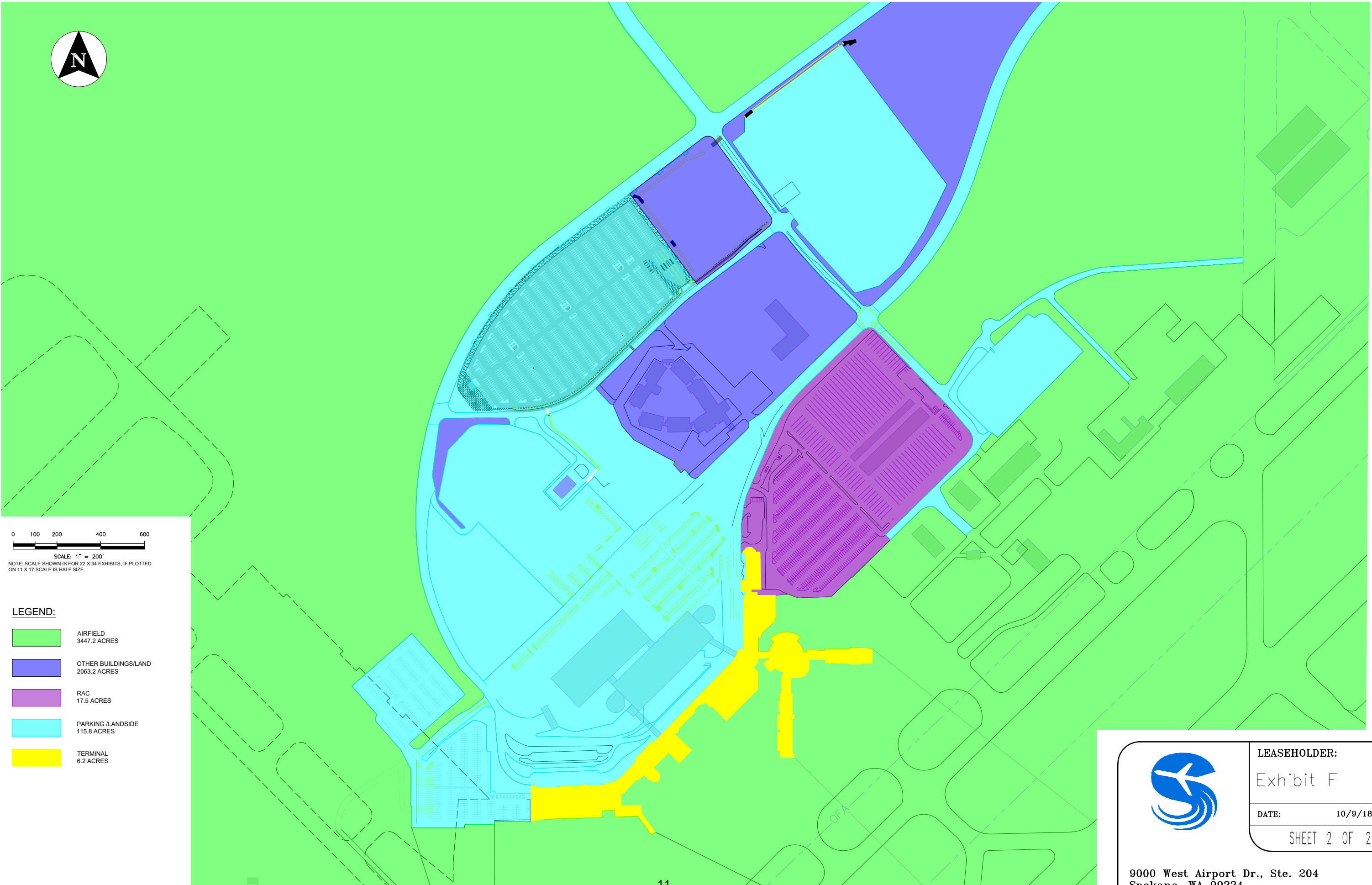
Exhibit F

DATE: 10/9/18

SHEET 1 OF 2

9000 West Airport Dr., Ste. 204
Spokane, WA 99224

O:\0420-Spokane\DCI-Civil\ENGINEERING\2018\18-42-0007-SIA-TASK-ORDER-A-E-SERVICES\AE18-043 COST-CENTER-MAP-UPDATE\120125-C14-064_SIA.dwg, 10/9/2018 12:03:17 PM, David Landrus, None



LEASEHOLDER:

Exhibit F

DATE: 10/9/18

SHEET 2 OF 2

9000 West Airport Dr., Ste. 204
Spokane, WA 99224

RATE MAKING

The Airport operates under an existing AOA with a residual ratemaking methodology where generally, with some exceptions, all revenues are shared with the signatory air carriers. Under the current AOA there is no specified methodology for each rate making calculation, however there are federal guidelines for establishing rates. Currently the signatory airlines participate, in a consultation role only, in the budget and rate setting process and agreed to modifications to the rate making methodology outlined in the Introduction section under Airline Operating Agreement (AOA).

Terminal Rental Rate Calculation

The existing AOA does not prescribe a methodology for calculating terminal building rentals, fees, and charges. However, exhibits provided annually to the air carriers with updated rates prescribes the fees and charges and the relative weighting of such charges. The Airport uses a cost center residual methodology to calculate a cost recovery terminal building rental rate.

The Terminal Building Cost includes allocable M&O Expenses, debt service (net of allocable PFC revenues), any debt service coverage, depreciation charges (net of bond-funded projects, grants, and PFC revenues), and M&O Reserve deposit requirements. The resulting Terminal Building Cost is reduced by Terminal Concession Revenues, Non-airline Terminal Rentals, a percentage of the estimated surplus revenue generated from the Parking/Landside cost center (with the remainder flowing into the airport residual landing fee rate), Other Terminal Payments, and Loading Bridge Fees to yield the Net Terminal Building Requirement.

The Net Terminal Building Requirement is divided by Rented Space weighted by the weight classifications to derive the Terminal Building Rental Rate per square foot per year. The current year terminal rental rate for Class 1 space (generally public areas) is \$59.19 per square foot per annum (sfpa), an decrease from \$64.73 (8.6%) sfpa in 2020. The calculation of the Terminal Rental Rate is displayed in Section V on page 15.

Landing Fee Calculation

The methodology for calculating the landing fee rate is based on an airport residual approach where the total cost of SIA is credited with airline terminals rentals, other airline fees and charges, non-signatory airline landing fees, and non-airline revenues to yield the landing fee revenue requirement. The landing fee requirement is then divided by signatory airline landed weight forecast to derive the landing fee rate per 1,000 pound unit.

More specifically, the methodology consists of the following steps:

1. The SIA rate base includes M&O Expenses, debt service, debt service coverage, depreciation charges (net of grants and PFC revenues), and the M&O Expense Reserve deposit requirement. The rate base also includes an amount to provide sufficient funding for the Airport's share of funding the CIP.

2. The Airfield Requirement is then reduced by airline revenues other than landing fees and non-airline revenues, and the prior year carry forward surplus (deficit) to yield the Landing Fee Requirement.

3. The Landing Fee Requirement is divided by forecast Total Landed Weight of passenger and cargo carriers (as weighted for premiums) to derive the Signatory Airline Landing Fee Rate per 1,000-pound unit.

The 2021 landing fee rate is \$2.10 per 1,000 pounds of landed weight, reduced from \$2.24 / 1,000 pounds (-6.3%). The calculation of the Landing Fee Rate is displayed in Section V on page 15.

COST PER ENPLANEMENT (CPE)

The calculations of the Terminal Rent Rate and the Landing Fee result in charges to the air carriers. Those charges contribute to both Terminal and Airfield Revenue. One measure of the cost to operate at an airport by an airline is the Cost Per Enplanement (CPE) calculation.

The average CPE represents the net cost incurred by the commercial airlines based on their regular operations at the Airport. The table below shows the CPE 2019 along with estimates for 2020 and 2021. The 2020 estimated CPE is \$10.70 as compared to a budgeted CPE of \$6.32 which was presented in the 2020 Budget. Although passenger activity has declined by 50%, the efforts to contain costs and apply CARES funds has minimized the increase in the 2020 CPE. This CPE estimate remains lower than comparable sized airports. Section IX on page 22 shows comparisons of the Airport's CPE and other metrics with other airports of similar size and destination airports.

Airline Cost per Enplaned Passenger

Spokane International Airport

(for the 12 months ending December 31; numbers in thousands except rates)

	Actual 2019	Projected 2020	Budget 2021
Passenger Airline Payments			
Passenger Airline Signatory Landing Fees	\$ 5,186	\$ 3,106	\$ 3,304
Terminal Building Rent	6,751	6,916	6,216
Loading Bridge Fees	247	260	262
RON and Airfield Fees	260	112	115
Subtotal	\$ 12,444	\$ 10,394	\$ 9,897
Nonsignatory Passenger Landing Fees	112	68	61
Total Airline Revenues	\$ 12,556	\$ 10,462	\$ 9,958
Enplaned Passengers	2,061	978	1,009
Airline Cost per Enplaned Passenger	\$ 6.09	\$ 10.70	\$ 9.87

Also shown is an estimated CPE of each individual air carrier. The estimates for 2020 and 2021 are based on presumed activity and costs. The actual 2020 CPE cannot be calculated until the final audit work is completed, or not until approximately June of 2021. The fluctuation between air carriers is related to both their incurred costs and their passenger activity.

Airline Cost per Enplaned Passenger by Airline Spokane International Airport

(for the 12 months ending December 31; numbers in thousands except rates)

	Actual 2019	Projected 2020	Budget 2021
Enplaned Passengers			
Alaska	706	332	343
Allegiant (non-sig)	0	-	-
Delta	501	214	221
Frontier (non-sig)	35	17	18
Southwest	464	255	263
American	123	68	70
United	229	91	94
Others	3	0	0
Total Airlines	2,061	977	1,009
Cost per Enplaned Passenger			
Alaska	\$ 6.09	\$ 11.03	\$ 9.85
Allegiant (non-sig)	8.01	-	-
Delta	5.58	10.24	10.03
Frontier (non-sig)	3.83	5.35	5.11
Southwest	5.46	8.92	8.19
American	8.07	11.69	12.37
United	7.38	14.61	12.79
Others	33.82	299.23	125.93
Average	\$ 6.09	\$ 10.70	\$ 9.87



Section V. Terminal Rent Rate and Landing Fee Calculations

The tables below show the actual calculation of the average terminal rent rate and the airfield landing fee as described in Section IV Rate Making.

Calculation of Terminal Building Rental Rate

Spokane International Airport

(for the 12 months ending December 31; numbers in thousands except rates)

	Actual 2019	Projected 2020	Budget 2021
TERMINAL BUILDING COST			
M&O Expenses	\$ 9,848	\$ 9,125	\$ 8,863
M&O Reserve Fund Requirement	653	-	-
Debt Service	-	-	-
Debt Service Coverage	-	-	-
Net Depreciation Charges	2,263	2,239	2,250
Terminal Building Cost	\$ 12,764	\$ 11,364	\$ 11,112
Less: Terminal Non-Airline Revenues Associated With Rented Space			
Terminal Concession Revenues	\$ (2,223)	\$ (1,291)	\$ (1,330)
Nonairline Terminal Rentals	(686)	(650)	(650)
Share of Parking Surplus @ 0% (2020 /2021)	(1,848)	-	-
Other Terminal Payments	(844)	(642)	(678)
TSA Security Reimbursement ²	-	-	-
CARES Reimbursement	-	(3,551)	(2,308)
Loading Bridge Fees	(247)	(260)	(262)
Net Terminal Building Cost	\$ 6,916	\$ 4,970	\$ 5,884
Weighted Rented Space	96	99	99
Charged Average Terminal Rental Rate (per sq ft per year)	\$ 61.44	\$ 64.73	
Calculated Average Terminal Rental Rate (per sq ft per year)	\$ 72.16	\$ 49.99	\$ 59.19
Rented Space	96	99	99
Subtotal	\$ 5,889	\$ 6,435	\$ 5,884
Other Airline Terminal Payments ¹	332	332	332
Airline Terminal Rentals	\$ 6,221	\$ 6,767	\$ 6,216

1. Storage – Outside, Janitorial, Boarding Areas Walkway, Ramp - Equipment Storage, and Glycol Pad.

Calculation of Landing Fee Spokane International Airport

(for the 12 months ending December 31; numbers in thousands except rates)

	Actual 2019	Projected 2020	Budget 2021
Landing Fee Requirements			
M&O Expenses	\$ 28,796	\$ 26,886	\$ 25,810
COVID Reduction			
Net Depreciation			
Total SIA Depreciation - Existing Assets	23,806	26,751	26,143
Less Grant, PFC and Bond Funded Assets	(14,120)	(15,686)	(15,886)
Future SAB Funded Assets (Net of AIP/PFC)	-	-	339
Return on Investment in Land	1,282	1,282	1,282
Additional Discretionary Cash Flow (if Extension)	1,800	1,800	1,800
Debt Service			
Debt Service - Outstanding GARBs	-	-	-
Debt Service - Future GARBs	-	-	-
Debt Service - Outstanding Subordinate Lien	468	468	368
Required Debt Service Coverage	-	-	-
M&O Reserve Fund	1,910	-	-
Subtotal	\$ 43,942	\$ 41,501	\$ 39,856
Less:			
Airline Terminal Building Rent	(6,751)	(6,916)	(6,216)
Other Airline Revenues	(1,333)	(1,189)	(1,195)
Total Nonairline Revenues, incl CARES	(28,179)	(27,253)	(26,499)
Subtotal	\$ 7,680	\$ 6,142	\$ 5,946
Less Non-Operating Revenues Available for Debt Service			
Interest Income	\$ (661)	\$ (661)	\$ (661)
Air Service Expenses/Waivers	(122)	(350)	(50)
Transaction Fees ("CFC") Allocable to Debt Service/Coverage	-	-	-
TOTAL LANDING FEE REQUIREMENT	\$ 6,897	\$ 5,131	\$ 5,235
Prior Year Carry Over (Surplus)/Deficit	(1,985)	(1,613)	(828)
Net Landing Fee Requirement	\$ 4,911	\$ 3,518	\$ 4,407
Landing Fees Paid	\$ 6,524	\$ 4,346	\$ 4,407
 Signatory Landed Weight (millions/lbs)	2,198	1,466	1,577
Signatory Cargo Landed Weight (adjusted for premium)	105% 450	405	425
Non-Signatory Use Agmt Landed Weight (adjusted)	115% 47	30	29
Itinerant Landed Weight (adjusted)	150% 99	184	73
Adjusted Total Landed Weight	2,794	2,085	2,103
Calculated Signatory Airline Landing Fee Rated (per 1000 lbs)	\$ 1.76	\$ 1.69	\$ 2.10
 Charged Signatory Airline Landing Fee Rate	2.36	2.24	2.10
Charged Signatory Cargo Airline Landing Fee Rate	105% 2.48	2.35	2.20
Non-Signatory Use Agmt Landing Fee Rate	115% 2.71	2.58	2.41
Itinerant Landing Fee Rate	150% 3.54	3.36	3.14



Section VI. Operating Revenues and Expenses

OPERATING REVENUES

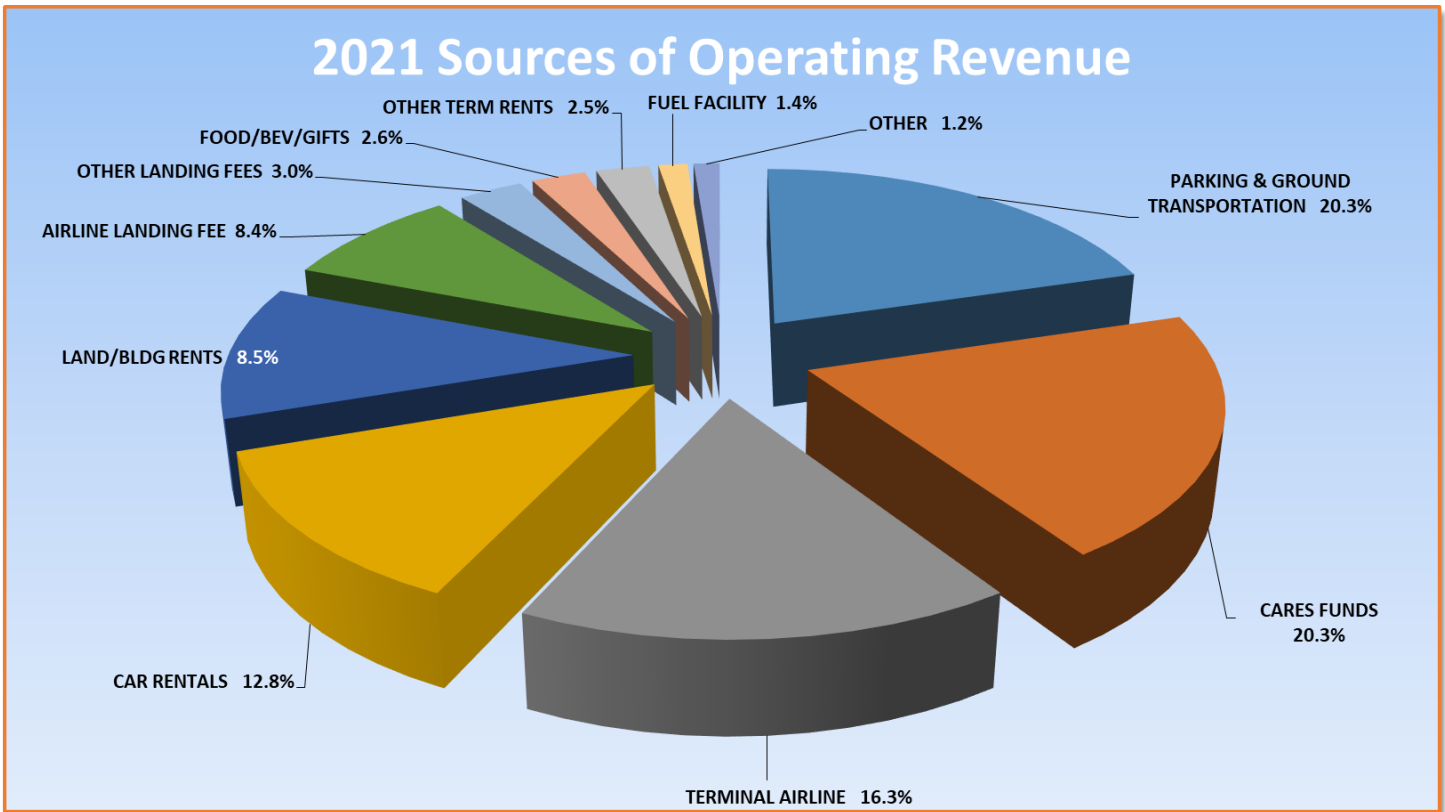
Operating revenues are revenues generated through the daily operations of the Airport. The cost centers outlined earlier, Airfield (including Fuel Facility), Parking and Ground Transportation, Other Buildings and Grounds, GEG Terminal, Rental Car Facilities, the ABP and Felts Field are broken down here into smaller components for analysis. Other revenue sources are typically Non-operating and are generated from facility charges and interest income.

Highlights:

- **Parking and Ground Transportation** is estimated to have a decrease in revenue of 50.0% from the 2020 budget (14.4% over estimated 2020 results). Parking and Ground Transportation comprises 20.3% of all operating revenues, down from 2019's 35.2%.
- **Airline Terminal and Other Commercial Land / Building Rents** are more resilient to the decrease in activity as leased space is necessary to continue to provide services to the public.
- **Landing Fees Revenue** show decreases due to the decrease in takeoffs and landings, especially by passenger carriers. Cargo carrier activity has not shown much of a decrease.
- **Food, Beverage, Retail and Car Rental Revenues** show decreases as revenue is directly related to passenger decreases.

This table and the chart on the next page shows the main revenue sources for 2020 and 2021.

Sources of Operating Revenue									
This table shows general categories of revenues as a percentage of total operating revenues									
Rank	Description	Budgeted	FY 2020 % of	Year End		Budgeted	FY 2021 % of	\$ Change	% Change
		2020	Total Rev	Estimate 2020	FY 2020 % of Total Rev	2021	Total Rev		
1	Parking / Landside / GTC	16,635,000	35.2%	7,269,149	17.3%	8,317,000	20.3%	(8,318,000)	-50.0%
2	CARES Funds	-	0.0%	9,721,859	23.2%	8,289,613	20.3%	8,289,613	
3	Terminal Rent (airline)	7,912,000	16.8%	7,175,796	17.1%	6,677,989	16.3%	(1,234,011)	-15.6%
4	Car Rentals	7,026,500	14.9%	5,662,372	13.5%	5,230,000	12.8%	(1,796,500)	-25.6%
5	Commercial Land / Bldg Rents	4,415,935	9.4%	4,604,819	11.0%	4,549,441	11.1%	133,506	3.0%
6	Airline Landing Fees	5,512,181	11.7%	3,278,683	7.8%	3,448,751	8.4%	(2,063,430)	-37.4%
7	Landing Fees (other)	1,328,292	2.8%	1,219,164	2.9%	1,229,923	3.0%	(98,369)	-7.4%
8	Food/Bevs/Gifts	1,929,600	4.1%	996,614	2.4%	1,043,100	2.6%	(886,500)	-45.9%
9	Terminal Rent (other)	1,177,700	2.5%	992,051	2.4%	1,028,150	2.5%	(149,550)	-12.7%
10	Fuel Facility	770,000	1.6%	531,612	1.3%	560,000	1.4%	(210,000)	-27.3%
	Other Revenue	514,216	1.1%	518,720	1.2%	500,306	1.2%	(13,910)	-2.7%
	Total Operating Revenues	\$ 47,221,424	100.0%	\$ 41,970,838	100.0%	\$ 40,874,273	100.0%	\$ (6,347,151)	-13.4%



OPERATING EXPENSES

Operating expenses are generated through the daily operations of the Airport. Operating expenses are tracked in the direct and indirect cost centers described earlier. Along with the direct cost centers tracking revenue, there are five indirect cost centers in Planning and Engineering, Airport Rescue and Fire Fighting (ARFF), Information Technology, Police and Dispatch along with Airport Administration. The table and chart following this breaks down expenses into smaller components for analysis.

Highlights:

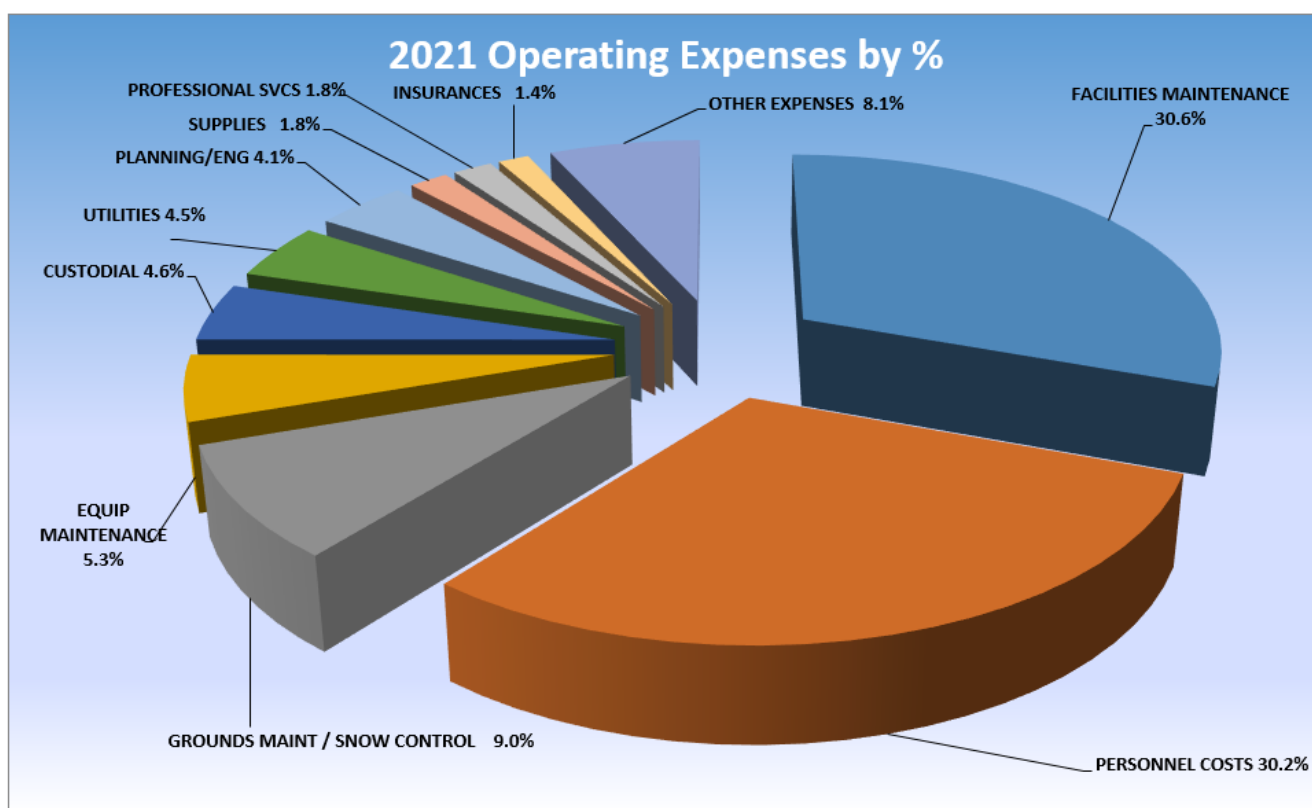
- Facilities and Grounds maintenance** is the largest estimated expense for 2021 at 30.6% of operating expenses. The Airport is an older facility (although significant rehabilitation has been accomplished in improvements with more to follow) and demands attention. Efforts were undertaken to reduce costs without affecting safety and security.
- Personnel Compensation and Benefits** is the second largest estimated area of expense totalling 30.2% of operating expenses. 2021 shows a decrease of 25.9% over the prior year and is related to decreases in staffing due to the decline in passenger activity.

Operating Expenditures by Rank

This table shows general categories of expenses as a percentage of total operating expenses

Rank	Description	Budgeted 2020	FY 2020 % of Total Exp	Year End Estimate 2020	FY 2020 % of Total Exp	Budgeted 2021	FY 2021 % of Total Exp	21 vs '20 Budget \$ Change	% Change
1	Facilities, Hangars, Bldgs, Depreciation	11,356,904	24.8%	13,517,525	32.9%	11,814,151	30.6%	457,247	4.0%
2	Personnel (Incl taxes / benefits)	15,741,929	34.4%	13,732,383	33.4%	11,670,001	30.2%	(4,071,928)	-25.9%
3	Grounds Maintenance / Snow Control	4,205,500	9.2%	2,818,950	6.9%	3,477,000	9.0%	(728,500)	-17.3%
4	Equipment Maintenance	2,127,450	4.6%	1,770,072	4.3%	2,039,869	5.3%	(87,581)	-4.1%
5	Custodial Services & Supplies	1,828,000	4.0%	1,728,681	4.2%	1,795,625	4.6%	(32,375)	-1.8%
6	Utilities (Power/Nat. Gas/Water/Sewer)	1,996,288	4.4%	1,692,668	4.1%	1,746,478	4.5%	(249,810)	-12.5%
7	Research / Planning / Engineering	2,205,000	4.8%	1,477,144	3.6%	1,592,738	4.1%	(612,262)	-27.8%
8	Other Supplies	1,002,400	2.2%	582,146	1.4%	679,660	1.8%	(322,740)	-32.2%
9	Professional Services	785,000	1.7%	652,180	1.6%	685,000	1.8%	(100,000)	-12.7%
10	Insurances	525,500	1.1%	539,882	1.3%	537,647	1.4%	12,147	2.3%
	Other Operating Expenses	4,034,815	8.8%	2,567,332	6.2%	2,620,110	6.8%	(1,414,705)	-35.1%
	Total Operating Expenses	\$ 45,808,786	100.0%	\$ 41,078,963	100.0%	\$ 38,658,279	100.0%	\$ (7,150,507)	-15.6%

Note: Minor reclassifications have been made to prior years to conform with 2021 presentation and some totals or % may not add exactly due to rounding





Section VII. Capital Improvement Program

The Capital Improvement Program (CIP) is developed annually and updated throughout each year. The purpose of the CIP process is to evaluate, prioritize, and coordinate proposed projects for, ideally, a five-year period. Projects that may require FAA funding in the future are updated with the FAA annually. The projects developed through the planning process require the use of Federal funding through the FAA, DOT, and the TSA along with State grants, PFCs, CFCs and cash generated by operation of the Airport.

The primary goal of the CIP is the development of a detailed capital budget for the current fiscal year and a plan for capital development during the next three to four years. By updating and approving the CIP, a strategy and schedule is set for budgeting and constructing facilities at SIA, ABP, and Felts Field.

The table below shows a summary of projects by area for 2029-2021.

2019 - 2020 CAPITAL IMPROVEMENT PROGRAM			
SPOKANE INTERNATIONAL AIRPORT	<u>Actual 2019</u>	<u>Projected 2020</u>	<u>2021 Budget</u>
Parking / Ground Transportation	\$3,742,347	\$563,428	\$625,000
Landside Roadways	985,259	1,792,997	2,405,000
Airfield / Ops / Fuel	4,503,841	95,942	9,000,000
Other Buildings & Grounds	2,322,115	1,655,197	22,350,000
Terminal	13,739,848	2,436,517	5,500,000
ARFF	56,079	762,052	-
I.T.	3,832,340	-	500,000
Police / Security	918,876	16,544	-
Admin	-	64,000	75,000
Land Acquisitions	<u>335,261</u>	<u>-</u>	<u>-</u>
Sub-Total	\$30,435,966	\$7,386,676	\$40,455,000
AIRPORT BUSINESS PARK	<u>36,290</u>	<u>52,870.00</u>	<u>\$1,500,000</u>
FELTS FIELD			
Airfield	52,226	268,046	440,000
Other Buildings / Grounds	4,096,106	119,912	50,000
Terminal	15,355	16,862	-
Land Acquisitions	79,507	-	-
Sub-Total	<u>\$4,243,194</u>	<u>\$404,820</u>	<u>\$490,000</u>
TOTAL CAPITAL IMPROVEMENTS	<u>\$34,715,450</u>	<u>\$7,844,366</u>	<u>\$42,445,000</u>



Section VIII. Debt Service

The Airport has covenants to maintain a bond debt service coverage ratio of not less than 1.3, which it has successfully maintained. The calculation for 2019, along with estimates for 2020 and 2021 is shown below. The Airport defeased a 2008 bond and redeemed a 2005 bond in 2017 leaving only four Washington State Community Economic Revitalization Board (CERB) loans as long term debt. These loans carry interest rates ranging from 0% - 1% and were utilized to complete projects housing tenants providing services for aeronautical clients of the Airport. The Airport is considering extinguishing the 2005 and 2006 loans in 2021, consequently there is no 2021 debt service requirement shown for them. Since the Airport does not carry any Municipal Bond debt, it has not been required to obtain a Bond Rating, however the most recent ratings in 2017 were A+ Standard & Poor's, A+ Fitch, and A-2 Moody's.

The Airport has entered into an agreement with Spokane County to borrow, on a short term basis, up to \$7,200,000 in funds to be used for various purposes. As of November 2020, the Airport exercised this option to bridge the initial concerns related to passenger activity decline. The short term borrowing will be extinguished prior to year end and there is no effect on the 2021 Budget at this time.

The low level of debt carried currently favorably positions the Airport to be able to pursue funding needed for upcoming projects needing long-term bond financing.

Debt Service by Series and Cost Center

Spokane International Airport

(for the 12 months ending December 31; numbers in thousands)

	Actual 2019	Projected 2020	Budget 2021
By Series			
Subordinate Lien			
2005 CERB Loan	\$ 51	\$ 51	\$ -
2006 CERB Loan	48	48	-
2008 CERB Loan	118	118	118
2010 CERB Loan	<u>250</u>	<u>250</u>	<u>250</u>
Total Subordinate Debt Service	\$ 468	\$ 468	\$ 368
 TOTAL DEBT SERVICE	 <u>\$ 468</u>	 <u>\$ 468</u>	 <u>\$ 368</u>
By Cost Center			
Other Buildings & Grounds	468	468	368
Felts Field	-	-	-
Airport Business Park	<u>-</u>	<u>-</u>	<u>-</u>
Debt Service and Business Park	<u>\$ 468</u>	<u>\$ 468</u>	<u>\$ 368</u>



Section IX. Benchmark Table of Other Airports

This table compares Spokane International Airport's various statistics with airports of similar size. The table on the following page shows the same statistics for those airports that are direct connections. Comparing statistics from one airport to another can be problematic as each airport operates under differing conditions and corporate formation. Spokane's Landing Fee (LF) and Cost per Enplanement (CPE) numbers show results from the year 2019.

SIMILAR SIZE AIRPORT COMPARISONS

Hub Size	Airport Name	LOC_ID	Enplanements	Signatory Landing Fee	CPE	FTEs @ Year End
M	EPPLEY AIRFIELD	OMA	2,509,997	2.86	6.69	168
S	CHARLESTON	CHS	2,349,364	-	8.49	173
S	MEMPHIS	MEM	2,260,642	1.24	6.06	310
S	WILL ROGERS	OKC	2,204,113	3.00	5.24	125
S	RENO/TAHOE	RNO	2,149,759	3.14	6.80	278
S	RICHMOND	RIC	2,142,840	1.38	5.45	188
S	SPOKANE	GEG	2,061,144	2.36	6.09	107
S	LOUISVILLE	SDF	2,047,746	1.15	5.42	147
S	TED GREEN	PVD	2,046,399	4.50	9.63	141
S	BOISE	BOI	2,025,960	1.51	3.53	110
S	NORFOLK	ORF	1,928,129	3.01	4.73	204
S	TUCSON	TUS	1,897,590	1.32	7.54	215
S	GERALD R FORD	GRR	1,804,358	2.97	8.79	108
S	EL PASO	ELP	1,764,324	1.75	5.30	233
S	LONG BEACH	LGB	1,757,122	4.00	8.54	113
S	ORLANDO SANFORD	SFB	1,603,220	1.37	2.03	95
S	BIRMINGHAM	BHM	1,533,824	5.20	11.03	160
S	ALBANY	ALB	1,518,969	2.75	5.90	208
S	TULSA	TUL	1,509,453	3.62	5.83	148
S	SAVANNAH	SAV	1,502,974	3.10	8.26	218
S	DES MOINES	DSM	1,459,566	3.07	9.08	57
		AVERAGE	1,908,452	2.54	6.69	167

Charleston, Memphis, Richmond, Louisville, and Boise have significant non-passenger traffic influencing rates
(Source: 2019 FAA Form 127)

DIRECT CONNECTION AIRPORT COMPARISONS

Hub Size	Airport Name	LOC_ID	Enplanements	Signatory Landing Fee	CPE
S	SPOKANE	GEG	2,061,144	2.36	6.09
L	ATLANTA	ATL	54,531,948	0.76	2.90
L	LOS ANGELES	LAX	44,207,464	4.75	17.58
L	DALLAS/FORT WORTH	DFW	36,623,417	2.31	12.95
L	DENVER	DEN	34,512,954	4.70	10.26
L	SAN FRANCISCO	SFO	28,620,387	5.54	17.08
L	SEATAC	SEA	25,873,875	3.78	12.87
L	MC CARRAN	LAS	25,223,715	1.70	10.06
L	PHOENIX	PHX	22,831,945	1.86	6.37
L	MINNEAPOLIS	MSP	19,783,380	3.23	6.96
L	SALT LAKE CITY	SLC	13,090,133	2.29	4.28
L	SAN DIEGO	SAN	12,356,286	1.73	10.78
L	PORTLAND	PDX	9,966,798	3.00	9.53
M	SAN JOSE	SJC	7,462,126	2.18	9.33
M	OAKLAND	OAK	6,807,835	3.14	12.23
M	SACRAMENTO	SMF	6,298,447	3.51	11.17
S	BOISE	BOI	<u>2,025,960</u>	<u>1.51</u>	<u>3.53</u>
AVERAGE W/O GEG			21,888,542	2.87	9.87




Section X. Budget Summaries by Cost Center

TRAVELER


BUSINESS

FELTS FIELD

 Spokane International Airport

Spokane Airport
36°
29°
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Spokane Airport Board Consolidated Summary
Spokane International Operations Summary
Business Park Operations Summary
Felts Field Operations Summary
Capital Improvement Program

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SPOKANE AIRPORT BOARD

2021 BUDGET SUMMARY

*Note: reclassifications have been made to prior years to conform with 2021 presentation
and some totals or % may not add exactly due to rounding*

	<u>2019 Actual</u>	<u>2020 Budget</u>	<u>Projected 2020 Yr End</u>	<u>2021 Budget</u>	<u>% Change 21 vs 20</u>
Operations Revenues (with CARES)					
Spokane International	\$ 42,803,423	\$ 45,234,537	\$ 39,574,039	\$ 38,527,553	
Airport Business Park	1,410,347	1,372,707	1,556,894	1,531,327	
Felts Field	772,149	614,180	839,905	815,392	
	<u>44,985,919</u>	<u>47,221,424</u>	<u>41,970,839</u>	<u>40,874,272</u>	-13.4%
Other Sources					
Spokane International	646,765	566,138	629,877	566,138	
Airport Business Park	30,666	29,653	26,264	24,524	
Felts Field	(18,896)	-	-	-	
(To) / From Reserves	<u>(3,681,625)</u>	<u>(1,553,793)</u>	<u>(1,040,511)</u>	<u>(2,443,912)</u>	
	<u>(3,023,090)</u>	<u>(958,002)</u>	<u>(384,370)</u>	<u>(1,853,250)</u>	
Total Operations / Other Sources	<u>\$ 41,962,829</u>	<u>\$ 46,263,422</u>	<u>\$ 41,586,469</u>	<u>\$ 39,021,022</u>	-15.7%
Operations Expenses					
Spokane International	\$ 29,452,623	\$ 34,107,941	\$ 27,483,751	\$ 26,455,794	
Airport Business Park	708,449	678,856	555,079	630,510	
Felts Field	596,296	671,555	520,025	617,800	
	<u>30,757,368</u>	<u>35,458,352</u>	<u>28,558,855</u>	<u>27,704,104</u>	-21.9%
Debt Service					
Spokane International	451,701	454,636	454,636	362,743	
Airport Business Park	-	-	-	-	
Felts Field	-	-	-	-	
	<u>451,701</u>	<u>454,636</u>	<u>454,636</u>	<u>362,743</u>	
Operations & Debt Service	<u>31,209,069</u>	<u>35,912,988</u>	<u>29,013,491</u>	<u>28,066,847</u>	-21.8%
Non-Cash Depreciation					
Spokane International	9,325,477	8,916,970	10,865,645	9,311,898	
Airport Business Park	556,887	527,839	540,435	516,185	
Felts Field	871,395	905,625	1,114,027	1,126,092	
	<u>10,753,760</u>	<u>10,350,434</u>	<u>12,520,108</u>	<u>10,954,175</u>	
Total Operations/ Other Expenditures	<u>\$ 41,962,829</u>	<u>\$ 46,263,422</u>	<u>\$ 41,533,599</u>	<u>\$ 39,021,022</u>	-15.7%
Capital Sources					
Spokane International	30,435,966	40,919,000	7,386,676	40,455,000	
Airport Business Park	36,290	620,000	52,870	1,500,000	
Felts Field	4,243,194	1,278,000	404,820	490,000	
	<u>\$ 34,715,450</u>	<u>\$ 42,817,000</u>	<u>\$ 7,844,366</u>	<u>\$ 42,445,000</u>	-0.9%
Capital Expenditures					
Spokane International	30,435,966	40,919,000	7,386,676	40,455,000	
Airport Business Park	36,290	620,000	52,870	1,500,000	
Felts Field	4,243,194	1,278,000	404,820	490,000	
	<u>\$ 34,715,450</u>	<u>\$ 42,817,000</u>	<u>\$ 7,844,366</u>	<u>\$ 42,445,000</u>	-0.9%
Consolidated Sources	<u>\$ 76,678,279</u>	<u>\$ 89,080,422</u>	<u>\$ 49,430,835</u>	<u>\$ 81,466,022</u>	-8.5%
Consolidated Expenditures	<u>\$ 76,678,279</u>	<u>\$ 89,080,422</u>	<u>\$ 49,377,965</u>	<u>\$ 81,466,022</u>	-8.5%

SPOKANE INTERNATIONAL AIRPORT

2021 OPERATIONS BUDGET SUMMARY

Note: reclassifications have been made to prior years to conform with 2021 presentation

and some totals or % may not add exactly due to rounding

	<u>2019 Actual</u>	<u>2020 Budget</u>	<u>Projected 2020 Yr End</u>	<u>2021 Budget</u>	<u>% Change 21 vs. 20</u>
Operations Revenues (with CARES)					
Parking / Ground Transportation	\$ 15,353,376	\$ 16,635,000	\$ 8,263,144	\$ 9,310,995	
Airfield	6,801,313	6,811,993	6,209,477	6,882,366	
Fuel Facility	825,519	770,000	674,687	703,075	
Other Buildings & Grounds	2,537,884	2,528,778	2,650,152	2,636,981	
Terminal	10,751,148	11,606,200	13,308,675	11,644,569	
Rental Car Facilities	6,328,214	6,726,500	5,362,372	4,930,000	
Other Revenues	205,969	156,066	164,484	155,367	
Indirect Cost Centers CARES Funds	-	-	2,941,047	2,264,200	
Total Operations Revenue	<u>\$ 42,803,423</u>	<u>\$ 45,234,537</u>	<u>\$ 39,574,039</u>	<u>\$ 38,527,553</u>	-14.8%
Operations Expenses					
Parking / GTC / Landside	5,556,086	6,759,300	4,412,654	4,609,250	
Airfield & Operations	5,873,452	6,937,110	5,698,713	5,776,310	
Fuel Facility	445,985	495,890	385,817	385,450	
Other Buildings & Grounds	483,123	471,113	421,749	360,983	
Terminal	5,666,229	5,655,785	4,954,130	4,893,098	
Rental Car Facilities	389,351	426,500	331,724	380,000	
Other Indirect Centers					
Engineering	527,494	677,126	457,551	343,783	
Fire	2,275,829	2,485,032	2,488,153	1,678,253	
Information Technology	590,495	882,656	637,294	634,160	
Police	1,989,129	2,376,864	1,947,560	1,563,625	
Communication	548,394	728,250	540,648	507,741	
General Administration	5,107,056	6,212,315	5,207,758	5,323,141	
Sub-Total Operations Expenses	<u>29,452,623</u>	<u>34,107,941</u>	<u>27,483,751</u>	<u>26,455,794</u>	-22.4%
Revenues over Expenses pre Deprec	<u>\$ 13,350,800</u>	<u>\$ 11,126,596</u>	<u>\$ 12,090,288</u>	<u>\$ 12,071,759</u>	
Net Depreciation	<u>(9,325,477)</u>	<u>(8,916,970)</u>	<u>(10,865,645)</u>	<u>(9,311,898)</u>	
Revenues over Expenses incl Deprec	<u>\$ 4,025,323</u>	<u>\$ 2,209,626</u>	<u>\$ 1,224,643</u>	<u>\$ 2,759,861</u>	
Other Sources Available / (Used)					
Interest Income	661,122	600,000	641,721	600,000	
Debt Interest	(14,357)	(33,862)	(11,844)	(33,862)	
Total Other Sources Available	<u>646,765</u>	<u>566,138</u>	<u>629,877</u>	<u>566,138</u>	0.0%
Available for Debt / Projects / Reserves	4,672,088	2,775,764	1,854,520	3,325,999	19.8%
Total Debt Principal	<u>(451,701)</u>	<u>(454,636)</u>	<u>(454,636)</u>	<u>(362,743)</u>	
Sources over (Expenses)	<u>\$ 4,220,387</u>	<u>\$ 2,321,128</u>	<u>\$ 1,399,884</u>	<u>\$ 2,963,256</u>	

AIRPORT BUSINESS PARK

2021 OPERATIONS BUDGET SUMMARY

*Note: reclassifications have been made to prior years to conform with 2021 presentation
and some totals or % may not add exactly due to rounding*

	<u>2019 Actual</u>	<u>2020 Budget</u>	<u>Projected 2020 Yr End</u>	<u>2021 Budget</u>	<u>% Change 21 vs. 20</u>
Operations Revenues (with CARES)					
Building / Office Leases	\$ 1,148,272	\$ 1,126,652	\$ 1,140,768	\$ 1,129,485	
Land Leases	239,825	222,805	303,889	301,090	
Miscellaneous	22,250	23,250	112,237	100,752	
Total Operating Revenues	<u>\$1,410,347</u>	<u>\$1,372,707</u>	<u>\$1,556,894</u>	<u>\$1,531,327</u>	11.6%
Operating Expenses					
Buildings	\$ 367,357	\$ 423,500	\$299,360	\$ 324,560	
Grounds	205,081	84,150	90,019	137,050	
General Administration	136,011	171,206	165,700	168,900	
Total Operating Expenses	<u>708,449</u>	<u>678,856</u>	<u>555,079</u>	<u>630,510</u>	-7.1%
Revenues over Expenses (Pre Deprec)	\$ 701,898	\$ 693,851	\$ 1,001,815	\$ 900,817	
Depreciation	<u>(556,887)</u>	<u>(527,839)</u>	<u>(540,435)</u>	<u>(516,185)</u>	
Operating Revenue over (Expense)	<u>\$ 145,011</u>	<u>\$ 166,012</u>	<u>\$ 461,380</u>	<u>\$ 384,632</u>	
Other Sources Available / Used					
Interest Income	15,127	15,000	13,039	13,039	
Other Non-Operating Income (Expense)	<u>15,539</u>	<u>14,653</u>	<u>13,225</u>	<u>11,485</u>	
Total Other Sources Available	<u>30,666</u>	<u>29,653</u>	<u>26,264</u>	<u>24,524</u>	
Available for Debt / Projects / Reserves	175,677	195,665	487,644	409,156	
Total Debt Principal	<u>-</u>	<u>-</u>	<u>-</u>	<u>-</u>	
Sources over Expenditures incl Deprec	<u>\$ 175,677</u>	<u>\$ 195,665</u>	<u>\$ 487,644</u>	<u>\$ 409,156</u>	

FELTS FIELD

2021 OPERATIONS BUDGET SUMMARY

*Note: reclassifications have been made to prior years to conform with 2021 presentation
and some totals or % may not add exactly due to rounding*

	<u>2019 Actual</u>	<u>2020 Budget</u>	<u>Projected 2020 Yr End</u>	<u>2021 Budget</u>	<u>% Change 21 vs. 20</u>
Operating Revenues (with CARES)					
Airfield	\$ 47,608	\$ 46,980	\$ 40,445	\$ 46,980	
Hangars/ Bldgs / Land	688,152	534,200	681,786	653,750	
Terminal	26,534	26,600	26,684	26,600	
Miscellaneous	9,855	6,400	90,990	88,062	
Total Revenues	\$ 772,149	\$ 614,180	\$ 839,905	\$ 815,392	32.8%
Operating Expenses					
Airfield	\$ 129,004	\$ 217,500	\$ 143,850	\$ 174,000	
Hangars/ Bldgs / Land	84,597	58,375	64,300	105,200	
Terminal	61,147	86,750	59,525	71,850	
General Admin	321,548	308,930	252,350	266,750	
Total Expenses	596,296	671,555	520,025	617,800	-8.0%
Revenue / (Expense) Pre Depreciation	\$ 175,853	\$ (57,375)	\$ 319,880	\$ 197,592	-444.4%
Depreciation	(1,742,876)	(1,777,106)	(1,985,508)	(1,997,573)	
Credit for Funded Assets	871,481	871,481	871,481	871,481	
Net Depreciation	(871,395)	(905,625)	(1,114,027)	(1,126,092)	
Revenue / (Expense) incl Depreciation	\$ (695,542)	\$ (963,000)	\$ (794,147)	\$ (928,500)	
Cash Available for Debt Service	\$ 175,853	\$ (57,375)	\$ 319,880	\$ 197,592	
Interest and Other Income	(18,896)	-	-	-	
Available for Debt / Projects / Reserves	156,957	(57,375)	319,880	197,592	
Debt Service	-	-	-	-	
Expenses over Sources	\$ 156,957	\$ (57,375)	\$ 319,880	\$ 197,592	

SPOKANE AIRPORT BOARD				
2021 CAPITAL SUMMARY				
<i>Note: reclassifications have been made to prior years to conform with 2021 presentation and some totals or % may not add exactly due to rounding</i>				
SPOKANE INTERNATIONAL AIRPORT	2019 Actual	2020 Budget	Projected 2020 Yr End	2021 Budget
SOURCES OF FUNDS				
Federal & AIP Grants	1,290,436	7,656,100	2,508,168	8,000,000
Other Federal Grants / Funds	121,131	3,000,000	75,976	13,800,000
Other State / Local Funds	1,981,266	1,500,000	236,289	1,000,000
Available / Used PFC Collections and Interest	16,076,555	11,000,000	1,027,136	2,500,000
Available CFC Collections	152,609	5,100,000	1,277,566	8,350,000
Funds From Operations, Unrestricted Cash, CARES	10,813,969	12,662,900	2,261,541	6,805,000
	30,435,966	40,919,000	7,386,676	40,455,000
EXPENDITURE OF FUNDS				
Parking / Ground Transportation	3,742,347	15,445,000	563,428	625,000
Landside Roadways	985,259	-	1,792,997	2,405,000
Airfield / Ops / Fuel	4,503,841	2,015,000	95,942	9,000,000
Other Buildings & Grounds	2,322,115	7,100,000	1,655,197	22,350,000
Terminal	13,739,848	15,060,000	2,436,517	5,500,000
ARFF	56,079	729,000	762,052	-
I.T.	3,832,340	120,000	-	500,000
Police / Security	918,876	300,000	16,544	-
Admin	-	150,000	64,000	75,000
Land Acquisitions	335,261	-	-	-
Total Expenditures	30,435,966	40,919,000	7,386,676	40,455,000
AIRPORT BUSINESS PARK	2019 Actual	2020 Budget	Projected 2020 Yr End	2021 Budget
SOURCES OF FUNDS				
Funds From Operations & Unrestricted Cash	36,290	620,000	52,870	1,500,000
Total Sources	36,290	620,000	52,870	1,500,000
EXPENDITURE OF FUNDS				
Total Capital Projects	838,943	620,000	52,870	1,500,000
Total Expenditures	\$ 36,290	\$ 620,000	\$ 52,870	\$ 1,500,000
FELTS FIELD	2019 Actual	2020 Budget	Projected 2020 Yr End	2021 Budget
SOURCES OF FUNDS				
Federal & AIP Grants	147,478	300,000		330,000
Other Grants		98,000		
Debt or Other Funds				-
Funds From Operations & Unrestricted Cash	4,095,716	880,000	404,820	160,000
Total Sources	4,243,194	1,278,000	404,820	490,000
EXPENDITURE OF FUNDS				
Airfield	52,226	630,000	268,046	440,000
Other Buildings / Grounds	4,096,106	50,000	119,912	50,000
Terminal	15,355	598,000	16,862	-
Land Acquisitions	79,507	-	-	-
Total Expenditures	4,243,194	1,278,000	404,820	490,000
<i>Note: The expenditures noted above do not necessarily represent specific projects, rather a variety of projects in a category that will be individually vetted by the Airport Board through the Committee process prior to authorization of expending of funds.</i>				



Selected 2021 Rates & Charges List

Space rates shown on a square foot per annum basis

Other rate basis are noted

(This list is not meant to be all inclusive.)

Change from 2020

Spokane International Airport	Signatory	Cargo Exempt*	w/ Use Agreement	Itinerant
Class 1 Space				
Airline Ticket Counters	\$ 59.19			
Airline Ticket Office (ATO)	59.19			
Queuing Areas	59.19			
Instant Travel Machines	59.19			
Concourse Hold Areas	59.19			
Baggage Claim	59.19			
Class 2 Space				
Baggage Service (BOS)	\$ 44.39			
Upper Concourse Office	44.39			
Ops Office	44.39			
Communications Office	44.39			
Baggage Make-Up	44.39			
Baggage Delivery	44.39			
Lower Concourse Office	44.39			
Storage Room	44.39			
Maintenance Office	44.39			
Baggage Cabinet	44.39			
Triturator Building	44.39			
Loading Bridge / mo	\$ 1,970.83			
Aircraft Parking / mo	500.00			
Boarding Walkway	11.25			
Ramp GSE Storage	\$ 4.34			
Glycol Pad	4.34			
Outside Storage	0.80			
Landing Fee / 1,000 lbs	\$ 2.10	\$ 2.20	\$ 2.41	\$ 3.14
Non Leased Loading Bridge / full turn	281.00		281.00	281.00
{Includes NON Leased Loading Bridge, A/C Parking, Concourse holding area}				
Non Leased Baggage System / turn	115.00		115.00	115.00
{Includes both baggage make-up for outbound AND baggage claim for inbound}				
Non Leased Ticketing / use	38.00		38.00	38.00
{Includes NON Leased Ticket Counter Only}				
Non Leased Aircraft Parking / use	100.00	105.00	125.00	150.00
{Includes NON Leased Aircraft Parking area away from a Bridge}				
Fuel Flowage Fee / Gallon	0.040	0.040	0.050	0.065

*Cargo Exempt: Cargo Carriers with on-airfield ramp & operation facilities



Selected 2021 Rates & Charges List

Space rates shown on a square foot per annum basis

Other rate basis are noted

(This list is not meant to be all inclusive.)

Change from 2020

Spokane International General Aviation	
Non Lease Aircraft Parking / day	
Single Engine Aircraft	\$ 25.00
Multi Engine / Small Jet	35.00
Q-400 / EJ / RJ or Greater	50.00

Use Agreement / month	
SIA	\$ 90.00
Felts Field	35.00

Felts Field Rates	
Landing Fee / 1000 lbs	\$ 2.50
Non Lease Aircraft / day	25.00
T-Hangar / mo	230.00
Tie-Down / mo	30.00
Fuel Flowage / Gallon	0.065

Mini Warehouse Rates	
amounts due in advance for entire billing term)	
Monthly Only	\$56.00
Includes WA St LH Tax	

Miscellaneous Items (not incl. applicable taxes)	
Security Badge Only	\$ 30.00
Badge Renewal or Fingerprinting	40.00
New Badge & Fingerprinting	60.00
1st Lost Badge	30.00
2nd Lost Badge	40.00
3rd Lost Badge	60.00
Telephone Line / mo	12.00
Fitness Center / mo	15.00
GTC Conf room / 4 hrs	25.00
Room Setup (if necessary)	25.00
Event or Conference Center / day	175.00
GTC Trip Fee / Trip	1.75

Force Account Rates	
Materials	Cost + 10%
Labor / hr.	\$ 62.50

ALL VEHICLE PARKING LOT RATES INCLUDE WA STATE SALES TAX					
Parking (Sales Tax Included)	Covered	Outside	Temporarily Closed		
Length of Stay	Garage	Surface	C-Lot	Economy	Hourly
0 - 2 HR	\$ 4.00	\$ 4.00			\$ 2.00 / HR
2 - 3 HRS	6.00	6.00			
3 - 4 HRS	8.00				
4 - 5 HRS	10.00				
5-24 HRS	11.00				
DAILY MAX.	\$ 11.00	\$ 6.00			

Miscellaneous Vehicle Parking	
Tenant Employee Lots / mo	\$ 20.00
Tenant Employee Surface Lot / mo	40.00
Garage Employee Parking / mo	100.00
Commuter Employee Lot / mo	30.00
Commuter Surface Lot / mo	50.00
Commuter Garage / mo	100.00
Passport / Executive / mo	200.00
Vendor / Contractor Parking / veh / year	50.00
Transponder Replacement	50.00
ALL VEHICLE PARKING INCLUDES WSST	

Notes: Certain other terminal and leasehold rental rates are set in conjunction with the Airline Operating Agreement (AOA) provisions while others are set by Fair Market Value appraisals. Rental Car and Concession rates are determined through negotiation and Request for Proposal (RFP) processes. Equipment rentals and other rates are available upon request.

Spokane Airports 2021 Budget

Spokane International Airport / Airport Business Park/ Felts Field



www.spokaneairports.net

Approved:
Spokane Airport Board, November 19, 2020

Spokane Airports Item - expenditure control form is not needed for this item.



Agenda Sheet for City Council Meeting of:
11/23/2020

Date Rec'd	11/10/2020
Clerk's File #	OPR 2020-0835
Renews #	
Cross Ref #	
Project #	
Bid #	
Requisition #	RE19406

Submitting Dept	FLEET OPERATIONS
Contact Name/Phone	MIKE LOWDON 625-7909
Contact E-Mail	MLOWDON@SPOKANECITY.ORG
Agenda Item Type	Purchase w/o Contract
Agenda Item Name	5100-PURCHASE OF TANK TRUCK

Agenda Wording

The Wastewater Department would like to purchase a Tank truck. The truck will be purchased from Kenworth Sales, Spokane, WA, for \$185,419.00 including tax. Purchase will be made using the Sourcewell Contract # 081716-KTC.

Summary (Background)

The Tank Truck will replace a unit that has reached the end of its economic life. We recommend approval for the purchase of a Tank Truck for the Wastewater Department. Funding for this is included in the Wastewater Department budget.

Lease? NO Grant related? NO Public Works? NO

Fiscal Impact

Expense	\$ 185,419.00	<u>Budget Account</u>	# 431043100943505640499999
Select	\$		#
Select	\$		#
Select	\$		#

Approvals

<u>Dept Head</u>	FLEIGER, NATHAN
<u>Division Director</u>	WALLACE, TONYA
<u>Finance</u>	ALBIN-MOORE, ANGELA
<u>Legal</u>	PICCOLO, MIKE
<u>For the Mayor</u>	ORMSBY, MICHAEL

Council Notifications

<u>Study Session\Other</u>	11/16/20
<u>Council Sponsor</u>	Breean Beggs

Distribution List

mmartinez

Additional Approvals

<u>Purchasing</u>	PRINCE, THEA
<u>WASTEWATER MANAGEMENT</u>	LOWDON, MICHAEL

Briefing Paper

Finance and Administration Committee

Division & Department:	Finance, Fleet Services
Subject:	Purchase of Tank Truck
Date:	November 16, 2020
Author (email & phone):	Micaela Martinez mmartinez@spokanecity.org 625-7823
City Council Sponsor:	Breean Beggs
Executive Sponsor:	Tonya Wallace
Committee(s) Impacted:	Finance and Administration Committee
Type of Agenda item:	<input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative
Alignment: (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)	Strategic Plan
Strategic Initiative:	Innovative Infrastructure: Maintaining our fleet of support equipment
Deadline:	
Outcome: (deliverables, delivery duties, milestones to meet)	
Background/History: The Wastewater Department would like to purchase a Tank truck. The truck will be purchased from Kenworth Sales, Spokane, WA, for \$185,419.00 including tax. Purchase will be made using the Sourcewell Contract # 081716-KTC.	
Executive Summary: <u>Impact</u> <ul style="list-style-type: none"> The Tank Truck will replace a unit that has reached the end of its economic life. <u>Action</u> <ul style="list-style-type: none"> We recommend approval for the purchase of a Tank Truck for the Wastewater Department. <u>Funding</u> <ul style="list-style-type: none"> Funding for this is included in the Wastewater Department budget. 	
Budget Impact: Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If new, specify funding source: Other budget impacts: (revenue generating, match requirements, etc.)	
Operations Impact: Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Specify changes required: Known challenges/barriers:	



KENWORTH SALES COMPANY - SPOKANE (T056)
6420 EAST BROADWAY
SPOKANE, Washington 99212

CITY OF SPOKANE FLEET SERVICES
915 N NELSON ST
SPOKANE, Washington 99202
United States of America

Shane Petersen
Cell Phone:
Office Phone: 509-534-2643
Email: spetersen@kwsco.com

MICAELA MARTINEZ
Cell Phone: 5099810500
Office Phone: 5099820500
Email: mmartinez@cityofspokane.org

Customer Quote

Equipment

Quantity:	1
Truck Price:	\$125,161
Dealer Options:	\$88,975
Extended Warranty:	\$0
Equipment Price:	\$191,607
Surcharges Not Subject to Discount:	\$0
Options Not Subject to Discount:	\$0
Factory Freight Cost:	\$2,475

Total Equipment Price: \$194,082

NET Sale Price: **\$169,953**

Miscellaneous

FET Tire Credit:	\$0
Net Chassis FET:	\$0
State Tax:	\$15,466
Body/Trailer/Accessories FET:	\$0
Fees:	\$0
Other:	\$0

Quotation Total: **\$185,419**

This quotation worksheet is provided to aid dealers in their pricing efforts. Since PACCAR Inc and its truck divisions have no control over data input and various transactional circumstances that may affect the FET calculation, it is not to be considered tax advice. The dealer should consult his own tax advisor for the proper calculation of any taxes under the variety of circumstances, which may occur.

Unpublished options may require review/approval.

Dimensional and performance data for unpublished options may vary from that displayed.

Price Level: January 1, 2019

Date: November 02, 2020

Deal: CITY OF SPOKANE FLEET

Quote Number: QUO-638417-V2H0X6

Printed On: 11/2/2020 2:28:23 PM



KENWORTH SALES COMPANY - SPOKANE (T056)
6420 EAST BROADWAY
SPOKANE, Washington 99212

CITY OF SPOKANE FLEET SERVICES
915 N NELSON ST
SPOKANE, Washington 99202
United States of America

Shane Petersen
Cell Phone:
Office Phone: 509-534-2643
Email: spetersen@kwsco.com

MICAELA MARTINEZ
Cell Phone: 5099810500
Office Phone: 5099820500
Email: mmartinez@cityofspokane.org

Vehicle Summary

Unit		Chassis	
Model:	T370 SERIES CONVENTIONAL	Fr Axle Load (lbs):	13220
Type:	FULL TRUCK	Rr Axle Load (lbs):	22700
Description 1:	CITY OF SPOKANE FLEET	G.C.W. (lbs):	35920
Description 2:	SWS TANK TRUCK		
Application		Road Conditions:	
Intended Serv.:	Construction: Vehicles used in the	Class A (Highway)	85
	cons		
Commodity:	Water.	Class B (Hwy/Mtn)	10
		Class C (Off-Hwy)	5
		Class D (Off-Road)	0
Body		Maximum Grade:	6
Type:	Vacuum tank.	Wheelbase (in):	188
Length (ft):	14	Overhang (in):	75
Height (ft):	10	Fr Axle to BOC (in):	68
Max Laden Weight (lbs):	4000		
		Cab to Axle (in):	120
Trailer		Cab to EOF (in):	195
No. of Trailer Axles:	0	Overall Comb. Length (in):	303.9
Type:			
Length (ft):	0	Special Req.	
Height (ft):	0	U.S. Domestic registry, 50-state.	
Kingpin Inset (in):	0		
Corner Radius (in):	0		
Restrictions			
Length (ft):	75		
Width (in):	102		
Height (ft):	13.5		

Approved by: _____

Date: _____

Note: All sales are F.O.B. designated plant of manufacture.

Std/ Opt	Description	\$ List	Weight
Model			
O	T370 SERIES CONVENTIONAL Electric Door locks LH/RH; Ignition & doors keyed alike; Single electric horn; Single-piece windshield; Electric windshield wipers, 2-speed plus intermittent; Electric windshield washers; Steering wheel 18in. 4-spoke; Glovebox door with locking latch; Dash-mounted cruise control with switches; Turn signal switch with column-mounted dimmer; Standard gray dash panels; Slate Gray interior primary color; Dark Slate Gray seat color; Floormat; Inside sunvisor, LH/RH; Door courtesy lights; Under-dash center console with 1 cup holder, 1 ashtray & 1 lighter.	90,052	9,509
S	T370 Class 7: medium-duty Conventional.	0	0
O	Chassis operation will include stationary application used in lower 48 states [US only]. Stationary operation is defined as running the engine under load while stationary at a substantial fraction of engine gross horsepower (60% or greater) for an extended period of time (longer than 5 - 10 minutes).	0	0
O	CARB Idle Emissions Reduction Feature for PX-7 and PX-9	100	0
O	Medium-duty 4x2 automatic.	0	0

Engine & Equipment

O	PACCAR PX-9 300 2017 300@2000 285@2200 860@1300 Includes turbo exhaust brake, no code is used. Diagnostic Plug for data link, Oil Cooler, Aluminum Flywheel Housing. N09260 P14 64....Max Vehicle Speed in Top Gea N09440 P11 NO....Engine Protection Shtdwn N09460 P06 NO....Gear Down Protection N09300 P19 64....Max Cruise Control Speed N09720 P230 YES...Enable Hot Ambient Automatic N09500 P02 NO....Cruise Control Auto Resume N09520 P04 NO....Auto Engine Brake in Cruise N09480 P26 1400..Max PTO Speed N09580 P32 5.....Idle Shtdwn Time N09680 P33 NO....Idle Shtdwn Override N09560 P520 YES...Enable Idle Shutdown Park Br N09600 P233 YES...Enable Impending Shutdown Wa N09780 P47 80....High Ambient Temperature Thr N09740 P46 40....Low Ambient Temperature Thre N09200 N205 120...Standard Maximum Speed Limit N09620 P234 60....Timer For Impending Shutdown N09360 N203 252...Reserve Speed Function Reset N09640 P516 35....Engine Load Threshold N09400 N206 10....Maximum Active Distance (N20 N09420 N201 0.....Reserve Speed Limit Offset (N09760 P56 60....Intermediate Ambient Tempera N09380 N202 0.....Maximum Cycle Distance (N202 N09220 N207 0.....Expiration Distance (N207) N09540 N209 0.....Expiration Distance (N209)	9,464	564
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Std/ Opt	Description	\$ List	Weight
S	PremierSpec	0	0
O	Enable engine regeneration in PTO mode Cummins	0	0
S	Gearing Analysis: Performance power before economy results.	0	0
S	Customer's Typical Operating Spd: 65 mph.	0	0
O	Effective VSL Setting NA	0	0
O	Engine Idle Shutdown Timer Enabled	0	0
O	Enable EIST Ambient Temp Override	0	0
O	Eff EIST NA Expiration Miles Use only with MX and Cummins engines	0	0
S	Air compressor: Cummins 18.7 CFM For Cummins And PACCAR PX engines.	0	0
S	Air Cleaner: Dry-type firewall mounted w/filter restriction indicator.	0	0
O	Fan Hub: Horton 2-Speed for ISL9, ISL-G, PX-8 or PX-9	436	0
S	Cooling module: 1000 square inches T170/T270/T370/T470. Includes metal surge tank on T170/T270/T370.	0	0
O	Exhaust:2017 EPA RH Under Cab DPF/SCR For PX-9 w/ Single Vertical RH Side of Cab Tailpipe.	1,432	215
O	Tailpipe: 5 in. single 24 in. 45 degree curved.	-35	-2
O	Fuel Filter:Fleetguard FS1003 Fuel/Water Separator for PX-9	0	0
O	Run Aid:None *For Fuel Filter	0	0
S	Start Aid:None *For Fuel Filter	0	0
O	Block heater, PACCAR 750 watt 120V for PX-6 and PX-7, 1000 watt for PX-8 and PX-9 or ISL9 engines .	115	2
S	Alternator: PACCAR 160 amp, brush type	0	0
S	Batteries: 2 PACCAR GP31 threaded post (700-730) 1400-1460 CCA dual purpose.	0	0
O	Mitsubishi 105P55 12V Starter with Cummins and PX PACCAR 12 volt electrical system. W/ centralized power distribution incorporating plug-in style relays. Circuit protection for serviceability, 12- volt light system w/circuit protection circuits number & color coded. Only for Cummins or PX engines.	0	0
O	Battery disconnect switches 2, mounted on battery box.	335	2
O	Multi-function engine connector for body builder interface for Cummins.	44	0

Std/ Opt	Description	\$ List	Weight
O	Jump start terminals under hood.	188	0
O	Body Builder Connector 500mm Back of Cab Requires 1900082	237	0
Transmission & Clutch			
O	Transmission: Allison 3000RDS 6-speed w/PTO drive gear. 5th Gen controls. Includes heat exchanger & oil level sensor. Rugged Duty Series for vocational applications. Transynd transmission fluid is standard on all Allison 1000, 2000, 3000 & 4000 series transmissions. Requires a push button shift control code. Oil temperature gauge is standard on class 8 models.	12,289	291
O	Driveline: 2 SPL170XL 1 centerbearing requires 3500057 interaxle driveline.	984	46
O	One bolted centerbearing crossmember. This option upgrades an existing crossmember. The cost does not include the centerbearing and bracket. Crossmember location will be in accordance with Kenworth engineering standards, using the major components specified on the DTPO.	75	11
O	Torque converter included w/Allison Transmission.	0	0
O	Delete Allison Fuel Sense	0	0
O	J1939 Park Brake Auto Neutral	0	0
O	Decline Allison Neutral at Stop Stand-Alone	0	0
O	Rear transmission support springs for transmission PTO applications are required to ensure that engine flywheel housings are not overloaded when transmission PTO's are installed.	75	0
Front Axle & Equipment			
O	Dana Spicer E-1462I Front Axle rated 14.6K 3-1/2in. drop.	313	30
S	Front brakes included w/ front hub package.	0	0
S	Front Brake: 14,600 lb. Bendix air brake package 16.5x5 brakes, cast drums, iron preset 10-bolt hub pilot hubs, hub caps & slack adjusters.	-234	23
O	Front Springs: Taperleaf 14.6K w/ shock absorbers for use on 2010+ chassis only.	267	80
O	Single power steering gear: 14.6K.	428	9
O	Threaded bushings for taperleaf springs 12K - 14.6K replacing rubber: T6/T8, W900B, W990. W9L gets as standard: code not required. Not available with W9S. 13K - 14.6K for T370.	43	0

Std/ Opt	Description	\$ List	Weight
Rear Axle & Equipment			
O	Single Dana Spicer S23-172 single reduction axle, single rear axle rated at 23K.	1,065	90
O	Rear Axle Ratio - 5.38.	0	0
S	Single rear brakes included w/rear hub package.	0	0
S	26K air brake package includes 16-1/2x7 in. brake , cast drums, iron preset 10-bolt hub pilot hubs, slack adjusters	-555	2
S	Spring Brake: 3030 long stroke single 3 in. travel. Helps keep brakes in adjustment longer.	2	2
S	Bendix 4S/4M anti-lock brake system.	0	0
O	Rear suspension: single Reyco 79KB taperleaf 23K. Medium-duty. Unladen Height: 9 in. Laden Height: 7.3 in. Not rear air disc brake compatible.	469	57
O	Bolted rear suspension crossmembers for Reyco 79KB. Replaces T3 standard.	99	41
Tires & Wheels			
O	Front Tires: Goodyear Endurance RSA 11R22.5 16PR	178	7
U	Rear Tires: Goodyear G182 RSD GHG 11R22.5 16PR	1,156	132
O	Rear Tire Quantity: 4	0	0
O	Front Wheel: Accuride 50344 22.5x8.25 steel Steel Armor(TM) powder coat, hub-pilot mount.heavy-duty 5 hand-hole hub pilot mount.	81	20
O	Rear Wheel: Accuride 50344 22.5x8.25 steel Steel Armor(TM) powder coat, hub-pilot mount. Heavy-duty 5 hand-hole hub pilot mount. Code is priced per pair of wheels.	162	40
O	Powder coat white steel wheel. Use in conjunction with front, dual front, rear, spare or lift axle wheel code(s). All wheels on chassis must have same finish color.	0	0
O	Rear Wheel/Rim Quantity: 4	0	0
Frame & Equipment			
O	Frame Rails: 10-3/4 x 3-1/2 x 3/8in. Steel 285in. to 336 in. Truck frame weight is 3.48 lb.-in. per pair of rails. Section modulus is 17.80, RBM is 2,132,000 in.-lbs per rail. Frame rail availability may be restricted based upon application, axle/suspension capacity, fifth wheel setting, or component/dimensional specifications. The results of the engineering review may result in a change to the requested frame rail. If a change is required Kenworth Application Engineering will advise the dealer of the appropriate material specification for a substitute rail.	535	354
S	Bumper: Aerodynamic, Painted. Requires a	0	0



Std/ Opt	Description	\$ List	Weight
	bumper setting code.		
S	40.9 in. Bumper setting. Requires a bumper code.	0	0
O	Removable Front Tow Hooks: 2.	253	15
S	Front mudflaps.	0	0
O	Custom Frame Layout: one chassis CFL AD: INSIDE FRAME BOC DRIVER SIDE CFL AT: FRONT OF REAR SUSPENSION PARALLEL TO RAIL	1,380	0
O	Battery box cantilever aluminum BOC with fiberglass cover.	23	0
O	Battery box location: RH Side.	92	15
O	T270/370 Non-polished 2010 or later DPF/SCR cover with cab access step assembly, RH under. End plates will be painted standard black frame color.	0	0
O	Five-piece bolted crossmember assembly with 12mm frame fasteners, center and rear frame.	162	-32
O	Bolted Rear Cab Support Crossmember. Replaces T3 standard.	134	17
S	Square end-of-frame w/o crossmember; non-towing.	0	0

Fuel Tanks & Equip

O	Fuel Tank: 75 US gallon 24.5in. aluminum under replace. Class 8 fuel tanks w/o locking caps include an anti-siphon device on the filler neck.	53	-16
O	Small round DEF tank. 11 gallons of usable volume. The DEF tank will be located on the side you specified. If you have specific configuration or body builder concerns, please utilize the Custom Frame Layout option. Standard capacity is calculated by fuel capacity of the vehicle and will accommodate two diesel fill-ups for every DEF fill-up. For 1:1 DEF fuel fill ratio, add 7889204.	142	0
O	Fuel Tank Steps: 6in. wide upper & lower F/1 LH under round tank 24.5in. NFPA Compliant. Includes fuel tank crossbrace.	155	8
O	Polished stainless steel tank straps for 1 tank.	62	0
S	DEF to fuel fill ratio 2:1 or greater.	0	0
S	DEF tank location is LH.	0	0
O	Location: 75 gal fuel tank LH under cab	0	0

Cab & Equipment

S	Cab: Curved Glass Conventional. Cab Includes aluminum & fiberglass fully hucked cab w/ all aluminum bulkhead doors & continuous stainless steel piano-style door hinges. Single electric horn standard. Incandescent exterior lights include	0	0
---	--	---	---

Std/ Opt	Description	\$ List	Weight
	diagnosable bulb detection and warning. Trailer cable on tractors includes integrity detection. Standard features include multiplex wiring for interior lights, automated pre-trip inspection (excluding T3 series), short and open check diagnostics. Warning alarm will sound when lights are left on.		
S	Hood: Sloped aerodynamic hood includes grill & separate bumper.	0	0
S	Cab heater: W/integral defrosters & A/C 45,000 btu cab heater. No sleeper heater/AC. Includes 5 mode rotary control. T660 include filter media.	0	0
S	Adjustable telescoping tilt steering column.	570	10
O	5 sets of keys. Replaces standard 2 sets of keys.	28	0
O	Information for customer-installed PTO Chelsea 880. 8-bolt. N/A w/ Allison 1000/2000 series transmissions.	47	0
O	Switch & Wiring for Customer-Installed PTO. Electric over hydraulic PTO. Includes switch guard. Wiring is routed to LH frame for connection to the customer installed PTO. No air controls are provided with this code.	221	0
U	Information for customer-installed PTO	44	0
O	Two spare switches: Wired to power.	78	0
O	30 Amp Power Lead With Ground. Wired Through Ignition To Behind Rider Seat.	91	1
O	Gauge: Dash mounted air filter restriction gauge.	121	0
O	Gauge: Fuel filter restriction gauge.	145	0
S	Instrument package: Includes speedometer, tachometer, fuel gauge, engine coolant temperature gauge, engine oil pressure, voltmeter. Class 8 also includes primary & secondary air reservoir gauges & an air application gauge. DEF level gauge and warning lamp are included with 2010+ engines. Engine hour meter and outside air temperature readouts are standard. Primary read out will be MPH. Add 8240620 to switch primary scale to KPH in Canada.	0	0
S	Cab interior: Pinnacle. Includes vinyl headliner & cab back panel, slate gray interior, dark slate gray seats, floormats, LH/RH inside sunvisor & door courtesy lights.	0	0
O	Driver seat: Kenworth Air cushion Plus HB cloth. Standard features includes 7 in. fore and aft slide adjustment w/isolator, 6-23 degree recline, air suspension with cover, dual armrests, and single chamber air lumbar support. Seat cushion is 20 inches wide w/ 2-position tilt and 2-position front cushion extension. Seat material has a horizontal stitch pattern and is 2-tone in color. Seat back is carpeted and includes a map pocket. Seat is manufactured by National. Includes inside visor and retractable 3-point matching seat belts. Grey seat belts.	144	0
O	Rider seat: Kenworth Air cushion Plus HB cloth. Standard features includes 7 in. fore and aft slide adjustment w/isolator, 6-23 degree recline, air suspension with cover, dual armrests, and	255	0

Std/ Opt	Description	\$ List	Weight
	single chamber air lumbar support. Seat cushion is 20 inches wide w/ 2-position tilt and 2-position front cushion extension. Seat material has a horizontal stitch pattern and is 2-tone in color. Seat back is carpeted. Seat is manufactured by National. Includes inside visor and retractable 3-point matching seat belts. Grey seat belts.		
O	Under-dash center console: W/2 cup holders, 1 ashtray, 1 lighter, 1 12V outlet & a storage compartment.	96	0
S	Self cancelling turn signal: W/head light dimmer switch .	22	0
S	Cab access contoured grabhandles, LH/RH.	0	0
O	Grabhandle: LH inside door frame above dash.	31	0
O	Grabhandle: RH inside door frame above dash.	31	2
S	Daylite Door: LH/RH includes RH peeper window	0	0
S	Dual convex mirrors 7-1/2 in. w/ offset mounting, and non-heated.	0	0
S	Look-Down, Pass. Door, Stainless 8.5x4.4	0	0
O	Mirror: Dual Prutsman mirror 7 in. x 16 in. polished stainless steel, thermostatically controlled. switch located on door pad.	47	0
S	Mirror brackets 8-1/2 ft load width.	0	0
S	Rear Cab Stationary Window 17 in. x 36 in.	0	0
O	Electric-powered LH & RH door window lifts. Switch located on door.	162	0
O	Exterior stainless steel sunvisor.	401	11
Lights & Instruments			
S	Headlamps: Halogen Projector Low Beam, Halogen Complex Reflector High Beam	0	0
O	Marker Lights: Five, rectangular, LED	89	0
S	Turn Signal Lights: Mounted on fender	0	0
O	Stop,Turn,Taillight: Trucklite 44 Series 6 LED Red With Two LED Backup Lights. Flange Mounted. Taillights Come On When Fog Lights Are On. With An LED License Plate.	151	3
O	Circuit Breakers: Replacing fuses. Does not apply to any 5-amp fuse box position. Brakers include stop/brake/turn, tail lamp, high & low beams, marker/clearance lamps, horn, fuel heat, gauges, air dryer, HVAC controls, panel lamps. Some circuits will remain fuses.	37	0
Air Equipment			

<i>Std/ Opt</i>	<i>Description</i>	<i>\$ List</i>	<i>Weight</i>
U	Air Dryer Bendix AD-IS heated Puraguard	0	0
O	Moisture ejection valve w/ pull cable drain.	14	0
O	Locate air dryer inside LH rail BOC. This code requires the use of a custom frame layout code.	0	0
O	Air tanks: mounted inside frame flanges where possible. This code requires the use of a custom frame layout code.	0	0

Extended Warranty

O	Base Warranty - PACCAR PX-9 Engine 24 months / 250,000 miles / 402,336 km / 6250 hours.	0	0
S	Base Warranty - Standard Service Medium Duty 12 months / Unlimited miles & km	0	0

Miscellaneous

O	GHG Secondary Manufacturer: Does Not Apply	0	0
---	---	---	---

Promotions

Paint

O	Paint color number(s).	0	0
	N9702 A - L0006 WHITE N9770 BUMPER L0006 WHITE N9720 FRAME N0001 BLACK		
S	Bumper Painted Color A	80	0
O	Day Cab Standard Paint	0	0
S	1 - Color Paint - Day Cab Color will be White if no other color is specified.	0	0
S	Base coat/clear coat. The Kenworth Color Selector contains additional instructions, as well as information on Kenworth paint guidelines and surface finish applications. Kenworth is standard with Dupont Imron Elite paint.	0	0

Order Comments



Total List Price (W/O Freight & Warranty & Surcharges)	\$125,161
Marketing and Service Support Fee	\$740
Prepaid Freight	\$2,475
Total Surcharge/Options Not Subject To Discount	\$0
Total Weight	11,559

Prices and Specifications Subject to Change Without Notice.

Unpublished options may require review/approval.
Dimensional and performance data for unpublished options may vary from that displayed in CRM.

PRICING DISCLAIMER

While we make every effort to maintain the web site to preserve pricing accuracy, prices are subject to change without notice. Although the information in this price list is presented in good faith and believed to be correct at the time of printing, we make no representations or warranties as to the completeness or accuracy of this information. We reserve the right to change, delete or otherwise modify the pricing information which is represented herein without any prior notice. We carefully check pricing specifications, but occasionally errors can occur, therefore we reserve the right to change such prices without notice. We disclaim all liability for any errors or omissions in the materials. In no event will we be responsible for any damages of any nature whatsoever from the reliance upon information from these materials. Please check your order prebills to confirm your pricing information

Shipping Destinations

Intermediate Destination:

Final Destinations	Quantity
--------------------	----------



SWS Equipment, Inc.

P.O. Box 13040
Spokane Valley, WA 99213-3040
509-533-9000 Fax: 509-533-1050
1-800-892-7831

QUOTE

Quote #: PHFDQ5753

Date: 10/01/19

Sales Rep: Phil Davison

Customer No:

Quote To:

Kenworth Sales - Spokane
Shane Petersen
6420 E Broadway Ave
Spokane Valley WA 99220
509-710-2106 Fax:

Ship To:

City of Spokane, Sewer Maintenance
Mike Lowdon
909 E Sprague Avenue
Spokane WA 99202
509-625-7927

FOB: Destination

Ship Via: Bestway

Est. Ship Date: 16 wks receipt of Chassis

Terms: Net 30

We are pleased to propose the following for your consideration

Qty	Description	Unit Price	Ext. Price
1	One (1) 2200 US gallon vacuum tank truck system, c/w Masport Hydra pump - mounted onto chassis.	\$88,985.72	\$88,985.72

Compartment: Single

Head Material: 5/16" carbon steel

Shell Material: 1/4" carbon steel

Tank Diameter: 69" x 120" shell length plus 26" heads = 146" OAL

Baffles: One (1) - A-Frame - 3/8" plate fully welded both sides with 1/4" pad

Walkways: Full-length tank mounted 10 gauge combination hose trays and fenders mounted both sides of unit - c/w four (4) anti-sail mud-flaps.

Ladder: Drivers side with anti-slip rungs.

Level Indicator: Three (3) 5" site glass assemblies mounted onto rear tank head.

Man-ways: One (1) 21" diameter top man-way c/w s/s bolts and eye nuts.

Man-ways: One (1) 21" diameter rear man-way mounted low as possible to allow complete drainage of tank.

Running and All lights sealed beam system with all wiring to junction box.

Clearance Lights: One (1) spotlight and (1) beacon light mounted on rear of tank (1) beacon light behind the cab.

Valving: (Discharge) One (1) 4" quick open brass gate valve c/w aluminum male coupling and cap.

Qty	Description	Unit Price	Ext. Price
	(Intake) One (1) 3" quick open brass gate valve c/w aluminum male coupling and cap including 3" diameter riser pipe and impact wear plate.		
	(Water Fill) One (1) 2 1/2" water fill piped to top of tank with ball valve shut off and camlock fittings.		
	Primary Shut Off: One (1) 3" port diameter - 500 cfm rated heavy-duty industrial shut-off. This system incorporates stainless steel "Rocket" float and stainless steel cartridge mesh filter that can easily be removed from top for cleaning purposes.		
	Secondary Shut Off: One (1) 3" port diameter - 500 cfm rated heavy-duty 10-gallon secondary c/w drain valve.		
	Relief Valve : One (1) 2" pressure relief & One (1) 1 1/2" vacuum relief, both valves adjustable.		
	Vacuum Pressure Gauge: One (1) 3 1/2" diameter 30/0/30 compound gauge.		
	Back Up Alarm: One (1) decibel rating 97.		
	Tool Box: One (1) tool box - 24" x 24" x 36" mounted on truck frame passenger side, tool box aluminum, waterproof c/w key loc and stainless steel hinge.		
	Hose Hooks: Two (2) hose hooks mounted on rear tank head.		
	Back-up Camera Third Eye Back Up Camera with Color Monitor		
	Vacuum Pump Masport Hydra vacuum pump (maximum 407 cfm and 28" hg.) liquid cooled, automatic oiling system c/w 4 way valve and check valve. Vacuum pump is guaranteed for two (2) years; parts and labor, by Transway Systems Inc.		
	Pump Drive Heavy Duty hydraulic drive in combination with truck PTO - electric shift control.		
	Hydraulic Hoist Front mounted telescopic hoist to lift tank four (4) feet with control valve mounted behind cab on drive side.		
	Pump Muffler and Oil Trap 3" port size rated 500 cfm pump muffler and oil trap c/w filter and drain valve.		
	Tank Finish Tank to be sandblasted exterior, primed two (2) coats and painted two (2) coats one color urethane paint, customers choice of color. Unit to be truck mounted, tested and ready for immediate operation. - White to match chassis.		

Qty	Description	Unit Price	Ext. Price
	Additions to Specification:		
	A support or stop on Large Hatch so the hatch doesn't bang around on the other hatch when open		
	Fill pipe/ 4" air gap needs to be centered over hole when filling and have a pin or lock so it doesn't move around on its own when filling and driving around		
	Hose vent valve to allow air in when draining hose so it can be placed back on truck		
	Additional Strobe or Beacon on or Behind the Cab of the Chassis.		
1	Custom Fabrication By SWS for Cone Holders, Pick Holders and Pole Holders		
1	Large LED Arrowboard		
1	Back up Camera		
1	PDI		
1	Freight		

Order Total \$88,985.72

Please contact me if I can be of further assistance.

PRICES SUBJECT TO CHANGE DUE TO CHANGING STEEL PRICES - THANK YOU!

By: _____ Accepted _____ Date _____

QUOTE VALID FOR 30 DAYS

PAYMENT DUE UPON COMPLETION OF WORK OR AS SPECIFIED ABOVE

ORIGINAL PO

DATE ISSUED: November 08, 2019

Page 1 of 1



CITY OF SPOKANE
PURCHASING
808 W. SPOKANE FALLS BLVD.
SPOKANE, WA 99201-3316
TELEPHONE (509) 625-6400
FAX (509) 625-6413

Purchase Order Number

DO-530459-000

This number must appear
on all invoices, papers and
shipments

Vendor: KENWORTH SALES-SPOKANE
6420 E BROADWAY
P O BOX 3067
SPOKANE WA 99212

Ship To: FLEET SERVICES DEPARTMENT
CITY OF SPOKANE
915 N. NELSON ST.
SPOKANE WA 99202

BUYER		BUYER PHONE #	TERMS	F.O.B.	DELIVERY DATE
MICAELA MARTINEZ		(509) 625-7823	NET 30 DAYS	DESTINATION	--
Quantity	U/M	Part Number/ Description		Unit Price	Total
1	EA	PURCHASE USING SOURCEWELL CONTRACT #081716-KTC. OPR 2019-0937. UNIT 428913.			
		T370 TANK TRUCK		163,763.00	163,763.00
		PRICE INCLUDES ALL OPTIONS			
1	EA	SALES TAX 9.1%		14,902.43	14,902.43
ORDER TO INCLUDE "MATERIAL SAFETY DATA SHEETS" IF REQUIRED				Total	178,665.43

AUTHORIZED SIGNATURE

STANDARD TERMS & CONDITIONS

1. **TAXES:** Unless otherwise indicated, the City agrees to pay all State of Washington sales taxes or use taxes. The City is exempt from federal excise taxes. Business, occupational and personal property taxes are the sole responsibility of the Seller.
2. **CHANGES:**
 - A. No alteration in any of the terms, conditions, delivery, price, quantity or specifications of items ordered will be effective without the written consent of the Accounting Director or above-named buyer.
 - B. In no event will the City agree to any disclaimer of warranties.
 - C. Any response to the City's order which does not contain the words "counteroffer and not acceptance" prominently will be treated as an acceptance of this purchase order on its terms.
3. **FREIGHT TERMS:**
 - A. Unless otherwise specified, all items are to be shipped prepaid F.O.B. Destination.
 - B. Packing lists shall be enclosed in every box or package.
 - C. Regardless of F.O.B. point, Seller agrees to bear all risk of loss, injury or destruction of items ordered while in transit.
4. **ORDERING POLICY:**
 - A. Items shall not be shipped to the City unless a purchase order is received or an authorized purchase order number is given over the phone.
 - B. Items received without an authorized purchase order number will be returned to the Seller at the Seller's expense.



Expenditure Control Form

All Expenditures \$100,000 or more

Today's Date: 6/4/2020	Type of expenditure: Goods
Department: Wastewater Maintenance	
Approving Supervisor: Mike Lowdon	
Amount of proposed Expenditure: 1,842,000	
Funding Source: Wastewater 2020 budget	
Please verify correct funding sources. Please indicate breakdown if more than one funding source.	
Why is this expenditure necessary now?	
Replacement of aging vehicles per the 6 year capital plan!	
What are the impacts if expenses are deferred?	
Increased maintenance costs and down time!	
What alternatives resources have been considered?	
none	
Description of the goods or service and any additional information.	
Equipment includes 1 mechanical rodder, 1 service truck, 1 tank truck, 1 medium excavator, 1 mini excavator, 1 combo sewer cleaner, 1 F150 pickup	
Person Submitting Form/Contact: Mike Lowdon	
CITY ADMINISTRATOR APPROVAL:	BUDGET APPROVAL:
Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>	Yes <input type="checkbox"/> No <input type="checkbox"/>

DocuSigned by:

 9C36E3376992442...

6/15/2020

DocuSigned by:

 CBC812B631244E9...

6/15/2020

**Agenda Sheet for City Council Meeting of:**

11/23/2020

Date Rec'd	11/10/2020
Clerk's File #	OPR 2020-0836
Renews #	
Cross Ref #	
Project #	
Bid #	BID 4394-17
Requisition #	RE19470

Submitting Dept	FLEET OPERATIONS
Contact Name/Phone	MIKE LOWDON 625-7909
Contact E-Mail	MLOWDON@SPOKANECITY.ORG
Agenda Item Type	Purchase w/o Contract
Agenda Item Name	5100-PURCHASE OF SERVICE TRUCK BODY

Agenda Wording

Wastewater would like to purchase a Service Truck. The Chassis will be purchased using Bid# 4394-17, from Freightliner. Total purchase price is \$82,287.46, including tax.

Summary (Background)

The Service Truck will replace a unit that has reached the end of its economic life. We recommend approval for the purchase of a Service Truck Body for the Wastewater Department. Funding for this is included in the Wastewater Department budget.

Lease? NO Grant related? NO Public Works? NO

Fiscal Impact

Expense	\$ 82,287.46	# 431043100943505640499999
Select	\$	#
Select	\$	#
Select	\$	#

Approvals

Dept Head	FLEIGER, NATHAN
Division Director	WALLACE, TONYA
Finance	ALBIN-MOORE, ANGELA
Legal	PICCOLO, MIKE
For the Mayor	ORMSBY, MICHAEL

Additional Approvals

Purchasing	PRINCE, THEA
WASTEWATER MANAGEMENT	LOWDON, MICHAEL

Council Notifications

Study Session\Other	11/16/20
Council Sponsor	Breean Beggs

Distribution List

mmartinez

Briefing Paper

Finance and Administration Committee

Division & Department:	Finance, Fleet Services
Subject:	Purchase of Service Truck Body
Date:	November 16, 2020
Author (email & phone):	Micaela Martinez mmartinez@spokanecity.org 625-7823
City Council Sponsor:	Breean Beggs
Executive Sponsor:	Tonya Wallace
Committee(s) Impacted:	Finance and Administration Committee
Type of Agenda item:	<input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative
Alignment: (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)	Strategic Plan
Strategic Initiative:	Innovative Infrastructure: Maintaining our fleet of support equipment
Deadline:	
Outcome: (deliverables, delivery duties, milestones to meet)	
Background/History: Wastewater would like to purchase a Service Truck. The Chassis will be purchased using Bid# 4394-17, from Freightliner. Total purchase price is \$90,000.00, including tax.	
Executive Summary: <u>Impact</u> <ul style="list-style-type: none"> The Service Truck will replace a unit that has reached the end of its economic life. <u>Action</u> <ul style="list-style-type: none"> We recommend approval for the purchase of a Service Truck Body for the Wastewater Department. <u>Funding</u> <ul style="list-style-type: none"> Funding for this is included in the Wastewater Department budget. 	
Budget Impact: Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If new, specify funding source: Other budget impacts: (revenue generating, match requirements, etc.)	
Operations Impact: Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Specify changes required: Known challenges/barriers:	



GORDON TRUCK CENTERS DEALER FAMILY

November 10, 2020

City Of Spokane

915 N. Nelson St

Spokane, WA 99202

Quote:

Wastewater Sewer Truck Build -

11' Service Body w/ custom Canopy 73" High.

Custom enclosed pipe storage box to be fabricated and installed on Streetside upper shelf compartment. Access door at rear of body.

Small windows in the top Streetside and Curbside of canopy. Window in Front wall of SB covered with wire mesh.

2000 Watt inverter installed and wired DC outlet at rear of body. 110 outlet at rear of body.

15 gallon Air Tank installed in the Truck Frame plumbed to an underhood compressor.

Work platform bumper with Vise, Cone holders, Custom pipe tool holders.

Cone Holders on Front Bumper (2)

Loud Speaker, LED Traffic advisor board, (2) Flood lights at rear of body.

Strobe lights at front and rear of body center with limb guard.

Expanded metal doors with latches to be fabricated on the streetside upper shelf compartments.

Open Shelf on the front bulkhead with hooks welded underneath to hold buckets.

Sign brackets to be fabricated and attached on side walls inside the cargo area.

(3) Holes cut in the body (front and back) at the bottom of mid height canopy shelf to hold long stick tools.

3" x 4' round pipe to be mounted on the Street side of the cargo side wall at a 20 degree angle to hold long tools (2-rows).

Water drain slots to be cut in the back wall of the body.

Hydraulic Reservoir, PTO & Pump.

10,000 Lbs Hydraulic Rope Winch to hold 1200 feet of (customer provided 1/2 Nylon Rope). Pulley system designed in the body and power switch panel at rear of body with air actuated Clutch brake.

Class V Hitch

Paint White

Price as per prior build on SR001096657:01	\$71,964.20
Model Year Price Escalator	\$ 3,598.21
Sales Tax 8.9%	<u>\$ 6,725.05</u>
Total:	\$82,287.46

Jim Banner | Freightliner Northwest – Spokane, WA | Fleet & Municipality Sales

(888) 744-0390 x 6550 | Cell (509) 991-4374 | jim.banner@freightlinernw.com

10310 Westbow Blvd Spokane, WA 99224



www.FreightlinerNorthwest.com



Expenditure Control Form

All Expenditures \$100,000 or more

Today's Date: 6/4/2020	Type of expenditure: Goods
Department: Wastewater Maintenance	
Approving Supervisor: Mike Lowdon	
Amount of proposed Expenditure: 1,842,000	
Funding Source: Wastewater 2020 budget	
Please verify correct funding sources. Please indicate breakdown if more than one funding source.	
Why is this expenditure necessary now?	
Replacement of aging vehicles per the 6 year capital plan!	
What are the impacts if expenses are deferred?	
Increased maintenance costs and down time!	
What alternatives resources have been considered?	
none	
Description of the goods or service and any additional information.	
Equipment includes 1 mechanical rodder, 1 service truck, 1 tank truck, 1 medium excavator, 1 mini excavator, 1 combo sewer cleaner, 1 F150 pickup	
Person Submitting Form/Contact: Mike Lowdon	
CITY ADMINISTRATOR APPROVAL:	BUDGET APPROVAL:
Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>	Yes <input type="checkbox"/> No <input type="checkbox"/>

DocuSigned by:

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6/15/2020

DocuSigned by:

 CBC812B631244E9...

6/15/2020

**Agenda Sheet for City Council Meeting of:**

11/23/2020

Date Rec'd

11/10/2020

Clerk's File #

OPR 2020-0837

Renews #**Submitting Dept**

FLEET OPERATIONS

Cross Ref #**Contact Name/Phone**

MIKE LOWDON 625-7909

Project #**Contact E-Mail**

MLOWDON@SPOKANECITY.ORG

Bid #**Agenda Item Type**

Purchase w/o Contract

Requisition #

RE19470

Agenda Item Name

5100-PURCHASE OF SERVICE TRUCK-CHASSIS

Agenda Wording

Wastewater would like to purchase a Service Truck. The Chassis will be purchased using Washington State Contract 05916, from Columbia Ford. Total purchase price is \$60,499.12, including tax.

Summary (Background)

The Service Truck will replace a unit that has reached the end of its economic life. We recommend approval for the purchase of a Service Truck Chassis for the Wastewater Department. Funding for this is included in the Wastewater Department budget.

Lease? NO

Grant related? NO

Public Works? NO

Fiscal Impact**Budget Account**

Expense \$ 60,499.12

431043100943505690499999

Select \$

#

Select \$

#

Select \$

#

Approvals**Council Notifications****Dept Head**

FLEIGER, NATHAN

Study Session\Other

11/16/20

Division Director

WALLACE, TONYA

Council Sponsor

Breean Beggs

Finance

ALBIN-MOORE, ANGELA

Distribution List**Legal**

PICCOLO, MIKE

mmartinez

For the Mayor

ORMSBY, MICHAEL

Additional Approvals**Purchasing**

PRINCE, THEA

**WASTEWATER
MANAGEMENT**

LOWDON, MICHAEL

Briefing Paper

Finance and Administration Committee

Division & Department:	Finance, Fleet Services
Subject:	Purchase of Service Truck Chassis
Date:	November 16, 2020
Author (email & phone):	Micaela Martinez mmartinez@spokanecity.org 625-7823
City Council Sponsor:	Breean Beggs
Executive Sponsor:	Tonya Wallace
Committee(s) Impacted:	Finance and Administration Committee
Type of Agenda item:	<input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative
Alignment: (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)	Strategic Plan
Strategic Initiative:	Innovative Infrastructure: Maintaining our fleet of support equipment
Deadline:	
Outcome: (deliverables, delivery duties, milestones to meet)	
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Budget Impact: Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If new, specify funding source: Other budget impacts: (revenue generating, match requirements, etc.)	
Operations Impact: Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Specify changes required: Known challenges/barriers:	

Martinez, Micaela

From: NOREPLY@des.wa.gov
Sent: Wednesday, November 4, 2020 12:34 PM
To: Martinez, Micaela
Cc: noreply@des.wa.gov
Subject: Vehicle Quote - 2020-11-58 - SPOKANE, CITY OF - 23210

[CAUTION - EXTERNAL EMAIL - Verify Sender]

Vehicle Quote Number: 2020-11-58 [Create Purchase Request](#) [View organization purchase requests](#)

This is a **quote** only. You must create a purchase request to order this vehicle(s)

Contract & Dealer Information

Contract #: 05916

Dealer: Columbia Ford (W403)
700 7th Avenue
Longview WA 98632

Dealer Contact: Marie Tellinghiusen
Dealer Phone: (360) 423-4321 Ext: 187
Dealer Email: orders@colford.com

Organization Information

Organization: SPOKANE, CITY OF - 23210
Email: mmartinez@spokanecity.org

Quote Notes: Wastewater Unit
Vehicle Location: SPOKANE CITY

Color Options & Qty

Oxford White (Z1) - 1

Tax Exempt: N

Vehicle Options

Order Code	Option Description	Qty	Unit Price	Ext. Price
2021-0914-0001	2021 Ford F550 4WD Cab and Chassis	1	\$37,584.00	\$37,584.00
2021-0914-0002	INFORMATION ONLY: Columbia Ford offers a \$300 Prompt Payment Discount if payment is received within 20 days of vehicle delivery.	1	\$0.00	\$0.00
2021-0914-0003	INFORMATION ONLY: Columbia Ford CARS Cancellation Fees: NO fee to cancel order if vehicle has not been scheduled for production and is able to be cancelled at factory. \$500 cancellation fee if vehicle has been serialized and is locked in for production by manufacturer. \$750 cancellation fee if vehicle has been delivered to customer and must be picked up by dealer and re-stocked into inventory. Absolutely NO cancellation if customer has licensed/registered vehicle. Upfits/Equipment ordered for vans, trucks, chassis cabs and police/fire vehicles will have a 10-30% re-stocking fee; custom bodies cannot be cancelled.	1	\$0.00	\$0.00
2021-0914-0004	INFORMATION ONLY: Chassis Upfits (service bodies, flatbeds, dump bodies, cranes, snow plows, liftgates, lighting, etc.) are available and will be installed prior to vehicle delivery. To view, at bottom of page, check the box to the right of DISPLAY UPFIT OPTIONS. (#1000-1999 Allied Body Works) (#2000-2999 PMI Truck Bodies) (#3000-3999 Northend Truck Equipment). Do not mix-n-match upfit options between upfitters. All upfits must be ordered from ONE (1) upfitter.	1	\$0.00	\$0.00
2021-0914-0010	2021 Ford F550 4WD Cab and Chassis, Regular Cab, 145WB, 60CA, DRW, 18,000# GVWR, 7.3L 2V DECVT NA V8 Gas, 10-Speed Automatic w/ Selectable Drive Modes, 225/70R19.5G BSW A/P Tires	1	\$0.00	\$0.00

(Set/6), 4.88 RAR (F5H/660A/145WB/99N/44G/TGJ/X48) -- THIS IS THE BASE VEHICLE, please review Vehicle Standard Specifications for complete description.

2021-0914-0015 Alternative Wheelbase, Extended Cab, 192WB, 84CA, 18,000# GVWR (X5H/192WB)	1	\$2,901.00	\$2,901.00
2021-0914-0020 GVWR Maximum (19,500# GVWR, 4.88 RAR with Limited Slip, High-Capacity Trailer Tow Package) (Increases GCW from 31,000 Lbs to 40,000 Lbs) (Only available with Diesel Engine)(68M/X8L/535)	1	\$2,001.00	\$2,001.00
2021-0914-0021 Alternative Engine, 6.7L 4-Valve OHV PowerStroke V8 Turbo Diesel B20 with manual push-button Engine-Exhaust Braking/TorqShift 10-Speed automatic w/ Selectable Drive Modes and Transmission Power Take-Off Provision (PTO) (Includes 4.10 RAR, Dual 750CCA Batteries, 220-AMP Alternator, Intelligent Oil Life Monitor) (18,000# GVWR) (99T/44G/62R/X41)	1	\$8,910.00	\$8,910.00
2021-0914-0029 Upfitter Interface Module (18A)	1	\$283.00	\$283.00
2021-0914-0030 Utility Lighting System (LED Side Mirror Spotlights) (Must also order Power Equipment Group #90L/54K) (63A)	1	\$154.00	\$154.00
2021-0914-0031 110V/400W Outlet (1) (includes one in-dash mounted outlet) (to be ordered w/ 40/20/40 seating) (If ordering w/ 6.7L Diesel Engine, includes 332-Amp Dual Alternators #67A) (43C/67A)	1	\$167.00	\$167.00
2021-0914-0042 Operator Commanded Regeneration (OCR) with Active Regeneration Inhibit (Only Available with diesel)(98R)	1	\$240.00	\$240.00
2021-0914-0044 Tires: XL (all cabs), XLT (Ext/Crew only) 225/70Rx19.5G BSW Traction (includes 4 traction tires on the rear and 2 A/P tires on the front (Set/6) (If spare tire option is also ordered, spare tire will be matching A/P tire) (TGM)	1	\$182.00	\$182.00
2021-0914-0048 Front Wheel Well Liners (factory) (61L)	1	\$172.00	\$172.00
2021-0914-0050 Trailer Brake Controller (verified to be compatible with electronic actuated drum brakes only, includes Smart Trailer Tow Connector) (N/A with Trailer Brake Wiring Kit #531)(52B)	1	\$259.00	\$259.00
2021-0914-0061 Cab Steps, Extended or Crew Cab (6in Angular Black Running Boards)(18B)	1	\$426.00	\$426.00
2021-0914-0062 Cruise Control (525)	1	\$225.00	\$225.00
2021-0914-0063 Daytime Running Lights (replaces standard on/off configuration) (942)	1	\$43.00	\$43.00
2021-0914-0064 Engine Block Heater (41H)	1	\$96.00	\$96.00
2021-0914-0065 Backup Alarm (76C)	1	\$134.00	\$134.00
2021-0914-0067 XL Only - Power Equipment Group (Regular/Extended Cab) [Includes manual-folding, manually-telescoping trailer tow mirrors with power heated glass, power windows, power door locks, remote keyless entry) (Includes (4) RKE Fobs w/ Integrated Key) (deletes passenger door lock cylinder) (90L/546)	1	\$1,110.00	\$1,110.00
2021-0914-0075 Rearview Camera and Prep Kit (includes loose camera and wiring bundle) (872) (Info: Upfitters offer rearview camera installation option with body orders)	1	\$396.00	\$396.00
2021-0914-0199 Warranty, Delayed Start (End-User submits request at www.fordwsd.com)	1	\$0.00	\$0.00
2021-0914-0206 Floor Mats, HD Rubber Molded, Front (Weather Tech)(DLR)	1	\$120.00	\$120.00
2021-0914-0207 Floor Mats, HD Rubber Molded, Rear (Weather Tech) (DLR)	1	\$100.00	\$100.00
2021-0914-0211 Service Manual, CD (DLR)	1	\$221.00	\$221.00

2021-0914-0212 Service Wiring Diagram, Paper (DLR)	1	\$87.00	\$87.00
--	---	---------	---------

Quote Totals

		Total Vehicles:	1
		Sub Total:	\$55,811.00
		8.4 % Sales Tax:	\$4,688.12
		Quote Total:	\$60,499.12

Contract Summary

Motor Vehicles

Contract #: 05916

Replaces: [03513](#) , [03613](#) ,
[03713](#) , [03813](#)

Contract Type: MASTER CONTRACT

Due to COVID -19 and the Defense Production Act, the following manufactures will have very limited capacity for orders. If your agency is looking to take delivery of vehicles please reach out to the dealership or Sean Hoffert at descarssystem@des.wa.gov .

FORD

CHEVROLET

DODGE

RAM

Statements from Dwayne Lane:

Current orders in the system and new orders taken will be delayed due to the current circumstances. We do not know how much of a delay this is going to be, but we will keep you informed of any updates. At this time we are still taking new orders for any vehicles that are open in CARS, with the understanding that delivery times will be delayed, and possible future cancellations could occur.

For dealerships site to government sales:

<https://www.powersportsnorthwest.com/--governmentsales>

State agencies, colleges and universities*

Step 1. Submit Passenger Vehicle Purchase Request

<https://des.wa.gov/services/travel-cars-parking/vehicle-purchasing/passenger-vehicle-purchase-request>

Step 2. Quote vehicles and submit orders through the Contract Automobile Request System (CARS).

<https://fortress.wa.gov/ga/apps/CARS/ContractVehicleMenu.aspx>

* DES Fleet Operations will handle this process for state agencies with fleets managed by DES

~~~~~

## Other Qualified Purchasers

### Step 1. Ensure you have a valid Master Contract Use Agreement

<https://apps.des.wa.gov/DESContracts/Home/MCUAListing>

### Step 2. Quote vehicles and submit orders through the Contract Automobile Request System (CARS).

<https://fortress.wa.gov/ga/apps/CARS/ContractVehicleMenu.aspx>

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Effective Date: **01-05-2017**

Current Term Ends On: **01-01-2022**

Est. Annual Worth: **\$115,000,000**

Final Term Ends On: **01-05-2025**

Commodity Code(s): **071-04, 071-05, 071-17, 071-56, 071-77, 071-80, 071-90, 071-91, 071-92, 072-01, 072-02, 072-03, 072-04, 072-05, 072-06, 072-07**

Diversity: **0% WBE 0% MBE**

# of Bids Received: **18**

---

Contact Info:

**Sean Hoffert**

📞 (360) 407-8016

✉️ [sean.hoffert@des.wa.gov](mailto:sean.hoffert@des.wa.gov)

---

Who Can Use This Contract?

- [Organizations with Master Contract Usage Agreements](#)
- [MCUA Customer Communication Profile](#)



- [Oregon Coop Members](#)

- Pricing & Ordering
- Specifications
- 05916a02\_Bud\_Clary\_Chevrolet\_Extension
  - 05916a02\_Columbia\_Ford\_Extension
  - 05916a02\_Nissan\_Extension
  - 05916a2\_Toyota\_Extension
  - Contract & Amendments
- Original Solicitation Documents
- Bid Tab
- Memo to File

*This Contract has no Resource Documents*

Showing 1 to 15 of 15 Vendors



All

▼

Vendors Per Page.

Search Vendors:

1

| Vendor                                       | Vendor # | Authorized Fulfillment Partners | OMWBE | Veteran | Small Business                                                                        |
|----------------------------------------------|----------|---------------------------------|-------|---------|---------------------------------------------------------------------------------------|
| NW84, INC                                    | W11525   |                                 |       |         |  |
| HASELWOOD COURTER ENTERPRISES, INC           | W11732   |                                 |       |         |                                                                                       |
| NORTHSOUND AUTO GROUP LLC                    | W1675    |                                 |       |         |                                                                                       |
| BUD CLARY AUTO DEALERSHIPS                   | W262     |                                 |       |         |                                                                                       |
| A & L SPORTS, INC. DBA POWERSPORTS NORTHWEST | W30668   |                                 |       |         |  |
| COLUMBIA FORD LINCOLN                        | W403     |                                 |       |         |                                                                                       |
| SPOKANE MERCEDES-BENZ OF SPOKANE             | W42721   |                                 |       |         |                                                                                       |
| BYD MOTORS INC.                              | W48491   |                                 |       |         |                                                                                       |



| Vendor <span>↑↓</span>         | Vendor # <span>↑↓</span> | Authorized Fulfillment Partners <span>↑↓</span> | OMWBE <span>↑↓</span> | Veteran <span>↑↓</span> | Small Business <span>↑↓</span>                                                      |
|--------------------------------|--------------------------|-------------------------------------------------|-----------------------|-------------------------|-------------------------------------------------------------------------------------|
| RWC INTERNATIONAL LTD          | W49789                   |                                                 |                       |                         |                                                                                     |
| TEC EQUIPMENT, INC.            | W5343                    |                                                 |                       |                         |                                                                                     |
| ALAN WEBB NISSAN               | W59283                   |                                                 |                       |                         |  |
| DONOLSON CORP.                 | W60074                   |                                                 |                       |                         |  |
| TESLA, INC.                    | W61494                   |                                                 |                       |                         |                                                                                     |
| RAYMOND HANDLING CONCEPTS CORP | W6181                    |                                                 |                       |                         |                                                                                     |
| TOYOTA OF YAKIMA               | W6870                    |                                                 |                       |                         |                                                                                     |

M = OMWBE Certified Minority Owned | W = OMWBE Certified Women Owned |  
MW = OMWBE Certified Minority Women Owned



= Veteran Owned



= Small Business

Didn't find what you were looking for?

The Contracts Resource Center is here to help.

 (360) 407-2210



[contractingandpurchasing@des.wa.gov](mailto:contractingandpurchasing@des.wa.gov)



 [Find a Contracts Specialist](#)



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# Expenditure Control Form

## All Expenditures \$100,000 or more

|                                                                                                                                                   |                                                          |
|---------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------|
| <b>Today's Date:</b> 6/4/2020                                                                                                                     | <b>Type of expenditure:</b> Goods                        |
| <b>Department:</b> Wastewater Maintenance                                                                                                         |                                                          |
| <b>Approving Supervisor:</b> Mike Lowdon                                                                                                          |                                                          |
| <b>Amount of proposed Expenditure:</b> 1,842,000                                                                                                  |                                                          |
| <b>Funding Source:</b> Wastewater 2020 budget                                                                                                     |                                                          |
| Please verify correct funding sources. Please indicate breakdown if more than one funding source.                                                 |                                                          |
| <b>Why is this expenditure necessary now?</b>                                                                                                     |                                                          |
| Replacement of aging vehicles per the 6 year capital plan!                                                                                        |                                                          |
| <b>What are the impacts if expenses are deferred?</b>                                                                                             |                                                          |
| Increased maintenance costs and down time!                                                                                                        |                                                          |
| <b>What alternatives resources have been considered?</b>                                                                                          |                                                          |
| none                                                                                                                                              |                                                          |
| <b>Description of the goods or service and any additional information.</b>                                                                        |                                                          |
| Equipment includes 1 mechanical rodder, 1 service truck, 1 tank truck, 1 medium excavator, 1 mini excavator, 1 combo sewer cleaner, 1 F150 pickup |                                                          |
| <b>Person Submitting Form/Contact:</b> Mike Lowdon                                                                                                |                                                          |
| <b>CITY ADMINISTRATOR APPROVAL:</b>                                                                                                               | <b>BUDGET APPROVAL:</b>                                  |
| Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>                                                                               | Yes <input type="checkbox"/> No <input type="checkbox"/> |

DocuSigned by:  
  
 9C36E3376992442...

6/15/2020

DocuSigned by:  
  
 CBC812B631244E9...

6/15/2020



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/10/2020

**Clerk's File #**

OPR 2019-0885

**Renews #****Submitting Dept**

FLEET OPERATIONS

**Cross Ref #****Contact Name/Phone**

DAVID PAINE 625-6878

**Project #****Contact E-Mail**

DPAINE@SPOKANECITY.ORG

**Bid #**

RFP 5153-20

**Agenda Item Type**

Contract Item

**Requisition #****Agenda Item Name**

5100-INSTALLATION OF RADIO AND ELECTRICAL EQUIPMENT CONTRACT RENEWAL

**Agenda Wording**

Fleet Services would like to renew the Installation of Radio and Electrical Equipment Contract with Racom Corporation for one year, using IRFP #5153-20. Yearly expenditure is \$150,000.

**Summary (Background)**

The Installation of Radio and Electrical Equipment contract will provide timely commissioning of City vehicles. Recommend approval of a renewal contract for Installation of Radio and Electrical Equipment. Funding for this contract is in the Fleet department's budget.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Expense \$ 150,000.00

# 5100-71700-48348-54803-99999

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

FLEIGER, NATHAN

**Study Session\Other**

11/16/20

**Division Director**

WALLACE, TONYA

**Council Sponsor**

Breean Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

mmartinez

**For the Mayor**

ORMSBY, MICHAEL

**Additional Approvals****Purchasing**

PRINCE, THEA



# Briefing Paper

## Finance and Administration Committee

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |                                                                                                                               |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | Finance, Fleet Services                                                                                                       |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | Installation of Radio and Electrical Equipment Contract Renewal                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | November 16, 2020                                                                                                             |
| <b>Author (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                            | Micaela Martinez mmartinez@spokanecity.org 625-7823                                                                           |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Breean Beggs                                                                                                                  |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | Tonya Wallace                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | Finance and Administration Committee                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | <input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                              | Strategic Plan                                                                                                                |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Innovative Infrastructure: Maintaining our fleet of support equipment                                                         |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <b>Background/History:</b><br>Fleet Services would like to renew the Installation of Radio and Electrical Equipment Contract with Racom Corporation for one year, using IRFP #5153-20. Yearly expenditure is \$150,000.                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Summary:</b><br><br><u>Impact</u> <ul style="list-style-type: none"> <li>The Installation of Radio and Electrical Equipment contract will provide timely commissioning of City vehicles.</li> </ul> <u>Action</u> <ul style="list-style-type: none"> <li>Recommend approval of a renewal contract for Installation of Radio and Electrical Equipment.</li> </ul> <u>Funding</u> <ul style="list-style-type: none"> <li>Funding for this contract is in the Fleet department's budget.</li> </ul> |                                                                                                                               |
| <b>Budget Impact:</b><br>Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>If new, specify funding source:<br>Other budget impacts: (revenue generating, match requirements, etc.)                                                                                                                                                               |                                                                                                                               |
| <b>Operations Impact:</b><br>Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>Specify changes required:<br>Known challenges/barriers:                                                                                                                                                                                   |                                                                                                                               |





**Agenda Sheet for City Council Meeting of:**  
11/23/2020

|                       |               |
|-----------------------|---------------|
| <b>Date Rec'd</b>     | 11/10/2020    |
| <b>Clerk's File #</b> | OPR 2019-0855 |
| <b>Renews #</b>       |               |
| <b>Cross Ref #</b>    |               |
| <b>Project #</b>      |               |
| <b>Bid #</b>          |               |
| <b>Requisition #</b>  |               |

|                           |                                     |
|---------------------------|-------------------------------------|
| <b>Submitting Dept</b>    | FLEET OPERATIONS                    |
| <b>Contact Name/Phone</b> | DAVID PAINE 625-6878                |
| <b>Contact E-Mail</b>     | DPAINE@SPOKANECITY.ORG              |
| <b>Agenda Item Type</b>   | Contract Item                       |
| <b>Agenda Item Name</b>   | 5100-TIRE SERVICES CONTRACT RENEWAL |

**Agenda Wording**

Fleet Services would like to renew the Tire Services Contract with Wingfoot Commercial Tire Systems, LLC for one year, using Washington State Contract 00519 for a yearly amount of \$150,000.

**Summary (Background)**

The Tire Service contract will provide timely tire services to all city vehicles. Recommend approval of a renewal contract for Tire Services. Funding for this contract is in the Fleet department's budget.

Lease? NO Grant related? NO Public Works? NO

**Fiscal Impact**

|         |               |                                |
|---------|---------------|--------------------------------|
| Expense | \$ 150,000.00 | # 5100-71700-48348-54803-99999 |
| Select  | \$            | #                              |
| Select  | \$            | #                              |
| Select  | \$            | #                              |

**Approvals**

|                                 |                 |
|---------------------------------|-----------------|
| <b><u>Dept Head</u></b>         | FLEIGER, NATHAN |
| <b><u>Division Director</u></b> | WALLACE, TONYA  |
| <b><u>Finance</u></b>           | ORLOB, KIMBERLY |
| <b><u>Legal</u></b>             | PICCOLO, MIKE   |
| <b><u>For the Mayor</u></b>     | ORMSBY, MICHAEL |

**Additional Approvals**

**Purchasing**

|  |
|--|
|  |
|  |
|  |
|  |

**Council Notifications**

|                                   |              |
|-----------------------------------|--------------|
| <b><u>Study Session\Other</u></b> | 11/16/20     |
| <b><u>Council Sponsor</u></b>     | Breean Beggs |

**Distribution List**



# Briefing Paper

## Finance and Administration Committee

|                                                                                                                                  |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                | Finance, Fleet Services                                                                                                       |
| <b>Subject:</b>                                                                                                                  | Tire Services Contract Renewal                                                                                                |
| <b>Date:</b>                                                                                                                     | November 16, 2020                                                                                                             |
| <b>Author (email &amp; phone):</b>                                                                                               | Micaela Martinez mmartinez@spokanecity.org 625-7823                                                                           |
| <b>City Council Sponsor:</b>                                                                                                     | Breean Beggs                                                                                                                  |
| <b>Executive Sponsor:</b>                                                                                                        | Tonya Wallace                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                    | Finance and Administration Committee                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                      | <input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan) | Strategic Plan                                                                                                                |
| <b>Strategic Initiative:</b>                                                                                                     | Innovative Infrastructure: Maintaining our fleet of support equipment                                                         |
| <b>Deadline:</b>                                                                                                                 |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                              |                                                                                                                               |

Background/History:  
 Fleet Services would like to renew the Tire Services Contract with Wingfoot Commercial Tire Systems, LLC for one year, using Washington State Contract 00519 for a yearly amount of \$150,000.

Executive Summary:

Impact

- The Tire Service contract will provide timely tire services to all city vehicles.

Action

- Recommend approval of a renewal contract for Tire Services.

Funding

- Funding for this contract is in the Fleet department's budget.

Budget Impact:  
 Approved in current year budget? ☒ Yes ☐ No  
 Annual/Reoccurring expenditure? ☐ Yes ☒ No  
 If new, specify funding source:  
 Other budget impacts: (revenue generating, match requirements, etc.)

Operations Impact:  
 Consistent with current operations/policy? ☒ Yes ☐ No  
 Requires change in current operations/policy? ☐ Yes ☒ No  
 Specify changes required:  
 Known challenges/barriers:



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/2/2020

**Clerk's File #**

OPR 2020-0838

**Renews #****Submitting Dept**

ENGINEERING SERVICES

**Cross Ref #****Contact Name/Phone**

DAN BULLER 625-6391

**Project #**

2021060

**Contact E-Mail**

DBULLER@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #**

MASTER

**Agenda Item Name**

0390 - TIERRA - HISTORIC RESOURCE ON-CALL SERVICES

**Agenda Wording**

Local Area A&E Professional Services Consultant Agreement with Tierra Right of Way, Ltd.; (Spokane, WA) for Historic Resource Services for 2021-2022 Non-Federal Aid Project for the amount not to exceed \$200,000.00. (Various Neighborhood Councils)

**Summary (Background)**

The Agreement for The Agreement for Historic Resource Services is for a period of two years. An option for a one year extension will be granted at the City's discretion. Task Assignments will be prepared under this agreement and scope for individual project needs. Fund shall be from the individual project.

Lease? NO

Grant related? YES

Public Works? YES

**Fiscal Impact****Budget Account**

Expense \$ 200,000.00

# Various

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

TWOHIG, KYLE

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

publicworksaccounting@spokanecity.org

**Additional Approvals**

kgoodman@spokanecity.org

**Purchasing**

dbuller@spokanecity.org

**GRANTS &  
CONTRACT MGMT**

STOPHER, SALLY

aduffey@spokanecity.org

mdickerson@tierra-row.com



## Briefing Paper

### PIES

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Engineering Services; Public Works                                                                                            |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | On-Call Engineering Consultants                                                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | October 26, 2020                                                                                                              |
| <b>Contact (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Dan Buller ( <a href="mailto:dbuller@spokanecity.org">dbuller@spokanecity.org</a> , 625-6391)                                 |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | Scott Simmons                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | PIES                                                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Innovative Infrastructure                                                                                                     |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Informational - background information for committee review                                                                   |
| <p><b>Background/History:</b> Engineering Services has “on-call” agreements with various consultants for specialized engineering or related services (geotech., surveying, historic resources, real estate acquisition and construction management) associated with the City’s public works projects. Those firms are selected on the basis of qualifications as required by RCW 39. These typically agreements last from 2-3 years.</p>                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <p><b>Executive Summary:</b></p> <ul style="list-style-type: none"> <li>• A request for qualifications is being advertised for the above name specialized services.</li> <li>• A review committee ranked the firms by qualifications. One or two firms will be selected for each discipline.</li> <li>• Engineering Services expects to bring five agreements to council over the next several weeks.</li> <li>• Costs incurred under the proposed contracts are paid as part of each public works project for which the consultant is used. The projects associated with these contracts are for all public works except Federal Highway Administration (FHWA) funded projects. (A similar briefing paper was submitted for the June PIES committee meeting for FHWA funded projects).</li> </ul> |                                                                                                                               |
| <p><b>Budget Impact:</b></p> <p>Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A</p> <p>Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <p><b>Operations Impact:</b></p> <p>Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A</p> <p>Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |





**City of Spokane**

**CONSULTANT AGREEMENT**

**Title: 2021-2022 HISTORIC RESOURCE  
ON-CALL SERVICES**

This Consultant Agreement is made and entered into by and between the **CITY OF SPOKANE** as ("City"), a Washington municipal corporation, and **TIERRA RIGHT OF WAY SERVICES, LTD.**, whose address is 1575 East River Drive, Suite 201, Tucson, Arizona 85718 as ("Consultant"), individually hereafter referenced as a "party", and together as the "parties".

*WHEREAS, the purpose of this Agreement is to provide **2021-2022 HISTORIC RESOURCE ON-CALL SERVICES** to the City; and*

*WHEREAS, the Consultant was selected through a Request for Qualification issued by the City.*

*-- NOW, THEREFORE, in consideration of the terms, conditions, covenants and performance of the Scope of Work contained herein, the City and Consultant mutually agree as follows:*

**1. TERM OF AGREEMENT.**

The term of this Agreement begins on January 1, 2021, and ends on December 31, 2022, unless amended by written agreement or terminated earlier under the provisions. The contract may be renewed for one (1) additional one-year contract period, subject to mutual agreement.

**2. TIME OF BEGINNING AND COMPLETION.**

The Consultant shall begin the work outlined in the "Scope of Work" ("Work") on the beginning date, above. The City will acknowledge in writing when the Work is complete. Time limits established under this Agreement shall not be extended because of delays for which the Consultant is responsible, but may be extended by the City, in writing, for the City's convenience or conditions beyond the Consultant's control.

**3. SCOPE OF WORK.**

The General Scope of Work for this Agreement is described in the City's Request for Qualification which is attached as Exhibit B and made a part of this Agreement. In the event of a conflict or discrepancy in the contract documents, the City Agreement controls.



The Work is subject to City review and approval. The Consultant shall confer with the City periodically, and prepare and present information and materials (e.g. detailed outline of completed Work) requested by the City to determine the adequacy of the Work or Consultant's progress.

#### **4. COMPENSATION.**

Total compensation for Consultant's services under this On-Call Agreement shall not exceed **TWO HUNDRED THOUSAND AND NO/100 DOLLARS (\$200,000.00)**, excluding tax, if applicable, unless modified by a written amendment to this Agreement. This is the maximum amount to be paid under this Agreement for the work described in Section 3 above, and shall not be exceeded without the prior written authorization of the City in the form of an executed amendment to this Agreement.

#### **5. PAYMENT.**

The Company shall submit its applications for payment to City of Spokane, Engineering Services Department, 808 West Spokane Falls Blvd., Spokane, WA 99201. **Payment will be made via direct deposit/ACH** within thirty (30) days after receipt of the Company's application except as provided by state law. If the City objects to all or any portion of the invoice, it shall notify the Company and pay that portion of the invoice not in dispute. In that event, the parties shall immediately make every effort to settle the disputed amount.

#### **6. REIMBURSABLES**

The reimbursables under this Agreement are to be included, and considered part of the maximum amount not to exceed (above), and require the Consultant's submittal of appropriate documentation and actual itemized receipts, the following limitations apply.

- A. City will reimburse the Consultant at actual cost for expenditures that are pre-approved by the City in writing and are necessary and directly applicable to the work required by this Contract provided that similar direct project costs related to the contracts of other clients are consistently accounted for in a like manner. Such direct project costs may not be charged as part of overhead expenses or include a markup. Other direct charges may include, but are not limited to the following types of items: travel, printing, cell phone, supplies, materials, computer charges, and fees of subconsultants.
- B. The billing for third party direct expenses specifically identifiable with this project shall be an itemized listing of the charges supported by copies of the original bills, invoices, expense accounts, subconsultant paid invoices, and other supporting documents used by the Consultant to generate invoice(s) to the City. The original supporting documents shall be available to the City for inspection upon request. All charges must be necessary for the services provided under this Contract.
- C. The City will reimburse the actual cost for travel expenses incurred as evidenced by copies of receipts (excluding meals) supporting such travel expenses, and in accordance with the City of Spokane Travel Policy, details of which can be provided upon request.
- D. **Airfare:** Not included in this contract.
- E. **Meals:** Not included in this contract.
- F. **Lodging:** Not included in this contract.
- G. **Vehicle mileage:** Vehicle mileage will be reimbursed at the Federal Internal Revenue Service Standard Business Mileage Rate in affect at the time the mileage expense is incurred. Please note: payment for mileage for long distances traveled will not be more than an equivalent trip round-trip airfare of a common carrier for a coach or economy class ticket.



- H. **Rental Car:** Not included in this contract.
- I. **Miscellaneous Travel** (e.g. parking, rental car gas, taxi, shuttle, toll fees, ferry fees, etc.): Miscellaneous travel expenses will be reimbursed at the actual cost incurred. Receipts are required for each expense of \$10.00 or more.
- J. **Miscellaneous other business expenses** (e.g. printing, photo development, binding): Other miscellaneous business expenses will be reimbursed at the actual cost incurred and may not include a mark up. Receipts are required for all miscellaneous expenses that are billed.

**Subconsultant:** Subconsultant expenses will be reimbursed at the actual cost incurred and a four percent (4%) markup. Copies of all Subconsultant invoices that are rebilled to the City are required.

## **7. TAXES, FEES AND LICENSES.**

- A. Consultant shall pay and maintain in current status, all necessary licenses, fees, assessments, permit charges, etc. necessary to conduct the work included under this Agreement. It is the Consultant's sole responsibility to monitor and determine changes or the enactment of any subsequent requirements for said fees, assessments, or changes and to immediately comply.
- B. Where required by state statute, ordinance or regulation, Consultant shall pay and maintain in current status all taxes necessary for performance. Consultant shall not charge the City for federal excise taxes. The City will furnish Consultant an exemption certificate where appropriate.
- C. The Director of Finance and Administrative Services may withhold payment pending satisfactory resolution of unpaid taxes and fees due the City.
- D. The cost of any permits, licenses, fees, etc. arising as a result of the projects included in this Agreement shall be included in the project budgets.

## **8. CITY OF SPOKANE BUSINESS LICENSE.**

Section 8.01.070 of the Spokane Municipal Code states that no person may engage in business with the City without first having obtained a valid annual business registration. The Consultant shall be responsible for contacting the State of Washington Business License Services at <http://bls.dor.wa.gov> or 1-800-451-7985 to obtain a business registration. If the Contractor does not believe it is required to obtain a business registration, it may contact the City's Taxes and Licenses Division at (509) 625-6070 to request an exemption status determination.

## **9. SOCIAL EQUITY REQUIREMENTS.**

No individual shall be excluded from participation in, denied the benefit of, subjected to discrimination under, or denied employment in the administration of or in connection with this Agreement because of age, sex, race, color, religion, creed, marital status, familial status, sexual orientation including gender expression or gender identity, national origin, honorably discharged veteran or military status, the presence of any sensory, mental or physical disability, or use of a service animal by a person with disabilities. Consultant agrees to comply with, and to require that all subcontractors comply with, Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act, as applicable to the Consultant. Consultant shall seek inclusion of woman and minority business for subcontracting. A woman or minority business is one that self-identifies to be at least 51% owned by a woman and/or minority. Such firms do not have to be certified by the State of Washington.



## **10. INDEMNIFICATION.**

The Consultant shall defend, indemnify, and hold the City and its officers and employees harmless from all claims, demands, or suits at law or equity asserted by third parties for bodily injury (including death) and/or property damage which arise from the Consultant's negligence or willful misconduct under this Agreement, including attorneys' fees and litigation costs; provided that nothing herein shall require a Consultant to indemnify the City against and hold harmless the City from claims, demands or suits based solely upon the negligence of the City, its agents, officers, and employees. If a claim or suit is caused by or results from the concurrent negligence of the Consultant's agents or employees and the City, its agents, officers and employees, this indemnity provision shall be valid and enforceable to the extent of the negligence of the Consultant, its agents or employees. The Consultant specifically assumes liability and agrees to defend, indemnify, and hold the City harmless for actions brought by the Consultant's own employees against the City and, solely for the purpose of this indemnification and defense, the Consultant specifically waives any immunity under the Washington State industrial insurance law, or Title 51 RCW. The Consultant recognizes that this waiver was specifically entered into pursuant to the provisions of RCW 4.24.115 and was the subject of mutual negotiation. The indemnity and agreement to defend and hold the City harmless provided for in this section shall survive any termination or expiration of this agreement.

## **11. INSURANCE.**

During the period of the Agreement, the Consultant shall maintain in force at its own expense, each insurance noted below with companies or through sources approved by the State Insurance Commissioner pursuant to RCW Title 48;

- A. Worker's Compensation Insurance in compliance with RCW 51.12.020, which requires subject employers to provide workers' compensation coverage for all their subject workers and Employer's Liability Insurance in the amount of \$1,000,000;
- B. General Liability Insurance on an occurrence basis, with a combined single limit of not less than \$1,000,000 each occurrence for bodily injury and property damage. It shall include contractual liability coverage for the indemnity provided under this agreement. It shall provide that the City, its officers and employees are additional insureds but only with respect to the Consultant's services to be provided under this Agreement; and
- C. Automobile Liability Insurance with a combined single limit, or the equivalent of not less than \$1,000,000 each accident for bodily injury and property damage, including coverage for owned, hired and non-owned vehicles.
- D. Professional Liability Insurance with a combined single limit of not less than \$1,000,000 each claim, incident or occurrence. This is to cover damages caused by the error, omission, or negligent acts related to the professional services to be provided under this Agreement. The coverage must remain in effect for at least two (2) years after the Agreement is completed.

There shall be no cancellation, material change, reduction of limits or intent not to renew the insurance coverage(s) without forty-five (45) days written notice from the Consultant or its insurer(s) to the City. As evidence of the insurance coverage(s) required by this Agreement, the Consultant shall furnish acceptable Certificates Of Insurance (COI) to the City at the time it returns this signed Agreement. The certificate shall specify the City of Spokane as "Additional



Insured” specifically for Consultant’s services under this Agreement, as well as all of the parties who are additional insureds, and include applicable policy endorsements, the forty-five (45) day cancellation clause, and the deduction or retention level. The Consultant shall be financially responsible for all pertinent deductibles, self-insured retentions, and/or self-insurance.

## **12. DEBARMENT AND SUSPENSION.**

The Contractor has provided its certification that it is in compliance with and shall not contract with individuals or organizations which are debarred, suspended, or otherwise excluded from or ineligible from participation in Federal Assistance Programs under Executive Order 12549 and “Debarment and Suspension”, codified at 29 CFR part 98.

## **13. AUDIT.**

Upon request, the Consultant shall permit the City and any other governmental agency (“Agency”) involved in the funding of the Work to inspect and audit all pertinent books and records. This includes work of the Consultant, any subconsultant, or any other person or entity that performed connected or related Work. Such books and records shall be made available upon reasonable notice of a request by the City, including up to three (3) years after final payment or release of withheld amounts. Such inspection and audit shall occur in Spokane County, Washington, or other reasonable locations mutually agreed to by the parties. The Consultant shall permit the City to copy such books and records at its own expense. The Consultant shall ensure that inspection, audit and copying rights of the City is a condition of any subcontract, agreement or other arrangement under which any other persons or entity may perform Work under this Agreement.

## **14. INDEPENDENT CONSULTANT.**

- A. The Consultant is an independent Consultant. This Agreement does not intend the Consultant to act as a City employee. The City has neither direct nor immediate control over the Consultant nor the right to control the manner or means by which the Consultant works. Neither the Consultant nor any Consultant employee shall be an employee of the City. This Agreement prohibits the Consultant to act as an agent or legal representative of the City. The Consultant is not granted express or implied rights or authority to assume or create any obligation or responsibility for or in the name of the City, or to bind the City. The City is not liable for or obligated to pay sick leave, vacation pay, or any other benefit of employment, nor to pay social security or other tax that may arise from employment. The Consultant shall pay all income and other taxes as due. The Consultant may perform work for other parties; the City is not the exclusive user of the services that the Consultant provides.
- B. If the City needs the Consultant to Work on City premises and/or with City equipment, the City may provide the necessary premises and equipment. Such premises and equipment are exclusively for the Work and not to be used for any other purpose.
- C. If the Consultant works on the City premises using City equipment, the Consultant remains an independent Consultant and not a City employee. The Consultant will notify the City Project Manager if s/he or any other Workers are within ninety (90) days of a consecutive 36-month placement on City property. If the City determines using City premises or equipment is unnecessary to complete the Work, the Consultant will be required to work from its own office space or in the field. The City may negotiate a reduction in Consultant fees or charge a rental fee based on the actual costs to the City, for City premises or equipment.

## **15. KEY PERSONS.**



The Consultant shall not transfer or reassign any individual designated in this Agreement as essential to the Work, nor shall those key persons, or employees of Consultant identified as to be involved in the Project Work be replaced, removed or withdrawn from the Work without the express written consent of the City, which shall not be unreasonably withheld. If any such individual leaves the Consultant's employment, the Consultant shall present to the City one or more individuals with greater or equal qualifications as a replacement, subject to the City's approval, which shall not be unreasonably withheld. The City's approval does not release the Consultant from its obligations under this Agreement.

#### **16. ASSIGNMENT AND SUBCONTRACTING.**

The Consultant shall not assign or subcontract its obligations under this Agreement without the City's written consent, which may be granted or withheld in the City's sole discretion. Any subcontract made by the Consultant shall incorporate by reference this Agreement, except as otherwise provided. The Consultant shall require that all subconsultants comply with the obligations and requirements of the subcontract. The City's consent to any assignment or subcontract does not release the consultant from liability or any obligation within this Agreement, whether before or after City consent, assignment or subcontract.

#### **17. CITY ETHICS CODE.**

- A. Consultant shall promptly notify the City in writing of any person expected to be a Consultant Worker (including any Consultant employee, subconsultant, principal, or owner) and was a former City officer or employee within the past twelve (12) months.
- B. Consultant shall ensure compliance with the City Ethics Code by any Consultant Worker when the Work or matter related to the Work is performed by a Consultant Worker who has been a City officer or employee within the past two (2) years.
- C. Consultant shall not directly or indirectly offer anything of value (such as retainers, loans, entertainment, favors, gifts, tickets, trips, favors, bonuses, donations, special discounts, work or meals) to any City employee, volunteer or official that is intended, or may appear to a reasonable person to be intended, to obtain or give special consideration to the Consultant. Promotional items worth less than \$25 may be distributed by the Consultant to a City employee if the Consultant uses the items as routine and standard promotional materials. Any violation of this provision may cause termination of this Agreement. Nothing in this Agreement prohibits donations to campaigns for election to City office, so long as the donation is disclosed as required by the election campaign disclosure laws of the City and of the State.

#### **18. NO CONFLICT OF INTEREST.**

Consultant confirms that the Consultant or workers have no business interest or a close family relationship with any City officer or employee who was or will be involved in the consultant selection, negotiation, drafting, signing, administration or evaluation of the Consultant's work. As used in this Section, the term Consultant includes any worker of the Consultant who was, is, or will be, involved in negotiation, drafting, signing, administration or performance of the Agreement. The term "close family relationship" refers to: spouse or domestic partner, any dependent parent, parent-in-law, child, son-in-law, daughter-in-law; or any parent, parent in-law, sibling, uncle, aunt, cousin, niece or nephew residing in the household of a City officer or employee described above.

#### **19. ERRORS AND OMISSIONS, CORRECTIONS.**



Consultant is responsible for professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on the behalf of the Consultant under this Agreement in the delivery of a final work product. The standard of care applicable to Consultant's services will be the degree of skill and diligence normally employed by professional engineers or Consultants performing the same or similar services at the time said services are performed. The Final Work Product is defined as a stamped, signed work product. Consultant, without additional compensation, shall correct or revise errors or mistakes in designs, drawings, specifications, and/or other consultant services immediately upon notification by the City. The obligation provided for in this Section regarding acts or omissions resulting from this Agreement survives Agreement termination or expiration.

## **20. INTELLECTUAL PROPERTY RIGHTS.**

- A. Copyrights. The Consultant shall retain the copyright (including the right of reuse) to all materials and documents prepared by the Consultant for the Work, whether or not the Work is completed. The Consultant grants to the City a non-exclusive, irrevocable, unlimited, royalty-free license to use copy and distribute every document and all the materials prepared by the Consultant for the City under this Agreement. If requested by the City, a copy of all drawings, prints, plans, field notes, reports, documents, files, input materials, output materials, the media upon which they are located (including cards, tapes, discs, and other storage facilities), software program or packages (including source code or codes, object codes, upgrades, revisions, modifications, and any related materials) and/or any other related documents or materials developed solely for and paid for by the City to perform the Work, shall be promptly delivered to the City.
- B. Patents: The Consultant assigns to the City all rights in any invention, improvement, or discovery, with all related information, including but not limited to designs, specifications, data, patent rights and findings developed with the performance of the Agreement or any subcontract. Notwithstanding the above, the Consultant does not convey to the City, nor does the City obtain, any right to any document or material utilized by the Consultant created or produced separate from the Agreement or was pre-existing material (not already owned by the City), provided that the Consultant has identified in writing such material as pre-existing prior to commencement of the Work. If pre-existing materials are incorporated in the work, the Consultant grants the City an irrevocable, non-exclusive right and/or license to use, execute, reproduce, display and transfer the pre-existing material, but only as an inseparable part of the work.
- C. The City may make and retain copies of such documents for its information and reference with their use on the project. The Consultant does not represent or warrant that such documents are suitable for reuse by the City or others, on extensions of the project or on any other project, and the City releases the Consultant from liability for any unauthorized reuse of such documents.

## **21. CONFIDENTIALITY.**

Under Washington State Law RCW Chapter 42.56) all materials received or created by the City of Spokane are **public records** which are subject to review and copying pursuant to a public records request. These records include but are not limited to bid or proposal submittals, agreement documents, contract work product, and other bid material. Some records or portions of records may be legally exempt from disclosure and can be redacted or withheld. RCW Ch. 42.56 describes those exemptions. Consultant must familiarize themselves with state law and the City of Spokane's process for managing records.



The City will endeavor to redact anything that clearly should be redacted under the law. For example, the City will generally redact Social Security Numbers, tax records, and financial account numbers before records are made available to a requestor. Consultant may identify any materials Consultant believes to be not subject to release under the Public Records Act. City will not be bound by Consultant's determination of whether any particular record or records are legally exempt from release under the Public Records Act.

If the City receives a public records request for records involving Consultant or Consultant's work product, City will release the records unless City determines that there are obvious exemptions or redactions (which City will make prior to release of the records). If City determines that there are exemptions that can be asserted only by Consultant, City will endeavor to notify Consultant and Consultant will be given ten days to obtain a Court order preventing the City from releasing the requested records. **If no Court order is procured by Consultant, the City will release the requested records.**

## **22. DISPUTES.**

Any dispute or misunderstanding that may arise under this Agreement, concerning the Consultant's performance, shall first be through negotiations, if possible, between the Consultant's Project Manager and the City's Project Manager. It shall be referred to the Director and the Consultant's senior executive(s). If such officials do not agree upon a decision within a reasonable period of time, either party may decline or discontinue such discussions and may then pursue the legal means to resolve such disputes, including but not limited to mediation, arbitration and/or alternative dispute resolution processes. Nothing in this dispute process shall mitigate the rights of the City to terminate the Agreement. Notwithstanding all of the above, if the City believes in good faith that some portion of the Work has not been completed satisfactorily, the City may require the Consultant to correct such work prior to the City payment. The City will provide to the Consultant an explanation of the concern and the remedy that the City expects. The City may withhold from any payment otherwise due, an amount that the City in good faith finds to be under dispute, or if the Consultant provides no sufficient remedy, the City may retain the amount equal to the cost to the City for otherwise correcting or remedying the work not properly completed. Waiver of any of these rights is not deemed a future waiver of any such right or remedy available at law, contract or equity.



### **23. TERMINATION.**

- A. For Cause: The City or Consultant may terminate the Agreement if the other party is in material breach of this Agreement, and such breach has not been corrected to the other party's reasonable satisfaction in a timely manner. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- B. For Reasons Beyond Control of Parties: Either party may terminate this Agreement without recourse by the other where performance is rendered impossible or impracticable for reasons beyond such party's reasonable control, such as, but not limited to, an act of nature, war or warlike operation, civil commotion, riot, labor dispute including strike, walkout or lockout, except labor disputes involving the Consultant's own employees, sabotage, or superior governmental regulation or control. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- C. For Convenience: Either party may terminate this Agreement without cause, upon thirty (30) days written notice to the other party.
- D. Actions upon Termination: if termination occurs not the fault of the Consultant, the Consultant shall be paid for the services properly performed prior to the actual termination date, with any reimbursable expenses then due, but such compensation shall not exceed the maximum compensation to be paid under the Agreement. The Consultant agrees this payment shall fully and adequately compensate the Consultant and all subconsultants for all profits, costs, expenses, losses, liabilities, damages, taxes and charges of any kind (whether foreseen or unforeseen) attributable to the termination of this Agreement.
- E. Upon termination, the Consultant shall provide the City with the most current design documents, contract documents, writings and other products the Consultant has produced to termination, along with copies of all project-related correspondence and similar items. The City shall have the same rights to use these materials as if termination had not occurred; provided however, that the City shall indemnify and hold the Consultant harmless from any claims, losses, or damages to the extent caused by modifications made by the City to the Consultant's work product.

### **24. EXPANSION FOR NEW WORK.**

This Agreement scope may be expanded for new work. Any expansion for New Work (work not specified within the original Scope of Work Section of this Agreement, and/or not specified in the original RFP as intended work for the Agreement) must comply with all the following limitations and requirements: (a) the New Work is not reasonable to solicit separately; (b) the New Work is for reasonable purpose; (c) the New Work was not reasonably known either the City or Consultant at time of contract or else was mentioned as a possibility in the solicitation (such as future phases of work, or a change in law); (d) the New Work is not significant enough to be reasonably regarded as an independent body of work; (e) the New Work would not have attracted a different field of competition; and (f) the change does not vary the essential identified or main purposes of the Agreement. The City may make exceptions for immaterial changes, emergency or sole source conditions, or other situations required in City opinion. Certain changes are not New Work subject to these limitations, such as additional phases of Work anticipated at the time of solicitation, time extensions, Work Orders issued on an On-Call contract, and similar. New Work must be mutually agreed and issued by the City through written Addenda. New Work performed before an authorizing Amendment may not be eligible for payment.



## **25. MISCELLANEOUS PROVISIONS.**

- A. Amendments: No modification of this Agreement shall be effective unless in writing and signed by an authorized representative of each of the parties hereto.
- B. Binding Agreement: This Agreement shall not be binding until signed by both parties. The provisions, covenants and conditions in this Agreement shall bind the parties, their legal heirs, representatives, successors and assigns.
- C. Americans with Disabilities Act (ADA): Specific attention by the designer is required in association with the Americans with Disabilities Act (ADA) 42 U.S.C. 12101-12213 and 47 U.S.C. 225 and 611, its requirements, regulations, standards and guidelines, which were updated in 2010 and are effective and mandatory for all State and local government facilities and places of public accommodation for construction projects including alteration of existing facilities, as of March 15, 2012. The City advises that the requirements for accessibility under the ADA, may contain provisions that differ substantively from accessibility provisions in applicable State and City codes, and if the provisions of the ADA impose a greater or equal protection for the rights of individuals with disabilities or individuals associated with them than the adopted local codes, the ADA prevail unless approval for an exception is obtained by a formal documented process. Where local codes provide exceptions from accessibility requirements that differ from the ADA Standards; such exceptions may not be permitted for publicly owned facilities subject to Title II requirements unless the same exception exists in the Title II regulations. It is the responsibility of the designer to determine the code provisions.
- D. The Consultant, at no expense to the City, shall comply with all laws of the United States and Washington, the Charter and ordinances of the City of Spokane; and rules, regulations, orders and directives of their administrative agencies and officers. Without limiting the generality of this paragraph, the Consultant shall comply with the requirements of this Section.
- E. This Agreement shall be construed and interpreted under the laws of Washington. The venue of any action brought shall be in the Superior Court of Spokane County.
- F. Remedies Cumulative: Rights under this Agreement are cumulative and nonexclusive of any other remedy of law or in equity.
- G. Captions: The titles of sections or subsections are for convenience only and do not define or limit the contents.
- H. Severability: If any term or provision is determined by a court of competent jurisdiction to be invalid or unenforceable, the remainder of this Agreement shall not be affected, and each term and provision shall be valid and enforceable to the fullest extent permitted by law.
- I. Waiver: No covenant, term or condition or the breach shall be deemed waived, except by written consent of the party against whom the waiver is claimed, and any waiver of the breach of any covenant, term or condition shall not be deemed a waiver of any preceding or succeeding breach of the same or any other covenant, term of condition. Neither the acceptance by the City of any performance by the Consultant after the time the same shall have become due nor payment to the Consultant for any portion of the Work shall constitute a waiver by the City of the breach or default of any covenant, term or condition unless otherwise expressly agreed to by the City in writing.
- J. Additional Provisions: This Agreement may be modified by additional terms and conditions ("Special Conditions") which shall be attached to this Agreement as an Exhibit. The parties agree that the Special Conditions shall supplement the terms and conditions of the Agreement, and in the event of ambiguity or conflict with the terms and conditions of the Agreement, these Special Conditions shall govern.



- K. Entire Agreement: This document along with any exhibits and all attachments, and subsequently issued addenda, comprises the entire agreement between the City and the Consultant. If conflict occurs between contract documents and applicable laws, codes, ordinances or regulations, the most stringent or legally binding requirement shall govern and be considered a part of this contract to afford the City the maximum benefits.
- L. Negotiated Agreement: The parties acknowledge this is a negotiated agreement, that they have had this Agreement reviewed by their respective legal counsel, and that the terms and conditions of this Agreement are not to be construed against any party on the basis of such party's draftsmanship.
- M. No personal liability: No officer, agent or authorized employee of the City shall be personally responsible for any liability arising under this Agreement, whether expressed or implied, nor for any statement or representation made or in any connection with this Agreement.

IN WITNESS WHEREOF, in consideration of the terms, conditions and covenants contained, or attached and incorporated and made a part, the parties have executed this Agreement by having legally-binding representatives affix their signatures below.

**TIERRA RIGHT OF WAY SERVICES, LTD.**

**CITY OF SPOKANE**

By \_\_\_\_\_  
Signature                      Date

By \_\_\_\_\_  
Signature                      Date

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

**Attachments:** Exhibit A – Certificate Regarding Debarment  
Exhibit B – City's Request for Qualifications  
Exhibit C – Billing Rate Schedule



## EXHIBIT A

### CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

1. The undersigned (i.e., signatory for the Subrecipient / Contractor / Consultant) certifies, to the best of its knowledge and belief, that it and its principals:
  - a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency;
  - b. Have not within a three-year period preceding this contract been convicted or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state, or local) transaction or contract under a public transaction; violation of federal or state antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, tax evasion, receiving stolen property, making false claims, or obstruction of justice;
  - c. Are not presently indicted or otherwise criminally or civilly charged by a government entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and,
  - d. Have not within a three-year period preceding this contract had one or more public transactions (federal, state, or local) terminated for cause or default.
2. The undersigned agrees by signing this contract that it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction.
3. The undersigned further agrees by signing this contract that it will include the following clause, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions:

#### Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transactions

1. The lower tier contractor certified, by signing this contract that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any federal department or agency.
  2. Where the lower tier contractor is unable to certify to any of the statements in this contract, such contractor shall attach an explanation to this contract.
4. I understand that a false statement of this certification may be grounds for termination of the contract.

|                                                                      |                                     |
|----------------------------------------------------------------------|-------------------------------------|
| <hr/> Name of Subrecipient / Contractor / Consultant (Type or Print) | <hr/> Program Title (Type or Print) |
| <hr/> Name of Certifying Official (Type or Print)                    | <hr/> Signature                     |
| <hr/> Title of Certifying Official (Type or Print)                   | <hr/> Date (Type or Print)          |



## EXHIBIT B





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 11-10-20

**Type of expenditure:** Goods ☐ Services ☐

**Department:** Engineering Services

**Approving Supervisor:** Kyle Twohig

**Amount of Proposed Expenditure:** \$200,000

**Funding Source:** local and state loan/grant

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

The proposed contract with Tierra is an on-call contract to be used to pay for historic resources consulting services on public works projects (water, sewer, sidewalk, etc.). Money paid to the consultant under this contract will generally be paid by state loan or grant funds or utility funds on a project by project basis.

**What are the impacts if expenses are deferred?**

Infrastructure upgrades would have to be deferred which, when projects are state funded, would potentially mean the loss of state funds.

**What alternative resources have been considered?**

None available.

**Description of the goods or service and any additional information?**

This expenditure is for hiring a consultant to provide specialized services which the City does not have in house. The work to be provided by the consultant and the associated fee are negotiated and authorized on a project by project basis.

**Person Submitting Form/Contact:** Dan Buller dbuller@spokanecity.org

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/2/2020

**Clerk's File #**

OPR 2020-0839

**Renews #****Submitting Dept**

ENGINEERING SERVICES

**Cross Ref #****Contact Name/Phone**

DAN BULLER 625-6391

**Project #**

2021061

**Contact E-Mail**

DBULLER@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #**

MASTER

**Agenda Item Name**

0390 - BUDINGER - GEOTECHNICAL ENGINEERING ON-CALL SERVICES

**Agenda Wording**

Local Area A&E Professional Services Consultant Agreement with Budinger & Associates; (Spokane, WA) for Geotechnical Engineering Services for 2021-2022 Non-Federal Aid Project for the amount not to exceed \$400,000.00. (Various Neighborhood Councils)

**Summary (Background)**

The Agreement for Geotechnical Engineering Services is for a period of two years. An option for a one year extension will be granted at the City's discretion. Task Assignments will be prepared under this agreement and scope for individual project needs. Funding shall be from the individual project.

Lease? NO

Grant related? YES

Public Works? YES

**Fiscal Impact****Budget Account**

Expense \$ 400,000.00

# Various

Expense \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

TWOHIG, KYLE

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

publicworksaccounting@spokanecity.org

**Additional Approvals**

kgoodman@spokanecity.org

**Purchasing**

dbuller@spokanecity.org

**GRANTS &  
CONTRACT MGMT**

STOPHER, SALLY

aduffey@spokanecity.org

jfinnegan@budingerinc.com



## Briefing Paper

### PIES

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Engineering Services; Public Works                                                                                            |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | On-Call Engineering Consultants                                                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | October 26, 2020                                                                                                              |
| <b>Contact (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Dan Buller ( <a href="mailto:dbuller@spokanecity.org">dbuller@spokanecity.org</a> , 625-6391)                                 |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | Scott Simmons                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | PIES                                                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Innovative Infrastructure                                                                                                     |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Informational - background information for committee review                                                                   |
| <p><b>Background/History:</b> Engineering Services has “on-call” agreements with various consultants for specialized engineering or related services (geotech., surveying, historic resources, real estate acquisition and construction management) associated with the City’s public works projects. Those firms are selected on the basis of qualifications as required by RCW 39. These typically agreements last from 2-3 years.</p>                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <p><b>Executive Summary:</b></p> <ul style="list-style-type: none"> <li>• A request for qualifications is being advertised for the above name specialized services.</li> <li>• A review committee ranked the firms by qualifications. One or two firms will be selected for each discipline.</li> <li>• Engineering Services expects to bring five agreements to council over the next several weeks.</li> <li>• Costs incurred under the proposed contracts are paid as part of each public works project for which the consultant is used. The projects associated with these contracts are for all public works except Federal Highway Administration (FHWA) funded projects. (A similar briefing paper was submitted for the June PIES committee meeting for FHWA funded projects).</li> </ul> |                                                                                                                               |
| <p><b>Budget Impact:</b></p> <p>Approved in current year budget?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Annual/Reoccurring expenditure?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                             |                                                                                                                               |
| <p><b>Operations Impact:</b></p> <p>Consistent with current operations/policy?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Requires change in current operations/policy?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                 |                                                                                                                               |





**City of Spokane**

**CONSULTANT AGREEMENT**

**Title: 2021-2022 GEOTECHNICAL  
ENGINEERING ON-CALL SERVICES**

This Consultant Agreement is made and entered into by and between the **CITY OF SPOKANE** as ("City"), a Washington municipal corporation, and **BUDINGER & ASSOCIATES, INC.**, whose address is 1101 North Fancher Road, Spokane, Washington, 99212 as ("Consultant"), individually hereafter referenced as a "party", and together as the "parties".

*WHEREAS, the purpose of this Agreement is to provide **2021-2022 GEOTECHNICAL ENGINEERING ON-CALL SERVICES** to the City; and*

*WHEREAS, the Consultant was selected through a Request for Qualification issued by the City.*

*-- NOW, THEREFORE, in consideration of the terms, conditions, covenants and performance of the Scope of Work contained herein, the City and Consultant mutually agree as follows:*

**1. TERM OF AGREEMENT.**

The term of this Agreement begins on January 1, 2021, and ends on December 31, 2022, unless amended by written agreement or terminated earlier under the provisions. The contract may be renewed for one (1) additional one-year contract period, subject to mutual agreement.

**2. TIME OF BEGINNING AND COMPLETION.**

The Consultant shall begin the work outlined in the "Scope of Work" ("Work") on the beginning date, above. The City will acknowledge in writing when the Work is complete. Time limits established under this Agreement shall not be extended because of delays for which the Consultant is responsible, but may be extended by the City, in writing, for the City's convenience or conditions beyond the Consultant's control.

**3. SCOPE OF WORK.**

The General Scope of Work for this Agreement is described in the City's Request for Qualification which is attached as Exhibit B and made a part of this Agreement. In the event of a conflict or discrepancy in the contract documents, the City Agreement controls.



The Work is subject to City review and approval. The Consultant shall confer with the City periodically, and prepare and present information and materials (e.g. detailed outline of completed Work) requested by the City to determine the adequacy of the Work or Consultant's progress.

#### **4. COMPENSATION.**

Total compensation for Consultant's services under this On-Call Agreement shall not exceed **FOUR HUNDRED THOUSAND AND NO/100 DOLLARS (\$400,000.00)**, excluding tax, if applicable, unless modified by a written amendment to this Agreement. This is the maximum amount to be paid under this Agreement for the work described in Section 3 above, and shall not be exceeded without the prior written authorization of the City in the form of an executed amendment to this Agreement.

#### **5. PAYMENT.**

The Company shall submit its applications for payment to City of Spokane, Engineering Services Department, 808 West Spokane Falls Blvd., Spokane, WA 99201. **Payment will be made via direct deposit/ACH** within thirty (30) days after receipt of the Company's application except as provided by state law. If the City objects to all or any portion of the invoice, it shall notify the Company and pay that portion of the invoice not in dispute. In that event, the parties shall immediately make every effort to settle the disputed amount.

#### **6. REIMBURSABLES**

The reimbursables under this Agreement are to be included, and considered part of the maximum amount not to exceed (above), and require the Consultant's submittal of appropriate documentation and actual itemized receipts, the following limitations apply.

- A. City will reimburse the Consultant at actual cost for expenditures that are pre-approved by the City in writing and are necessary and directly applicable to the work required by this Contract provided that similar direct project costs related to the contracts of other clients are consistently accounted for in a like manner. Such direct project costs may not be charged as part of overhead expenses or include a markup. Other direct charges may include, but are not limited to the following types of items: travel, printing, cell phone, supplies, materials, computer charges, and fees of subconsultants.
- B. The billing for third party direct expenses specifically identifiable with this project shall be an itemized listing of the charges supported by copies of the original bills, invoices, expense accounts, subconsultant paid invoices, and other supporting documents used by the Consultant to generate invoice(s) to the City. The original supporting documents shall be available to the City for inspection upon request. All charges must be necessary for the services provided under this Contract.
- C. The City will reimburse the actual cost for travel expenses incurred as evidenced by copies of receipts (excluding meals) supporting such travel expenses, and in accordance with the City of Spokane Travel Policy, details of which can be provided upon request.
- D. **Airfare:** Airfare will be reimbursed at the actual cost of the airline ticket. The City will reimburse for Economy or Coach Fare only. Receipts detailing each airfare are required.
- E. **Meals:** Meals will be reimbursed at the Federal Per Diem daily meal rate for the city in which the work is performed. *Receipts are not required as documentation.* The invoice shall state "the meals are being billed at the Federal Per Diem daily meal rate", and shall detail how many of each meal is being billed (e.g. the number of breakfasts, lunches, and dinners). The City will not reimburse for alcohol at any time.



- F. **Lodging:** Lodging will be reimbursed at actual cost incurred up to a maximum of the published General Services Administration (GSA) Index for the city in which the work is performed (*the current maximum allowed reimbursement amount can be provided upon request*). Receipts detailing each day / night lodging are required. The City will not reimburse for ancillary expenses charged to the room (e.g. movies, laundry, mini bar, refreshment center, fitness center, sundry items, etc.)
- G. **Vehicle mileage:** Vehicle mileage will be reimbursed at the Federal Internal Revenue Service Standard Business Mileage Rate in affect at the time the mileage expense is incurred. Please note: payment for mileage for long distances traveled will not be more than an equivalent trip round-trip airfare of a common carrier for a coach or economy class ticket.
- H. **Rental Car:** Rental car expenses will be reimbursed at the actual cost of the rental. Rental car receipts are required for all rental car expenses. The City will reimburse for a standard car of a mid-size class or less. The City will not reimburse for ancillary expenses charged to the car rental (e.g. GPS unit).
- I. **Miscellaneous Travel** (e.g. parking, rental car gas, taxi, shuttle, toll fees, ferry fees, etc.): Miscellaneous travel expenses will be reimbursed at the actual cost incurred. Receipts are required for each expense of \$10.00 or more.
- J. **Miscellaneous other business expenses** (e.g. printing, photo development, binding): Other miscellaneous business expenses will be reimbursed at the actual cost incurred and may not include a mark up. Receipts are required for all miscellaneous expenses that are billed.

**Subconsultant:** Subconsultant expenses will be reimbursed at the actual cost incurred and a four percent (4%) markup. Copies of all Subconsultant invoices that are rebilled to the City are required.

## **7. TAXES, FEES AND LICENSES.**

- A. Consultant shall pay and maintain in current status, all necessary licenses, fees, assessments, permit charges, etc. necessary to conduct the work included under this Agreement. It is the Consultant's sole responsibility to monitor and determine changes or the enactment of any subsequent requirements for said fees, assessments, or changes and to immediately comply.
- B. Where required by state statute, ordinance or regulation, Consultant shall pay and maintain in current status all taxes necessary for performance. Consultant shall not charge the City for federal excise taxes. The City will furnish Consultant an exemption certificate where appropriate.
- C. The Director of Finance and Administrative Services may withhold payment pending satisfactory resolution of unpaid taxes and fees due the City.
- D. The cost of any permits, licenses, fees, etc. arising as a result of the projects included in this Agreement shall be included in the project budgets.

## **8. CITY OF SPOKANE BUSINESS LICENSE.**

Section 8.01.070 of the Spokane Municipal Code states that no person may engage in business with the City without first having obtained a valid annual business registration. The Consultant shall be responsible for contacting the State of Washington Business License Services at <http://bls.dor.wa.gov> or 1-800-451-7985 to obtain a business registration. If the Contractor does not believe it is required to obtain a business registration, it may contact the City's Taxes and Licenses Division at (509) 625-6070 to request an exemption status determination.



## **9. SOCIAL EQUITY REQUIREMENTS.**

No individual shall be excluded from participation in, denied the benefit of, subjected to discrimination under, or denied employment in the administration of or in connection with this Agreement because of age, sex, race, color, religion, creed, marital status, familial status, sexual orientation including gender expression or gender identity, national origin, honorably discharged veteran or military status, the presence of any sensory, mental or physical disability, or use of a service animal by a person with disabilities. Consultant agrees to comply with, and to require that all subcontractors comply with, Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act, as applicable to the Consultant. Consultant shall seek inclusion of woman and minority business for subcontracting. A woman or minority business is one that self-identifies to be at least 51% owned by a woman and/or minority. Such firms do not have to be certified by the State of Washington.

## **10. INDEMNIFICATION.**

The Consultant shall defend, indemnify, and hold the City and its officers and employees harmless from all claims, demands, or suits at law or equity asserted by third parties for bodily injury (including death) and/or property damage which arise from the Consultant's negligence or willful misconduct under this Agreement, including attorneys' fees and litigation costs; provided that nothing herein shall require a Consultant to indemnify the City against and hold harmless the City from claims, demands or suits based solely upon the negligence of the City, its agents, officers, and employees. If a claim or suit is caused by or results from the concurrent negligence of the Consultant's agents or employees and the City, its agents, officers and employees, this indemnity provision shall be valid and enforceable to the extent of the negligence of the Consultant, its agents or employees. The Consultant specifically assumes liability and agrees to defend, indemnify, and hold the City harmless for actions brought by the Consultant's own employees against the City and, solely for the purpose of this indemnification and defense, the Consultant specifically waives any immunity under the Washington State industrial insurance law, or Title 51 RCW. The Consultant recognizes that this waiver was specifically entered into pursuant to the provisions of RCW 4.24.115 and was the subject of mutual negotiation. The indemnity and agreement to defend and hold the City harmless provided for in this section shall survive any termination or expiration of this agreement.

## **11. INSURANCE.**

During the period of the Agreement, the Consultant shall maintain in force at its own expense, each insurance noted below with companies or through sources approved by the State Insurance Commissioner pursuant to RCW Title 48;

A. Worker's Compensation Insurance in compliance with RCW 51.12.020, which requires subject employers to provide workers' compensation coverage for all their subject workers and Employer's Liability Insurance in the amount of \$1,000,000;

B. General Liability Insurance on an occurrence basis, with a combined single limit of not less than \$1,000,000 each occurrence for bodily injury and property damage. It shall include contractual liability coverage for the indemnity provided under this agreement. It shall provide that the City, its officers and employees are additional insureds but only with respect to the Consultant's services to be provided under this Agreement; and



C. Automobile Liability Insurance with a combined single limit, or the equivalent of not less than \$1,000,000 each accident for bodily injury and property damage, including coverage for owned, hired and non-owned vehicles.

D. Professional Liability Insurance with a combined single limit of not less than \$1,000,000 each claim, incident or occurrence. This is to cover damages caused by the error, omission, or negligent acts related to the professional services to be provided under this Agreement. The coverage must remain in effect for at least two (2) years after the Agreement is completed.

There shall be no cancellation, material change, reduction of limits or intent not to renew the insurance coverage(s) without forty-five (45) days written notice from the Consultant or its insurer(s) to the City. As evidence of the insurance coverage(s) required by this Agreement, the Consultant shall furnish acceptable Certificates Of Insurance (COI) to the City at the time it returns this signed Agreement. The certificate shall specify the City of Spokane as "Additional Insured" specifically for Consultant's services under this Agreement, as well as all of the parties who are additional insureds, and include applicable policy endorsements, the forty-five (45) day cancellation clause, and the deduction or retention level. The Consultant shall be financially responsible for all pertinent deductibles, self-insured retentions, and/or self-insurance.

## **12. DEBARMENT AND SUSPENSION.**

The Contractor has provided its certification that it is in compliance with and shall not contract with individuals or organizations which are debarred, suspended, or otherwise excluded from or ineligible from participation in Federal Assistance Programs under Executive Order 12549 and "Debarment and Suspension", codified at 29 CFR part 98.

## **13. AUDIT.**

Upon request, the Consultant shall permit the City and any other governmental agency ("Agency") involved in the funding of the Work to inspect and audit all pertinent books and records. This includes work of the Consultant, any subconsultant, or any other person or entity that performed connected or related Work. Such books and records shall be made available upon reasonable notice of a request by the City, including up to three (3) years after final payment or release of withheld amounts. Such inspection and audit shall occur in Spokane County, Washington, or other reasonable locations mutually agreed to by the parties. The Consultant shall permit the City to copy such books and records at its own expense. The Consultant shall ensure that inspection, audit and copying rights of the City is a condition of any subcontract, agreement or other arrangement under which any other persons or entity may perform Work under this Agreement.

## **14. INDEPENDENT CONSULTANT.**

A. The Consultant is an independent Consultant. This Agreement does not intend the Consultant to act as a City employee. The City has neither direct nor immediate control over the Consultant nor the right to control the manner or means by which the Consultant works. Neither the Consultant nor any Consultant employee shall be an employee of the City. This Agreement prohibits the Consultant to act as an agent or legal representative of the City. The Consultant is not granted express or implied rights or authority to assume or create any obligation or responsibility for or in the name of the City, or to bind the City. The City is not liable for or obligated to pay sick leave, vacation pay, or any other benefit of employment, nor to pay social security or other tax that may arise from employment. The Consultant shall



pay all income and other taxes as due. The Consultant may perform work for other parties; the City is not the exclusive user of the services that the Consultant provides.

- B. If the City needs the Consultant to Work on City premises and/or with City equipment, the City may provide the necessary premises and equipment. Such premises and equipment are exclusively for the Work and not to be used for any other purpose.
- C. If the Consultant works on the City premises using City equipment, the Consultant remains an independent Consultant and not a City employee. The Consultant will notify the City Project Manager if s/he or any other Workers are within ninety (90) days of a consecutive 36-month placement on City property. If the City determines using City premises or equipment is unnecessary to complete the Work, the Consultant will be required to work from its own office space or in the field. The City may negotiate a reduction in Consultant fees or charge a rental fee based on the actual costs to the City, for City premises or equipment.

#### **15. KEY PERSONS.**

The Consultant shall not transfer or reassign any individual designated in this Agreement as essential to the Work, nor shall those key persons, or employees of Consultant identified as to be involved in the Project Work be replaced, removed or withdrawn from the Work without the express written consent of the City, which shall not be unreasonably withheld. If any such individual leaves the Consultant's employment, the Consultant shall present to the City one or more individuals with greater or equal qualifications as a replacement, subject to the City's approval, which shall not be unreasonably withheld. The City's approval does not release the Consultant from its obligations under this Agreement.

#### **16. ASSIGNMENT AND SUBCONTRACTING.**

The Consultant shall not assign or subcontract its obligations under this Agreement without the City's written consent, which may be granted or withheld in the City's sole discretion. Any subcontract made by the Consultant shall incorporate by reference this Agreement, except as otherwise provided. The Consultant shall require that all subconsultants comply with the obligations and requirements of the subcontract. The City's consent to any assignment or subcontract does not release the consultant from liability or any obligation within this Agreement, whether before or after City consent, assignment or subcontract.

#### **17. CITY ETHICS CODE.**

- A. Consultant shall promptly notify the City in writing of any person expected to be a Consultant Worker (including any Consultant employee, subconsultant, principal, or owner) and was a former City officer or employee within the past twelve (12) months.
- B. Consultant shall ensure compliance with the City Ethics Code by any Consultant Worker when the Work or matter related to the Work is performed by a Consultant Worker who has been a City officer or employee within the past two (2) years.
- C. Consultant shall not directly or indirectly offer anything of value (such as retainers, loans, entertainment, favors, gifts, tickets, trips, favors, bonuses, donations, special discounts, work or meals) to any City employee, volunteer or official that is intended, or may appear to a reasonable person to be intended, to obtain or give special consideration to the Consultant. Promotional items worth less than \$25 may be distributed by the Consultant to a City employee if the Consultant uses the items as routine and standard promotional materials. Any violation of this provision may cause termination of this Agreement. Nothing in this Agreement prohibits donations to campaigns for election to City office, so long as the



donation is disclosed as required by the election campaign disclosure laws of the City and of the State.

#### **18. NO CONFLICT OF INTEREST.**

Consultant confirms that the Consultant or workers have no business interest or a close family relationship with any City officer or employee who was or will be involved in the consultant selection, negotiation, drafting, signing, administration or evaluation of the Consultant's work. As used in this Section, the term Consultant includes any worker of the Consultant who was, is, or will be, involved in negotiation, drafting, signing, administration or performance of the Agreement. The term "close family relationship" refers to: spouse or domestic partner, any dependent parent, parent-in-law, child, son-in-law, daughter-in-law; or any parent, parent in-law, sibling, uncle, aunt, cousin, niece or nephew residing in the household of a City officer or employee described above.

#### **19. ERRORS AND OMISSIONS, CORRECTIONS.**

Consultant is responsible for professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on the behalf of the Consultant under this Agreement in the delivery of a final work product. The standard of care applicable to Consultant's services will be the degree of skill and diligence normally employed by professional engineers or Consultants performing the same or similar services at the time said services are performed. The Final Work Product is defined as a stamped, signed work product. Consultant, without additional compensation, shall correct or revise errors or mistakes in designs, drawings, specifications, and/or other consultant services immediately upon notification by the City. The obligation provided for in this Section regarding acts or omissions resulting from this Agreement survives Agreement termination or expiration.

#### **20. INTELLECTUAL PROPERTY RIGHTS.**

- A. Copyrights. The Consultant shall retain the copyright (including the right of reuse) to all materials and documents prepared by the Consultant for the Work, whether or not the Work is completed. The Consultant grants to the City a non-exclusive, irrevocable, unlimited, royalty-free license to use copy and distribute every document and all the materials prepared by the Consultant for the City under this Agreement. If requested by the City, a copy of all drawings, prints, plans, field notes, reports, documents, files, input materials, output materials, the media upon which they are located (including cards, tapes, discs, and other storage facilities), software program or packages (including source code or codes, object codes, upgrades, revisions, modifications, and any related materials) and/or any other related documents or materials developed solely for and paid for by the City to perform the Work, shall be promptly delivered to the City.
- B. Patents: The Consultant assigns to the City all rights in any invention, improvement, or discovery, with all related information, including but not limited to designs, specifications, data, patent rights and findings developed with the performance of the Agreement or any subcontract. Notwithstanding the above, the Consultant does not convey to the City, nor does the City obtain, any right to any document or material utilized by the Consultant created or produced separate from the Agreement or was pre-existing material (not already owned by the City), provided that the Consultant has identified in writing such material as pre-existing prior to commencement of the Work. If pre-existing materials are incorporated in the work, the Consultant grants the City an irrevocable, non-exclusive right and/or license to use, execute, reproduce, display and transfer the pre-existing material, but only as an inseparable part of the work.



- C. The City may make and retain copies of such documents for its information and reference with their use on the project. The Consultant does not represent or warrant that such documents are suitable for reuse by the City or others, on extensions of the project or on any other project, and the City releases the Consultant from liability for any unauthorized reuse of such documents.

## **21. CONFIDENTIALITY.**

Under Washington State Law RCW Chapter 42.56) all materials received or created by the City of Spokane are **public records** which are subject to review and copying pursuant to a public records request. These records include but are not limited to bid or proposal submittals, agreement documents, contract work product, and other bid material. Some records or portions of records may be legally exempt from disclosure and can be redacted or withheld. RCW Ch. 42.56 describes those exemptions. Consultant must familiarize themselves with state law and the City of Spokane's process for managing records.

The City will endeavor to redact anything that clearly should be redacted under the law. For example, the City will generally redact Social Security Numbers, tax records, and financial account numbers before records are made available to a requestor. Consultant may identify any materials Consultant believes to be not subject to release under the Public Records Act. City will not be bound by Consultant's determination of whether any particular record or records are legally exempt from release under the Public Records Act.

If the City receives a public records request for records involving Consultant or Consultant's work product, City will release the records unless City determines that there are obvious exemptions or redactions (which City will make prior to release of the records). If City determines that there are exemptions that can be asserted only by Consultant, City will endeavor to notify Consultant and Consultant will be given ten days to obtain a Court order preventing the City from releasing the requested records. **If no Court order is procured by Consultant, the City will release the requested records.**

## **22. DISPUTES.**

Any dispute or misunderstanding that may arise under this Agreement, concerning the Consultant's performance, shall first be through negotiations, if possible, between the Consultant's Project Manager and the City's Project Manager. It shall be referred to the Director and the Consultant's senior executive(s). If such officials do not agree upon a decision within a reasonable period of time, either party may decline or discontinue such discussions and may then pursue the legal means to resolve such disputes, including but not limited to mediation, arbitration and/or alternative dispute resolution processes. Nothing in this dispute process shall mitigate the rights of the City to terminate the Agreement. Notwithstanding all of the above, if the City believes in good faith that some portion of the Work has not been completed satisfactorily, the City may require the Consultant to correct such work prior to the City payment. The City will provide to the Consultant an explanation of the concern and the remedy that the City expects. The City may withhold from any payment otherwise due, an amount that the City in good faith finds to be under dispute, or if the Consultant provides no sufficient remedy, the City may retain the amount equal to the cost to the City for otherwise correcting or remedying the work not properly completed. Waiver of any of these rights is not deemed a future waiver of any such right or remedy available at law, contract or equity.



### **23. TERMINATION.**

- A. For Cause: The City or Consultant may terminate the Agreement if the other party is in material breach of this Agreement, and such breach has not been corrected to the other party's reasonable satisfaction in a timely manner. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- B. For Reasons Beyond Control of Parties: Either party may terminate this Agreement without recourse by the other where performance is rendered impossible or impracticable for reasons beyond such party's reasonable control, such as, but not limited to, an act of nature, war or warlike operation, civil commotion, riot, labor dispute including strike, walkout or lockout, except labor disputes involving the Consultant's own employees, sabotage, or superior governmental regulation or control. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- C. For Convenience: Either party may terminate this Agreement without cause, upon thirty (30) days written notice to the other party.
- D. Actions upon Termination: if termination occurs not the fault of the Consultant, the Consultant shall be paid for the services properly performed prior to the actual termination date, with any reimbursable expenses then due, but such compensation shall not exceed the maximum compensation to be paid under the Agreement. The Consultant agrees this payment shall fully and adequately compensate the Consultant and all subconsultants for all profits, costs, expenses, losses, liabilities, damages, taxes and charges of any kind (whether foreseen or unforeseen) attributable to the termination of this Agreement.
- E. Upon termination, the Consultant shall provide the City with the most current design documents, contract documents, writings and other products the Consultant has produced to termination, along with copies of all project-related correspondence and similar items. The City shall have the same rights to use these materials as if termination had not occurred; provided however, that the City shall indemnify and hold the Consultant harmless from any claims, losses, or damages to the extent caused by modifications made by the City to the Consultant's work product.

### **24. EXPANSION FOR NEW WORK.**

This Agreement scope may be expanded for new work. Any expansion for New Work (work not specified within the original Scope of Work Section of this Agreement, and/or not specified in the original RFP as intended work for the Agreement) must comply with all the following limitations and requirements: (a) the New Work is not reasonable to solicit separately; (b) the New Work is for reasonable purpose; (c) the New Work was not reasonably known either the City or Consultant at time of contract or else was mentioned as a possibility in the solicitation (such as future phases of work, or a change in law); (d) the New Work is not significant enough to be reasonably regarded as an independent body of work; (e) the New Work would not have attracted a different field of competition; and (f) the change does not vary the essential identified or main purposes of the Agreement. The City may make exceptions for immaterial changes, emergency or sole source conditions, or other situations required in City opinion. Certain changes are not New Work subject to these limitations, such as additional phases of Work anticipated at the time of solicitation, time extensions, Work Orders issued on an On-Call contract, and similar. New Work must be mutually agreed and issued by the City through written Addenda. New Work performed before an authorizing Amendment may not be eligible for payment.



## **25. MISCELLANEOUS PROVISIONS.**

- A. Amendments: No modification of this Agreement shall be effective unless in writing and signed by an authorized representative of each of the parties hereto.
- B. Binding Agreement: This Agreement shall not be binding until signed by both parties. The provisions, covenants and conditions in this Agreement shall bind the parties, their legal heirs, representatives, successors and assigns.
- C. Americans with Disabilities Act (ADA): Specific attention by the designer is required in association with the Americans with Disabilities Act (ADA) 42 U.S.C. 12101-12213 and 47 U.S.C. 225 and 611, its requirements, regulations, standards and guidelines, which were updated in 2010 and are effective and mandatory for all State and local government facilities and places of public accommodation for construction projects including alteration of existing facilities, as of March 15, 2012. The City advises that the requirements for accessibility under the ADA, may contain provisions that differ substantively from accessibility provisions in applicable State and City codes, and if the provisions of the ADA impose a greater or equal protection for the rights of individuals with disabilities or individuals associated with them than the adopted local codes, the ADA prevail unless approval for an exception is obtained by a formal documented process. Where local codes provide exceptions from accessibility requirements that differ from the ADA Standards; such exceptions may not be permitted for publicly owned facilities subject to Title II requirements unless the same exception exists in the Title II regulations. It is the responsibility of the designer to determine the code provisions.
- D. The Consultant, at no expense to the City, shall comply with all laws of the United States and Washington, the Charter and ordinances of the City of Spokane; and rules, regulations, orders and directives of their administrative agencies and officers. Without limiting the generality of this paragraph, the Consultant shall comply with the requirements of this Section.
- E. This Agreement shall be construed and interpreted under the laws of Washington. The venue of any action brought shall be in the Superior Court of Spokane County.
- F. Remedies Cumulative: Rights under this Agreement are cumulative and nonexclusive of any other remedy of law or in equity.
- G. Captions: The titles of sections or subsections are for convenience only and do not define or limit the contents.
- H. Severability: If any term or provision is determined by a court of competent jurisdiction to be invalid or unenforceable, the remainder of this Agreement shall not be affected, and each term and provision shall be valid and enforceable to the fullest extent permitted by law.
- I. Waiver: No covenant, term or condition or the breach shall be deemed waived, except by written consent of the party against whom the waiver is claimed, and any waiver of the breach of any covenant, term or condition shall not be deemed a waiver of any preceding or succeeding breach of the same or any other covenant, term of condition. Neither the acceptance by the City of any performance by the Consultant after the time the same shall have become due nor payment to the Consultant for any portion of the Work shall constitute a waiver by the City of the breach or default of any covenant, term or condition unless otherwise expressly agreed to by the City in writing.
- J. Additional Provisions: This Agreement may be modified by additional terms and conditions ("Special Conditions") which shall be attached to this Agreement as an Exhibit. The parties agree that the Special Conditions shall supplement the terms and conditions of the Agreement, and in the event of ambiguity or conflict with the terms and conditions of the Agreement, these Special Conditions shall govern.



- K. Entire Agreement: This document along with any exhibits and all attachments, and subsequently issued addenda, comprises the entire agreement between the City and the Consultant. If conflict occurs between contract documents and applicable laws, codes, ordinances or regulations, the most stringent or legally binding requirement shall govern and be considered a part of this contract to afford the City the maximum benefits.
- L. Negotiated Agreement: The parties acknowledge this is a negotiated agreement, that they have had this Agreement reviewed by their respective legal counsel, and that the terms and conditions of this Agreement are not to be construed against any party on the basis of such party's draftsmanship.
- M. No personal liability: No officer, agent or authorized employee of the City shall be personally responsible for any liability arising under this Agreement, whether expressed or implied, nor for any statement or representation made or in any connection with this Agreement.

IN WITNESS WHEREOF, in consideration of the terms, conditions and covenants contained, or attached and incorporated and made a part, the parties have executed this Agreement by having legally-binding representatives affix their signatures below.

**BUDINGER & ASSOCIATES, INC.**

**CITY OF SPOKANE**

By \_\_\_\_\_  
Signature                      Date

By \_\_\_\_\_  
Signature                      Date

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

**Attachments:** Exhibit A – Certificate Regarding Debarment  
Exhibit B – City's Request for Qualifications

20-194



## EXHIBIT A

### CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

1. The undersigned (i.e., signatory for the Subrecipient / Contractor / Consultant) certifies, to the best of its knowledge and belief, that it and its principals:
  - a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency;
  - b. Have not within a three-year period preceding this contract been convicted or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state, or local) transaction or contract under a public transaction; violation of federal or state antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, tax evasion, receiving stolen property, making false claims, or obstruction of justice;
  - c. Are not presently indicted or otherwise criminally or civilly charged by a government entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and,
  - d. Have not within a three-year period preceding this contract had one or more public transactions (federal, state, or local) terminated for cause or default.
2. The undersigned agrees by signing this contract that it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction.
3. The undersigned further agrees by signing this contract that it will include the following clause, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions:

#### Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transactions

1. The lower tier contractor certified, by signing this contract that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any federal department or agency.
  2. Where the lower tier contractor is unable to certify to any of the statements in this contract, such contractor shall attach an explanation to this contract.
4. I understand that a false statement of this certification may be grounds for termination of the contract.

|                                                                      |                                     |
|----------------------------------------------------------------------|-------------------------------------|
| <hr/> Name of Subrecipient / Contractor / Consultant (Type or Print) | <hr/> Program Title (Type or Print) |
| <hr/> Name of Certifying Official (Type or Print)                    | <hr/> Signature                     |
| <hr/> Title of Certifying Official (Type or Print)                   | <hr/> Date (Type or Print)          |



## **EXHIBIT B**





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 11-10-20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** Engineering Services

**Approving Supervisor:** Kyle Twohig

**Amount of Proposed Expenditure:** \$400,000

**Funding Source:** local and state loan/grant

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

The proposed contract with Budinger is an on-call contract to be used to pay for geotechnical engineering services on public works projects (water, sewer, sidewalk, etc.). Money paid to the consultant under this contract will generally be paid by state loan or grant funds or utility funds on a project by project basis.

**What are the impacts if expenses are deferred?**

Infrastructure upgrades would have to be deferred which, when projects are state funded, would potentially mean the loss of state funds.

**What alternative resources have been considered?**

None available.

**Description of the goods or service and any additional information?**

This expenditure is for hiring a consultant to provide specialized services which the City does not have in house. The work to be provided by the consultant and the associated fee are negotiated and authorized on a project by project basis.

**Person Submitting Form/Contact:** Dan Buller dbuller@spokanecity.org

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/2/2020

**Clerk's File #**

OPR 2020-0840

**Renews #****Submitting Dept**

ENGINEERING SERVICES

**Cross Ref #****Contact Name/Phone**

DAN BULLER 625-6391

**Project #**

2021062

**Contact E-Mail**

DBULLER@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #**

MASTER

**Agenda Item Name**

0390 - COMMONSTREET - REAL ESTATE ON-CALL CONSULTING SERVICES

**Agenda Wording**

Local Area A&E Professional Services Consultant Agreement with CommonStreet Consulting LLC;(Spokane,WA) for Real Estate Consultant Services for 2021-2022 Non-Federal Aid Project for the amount not to exceed \$200,000.00. (Various Neighborhoods)

**Summary (Background)**

The Agreement for Real Estate Consultant Services is for a period of two years. An option for a one year extension will be granted at the City's discretion. Task Assignment will be prepared under this agreement and scope for individual project needs. Funding shall be from the individual project.

Lease? NO

Grant related? YES

Public Works? YES

**Fiscal Impact****Budget Account**

Expense \$ 200,000.00

# Various

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

TWOHIG, KYLE

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

publicworksaccounting@spokanecity.org

**Additional Approvals**

kgoodman@spokanecity.org

**Purchasing**

dbuller@spokanecity.org

**GRANTS &  
CONTRACT MGMT**

STOPHER, SALLY

aduffey@spokanecity.org

hutch@csrow.com

dsteele@spokanecity.org



## Briefing Paper

### PIES

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Engineering Services; Public Works                                                                                            |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | On-Call Engineering Consultants                                                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | October 26, 2020                                                                                                              |
| <b>Contact (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Dan Buller ( <a href="mailto:dbuller@spokanecity.org">dbuller@spokanecity.org</a> , 625-6391)                                 |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | Scott Simmons                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | PIES                                                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Innovative Infrastructure                                                                                                     |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Informational - background information for committee review                                                                   |
| <p><b>Background/History:</b> Engineering Services has “on-call” agreements with various consultants for specialized engineering or related services (geotech., surveying, historic resources, real estate acquisition and construction management) associated with the City’s public works projects. Those firms are selected on the basis of qualifications as required by RCW 39. These typically agreements last from 2-3 years.</p>                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <p><b>Executive Summary:</b></p> <ul style="list-style-type: none"> <li>• A request for qualifications is being advertised for the above name specialized services.</li> <li>• A review committee ranked the firms by qualifications. One or two firms will be selected for each discipline.</li> <li>• Engineering Services expects to bring five agreements to council over the next several weeks.</li> <li>• Costs incurred under the proposed contracts are paid as part of each public works project for which the consultant is used. The projects associated with these contracts are for all public works except Federal Highway Administration (FHWA) funded projects. (A similar briefing paper was submitted for the June PIES committee meeting for FHWA funded projects).</li> </ul> |                                                                                                                               |
| <p><b>Budget Impact:</b></p> <p>Approved in current year budget?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Annual/Reoccurring expenditure?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                             |                                                                                                                               |
| <p><b>Operations Impact:</b></p> <p>Consistent with current operations/policy?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Requires change in current operations/policy?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                 |                                                                                                                               |





**City of Spokane**

**CONSULTANT AGREEMENT**

**Title: 2021-2022 REAL ESTATE  
ON-CALL CONSULTING SERVICES**

This Consultant Agreement is made and entered into by and between the **CITY OF SPOKANE** as ("City"), a Washington municipal corporation, and **COMMONSTREET CONSULTING, LLC**, whose address is 100 South King Street, Seattle Washington 98104 as ("Consultant"), individually hereafter referenced as a "party", and together as the "parties".

*WHEREAS, the purpose of this Agreement is to provide **2021-2022 REAL ESTATE ON-CALL CONSULTING SERVICES** to the City; and*

*WHEREAS, the Consultant was selected through a Request for Qualification issued by the City.*

*-- NOW, THEREFORE, in consideration of the terms, conditions, covenants and performance of the Scope of Work contained herein, the City and Consultant mutually agree as follows:*

**1. TERM OF AGREEMENT.**

The term of this Agreement begins on January 1, 2021, and ends on December 31, 2022, unless amended by written agreement or terminated earlier under the provisions. The contract may be renewed for one (1) additional one-year contract period, subject to mutual agreement.

**2. TIME OF BEGINNING AND COMPLETION.**

The Consultant shall begin the work outlined in the "Scope of Work" ("Work") on the beginning date, above. The City will acknowledge in writing when the Work is complete. Time limits established under this Agreement shall not be extended because of delays for which the Consultant is responsible, but may be extended by the City, in writing, for the City's convenience or conditions beyond the Consultant's control.

**3. SCOPE OF WORK.**

The General Scope of Work for this Agreement is described in the City's Request for Qualification which is attached as Exhibit B and made a part of this Agreement. In the event of a conflict or discrepancy in the contract documents, the City Agreement controls.



The Work is subject to City review and approval. The Consultant shall confer with the City periodically, and prepare and present information and materials (e.g. detailed outline of completed Work) requested by the City to determine the adequacy of the Work or Consultant's progress.

#### **4. COMPENSATION.**

Total compensation for Consultant's services under this On-Call Agreement shall not exceed **TWO HUNDRED THOUSAND AND NO/100 DOLLARS (\$200,000.00)**, excluding tax, if applicable, unless modified by a written amendment to this Agreement. This is the maximum amount to be paid under this Agreement for the work described in Section 3 above, and shall not be exceeded without the prior written authorization of the City in the form of an executed amendment to this Agreement.

#### **5. PAYMENT.**

The Company shall submit its applications for payment to City of Spokane, Engineering Services Department, 808 West Spokane Falls Blvd., Spokane, WA 99201. **Payment will be made via direct deposit/ACH** within thirty (30) days after receipt of the Company's application except as provided by state law. If the City objects to all or any portion of the invoice, it shall notify the Company and pay that portion of the invoice not in dispute. In that event, the parties shall immediately make every effort to settle the disputed amount.

#### **6. REIMBURSABLES**

The reimbursables under this Agreement are to be included, and considered part of the maximum amount not to exceed (above), and require the Consultant's submittal of appropriate documentation and actual itemized receipts, the following limitations apply.

- A. City will reimburse the Consultant at actual cost for expenditures that are pre-approved by the City in writing and are necessary and directly applicable to the work required by this Contract provided that similar direct project costs related to the contracts of other clients are consistently accounted for in a like manner. Such direct project costs may not be charged as part of overhead expenses or include a markup. Other direct charges may include, but are not limited to the following types of items: travel, printing, cell phone, supplies, materials, computer charges, and fees of subconsultants.
- B. The billing for third party direct expenses specifically identifiable with this project shall be an itemized listing of the charges supported by copies of the original bills, invoices, expense accounts, subconsultant paid invoices, and other supporting documents used by the Consultant to generate invoice(s) to the City. The original supporting documents shall be available to the City for inspection upon request. All charges must be necessary for the services provided under this Contract.
- C. The City will reimburse the actual cost for travel expenses incurred as evidenced by copies of receipts (excluding meals) supporting such travel expenses, and in accordance with the City of Spokane Travel Policy, details of which can be provided upon request.
- D. **Airfare:** Airfare will be reimbursed at the actual cost of the airline ticket. The City will reimburse for Economy or Coach Fare only. Receipts detailing each airfare are required.
- E. **Meals:** Meals will be reimbursed at the Federal Per Diem daily meal rate for the city in which the work is performed. *Receipts are not required as documentation.* The invoice shall state "the meals are being billed at the Federal Per Diem daily meal rate", and shall detail how many of each meal is being billed (e.g. the number of breakfasts, lunches, and dinners). The City will not reimburse for alcohol at any time.



- F. **Lodging:** Lodging will be reimbursed at actual cost incurred up to a maximum of the published General Services Administration (GSA) Index for the city in which the work is performed (*the current maximum allowed reimbursement amount can be provided upon request*). Receipts detailing each day / night lodging are required. The City will not reimburse for ancillary expenses charged to the room (e.g. movies, laundry, mini bar, refreshment center, fitness center, sundry items, etc.)
- G. **Vehicle mileage:** Vehicle mileage will be reimbursed at the Federal Internal Revenue Service Standard Business Mileage Rate in affect at the time the mileage expense is incurred. Please note: payment for mileage for long distances traveled will not be more than an equivalent trip round-trip airfare of a common carrier for a coach or economy class ticket.
- H. **Rental Car:** Rental car expenses will be reimbursed at the actual cost of the rental. Rental car receipts are required for all rental car expenses. The City will reimburse for a standard car of a mid-size class or less. The City will not reimburse for ancillary expenses charged to the car rental (e.g. GPS unit).
- I. **Miscellaneous Travel** (e.g. parking, rental car gas, taxi, shuttle, toll fees, ferry fees, etc.): Miscellaneous travel expenses will be reimbursed at the actual cost incurred. Receipts are required for each expense of \$10.00 or more.
- J. **Miscellaneous other business expenses** (e.g. printing, photo development, binding): Other miscellaneous business expenses will be reimbursed at the actual cost incurred and may not include a mark up. Receipts are required for all miscellaneous expenses that are billed.

**Subconsultant:** Subconsultant expenses will be reimbursed at the actual cost incurred and a four percent (4%) markup. Copies of all Subconsultant invoices that are rebilled to the City are required.

## **7. TAXES, FEES AND LICENSES.**

- A. Consultant shall pay and maintain in current status, all necessary licenses, fees, assessments, permit charges, etc. necessary to conduct the work included under this Agreement. It is the Consultant's sole responsibility to monitor and determine changes or the enactment of any subsequent requirements for said fees, assessments, or changes and to immediately comply.
- B. Where required by state statute, ordinance or regulation, Consultant shall pay and maintain in current status all taxes necessary for performance. Consultant shall not charge the City for federal excise taxes. The City will furnish Consultant an exemption certificate where appropriate.
- C. The Director of Finance and Administrative Services may withhold payment pending satisfactory resolution of unpaid taxes and fees due the City.
- D. The cost of any permits, licenses, fees, etc. arising as a result of the projects included in this Agreement shall be included in the project budgets.

## **8. CITY OF SPOKANE BUSINESS LICENSE.**

Section 8.01.070 of the Spokane Municipal Code states that no person may engage in business with the City without first having obtained a valid annual business registration. The Consultant shall be responsible for contacting the State of Washington Business License Services at <http://bls.dor.wa.gov> or 1-800-451-7985 to obtain a business registration. If the Contractor does not believe it is required to obtain a business registration, it may contact the City's Taxes and Licenses Division at (509) 625-6070 to request an exemption status determination.



## **9. SOCIAL EQUITY REQUIREMENTS.**

No individual shall be excluded from participation in, denied the benefit of, subjected to discrimination under, or denied employment in the administration of or in connection with this Agreement because of age, sex, race, color, religion, creed, marital status, familial status, sexual orientation including gender expression or gender identity, national origin, honorably discharged veteran or military status, the presence of any sensory, mental or physical disability, or use of a service animal by a person with disabilities. Consultant agrees to comply with, and to require that all subcontractors comply with, Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act, as applicable to the Consultant. Consultant shall seek inclusion of woman and minority business for subcontracting. A woman or minority business is one that self-identifies to be at least 51% owned by a woman and/or minority. Such firms do not have to be certified by the State of Washington.

## **10. INDEMNIFICATION.**

The Consultant shall defend, indemnify, and hold the City and its officers and employees harmless from all claims, demands, or suits at law or equity asserted by third parties for bodily injury (including death) and/or property damage which arise from the Consultant's negligence or willful misconduct under this Agreement, including attorneys' fees and litigation costs; provided that nothing herein shall require a Consultant to indemnify the City against and hold harmless the City from claims, demands or suits based solely upon the negligence of the City, its agents, officers, and employees. If a claim or suit is caused by or results from the concurrent negligence of the Consultant's agents or employees and the City, its agents, officers and employees, this indemnity provision shall be valid and enforceable to the extent of the negligence of the Consultant, its agents or employees. The Consultant specifically assumes liability and agrees to defend, indemnify, and hold the City harmless for actions brought by the Consultant's own employees against the City and, solely for the purpose of this indemnification and defense, the Consultant specifically waives any immunity under the Washington State industrial insurance law, or Title 51 RCW. The Consultant recognizes that this waiver was specifically entered into pursuant to the provisions of RCW 4.24.115 and was the subject of mutual negotiation. The indemnity and agreement to defend and hold the City harmless provided for in this section shall survive any termination or expiration of this agreement.

## **11. INSURANCE.**

During the period of the Agreement, the Consultant shall maintain in force at its own expense, each insurance noted below with companies or through sources approved by the State Insurance Commissioner pursuant to RCW Title 48;

A. Worker's Compensation Insurance in compliance with RCW 51.12.020, which requires subject employers to provide workers' compensation coverage for all their subject workers and Employer's Liability Insurance in the amount of \$1,000,000;

B. General Liability Insurance on an occurrence basis, with a combined single limit of not less than \$1,000,000 each occurrence for bodily injury and property damage. It shall include contractual liability coverage for the indemnity provided under this agreement. It shall provide that the City, its officers and employees are additional insureds but only with respect to the Consultant's services to be provided under this Agreement; and



C. Automobile Liability Insurance with a combined single limit, or the equivalent of not less than \$1,000,000 each accident for bodily injury and property damage, including coverage for owned, hired and non-owned vehicles.

D. Professional Liability Insurance with a combined single limit of not less than \$1,000,000 each claim, incident or occurrence. This is to cover damages caused by the error, omission, or negligent acts related to the professional services to be provided under this Agreement. The coverage must remain in effect for at least two (2) years after the Agreement is completed.

There shall be no cancellation, material change, reduction of limits or intent not to renew the insurance coverage(s) without forty-five (45) days written notice from the Consultant or its insurer(s) to the City. As evidence of the insurance coverage(s) required by this Agreement, the Consultant shall furnish acceptable Certificates Of Insurance (COI) to the City at the time it returns this signed Agreement. The certificate shall specify the City of Spokane as "Additional Insured" specifically for Consultant's services under this Agreement, as well as all of the parties who are additional insureds, and include applicable policy endorsements, the forty-five (45) day cancellation clause, and the deduction or retention level. The Consultant shall be financially responsible for all pertinent deductibles, self-insured retentions, and/or self-insurance.

## **12. DEBARMENT AND SUSPENSION.**

The Contractor has provided its certification that it is in compliance with and shall not contract with individuals or organizations which are debarred, suspended, or otherwise excluded from or ineligible from participation in Federal Assistance Programs under Executive Order 12549 and "Debarment and Suspension", codified at 29 CFR part 98.

## **13. AUDIT.**

Upon request, the Consultant shall permit the City and any other governmental agency ("Agency") involved in the funding of the Work to inspect and audit all pertinent books and records. This includes work of the Consultant, any subconsultant, or any other person or entity that performed connected or related Work. Such books and records shall be made available upon reasonable notice of a request by the City, including up to three (3) years after final payment or release of withheld amounts. Such inspection and audit shall occur in Spokane County, Washington, or other reasonable locations mutually agreed to by the parties. The Consultant shall permit the City to copy such books and records at its own expense. The Consultant shall ensure that inspection, audit and copying rights of the City is a condition of any subcontract, agreement or other arrangement under which any other persons or entity may perform Work under this Agreement.

## **14. INDEPENDENT CONSULTANT.**

A. The Consultant is an independent Consultant. This Agreement does not intend the Consultant to act as a City employee. The City has neither direct nor immediate control over the Consultant nor the right to control the manner or means by which the Consultant works. Neither the Consultant nor any Consultant employee shall be an employee of the City. This Agreement prohibits the Consultant to act as an agent or legal representative of the City. The Consultant is not granted express or implied rights or authority to assume or create any obligation or responsibility for or in the name of the City, or to bind the City. The City is not liable for or obligated to pay sick leave, vacation pay, or any other benefit of employment, nor to pay social security or other tax that may arise from employment. The Consultant shall



pay all income and other taxes as due. The Consultant may perform work for other parties; the City is not the exclusive user of the services that the Consultant provides.

- B. If the City needs the Consultant to Work on City premises and/or with City equipment, the City may provide the necessary premises and equipment. Such premises and equipment are exclusively for the Work and not to be used for any other purpose.
- C. If the Consultant works on the City premises using City equipment, the Consultant remains an independent Consultant and not a City employee. The Consultant will notify the City Project Manager if s/he or any other Workers are within ninety (90) days of a consecutive 36-month placement on City property. If the City determines using City premises or equipment is unnecessary to complete the Work, the Consultant will be required to work from its own office space or in the field. The City may negotiate a reduction in Consultant fees or charge a rental fee based on the actual costs to the City, for City premises or equipment.

#### **15. KEY PERSONS.**

The Consultant shall not transfer or reassign any individual designated in this Agreement as essential to the Work, nor shall those key persons, or employees of Consultant identified as to be involved in the Project Work be replaced, removed or withdrawn from the Work without the express written consent of the City, which shall not be unreasonably withheld. If any such individual leaves the Consultant's employment, the Consultant shall present to the City one or more individuals with greater or equal qualifications as a replacement, subject to the City's approval, which shall not be unreasonably withheld. The City's approval does not release the Consultant from its obligations under this Agreement.

#### **16. ASSIGNMENT AND SUBCONTRACTING.**

The Consultant shall not assign or subcontract its obligations under this Agreement without the City's written consent, which may be granted or withheld in the City's sole discretion. Any subcontract made by the Consultant shall incorporate by reference this Agreement, except as otherwise provided. The Consultant shall require that all subconsultants comply with the obligations and requirements of the subcontract. The City's consent to any assignment or subcontract does not release the consultant from liability or any obligation within this Agreement, whether before or after City consent, assignment or subcontract.

#### **17. CITY ETHICS CODE.**

- A. Consultant shall promptly notify the City in writing of any person expected to be a Consultant Worker (including any Consultant employee, subconsultant, principal, or owner) and was a former City officer or employee within the past twelve (12) months.
- B. Consultant shall ensure compliance with the City Ethics Code by any Consultant Worker when the Work or matter related to the Work is performed by a Consultant Worker who has been a City officer or employee within the past two (2) years.
- C. Consultant shall not directly or indirectly offer anything of value (such as retainers, loans, entertainment, favors, gifts, tickets, trips, favors, bonuses, donations, special discounts, work or meals) to any City employee, volunteer or official that is intended, or may appear to a reasonable person to be intended, to obtain or give special consideration to the Consultant. Promotional items worth less than \$25 may be distributed by the Consultant to a City employee if the Consultant uses the items as routine and standard promotional materials. Any violation of this provision may cause termination of this Agreement. Nothing in this Agreement prohibits donations to campaigns for election to City office, so long as the



donation is disclosed as required by the election campaign disclosure laws of the City and of the State.

#### **18. NO CONFLICT OF INTEREST.**

Consultant confirms that the Consultant or workers have no business interest or a close family relationship with any City officer or employee who was or will be involved in the consultant selection, negotiation, drafting, signing, administration or evaluation of the Consultant's work. As used in this Section, the term Consultant includes any worker of the Consultant who was, is, or will be, involved in negotiation, drafting, signing, administration or performance of the Agreement. The term "close family relationship" refers to: spouse or domestic partner, any dependent parent, parent-in-law, child, son-in-law, daughter-in-law; or any parent, parent in-law, sibling, uncle, aunt, cousin, niece or nephew residing in the household of a City officer or employee described above.

#### **19. ERRORS AND OMISSIONS, CORRECTIONS.**

Consultant is responsible for professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on the behalf of the Consultant under this Agreement in the delivery of a final work product. The standard of care applicable to Consultant's services will be the degree of skill and diligence normally employed by professional engineers or Consultants performing the same or similar services at the time said services are performed. The Final Work Product is defined as a stamped, signed work product. Consultant, without additional compensation, shall correct or revise errors or mistakes in designs, drawings, specifications, and/or other consultant services immediately upon notification by the City. The obligation provided for in this Section regarding acts or omissions resulting from this Agreement survives Agreement termination or expiration.

#### **20. INTELLECTUAL PROPERTY RIGHTS.**

- A. Copyrights. The Consultant shall retain the copyright (including the right of reuse) to all materials and documents prepared by the Consultant for the Work, whether or not the Work is completed. The Consultant grants to the City a non-exclusive, irrevocable, unlimited, royalty-free license to use copy and distribute every document and all the materials prepared by the Consultant for the City under this Agreement. If requested by the City, a copy of all drawings, prints, plans, field notes, reports, documents, files, input materials, output materials, the media upon which they are located (including cards, tapes, discs, and other storage facilities), software program or packages (including source code or codes, object codes, upgrades, revisions, modifications, and any related materials) and/or any other related documents or materials developed solely for and paid for by the City to perform the Work, shall be promptly delivered to the City.
- B. Patents: The Consultant assigns to the City all rights in any invention, improvement, or discovery, with all related information, including but not limited to designs, specifications, data, patent rights and findings developed with the performance of the Agreement or any subcontract. Notwithstanding the above, the Consultant does not convey to the City, nor does the City obtain, any right to any document or material utilized by the Consultant created or produced separate from the Agreement or was pre-existing material (not already owned by the City), provided that the Consultant has identified in writing such material as pre-existing prior to commencement of the Work. If pre-existing materials are incorporated in the work, the Consultant grants the City an irrevocable, non-exclusive right and/or license to use, execute, reproduce, display and transfer the pre-existing material, but only as an inseparable part of the work.



- C. The City may make and retain copies of such documents for its information and reference with their use on the project. The Consultant does not represent or warrant that such documents are suitable for reuse by the City or others, on extensions of the project or on any other project, and the City releases the Consultant from liability for any unauthorized reuse of such documents.

## **21. CONFIDENTIALITY.**

Under Washington State Law RCW Chapter 42.56) all materials received or created by the City of Spokane are **public records** which are subject to review and copying pursuant to a public records request. These records include but are not limited to bid or proposal submittals, agreement documents, contract work product, and other bid material. Some records or portions of records may be legally exempt from disclosure and can be redacted or withheld. RCW Ch. 42.56 describes those exemptions. Consultant must familiarize themselves with state law and the City of Spokane's process for managing records.

The City will endeavor to redact anything that clearly should be redacted under the law. For example, the City will generally redact Social Security Numbers, tax records, and financial account numbers before records are made available to a requestor. Consultant may identify any materials Consultant believes to be not subject to release under the Public Records Act. City will not be bound by Consultant's determination of whether any particular record or records are legally exempt from release under the Public Records Act.

If the City receives a public records request for records involving Consultant or Consultant's work product, City will release the records unless City determines that there are obvious exemptions or redactions (which City will make prior to release of the records). If City determines that there are exemptions that can be asserted only by Consultant, City will endeavor to notify Consultant and Consultant will be given ten days to obtain a Court order preventing the City from releasing the requested records. **If no Court order is procured by Consultant, the City will release the requested records.**

## **22. DISPUTES.**

Any dispute or misunderstanding that may arise under this Agreement, concerning the Consultant's performance, shall first be through negotiations, if possible, between the Consultant's Project Manager and the City's Project Manager. It shall be referred to the Director and the Consultant's senior executive(s). If such officials do not agree upon a decision within a reasonable period of time, either party may decline or discontinue such discussions and may then pursue the legal means to resolve such disputes, including but not limited to mediation, arbitration and/or alternative dispute resolution processes. Nothing in this dispute process shall mitigate the rights of the City to terminate the Agreement. Notwithstanding all of the above, if the City believes in good faith that some portion of the Work has not been completed satisfactorily, the City may require the Consultant to correct such work prior to the City payment. The City will provide to the Consultant an explanation of the concern and the remedy that the City expects. The City may withhold from any payment otherwise due, an amount that the City in good faith finds to be under dispute, or if the Consultant provides no sufficient remedy, the City may retain the amount equal to the cost to the City for otherwise correcting or remedying the work not properly completed. Waiver of any of these rights is not deemed a future waiver of any such right or remedy available at law, contract or equity.



### **23. TERMINATION.**

- A. For Cause: The City or Consultant may terminate the Agreement if the other party is in material breach of this Agreement, and such breach has not been corrected to the other party's reasonable satisfaction in a timely manner. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- B. For Reasons Beyond Control of Parties: Either party may terminate this Agreement without recourse by the other where performance is rendered impossible or impracticable for reasons beyond such party's reasonable control, such as, but not limited to, an act of nature, war or warlike operation, civil commotion, riot, labor dispute including strike, walkout or lockout, except labor disputes involving the Consultant's own employees, sabotage, or superior governmental regulation or control. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- C. For Convenience: Either party may terminate this Agreement without cause, upon thirty (30) days written notice to the other party.
- D. Actions upon Termination: if termination occurs not the fault of the Consultant, the Consultant shall be paid for the services properly performed prior to the actual termination date, with any reimbursable expenses then due, but such compensation shall not exceed the maximum compensation to be paid under the Agreement. The Consultant agrees this payment shall fully and adequately compensate the Consultant and all subconsultants for all profits, costs, expenses, losses, liabilities, damages, taxes and charges of any kind (whether foreseen or unforeseen) attributable to the termination of this Agreement.
- E. Upon termination, the Consultant shall provide the City with the most current design documents, contract documents, writings and other products the Consultant has produced to termination, along with copies of all project-related correspondence and similar items. The City shall have the same rights to use these materials as if termination had not occurred; provided however, that the City shall indemnify and hold the Consultant harmless from any claims, losses, or damages to the extent caused by modifications made by the City to the Consultant's work product.

### **24. EXPANSION FOR NEW WORK.**

This Agreement scope may be expanded for new work. Any expansion for New Work (work not specified within the original Scope of Work Section of this Agreement, and/or not specified in the original RFP as intended work for the Agreement) must comply with all the following limitations and requirements: (a) the New Work is not reasonable to solicit separately; (b) the New Work is for reasonable purpose; (c) the New Work was not reasonably known either the City or Consultant at time of contract or else was mentioned as a possibility in the solicitation (such as future phases of work, or a change in law); (d) the New Work is not significant enough to be reasonably regarded as an independent body of work; (e) the New Work would not have attracted a different field of competition; and (f) the change does not vary the essential identified or main purposes of the Agreement. The City may make exceptions for immaterial changes, emergency or sole source conditions, or other situations required in City opinion. Certain changes are not New Work subject to these limitations, such as additional phases of Work anticipated at the time of solicitation, time extensions, Work Orders issued on an On-Call contract, and similar. New Work must be mutually agreed and issued by the City through written Addenda. New Work performed before an authorizing Amendment may not be eligible for payment.



## **25. MISCELLANEOUS PROVISIONS.**

- A. Amendments: No modification of this Agreement shall be effective unless in writing and signed by an authorized representative of each of the parties hereto.
- B. Binding Agreement: This Agreement shall not be binding until signed by both parties. The provisions, covenants and conditions in this Agreement shall bind the parties, their legal heirs, representatives, successors and assigns.
- C. Americans with Disabilities Act (ADA): Specific attention by the designer is required in association with the Americans with Disabilities Act (ADA) 42 U.S.C. 12101-12213 and 47 U.S.C. 225 and 611, its requirements, regulations, standards and guidelines, which were updated in 2010 and are effective and mandatory for all State and local government facilities and places of public accommodation for construction projects including alteration of existing facilities, as of March 15, 2012. The City advises that the requirements for accessibility under the ADA, may contain provisions that differ substantively from accessibility provisions in applicable State and City codes, and if the provisions of the ADA impose a greater or equal protection for the rights of individuals with disabilities or individuals associated with them than the adopted local codes, the ADA prevail unless approval for an exception is obtained by a formal documented process. Where local codes provide exceptions from accessibility requirements that differ from the ADA Standards; such exceptions may not be permitted for publicly owned facilities subject to Title II requirements unless the same exception exists in the Title II regulations. It is the responsibility of the designer to determine the code provisions.
- D. The Consultant, at no expense to the City, shall comply with all laws of the United States and Washington, the Charter and ordinances of the City of Spokane; and rules, regulations, orders and directives of their administrative agencies and officers. Without limiting the generality of this paragraph, the Consultant shall comply with the requirements of this Section.
- E. This Agreement shall be construed and interpreted under the laws of Washington. The venue of any action brought shall be in the Superior Court of Spokane County.
- F. Remedies Cumulative: Rights under this Agreement are cumulative and nonexclusive of any other remedy of law or in equity.
- G. Captions: The titles of sections or subsections are for convenience only and do not define or limit the contents.
- H. Severability: If any term or provision is determined by a court of competent jurisdiction to be invalid or unenforceable, the remainder of this Agreement shall not be affected, and each term and provision shall be valid and enforceable to the fullest extent permitted by law.
- I. Waiver: No covenant, term or condition or the breach shall be deemed waived, except by written consent of the party against whom the waiver is claimed, and any waiver of the breach of any covenant, term or condition shall not be deemed a waiver of any preceding or succeeding breach of the same or any other covenant, term of condition. Neither the acceptance by the City of any performance by the Consultant after the time the same shall have become due nor payment to the Consultant for any portion of the Work shall constitute a waiver by the City of the breach or default of any covenant, term or condition unless otherwise expressly agreed to by the City in writing.
- J. Additional Provisions: This Agreement may be modified by additional terms and conditions ("Special Conditions") which shall be attached to this Agreement as an Exhibit. The parties agree that the Special Conditions shall supplement the terms and conditions of the Agreement, and in the event of ambiguity or conflict with the terms and conditions of the Agreement, these Special Conditions shall govern.



- K. Entire Agreement: This document along with any exhibits and all attachments, and subsequently issued addenda, comprises the entire agreement between the City and the Consultant. If conflict occurs between contract documents and applicable laws, codes, ordinances or regulations, the most stringent or legally binding requirement shall govern and be considered a part of this contract to afford the City the maximum benefits.
- L. Negotiated Agreement: The parties acknowledge this is a negotiated agreement, that they have had this Agreement reviewed by their respective legal counsel, and that the terms and conditions of this Agreement are not to be construed against any party on the basis of such party's draftsmanship.
- M. No personal liability: No officer, agent or authorized employee of the City shall be personally responsible for any liability arising under this Agreement, whether expressed or implied, nor for any statement or representation made or in any connection with this Agreement.

IN WITNESS WHEREOF, in consideration of the terms, conditions and covenants contained, or attached and incorporated and made a part, the parties have executed this Agreement by having legally-binding representatives affix their signatures below.

**COMMONSTREET CONSULTING, LLC**

**CITY OF SPOKANE**

By \_\_\_\_\_  
Signature                      Date

By \_\_\_\_\_  
Signature                      Date

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

**Attachments:** Exhibit A – Certificate Regarding Debarment  
Exhibit B – City's Request for Qualifications



## EXHIBIT A

### CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

1. The undersigned (i.e., signatory for the Subrecipient / Contractor / Consultant) certifies, to the best of its knowledge and belief, that it and its principals:
  - a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency;
  - b. Have not within a three-year period preceding this contract been convicted or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state, or local) transaction or contract under a public transaction; violation of federal or state antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, tax evasion, receiving stolen property, making false claims, or obstruction of justice;
  - c. Are not presently indicted or otherwise criminally or civilly charged by a government entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and,
  - d. Have not within a three-year period preceding this contract had one or more public transactions (federal, state, or local) terminated for cause or default.
2. The undersigned agrees by signing this contract that it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction.
3. The undersigned further agrees by signing this contract that it will include the following clause, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions:

#### Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transactions

1. The lower tier contractor certified, by signing this contract that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any federal department or agency.
  2. Where the lower tier contractor is unable to certify to any of the statements in this contract, such contractor shall attach an explanation to this contract.
4. I understand that a false statement of this certification may be grounds for termination of the contract.

|                                                                      |                                     |
|----------------------------------------------------------------------|-------------------------------------|
| <hr/> Name of Subrecipient / Contractor / Consultant (Type or Print) | <hr/> Program Title (Type or Print) |
| <hr/> Name of Certifying Official (Type or Print)                    | <hr/> Signature                     |
| <hr/> Title of Certifying Official (Type or Print)                   | <hr/> Date (Type or Print)          |



## **EXHIBIT B**





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 11-10-20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** Engineering Services

**Approving Supervisor:** Kyle Twohig

**Amount of Proposed Expenditure:** \$200,000

**Funding Source:** local and state loan/grant

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

The proposed contract with CommonStreet is an on-call contract to be used to pay for real estate acquisition services on public works projects (water, sewer, sidewalk, etc.). Money paid to the consultant under this contract will generally be paid by state loan or grant funds or utility funds on a project by project basis.

**What are the impacts if expenses are deferred?**

Infrastructure upgrades would have to be deferred which, when projects are state funded, would potentially mean the loss of state funds.

**What alternative resources have been considered?**

None available.

**Description of the goods or service and any additional information?**

This expenditure is for hiring a consultant to provide specialized services which the City does not have in house. The work to be provided by the consultant and the associated fee are negotiated and authorized on a project by project basis.

**Person Submitting Form/Contact:** Dan Buller dbuller@spokanecity.org

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/2/2020

**Clerk's File #**

OPR 2020-0841

**Renews #****Submitting Dept**

ENGINEERING SERVICES

**Cross Ref #****Contact Name/Phone**

DAN BULLER 625-6391

**Project #**

2021063

**Contact E-Mail**

DBULLER@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #**

MASTER

**Agenda Item Name**

0390 - PARAMETRIX - ON-CALL SURVEYING SERVICES

**Agenda Wording**

Local Area A&E Professional Services Consultant Agreement with Parametrix, Inc. (Spokane, WA) for Surveying Services for 2021-2022 Non-Federal Aid Project for the amount not to exceed \$150,000.00. (Various Neighborhood Councils)

**Summary (Background)**

The Agreement for Surveying Services is for a period of two years. An option for a one year extension will be granted at the City's discretion. Task Assignments will be prepared under this agreement and scope for individual project needs. Fund shall be from the individual project

Lease? NO

Grant related? YES

Public Works? YES

**Fiscal Impact****Budget Account**

Expense \$ 150,000.00

# Various

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

TWOHIG, KYLE

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

publicworksaccounting@spokanecity.org

**Additional Approvals**

kgoodman@spokanecity.org

**Purchasing**

dbuller@spokanecity.org

**GRANTS &  
CONTRACT MGMT**

STOPHER, SALLY

aduffey@spokanecity.org

rflint@parametrix.com



## Briefing Paper PIES

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Engineering Services; Public Works                                                                                            |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | On-Call Engineering Consultants                                                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | October 26, 2020                                                                                                              |
| <b>Contact (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Dan Buller ( <a href="mailto:dbuller@spokanecity.org">dbuller@spokanecity.org</a> , 625-6391)                                 |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | Scott Simmons                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | PIES                                                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Innovative Infrastructure                                                                                                     |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Informational - background information for committee review                                                                   |
| <p><b>Background/History:</b> Engineering Services has “on-call” agreements with various consultants for specialized engineering or related services (geotech., surveying, historic resources, real estate acquisition and construction management) associated with the City’s public works projects. Those firms are selected on the basis of qualifications as required by RCW 39. These typically agreements last from 2-3 years.</p>                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <p><b>Executive Summary:</b></p> <ul style="list-style-type: none"> <li>• A request for qualifications is being advertised for the above name specialized services.</li> <li>• A review committee ranked the firms by qualifications. One or two firms will be selected for each discipline.</li> <li>• Engineering Services expects to bring five agreements to council over the next several weeks.</li> <li>• Costs incurred under the proposed contracts are paid as part of each public works project for which the consultant is used. The projects associated with these contracts are for all public works except Federal Highway Administration (FHWA) funded projects. (A similar briefing paper was submitted for the June PIES committee meeting for FHWA funded projects).</li> </ul> |                                                                                                                               |
| <p><b>Budget Impact:</b></p> <p>Approved in current year budget?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Annual/Reoccurring expenditure?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                             |                                                                                                                               |
| <p><b>Operations Impact:</b></p> <p>Consistent with current operations/policy?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Requires change in current operations/policy?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                 |                                                                                                                               |





**City of Spokane**

**CONSULTANT AGREEMENT**

**Title: 2021-2022 ON-CALL  
SURVEYING SERVICES**

This Consultant Agreement is made and entered into by and between the **CITY OF SPOKANE** as ("City"), a Washington municipal corporation, and **PARAMETRIX**, whose address is 106 West Mission Avenue, Spokane, Washington 99201 as ("Consultant"), individually hereafter referenced as a "party", and together as the "parties".

*WHEREAS, the purpose of this Agreement is to provide **2021-2022 ON-CALL SURVEYING SERVICES** to the City; and*

*WHEREAS, the Consultant was selected through a Request for Qualification issued by the City.*

*-- NOW, THEREFORE, in consideration of the terms, conditions, covenants and performance of the Scope of Work contained herein, the City and Consultant mutually agree as follows:*

**1. TERM OF AGREEMENT.**

The term of this Agreement begins on January 1, 2021, and ends on December 31, 2022, unless amended by written agreement or terminated earlier under the provisions. The contract may be renewed for one (1) additional one-year contract period, subject to mutual agreement.

**2. TIME OF BEGINNING AND COMPLETION.**

The Consultant shall begin the work outlined in the "Scope of Work" ("Work") on the beginning date, above. The City will acknowledge in writing when the Work is complete. Time limits established under this Agreement shall not be extended because of delays for which the Consultant is responsible, but may be extended by the City, in writing, for the City's convenience or conditions beyond the Consultant's control.

**3. SCOPE OF WORK.**

The General Scope of Work for this Agreement is described in the City's Request for Qualification which is attached as Exhibit B and made a part of this Agreement. In the event of a conflict or discrepancy in the contract documents, the City Agreement controls.



The Work is subject to City review and approval. The Consultant shall confer with the City periodically, and prepare and present information and materials (e.g. detailed outline of completed Work) requested by the City to determine the adequacy of the Work or Consultant's progress.

#### **4. COMPENSATION.**

Total compensation for Consultant's services under this On-Call Agreement shall not exceed **ONE HUNDRED FIFTY THOUSAND AND NO/100 DOLLARS (\$150,000.00)**, excluding tax, if applicable, unless modified by a written amendment to this Agreement. This is the maximum amount to be paid under this Agreement for the work described in Section 3 above, and shall not be exceeded without the prior written authorization of the City in the form of an executed amendment to this Agreement.

#### **5. PAYMENT.**

The Company shall submit its applications for payment to City of Spokane, Engineering Services Department, 808 West Spokane Falls Blvd., Spokane, WA 99201. **Payment will be made via direct deposit/ACH** within thirty (30) days after receipt of the Company's application except as provided by state law. If the City objects to all or any portion of the invoice, it shall notify the Company and pay that portion of the invoice not in dispute. In that event, the parties shall immediately make every effort to settle the disputed amount.

#### **6. REIMBURSABLES**

The reimbursables under this Agreement are to be included, and considered part of the maximum amount not to exceed (above), and require the Consultant's submittal of appropriate documentation and actual itemized receipts, the following limitations apply.

- A. City will reimburse the Consultant at actual cost for expenditures that are pre-approved by the City in writing and are necessary and directly applicable to the work required by this Contract provided that similar direct project costs related to the contracts of other clients are consistently accounted for in a like manner. Such direct project costs may not be charged as part of overhead expenses or include a markup. Other direct charges may include, but are not limited to the following types of items: travel, printing, cell phone, supplies, materials, computer charges, and fees of subconsultants.
- B. The billing for third party direct expenses specifically identifiable with this project shall be an itemized listing of the charges supported by copies of the original bills, invoices, expense accounts, subconsultant paid invoices, and other supporting documents used by the Consultant to generate invoice(s) to the City. The original supporting documents shall be available to the City for inspection upon request. All charges must be necessary for the services provided under this Contract.
- C. The City will reimburse the actual cost for travel expenses incurred as evidenced by copies of receipts (excluding meals) supporting such travel expenses, and in accordance with the City of Spokane Travel Policy, details of which can be provided upon request.
- D. **Airfare:** Airfare will be reimbursed at the actual cost of the airline ticket. The City will reimburse for Economy or Coach Fare only. Receipts detailing each airfare are required.
- E. **Meals:** Meals will be reimbursed at the Federal Per Diem daily meal rate for the city in which the work is performed. *Receipts are not required as documentation.* The invoice shall state "the meals are being billed at the Federal Per Diem daily meal rate", and shall detail how many of each meal is being billed (e.g. the number of breakfasts, lunches, and dinners). The City will not reimburse for alcohol at any time.



- F. **Lodging:** Lodging will be reimbursed at actual cost incurred up to a maximum of the published General Services Administration (GSA) Index for the city in which the work is performed (*the current maximum allowed reimbursement amount can be provided upon request*). Receipts detailing each day / night lodging are required. The City will not reimburse for ancillary expenses charged to the room (e.g. movies, laundry, mini bar, refreshment center, fitness center, sundry items, etc.)
- G. **Vehicle mileage:** Vehicle mileage will be reimbursed at the Federal Internal Revenue Service Standard Business Mileage Rate in affect at the time the mileage expense is incurred. Please note: payment for mileage for long distances traveled will not be more than an equivalent trip round-trip airfare of a common carrier for a coach or economy class ticket.
- H. **Rental Car:** Rental car expenses will be reimbursed at the actual cost of the rental. Rental car receipts are required for all rental car expenses. The City will reimburse for a standard car of a mid-size class or less. The City will not reimburse for ancillary expenses charged to the car rental (e.g. GPS unit).
- I. **Miscellaneous Travel** (e.g. parking, rental car gas, taxi, shuttle, toll fees, ferry fees, etc.): Miscellaneous travel expenses will be reimbursed at the actual cost incurred. Receipts are required for each expense of \$10.00 or more.
- J. **Miscellaneous other business expenses** (e.g. printing, photo development, binding): Other miscellaneous business expenses will be reimbursed at the actual cost incurred and may not include a mark up. Receipts are required for all miscellaneous expenses that are billed.

**Subconsultant:** Subconsultant expenses will be reimbursed at the actual cost incurred and a four percent (4%) markup. Copies of all Subconsultant invoices that are rebilled to the City are required.

## **7. TAXES, FEES AND LICENSES.**

- A. Consultant shall pay and maintain in current status, all necessary licenses, fees, assessments, permit charges, etc. necessary to conduct the work included under this Agreement. It is the Consultant's sole responsibility to monitor and determine changes or the enactment of any subsequent requirements for said fees, assessments, or changes and to immediately comply.
- B. Where required by state statute, ordinance or regulation, Consultant shall pay and maintain in current status all taxes necessary for performance. Consultant shall not charge the City for federal excise taxes. The City will furnish Consultant an exemption certificate where appropriate.
- C. The Director of Finance and Administrative Services may withhold payment pending satisfactory resolution of unpaid taxes and fees due the City.
- D. The cost of any permits, licenses, fees, etc. arising as a result of the projects included in this Agreement shall be included in the project budgets.

## **8. CITY OF SPOKANE BUSINESS LICENSE.**

Section 8.01.070 of the Spokane Municipal Code states that no person may engage in business with the City without first having obtained a valid annual business registration. The Consultant shall be responsible for contacting the State of Washington Business License Services at <http://bls.dor.wa.gov> or 1-800-451-7985 to obtain a business registration. If the Contractor does not believe it is required to obtain a business registration, it may contact the City's Taxes and Licenses Division at (509) 625-6070 to request an exemption status determination.



## **9. SOCIAL EQUITY REQUIREMENTS.**

No individual shall be excluded from participation in, denied the benefit of, subjected to discrimination under, or denied employment in the administration of or in connection with this Agreement because of age, sex, race, color, religion, creed, marital status, familial status, sexual orientation including gender expression or gender identity, national origin, honorably discharged veteran or military status, the presence of any sensory, mental or physical disability, or use of a service animal by a person with disabilities. Consultant agrees to comply with, and to require that all subcontractors comply with, Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act, as applicable to the Consultant. Consultant shall seek inclusion of woman and minority business for subcontracting. A woman or minority business is one that self-identifies to be at least 51% owned by a woman and/or minority. Such firms do not have to be certified by the State of Washington.

## **10. INDEMNIFICATION.**

The Consultant shall defend, indemnify, and hold the City and its officers and employees harmless from all claims, demands, or suits at law or equity asserted by third parties for bodily injury (including death) and/or property damage which arise from the Consultant's negligence or willful misconduct under this Agreement, including attorneys' fees and litigation costs; provided that nothing herein shall require a Consultant to indemnify the City against and hold harmless the City from claims, demands or suits based solely upon the negligence of the City, its agents, officers, and employees. If a claim or suit is caused by or results from the concurrent negligence of the Consultant's agents or employees and the City, its agents, officers and employees, this indemnity provision shall be valid and enforceable to the extent of the negligence of the Consultant, its agents or employees. The Consultant specifically assumes liability and agrees to defend, indemnify, and hold the City harmless for actions brought by the Consultant's own employees against the City and, solely for the purpose of this indemnification and defense, the Consultant specifically waives any immunity under the Washington State industrial insurance law, or Title 51 RCW. The Consultant recognizes that this waiver was specifically entered into pursuant to the provisions of RCW 4.24.115 and was the subject of mutual negotiation. The indemnity and agreement to defend and hold the City harmless provided for in this section shall survive any termination or expiration of this agreement.

## **11. INSURANCE.**

During the period of the Agreement, the Consultant shall maintain in force at its own expense, each insurance noted below with companies or through sources approved by the State Insurance Commissioner pursuant to RCW Title 48;

A. Worker's Compensation Insurance in compliance with RCW 51.12.020, which requires subject employers to provide workers' compensation coverage for all their subject workers and Employer's Liability Insurance in the amount of \$1,000,000;

B. General Liability Insurance on an occurrence basis, with a combined single limit of not less than \$1,000,000 each occurrence for bodily injury and property damage. It shall include contractual liability coverage for the indemnity provided under this agreement. It shall provide that the City, its officers and employees are additional insureds but only with respect to the Consultant's services to be provided under this Agreement; and



C. Automobile Liability Insurance with a combined single limit, or the equivalent of not less than \$1,000,000 each accident for bodily injury and property damage, including coverage for owned, hired and non-owned vehicles.

D. Professional Liability Insurance with a combined single limit of not less than \$1,000,000 each claim, incident or occurrence. This is to cover damages caused by the error, omission, or negligent acts related to the professional services to be provided under this Agreement. The coverage must remain in effect for at least two (2) years after the Agreement is completed.

There shall be no cancellation, material change, reduction of limits or intent not to renew the insurance coverage(s) without forty-five (45) days written notice from the Consultant or its insurer(s) to the City. As evidence of the insurance coverage(s) required by this Agreement, the Consultant shall furnish acceptable Certificates Of Insurance (COI) to the City at the time it returns this signed Agreement. The certificate shall specify the City of Spokane as "Additional Insured" specifically for Consultant's services under this Agreement, as well as all of the parties who are additional insureds, and include applicable policy endorsements, the forty-five (45) day cancellation clause, and the deduction or retention level. The Consultant shall be financially responsible for all pertinent deductibles, self-insured retentions, and/or self-insurance.

## **12. DEBARMENT AND SUSPENSION.**

The Contractor has provided its certification that it is in compliance with and shall not contract with individuals or organizations which are debarred, suspended, or otherwise excluded from or ineligible from participation in Federal Assistance Programs under Executive Order 12549 and "Debarment and Suspension", codified at 29 CFR part 98.

## **13. AUDIT.**

Upon request, the Consultant shall permit the City and any other governmental agency ("Agency") involved in the funding of the Work to inspect and audit all pertinent books and records. This includes work of the Consultant, any subconsultant, or any other person or entity that performed connected or related Work. Such books and records shall be made available upon reasonable notice of a request by the City, including up to three (3) years after final payment or release of withheld amounts. Such inspection and audit shall occur in Spokane County, Washington, or other reasonable locations mutually agreed to by the parties. The Consultant shall permit the City to copy such books and records at its own expense. The Consultant shall ensure that inspection, audit and copying rights of the City is a condition of any subcontract, agreement or other arrangement under which any other persons or entity may perform Work under this Agreement.

## **14. INDEPENDENT CONSULTANT.**

A. The Consultant is an independent Consultant. This Agreement does not intend the Consultant to act as a City employee. The City has neither direct nor immediate control over the Consultant nor the right to control the manner or means by which the Consultant works. Neither the Consultant nor any Consultant employee shall be an employee of the City. This Agreement prohibits the Consultant to act as an agent or legal representative of the City. The Consultant is not granted express or implied rights or authority to assume or create any obligation or responsibility for or in the name of the City, or to bind the City. The City is not liable for or obligated to pay sick leave, vacation pay, or any other benefit of employment, nor to pay social security or other tax that may arise from employment. The Consultant shall



pay all income and other taxes as due. The Consultant may perform work for other parties; the City is not the exclusive user of the services that the Consultant provides.

- B. If the City needs the Consultant to Work on City premises and/or with City equipment, the City may provide the necessary premises and equipment. Such premises and equipment are exclusively for the Work and not to be used for any other purpose.
- C. If the Consultant works on the City premises using City equipment, the Consultant remains an independent Consultant and not a City employee. The Consultant will notify the City Project Manager if s/he or any other Workers are within ninety (90) days of a consecutive 36-month placement on City property. If the City determines using City premises or equipment is unnecessary to complete the Work, the Consultant will be required to work from its own office space or in the field. The City may negotiate a reduction in Consultant fees or charge a rental fee based on the actual costs to the City, for City premises or equipment.

#### **15. KEY PERSONS.**

The Consultant shall not transfer or reassign any individual designated in this Agreement as essential to the Work, nor shall those key persons, or employees of Consultant identified as to be involved in the Project Work be replaced, removed or withdrawn from the Work without the express written consent of the City, which shall not be unreasonably withheld. If any such individual leaves the Consultant's employment, the Consultant shall present to the City one or more individuals with greater or equal qualifications as a replacement, subject to the City's approval, which shall not be unreasonably withheld. The City's approval does not release the Consultant from its obligations under this Agreement.

#### **16. ASSIGNMENT AND SUBCONTRACTING.**

The Consultant shall not assign or subcontract its obligations under this Agreement without the City's written consent, which may be granted or withheld in the City's sole discretion. Any subcontract made by the Consultant shall incorporate by reference this Agreement, except as otherwise provided. The Consultant shall require that all subconsultants comply with the obligations and requirements of the subcontract. The City's consent to any assignment or subcontract does not release the consultant from liability or any obligation within this Agreement, whether before or after City consent, assignment or subcontract.

#### **17. CITY ETHICS CODE.**

- A. Consultant shall promptly notify the City in writing of any person expected to be a Consultant Worker (including any Consultant employee, subconsultant, principal, or owner) and was a former City officer or employee within the past twelve (12) months.
- B. Consultant shall ensure compliance with the City Ethics Code by any Consultant Worker when the Work or matter related to the Work is performed by a Consultant Worker who has been a City officer or employee within the past two (2) years.
- C. Consultant shall not directly or indirectly offer anything of value (such as retainers, loans, entertainment, favors, gifts, tickets, trips, favors, bonuses, donations, special discounts, work or meals) to any City employee, volunteer or official that is intended, or may appear to a reasonable person to be intended, to obtain or give special consideration to the Consultant. Promotional items worth less than \$25 may be distributed by the Consultant to a City employee if the Consultant uses the items as routine and standard promotional materials. Any violation of this provision may cause termination of this Agreement. Nothing in this Agreement prohibits donations to campaigns for election to City office, so long as the



donation is disclosed as required by the election campaign disclosure laws of the City and of the State.

#### **18. NO CONFLICT OF INTEREST.**

Consultant confirms that the Consultant or workers have no business interest or a close family relationship with any City officer or employee who was or will be involved in the consultant selection, negotiation, drafting, signing, administration or evaluation of the Consultant's work. As used in this Section, the term Consultant includes any worker of the Consultant who was, is, or will be, involved in negotiation, drafting, signing, administration or performance of the Agreement. The term "close family relationship" refers to: spouse or domestic partner, any dependent parent, parent-in-law, child, son-in-law, daughter-in-law; or any parent, parent in-law, sibling, uncle, aunt, cousin, niece or nephew residing in the household of a City officer or employee described above.

#### **19. ERRORS AND OMISSIONS, CORRECTIONS.**

Consultant is responsible for professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on the behalf of the Consultant under this Agreement in the delivery of a final work product. The standard of care applicable to Consultant's services will be the degree of skill and diligence normally employed by professional engineers or Consultants performing the same or similar services at the time said services are performed. The Final Work Product is defined as a stamped, signed work product. Consultant, without additional compensation, shall correct or revise errors or mistakes in designs, drawings, specifications, and/or other consultant services immediately upon notification by the City. The obligation provided for in this Section regarding acts or omissions resulting from this Agreement survives Agreement termination or expiration.

#### **20. INTELLECTUAL PROPERTY RIGHTS.**

- A. Copyrights. The Consultant shall retain the copyright (including the right of reuse) to all materials and documents prepared by the Consultant for the Work, whether or not the Work is completed. The Consultant grants to the City a non-exclusive, irrevocable, unlimited, royalty-free license to use copy and distribute every document and all the materials prepared by the Consultant for the City under this Agreement. If requested by the City, a copy of all drawings, prints, plans, field notes, reports, documents, files, input materials, output materials, the media upon which they are located (including cards, tapes, discs, and other storage facilities), software program or packages (including source code or codes, object codes, upgrades, revisions, modifications, and any related materials) and/or any other related documents or materials developed solely for and paid for by the City to perform the Work, shall be promptly delivered to the City.
- B. Patents: The Consultant assigns to the City all rights in any invention, improvement, or discovery, with all related information, including but not limited to designs, specifications, data, patent rights and findings developed with the performance of the Agreement or any subcontract. Notwithstanding the above, the Consultant does not convey to the City, nor does the City obtain, any right to any document or material utilized by the Consultant created or produced separate from the Agreement or was pre-existing material (not already owned by the City), provided that the Consultant has identified in writing such material as pre-existing prior to commencement of the Work. If pre-existing materials are incorporated in the work, the Consultant grants the City an irrevocable, non-exclusive right and/or license to use, execute, reproduce, display and transfer the pre-existing material, but only as an inseparable part of the work.



- C. The City may make and retain copies of such documents for its information and reference with their use on the project. The Consultant does not represent or warrant that such documents are suitable for reuse by the City or others, on extensions of the project or on any other project, and the City releases the Consultant from liability for any unauthorized reuse of such documents.

## **21. CONFIDENTIALITY.**

Under Washington State Law RCW Chapter 42.56) all materials received or created by the City of Spokane are **public records** which are subject to review and copying pursuant to a public records request. These records include but are not limited to bid or proposal submittals, agreement documents, contract work product, and other bid material. Some records or portions of records may be legally exempt from disclosure and can be redacted or withheld. RCW Ch. 42.56 describes those exemptions. Consultant must familiarize themselves with state law and the City of Spokane's process for managing records.

The City will endeavor to redact anything that clearly should be redacted under the law. For example, the City will generally redact Social Security Numbers, tax records, and financial account numbers before records are made available to a requestor. Consultant may identify any materials Consultant believes to be not subject to release under the Public Records Act. City will not be bound by Consultant's determination of whether any particular record or records are legally exempt from release under the Public Records Act.

If the City receives a public records request for records involving Consultant or Consultant's work product, City will release the records unless City determines that there are obvious exemptions or redactions (which City will make prior to release of the records). If City determines that there are exemptions that can be asserted only by Consultant, City will endeavor to notify Consultant and Consultant will be given ten days to obtain a Court order preventing the City from releasing the requested records. **If no Court order is procured by Consultant, the City will release the requested records.**

## **22. DISPUTES.**

Any dispute or misunderstanding that may arise under this Agreement, concerning the Consultant's performance, shall first be through negotiations, if possible, between the Consultant's Project Manager and the City's Project Manager. It shall be referred to the Director and the Consultant's senior executive(s). If such officials do not agree upon a decision within a reasonable period of time, either party may decline or discontinue such discussions and may then pursue the legal means to resolve such disputes, including but not limited to mediation, arbitration and/or alternative dispute resolution processes. Nothing in this dispute process shall mitigate the rights of the City to terminate the Agreement. Notwithstanding all of the above, if the City believes in good faith that some portion of the Work has not been completed satisfactorily, the City may require the Consultant to correct such work prior to the City payment. The City will provide to the Consultant an explanation of the concern and the remedy that the City expects. The City may withhold from any payment otherwise due, an amount that the City in good faith finds to be under dispute, or if the Consultant provides no sufficient remedy, the City may retain the amount equal to the cost to the City for otherwise correcting or remedying the work not properly completed. Waiver of any of these rights is not deemed a future waiver of any such right or remedy available at law, contract or equity.



### **23. TERMINATION.**

- A. For Cause: The City or Consultant may terminate the Agreement if the other party is in material breach of this Agreement, and such breach has not been corrected to the other party's reasonable satisfaction in a timely manner. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- B. For Reasons Beyond Control of Parties: Either party may terminate this Agreement without recourse by the other where performance is rendered impossible or impracticable for reasons beyond such party's reasonable control, such as, but not limited to, an act of nature, war or warlike operation, civil commotion, riot, labor dispute including strike, walkout or lockout, except labor disputes involving the Consultant's own employees, sabotage, or superior governmental regulation or control. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- C. For Convenience: Either party may terminate this Agreement without cause, upon thirty (30) days written notice to the other party.
- D. Actions upon Termination: if termination occurs not the fault of the Consultant, the Consultant shall be paid for the services properly performed prior to the actual termination date, with any reimbursable expenses then due, but such compensation shall not exceed the maximum compensation to be paid under the Agreement. The Consultant agrees this payment shall fully and adequately compensate the Consultant and all subconsultants for all profits, costs, expenses, losses, liabilities, damages, taxes and charges of any kind (whether foreseen or unforeseen) attributable to the termination of this Agreement.
- E. Upon termination, the Consultant shall provide the City with the most current design documents, contract documents, writings and other products the Consultant has produced to termination, along with copies of all project-related correspondence and similar items. The City shall have the same rights to use these materials as if termination had not occurred; provided however, that the City shall indemnify and hold the Consultant harmless from any claims, losses, or damages to the extent caused by modifications made by the City to the Consultant's work product.

### **24. EXPANSION FOR NEW WORK.**

This Agreement scope may be expanded for new work. Any expansion for New Work (work not specified within the original Scope of Work Section of this Agreement, and/or not specified in the original RFP as intended work for the Agreement) must comply with all the following limitations and requirements: (a) the New Work is not reasonable to solicit separately; (b) the New Work is for reasonable purpose; (c) the New Work was not reasonably known either the City or Consultant at time of contract or else was mentioned as a possibility in the solicitation (such as future phases of work, or a change in law); (d) the New Work is not significant enough to be reasonably regarded as an independent body of work; (e) the New Work would not have attracted a different field of competition; and (f) the change does not vary the essential identified or main purposes of the Agreement. The City may make exceptions for immaterial changes, emergency or sole source conditions, or other situations required in City opinion. Certain changes are not New Work subject to these limitations, such as additional phases of Work anticipated at the time of solicitation, time extensions, Work Orders issued on an On-Call contract, and similar. New Work must be mutually agreed and issued by the City through written Addenda. New Work performed before an authorizing Amendment may not be eligible for payment.



## **25. MISCELLANEOUS PROVISIONS.**

- A. Amendments: No modification of this Agreement shall be effective unless in writing and signed by an authorized representative of each of the parties hereto.
- B. Binding Agreement: This Agreement shall not be binding until signed by both parties. The provisions, covenants and conditions in this Agreement shall bind the parties, their legal heirs, representatives, successors and assigns.
- C. Americans with Disabilities Act (ADA): Specific attention by the designer is required in association with the Americans with Disabilities Act (ADA) 42 U.S.C. 12101-12213 and 47 U.S.C. 225 and 611, its requirements, regulations, standards and guidelines, which were updated in 2010 and are effective and mandatory for all State and local government facilities and places of public accommodation for construction projects including alteration of existing facilities, as of March 15, 2012. The City advises that the requirements for accessibility under the ADA, may contain provisions that differ substantively from accessibility provisions in applicable State and City codes, and if the provisions of the ADA impose a greater or equal protection for the rights of individuals with disabilities or individuals associated with them than the adopted local codes, the ADA prevail unless approval for an exception is obtained by a formal documented process. Where local codes provide exceptions from accessibility requirements that differ from the ADA Standards; such exceptions may not be permitted for publicly owned facilities subject to Title II requirements unless the same exception exists in the Title II regulations. It is the responsibility of the designer to determine the code provisions.
- D. The Consultant, at no expense to the City, shall comply with all laws of the United States and Washington, the Charter and ordinances of the City of Spokane; and rules, regulations, orders and directives of their administrative agencies and officers. Without limiting the generality of this paragraph, the Consultant shall comply with the requirements of this Section.
- E. This Agreement shall be construed and interpreted under the laws of Washington. The venue of any action brought shall be in the Superior Court of Spokane County.
- F. Remedies Cumulative: Rights under this Agreement are cumulative and nonexclusive of any other remedy of law or in equity.
- G. Captions: The titles of sections or subsections are for convenience only and do not define or limit the contents.
- H. Severability: If any term or provision is determined by a court of competent jurisdiction to be invalid or unenforceable, the remainder of this Agreement shall not be affected, and each term and provision shall be valid and enforceable to the fullest extent permitted by law.
- I. Waiver: No covenant, term or condition or the breach shall be deemed waived, except by written consent of the party against whom the waiver is claimed, and any waiver of the breach of any covenant, term or condition shall not be deemed a waiver of any preceding or succeeding breach of the same or any other covenant, term of condition. Neither the acceptance by the City of any performance by the Consultant after the time the same shall have become due nor payment to the Consultant for any portion of the Work shall constitute a waiver by the City of the breach or default of any covenant, term or condition unless otherwise expressly agreed to by the City in writing.
- J. Additional Provisions: This Agreement may be modified by additional terms and conditions ("Special Conditions") which shall be attached to this Agreement as an Exhibit. The parties agree that the Special Conditions shall supplement the terms and conditions of the Agreement, and in the event of ambiguity or conflict with the terms and conditions of the Agreement, these Special Conditions shall govern.



- K. Entire Agreement: This document along with any exhibits and all attachments, and subsequently issued addenda, comprises the entire agreement between the City and the Consultant. If conflict occurs between contract documents and applicable laws, codes, ordinances or regulations, the most stringent or legally binding requirement shall govern and be considered a part of this contract to afford the City the maximum benefits.
- L. Negotiated Agreement: The parties acknowledge this is a negotiated agreement, that they have had this Agreement reviewed by their respective legal counsel, and that the terms and conditions of this Agreement are not to be construed against any party on the basis of such party's draftsmanship.
- M. No personal liability: No officer, agent or authorized employee of the City shall be personally responsible for any liability arising under this Agreement, whether expressed or implied, nor for any statement or representation made or in any connection with this Agreement.

IN WITNESS WHEREOF, in consideration of the terms, conditions and covenants contained, or attached and incorporated and made a part, the parties have executed this Agreement by having legally-binding representatives affix their signatures below.

**PARAMETRIX**

**CITY OF SPOKANE**

By \_\_\_\_\_  
Signature                      Date

By \_\_\_\_\_  
Signature                      Date

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

**Attachments:** Exhibit A – Certificate Regarding Debarment  
Exhibit B – City's Request for Qualifications



## EXHIBIT A

### CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

1. The undersigned (i.e., signatory for the Subrecipient / Contractor / Consultant) certifies, to the best of its knowledge and belief, that it and its principals:
  - a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency;
  - b. Have not within a three-year period preceding this contract been convicted or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state, or local) transaction or contract under a public transaction; violation of federal or state antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, tax evasion, receiving stolen property, making false claims, or obstruction of justice;
  - c. Are not presently indicted or otherwise criminally or civilly charged by a government entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and,
  - d. Have not within a three-year period preceding this contract had one or more public transactions (federal, state, or local) terminated for cause or default.
2. The undersigned agrees by signing this contract that it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction.
3. The undersigned further agrees by signing this contract that it will include the following clause, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions:

#### Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transactions

1. The lower tier contractor certified, by signing this contract that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any federal department or agency.
  2. Where the lower tier contractor is unable to certify to any of the statements in this contract, such contractor shall attach an explanation to this contract.
4. I understand that a false statement of this certification may be grounds for termination of the contract.

|                                                                      |                                     |
|----------------------------------------------------------------------|-------------------------------------|
| <hr/> Name of Subrecipient / Contractor / Consultant (Type or Print) | <hr/> Program Title (Type or Print) |
| <hr/> Name of Certifying Official (Type or Print)                    | <hr/> Signature                     |
| <hr/> Title of Certifying Official (Type or Print)                   | <hr/> Date (Type or Print)          |



## **EXHIBIT B**





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 11-10-20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** Engineering Services

**Approving Supervisor:** Kyle Twohig

**Amount of Proposed Expenditure:** \$150,000

**Funding Source:** local and state loan/grant

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

The proposed contract with Parametrix is an on-call contract to be used to pay for surveying on public works projects (water, sewer, sidewalk, etc.). Money paid to the consultant under this contract will generally be paid by state loan or grant funds or utility funds on a project by project basis.

**What are the impacts if expenses are deferred?**

Infrastructure upgrades would have to be deferred which, when projects are state funded, would potentially mean the loss of state funds.

**What alternative resources have been considered?**

None available.

**Description of the goods or service and any additional information?**

This expenditure is for hiring a consultant to provide specialized services which, in the case of surveying, the City may not have the capacity to perform with existing personnel. This capacity limitation, if it exists in the coming 2021-2022 construction seasons, will be of a limited duration. That is, as a general rule city personnel have capacity to do the City's surveying. The work to be provided by the consultant and the associated fee are negotiated and authorized on a project by project basis.

**Person Submitting Form/Contact:** Dan Buller dbuller@spokanecity.org

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

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**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/4/2020

**Clerk's File #**

OPR 2020-0842

**Renews #****Submitting Dept**

ENGINEERING SERVICES

**Cross Ref #****Contact Name/Phone**

DAN BULLER 625-6391

**Project #**

2021061

**Contact E-Mail**

DBULLER@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #**

MASTER

**Agenda Item Name**

0390 - GEO ENGINEERS - GEOTECHNICAL ENGINEERING ON-CALL SERVICES

**Agenda Wording**

Local Area A&E Professional Services Consultant Agreement with Geo Engineers Inc.; (Spokane, WA) for Geotechnical Engineering Services for 2021-2022 Non-Federal Aid Project for the amount not to exceed \$800,000.00. (Various Neighborhood Councils)

**Summary (Background)**

The Agreement for Geotechnical Engineering Services is for a period of two years. An option for a one year extension will be granted at the City's discretion. Task Assignments will be prepared under this agreement and scope for individual project needs. Fund shall be from the individual project.

Lease? NO

Grant related? YES

Public Works? YES

**Fiscal Impact****Budget Account**

Expense \$ 800,000.00

# Various

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

TWOHIG, KYLE

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

Beggs

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

publicworksaccounting@spokanecity.org

**Additional Approvals**

kgoodman@spokanecity.org

**Purchasing**

dbuller@spokanecity.org

**GRANTS &  
CONTRACT MGMT**

STOPHER, SALLY

aduffey@spokanecity.org

tdugger@geoengineers.com



## Briefing Paper PIES

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Engineering Services; Public Works                                                                                            |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    | On-Call Engineering Consultants                                                                                               |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | October 26, 2020                                                                                                              |
| <b>Contact (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Dan Buller ( <a href="mailto:dbuller@spokanecity.org">dbuller@spokanecity.org</a> , 625-6391)                                 |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                                                                                                                               |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | Scott Simmons                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | PIES                                                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Innovative Infrastructure                                                                                                     |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Informational - background information for committee review                                                                   |
| <p><b>Background/History:</b> Engineering Services has “on-call” agreements with various consultants for specialized engineering or related services (geotech., surveying, historic resources, real estate acquisition and construction management) associated with the City’s public works projects. Those firms are selected on the basis of qualifications as required by RCW 39. These typically agreements last from 2-3 years.</p>                                                                                                                                                                                                                                                                                                                                                           |                                                                                                                               |
| <p><b>Executive Summary:</b></p> <ul style="list-style-type: none"> <li>• A request for qualifications is being advertised for the above name specialized services.</li> <li>• A review committee ranked the firms by qualifications. One or two firms will be selected for each discipline.</li> <li>• Engineering Services expects to bring five agreements to council over the next several weeks.</li> <li>• Costs incurred under the proposed contracts are paid as part of each public works project for which the consultant is used. The projects associated with these contracts are for all public works except Federal Highway Administration (FHWA) funded projects. (A similar briefing paper was submitted for the June PIES committee meeting for FHWA funded projects).</li> </ul> |                                                                                                                               |
| <p><b>Budget Impact:</b></p> <p>Approved in current year budget?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Annual/Reoccurring expenditure?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                             |                                                                                                                               |
| <p><b>Operations Impact:</b></p> <p>Consistent with current operations/policy?      <input checked="" type="checkbox"/> Yes    <input type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Requires change in current operations/policy?      <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No    <input type="checkbox"/> N/A</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                 |                                                                                                                               |





**City of Spokane**

**CONSULTANT AGREEMENT**

**Title: 2021-2022 GEOTECHNICAL ENGINEERING  
SERVICES ON-CALL SERVICES**

This Consultant Agreement is made and entered into by and between the **CITY OF SPOKANE** as ("City"), a Washington municipal corporation, and **GEOENGINEERS, INC.**, whose address is 523 East Second Avenue, Spokane, Washington, 99202 as ("Consultant"), individually hereafter referenced as a "party", and together as the "parties".

*WHEREAS, the purpose of this Agreement is to provide **2021-2022 GEOTECHNICAL ENGINEERING ON-CALL SERVICES** to the City; and*

*WHEREAS, the Consultant was selected through a Request for Qualification issued by the City.*

*-- NOW, THEREFORE, in consideration of the terms, conditions, covenants and performance of the Scope of Work contained herein, the City and Consultant mutually agree as follows:*

**1. TERM OF AGREEMENT.**

The term of this Agreement begins on January 1, 2021, and ends on December 31, 2022, unless amended by written agreement or terminated earlier under the provisions. The contract may be renewed for one (1) additional one-year contract period, subject to mutual agreement.

**2. TIME OF BEGINNING AND COMPLETION.**

The Consultant shall begin the work outlined in the "Scope of Work" ("Work") on the beginning date, above. The City will acknowledge in writing when the Work is complete. Time limits established under this Agreement shall not be extended because of delays for which the Consultant is responsible, but may be extended by the City, in writing, for the City's convenience or conditions beyond the Consultant's control.

**3. SCOPE OF WORK.**

The General Scope of Work for this Agreement is described in the City's Request for Qualification which is attached as Exhibit B and made a part of this Agreement. In the event of a conflict or discrepancy in the contract documents, the City Agreement controls.



The Work is subject to City review and approval. The Consultant shall confer with the City periodically, and prepare and present information and materials (e.g. detailed outline of completed Work) requested by the City to determine the adequacy of the Work or Consultant's progress.

#### **4. COMPENSATION.**

Total compensation for Consultant's services under this On-Call Agreement shall not exceed **EIGHT HUNDRED THOUSAND AND NO/100 DOLLARS (\$800,000.00)**, excluding tax, if applicable, unless modified by a written amendment to this Agreement. This is the maximum amount to be paid under this Agreement for the work described in Section 3 above, and shall not be exceeded without the prior written authorization of the City in the form of an executed amendment to this Agreement.

#### **5. PAYMENT.**

The Company shall submit its applications for payment to City of Spokane, Engineering Services Department, 808 West Spokane Falls Blvd., Spokane, WA 99201. **Payment will be made via direct deposit/ACH** within thirty (30) days after receipt of the Company's application except as provided by state law. If the City objects to all or any portion of the invoice, it shall notify the Company and pay that portion of the invoice not in dispute. In that event, the parties shall immediately make every effort to settle the disputed amount.

#### **6. REIMBURSABLES**

The reimbursables under this Agreement are to be included, and considered part of the maximum amount not to exceed (above), and require the Consultant's submittal of appropriate documentation and actual itemized receipts, the following limitations apply.

- A. City will reimburse the Consultant at actual cost for expenditures that are pre-approved by the City in writing and are necessary and directly applicable to the work required by this Contract provided that similar direct project costs related to the contracts of other clients are consistently accounted for in a like manner. Such direct project costs may not be charged as part of overhead expenses or include a markup. Other direct charges may include, but are not limited to the following types of items: travel, printing, cell phone, supplies, materials, computer charges, and fees of subconsultants.
- B. The billing for third party direct expenses specifically identifiable with this project shall be an itemized listing of the charges supported by copies of the original bills, invoices, expense accounts, subconsultant paid invoices, and other supporting documents used by the Consultant to generate invoice(s) to the City. The original supporting documents shall be available to the City for inspection upon request. All charges must be necessary for the services provided under this Contract.
- C. The City will reimburse the actual cost for travel expenses incurred as evidenced by copies of receipts (excluding meals) supporting such travel expenses, and in accordance with the City of Spokane Travel Policy, details of which can be provided upon request.
- D. **Airfare:** Airfare will be reimbursed at the actual cost of the airline ticket. The City will reimburse for Economy or Coach Fare only. Receipts detailing each airfare are required.
- E. **Meals:** Meals will be reimbursed at the Federal Per Diem daily meal rate for the city in which the work is performed. *Receipts are not required as documentation.* The invoice shall state "the meals are being billed at the Federal Per Diem daily meal rate", and shall detail how many of each meal is being billed (e.g. the number of breakfasts, lunches, and dinners). The City will not reimburse for alcohol at any time.



- F. **Lodging:** Lodging will be reimbursed at actual cost incurred up to a maximum of the published General Services Administration (GSA) Index for the city in which the work is performed (*the current maximum allowed reimbursement amount can be provided upon request*). Receipts detailing each day / night lodging are required. The City will not reimburse for ancillary expenses charged to the room (e.g. movies, laundry, mini bar, refreshment center, fitness center, sundry items, etc.)
- G. **Vehicle mileage:** Vehicle mileage will be reimbursed at the Federal Internal Revenue Service Standard Business Mileage Rate in affect at the time the mileage expense is incurred. Please note: payment for mileage for long distances traveled will not be more than an equivalent trip round-trip airfare of a common carrier for a coach or economy class ticket.
- H. **Rental Car:** Rental car expenses will be reimbursed at the actual cost of the rental. Rental car receipts are required for all rental car expenses. The City will reimburse for a standard car of a mid-size class or less. The City will not reimburse for ancillary expenses charged to the car rental (e.g. GPS unit).
- I. **Miscellaneous Travel** (e.g. parking, rental car gas, taxi, shuttle, toll fees, ferry fees, etc.): Miscellaneous travel expenses will be reimbursed at the actual cost incurred. Receipts are required for each expense of \$10.00 or more.
- J. **Miscellaneous other business expenses** (e.g. printing, photo development, binding): Other miscellaneous business expenses will be reimbursed at the actual cost incurred and may not include a mark up. Receipts are required for all miscellaneous expenses that are billed.

**Subconsultant:** Subconsultant expenses will be reimbursed at the actual cost incurred and a four percent (4%) markup. Copies of all Subconsultant invoices that are rebilled to the City are required.

## **7. TAXES, FEES AND LICENSES.**

- A. Consultant shall pay and maintain in current status, all necessary licenses, fees, assessments, permit charges, etc. necessary to conduct the work included under this Agreement. It is the Consultant's sole responsibility to monitor and determine changes or the enactment of any subsequent requirements for said fees, assessments, or changes and to immediately comply.
- B. Where required by state statute, ordinance or regulation, Consultant shall pay and maintain in current status all taxes necessary for performance. Consultant shall not charge the City for federal excise taxes. The City will furnish Consultant an exemption certificate where appropriate.
- C. The Director of Finance and Administrative Services may withhold payment pending satisfactory resolution of unpaid taxes and fees due the City.
- D. The cost of any permits, licenses, fees, etc. arising as a result of the projects included in this Agreement shall be included in the project budgets.

## **8. CITY OF SPOKANE BUSINESS LICENSE.**

Section 8.01.070 of the Spokane Municipal Code states that no person may engage in business with the City without first having obtained a valid annual business registration. The Consultant shall be responsible for contacting the State of Washington Business License Services at <http://bls.dor.wa.gov> or 1-800-451-7985 to obtain a business registration. If the Contractor does not believe it is required to obtain a business registration, it may contact the City's Taxes and Licenses Division at (509) 625-6070 to request an exemption status determination.



## **9. SOCIAL EQUITY REQUIREMENTS.**

No individual shall be excluded from participation in, denied the benefit of, subjected to discrimination under, or denied employment in the administration of or in connection with this Agreement because of age, sex, race, color, religion, creed, marital status, familial status, sexual orientation including gender expression or gender identity, national origin, honorably discharged veteran or military status, the presence of any sensory, mental or physical disability, or use of a service animal by a person with disabilities. Consultant agrees to comply with, and to require that all subcontractors comply with, Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act, as applicable to the Consultant. Consultant shall seek inclusion of woman and minority business for subcontracting. A woman or minority business is one that self-identifies to be at least 51% owned by a woman and/or minority. Such firms do not have to be certified by the State of Washington.

## **10. INDEMNIFICATION.**

The Consultant shall defend, indemnify, and hold the City and its officers and employees harmless from all claims, demands, or suits at law or equity asserted by third parties for bodily injury (including death) and/or property damage which arise from the Consultant's negligence or willful misconduct under this Agreement, including attorneys' fees and litigation costs; provided that nothing herein shall require a Consultant to indemnify the City against and hold harmless the City from claims, demands or suits based solely upon the negligence of the City, its agents, officers, and employees. If a claim or suit is caused by or results from the concurrent negligence of the Consultant's agents or employees and the City, its agents, officers and employees, this indemnity provision shall be valid and enforceable to the extent of the negligence of the Consultant, its agents or employees. The Consultant specifically assumes liability and agrees to defend, indemnify, and hold the City harmless for actions brought by the Consultant's own employees against the City and, solely for the purpose of this indemnification and defense, the Consultant specifically waives any immunity under the Washington State industrial insurance law, or Title 51 RCW. The Consultant recognizes that this waiver was specifically entered into pursuant to the provisions of RCW 4.24.115 and was the subject of mutual negotiation. The indemnity and agreement to defend and hold the City harmless provided for in this section shall survive any termination or expiration of this agreement.

## **11. INSURANCE.**

During the period of the Agreement, the Consultant shall maintain in force at its own expense, each insurance noted below with companies or through sources approved by the State Insurance Commissioner pursuant to RCW Title 48;

A. Worker's Compensation Insurance in compliance with RCW 51.12.020, which requires subject employers to provide workers' compensation coverage for all their subject workers and Employer's Liability Insurance in the amount of \$1,000,000;

B. General Liability Insurance on an occurrence basis, with a combined single limit of not less than \$1,000,000 each occurrence for bodily injury and property damage. It shall include contractual liability coverage for the indemnity provided under this agreement. It shall provide that the City, its officers and employees are additional insureds but only with respect to the Consultant's services to be provided under this Agreement; and



C. Automobile Liability Insurance with a combined single limit, or the equivalent of not less than \$1,000,000 each accident for bodily injury and property damage, including coverage for owned, hired and non-owned vehicles.

D. Professional Liability Insurance with a combined single limit of not less than \$1,000,000 each claim, incident or occurrence. This is to cover damages caused by the error, omission, or negligent acts related to the professional services to be provided under this Agreement. The coverage must remain in effect for at least two (2) years after the Agreement is completed.

There shall be no cancellation, material change, reduction of limits or intent not to renew the insurance coverage(s) without forty-five (45) days written notice from the Consultant or its insurer(s) to the City. As evidence of the insurance coverage(s) required by this Agreement, the Consultant shall furnish acceptable Certificates Of Insurance (COI) to the City at the time it returns this signed Agreement. The certificate shall specify the City of Spokane as "Additional Insured" specifically for Consultant's services under this Agreement, as well as all of the parties who are additional insureds, and include applicable policy endorsements, the forty-five (45) day cancellation clause, and the deduction or retention level. The Consultant shall be financially responsible for all pertinent deductibles, self-insured retentions, and/or self-insurance.

## **12. DEBARMENT AND SUSPENSION.**

The Contractor has provided its certification that it is in compliance with and shall not contract with individuals or organizations which are debarred, suspended, or otherwise excluded from or ineligible from participation in Federal Assistance Programs under Executive Order 12549 and "Debarment and Suspension", codified at 29 CFR part 98.

## **13. AUDIT.**

Upon request, the Consultant shall permit the City and any other governmental agency ("Agency") involved in the funding of the Work to inspect and audit all pertinent books and records. This includes work of the Consultant, any subconsultant, or any other person or entity that performed connected or related Work. Such books and records shall be made available upon reasonable notice of a request by the City, including up to three (3) years after final payment or release of withheld amounts. Such inspection and audit shall occur in Spokane County, Washington, or other reasonable locations mutually agreed to by the parties. The Consultant shall permit the City to copy such books and records at its own expense. The Consultant shall ensure that inspection, audit and copying rights of the City is a condition of any subcontract, agreement or other arrangement under which any other persons or entity may perform Work under this Agreement.

## **14. INDEPENDENT CONSULTANT.**

A. The Consultant is an independent Consultant. This Agreement does not intend the Consultant to act as a City employee. The City has neither direct nor immediate control over the Consultant nor the right to control the manner or means by which the Consultant works. Neither the Consultant nor any Consultant employee shall be an employee of the City. This Agreement prohibits the Consultant to act as an agent or legal representative of the City. The Consultant is not granted express or implied rights or authority to assume or create any obligation or responsibility for or in the name of the City, or to bind the City. The City is not liable for or obligated to pay sick leave, vacation pay, or any other benefit of employment, nor to pay social security or other tax that may arise from employment. The Consultant shall



pay all income and other taxes as due. The Consultant may perform work for other parties; the City is not the exclusive user of the services that the Consultant provides.

- B. If the City needs the Consultant to Work on City premises and/or with City equipment, the City may provide the necessary premises and equipment. Such premises and equipment are exclusively for the Work and not to be used for any other purpose.
- C. If the Consultant works on the City premises using City equipment, the Consultant remains an independent Consultant and not a City employee. The Consultant will notify the City Project Manager if s/he or any other Workers are within ninety (90) days of a consecutive 36-month placement on City property. If the City determines using City premises or equipment is unnecessary to complete the Work, the Consultant will be required to work from its own office space or in the field. The City may negotiate a reduction in Consultant fees or charge a rental fee based on the actual costs to the City, for City premises or equipment.

#### **15. KEY PERSONS.**

The Consultant shall not transfer or reassign any individual designated in this Agreement as essential to the Work, nor shall those key persons, or employees of Consultant identified as to be involved in the Project Work be replaced, removed or withdrawn from the Work without the express written consent of the City, which shall not be unreasonably withheld. If any such individual leaves the Consultant's employment, the Consultant shall present to the City one or more individuals with greater or equal qualifications as a replacement, subject to the City's approval, which shall not be unreasonably withheld. The City's approval does not release the Consultant from its obligations under this Agreement.

#### **16. ASSIGNMENT AND SUBCONTRACTING.**

The Consultant shall not assign or subcontract its obligations under this Agreement without the City's written consent, which may be granted or withheld in the City's sole discretion. Any subcontract made by the Consultant shall incorporate by reference this Agreement, except as otherwise provided. The Consultant shall require that all subconsultants comply with the obligations and requirements of the subcontract. The City's consent to any assignment or subcontract does not release the consultant from liability or any obligation within this Agreement, whether before or after City consent, assignment or subcontract.

#### **17. CITY ETHICS CODE.**

- A. Consultant shall promptly notify the City in writing of any person expected to be a Consultant Worker (including any Consultant employee, subconsultant, principal, or owner) and was a former City officer or employee within the past twelve (12) months.
- B. Consultant shall ensure compliance with the City Ethics Code by any Consultant Worker when the Work or matter related to the Work is performed by a Consultant Worker who has been a City officer or employee within the past two (2) years.
- C. Consultant shall not directly or indirectly offer anything of value (such as retainers, loans, entertainment, favors, gifts, tickets, trips, favors, bonuses, donations, special discounts, work or meals) to any City employee, volunteer or official that is intended, or may appear to a reasonable person to be intended, to obtain or give special consideration to the Consultant. Promotional items worth less than \$25 may be distributed by the Consultant to a City employee if the Consultant uses the items as routine and standard promotional materials. Any violation of this provision may cause termination of this Agreement. Nothing in this Agreement prohibits donations to campaigns for election to City office, so long as the



donation is disclosed as required by the election campaign disclosure laws of the City and of the State.

#### **18. NO CONFLICT OF INTEREST.**

Consultant confirms that the Consultant or workers have no business interest or a close family relationship with any City officer or employee who was or will be involved in the consultant selection, negotiation, drafting, signing, administration or evaluation of the Consultant's work. As used in this Section, the term Consultant includes any worker of the Consultant who was, is, or will be, involved in negotiation, drafting, signing, administration or performance of the Agreement. The term "close family relationship" refers to: spouse or domestic partner, any dependent parent, parent-in-law, child, son-in-law, daughter-in-law; or any parent, parent in-law, sibling, uncle, aunt, cousin, niece or nephew residing in the household of a City officer or employee described above.

#### **19. ERRORS AND OMISSIONS, CORRECTIONS.**

Consultant is responsible for professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on the behalf of the Consultant under this Agreement in the delivery of a final work product. The standard of care applicable to Consultant's services will be the degree of skill and diligence normally employed by professional engineers or Consultants performing the same or similar services at the time said services are performed. The Final Work Product is defined as a stamped, signed work product. Consultant, without additional compensation, shall correct or revise errors or mistakes in designs, drawings, specifications, and/or other consultant services immediately upon notification by the City. The obligation provided for in this Section regarding acts or omissions resulting from this Agreement survives Agreement termination or expiration.

#### **20. INTELLECTUAL PROPERTY RIGHTS.**

- A. Copyrights. The Consultant shall retain the copyright (including the right of reuse) to all materials and documents prepared by the Consultant for the Work, whether or not the Work is completed. The Consultant grants to the City a non-exclusive, irrevocable, unlimited, royalty-free license to use copy and distribute every document and all the materials prepared by the Consultant for the City under this Agreement. If requested by the City, a copy of all drawings, prints, plans, field notes, reports, documents, files, input materials, output materials, the media upon which they are located (including cards, tapes, discs, and other storage facilities), software program or packages (including source code or codes, object codes, upgrades, revisions, modifications, and any related materials) and/or any other related documents or materials developed solely for and paid for by the City to perform the Work, shall be promptly delivered to the City.
- B. Patents: The Consultant assigns to the City all rights in any invention, improvement, or discovery, with all related information, including but not limited to designs, specifications, data, patent rights and findings developed with the performance of the Agreement or any subcontract. Notwithstanding the above, the Consultant does not convey to the City, nor does the City obtain, any right to any document or material utilized by the Consultant created or produced separate from the Agreement or was pre-existing material (not already owned by the City), provided that the Consultant has identified in writing such material as pre-existing prior to commencement of the Work. If pre-existing materials are incorporated in the work, the Consultant grants the City an irrevocable, non-exclusive right and/or license to use, execute, reproduce, display and transfer the pre-existing material, but only as an inseparable part of the work.



- C. The City may make and retain copies of such documents for its information and reference with their use on the project. The Consultant does not represent or warrant that such documents are suitable for reuse by the City or others, on extensions of the project or on any other project, and the City releases the Consultant from liability for any unauthorized reuse of such documents.

## **21. CONFIDENTIALITY.**

Under Washington State Law RCW Chapter 42.56) all materials received or created by the City of Spokane are **public records** which are subject to review and copying pursuant to a public records request. These records include but are not limited to bid or proposal submittals, agreement documents, contract work product, and other bid material. Some records or portions of records may be legally exempt from disclosure and can be redacted or withheld. RCW Ch. 42.56 describes those exemptions. Consultant must familiarize themselves with state law and the City of Spokane's process for managing records.

The City will endeavor to redact anything that clearly should be redacted under the law. For example, the City will generally redact Social Security Numbers, tax records, and financial account numbers before records are made available to a requestor. Consultant may identify any materials Consultant believes to be not subject to release under the Public Records Act. City will not be bound by Consultant's determination of whether any particular record or records are legally exempt from release under the Public Records Act.

If the City receives a public records request for records involving Consultant or Consultant's work product, City will release the records unless City determines that there are obvious exemptions or redactions (which City will make prior to release of the records). If City determines that there are exemptions that can be asserted only by Consultant, City will endeavor to notify Consultant and Consultant will be given ten days to obtain a Court order preventing the City from releasing the requested records. **If no Court order is procured by Consultant, the City will release the requested records.**

## **22. DISPUTES.**

Any dispute or misunderstanding that may arise under this Agreement, concerning the Consultant's performance, shall first be through negotiations, if possible, between the Consultant's Project Manager and the City's Project Manager. It shall be referred to the Director and the Consultant's senior executive(s). If such officials do not agree upon a decision within a reasonable period of time, either party may decline or discontinue such discussions and may then pursue the legal means to resolve such disputes, including but not limited to mediation, arbitration and/or alternative dispute resolution processes. Nothing in this dispute process shall mitigate the rights of the City to terminate the Agreement. Notwithstanding all of the above, if the City believes in good faith that some portion of the Work has not been completed satisfactorily, the City may require the Consultant to correct such work prior to the City payment. The City will provide to the Consultant an explanation of the concern and the remedy that the City expects. The City may withhold from any payment otherwise due, an amount that the City in good faith finds to be under dispute, or if the Consultant provides no sufficient remedy, the City may retain the amount equal to the cost to the City for otherwise correcting or remedying the work not properly completed. Waiver of any of these rights is not deemed a future waiver of any such right or remedy available at law, contract or equity.



### **23. TERMINATION.**

- A. For Cause: The City or Consultant may terminate the Agreement if the other party is in material breach of this Agreement, and such breach has not been corrected to the other party's reasonable satisfaction in a timely manner. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- B. For Reasons Beyond Control of Parties: Either party may terminate this Agreement without recourse by the other where performance is rendered impossible or impracticable for reasons beyond such party's reasonable control, such as, but not limited to, an act of nature, war or warlike operation, civil commotion, riot, labor dispute including strike, walkout or lockout, except labor disputes involving the Consultant's own employees, sabotage, or superior governmental regulation or control. Notice of termination under this Section shall be given by the party terminating this Agreement to the other, not fewer than thirty (30) business days prior to the effective date of termination.
- C. For Convenience: Either party may terminate this Agreement without cause, upon thirty (30) days written notice to the other party.
- D. Actions upon Termination: if termination occurs not the fault of the Consultant, the Consultant shall be paid for the services properly performed prior to the actual termination date, with any reimbursable expenses then due, but such compensation shall not exceed the maximum compensation to be paid under the Agreement. The Consultant agrees this payment shall fully and adequately compensate the Consultant and all subconsultants for all profits, costs, expenses, losses, liabilities, damages, taxes and charges of any kind (whether foreseen or unforeseen) attributable to the termination of this Agreement.
- E. Upon termination, the Consultant shall provide the City with the most current design documents, contract documents, writings and other products the Consultant has produced to termination, along with copies of all project-related correspondence and similar items. The City shall have the same rights to use these materials as if termination had not occurred; provided however, that the City shall indemnify and hold the Consultant harmless from any claims, losses, or damages to the extent caused by modifications made by the City to the Consultant's work product.

### **24. EXPANSION FOR NEW WORK.**

This Agreement scope may be expanded for new work. Any expansion for New Work (work not specified within the original Scope of Work Section of this Agreement, and/or not specified in the original RFP as intended work for the Agreement) must comply with all the following limitations and requirements: (a) the New Work is not reasonable to solicit separately; (b) the New Work is for reasonable purpose; (c) the New Work was not reasonably known either the City or Consultant at time of contract or else was mentioned as a possibility in the solicitation (such as future phases of work, or a change in law); (d) the New Work is not significant enough to be reasonably regarded as an independent body of work; (e) the New Work would not have attracted a different field of competition; and (f) the change does not vary the essential identified or main purposes of the Agreement. The City may make exceptions for immaterial changes, emergency or sole source conditions, or other situations required in City opinion. Certain changes are not New Work subject to these limitations, such as additional phases of Work anticipated at the time of solicitation, time extensions, Work Orders issued on an On-Call contract, and similar. New Work must be mutually agreed and issued by the City through written Addenda. New Work performed before an authorizing Amendment may not be eligible for payment.



## **25. MISCELLANEOUS PROVISIONS.**

- A. Amendments: No modification of this Agreement shall be effective unless in writing and signed by an authorized representative of each of the parties hereto.
- B. Binding Agreement: This Agreement shall not be binding until signed by both parties. The provisions, covenants and conditions in this Agreement shall bind the parties, their legal heirs, representatives, successors and assigns.
- C. Americans with Disabilities Act (ADA): Specific attention by the designer is required in association with the Americans with Disabilities Act (ADA) 42 U.S.C. 12101-12213 and 47 U.S.C. 225 and 611, its requirements, regulations, standards and guidelines, which were updated in 2010 and are effective and mandatory for all State and local government facilities and places of public accommodation for construction projects including alteration of existing facilities, as of March 15, 2012. The City advises that the requirements for accessibility under the ADA, may contain provisions that differ substantively from accessibility provisions in applicable State and City codes, and if the provisions of the ADA impose a greater or equal protection for the rights of individuals with disabilities or individuals associated with them than the adopted local codes, the ADA prevail unless approval for an exception is obtained by a formal documented process. Where local codes provide exceptions from accessibility requirements that differ from the ADA Standards; such exceptions may not be permitted for publicly owned facilities subject to Title II requirements unless the same exception exists in the Title II regulations. It is the responsibility of the designer to determine the code provisions.
- D. The Consultant, at no expense to the City, shall comply with all laws of the United States and Washington, the Charter and ordinances of the City of Spokane; and rules, regulations, orders and directives of their administrative agencies and officers. Without limiting the generality of this paragraph, the Consultant shall comply with the requirements of this Section.
- E. This Agreement shall be construed and interpreted under the laws of Washington. The venue of any action brought shall be in the Superior Court of Spokane County.
- F. Remedies Cumulative: Rights under this Agreement are cumulative and nonexclusive of any other remedy of law or in equity.
- G. Captions: The titles of sections or subsections are for convenience only and do not define or limit the contents.
- H. Severability: If any term or provision is determined by a court of competent jurisdiction to be invalid or unenforceable, the remainder of this Agreement shall not be affected, and each term and provision shall be valid and enforceable to the fullest extent permitted by law.
- I. Waiver: No covenant, term or condition or the breach shall be deemed waived, except by written consent of the party against whom the waiver is claimed, and any waiver of the breach of any covenant, term or condition shall not be deemed a waiver of any preceding or succeeding breach of the same or any other covenant, term of condition. Neither the acceptance by the City of any performance by the Consultant after the time the same shall have become due nor payment to the Consultant for any portion of the Work shall constitute a waiver by the City of the breach or default of any covenant, term or condition unless otherwise expressly agreed to by the City in writing.
- J. Additional Provisions: This Agreement may be modified by additional terms and conditions ("Special Conditions") which shall be attached to this Agreement as an Exhibit. The parties agree that the Special Conditions shall supplement the terms and conditions of the Agreement, and in the event of ambiguity or conflict with the terms and conditions of the Agreement, these Special Conditions shall govern.



- K. Entire Agreement: This document along with any exhibits and all attachments, and subsequently issued addenda, comprises the entire agreement between the City and the Consultant. If conflict occurs between contract documents and applicable laws, codes, ordinances or regulations, the most stringent or legally binding requirement shall govern and be considered a part of this contract to afford the City the maximum benefits.
- L. Negotiated Agreement: The parties acknowledge this is a negotiated agreement, that they have had this Agreement reviewed by their respective legal counsel, and that the terms and conditions of this Agreement are not to be construed against any party on the basis of such party's draftsmanship.
- M. No personal liability: No officer, agent or authorized employee of the City shall be personally responsible for any liability arising under this Agreement, whether expressed or implied, nor for any statement or representation made or in any connection with this Agreement.

IN WITNESS WHEREOF, in consideration of the terms, conditions and covenants contained, or attached and incorporated and made a part, the parties have executed this Agreement by having legally-binding representatives affix their signatures below.

**GEOENGINEERS, INC.**

**CITY OF SPOKANE**

By \_\_\_\_\_  
Signature                      Date

By \_\_\_\_\_  
Signature                      Date

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Type or Print Name

\_\_\_\_\_  
Title

\_\_\_\_\_  
Title

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

**Attachments:** Exhibit A – Certificate Regarding Debarment  
Exhibit B – Scope of Services

\_\_\_\_\_



## EXHIBIT A

### CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

1. The undersigned (i.e., signatory for the Subrecipient / Contractor / Consultant) certifies, to the best of its knowledge and belief, that it and its principals:
  - a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any federal department or agency;
  - b. Have not within a three-year period preceding this contract been convicted or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (federal, state, or local) transaction or contract under a public transaction; violation of federal or state antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, tax evasion, receiving stolen property, making false claims, or obstruction of justice;
  - c. Are not presently indicted or otherwise criminally or civilly charged by a government entity (federal, state, or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification; and,
  - d. Have not within a three-year period preceding this contract had one or more public transactions (federal, state, or local) terminated for cause or default.
2. The undersigned agrees by signing this contract that it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction.
3. The undersigned further agrees by signing this contract that it will include the following clause, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions:

#### Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – Lower Tier Covered Transactions

1. The lower tier contractor certified, by signing this contract that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any federal department or agency.
  2. Where the lower tier contractor is unable to certify to any of the statements in this contract, such contractor shall attach an explanation to this contract.
4. I understand that a false statement of this certification may be grounds for termination of the contract.

|                                                                      |                                     |
|----------------------------------------------------------------------|-------------------------------------|
| <hr/> Name of Subrecipient / Contractor / Consultant (Type or Print) | <hr/> Program Title (Type or Print) |
| <hr/> Name of Certifying Official (Type or Print)                    | <hr/> Signature                     |
| <hr/> Title of Certifying Official (Type or Print)                   | <hr/> Date (Type or Print)          |



## EXHIBIT B





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 11-10-20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** Engineering Services

**Approving Supervisor:** Kyle Twohig

**Amount of Proposed Expenditure:** \$800,000

**Funding Source:** local and state loan/grant

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

The proposed contract with GeoEngineers is an on-call contract to be used to pay for geotechnical engineering services on public works projects (water, sewer, sidewalk, etc.). Money paid to the consultant under this contract will generally be paid by state loan or grant funds or utility funds on a project by project basis.

**What are the impacts if expenses are deferred?**

Infrastructure upgrades would have to be deferred which, when projects are state funded, would potentially mean the loss of state funds.

**What alternative resources have been considered?**

None available.

**Description of the goods or service and any additional information?**

This expenditure is for hiring a consultant to provide specialized services which the City does not have in house. The work to be provided by the consultant and the associated fee are negotiated and authorized on a project by project basis.

**Person Submitting Form/Contact:** Dan Buller dbuller@spokanecity.org

**FINANCE SIGNATURE:**

Tonya Wallace  
Digitally signed by Tonya Wallace  
Date: 2020.11.11 15:10:17 -0800

**CITY ADMINISTRATOR SIGNATURE:**

Scott Simon  
Digitally signed by Scott  
Simmons  
Date: 2020.11.11 12:46:54  
-0800



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/8/2020

**Clerk's File #**

OPR 2020-0843

**Renews #****Submitting Dept**

HISTORIC PRESERVATION

**Cross Ref #****Contact Name/Phone**

MEGAN DUVALL 625-6543

**Project #****Contact E-Mail**

MDUVALL@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

Contract Item

**Requisition #****Agenda Item Name**

0470-WEBSTER BLDG NOMINATION TO THE SPOKANE REGISTER OF HISTORIC PLACES

**Agenda Wording**

Recommendation to list the Webster Building, 415 W Sprague Ave, on the Spokane Register of Historic Places.

**Summary (Background)**

SMC #17D.100.040 provides that the City/County Historic Landmark Commission can recommend to the City Council that certain properties be placed on the Spokane Register of Historic Places. The Webster Building has been found to meet the criteria set forth for such designation, and a management agreement has been signed by the owners.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Neutral \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

DUVALL, MEGAN

**Study Session\Other****Division Director**

DUVALL, MEGAN

**Council Sponsor****Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

PICCOLO, MIKE

mduvall@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

lcamporeale@spokanecity.org

**Additional Approvals**

sbishop@spokanecity.org

**Purchasing**



# Findings of Fact and Decision for Council Review

## Nomination to the Spokane Register of Historic Places

### Webster Building – 415 W Sprague Avenue

#### **FINDINGS OF FACT**

**1. SMC 17D.100.090: "Generally a building, structure, object, site, or district which is more than fifty years old may be designated an historic landmark or historic district if it has significant character, interest, or value as part of the development, heritage, or cultural characteristics of the city, county, state, or nation."**

- Originally built in 1893, the Webster Building meets the age criteria for listing on the Spokane Register of Historic Places.

**2. SMC 17D.100.090: The property must qualify under one or more categories for the Spokane Register (A, B, C, D).**

- Eligible under **Category A** as a contributing building to the East Downtown Historic District (NRHP 2003), the mixed use building houses storefronts on the main floor and single room occupancy on the second floor.
- The 1893 Webster Building is eligible under Category A because it was constructed during the frenzy to rebuild downtown Spokane after the devastating 1889 fire. Its construction, unfortunately, coincided with the economic Panic of 1893 and subsequent depression, one that left almost a five-year gap in Spokane's rebuilding, and changed the composition of building ownership in downtown Spokane. The Panic of 1893 slowed new construction which would not begin again until 1898 with the construction of the Peyton Building.
- The Webster Building is one of approximately fifteen brick buildings built in the immediate post-fire era (1889 to 1893) that remain extant in downtown Spokane. It thus remains as a significant physical manifestation of the pivotal period in the city's early development.
- The Webster Building is not being considered under Category C – Architecture due to multiple changes to the storefronts over the years, although the recently reconfigured storefront is considered appropriate as a modern interpretation of commercial vernacular architecture. Other changes to the building that preclude its listing under Category C are the new windows (although in the original openings and of the property material), the configuration of the second story which was originally single-room occupancy, and the loss of any original material at the storefront level of the building which is currently gutted.
- The Webster Building nomination first was brought to the Historic Preservation Office in January of 2020. The building had recently been painted a dark blue color and had white interior tiles affixed to two cast iron columns. At the time, the Nominations Committee felt that these changes to the exterior of the building adversely affected its integrity to the point of not being eligible for listing. Nomination categories and the focus of the nomination document draft were also questioned and suggestions made to remedy the areas of significance. The building has since been repainted a more historically appropriate color and the interior tiles have been removed from the exterior. Brick veneer replaced the inappropriate white tile and were painted to match the rest of the building.

**3. SMC17D.100.090: "The property must also possess integrity of location, design, materials, workmanship, and association." From NPS Bulletin 15: "Integrity is the ability of a property to convey its significance...it is not necessary for a property to retain all its historic physical features...the property must retain, however, the essential physical features that enable it to convey its historic identity."**

- The Webster Building retains fair architectural integrity in original location, design, materials, workmanship, and association. The building has had changes to the exterior that include a new, period



appropriate storefront, new metal clad wood windows in original openings, and a reconfiguration of the second story. Even with these changes, the building does possess enough essential physical features to convey its historic identity, especially since it is being considered solely under Category A for its significance to downtown Spokane as a relatively rare example of immediate post-fire construction.

**4. Once listed, this property will be eligible to apply for incentives, including:**

Special Valuation (property tax abatement), Spokane Register historical marker, and special code considerations.

**RECOMMENDATION**

---

The Spokane Historic Landmarks Commission evaluated the Webster Building according to the appropriate criteria at a public hearing on 10/21/20 and recommends that the Webster Building be listed on the Spokane Register of Historic Places.



After Recording Return to:  
Clerk of the Board  
Spokane County Commissioner's Office  
1116 W. Broadway, Room 100  
Spokane, WA 99260

***NOTICE OF MANAGEMENT AGREEMENT***

*NOTICE IS HEREBY GIVEN that the property legally described as:*

RAILROAD ADD E1/2 OF N80FT L2;N80FT OF L3 B10

*Parcel Number(s) 35191.2102, is governed by a Management Agreement between the City of Spokane and the Owner(s), B&H Enterprises, LLC Number 1, of the subject property.*

*The Management Agreement is intended to constitute a covenant that runs with the land and is entered into pursuant to Spokane Municipal Code Chapter 6.05. The Management Agreement requires the Owner of the property to abide by the "Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings" (36 CFR Part 67) and other standards promulgated by the Historic Landmarks Commission.*

*Said Management Agreement was approved by the Spokane City Council on \_\_\_\_\_. I certify that the original Management Agreement is on file in the Office of the City Clerk under File No. \_\_\_\_\_.*

*I certify that the above is true and correct.*

*Spokane City Clerk*

*Historic Preservation Officer*

\_\_\_\_\_  
*Dated:* \_\_\_\_\_

\_\_\_\_\_  
*Dated:* \_\_\_\_\_



### **MANAGEMENT AGREEMENT**

The Management Agreement is entered into this **21st** day of **October 2020**, by and between the City of Spokane (hereinafter "City"), acting through its Historic Landmarks Commission ("Commission"), and **B&H Enterprises, LLC Number 1** (hereinafter "Owner(s)"), the owner of the property located at **415 West Sprague Avenue** commonly known as the **Webster Building** in the City of Spokane.

WHEREAS, the City of Spokane has enacted Chapter 4.35 of the Spokane Municipal Code (SMC) and Spokane has enacted Chapter 1.48 of the Spokane County Code (SCC), both regarding the establishment of the Historic Landmarks Commission with specific duties to recognize, protect, enhance and preserve those buildings, districts, objects, sites and structures which serve as visible reminders of the historical, archaeological, architectural, educational and cultural heritage of the city and county is a public necessity and.

WHEREAS, both Ch. 17D.100 SMC and Ch. 1.48 SCC provide that the City/County Historic Landmarks Commission (hereinafter "Commission") is responsible for the stewardship of historic and architecturally significant properties in the City of Spokane and Spokane County; and

WHEREAS, the City has authority to contract with property owners to assure that any owner who directly benefits by action taken pursuant to City ordinance will bind her/his benefited property to mutually agreeable management standards assuring the property will retain those characteristics which make it architecturally or historically significant;

NOW THEREFORE, -- the City and the Owner(s), for mutual consideration hereby agree to the following covenants and conditions:

1. CONSIDERATION. The City agrees to designate the Owner's property an Historic Landmark on the Spokane Register of Historic Places, with all the rights, duties, and privileges attendant thereto. In return, the Owner(s) agrees to abide by the below referenced Management Standards for his/her property.

2. COVENANT. This Agreement shall be filed as a public record. The parties intend this Agreement to constitute a covenant that runs with the land, and that the land is bound by this Agreement. Owner intends his/her successors and assigns to be bound by this instrument. This covenant benefits and burdens the property of both parties.



3. ALTERATION OR EXTINGUISHMENT. The covenant and servitude and all attendant rights and obligations created by this Agreement may be altered or extinguished by mutual agreement of the parties or their successors or assigns. In the event Owner(s) fails to comply with the Management Standards or any City ordinances governing historic landmarks, the Commission may revoke, after notice and an opportunity for a hearing, this Agreement.

4. PROMISE OF OWNERS. The Owner(s) agrees to and promises to fulfill the following Management Standards for his/her property which is the subject of the Agreement. Owner intends to bind his/her land and all successors and assigns. The Management Standards are: "THE SECRETARY OF THE INTERIOR'S STANDARDS FOR REHABILITATION AND GUIDELINES FOR REHABILITATING HISTORIC BUILDINGS (36 CFR Part 67)." Compliance with the Management Standards shall be monitored by the Historic Landmarks Commission.

5. HISTORIC LANDMARKS COMMISSION. The Owner(s) must first obtain from the Commission a "Certificate of Appropriateness" for any action which would affect any of the following:

- (A) demolition;
- (B) relocation;
- (C) change in use;
- (D) any work that affects the exterior appearance of the historic landmark; or
- (E) any work affecting items described in Exhibit A.

6. In the case of an application for a "Certificate of Appropriateness" for the demolition of a landmark, the Owner(s) agrees to meet with the Commission to seek alternatives to demolition. These negotiations may last no longer than forty-five (45) days. If no alternative is found within that time, the Commission may take up to forty-five (45) additional days to attempt to develop alternatives, and/or to arrange for the salvage of architectural artifacts and structural recording. Additional and supplemental provisions are found in City ordinances governing historic landmarks.



This Agreement is entered into the year and date first above written.

\_\_\_\_\_  
Owner

\_\_\_\_\_  
Owner

**CITY OF SPOKANE**

**HISTORIC PRESERVATION OFFICER**

**MAYOR**

\_\_\_\_\_  
Megan M.K. Duvall

\_\_\_\_\_  
Nadine Woodward

**ATTEST:**

\_\_\_\_\_  
City Clerk

**Approved as to form:**

\_\_\_\_\_  
Assistant City Attorney



STATE OF \_\_\_\_\_ )  
 ) ss.  
County of \_\_\_\_\_ )

On this \_\_\_\_\_ day of \_\_\_\_\_, 2020, before me, the undersigned, a Notary Public in and for the State of \_\_\_\_\_, personally appeared \_\_\_\_\_, to me known to be the individual(s) described in and who executed the within and foregoing instrument, and acknowledged that \_\_\_\_\_ (he/she/they) signed the same as \_\_\_\_\_ (his/her/their) free and voluntary act and deed, for the uses and purposes therein mentioned.

IN WITNESS WHEREOF, I have hereunto set my hand and official seal this \_\_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Notary Public in and for the State  
of \_\_\_\_\_, residing at \_\_\_\_\_  
My commission expires \_\_\_\_\_

STATE OF WASHINGTON )  
 ) ss.  
County of Spokane )

On this \_\_\_\_\_ day of \_\_\_\_\_, 2020, before me, the undersigned, a Notary Public in and for the State of Washington, personally appeared NADINE WOODWARD, MAYOR and TERRI L. PFISTER, to me known to be the Mayor and the City Clerk, respectively, of the CITY OF SPOKANE, the municipal corporation that executed the within and foregoing instrument, and acknowledged the said instrument to be the free and voluntary act and deed of said municipal corporation, for the uses and purposes therein mentioned, and on oath stated that they were authorized to execute said instrument and that the seal affixed is the corporate seal of said corporation.

IN WITNESS WHEREOF, I have hereunto set my hand and official seal this \_\_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Notary Public in and for the State  
of Washington, residing at Spokane  
My commission expires \_\_\_\_\_



## **Attachment A**



## **Secretary of The Interior's Standards**

**1.** A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.

**2.** The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

**3.** Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.

**4.** Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.

**5.** Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.

**6.** Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color,

texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

**7.** Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.

**8.** Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.

**9.** New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

**10.** New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.



# Spokane Register of Historic Places Nomination

*Spokane City/County Historic Preservation Office, City Hall, Third Floor  
808 Spokane Falls Boulevard, Spokane, Washington 99201-3337*

## 1. Name of Property

Historic Name: Webster Building  
And/Or Common Name: Stowell Drug & Assay

## 2. Location

Street & Number: 415 W. Sprague Avenue  
City, State, Zip Code: Spokane, WA 99201  
Parcel Number: 35191.2102

## 3. Classification

| Category                                     | Ownership                                                     | Status                                               | Present Use                                    |                                                 |
|----------------------------------------------|---------------------------------------------------------------|------------------------------------------------------|------------------------------------------------|-------------------------------------------------|
| <input checked="" type="checkbox"/> building | <input type="checkbox"/> public <input type="checkbox"/> both | <input type="checkbox"/> occupied                    | <input type="checkbox"/> agricultural          | <input type="checkbox"/> museum                 |
| <input type="checkbox"/> site                | <input checked="" type="checkbox"/> private                   | <input checked="" type="checkbox"/> work in progress | <input checked="" type="checkbox"/> commercial | <input type="checkbox"/> park                   |
| <input type="checkbox"/> structure           |                                                               |                                                      | <input type="checkbox"/> educational           | <input checked="" type="checkbox"/> residential |
| <input type="checkbox"/> object              | <b>Public Acquisition</b>                                     | <b>Accessible</b>                                    | <input type="checkbox"/> entertainment         | <input type="checkbox"/> religious              |
|                                              | <input type="checkbox"/> in process                           | <input checked="" type="checkbox"/> yes, restricted  | <input type="checkbox"/> government            | <input type="checkbox"/> scientific             |
|                                              | <input type="checkbox"/> being considered                     | <input type="checkbox"/> yes, unrestricted           | <input type="checkbox"/> industrial            | <input type="checkbox"/> transportation         |
|                                              |                                                               | <input type="checkbox"/> no                          | <input type="checkbox"/> military              | <input type="checkbox"/> other                  |

## 4. Owner of Property

Name: B & H Enterprises, LLC Number 1  
Street & Number: 1420 E. Overbluff  
City, State, Zip Code: Spokane, WA 99203  
Telephone Number/E-mail: 509-981-3358; jheath@Watrust.com

## 5. Location of Legal Description

|                               |                           |
|-------------------------------|---------------------------|
| Courthouse, Registry of Deeds | Spokane County Courthouse |
| Street Number:                | 1116 West Broadway        |
| City, State, Zip Code:        | Spokane, WA 99260         |
| County:                       | Spokane                   |

## 6. Representation in Existing Surveys

Title: East Downtown Historic District National Register Nomination  
Date: 2003 ☒ Federal ☐ State ☐ County ☐ Local  
Depository for Survey Records: Spokane Historic Preservation Office



## 7. Description

### Architectural Classification

### Condition

- ☐ excellent  
☒ good  
☐ fair  
☐ deteriorated  
☐ ruins  
☐ unexposed

### Check One

- ☐ unaltered  
☒ altered

### Check One

- ☒ original site  
☐ moved & date \_\_\_\_\_

*Narrative statement of description is found on one or more continuation sheets.*

## 8. Spokane Register Criteria and Statement of Significance

**Applicable Spokane Register of Historic Places criteria: Mark "x" on one or more for the categories that qualify the property for the Spokane Register listing:**

- ☒ A Property is associated with events that have made a significant contribution to the broad patterns of Spokane history.
- ☐ B Property is associated with the lives of persons significant in our past.
- ☐ C Property embodies the distinctive characteristics of a type, period, or method of construction, or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- ☐ D Property has yielded, or is likely to yield, information important in prehistory history.

*Narrative statement of significance is found on one or more continuation sheets.*

## 9. Major Bibliographical References

*Bibliography is found on one or more continuation sheets.*

## 10. Geographical Data

Acreage of Property: less than 1

Verbal Boundary Description: RAILROAD ADD E1/2 OF N 80 FT L2; N 80 FT OF L3 B10

Verbal Boundary Justification: Nominated property includes entire parcel and urban legal description.

## 11. Form Prepared By

Name and Title: Jim Kolva  
Organization: Jim Kolva Associates, LLC  
Street, City, State, Zip Code: 115 South Adams Street, Suite 1  
Telephone Number: 509-458-5517  
E-mail Address: jim@jimkolvaassociates.com  
Date Final Nomination Heard:

## 12. Additional Documentation

*Additional documentation is found on one or more continuation sheets.*



**13. Signature of Owner(s)**

\_\_\_\_\_  
\_\_\_\_\_

**14. For Official Use Only:**

Date nomination application filed: \_\_\_\_\_

Date of Landmarks Commission Hearing: \_\_\_\_\_

Landmarks Commission decision: \_\_\_\_\_

Date of City Council/Board of County Commissioners' hearing: \_\_\_\_\_

**I hereby certify that this property has been listed in the Spokane Register of Historic Places based upon the action of either the City Council or the Board of County Commissioners as set forth above.**

\_\_\_\_\_  
**Megan Duvall**  
**City/County Historic Preservation Officer**  
City/County Historic Preservation Office  
Third Floor – City Hall  
808 W. Spokane Falls Blvd.  
Spokane, WA 99201

\_\_\_\_\_  
**Date**

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney



### SUMMARY STATEMENT

Rising from a brick and concrete foundation, the two-story Webster Building is an unreinforced brick building. Built in 1893 with stores on the ground floor and a single room occupancy hotel above, and remodeled in 2018-19, the building is midblock along the south side of Sprague Avenue between Washington Street and Stevens Street. The front façade is symmetrically-arranged, divided into four equally-spaced commercial bays, two bays on each side of a centered single-door entry that provides access to the second floor. On the second floor, over each of the commercial bays are three equally-spaced window bays. Square brick piers divide the bays and define each corner. The narrow flat-ached window bays are topped with voussoired brick soldiers. Double brick string courses above the first floor transoms, at the second floor sill line, and above the brick arches provide horizontal elements to the façade. Detailing is provided by brick corbel dentils on the cornice terminating the parapet wall of the flat-roof building.

### DESCRIPTION OF PROPERTY – CURRENT CONDITION AND APPEARANCE

The two-story front façade faces north along Sprague Avenue. The Webster Building is flanked by an asphalt parking lot to the east and the six-story City Ramp Parking Garage (1928, SRHP, NRHD) to the west. A single-story commercial bay is between the two buildings. Historic Fire Station #1 (1890, NRHD) is to the rear (south) of the Webster Building fronting on First Avenue. The building is 75-feet wide and 65 feet in depth on a lot of 75 feet by 80 feet.

Constructed of painted red brick in common bond, the building is relatively plain, symmetrically-arranged, and divided into four commercial bays separated by square brick piers. The brick piers flanking the center second floor entry and the piers at the corners extend from a corbeled base on the sidewalk grade. The two original piers dividing each half of the façade are a composite of original brick piers that extend between the juncture of the first and second floors and the cornice; and brick-clad cast iron posts that support the second story piers (cast iron posts wrapped with brick in 2020 renovation). These piers, the centered entry bay to the second floor, and the second-floor string courses, cornice, and window bays represent the original configuration of the building. The storefronts have been altered significantly over the years but remain divided into four commercial bays.

The existing building has recently been remodeled and retains four basic divisions. The easterly bay is divided into four bay segments, a pedestrian door in the east corner, and three equally-divided storefront bays. Four fixed-glass panel transom windows are above and correspond to the storefront pattern. A 12-inch wood beam extending across the façade separates the storefront and transom windows (beam was required for structural support). A low bulkhead wall with recessed panels corresponding to the storefront pattern rests on a concrete foundation and runs along the entire façade. Flat wood mullions divide the window panels. The second bay is divided into three bay segments, two windows and one entry in the westernmost corner. The transoms are likewise divided into three segments corresponding to the storefront. The entry to the second floor is a recessed and single-door. The third bay is divided into four storefront segments with four transom windows above. The fourth, and western-most bay, is also divided into four storefront segments, three windows and one entry in the west corner.

### **East and West Façades**

Adjacent to a one-story commercial bay of the six-story City Ramp Parking Garage, the west façade is against the party wall of the building to the west.



Likewise, the blank east façade is a flat brick wall without detail aside from at least four embedded window arches from windows that have been filled in with brick. Two large billboards cover a portion of the upper north half of the wall and looms over the rooftop. The parapet wall steps down from the front cornice to the rear (south) end. A white sheet metal coping covers the top of the wall.

### **Rear Façade**

The rear elevation opens south to what was originally an open court between the subject building and adjacent buildings to the south, east and west. The flat brick elevation is composed of two stories—the first floor is partially below grade and has been essentially removed, with only the bricked-in upper portions of the original segmental arch window openings remaining. The second story is intact with six window openings on each side of a chimney that extends above the flat featureless parapet wall. The segmental-arch window openings are formed by vertical brick soldier voussoirs and brick header sills. The windows are new, double-hung one-over-one wood sash.

A one-story concrete block addition is in the rear southwest corner and fills in the gap between the adjacent buildings (Fire Station #1 and City Ramp Garage). One door opening, with a flat metal slab door, is in the east wall.

### **Interior**

The first floor was completely gutted down to bare earth and a new concrete floor and foundation walls have been poured.

The second floor was gutted to the studs and sub floor and four new apartment units were constructed. The central stairway ascends to a vestibule from which the units, one in each quadrant are entered. Each unit has a kitchen, full bathroom and two bedrooms.

### ORIGINAL APPEARANCE & SUBSEQUENT MODIFICATIONS

Below are two historic 1928 images of the building from the Northwest Museum of Arts and Culture's Ferris Digital Image Collection (L87-1.36817-28 and L87-1.37981-28). The storefronts have been altered numerous times over the past one hundred years, including cladding two cast iron columns with brick. Likewise, the second floor window sash was originally double-hung one-over-one wood, was plywood for years, single-panel glass for several years, and is now back to an approximation of the original configuration. The building has been painted several colors over its life, including a dark blue, that has been recently been repainted a beige color.





## **SECTION 8: STATEMENT OF SIGNIFICANCE**

**Area of Significance:** A – Broad Patterns of Spokane History  
**Significant Dates:** 1893, 2019  
**Period of Significance:** 1893  
**Architect:** Unknown  
**Building Developer:** Edgar J. Webster  
**Building Contractor:** unknown

### **SUMMARY STATEMENT**

#### **Significant under Category A – Broad Patterns of Spokane History**

The 1893 Webster Building is eligible under Category A because it was constructed during the frenzy to rebuild downtown Spokane after the devastating 1889 fire. Its construction, unfortunately, coincided with the economic Panic of 1893 and subsequent depression, one that left almost a five-year gap in Spokane's rebuilding, and changed the composition of building ownership in downtown Spokane. The Panic of 1893 slowed new construction which would not begin again until 1898 with the construction of the Peyton Building. The Webster Building is one of approximately fifteen brick buildings built in the immediate post-fire era (1889 to 1893) that remain extant in downtown Spokane. It thus remains as a significant physical manifestation of the pivotal period in the city's early development.

Built as a single room occupancy hotel (SRO) in the commercial vernacular style, the Webster Building was a foundational property type in downtown Spokane during the decades flanking 1900. The Webster Building was one of many such hotels and apartment buildings that filled the downtown blocks on the east, south, and west of the business core. E. J. Webster, the building's developer, was a prominent figure in Spokane's early growth beginning in the early 1880s with real estate development and legal practice, and continuing in the post-fire period with new brick buildings during the post-fire period, including the Genesee Block.

Included as a contributing resource within the East Downtown Historic District boundary, the Webster Building meets the characteristics of the building types and uses included and described within that district and that nomination as cited below.

#### **Architecture**

The two-story brick building is a good example in the downtown of a mid-block vernacular commercial building with residences above, however is not being nominated under Category C for Architecture. With commercial use on the ground floor and apartments above, the building operated as a single room occupancy hotel (SRO) into the mid-1950s. Its basalt rubble and brick foundation, symmetrical brick façade, brick piers dividing commercial bays, voussoired flat-arched window bays, brick string courses, and corbeled cornice provide detailing characteristic of its era.

The window configuration on both the front and rear facades is somewhat unique in Spokane. Although narrow in comparison to other buildings of its type, the four bays of three windows each gave prominence to the pattern of the openings and allow an abundant supply of light to the residences within.



The building, however, has been altered over the years, like most downtown buildings, by an ever-changing storefront, especially over the past twenty years. The second floor configuration and openings are unaltered, and new double-hung wood sash windows have replaced the fixed single glass panels that had previously occupied the openings (having replaced plywood years before). In spite of the first floor alterations, the building retains the façade elements described in the East Downtown Historic District National Register Nomination. The building retains the character defining elements of its original construction: the brick structure and form, the four major ground floor bays divided by brick piers with each bay composed of bulkhead wall, storefront windows, and transom windows, the original centered second floor entry bay with original transom window, brick string course, and the original second floor window openings—three narrow openings within each pier division, brick string course, and terminating corbeled parapet cornice.

The Webster Building is included in the nomination (Site ID #: 079) with a classification of “Historic Contributing.” The building (as it appeared in 2003) and some of its uses are described in the nomination:

Piers divide the primary, northern elevation of this two-story, square brick building into four bays. The original, narrow, one over one, double hung wood windows openings on the second story are intact but boarded up. A brick stringcourse runs below the second story windows. The cornice is adorned with corbeled brick dentils. On the ground level, the original storefronts have seen extensive alterations. The building's piers define four distinct commercial spaces. The original red brick appears only on the most obscured rear façade of the building, where a single-story cinderblock addition has been made; the east façade is painted white, the north a tan color.

Cultural Data: Early city directories list this building as the Webster, with lodging space on the second floor and commercial space below. The upper floor is currently vacant, as it has occasionally been over the past decades. Commercial spaces on the ground floor have housed a variety of businesses, most notable has been Stowell Drug which occupied the space from 1912 to 1960. Sartori and Wolff, manufacturing jewelers, were early occupants in the eastern part of the building. Currently, this site houses a tavern, as it has, by and large, since the 1930s.

### **Historical Context**

The historical context for Spokane has been included in several National and Spokane Register nominations, including the East Downtown National Historic District (Woo, 2003) and National Historic Register multiple-property listings: Single Room Occupancy Hotels in the Central Business District of Spokane, WA 1900-1910 (Holstine, 1993); thus the Spokane historic context discussion is abbreviated.

The Spokane River and its falls had long been a gathering place for Native American tribes. It also attracted white settlers, J.J. Downing and family, and S.R. Scranton who established a claim at Spokane Falls in 1871. James N. Glover and Jasper Matheney soon followed and purchased the claims of 160 acres and the sawmill from Downing and Scranton. Early industry used the water power for milling and sawing lumber and to generate electrical power. The settlement grew slowly until the coming of the railroad.



The Northern Pacific Railroad arrived in Spokane Falls in 1881, the year of Spokane's incorporation, and with the connection of the eastern and western branches in 1883, transcontinental service through Spokane Falls was established. Spokane continued to grow as a regional shipping and distribution center through the 1880s. Between 1886 and 1889 the population increased from 3,500 to 20,000 people. Although suffering a set back by the fire of August 4, 1889, which destroyed approximately thirty-two blocks of the business district from the railroad tracks to the river and from Lincoln Street to Washington Street, the city quickly rebounded as new brick buildings rose after clearing of the ashes debris. The devastation wrought by the fire resulted in a city ordinance to reduce fire hazard, leading to brick and terra cotta becoming the dominant building materials of the rebuilt downtown.

### **East Downtown National Historic District**

According to the nomination (Woo, 2003), the period of significance for the East Downtown Historic District begins in 1890 with the construction of the Northern Pacific Depot and ends in 1953. Approximately two-thirds of the existing buildings in the district were built in the first decade of the twentieth century during Spokane's first significant era of economic and population growth. The nomination describes typical characteristics of the district's extant buildings:

The East Downtown Historic District is located on the fringe of downtown. Building heights range from one-story to eight stories, with most averaging two or three stories in the industrial area adjacent to and south of the tracks. Most of the buildings in the district are commercial vernacular in style and clad in brick. Foundations are stone, brick, or concrete. Typically, the ground floor is occupied by small businesses while the upper floors are used for offices or hotels/residential apartments.

The East Downtown Historic District contains many of the city's SROs, a specific type of working-class housing that was developed to house the itinerant workers who came to Spokane in great numbers. Mostly constructed between 1900 and 1910 to meet the housing demand the itinerant workers created, the SRO in Spokane was typically two to four stories in height with ground floor commercial/retail space and hotel rooms on the upper floors. The businesses on the ground floor catered to the residents of the hotel as well as the general population.

Buildings within the district varied in use but generally fell into the following categories: transportation; commercial; industrial; and single room occupancy hotels (SROs). The diversity of businesses allowed for a community to live and thrive in downtown. The Webster Building was similar to the typical SRO in that it had small, undefined rooms (simple rooms that could be used as living or bedrooms), some of which were combined by a common interior door, with no separate kitchen and a shared bathroom down each hall.

### **The Year 1890**

On August 5<sup>th</sup>, 1890, the Spokane Falls Review surveyed the phenomenal rebirth of the city by listing all the buildings that had been constructed in and near the downtown.

SIX MILLIONS ... A Grand Record of a Year's Growth ... Brick and Granite ...  
Over 150 Brick Buildings Since the Fire ... A Mighty Faith in Spokane ...  
Magnificent Blocks Cover All the Old Fire Ruins of a Year Ago



The Review today gives a complete record of the tangible results of the energy and confidence of the citizens of Spokane. It has been the aim of the Review in this building article to give a list of all the buildings that have been erected in the business portion of the city since the great fire of August 4, 1889. It is an anniversary edition, and the scope of this article has been confined exclusively to within the fire limits of a year ago, at the same time showing the natural growth of business beyond those boundaries.

...

Spokane has spent in round numbers six millions of dollars in business blocks since last August and is even now preparing for even greater expenditures in the same line.

In the face of so many buildings being thrown open at about the same time it is a noticeable fact, and a most encouraging sign of the times, that all of them are rapidly filling up with tenants. No sooner is a building declared ready for occupancy than the rooms and stores are immediately rented.

In a recapitulation of the investment touted in the August 5<sup>th</sup> article, the following table lists streets and expenditures on their frontages:

|                  |                   |
|------------------|-------------------|
| Riverside Avenue | \$2,607,000       |
| Main Street      | \$1,424,500       |
| Sprague Street   | \$ 816,000        |
| Howard Street    | \$ 402,400        |
| First Street     | \$ 264,000        |
| Front Street     | \$ 95,000         |
| Second Street    | \$ 93,500         |
| Other streets    | <u>\$ 181,000</u> |
| Total            | \$5,975,400       |

In a similar vein, writer Nelson Wayne Durham reported in his *History of The City of Spokane and Spokane County, Washington* (1912), the birth of a new Spokane: By January 1, 1891, he noted, "a new and imposing business district had taken the place of that leveled by flame. The census of June had given Spokane nearly 20,000 population, but that count embraced only people living within the old city limits, two miles square. Population with the annexed territory gave a true total of nearly 25,000, and by January 1 the newspapers claimed 28,000.

Durham also describe the city's economic recovery with a typical abundance of figures: "Real estate transfers for 1890 were totaled at \$18,000,000. The assessed valuation of city property was \$18,790,000. The flour mills had a daily capacity of 700 barrels, and the year's lumber cut was 30,000,000 feet. Eleven banks had \$5,000,000 on deposit. The year's post office receipts were \$52,705, as against \$19,612 in 1888. A census of manufacturing interests showed 223 concerns employing hands. The railroads that year had carried in and out of Spokane 257,500 tons of freight. The telephone system had 410 subscribers, and thirty four miles of street railway were in operations. The city had nine public school buildings, valued at \$425,000; forty-three teachers and 2,500 pupils. There were thirty churches. As revealing the growth of a decade, the Review contrasted these figures with the showing of 1880, when the town had a population of 350, an assessed



valuation of \$50,000, real estate transfers of \$25,000, a milling capacity of ten barrels daily, an annual lumber cut of 500,00 feet, and a payroll of twenty-five hands.

Although the Webster Building was not constructed until 1893, it is at the premature close of the post fire building boom that likely would have continued had it not been squelched by the Panic of 1893.

#### **Development of the Webster Building block**

The Webster Building is Block 10 of Railroad Addition to Spokane bounded by Sprague Avenue on the north, Washington Street on the east, First Avenue on the south and Stevens Street on the west. The block is 300 feet long (east-west), and narrow, approximately 150 feet wide, with no alley.

The 1888 Sanborn Fire Insurance Map indicates that the block was divided into four parcels all occupied by a wood frame dwellings and accessory sheds and outbuildings that fronted along Sprague Avenue. In addition, in the northeast corner, with a dwelling on the southeast corner, was a frame "Public School."

The 1889 Sanborn Map depicted the same pattern as in 1888.

In 1890, however, the site had been divided, with the same dwelling from 1888 on the subject site, and to the south, fronting along "E. 1st ST." was "Steam Fire Eng. Ho," a vacant shed, and a frame building labeled "Chinese." A three-story brick building was being built on the Sprague Avenue frontage with a dwelling and a "Chinese Laundry" on the southern half of the lot. The "Arlington New Stables" occupied the east end of the block.

The 1891 Sanborn depicts essentially the same building pattern as 1890 but a new brick building, the Erie Building, had been constructed on the northwest corner of the block. The dwelling that had been on the site remained for one more year.

In 1902, the subject site was occupied by the two-story brick Webster Building. A two-story brick block was adjacent to the west, and brick buildings, two-story in the middle and one-story on the southwest corner, filled the frontage along Stevens Street and wrapped the corner of First Avenue. A vacant lot was between the corner building and Fire Station No. 1. East of the Webster Building was a vacant lot with a three-story brick building in the northeastern corner of that lot. The New Arlington Stables remained at the east end.

By 1910, Sanborn shows that the block was entirely built out. Filling the gap between the corner building and Fire Station No. 1 was a one-story brick building. A one-story brick building also filled in the east side of the Webster building, abutting the three-story brick building in the northeastern corner of that lot. The New Arlington Stables had been replaced by a one-story brick building fronting on Sprague Avenue, and a one-story brick building fronting on Washington Street. A one-story brick building, fronting on First Avenue, filled in the remainder of the block except a narrow eight-foot lane along the east side of the Fire Station.

In 1928, the entire block was filled with brick buildings with the six-story concrete City Ramp Parking Garage (1928, SRHP, NRHP) anchoring and dominating the west end of the block. Fire Station No. 1 and the Webster Building occupied the remainder of the west half of the block. In the east half, brick buildings, single-story commercial buildings and a three-story "Lodgings"



filled in the block. Two clothes cleaning businesses, an office, a printing shop, and a creamery with testing room occupied those buildings.

**Career of E.J. Webster and Edgar Webster, businessman in 19<sup>th</sup> century Spokane**

Edgar J. Webster arrived in Spokane Falls in 1882 bringing his law practice from Oakland, California. He quickly invested in Spokane real estate, buying property in the burgeoning downtown and outlying properties in northeast and northwest quadrants of the city, properties that would become Fairmount Memorial Park (1888) and Minnehaha Park. He became involved in Spokane affairs -- school board meetings were held in his office building on Sprague, he was elected to the city freeholder committee that drafted the city charter, he was a founder and served as president and majority owner of Fairmont Cemetery, he owned the land out of which Minnehaha Park was formed, and was an owner and president of the Ross Park Electric Railway Line. He made a fortune in real estate and mining and was one of Spokane's early millionaires.

The June 9, 1883, *Spokesman-Review* displays an advertisement for "E. J. Webster, Attorney at Law, and Chas. A. Webster, Life and Accident Ins. Agent, and Webster Brothers Real Estate Agents, Spokane Falls, Washington Territory." On the same page and article captioned "Dinna Ye Hear the Slogan?" extolls Spokane's future as a solid and prosperous city and lists the buildings underway, including that of E. J. Webster: "E.J. Webster is building a new frame business house on his lot on the south side of Riverside avenue between Howard and Stevens streets. The structure will be one story, 30x40, with two rooms, one for a store and the other to be occupied by the firm of Webster Bros., as a law, insurance and real estate office."

The following February, *The Spokesman-Review* reported that a new law firm was joining the growing business ranks of Spokane: "One of the latest addition to our law firms is that of Webster & Miller. The senior partner is E. J. Webster, who has led an active life in our midst during the past year as an attorney and dealer in real estate, and who has done exceedingly well in all of his ventures. The new man is Eugene C. Miller, who stopped at the Falls on his way to Portland, which act insured to use a new and most desirable citizen. Mr. Miller is a member of the bar of Butler county, Pa., where he has practiced law since 1876. ..."

In March 1884, *The Spokesman-Review* reported that E. J. Webster was building a two-story addition to the rear of his business on Howard Street.

The mining business was also one of Mr. Webster's endeavors as reported in the *Spokane Falls Review* of February 24, 1886. The article included a list of some fifteen sales of mining property made by E. J. Webster in the past thirty days. On October 5<sup>th</sup>, the *Review* reported that Webster and Dr. Burch planned to build an immense three-story brick at the southeast corner of Riverside Avenue and Howard Street. Dr. Burch owned a large frontage on both streets, and Webster's property joined on Burch's east. "The building will be a magnificent structure, and with Carson's new building on the corner of Sprague and Howard, it will make that block almost solid."

E. J. Webster was one of Spokane's businessmen who took up the hammer in building Spokane's Exposition Building while the carpenters were on strike. As reported in the *Spokane Falls Review* on November 13, 1890:

A very remarkable sight was witnessed at the exposition building yesterday afternoon, and one that will probably never be paralleled and certainly never has been, even in this free and glorious Pacific Northwest



It was a scene long to be remembered. Gray-headed bankers in silk hats and Prince Albert coats, wealthy mine owner, prosperous merchants and retire capitalists, with hammer and saw were at work on the roof of the new exposition building, which must be ready for opening fourteen days hence.

The carpenters' union thinking that it had the ship hand, struck and refused to work.

The citizens of Spokane, many of whom had made their start in life at the carpenter's bench, at once went to the building and, amid the awed silence of the striking workmen, pulled off their coats and began to work.

E.J. Webster, the millionaire real estate owner, was the first to nail a shingle, and he worked for six long hours, and with F.B. Chandler, C.G. Betts, and the city engineer, Oskar Huber, finished over 800 square feet of work during the afternoon.

The *Spokane Chronicle* reported on February 20, 1891, that E.J. Webster had been granted a street railroad franchise by the county commissioners. He was authorized to construct and operate an electric road beginning at the east end of the Ross Park street railroad to Minnehaha Park. Webster was a stockholder and, at that time, president of the line. He also owned and had a summer home at Minnehaha Park, discovered a mineral water spring on the property, and turned the area into a spa (Rebstock, 2020).

On March 14, 1891, the *Spokane Chronicle* published final certificate for city charter for the City of Spokane Falls. E. J. Webster was among the esteemed group of fifteen freeholders who had framed the charter; others included David P. Jenkins. A. M. Cannon, H. E. Houghton, I. S. Kaufmann, Frank a Bettis, James N. Glover, and George H. Leonard.

The *Spokane Chronicle* reported in its October 28, 1891 edition that Webster had recently purchased the Tidball Block in downtown Spokane. He traded 145 acres of land, and \$750 cash and assumed a \$30,000 mortgage for the four-story brick building (Rand and Dow, 1891, razed).

On February 4, 1892, *The Spokane Review* reported the sale of the Ross Park streetcar line to Spokane Street Railway. "The Ross Park line, that has just been sold, was built about three years ago at a cost of \$200,000. It was the first electric road in the city. Taking it all in all, it was supposed to be the staunchest built road, as to mechanism, etc., to be found in the county. It was built and operated by G. B. Dennis, who was its president for the first two years. He was succeeded by E. J. Webster, who was president for one year, Mr. Webster in turn being replaced by H. N. Belt, its present president. The present stockholders of the road are H. N. Belt, E. J. Webster, and several others."

Demonstrating his "substantial" generosity, Mr. Webster gave a boost to Mr. J. J. Hill's Great Northern Railroad as it sought a route through Spokane. As reported by the *Spokane Chronicle* on February 12, 1892: "Mr. E.J. Webster was next called upon he said: 'So mild, rosy and acceptable a proposition was never received by the citizens of Spokane before from any railroad. I have agreed to give my Hill the right of way across my property, which measures over a mile, and a tier of lots on each side into the bargain, and I hope the property owners of the west side of the city will deal as generously by him when it comes their turn.' Mr. Webster's statement was vociferously applauded.



### **Construction of the Webster Building**

A news article in the May 1893 edition of the *Spokane Review* indicated that E. J. Webster and Eugene Miller were planning to erect a two-story brick on Sprague Avenue. (According to the Spokane County Assessor Field File, the Webster Building was constructed in 1890, but the East Downtown National Register District nomination provides a date of 1892.) Webster had purchased the property on which he would build his new building in February 1889. He invested a sum of \$20,000 for the premises, a frame dwelling on the north eighty feet of the east half of lot 2 and in the north eighty feet of lot 3, both in block 10 of Railroad Addition to Spokane. Three months later, on May 11<sup>th</sup>, Ida L. Webster conveyed her interest in that and several other properties to E.J. Webster, her husband. But June 20, 1890, "Judge Blake granted E. J. Webster a decree of divorce from his wife, Ida L. Webster." The 1889, 1890 and 1891 Sanborn Insurance maps shows a frame dwelling on the site.

Optimism glowed in the *Spokane Review*'s May 8, 1893 edition: "Architects and contractors predict a building boom this summer. Many of the former have all the work they can attend to, and the contractors are busy making estimates on a variety of small jobs. ... The building of business blocks has been delayed on account of the scarcity of brick. The news report listed the drivers of Spokane's growth: Dr. J.E. Gandy, F.M. Tull, C.F. Clough, Peter Costello. L. B. Whitten, James Holman, Millard Hartson, Eugene Hyde and Rollin Hyde were building new two and three-story brick business blocks along Sprague, Riverside, and Main. As stated: "E.J. Webster and Eugene Miller will build a two-story brick 75x75, two stories, on Sprague avenue, adjoining the Erie Block." (In the same month, by Warranty Deed, Edgar J. Webster (unmarried) conveyed to Eugene G. Miller one-half interest in the property). The article ended by observing that "The above list is only a fraction of the buildings in course of erection and to be erected this summer. It is safe to say that there will be business blocks and residences enough built in Spokane this year to take a good-sized town if put off by themselves, but Spokane is now so big, and is growing so rapidly, that the effect will hardly be noticeable."

May 1893 proved to be pivotal in the history of the Webster Building, it was constructed, but as it was being built, the U.S. economy was crumbling. In May 1893, a financial panic struck the nation. In the ensuing trans-Atlantic depression banks and businesses failed, credit froze, railroads went bankrupt, unemployment, and loss of homes, savings, and buildings devastated rich and poor alike. The financial implosion hit the stock market for the first time on May 3, 1893, when a massive sell-off caused share prices to tumble. The market crash accelerated, and the Panic of 1893 was in full bloom and would last to August of 1893. The ensuing depression would last several years. The severity and duration of the economic disaster would only be surpassed by the Great Depression of the 1930s.

Meanwhile in Spokane some forty new buildings had arisen in the commercial district following the devastation of 1889. And as reported above, as the economy was collapsing, new brick edifices were being erected and planned for downtown Spokane. Almost all of these buildings, including the new building of Webster and Miller, had been mortgaged to finance construction; more than thirty of these would be lost to foreclosure during the initial panic and the following depression. (Caldbeck, 2019)

Nine banks failed in Spokane, a significant percentage of municipal funds was held in banks that had suspended operations and was inaccessible, hundreds of families and individuals lost much or all of their savings, homes were foreclosed, unemployment soared, and the rich did not get richer. Indeed, many lost all, including pioneer banker Anthony M. Cannon (1839-1895) who



opened the first bank in Spokane in 1879. His Bank of Spokane Falls was the third in the state to fail, on June 6, 1893. According to historian John Caldbick, within 24 hours, two other Spokane banks that were financially entwined with Cannon shut their doors, and a third closed after a run by its depositors. Cannon died two years later in New York. James N. Glover (1837-1921), another of the city's most prominent developers, built an opulent 12,000-square-foot mansion in 1888. In July 1893 he lost much of his fortune when the city's First National Bank, of which he served as president, failed. The mortgage on his mansion was foreclosed, and Glover was forced to sell it two years later to satisfy his debts.

Concurrently, the Panic of 1893 had its impact on the Webster Building as the case with numerous other downtown Spokane buildings. As discussed by historian Holly George (2016) in telling of the failure of the Auditorium Theater, the economic impact was widespread.

The story began four years earlier, after the 1889 fire that destroyed so much of downtown Spokane. In a flurry of civic pride Spokanites declared that their city would arise from the fire bigger and better than before. And it did, at a price. Before 1889 Inland Northwesterners had dealt with a Dutch investment firm called the Northwestern and Pacific Hypotheek Bank. In order to rebuild, Spokane borrowed heavily from the Hypotheek Bank, which by the end of 1892 had loaned more than \$4.8 million on urban properties.

Then came the Panic of 1893, when a burst railroad bubble and a precipitous drop in the gold supply conspired to plunge the United States into depression.

Institutions and fortunes throughout the nation failed in the panic, a contagion from which Spokane was hardly immune. Indeed, as the distress spread to the Northwest, it soon became evident that many leading Spokanites had badly overextended themselves.

Two years after construction of the building, in May 1895, Webster conveyed his remaining interest by Quit Claim Deed to Eugene G. Miller.

On October 24, 1896, the foreclosure of Edgar Webster, Eugene G. Miller, Spokane Savings Bank, Fred B. Grinnell as Receiver of the Spokane Savings Bank, et al. was recorded in the Spokane County Deed books. *The Spokesman-Review* would report "Judge Moore has made an order, on petition of Blake and Post, attorneys for the Northwestern and Pacific Hypotheekbank, authorizing the latter to join Fred B. Grinnell, receiver of the Spokane Savings bank, in its suit against E.J. Webster, Eugene G. Miller and others for foreclosure for a mortgage (for \$17,000).

In June 1897, Webster offered to donate Minnehaha park to the city. According to the *Spokane Chronicle* (June 8<sup>th</sup>), Hypotheekbank held a \$6,500 mortgage on which the bank was willing "to throw off \$2,000 if the city will pay the balance."

On November 30, 1897 by Sheriff's Deed 18902 (C. C. Dempsey, Sheriff) Sheriff sale 21,792.60, the plaintiff, the Northwestern and Pacific Hypotheekbank (Northwestern and Pacific Mortgage Company) received title to the Webster building on the steps of the Spokane County Courthouse.



Hence, the history of the Webster Building property mirrored that of many others in downtown during the 1890s.

**R.L. Polk Directory Information – 415-417-417-1/2-419-421 West Sprague Avenue**

The first listing in the Spokane City Polk Directory (Polk) for the building, 415-423 Sprague Avenue was in 1895 for the W.D. Knight Co. bookbinder, and later through 1899, printers and binders. Hotel Webster was listed in 1905 at 415-1/2 Sprague, and in 1910 at 417-1/2 Sprague. The Hotel Vera appeared in 1915, the Hotel King in 1917 and 1918, and the Plains Hotel in 1920 through 1930, the Model Hotel in 1936, Dixon Hotel in 1940, and back to the Webster Hotel and Apartments in 1945, and Webster Apartments in 1956. The year 1956 was the last year that 417-1/2 was listed as being occupied. At times during this period, Polk listed the names of the tenants by room number. The 1945 Polk Directory listed fifteen residents in fifteen numbered units through the number 21. No units were listed under 4, 13, 15, or 18, 19, 20. It is likely that some of the units were combined. As building declined in the 1950s, the number of residents slipped to seven in 1955 and 1956, and some of those occupied two or more numbered units. In 1956, four units were listed as vacant, and eight numbers were combined into four units.

According to 1910 U.S. Census enumeration, 417-1/2 Sprague contained twelve lodgers listed in eleven households. One was a man and wife; one other was a woman. Occupations included: farm laborer, farmer (2), machinist, blacksmith, none, laborer (2), barber, and sales lady.

A sampling of ground floor businesses is included below. Bars, cafes, restaurant, and lounges occupied the 415 address from 1914 or before through 2018.

**415 West Sprague**

1895-96 – W.D. Knight Co., bookbinder at 415-423 Sprague  
1897-98 - W.D. Knight Co., bookbinder at 417 Sprague  
1897-98 - W.D. Knight Co., bookbinder  
1899-1900 - W.D. Knight Co., bookbinder at 415-421  
1913-1914 – Good Eat Café; electrical permit issued, 5/12/1913  
1915 – M & R Lunch  
1919 – 1926 White Front Café, electrical sign permit, 1/16/1926  
1930 - Blue Bird Cigar Store  
1935 – Senate Café and on 11/23/1935, the Senate Beer Parlor, electrical permits 1935-1937  
1938 – Senate Beer Parlor, building permit to alter store front  
1940 - Senate Beer Parlor  
1946 – 1965 - Senate Tavern, various owners, storefront permits & electrical permits, 1946-1954  
1966-1970 - GoGo Tavern  
1973 – Merchant's Tavern  
1974-Intrepred Fox Restaurant  
1975 – Robby's Tavern  
1979 – Little Hofbrauhaus  
1983 -1985– Mister 'Zs' Restaurant  
1990 – Cut Above Restaurant  
2000 – Our Place Restaurant  
2006 – address not listed



2010 – 2017 – Irv's (lounge)

2018 - Stray

**417 West Sprague**

1898 – Fremming & Gabriel home furnishing and furniture (415-417)

1930 – Brodie Sales Co. bottlers supplies

1935 – vacant

1940 – 1946 – address not listed

1950 - Chas. J. Lay Real Estate

1955 - vacant

1960 - 1961 – Gene's Pizza

1970 to present – address not listed

**417-1/2 West Sprague**

A variety of hotels listed vacant after 1905 through 1956 after which the second floor was vacant.

**419 West Sprague**

1890 -1891 – New York Tea Company

1900 – The Farnsworth-Herald Col

1903 – 1905 – A.C. Thompson for Pierce Bicycles and Automobiles (419-421)

1905 – Stutes Printing Company

1930 – Boundary Line Realty Co.

1935 – 1940 - Jas. E. Thompson Real Estate 1950 – 1960 Real Estate Companies

1956-46 – Amos Hill, watchmaker

1950 – 1955 – Ruell J. Burrill and Alfred Shaw, Real Estate

1961– 1965 – vacant

1966 - 1990 – Meyers Cleaners

2000 – not verified

2010 to present – not listed

**421 West Sprague**

1912 – Stowell Drug Company

1929 and 1938 and 1943, 1960 - – Stowell Drug Company

1960 – 1965 – vacant

1967 – 1990 Remington Rand, Lesco's Shaver Service, Northwest Shaver

2000 – Michael Ankney – Fusion Hair Styling salon

2006 to present – address not listed

**Edgar J. Webster, World Traveler**

Mr. Webster was a noted world traveler and *The Spokesman-Review* reported his adventures in the Australian region on February 23, 1905. In recounting a letter sent from Hobart, Tasmania to a Spokane friend, Webster told of his experiences and that he had visiting Porto Rico [sic], Hawaii, and parts of Asia. He wrote: "I am at the extreme end of my journey and can go no farther in this direction, so day after tomorrow I sail for southern New Zealand." He had been in and enjoyed Australia for a month and extolled its people and climate.

Mr. Webster continued his travels, and on June 13, 1919, *The Spokesman-Review*, declared: "The world's champion globe-trotter is a Spokane man." The article recounted the 71-year-old's experiences as a globe trotter and his close calls in faraway places -- in the Himalaya Mountains,



Persia, Russia, and the Balkans. He spoke of small countries with no flags and in which his passports were not good and, thus, he had not visited. "Anyway," he stated, "it would have taken an army to get into them, and there is no telling just how you would get out." But he had seen them from above, up in the Himalaya Mountains where he could look down on those countries. His next project was to add illustrative stories to the great many pictures he had taken. He still had twenty more years to travel until his death in 1939.

On May 24, 1931, Spokane welcomed home one of its most adventurous pioneers and told the story of E. J. Webster, Civil War veteran, lawyer, developer, and one of the city's "most widely traveled and picturesque citizens."

E.J. Webster passed away in Pasadena, California on May 8, 1939 and *The Spokesman-Review* published his death notice on May 11, noting his service in the Union Army, his engagement in the real estate, his investments in the growth of Spokane, and the broader impact of his civic involvement.

**Eugene G. Miller, partner in Webster Building**

Eugene Miller was born in 1853 in Shreveport, Louisiana and graduated at Centenary College. He studied law in Pennsylvania and became a lawyer there in 1877, he was admitted to the bar and practiced for about seven years. He arrived in Spokane, formed a partnership with E. J. Webster in February 1884, and was admitted to the bar in April 1884. After a year, he moved to Minneapolis where he practiced law until returning to Spokane in 1887. In July 1892, Miller became engaged in city politics when he was sworn in as a member of the board of city commissioners as Police Commissioner. Shortly thereafter, in November of 1892, the Honorable Eugene Miller resigned that position after he had been elected by Spokane voters as judge of the municipal court. After his service as a judge, Miller returned to his legal practice and ended his career by working for the Spokane Chronicle until 1926. He resided with his wife Bessie initially at 2006 First Avenue in Browne's Addition in 1900, then moved to the South Hill on Monroe and 24<sup>th</sup> where he was residing at his death. Eugene passed away as a widower in Spokane in 1938 at age 87, was cremated with his ashes scattered to the Montana winds.



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- . "Our Building Record." 10/18/1889.
- . "Building Progress. 12/27/1889.
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- . "Get Your Saw." 9/18/1890. P 1:3.
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- . "Story of a Year." 8/4/1890. p3. --.
- . "People's Ticket." 9/17/1890. P8:4.
- . "They Want to Sell." 116/1890. P5:2.
- . "New Street Railway Franchise. 2/20/1891. P5:2.
- . "Final Certificate." 3/14/1891. P7:7.
- . "Sale of Tidball Block. 10/28/1891. P8:2.
- . "Mr. Webster Gives Substantial Evidence of Generosity. 2/12/1892. P1:6.
- . "Fire Proof Buildings. 3/4/1892. P5.
- . "Weymouth Named." 6/8/1897. P1:7.

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- . "Mr. Miller Qualifies." 7/21/1892. P3:5-6.

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- . "Dinna Ye Hear the Slogan? 6/9/1883. P3:6.
- . "Commission Business." 8/18/1883. P3:4.
- . "New Firm." 2/16/1884. P3:4.
- . "E.J. Webster is building ..." 7/14/1883. P3:3.
- . "Mr. Miller Qualifies." 7/21/1892. P3:5-6.
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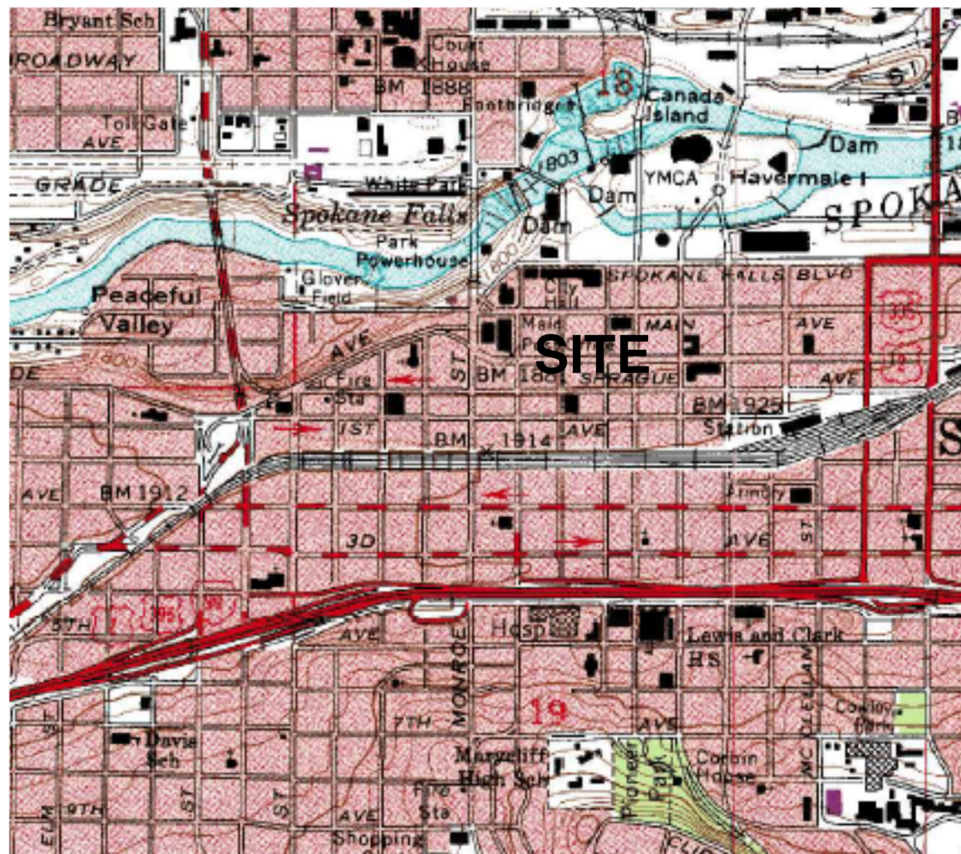
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## **Maps, Drawings, and Photographs**





USGS 7.5-Minute Quadrangle. Spokane NW, Wash. 1974. Photorevised 1986

## WEBSTER BUILDING - 415 WEST SPRAGUE SITE LOCATION

N  
1' = 2000'





Spokane City Map – 11/23/19

## WEBSTER BUILDING - 415 W. SPRAGUE SITE AERIAL

N  
1' = 2000'



## ASSESSOR FIELD FILE SKETCH AND PHOTO

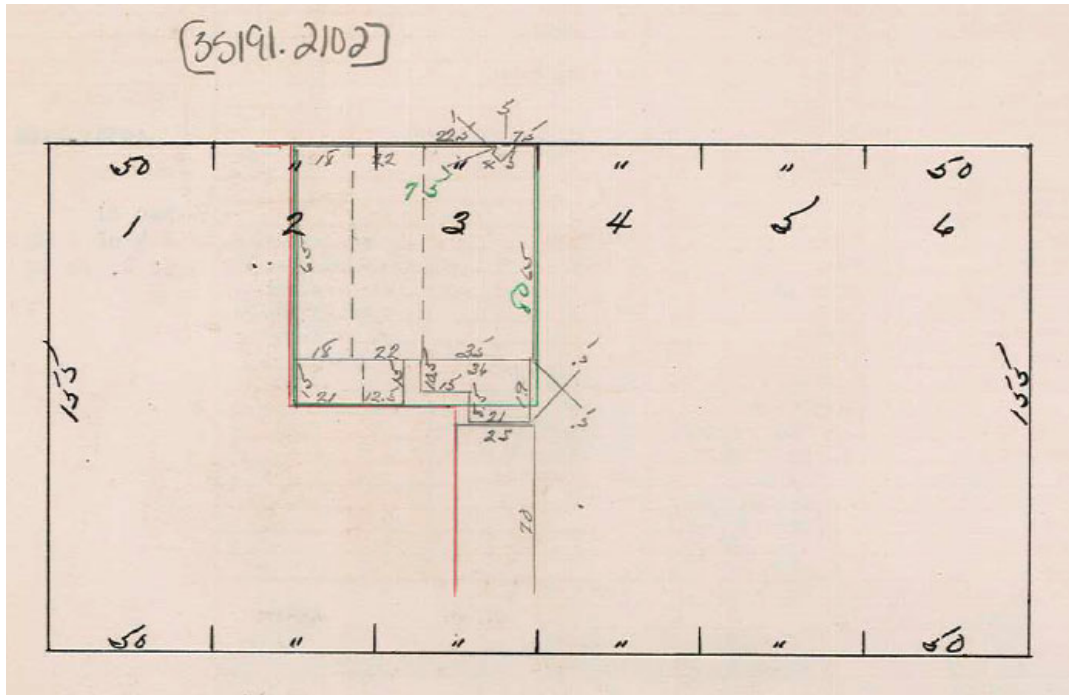


Photo and files, ca. 2004



[illegible]

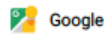


Google Maps 414 W Sprague Ave



Image capture: Jul 2017 © 2019 Google

Spokane, Washington



Street View - Jul 2017

Google Street View Photo of 415 West Sprague Avenue, 2017





1. Webster Building context along Sprague Avenue, looking southeast



2. Webster Building context along Sprague Avenue, looking west





3. Northwest corner and front facade, looking southeast

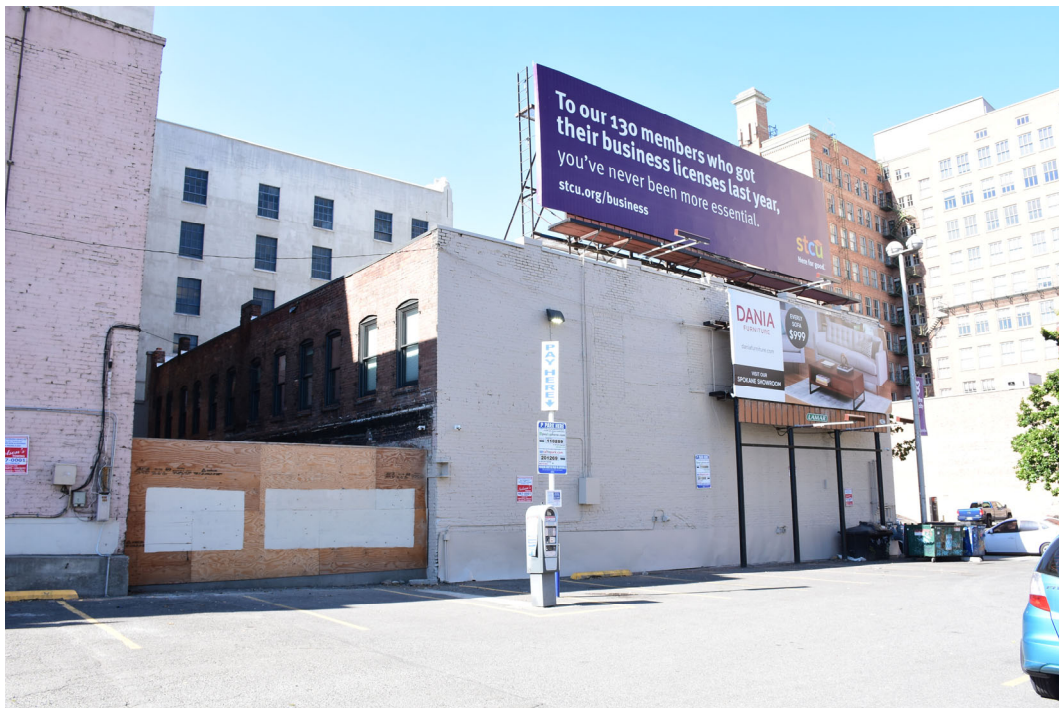


4. Northeast corner and front facade, looking southwest





5. Front Facade, looking south



6. Southeast corner, showing east and south facades, looking northwest





7. Bay 2 – Details of second floor and cornice, looking south



8. Front façade - Entry bay to second floor apartments, looking south





9. Looking south upstairs to apartments



10. Second floor - stair landing/vestibule to apartments, looking east





11. Apartment No.1 - living/dining/kitchen, looking northeast



12. Apartment No. 1 - guest bathroom, looking north



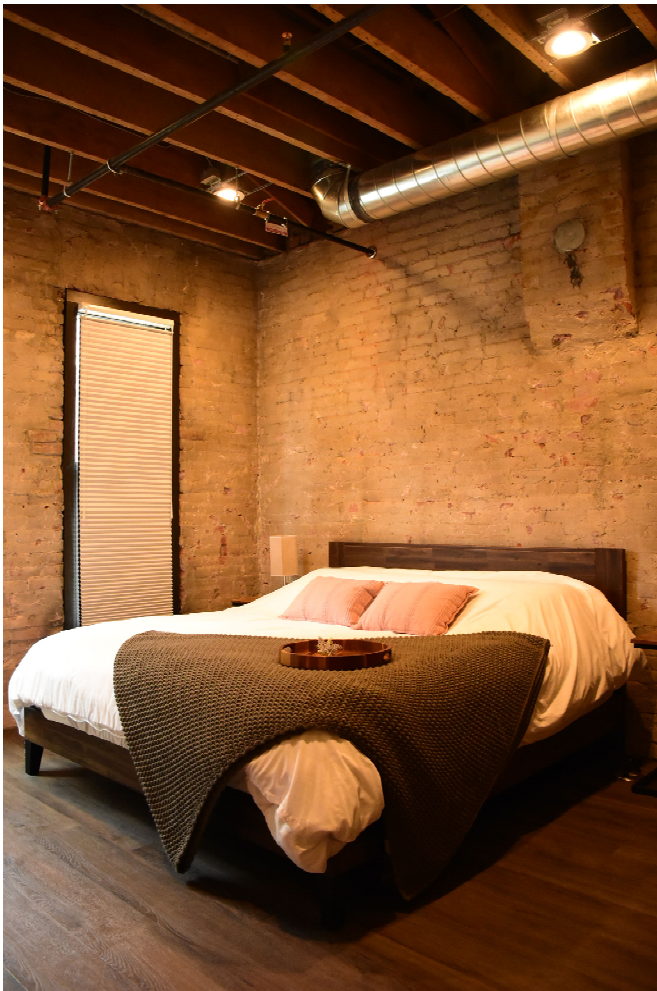


13. Bedroom No. 1, dining, living areas, looking northeast



14. Bedroom No. 1, looking north





15. Bedroom No. 2, looking northeast

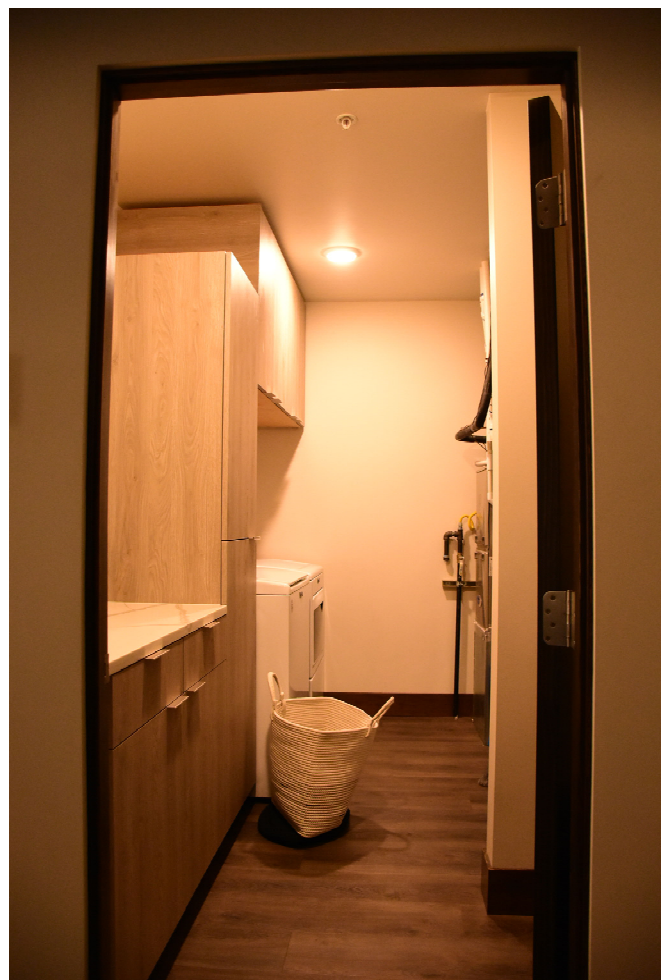


16. Bedroom No. 2 - Bath - vanity, sink, shower,  
looking southeast





17. Bedroom No. 2 - Bath - vanity, sink, toilet



18. Laundry, utility room, looking east





19. Dining area and kitchen, looking southwest



20. Living room, looking north



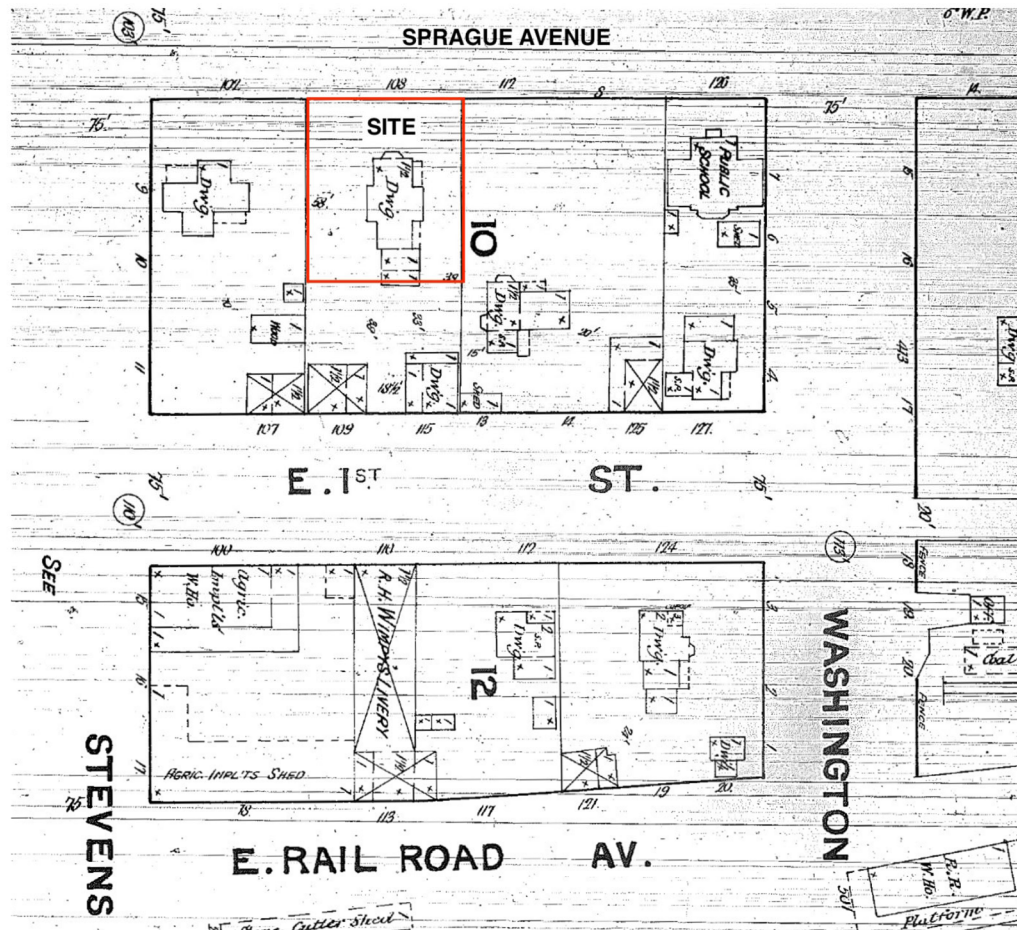


15. First floor - east commercial bays looking north



16. First floor - west commercial bays looking northwest



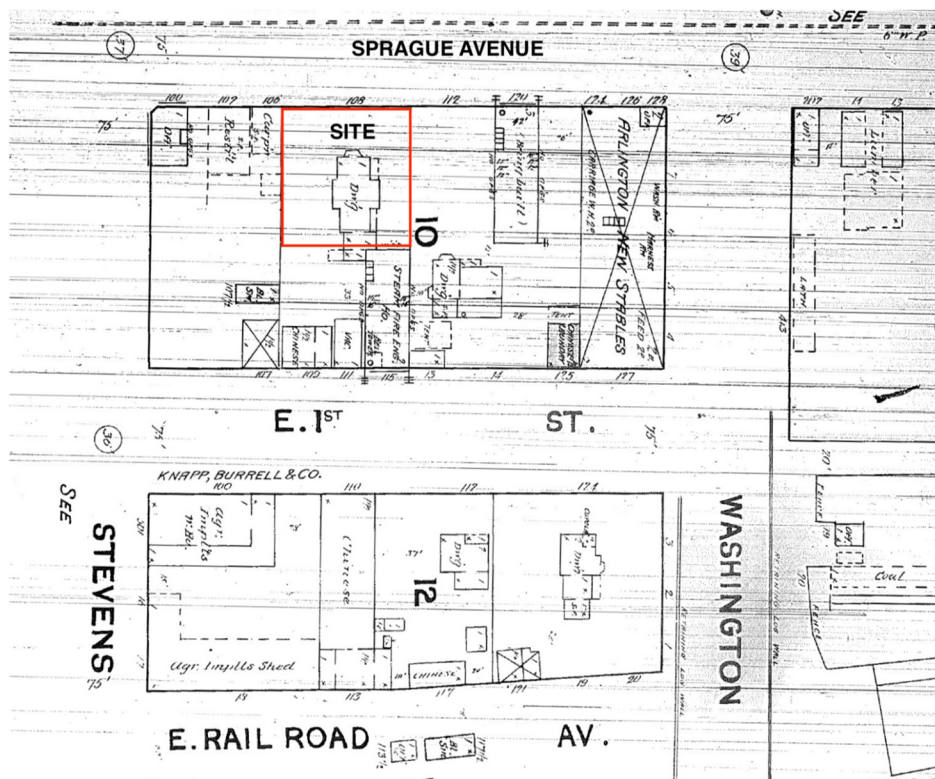


Sanborn Insurance Map – 1889 – page 7

415 WEST SPRAGUE AVENUE  
1889 SANBORN MAP

N  
1' = 100'



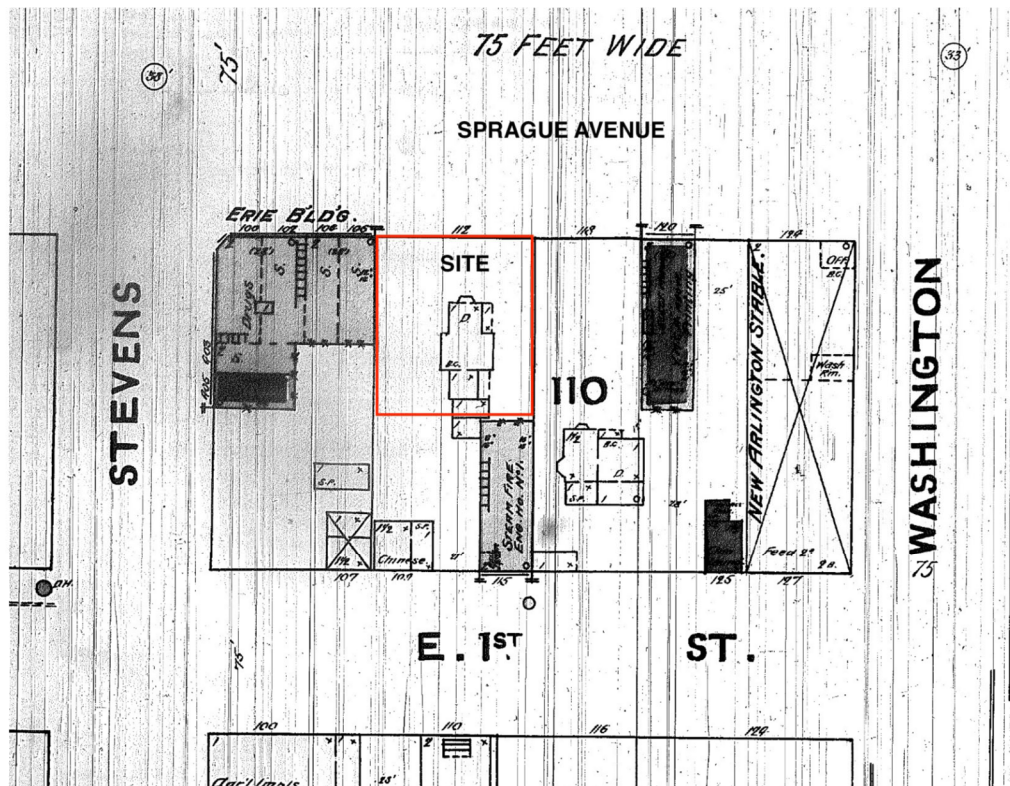


Sanborn Insurance Map – 1890 – page 7

**415 WEST SPRAGUE AVENUE**  
**1890 SANBORN MAP**

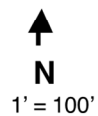
**N**  
1' = 100'



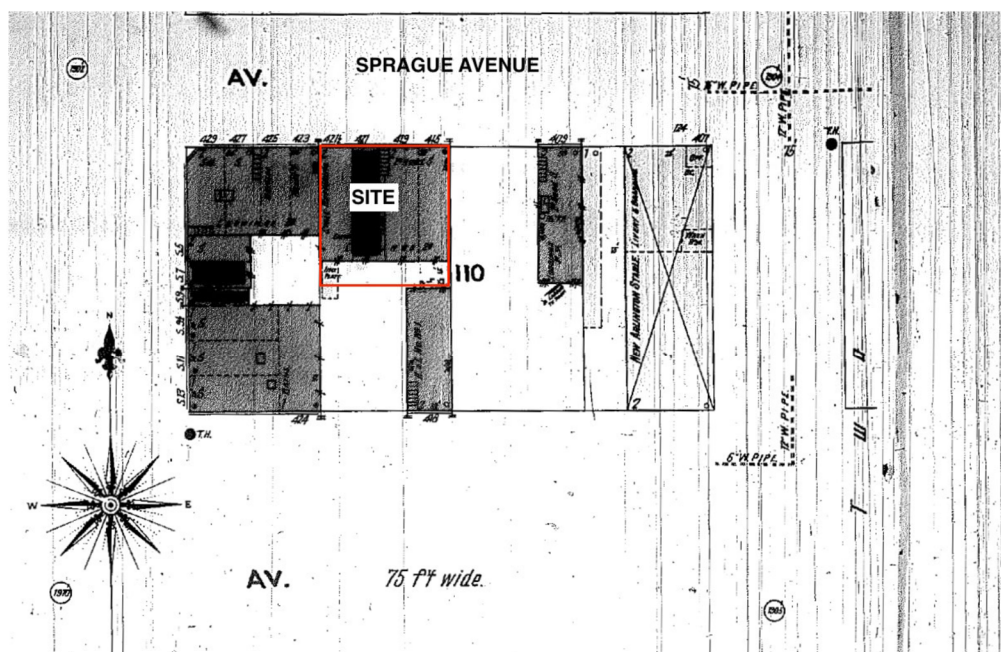


Sanborn Insurance Map – 1891 – page 9

**415 WEST SPRAGUE AVENUE**  
**1891 SANBORN MAP**





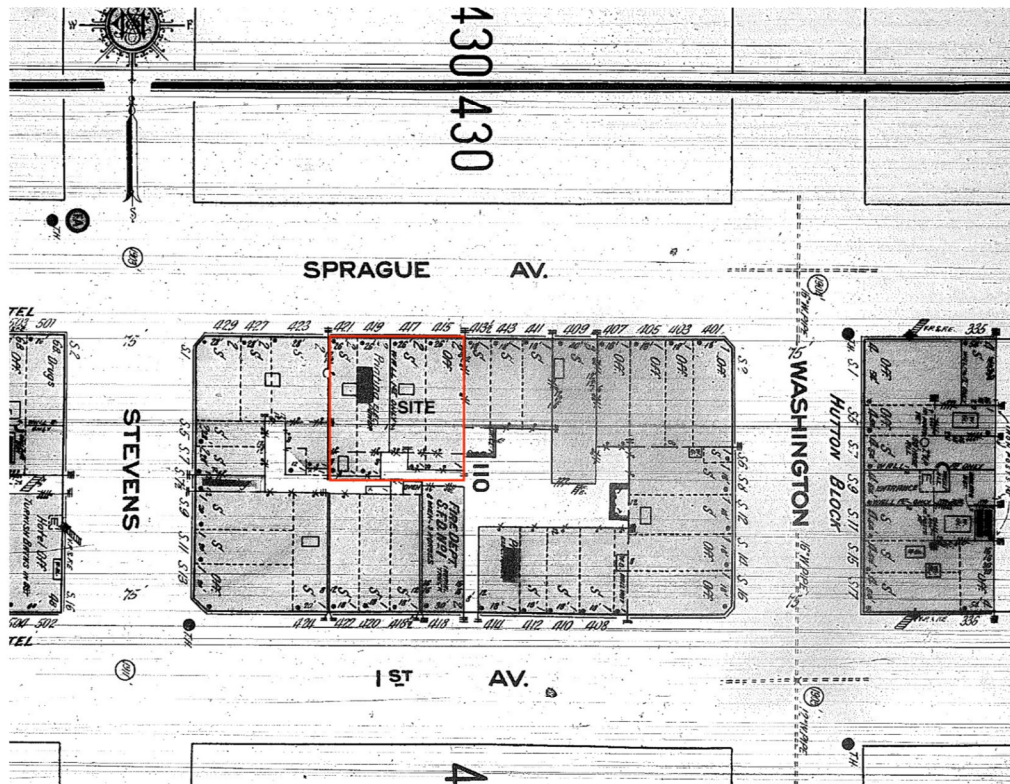


Sanborn Insurance Map – 1902 – page 9

**415 WEST SPRAGUE AVENUE**  
**1902 SANBORN MAP**

↑  
N  
1' = 100'



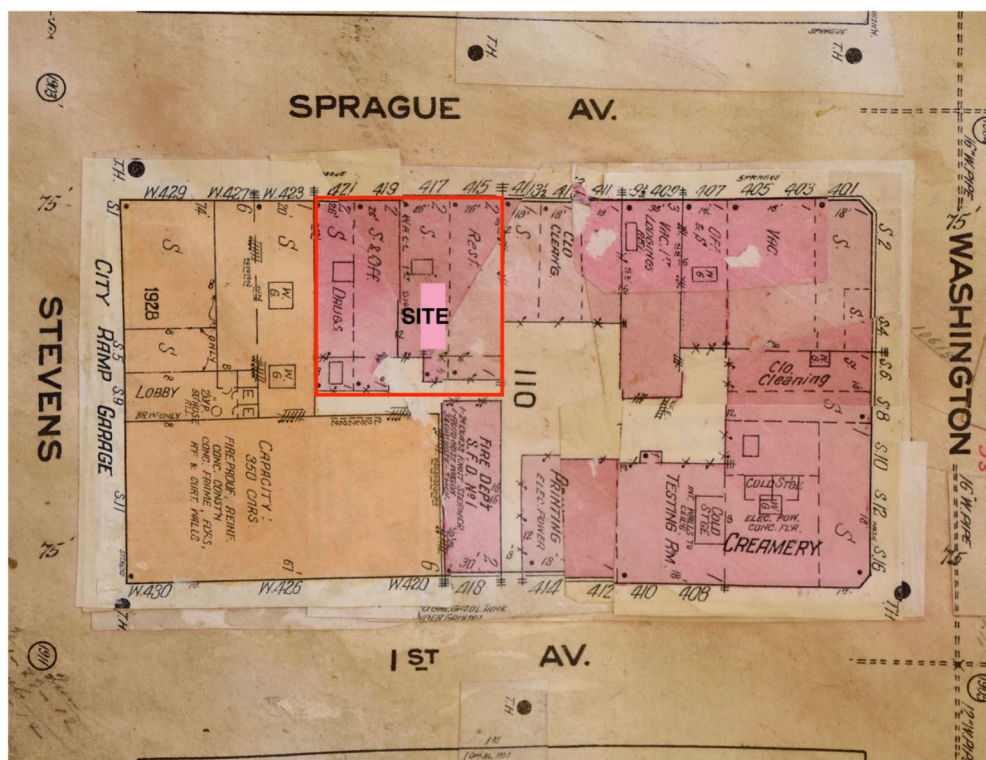


Sanborn Insurance Map – 1910 – page 430

**415 WEST SPRAGUE AVENUE**  
**1910 SANBORN MAP**

N  
1' = 100'



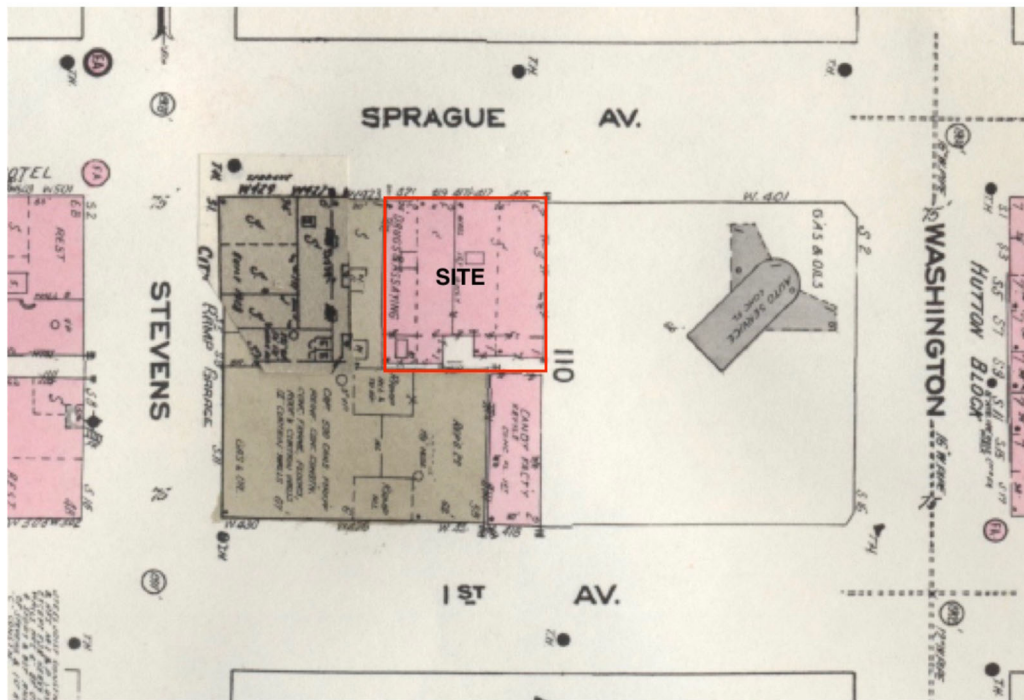


Sanborn Insurance Map – 1910 updated to 1928– page 430

**415 WEST SPRAGUE AVENUE**  
**1910 SANBORN TO 1928**

N  
 1' = 100'





Sanborn Insurance Map –1958– page 430

**415 WEST SPRAGUE AVENUE  
1958 SANBORN MAP**

**N**  
1' = 100'



Always a complete stock of  
**Patent Medicines and Toilet Articles.**  
**W. L. Turner,** Druggist and Apothecary.

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|                                                                           |                              |
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**Real Estate Agents.**

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*Houses to Rent, Loans Negotiated, Rents Collected, and Management of Property  
 for Non-residents made a Specialty. References—Bank of  
 Spokane Falls, and First National Bank.*

SPOKANE FALLS, " " " " " WASHINGTON TERRITORY.

The Spokesman Review 7/14/1883

**M**RS. R. A. OUGH, recently from Quincy,  
 Ills., and who for the past few months  
 has been visiting friends in Spokane, has de-  
 cided to open millinery here with the assist-  
 ance of Mrs. N. M. Barnard. Mrs. Barnard's  
 former customers and friends will be pleased  
 to learn that she has engaged with Mrs.  
 Ough as trimmer and saleslady. Opening  
 days, Monday, Tuesday and Wednesday,  
 Sept. 28th, 29th and 30th. Fine millinery a  
 specialty. Millinery parlors in E. J. Web-  
 ster's block, on Riverside avenue, east of  
 Howard street.

4w

Spokane Evening Review. 9/17/1885



ASH., TUESDAY., JULY 25, 1893.

**FREE! FREE!! FREE!!!**

Do not Drink River Water when you can get Carbonated

**Minnehaha Water Without Charge**

By Simply calling at Schafer & Co.'s Drug Store, Carleton  
Block, on Main Avenue.

As all lakes, wells, rivers and ponds are known to be impregnated with disease germs, our only safety is to drink a mineral water in which there is no organic matter. With this end in view, a Depot has been opened at Schafer & Co.'s Drug Store, Carleton Block, Corner Main and Howard Streets, where the celebrated Minnehaha Water will be dispensed to the public free. This water is pronounced by the leading physicians the most marvelous and purest water in the world—not only for table use, but as a cure for many of the ills that flesh is heir to.

I manufacture from the above water at MINNEHAHA PARK (where the public are always welcome to inspect the plant, and sample the goods) the following line of goods: Siphons, \$1.75 per dozen; Table Water, quarts, \$1.75 per doz. \$6.00 per case (50); Table Water, pints, \$1.00 per doz. \$3.75 per case (50); Ginger Ale, pints, \$1.50 per doz; Champagne Cider, pints, \$2.00 per doz; Imperial Tonic, quarts, \$3.00 per doz; Sarsaparilla, quarts, \$2.00 per doz; Orange Soda, quarts, \$2.00 per dozen; Banana Soda, quarts, \$2.00 per dozen; Chocolate Soda, quarts, \$2.00 per dozen; Lemon Soda, quarts, \$2.00 per dozen; Birch, quarts, \$2.00 per dozen; Ginger Fruit, quarts, \$2.00 per dozen.

The above goods are also kept on ice at the depot and are dispensed by the glass, so the ladies and the public can become familiar with the different flavors and be able to make their orders to delivery wagon according to their preferences. My wagon will deliver at your residence or place of business any of the above line, at the above prices, which does not include bottles. I have spared no time or money in bringing these goods to a high state of perfection.

Try them and Satisfy Yourself. Respectfully,

**MINNEHAHA SPRINGS.**

Telephone 148.

E. J. WEBSTER, Proprietor.





# Expenditure Control Form

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2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/10/2020

**Clerk's File #**

CPR 1982-0071

**Renews #****Cross Ref #****Submitting Dept**

MAYOR

**Contact Name/Phone**

BRANDY COTE 625-6774

**Project #****Contact E-Mail**

BCOTE@SPOKANECITY.ORG

**Bid #****Agenda Item Type**Boards and Commissions  
Appointments**Requisition #****Agenda Item Name**

0520 REAPPOINTMENT TO THE SPOKANE AIRPORT BOARD

**Agenda Wording**

Reappointment of Ezra Eckhardt to a three year term on the Spokane Airport Board (in advance of January's term expiration), from 1/26/2021 - 12/31/23.

**Summary (Background)**

Reappointment of Ezra Eckhardt to a three year term on the Spokane Airport Board (in advance of January's term expiration), from 1/26/2021 - 12/31/23.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Select \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

SIMMONS, SCOTT M.

**Study Session\Other****Division Director****Council Sponsor****Finance****Distribution List****Legal**

bcote@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

thart@spokaneairports.net

**Additional Approvals****Purchasing**





# Expenditure Control Form

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**Today's Date:** 9/30/20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** 0520 Mayor's Office

**Approving Supervisor:** Brandy Cote

**Amount of Proposed Expenditure:** 0.00

**Funding Source:** N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

No expense related to volunteer board and commission appointment.

**What are the impacts if expenses are deferred?**

N/A

**What alternative resources have been considered?**

N/A

**Description of the goods or service and any additional information?**

Board and commission appointments.

**Person Submitting Form/Contact:** Brandy Cote 625-6774

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/11/2020

**Clerk's File #**

CPR 2000-0031

**Renews #****Cross Ref #****Submitting Dept**

MAYOR

**Contact Name/Phone**

BRANDY COTE 625-6774

**Project #****Contact E-Mail**

BCOTE@SPOKANECITY.ORG

**Bid #****Agenda Item Type**Boards and Commissions  
Appointments**Requisition #****Agenda Item Name**

0520 LTAC REAPPOINTMENTS

**Agenda Wording**

Reappointment of Dan Zimmerer to a term of 07/01/2019 - 07/10/2022 and Dean Feldmeier to a term of 07/01/2020 - 07/01/2023 on the Lodging Tax Advisory Committee.

**Summary (Background)**

Reappointment of Dan Zimmerer to a term of 07/01/2019 - 07/10/2022 and Dean Feldmeier to a term of 07/01/2020 - 07/01/2023 on the Lodging Tax Advisory Committee.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Select \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

SIMMONS, SCOTT M.

**Study Session\Other****Division Director****Council Sponsor****Finance****Distribution List****Legal**

bcote@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

mcarlos@spokanecity.org

**Additional Approvals****Purchasing**





# Expenditure Control Form

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**Today's Date:** 9/30/20

**Type of expenditure:** Goods ☐ Services ☒

**Department:** 0520 Mayor's Office

**Approving Supervisor:** Brandy Cote

**Amount of Proposed Expenditure:** 0.00

**Funding Source:** N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

No expense related to volunteer board and commission appointment.

**What are the impacts if expenses are deferred?**

N/A

**What alternative resources have been considered?**

N/A

**Description of the goods or service and any additional information?**

Board and commission appointments.

**Person Submitting Form/Contact:** Brandy Cote 625-6774

**FINANCE SIGNATURE:**

---

**CITY ADMINISTRATOR SIGNATURE:**

---





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                       |             |
|-----------------------|-------------|
| <b>Date Rec'd</b>     | 10/26/2020  |
| <b>Clerk's File #</b> | ORD C35972  |
| <b>Renews #</b>       |             |
| <b>Cross Ref #</b>    |             |
| <b>Project #</b>      | Z19-499COMP |
| <b>Bid #</b>          |             |
| <b>Requisition #</b>  |             |

|                           |                                                      |
|---------------------------|------------------------------------------------------|
| <b>Submitting Dept</b>    | PLANNING                                             |
| <b>Contact Name/Phone</b> | KEVIN FREIBOTT 625-6184                              |
| <b>Contact E-Mail</b>     | KFREIBOTT@SPOKANECITY.ORG                            |
| <b>Agenda Item Type</b>   | First Reading Ordinance                              |
| <b>Agenda Item Name</b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – LIBERTY AVENUE |

**Agenda Wording**

An Ordinance relating to application Z19-499COMP by Liberty Project, LLC, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to General Commercial for 0.85 acres and a change to the Zoning Map from RSF to GC-70.

**Summary (Background)**

The proposal concerns 3001, 3011, 3027 E Liberty Avenue, parcels 35033.1304, 35033.1305, and 35033.1306. This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended approval of the amendment.

|                                    |                                       |                                     |                           |
|------------------------------------|---------------------------------------|-------------------------------------|---------------------------|
| <b><u>Fiscal Impact</u></b>        | Grant related? NO<br>Public Works? NO | <b><u>Budget Account</u></b>        |                           |
| Neutral                            | \$                                    | #                                   |                           |
| Select                             | \$                                    | #                                   |                           |
| Select                             | \$                                    | #                                   |                           |
| Select                             | \$                                    | #                                   |                           |
| <b><u>Approvals</u></b>            |                                       | <b><u>Council Notifications</u></b> |                           |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS                         | <b><u>Study Session\Other</u></b>   | CC Study Session 10-29-20 |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M.                     | <b><u>Council Sponsor</u></b>       | CM Mumm                   |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY                       | <b><u>Distribution List</u></b>     |                           |
| <b><u>Legal</u></b>                | RICHMAN, JAMES                        | tblack@spokanecity.org              |                           |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL                       | jrichman@spokanecity.org            |                           |
| <b><u>Additional Approvals</u></b> |                                       | sbishop@spokanecity.org             |                           |
| <b><u>Purchasing</u></b>           |                                       | liamt@storhaug.com                  |                           |
|                                    |                                       | alex@storhaug.com                   |                           |
|                                    |                                       | lmeuler@spokanecity.org             |                           |
|                                    |                                       |                                     |                           |



Ordinance No. C35972

AN ORDINANCE RELATING TO APPLICATION FILE Z19-499COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "GENERAL COMMERCIAL" FOR APPROXIMATELY 0.85 ACRES LOCATED AT 3001, 3011, AND 3027 E LIBERTY AVENUE (PARCELS 35033.1304, 35033.1305, AND 35033.1306) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "GENERAL COMMERCIAL (GC-70)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-499COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-499COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 0.85 acres from "Residential 4-10" to "General Commercial"; if approved, the implementing zoning destination requested is "General Commercial (GC-70)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-499COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-499COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-499COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend approval of Application Z19-499COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-499COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "General Commercial" for 0.85 acres, as shown in Exhibits A and B.
3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from "Residential Single Family" to "General Commercial (GC-70)," as shown in Exhibits C and D.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

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Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





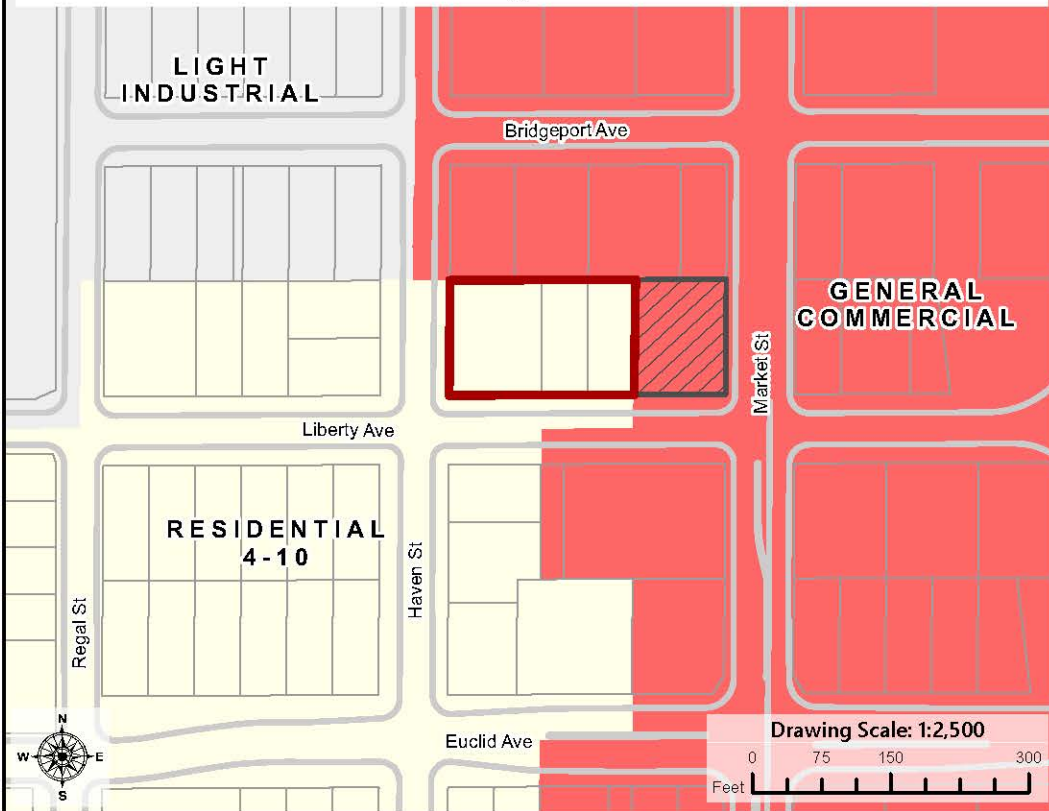
# Z19-499COMP (3001, 3022, & 3027 E Liberty Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

### Land Use Plan Designation

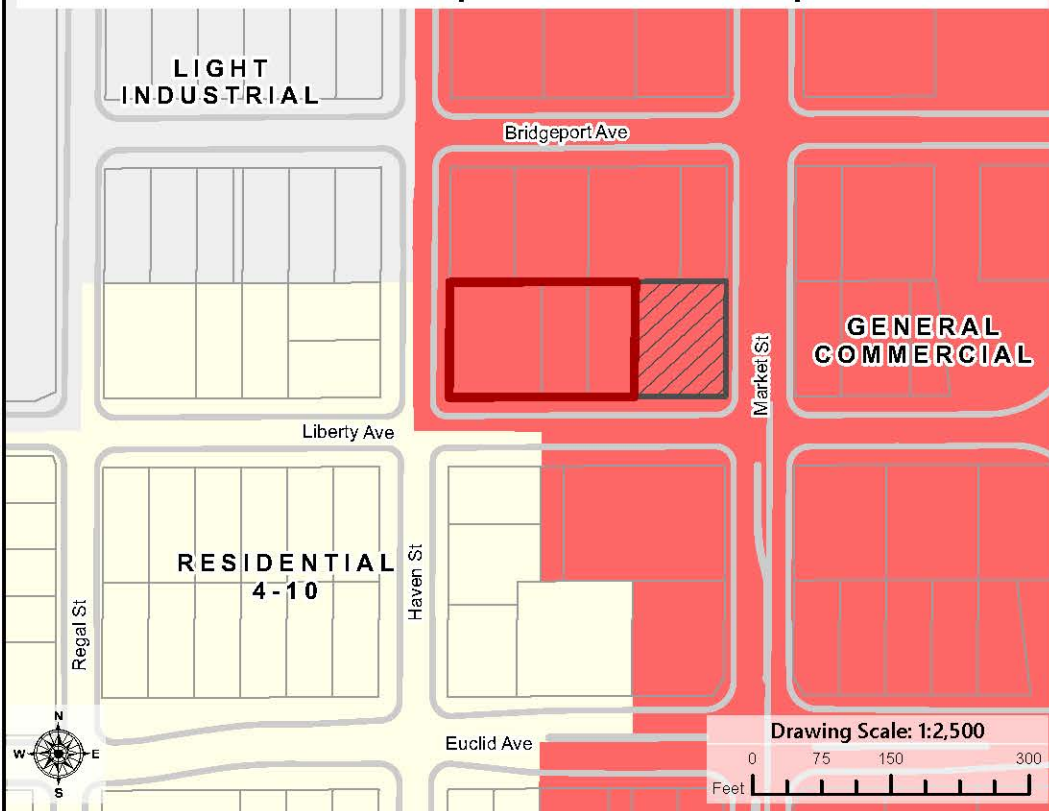
Residential 4-10

General Commercial

Light Industrial

Acres (Proposal): 0.85  
Acres (Adjacent): 0.28

## EXHIBIT B: Proposed Land Use Plan Map



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

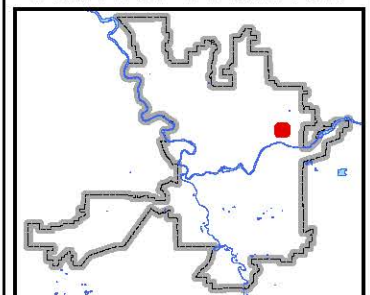
### Proposed Land Use

Residential 4-10

General Commercial

Light Industrial

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# **Z19-499COMP** **(3001, 3011, & 3027 E Liberty Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

### Current Zoning

General Commercial (GC)

Light Industrial (LI)

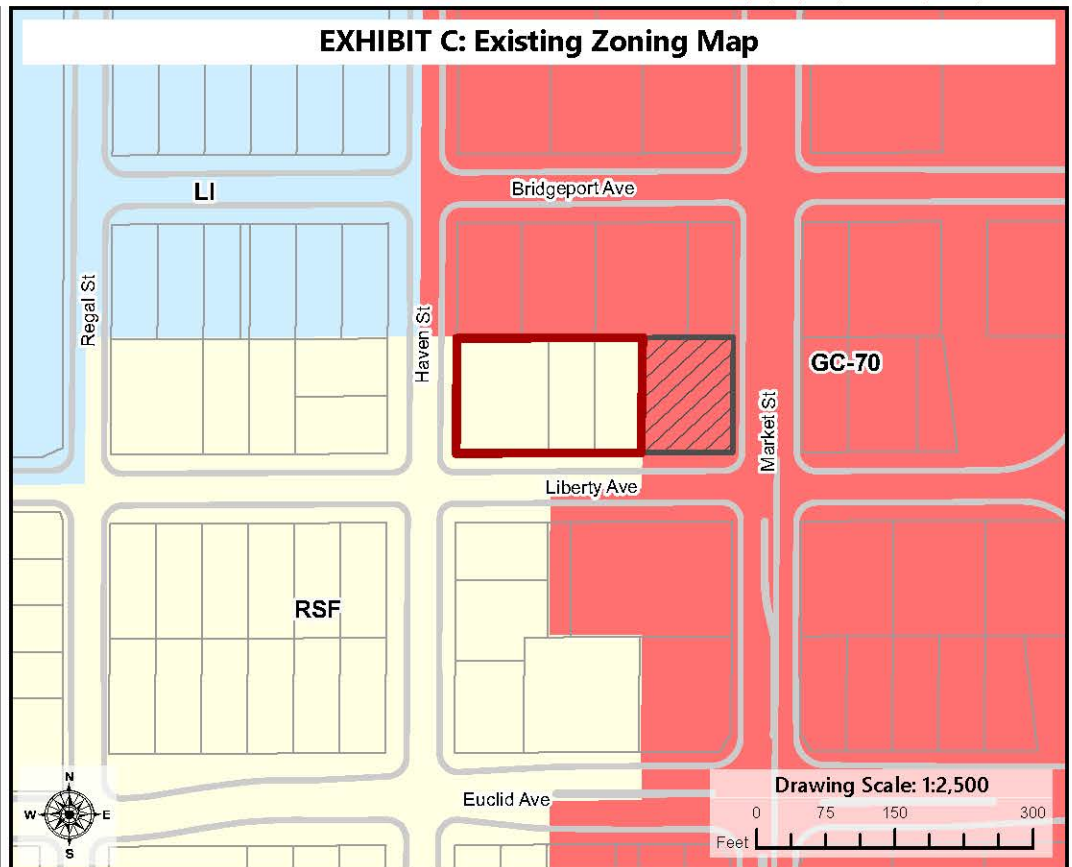
Residential Single-Family (RSF)

*Numbers after a Zone Label denote the height limits in that area.*

Acres (Proposal): 0.85

Acres (Adjacent): 0.28

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

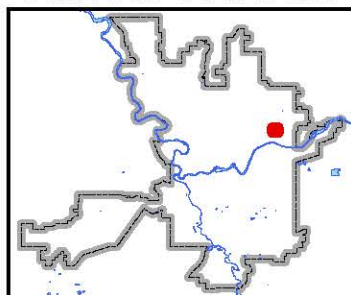
### Proposed Zoning

General Commercial (GC)

Light Industrial (LI)

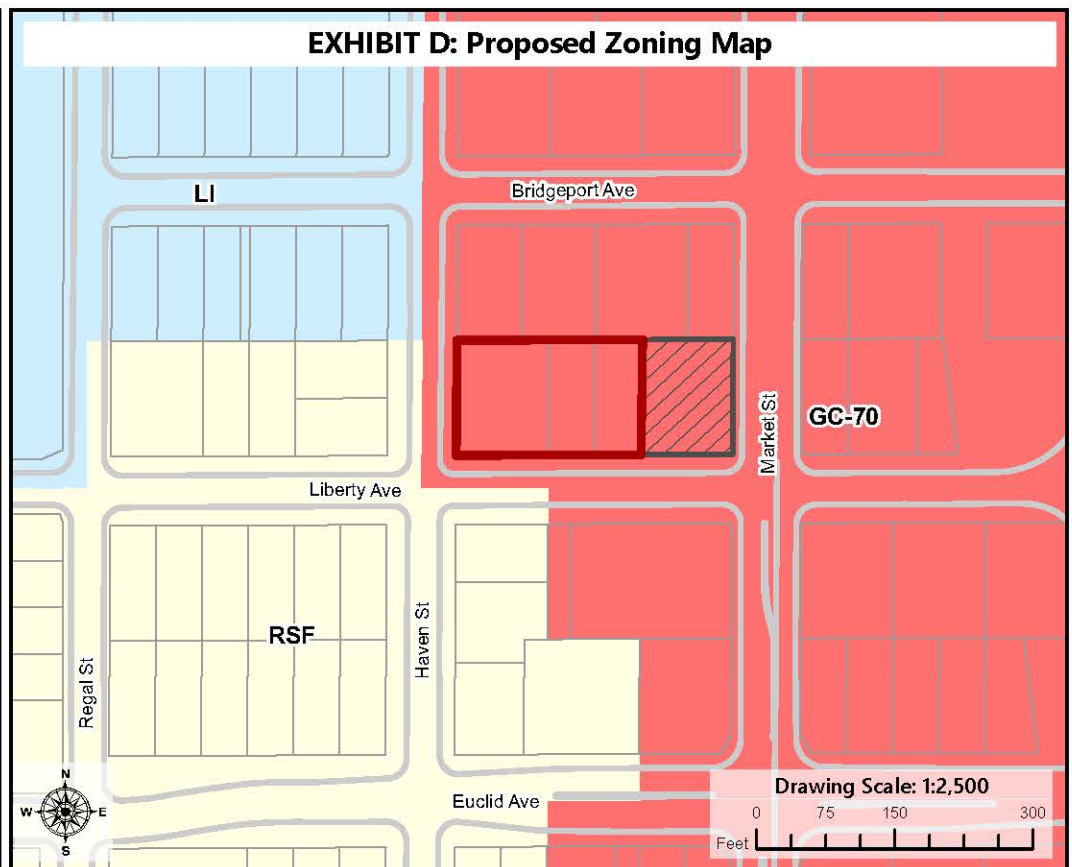
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





**Exhibit E: Legal Description**

Lots 7 through 12, Block 13, Minnehaha Addition in the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-499COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map (Map LU1) designation and zoning of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                           |
|---------------------------|---------------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 35033.1304, 35033.1305, and 35033.1306 (partial)                          |
| <b>Address(es):</b>       | 3001, 3011, and 3027 E Liberty Avenue                                     |
| <b>Property Size:</b>     | 0.85 acres (area of change), 1.13 acres in common ownership.              |
| <b>Legal Description:</b> | Lots 7 through 12, Block 13, Minnehaha Addition                           |
| <b>General Location:</b>  | North side of E Liberty Avenue between N Haven Street and N Market Street |
| <b>Current Use:</b>       | Residential home and one retail/commercial building.                      |

**II. APPLICANT SUMMARY**

|                        |                                                  |
|------------------------|--------------------------------------------------|
| <b>Agent:</b>          | Leslie Perez & Alex Durkin, Storhäug Engineering |
| <b>Applicant:</b>      | Jordan Tampien, Liberty Project LLC              |
| <b>Property Owner:</b> | Liberty Project LLC, Spokane WA                  |

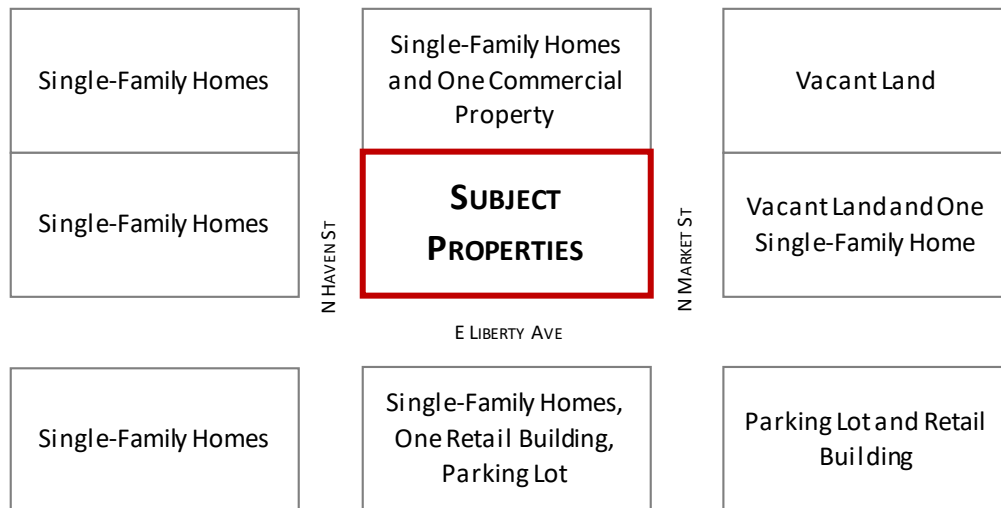
**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 Dwelling Units/Acre (R 4-10)                                                                                               |
| <b>Proposed Land Use Designation:</b> | General Commercial (GC)                                                                                                                     |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | General Commercial, 70-foot height limit (GC-70)                                                                                            |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | Recommended                                                                                                                                 |



#### IV. BACKGROUND INFORMATION

- 1. General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for three properties located in the Bemiss Neighborhood. The intent of the applicant is to potentially develop non-residential uses on the entire south half of the block, all in common ownership by the applicant.
- 2. Site Description and Physical Conditions:** The proposal concerns three parcels: 35033.1304, 35033.1305, and part of 35033.1306. All three comprise the south half of the block on the north side of E Liberty Avenue, between N Haven Street and N Market Street. Parcel 35033.1304 contains an unpaved driveway and no other improvements. Parcel 35033.1305 contains one residential house, currently rented out. Parcel 35033.1306 contains a commercial/retail building currently containing a restaurant. Other improvements include landscaping and a parking lot.
- 3. Property Ownership:** All of the subject properties are owned by the same owner, Liberty Project LLC. Liberty Project LLC is a registered WA State Limited Liability Company based in Spokane, WA.
- 4. Adjacent Property Improvements and Uses:** The proposal is surrounded by existing development of the following nature:



- 5. Street Class Designations:** N Haven Street and E Liberty Avenue are currently designated as local streets. N Market Street is designated as a Major Arterial. The Arterial Street Map in the Comprehensive Plan does not indicate that these designations should change. Likewise, no change of street class designation is proposed as part of this application.
- 6. Current Land Use Designation and History:** As shown in **Exhibit A**, the subject properties are currently designated for the “Residential 4-10” (between 4 and 10 dwelling units per acre) except for the east 100 feet of parcel 35033.1306, which is designated for “General Commercial” land use. The subject properties have been designated for these uses since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001.



7. **Proposed Land Use Designation:** As shown in **Exhibit B**, the proposal is to amend the Land Use Plan Map designation so that the entirety of the subject properties are designated General Commercial.
8. **Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF), except for the east 100 feet of parcel 35033.1306, which is zoned General Commercial with a 70-foot height limit (GC-70). This zoning has been the same since the current zoning map was adopted in 2006.

Historically, between 1975 and 2006, the western 2/3 of the properties were zoned “R2,” indicated for two-family homes, and the eastern 1/3 were zoned for “M1,” light industrial uses. In 1958, the properties were all zoned “Class 1 Residential,” indicated for single-family homes.

9. **Proposed Zoning:** As shown in **Exhibit D**, the proposal seeks to amend the zoning so that the entirety of the subject properties are zoned General Commercial with a 70-foot height limit (GC-70).

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | June 10, 2020     |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |
| SEPA Determination Issued .....                               | August 24, 2020   |
| Notice of Public Hearing Posted .....                         | August 26, 2020   |
| Plan Commission Hearing Date (Scheduled) .....                | September 9, 2020 |

2. **Comments Received:** A request for comments was issued to City departments, local agencies, and departments, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- Joellie Eliason, Spokane Development Services Center

---

<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



- Randy Abrahamson, Spokane Tribe Historic Preservation

Ms. Eliason communicated that the Spokane Development Services Center has no concerns or objection to the proposal. As for the Spokane Tribe, Mr. Abrahamson recommends that prior to any site development a cultural survey and sub-surface testing be conducted to identify and protect any historic or cultural resources on the site. Copies of these comments are included in this staff report as **Exhibit L**.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. City staff emailed notice to the neighborhood council as well and to any nearby neighborhood councils. A packet of comment letters was submitted, dated July 18, each of which contained the identical message of opposition to the proposal. Staff received a total of 17 of these comments. Copies of these comments are included in this staff report as **Exhibit M**.

3. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 10, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was provided an opportunity to speak during the workshop but no public comment was taken.

## VI. APPLICATION REVIEW AND ANALYSIS

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:
  - A. Keep the comprehensive plan alive and responsive to the community.
  - B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
  - C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
  - D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
  - E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
  - F. Amendments to the comprehensive plan must result in a net benefit to the general public.
2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.



- A. Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency or City Department comment request or require a traffic impact analysis for the proposal. The subject properties are already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

- E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should*



*strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.

*Neighborhood Planning Documents Adopted after 2001.* The Greater Hillyard North-East Planning Alliance, made up of the Bemiss, Hillyard, and Whitman neighborhoods, adopted its final neighborhood plan in 2010. None of the feature or recommendations in that plan would be affected by the change in use of the subject parcels.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

The proposal meets this criterion.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: The proposal is generally consistent with current Comprehensive Plan policies, as described in further detail in the staff analysis of Criterion K.2 below and other criteria in this report. Therefore, no amendment to policy wording is necessary and this criterion does not apply to the subject proposal.

The proposal meets this criterion.

- F. **Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*



**Staff Analysis:** The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues. No comments have been received from any agency, City department, or neighboring jurisdiction which would indicate that this proposal is not regionally consistent.

The proposal meets this criterion.

**G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments as part of an annual plan amendment cycle. Six applications are for land use plan map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse

---

<sup>3</sup> State Environmental Policy Act



environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

- I. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of a previously developed area served by public facilities and services described in CFU 2.1. The proposed change in land-use designations affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

K. **Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);



Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.8, General Commercial Uses. That policy generally calls for containment of non-residential uses to centers and corridors. The subject properties are well outside the area of any Center or Corridor designated by the City. However, the policy also allows for “limited expansions” of commercial areas outside Centers, provided the following factors are considered:

- Maintaining minimum depth from an arterial street necessary for the establishment or expansion of a general commercial neighborhood business;
- Avoiding intrusion where incompatible into established neighborhoods; and
- Implementing transitioning land uses with the intent of protecting neighborhood character.

Two of the three subject parcels are already surrounded on three sides by General Commercial land use and zoning. Additionally, the entire northern half of the block is already designated for General Commercial. The proposed action would terminate at City streets on the west and south. Furthermore, provisions exist within the SMC for landscaping buffers, height transitions, and other features that would mitigate impacts to the nearby residences and their occupants. These facts provide a basis for arguing that this application is consistent with policy LU 1.8.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude commercial development on the site. The site is adequately served by all utilities and by a major arterial street, bus service is nearby at the intersection of Francis and Nevada, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: As discussed in ‘a’ above, designation of this location for non-residential uses would comply with the requirements of the Comprehensive Plan.

The proposal meets this criterion.

- 3. Rezones, Land Use Plan Amendment:** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*



Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property will change from RSF (Residential Single-Family) to GC-70 (General Commercial, 70-foot height limit). The GC zone implements the General Commercial land-use designation proposed by the applicant. No policy language changes have been identified as necessary to support the proposed Land Use Plan Map amendment. The proposal meets this criterion.

The proposal meets this criterion.

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears consistent with criteria for a comprehensive plan amendment as provided in SMC 17G.020.030.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Staff recommends that the Plan Commission and City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                  |                                           |
|----------------------------------|-------------------------------------------|
| A. Existing Land Use Plan Map    | H. List of Relevant Comp Plan Policies    |
| B. Proposed Land Use Plan Map    | I. Application Materials                  |
| C. Existing Zoning Map           | J. SEPA Checklist                         |
| D. Proposed Zoning Map           | K. SEPA Determination of Non-Significance |
| E. Application Notification Area | L. Agency Comments                        |
| F. Detail Aerial                 | M. Public Comments                        |
| G. Wide-Area Aerial              |                                           |





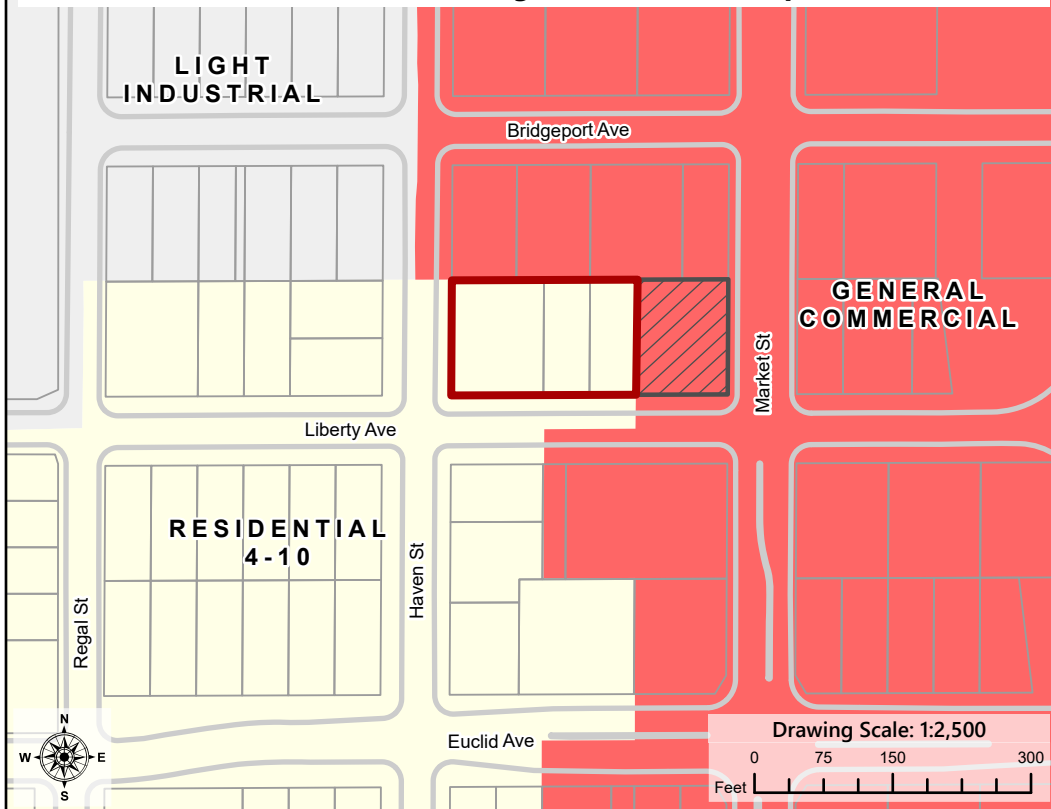
# Z19-499COMP (3001, 3022, & 3027 E Liberty Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

### Land Use Plan Designation

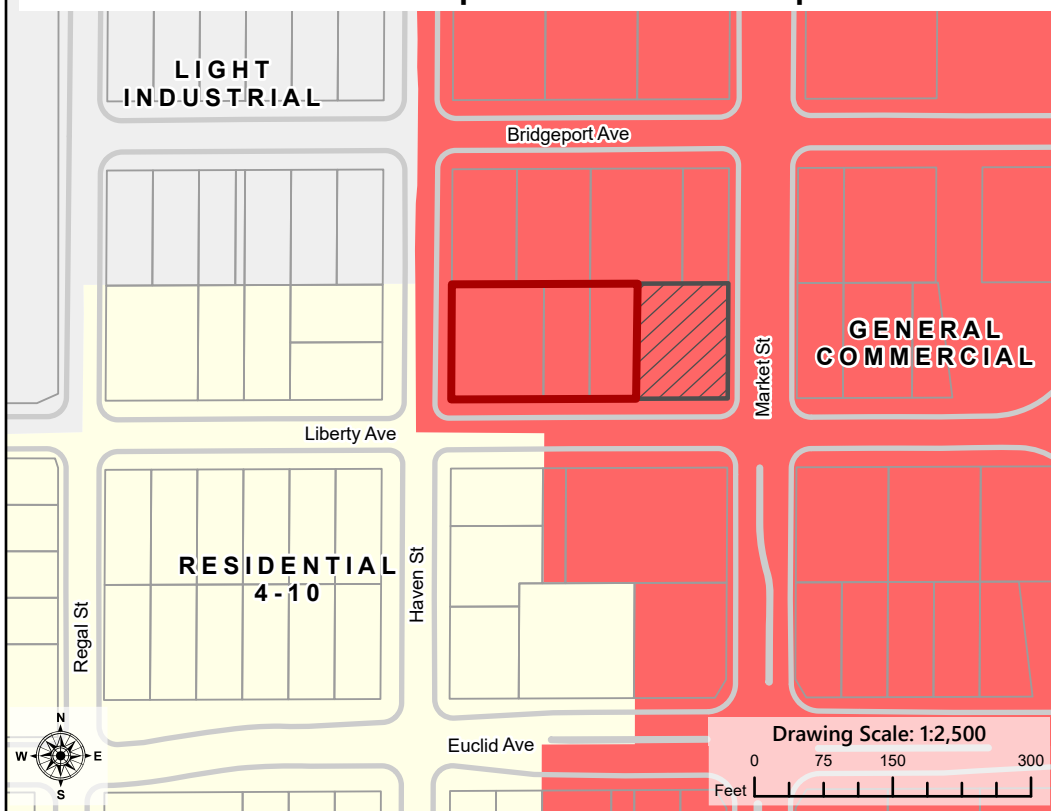
Residential 4-10

General Commercial

Light Industrial

Acres (Proposal): 0.85  
Acres (Adjacent): 0.28

## EXHIBIT B: Proposed Land Use Plan Map



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

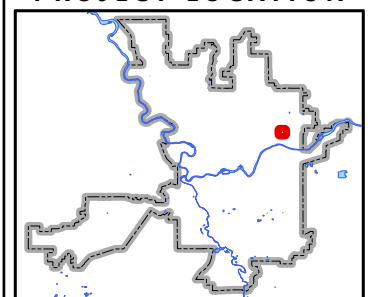
### Proposed Land Use

Residential 4-10

General Commercial

Light Industrial

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-499COMP (3001, 3011, & 3027 E Liberty Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

### Current Zoning

General Commercial (GC)

Light Industrial (LI)

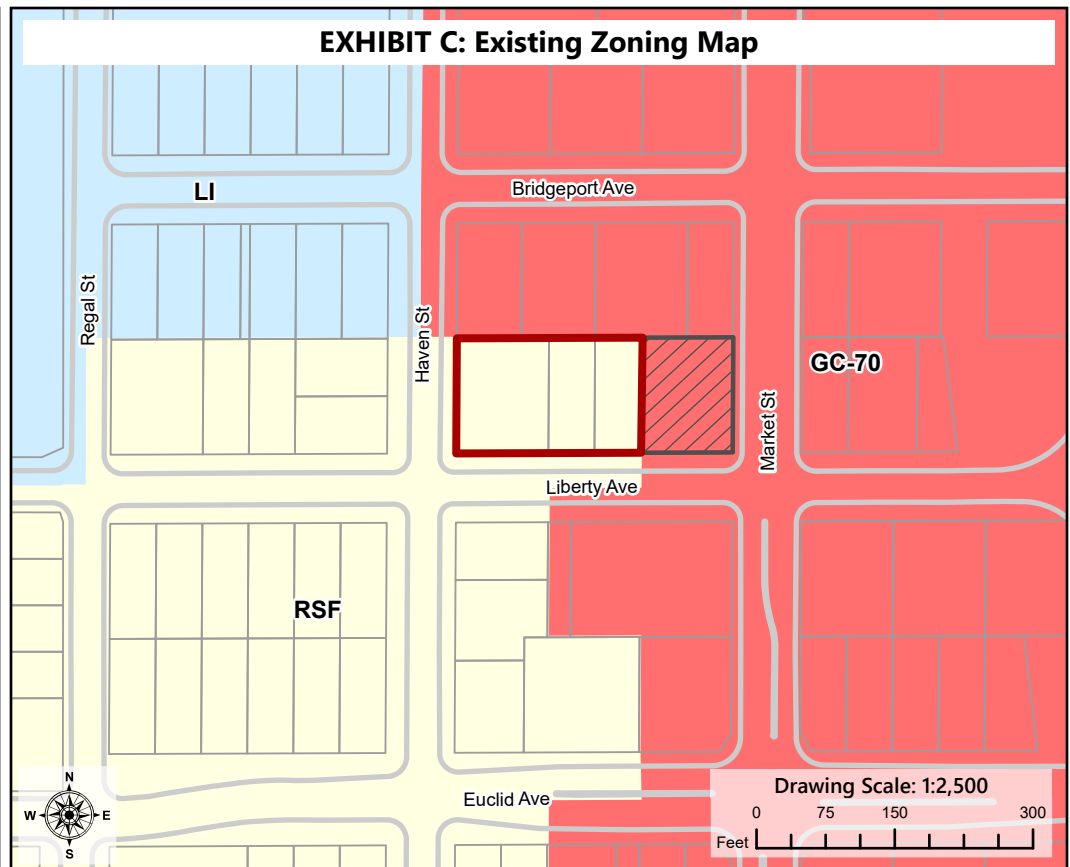
Residential Single-Family (RSF)

*Numbers after a Zone Label denote the height limits in that area.*

Acres (Proposal): 0.85

Acres (Adjacent): 0.28

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

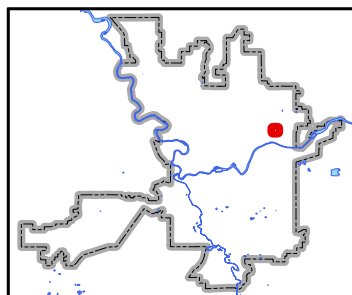
### Proposed Zoning

General Commercial (GC)

Light Industrial (LI)

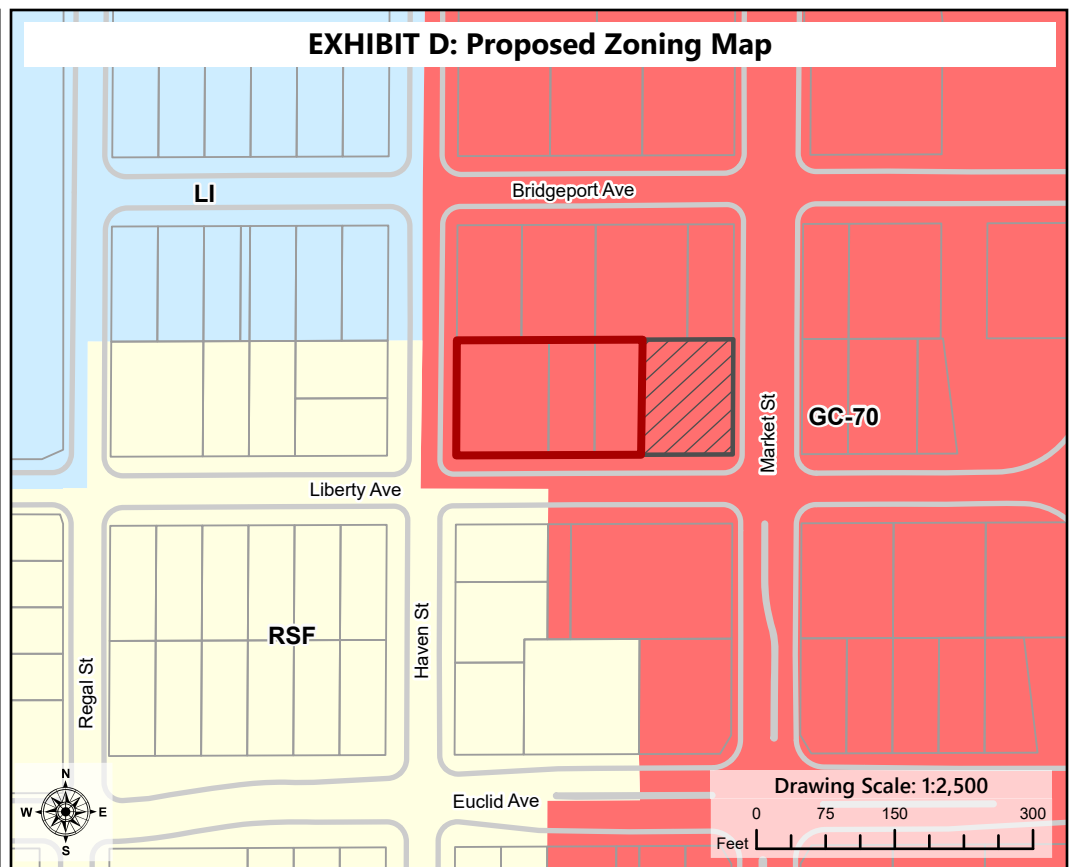
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





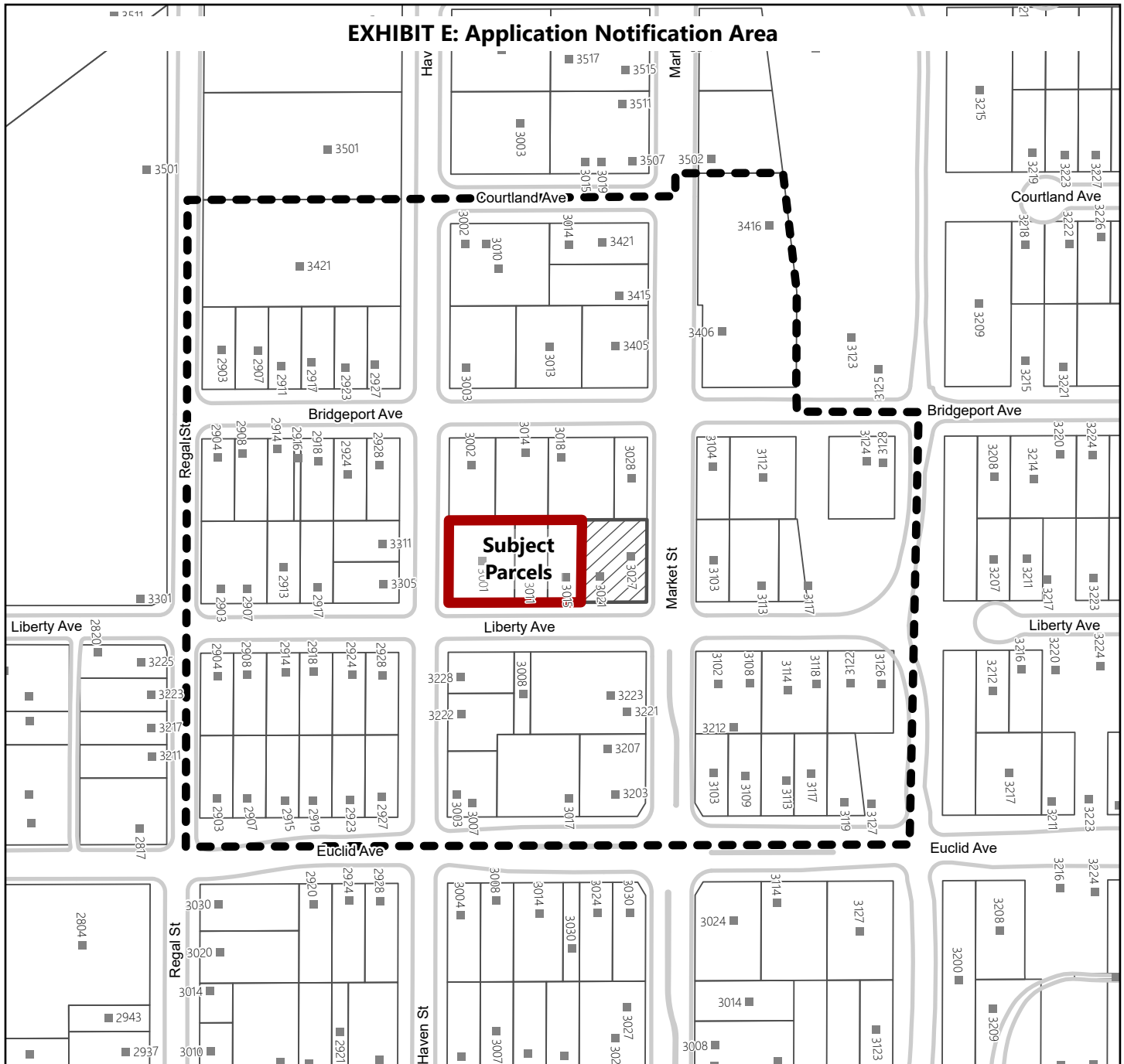


# Z19-499COMP

## (3001, 3011, & 3027 E Liberty Ave)

2019/2020 Comprehensive Plan Amendment Proposals

THIS IS NOT A LEGAL DOCUMENT: The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



### Legend

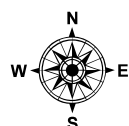
- Parcel
- Notification Boundary
- Curb Line
- Address Point

### Subject Parcels

- Subject Parcels
- Same Owner

### Application proposes to:

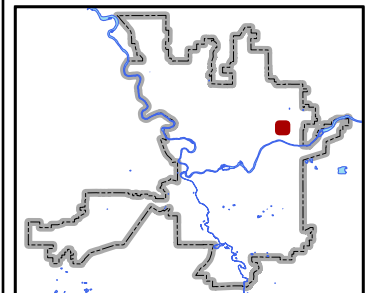
Change Land Use Designation from Residential 4-10 to General Commercial.



Project Size: 0.85 Acres (Approximate)  
Drawing Date: 12/3/2019 Drawing Scale: 1:2,700

0 75 150 300  
Feet

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-499COMP (3001, 3011, & 3027 E Liberty Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT F: Detail Aerial



## Legend

Subject Parcels

Adjacent Ownership

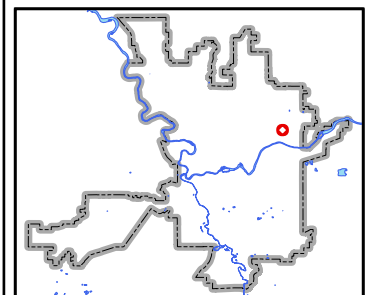
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 0.85  
Acres (Adjacent): 0.28

## EXHIBIT G: Wide-Area Aerial



## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-499COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.8 General Commercial Uses

Direct new General Commercial uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* General Commercial areas provide locations for a wide range of commercial uses. Typical development in these areas includes freestanding business sites and larger grouped businesses (shopping centers). Commercial uses that are auto-oriented and include outdoor sales and warehousing are also allowed in this designation. Land designated for General Commercial use is usually located at the intersection of or in strips along principal arterial streets. In many areas such as along Northwest Boulevard, this designation is located near residential neighborhoods.

To address conflicts that may occur in these areas, zoning categories should be implemented that limit the range of uses, and site development standards should be adopted to minimize detrimental impacts on the residential area. New General Commercial areas should not be designated in locations outside Centers and Corridors. Existing commercial strips should be contained within their current boundaries with no further extension along arterial streets allowed.

However, recognizing existing investments, and given deference to existing land-use patterns, exceptions to the containment policy may be allowed for limited expansions adjacent to existing General Commercial areas located outside Centers and Corridors. The factors to consider in such adjacent expansions include: maintaining the minimum depth from an arterial street necessary for the establishment or expansion of a general commercial neighborhood business; avoiding intrusion where incompatible into established neighborhoods; and implementing transitional land uses with the intent of protecting neighborhood character.

Areas designated General Commercial within Centers and Corridors are encouraged to be developed in accordance with the policies for Centers and Corridors. Through a neighborhood planning process



for the Center, these General Commercial areas will be designated in a land use category that is appropriate in the context of a Center and to meet the needs of the neighborhood.

Residential uses are permitted in these areas. Residences may be in the form of single-family homes on individual lots, upper-floor apartments above business establishments, or other higher density residential uses.

#### LU 3.1 Coordinated and Efficient Land Use

Encourage coordinated and efficient growth and development through infrastructure financing and construction programs, tax and regulatory incentives, and by focusing growth in areas where adequate services and facilities exist or can be economically extended.

*Discussion:* Future growth should be directed to locations where adequate services and facilities are available. Otherwise, services and facilities should be extended or upgraded only when it is economically feasible to do so.

The Centers and Corridors designated on the Land Use Plan Map are the areas of the city where incentives and other tools should be used to encourage infill development, redevelopment and new development. Examples of incentives the city could use include assuring public participation, using public facilities and lower development fees to attract investment, assisting with project financing, zoning for mixed-use and higher density development, encouraging rehabilitation, providing in-kind assistance, streamlining the permit process, providing public services, and addressing toxic contamination, among other things.

#### LU 5.3 Off-Site Impacts

Ensure that off-street parking, access, and loading facilities do not adversely impact the surrounding area.

*Discussion:* Off-street parking, access, and loading facilities are usually associated with the development of higher density residential, office, and commercial uses. These features often have major impacts on single-family residential areas. The impacts are most significant when these facilities are next to or intrude between homes. When these facilities are accessory to a higher density residential or nonresidential use, they should be developed according to the same policies and zoning regulations as govern the primary use. New parking lots should also have the same zoning classification as the primary use. In addition, these facilities should be developed to minimize adverse impacts to adjacent properties. All parking lots should be paved. Parking lots and loading areas should have appropriate buffers to fully screen them from adjacent, less intensive uses. Access to business and higher density residential sites should be controlled to avoid impacts on adjacent uses, pedestrian movement, and street functions.

#### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

### **Chapter 11—Neighborhoods**

#### N 8.4 Consistency of Plans



Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.





October 28, 2019

Kevin Freibott, Planner II  
City of Spokane, Planning & Development  
808 W Spokane Falls Blvd,  
Spokane, WA 99201

RE: Tampien Liberty Multi-Family Rezone Project  
Rezone/Comprehensive Plan Amendment  
City of Spokane, Washington  
Storhaug Engineering Project #19-272



Dear Kevin,

The following proposal is for a rezone/comprehensive plan map amendment to change the zoning classification of three (3) Residential Single-Family zoned parcels into General Commercial. The ultimate purpose of this is for a future multi-family, mixed use project. The site is located at 3001, 3011, 3027 E Liberty Ave, in the City of Spokane, WA, 99207, parcel numbers 35033.1304, 35033.1305, and 35033.1306, respectively. See rezone/comprehensive map amendment exhibit for more information. The following language is a description of how the application for a Comprehensive Plan Amendment for the Tampien Liberty Multi-Family Rezone project satisfies the threshold review criteria in SMC 17G.020.026. The threshold review criteria are restated and answered below:

- 1.) Describe how the proposal amendment is appropriately addressed as a Comprehensive Plan Amendment.

**The Comprehensive Plan Amendment Procedure (SMC 17G.020.025.A.1) grants members of the public or persons or entities other than the City Council and Spokane Plan Commission to initiate comprehensive plan amendment proposals.**

- 2.) The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City Council or by a neighborhood or subarea planning process.

**There are no ongoing work programs approved by the City Council or a neighborhood or subarea planning process that address this area and request.**

- 3.) The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.



The request is to change approximately 0.57 acres of RSF into GC-70 (approximately 0.14 of said acres belongs to a split-zoned parcel that is both RSF and GC-70). If necessary, a section of the proposal site can be zoned Office to act as a transition zone for the adjacent to the RSF zoned area. This can be reasonably reviewed within the resources and time frame for annual amendments.

- 4.) In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?

The client nor the agent has had any outreach to the surrounding property owners. Efforts to contact and meet with the Bemiss Neighborhood Council have been made, and if necessary, efforts to contact and meet with the Minnehaha Neighborhood Council will be made, as it is adjacent to the site.

- 5.) Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.

One of the guiding principles of the annual amendment process is to keep the comprehensive plan alive and responsive to the community (SMC 17G.020.010.B.1). The proposal area is adjacent to several RSF zoned parcels, and it is currently zoned RSF. However, the proposal is also surrounded by several GC-70 zoned parcels, which seem to form a small, unofficial "Corridor" in the area, and we believe that the proposed comprehensive plan amendment and associated zoning change would be beneficial to this "Corridor," and therefore keeps the comprehensive plan alive and responsive. On September 25, 2019, the Spokane Plan Commission recommended a policy text change for the comprehensive plan, for LU 1.8, General Commercial Uses. The new text amendment would allow for "expansions adjacent to existing General Commercial areas located outside Centers and Corridors." The proposal would be in conformance with the Plan Commissions' recommended text amendment.

Policy Topic 3 of the Countywide Planning Policies for Spokane County (CWPP) is the Promotion of Contiguous and Orderly Development and Provision of Urban Services, which states "[r]ealistically, growth would go first to areas with existing public services and facilities..." The proposal site is already in a developed urban area and has access to water and sewer. At the time of development (or potentially during the SEPA review process for this



proposal), the City of Spokane will determine if there are adequate services to the site, and that the levels of service will be maintained.

The first planning goal of the Growth Management Act (GMA) is to “[e]ncourage development in urban areas where adequate public facilities and services exist...” (RCW 36.70A.020.1). The proposal site is within a developed urban area in the City of Spokane, Washington, and already has access to water, sewer, public roads, and emergency services. Therefore, the proposal is consistent with the policy implementation of the GMA. It is also worth noting that the subject parcels are located within the Multi-Family Tax Exemption (MFTE) program area.

- 6.) The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year’s threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.

**The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year’s threshold review process and was not included in the Annual Comprehensive Plan Amendment Work Program.**

- 7.) If this change is directed by state law or a decision of a court or administrative agency, please describe.

**N/A**

- 8.) Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.

**Please see attached correspondence.**

If you have any questions, please do not hesitate to get in contact with us.

Respectfully Submitted,  
Liam Taylor



Enclosures:

- 1.) Rezone/Comprehensive Plan Map Amendment Exhibit
- 2.) Outreach to Bemiss Neighborhood Council





# General Application

Rev.20180104

## DESCRIPTION OF PROPOSAL

The rezone/comp plan amendment of 3 parcels from RSF to GC for a mixed use/multi-family development.

Address of Site Proposal (if not yet assigned, obtain address from Public Works before submitting application)

3001, 3011, 3027 E Liberty Ave

RECEIVED

OCT 29 2019

## APPLICANT

Name: Jordan Tampien

Neighborhood and  
Planning Services

Address: 915 W 2nd Ave, Spokane, WA 99201

Phone: 509-413-1956 Email: Jordan@4degrees.com

## PROPERTY OWNER

Name: Liberty Project LLC

Address: 915 W 2nd Ave, Spokane WA 99201

Phone: 509-413-1956 Email: Jordan@4degrees.com

## AGENT

Name: Storhang Engineering Inc. (Leslie Perez)

Address: 510 E Third Ave, Spokane, WA 99202

Phone: 509-242-1000 Email: lesliep@storhang.com

Assessor's Parcel Numbers: 35033.1304, 35033.1305, 35033.1306

Legal Description of Site: Minnehaha ADD L7-12 B13



# BEMISS NEIGHBORHOOD OUTREACH

Application Z19-499COMP

liamt@storhaug.com

---

**From:** lesliiep@storhaug.com  
**Sent:** Tuesday, October 22, 2019 4:40 PM  
**To:** bemissneighborhood@gmail.com  
**Cc:** jerryd@storhaug.com; Liam Taylor  
**Subject:** Comprehensive Plan Amendment Notice  
**Attachments:** 19-272 Rezone Exhibit.pdf

To Whom it May Concern,

We are reaching out to you regarding a possible Comprehensive Plan Amendment located within the Bemiss Neighborhood. The subject parcel numbers are [35033.1305](#) and [35033.1304](#), located at Market and Liberty (3001 and 3011 E Liberty Ave., Spokane, WA). Our client is considering a mixed-use/multi-family project in this location with a base zoning of GC-70 or Office. The client also owns the directly adjacent property at 3027 E Liberty, which is already zoned GC-70, and there currently exists a restaurant. As you can see in Scout (and the attached exhibit) the subject parcels are within an unofficial "corridor" of GC zoned properties to the north, east and west. Should you have any questions, require additional information, or would like to schedule a meeting to further discuss please feel free to get in touch.

My Best,

**Leslie Perez, Planner III**



civil engineering | planning  
landscape architecture | surveying  
510 east third avenue | spokane, wa 99202  
p. 509.242.1000 | w. [www.storhaug.com](http://www.storhaug.com)





REZONE/COMPREHENSIVE PLAN AMENDMENT

# TAMPIEN LIBERTY

A PORTION OF THE SOUTHWEST 1/4 OF SECTION 03,  
TOWNSHIP 27 NORTH, RANGE 37 EAST, W.M.  
CITY OF SPOKANE, WASHINGTON

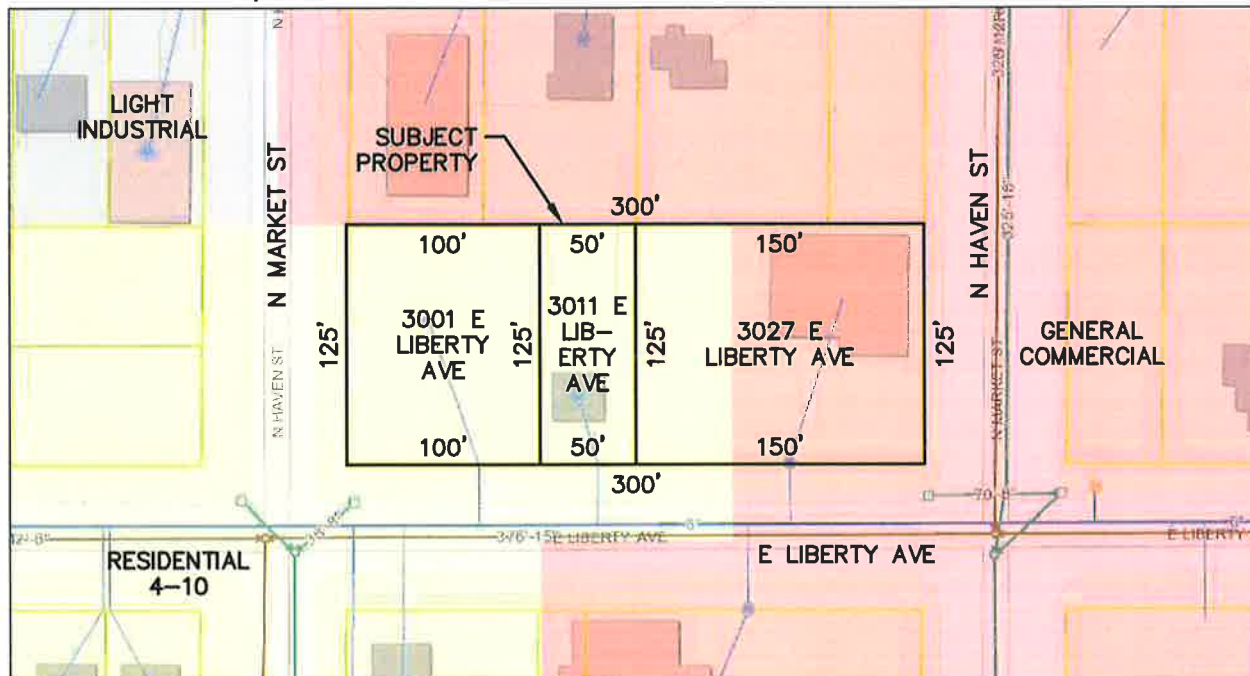
## OWNER/APPLICANT

LIBERTY PROJECT LLC  
915 W 2ND AVE  
SPOKANE, WA 99201  
509-499-2231

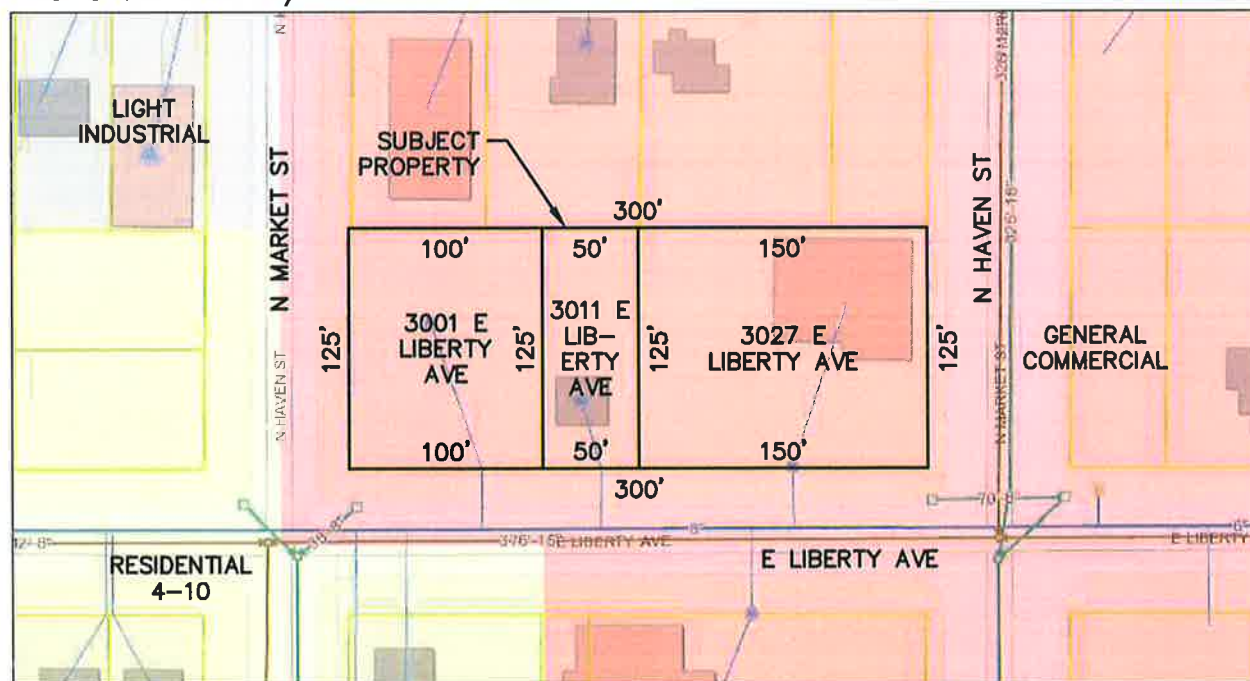
## LEGAL DESCRIPTION

MINNEHAHA ADD  
L7-12 B13

### EXISTING ZONING/LAND USE PLAN:



### PROPOSED ZONING/LAND USE PLAN:



CALL BEFORE YOU DIG 8-11



510 east third avenue | spokane, washington 99202 | p 509.242.1000 f 509.242.1001

TAMPIEN LIBERTY  
REZONE/COMPREHENSIVE PLAN AMENDMENT  
SPOKANE, WA.

|         |     |       |            |         |        |
|---------|-----|-------|------------|---------|--------|
| DRAWN   | LJT | DATE  | 10/28/2019 | EXHIBIT | 1 OF 1 |
| CHECKED | JDS | SCALE | 1" = 100'  | PROJECT | 19-272 |



REZONE/COMPREHENSIVE PLAN AMENDMENT

**TAMPIEN LIBERTY**

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TOWNSHIP 27 NORTH, RANGE 37 EAST, W.M.  
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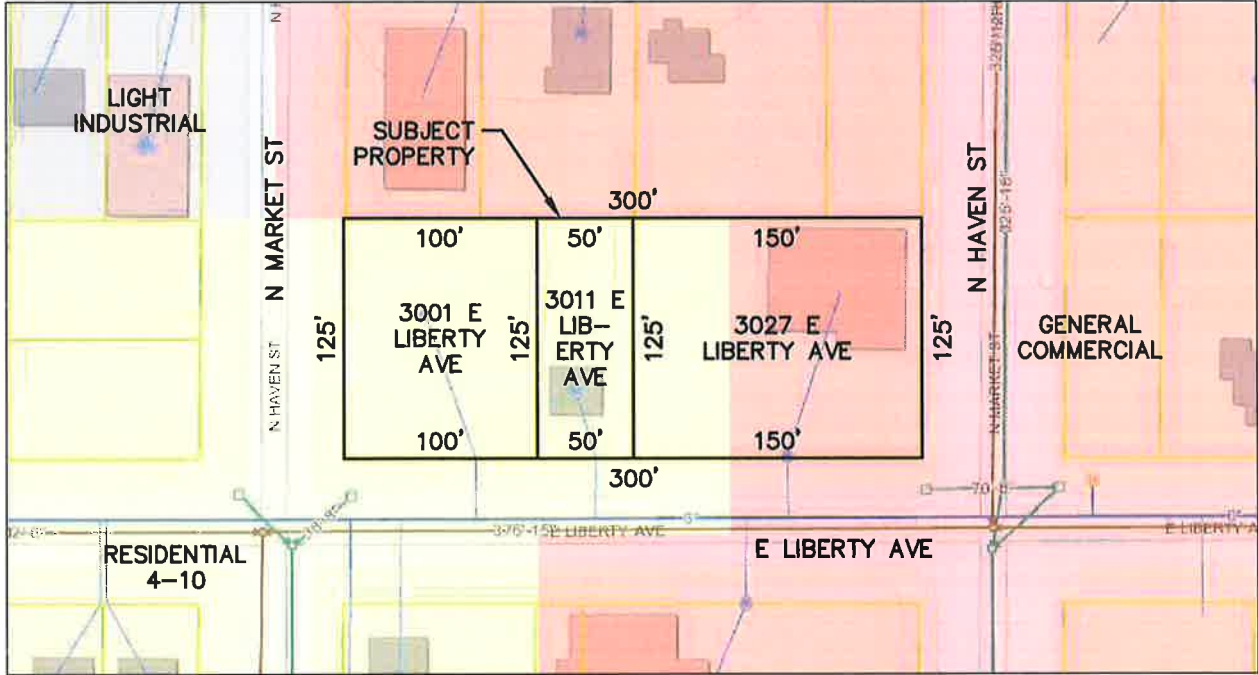
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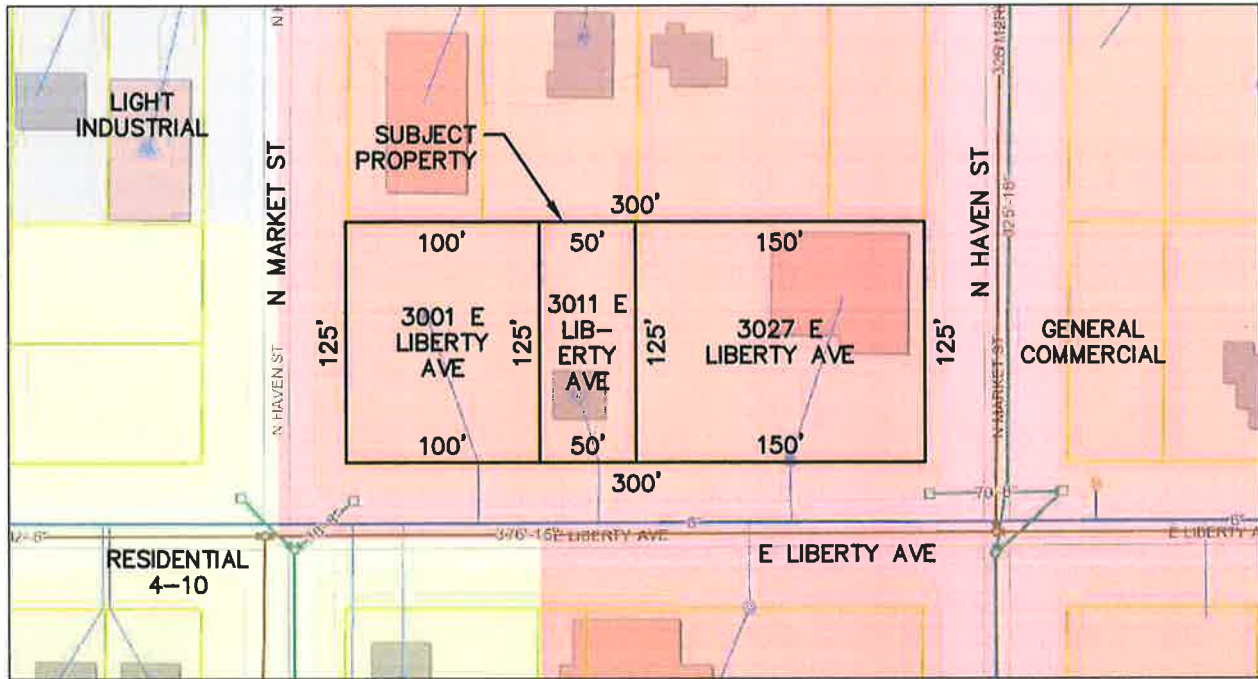
LEGAL DESCRIPTION

MINNEHAHA ADD  
L7-12 B13

EXISTING ZONING/LAND USE PLAN:



PROPOSED ZONING/LAND USE PLAN:



CALL BEFORE YOU DIG 8-11



civil engineering | planning  
landscape architecture | surveying

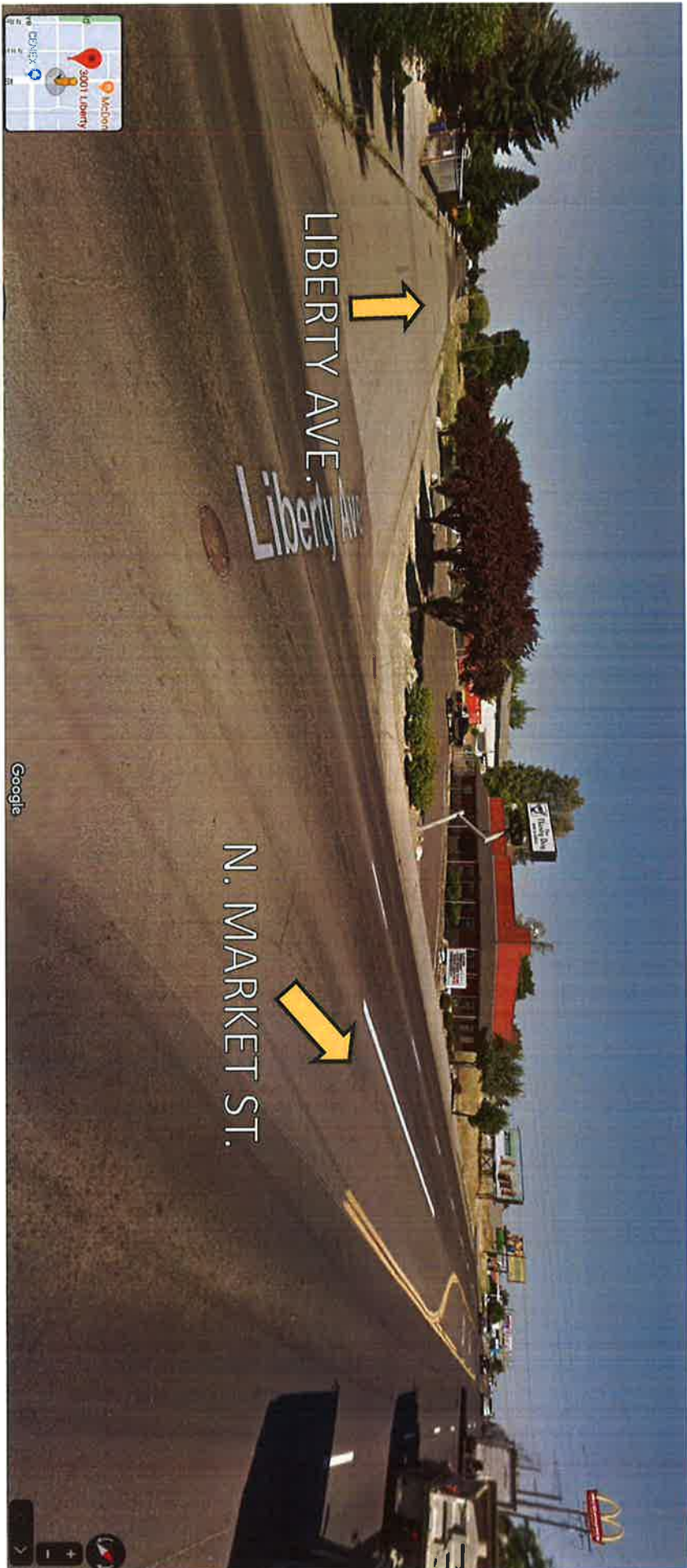
510 east third avenue | spokane, washington 99202 | p 509.242.1000 f 509.242.1001

**TAMPIEN LIBERTY**  
REZONE/COMPREHENSIVE PLAN AMENDMENT  
SPOKANE, WA.

|         |     |       |            |         |        |
|---------|-----|-------|------------|---------|--------|
| DRAWN   | LJT | DATE  | 10/28/2019 | EXHIBIT | 1 OF 1 |
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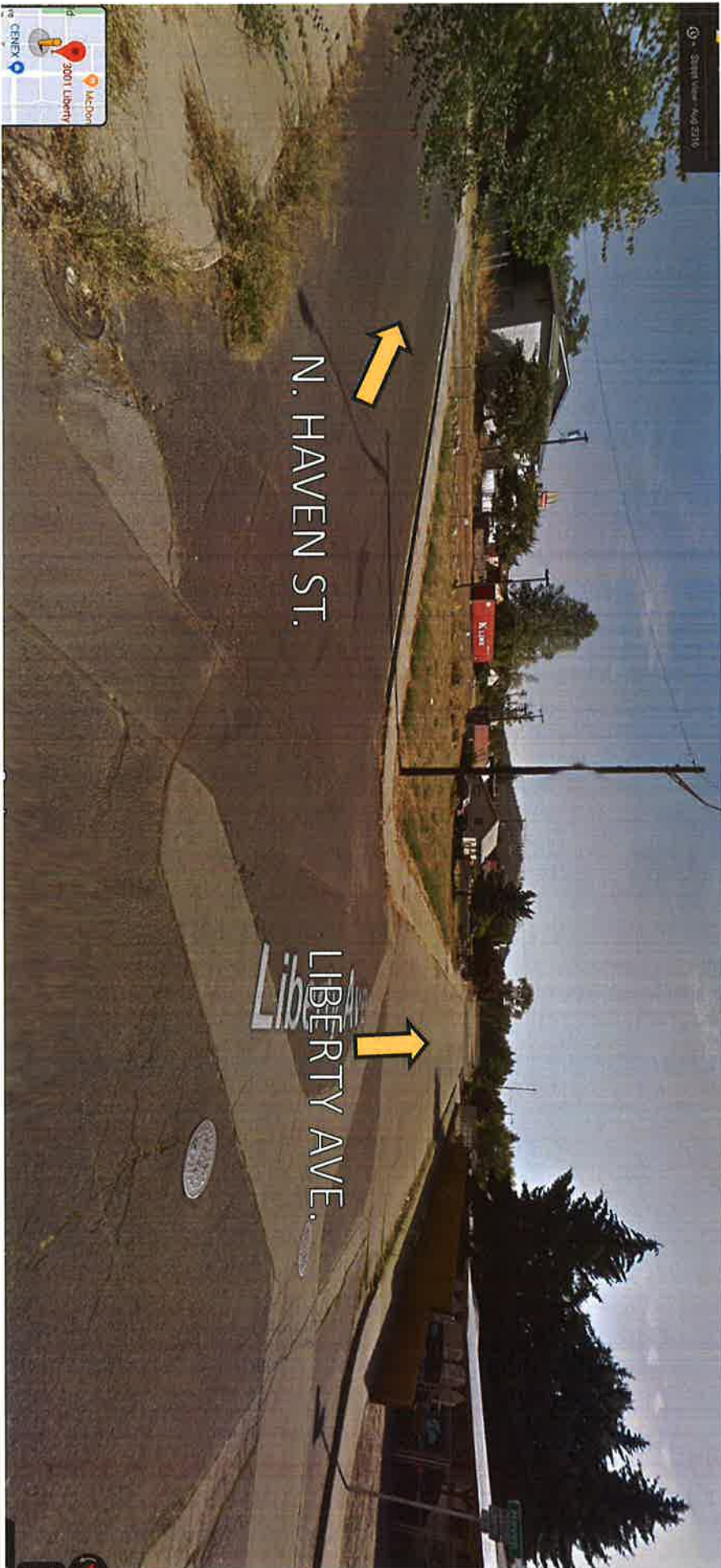


## Liberty Ave. and N. Market St. Intersection



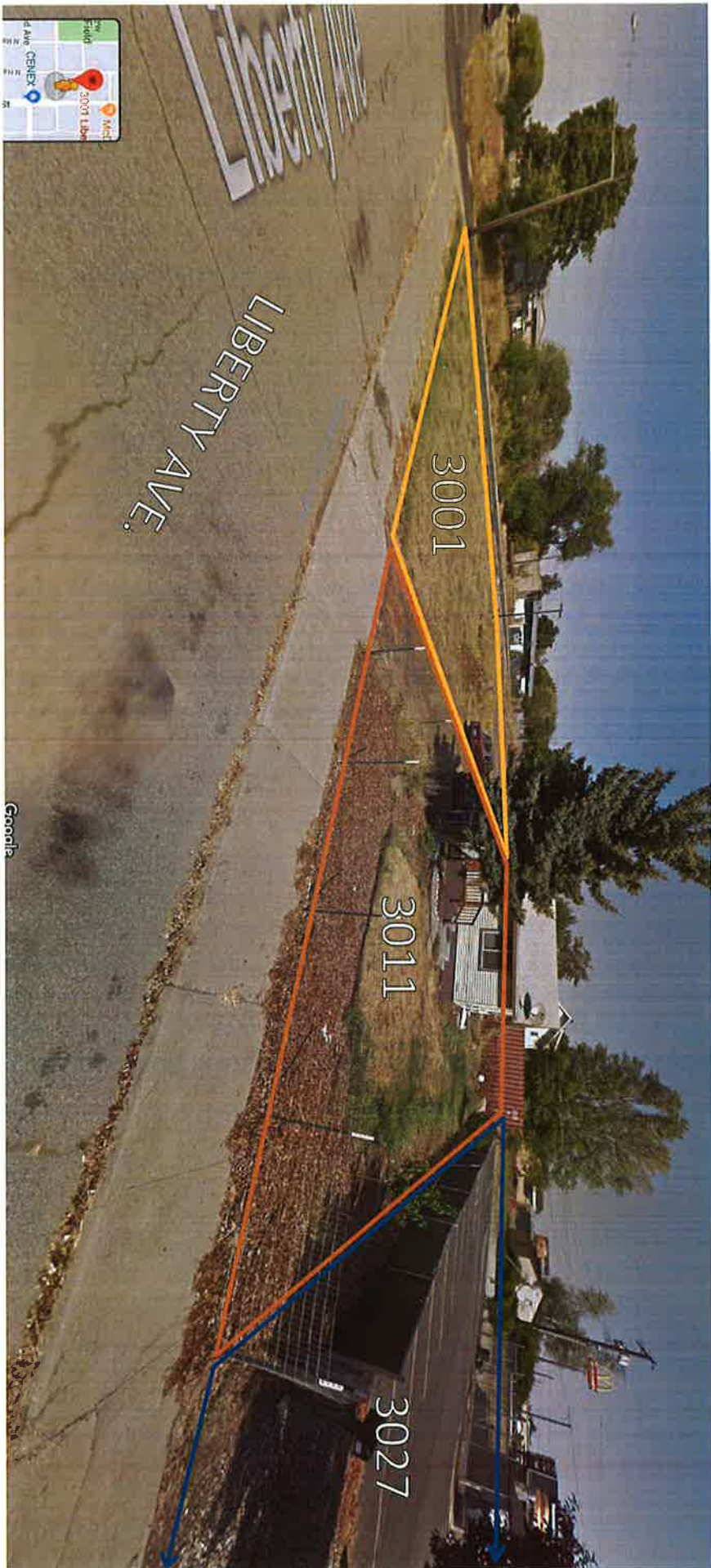


## Liberty Ave. and N. Haven St. Intersection





View of 3001, 3011, and portion of 3027 E. Liberty Ave.





**State Environmental Policy Act (SEPA)  
ENVIRONMENTAL CHECKLIST**

**File No.** \_\_\_\_\_

**PLEASE READ CAREFULLY BEFORE COMPLETING THE CHECKLIST!**

**Purpose of Checklist:**

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

**Instructions for Applicants:**

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

**Use of checklist for nonproject proposals:**

Complete this checklist for nonproject proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



## A. BACKGROUND

1. Name of proposed project: Liberty Avenue Comprehensive Plan Amendment
2. Applicant: Jordan Tampien
3. Address: 915 W. 2<sup>nd</sup> Ave.  
City/State/Zip: Spokane, WA 99201 Phone: (509) 413-1956  
Agent or Primary Contact: Storhaug Engineering  
Address: 510 E. 3<sup>rd</sup> Avenue  
City/State/Zip: Spokane, WA 99202 Phone: (509) 242-1000  
Location of Project: Liberty and Market Avenue  
Address: 3001, 3011, and 3027 E. Liberty Avenue  
Section: (Minnehaha Add L10-11-12B13) 03 Quarter: Southwest Township: 25N Range: 43E Tax  
Parcel Number(s) 35033.1304, 35033.1305, 35033.1306
4. Date checklist prepared: March 16, 2020
5. Agency requesting checklist: City of Spokane
6. Proposed timing or schedule (including phasing, if applicable): December 2020. No phasing  
proposed at this time.
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected  
with this proposal? If yes, explain. No.
- b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. No.
8. List any environmental information you know about that has been prepared, or will be prepared,  
directly related to this proposal. No information at this time. Our next land action is expected in  
2021, at which point additional information will be provided with the subsequent SEPA  
application.



9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. **No pending applications or proposals known at this time.** \_\_\_\_\_

10. List any government approvals or permits that will be needed for your proposal, if known. **Approval of Comprehensive Plan Amendment/Rezone.** \_\_\_\_\_

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC. Individual parcel characteristics are as follows:**

- a. **1304 is 0.29 acres, is currently vacant, and has about 124' of frontage on N. Haven St and about 100' of frontage on E. Liberty Ave.**
- b. **1305 is 0.14 acres, is currently a residence, and has about 50' of frontage on E. Liberty Ave.**
- c. **1306 is 0.43 acres, is currently a restaurant, and has about 150' of frontage on E. Liberty Ave. and about 124' of frontage on N. Market St.**

**Total property characteristics: Area is 0.86 acres, 0.58 acres of which falls in RSF zoning, and total frontage is about 548'.** \_\_\_\_\_



12. Location of the proposal: Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist. \_\_\_\_\_

**The subject property includes 3001, 3011, 3027 E. Liberty Ave. Spokane, WA., which is also Minnehaha Addition, Lots 7-13, Block 13. These lots front the North right-of-way of East Liberty Avenue between North Havana Street and North Market Street and is about 2 blocks East of Andrew Rypien Field.** \_\_\_\_\_

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.) **The proposed action lies within the City of Spokane, the ASA, the GSSA and the PSSA.** \_\_\_\_\_

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

(1) Describe any systems, other than those designed for the disposal of sanitary waste installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities). **Stormwater will be handled in accordance with the City of Spokane standards. Design of a stormwater system has not been completed.** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

(2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored? **No.** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_



(3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems. **Future site development will meet all permitting standards for groundwater protection.** \_\_\_\_\_

(4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater? **No chemical storage is anticipated for use of property.**

b. Stormwater

(1) What are the depths on the site to groundwater and to bedrock (if known)? **Unknown.** \_\_\_\_\_

(2) Will stormwater be discharged into the ground? If so, describe any potential impacts. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site requiring discharge of stormwater.** \_\_\_\_\_

c. ENVIRONMENTAL ELEMENTS

1. Earth

a. General description of the site (check one):

☒ Flat   ☐ Rolling   ☐ Hilly   ☐ Steep slopes   ☐ Mountainous

Other: \_\_\_\_\_

b. What is the steepest slope on the site (approximate percent slope)? **3-8% slopes** \_\_\_\_\_  
\_\_\_\_\_



- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. \_\_\_\_  
**Per the National Web Soil Survey (NRCS), the soil type is 100% Urban Land-Opportunity, disturbed complex, 3-8% slopes.**  
\_\_\_\_\_
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. \_\_  
**None known.** \_\_\_\_\_
- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill: **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site through grading or otherwise.**
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site causing erosion.** \_\_\_\_\_
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt, or buildings)? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the impervious surfacing onsite.** \_\_\_\_\_
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development.**



**The current proposal would not result in any physical change to the site requiring erosion control.**

**2. Air**

- a. What type of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site or any associated emissions.**
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. **None known.** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_
- c. Proposed measures to reduce or control emissions or other impacts to air, if any: **Conformance to all applicable local, state and federal emission control requirements and subordination to Spokane County Air Pollution Control Authority oversight.**  
\_\_\_\_\_



### 3. Water

#### a. SURFACE WATER:

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. **National Wetlands Inventory show no surface water body (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands) within the immediate vicinity of the site.** \_\_\_\_\_
- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. **No**
- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. **No**
- (4) Will the proposal require surface water withdrawals or diversions? If yes, give general description, purpose, and approximate quantities if known. **None known.** \_\_\_\_\_
- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. **No.**
- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. **No.**



b. GROUNDWATER:

- (1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future land actions will take place if zoning is approved and a subsequent SEPA application will be required with more detailed information regarding the development proposal. Water is currently supplied by City of Spokane.**
- 

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future land actions will take place if zoning is approved and a subsequent SEPA application will be required with more detailed information regarding the development proposal. Sewer is currently supplied by City of Spokane.**
- 

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site requiring stormwater treatment.**
- (2) Could waste materials enter ground or surface waters? If so, generally describe. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development.**



**The current proposal would not result in any physical change to the site affecting infiltration.**

(3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site affecting area stormwater.**

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site affecting stormwater.**\_\_\_\_\_



#### 4. Plants

- a. Check the type of vegetation found on the site:

Deciduous tree: ☐ alder ☐ maple ☐ aspen

Other: Plum, Cherry

Evergreen tree: ☐ fir ☐ cedar ☐ pine

Other: Spruce

☒ Shrubs ☒ Grass ☐ Pasture ☐ Crop or grain

☐ Orchards, vineyards or other permanent crops

Wet soil plants: ☐ cattail ☐ buttercup ☐ bullrush ☐ skunk cabbage

Other: \_\_\_\_\_

Water plants: ☐ water lily ☐ eelgrass ☐ milfoil

Other: \_\_\_\_\_

Other types of vegetation: weeds, burning bush, potentilla, juniper

- b. What kind and amount of vegetation will be removed or altered? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site affecting vegetation.**
- c. List threatened and endangered species known to be on or near the site. **None known.**
- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: **None.**



- e. List all noxious weeds and invasive species known to be on or near the site. **None known.** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**5. Animals**

- a. Check and List any birds and other animals which have been observed on or near the site or are known to be on or near the site:

Birds: ☐ hawk ☐ heron ☐ eagle ☐ songbirds

Other: **Typical of an urban setting** \_\_\_\_\_

Mammals: ☐ deer ☐ bear ☐ elk ☐ beaver

Other: **Typical of an urban setting** \_\_\_\_\_

Fish: ☐ bass ☐ salmon ☐ trout ☐ herring ☐ shellfish

Other: \_\_\_\_\_

Other (not listed in above categories): \_\_\_\_\_  
\_\_\_\_\_

- b. List any threatened or endangered animal species known to be on or near the site.

**None known. Site is an existing urbanized area.** \_\_\_\_\_  
\_\_\_\_\_

- c. Is the site part of a migration route? If so, explain. **No.** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

- d. Proposed measures to preserve or enhance wildlife, if any: **None proposed. Maintaining native species where feasible.** \_\_\_\_\_  
\_\_\_\_\_



- e. List any invasive animal species known to be on or near the site. **None known.** \_\_\_\_\_  
\_\_\_\_\_

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future land actions will take place if zoning is approved and a subsequent SEPA application will be required with more detailed information regarding the development proposal. Existing electrical and gas utilities are available and would require no extensions.** \_\_\_\_\_  
\_\_\_\_\_

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. **No.** \_\_\_\_\_  
\_\_\_\_\_

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: **None.** \_\_\_\_\_

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. **None known.** \_\_\_\_\_



- (1) Describe any known or possible contamination at the site from present or past uses. **None known.** \_\_\_\_\_
- (2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. **None known.** \_\_\_\_\_
- (3) Describe any toxic or hazardous chemicals/conditions that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. **None known.** \_\_\_\_\_
- (4) Describe special emergency services that might be required. **All applicable State and Federal regulations will be followed. However, no additional special emergency services are known to be required.** \_\_\_\_\_
- (5) Proposed measures to reduce or control environmental health hazards, if any: **None.**
- \_\_\_\_\_

b. NOISE:

- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? **There is noise associated with traffic along N. Market St. and Liberty Ave., but it is not expected to impact the project.** \_\_\_\_\_
- \_\_\_\_\_
- \_\_\_\_\_
- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development.**



**The current proposal would not result in any physical change to the site impacting area noise levels.**

(3) Proposed measure to reduce or control noise impacts, if any: **None.**

**8. Land and shoreline use**

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. **To the north, east, south and west of site are a mixture of light industrial uses and single-family residential. The proposal area is adjacent to several RSF zoned parcels, and it is currently zoned RSF. However, the proposal is also surrounded by many GC-70 zoned parcels, which form a corridor of GC in the area.**
- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? **No.**
- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: **No.**
- c. Describe any structures on the site. **There exists one single-family home on site.**
- d. Will any structures be demolished? If so, which? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site affecting existing structures.**
- e. What is the current zoning classification of the site? **RSF (Residential Single-Family) and GC-70**



- f. What is the current comprehensive plan designation of the site? **The City's Land Use Plan designation is R 4-10 and General Commercial.**
- g. If applicable, what is the current shoreline master program designation of the site? **N/A**
- h. Has any part of the site been classified as a critical area by the city or the county? If so, specify.  
**No.**
- i. Approximately how many people would reside or work in the completed project? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any change to dwelling or employment on the site.**
- j. Approximately how many people would the completed project displace? **There is currently one single-family home with one tenant renting on a month-to-month lease. The current proposal will not cause any displacement.**

Proposed measures to avoid or reduce displacement impacts, if any: **No proposed measures at this time.**

- k. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: **Compliance with all applicable development standards.**
- l. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any: **N/A**

## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future land actions will take place if zoning is approved and a subsequent SEPA application will be required with more detailed information regarding the development proposal.**



- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. **The current proposal would not result in any change to the site regarding residential units.**
- c. Proposed measures to reduce or control housing impacts, if any: **None.**

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future development would conform to the GC-70 zone to which these parcels would be added and building heights would be 70' or less.**
- 
- b. What views in the immediate vicinity would be altered or obstructed? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to area views.**
- 
- c. Proposed measures to reduce or control aesthetic impacts, if any: **The development will conform to the applicable zoning, building, safety and fire codes.**
- 

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any change in light glare.**
- 
- b. Could light or glare from the finished project be a safety hazard or interfere with views? **No impact or interference is anticipated.**
-



- 
- c. What existing off-site sources of light or glare may affect your proposal? **None known.** \_\_\_\_\_

- 
- d. Proposed measures to reduce or control light and glare impacts, if any: **None.** \_\_\_\_\_

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? **Spokane Youth Sports Association (Andrew Rypien Field) is located 0.2 miles directly west of the site. Esmeralda Golf Course is located 0.8 miles to the NE. Minnehaha Park is located 0.8 miles to the east. Courtland Park is located 0.6 miles to the NW. Hays Park is located 1 mile to the NW. Wildhorse Park is located 0.6 miles to the north.**

- 
- b. Would the proposed project displace any existing recreational uses? If so, describe. **No.** \_\_\_\_\_

- 
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: **None.**

## 13. Historic and cultural preservation

- a. Are there any buildings, structures, or sites, located on or near the sited that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. **None known.**



- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. None known.
- 
- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archaeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. Future land actions will take place if zoning is approved and a subsequent SEPA application will be required with more detailed information regarding the development proposal. All required measures shall be undertaken in the event of future development.
- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required None.
- 

#### 14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. Currently access is provided from N. Haven St., E. Liberty Ave., and N. Market St. No information on proposed future access at this time. Additional information will be provided with the subsequent SEPA application.
- b. Is site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop. The subject site is served by public transit. The closest transit stop is Market @ Euclid Bus Stop 0.1 mile south of site.
- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site, which currently has 26 parking spaces.



- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any physical change to the site regarding transportation.**
- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. **No.** \_\_\_\_\_
- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and non-passenger vehicles). What data or transportation models were used to make these estimates? **This SEPA application is tied to a comprehensive plan amendment from RSF to GC for a multi-family development. The current proposal would not result in any vehicular trip changes.** (Note: to assist in review and if known, indicate vehicle trips during PM peak, AM Peak, and Weekday (24 hours).)
- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, general describe. **No.** \_\_\_\_\_
- h. Proposed measures to reduce or control transportation impacts, if any: **Future development on the site would be subject to City of Spokane traffic impact fees, providing for transportation improvements where necessary.**

## 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. **The project is currently served by City of Spokane Fire District and School District #82 public schools. Future development would require service commensurate with typical General Commercial uses.**
- b. Proposed measures to reduce or control direct impacts on public services, if any: **None currently proposed.** \_\_\_\_\_



## 16. Utilities

a. Check utilities currently available at the site:

- ☒ electricity
- ☒ natural gas
- ☒ water
- ☒ refuse service
- ☒ telephone
- ☒ sanitary sewer
- ☐ septic system

Other: \_\_\_\_\_  
\_\_\_\_\_

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed:

**Water: Water in this area is under the jurisdiction of the City of Spokane.**

**Sewer: Sanitary services provided by the City of Spokane**

**Gas/Power: Avista** \_\_\_\_\_



### C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/10/2020

Signature:



**Please Print or Type:**

**Jordan Tampien**

Proponent: Jordan Tampien

Address: 915 West 2nd Avenue  
Spokane, WA 99201

Phone: 509-413-1956

Person completing form (if different from proponent): Alex Durkin - Storhaug Engineering

Phone: 509-242-1000

Address: 510 East 3rd Avenue  
Spokane, WA 99202

#### FOR STAFF USE ONLY

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



#### D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

*(Do not use this sheet for project actions)*

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise? **The proposal would not directly increase discharge to water, emissions to air, the production and storage of toxic or hazardous substances or noise.** \_\_\_\_\_

Proposed measures to avoid or reduce such increases are: **No such measures are proposed at this time.** \_\_\_\_\_

2. How would the proposal be likely to affect plants, animals, fish or marine life? **The Spokane Municipal Code includes standards related to protection of critical areas and habitat. No additional measures are proposed to specifically address the conservation of plants and animals with this proposal.** \_\_\_\_\_

Proposed measures to protect or conserve plants, animals, fish or marine life are: **No such measures are proposed at this time.** \_\_\_\_\_

3. How would the proposal be likely to deplete energy or natural resources? **The proposed comprehensive plan amendment will not directly affect energy or natural resources.** \_\_\_\_\_

Proposed measures to protect or conserve energy and natural resources are: **N/A** \_\_\_\_\_



4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands? **No Known environmentally sensitive areas exist on or in the vicinity of the site. The proposed comprehensive plan amendment will not directly affect environmentally sensitive areas. New development would be subject to the critical area standards of the SMC.**

Proposed measures to protect such resources or to avoid or reduce impacts are: **No additional measures are proposed. Project impacts will be addressed at the time of permit application in accordance with the standards of the SMC.**

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans? **The project site is outside any shoreline areas**

Proposed measures to avoid or reduce shoreline and land use impacts are: **None**

6. How would the proposal be likely to increase demands on transportation or public services and utilities? **The proposal site is within a developed urban area in the City of Spokane, and already has access to water, sewer, public roads, and emergency services. Additional demands on transportation or public services and utilities would be addressed at the time of development permit approval as required by existing regulations.**

Proposed measures to reduce or respond to such demand(s) are: **No additional measures are proposed at this time.**

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment. **The proposal does not conflict with local, state or federal laws or requirements for protection of the environment.**



C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/10/2020

Signature: \_\_\_\_\_



**Please Print or Type:**

**Jordan Tampien**

Proponent: Jordan Tampien

Address: 915 West 2nd Avenue  
Spokane, WA 99201

Phone: 509-413-1956

Person completing form (if different from proponent): Alex Durkin - Storhaug Engineering

Phone: 509-242-1000

Address: 510 East 3rd Avenue  
Spokane, WA 99202

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- A. ☒ there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- B. ☐ probable significant adverse impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- C. ☐ there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z19-499COMP

**PROPONENT:** Jordan Tampien, LibertyProject LLC (Agent: Alex Durkin, Storhäg Engineering)

**DESCRIPTION OF PROPOSAL:** Amendment of the Land Use Plan Map designation for three parcels totaling 0.85 acres from "Residential 4-10" to "General Commercial" and a concurrent change of zoning from "Residential Single Family (RSF)" to "General Commercial (GC-70)." No specific development proposal is being approved at this time.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns three parcels: 35033.1304, 35033.1305, and part of 35033.1306. These parcels are located at 3001, 3022, and 3207 E Liberty Avenue. All three comprise the south half of the block on the north side of E Liberty Avenue, between N Haven Street and N Market Street in the Bemiss Neighborhood.

Legal Description: Lots 7 through 12, Block 13, Minnehaha Addition to the City of Spokane in Section 3, Township 25N, Range 43E.

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

- [ ] There is no comment period for this DNS.
- [ ] This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.
- [ X ] This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020

**Signature:**

DocuSigned by:  
Louis Meuler

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*



**From:** [Eliason, Joelle](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Brown, Eldon](#); [Nilsson, Mike](#); [Kells, Patty](#); [West, Jacque](#); [Johnson, Erik D.](#)  
**Subject:** Regarding RFC Z19-499COMP Liberty & Haven  
**Date:** Tuesday, May 5, 2020 2:31:05 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Kevin,

Thank you for the opportunity to review and comment on the proposed zoning change on the NE corner of E Liberty Ave and N Haven St. Development Services has no objection to the proposed zoning change from Residential Single Family to General Commercial. Further comments regarding the design, any potential utility conflicts, stormwater management, etc. will be handled outside of this Comp Plan Amendment and associated SEPA.

Sincerely,  
Joelle Eliason

*I will be working remotely until further notice and will respond to emails as quickly as possible. Thank you for your patience!*



Joelle Eliason | City of Spokane | Development Services Center  
509.625.6385 | fax 509.625.6013 | [jeliason@spokanecity.org](mailto:jeliason@spokanecity.org)







**Spokane Tribe of Indians**  
Tribal Historic Preservation Office  
P.O. Box 100 Wellpinit WA 99040

May 5, 2020

**TO: Kevin Freibott, Assistant Planner**

**RE: Z19-499COMP**

Mr. Freibott,

Thank you for contacting the Tribe's Historic Preservation Office. We appreciate the opportunity to provide a cultural consent for your project.

We have reviewed your project forwarded to our office; we are concerned that the project area potentially contains cultural resources which would be impacted by the proposed ground disturbing activity, and is a high-risk area for archeological sites and human remains.

**Recommendation:** Cultural Survey, Sub-surface testing.

Once the survey / sub-surface testing is completed we will do more mitigation to discuss the plan of action if cultural sites are identified during the cultural survey.

However, if any artifacts or human remains are found upon excavation activity this office is to be notified and the immediate area cease

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist us in protecting our shared heritage.

If questions arise, please contact me at 258-4222

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306      File No 219-499 comp

Thank you,

Signature:  \_\_\_\_\_

Address: 2917 E Liberty  
Spokane WA 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File # 219-499 Comp

Thank you,

Signature: Kathy Kelly

Address: 3228 N Haven  
Spokane wt 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File # 219-499 Comp

Thank you,

Signature:

Joseph J. Sperry

Address:

3022 N. HAVEN  
SPOKANE, WA. 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File # 219-499 Comp

Thank you,

Signature: Kevin Hettlinger

Address: 2911 E Bridgeport



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File # 219-499 Comp

Thank you,

Signature: Nicholas Edward Merrill

Address: 3007 E. Euclid Ave  
Spokane, WA 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

file # 219-499 Comp

Thank you,

Signature: Nicholas Edward Merrill

Address: 3017 E EUCLID AVE  
SPOKANE WA 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

file no 219-499 COMB

Thank you,

Signature: Charmian Standish

Address: 2904 E Liberty  
Spokane WA 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File #719-499Comp

Thank you,

Signature: \_\_\_\_\_

A handwritten signature in blue ink, appearing to read "Doug Parker", written over a horizontal line.

Address: \_\_\_\_\_

2903 E Brigant Ave  
Spokane WA 99207.



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

File # 219-499 comp

Thank you,

Signature: \_\_\_\_\_

*Ronald Wise*

Address: \_\_\_\_\_

*3924 E  
Liberty*




July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306      file # 219-499 Comp

Thank you,

Signature:   
ANTHONY A NEMES

Address: 2923 E Bridgeport  
Spokane WA 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002, & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

file # 219-499 Comp

Thank you,

Signature:



Address:

2927 E Euclid Ave  
Spokane 99207



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306      File # 219-499 Comp.

Thank you,

Signature: Mark A. Pett

Address: 2908 E Bridgeport Ave

\_\_\_\_\_



July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

file # 219-499 comp

Thank you,

Signature: \_\_\_\_\_

*Mark P. [unclear] II*

Address: \_\_\_\_\_

*3305 N Haven St  
Spokane, WA 99207*



July 18, 2020

File No: 219-499COMP

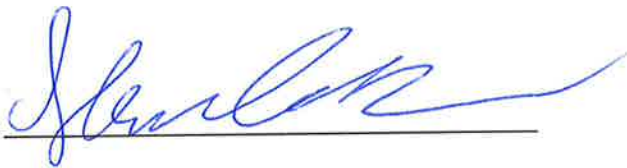
I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

Thank you,

Signature: \_\_\_\_\_



Address: \_\_\_\_\_

2928 e Liberty ave  
Spokane WA 99207



File No: 219-499 COMP

July 18, 2020

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

Thank you,

Signature:

Barbara Silvey

Address:

2927 E. Bridgeport  
Spokane, WA 99207



July 18, 2020

File No. 219-499 COMP

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

Thank you,

Signature: \_\_\_\_\_



Address: \_\_\_\_\_

2907 E Bridgeport Ave



July 18, 2020

File No: 219-499COMP

I am opposed to making the following properties General Commercial properties:

3001, 3002. & 3027 E Liberty, Spokane, WA, 99207.

Parcels: 35033.1304, 35033.1305, 35033.1306

Thank you,

Signature: Elaine Bush

Address: 2924 E Bridgeport Ave  
Spokane WA 99207





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-499COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “General Commercial” for a 0.85-acre area located at 3001, 3011, and 3027 E Liberty Avenue. The implementing zoning designation requested is General Commercial with 70-foot height limit (GC-70).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-499COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a 0.85-acre area located at 3001, 3022, 3027 E Liberty Avenue (the “Properties”) from “Residential 4-10” to “General Commercial” with a corresponding change in zoning from Residential Single Family (RSF) to General Commercial with a 70-foot height limit (GC-70).
- E. The owner of the Properties also owns the parcel immediately to the east of the properties, resulting in common ownership holding that spans the area between N Haven Street and N Market Street.
- F. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- G. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- H. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, and including the Application in the Work Program.
- I. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. The City received two comment letters regarding the Application, one from the Spokane Development Services Center, stating no concerns, and one from the Spokane Tribe, requesting a site survey be conducted for cultural resources prior to any development.



- J. A Notice of Application was published on June 8, 2020 in the Spokesman Review and was mailed to all properties and owners within a 400-foot radius of the subject Properties and any adjacent properties with the same ownership. Signs were also placed on the subject Properties in plain view of the public. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City received comment cards expressing general opposition to the application.
- K. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and was provided with information regarding the dates of Plan Commission workshops and hearings.
- L. On June 10, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- M. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- N. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
- O. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- P. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of a SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- Q. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the Properties and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject Properties.
- R. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Application recommended approval of the Application.
- S. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- T. Two members of the public testified in opposition of the Application during the hearing on September 9, 2020, citing concerns about intrusion of higher intensity and/or non-residential uses into a single family neighborhood.



- U. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- V. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the "Staff Report").
- W. The Plan Commission finds that the proposal meets the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.8 concerning the establishment of General Commercial land uses in the City.
- X. The Plan Commission finds that the proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-499COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review application was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Application is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Application is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Application.



9. The Application will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Application proposes a land use designation that is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment and site is suitable for the proposed designation.
12. The map amendment would implement applicable comprehensive plan policies better than the current map designation.

#### **RECOMMENDATIONS:**

In the matter of Z19-499COMP, a request by Liam Taylor of Storhäug Engineering on behalf of Liberty Project LLC to change the land use plan designation on 0.85 acres of land from "Residential 4-10" to "General Commercial" with a corresponding change of the implementing zoning to GC-70 (General Commercial, 70-foot height limit), as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to the Land Use Plan Map of the City's Comprehensive Plan with corresponding amendment to the City's Zoning Map, and authorizes the President to prepare and sign on the Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the application.



---

**Todd Beyreuther, President**

Spokane Plan Commission  
October 21, 2020



**From:** [Freibott, Kevin](#)  
**To:** [NICK MERRILL](#)  
**Cc:** [Black, Tirrell](#)  
**Subject:** RE: Staff Report Z19-499COMP  
**Date:** Tuesday, September 15, 2020 1:51:00 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[Hearing SEPA Notice For Mailing - Z19-499COMP.pdf](#)  
[SEPA DNS - Z19-499COMP.pdf](#)

NOTE: The following comment was received after completion of the Staff Report and after the Plan Commission closed the public record for their consideration of this proposal.

---

Good morning, Mr. Merrill. As the staff member processing the Comprehensive Plan Amendments this year, Director Meuler thought I might best be able to answer your question. All nine applications proposed this year have been reviewed for their potential environmental effects, including the application on Liberty Ave (Z19-499COMP). The SEPA checklist submitted by the applicant can be reviewed on the project website here:

<https://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/liberty-avenue/>

I've attached the Determination of Non-Significance for this project that was issued in August. SEPA for Comprehensive Plan Amendments is a little different, in that they are considered "non-project actions" and thus a lot of the physical effects of development (noise, traffic) are considered at a high level and only in comparison to what *could* be developed between the two land use designations. In the case of this project, we have to assume that the noise mitigating requirements ([SMC 10.08D](#)) and traffic impact fees ([SMC 17D.075](#)) of the municipal code will serve to reduce any such impacts. Rest assured, if and when the property owner decides to develop, those plans will be analyzed for their noise and traffic impacts again, along with the other development-specific impacts that can occur.

As the Plan Commission has closed the public record while they deliberate, they cannot accept any new comments. However, City Council is still set make the final decision on these applications later this year. I will share your email below with City Council, so they may consider your concerns in light of the application. In the meantime, if you have any additional questions please don't hesitate to ask. Thanks and have a great day!

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



---

**From:** NICK MERRILL <n.merrill@comcast.net>



**Sent:** Saturday, September 12, 2020 11:52 PM

**To:** Meuler, Louis <lmeuler@spokanecity.org>

**Cc:** Dianne Merrill <dianne.merrill@comcast.net>; Patty Speranzi <pjsperanzi@centurylink.net>

**Subject:** RE: Staff Report Z19-499COMP

[CAUTION - EXTERNAL EMAIL - Verify Sender]

Louis,

My name is Nick Merrill and I live at 3017 E Euclid Avenue, one block south of the proposed Liberty Avenue rezone. I, along with the other 2 residential property owners (Kathy Kelly at 3228 N Haven St, and, Pat and Joe Speranzi at 3222 N Haven St.) on this block are on record as being opposed to this project. The NSC project has brought more traffic and noise to our neighborhood and I feel that we don't need 20-30 more cars travelling around the block on a daily basis adding to it. Market & Euclid is the noisiest it has ever been and I have lived in this one block for 70+ years. In fact, if anything, a 'traffic calming' project for Market/Euclid would be more than welcome around here. The reclassification of those lots will bring nothing of good value or benefit to those of us who live on the residential lots around the project area. I don't know if this will do any good, but, we need to start restricting traffic, not adding traffic.

I have a question – I haven't seen anything about an environmental impact statement, or, any other environmental reports concerning this project – maybe a report on noise and pollution - is there a link or something you can provide so we could see the conclusions?

Thanks for your time and consideration.

Nick Merrill  
3017 E Euclid Ave  
Spokane, WA 99207  
509-499-5692  
[n.merrill@comcast.net](mailto:n.merrill@comcast.net)

Sent from [Mail](#) for Windows 10





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                              |            |
|------------------------------|------------|
| <b><u>Date Rec'd</u></b>     | 10/26/2020 |
| <b><u>Clerk's File #</u></b> | ORD C35973 |
| <b><u>Renews #</u></b>       |            |

|                                  |                                                      |                             |             |
|----------------------------------|------------------------------------------------------|-----------------------------|-------------|
| <b><u>Submitting Dept</u></b>    | PLANNING                                             | <b><u>Cross Ref #</u></b>   |             |
| <b><u>Contact Name/Phone</u></b> | KEVIN FREIBOTT 625-6184                              | <b><u>Project #</u></b>     | Z19-501COMP |
| <b><u>Contact E-Mail</u></b>     | KFREIBOTT@SPOKANECITY.ORG                            | <b><u>Bid #</u></b>         |             |
| <b><u>Agenda Item Type</u></b>   | First Reading Ordinance                              | <b><u>Requisition #</u></b> |             |
| <b><u>Agenda Item Name</u></b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – NEVADA DECATUR |                             |             |

**Agenda Wording**

An Ordinance relating to application Z19-501COMP by the Francis Nevada Partnership, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to General Commercial for 0.51 acres and a change to the Zoning Map from RSF to CB-55.

**Summary (Background)**

The proposal concerns 6204 N Nevada Street and 1015 E Decatur Avenue, parcels 36321.0209 and 36321.0210. This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended denial of the amendment.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |

|         |    |   |
|---------|----|---|
| Neutral | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |

|                                    |                   |                                     |                           |
|------------------------------------|-------------------|-------------------------------------|---------------------------|
| <b><u>Approvals</u></b>            |                   | <b><u>Council Notifications</u></b> |                           |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS     | <b><u>Study Session\Other</u></b>   | CC Study Session 10-29-20 |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M. | <b><u>Council Sponsor</u></b>       | CM Mumm                   |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY   | <b><u>Distribution List</u></b>     |                           |
| <b><u>Legal</u></b>                | RICHMAN, JAMES    | tblack@spokanecity.org              |                           |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL   | jrichman@spokanecity.org            |                           |
| <b><u>Additional Approvals</u></b> |                   | sbishop@spokanecity.org             |                           |
| <b><u>Purchasing</u></b>           |                   | dhume@spokane-landuse.com           |                           |
|                                    |                   | lmeuler@spokanecity.org             |                           |
|                                    |                   |                                     |                           |
|                                    |                   |                                     |                           |



Ordinance No. C35973

AN ORDINANCE RELATING TO APPLICATION FILE Z19-501COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "GENERAL COMMERCIAL" FOR APPROXIMATELY 0.51 ACRES LOCATED AT 6204 NEVADA STREET AND 1015 E DECATUR AVENUE (PARCELS 36321.0209 AND 36321.0210) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "COMMUNITY BUSINESS (CB-55)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-501COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-501COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 0.51 acres from "Residential 4-10" to "General Commercial"; if approved, the implementing zoning destination requested is "Community Business (CB-55)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-501COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-501COMP is inconsistent with and does not implement the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-501COMP does not meet the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend denial of Application Z19-501COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-501COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "General Commercial" for 0.51 acres, as shown in Exhibits A and B.
3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from "Residential Single Family" to "Community Business (CB-55)," as shown in Exhibits C and D.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





**Z19-501COMP**  
**(6204 N Nevada St & 1015 E Decatur Ave)**  
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

**EXHIBIT A: Existing Land Use Plan Map**



**Legend**

Subject Parcels

Same Owner

Parcel

Curb Line

**Land Use Plan Designation**

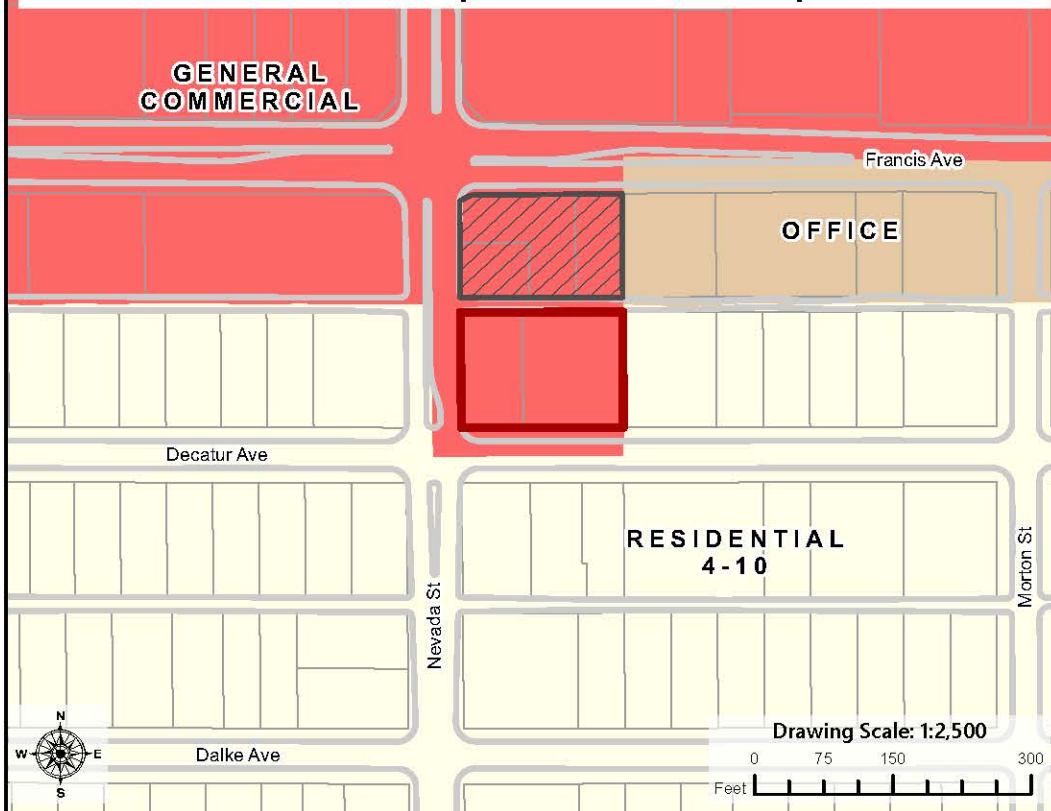
Residential 4-10

Office

General Commercial

Acres (Proposal): 0.51  
Acres (Adjacent): 0.45

**EXHIBIT B: Proposed Land Use Plan Map**



**Legend**

Subject Parcels

Same Owner

Parcel

Curb Line

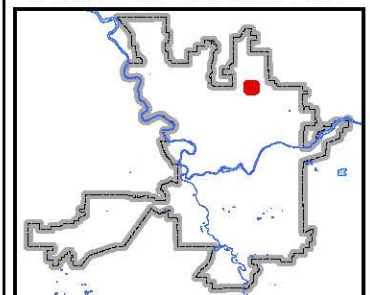
**Proposed Land Use**

Residential 4-10

Office

General Commercial

**PROJECT LOCATION**



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# Z19-501COMP (6204 N Nevada St & 1015 E Decatur Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

### Current Zoning

Community Business (CB)

General Commercial (GC)

Light Industrial (LI)

Office (O)

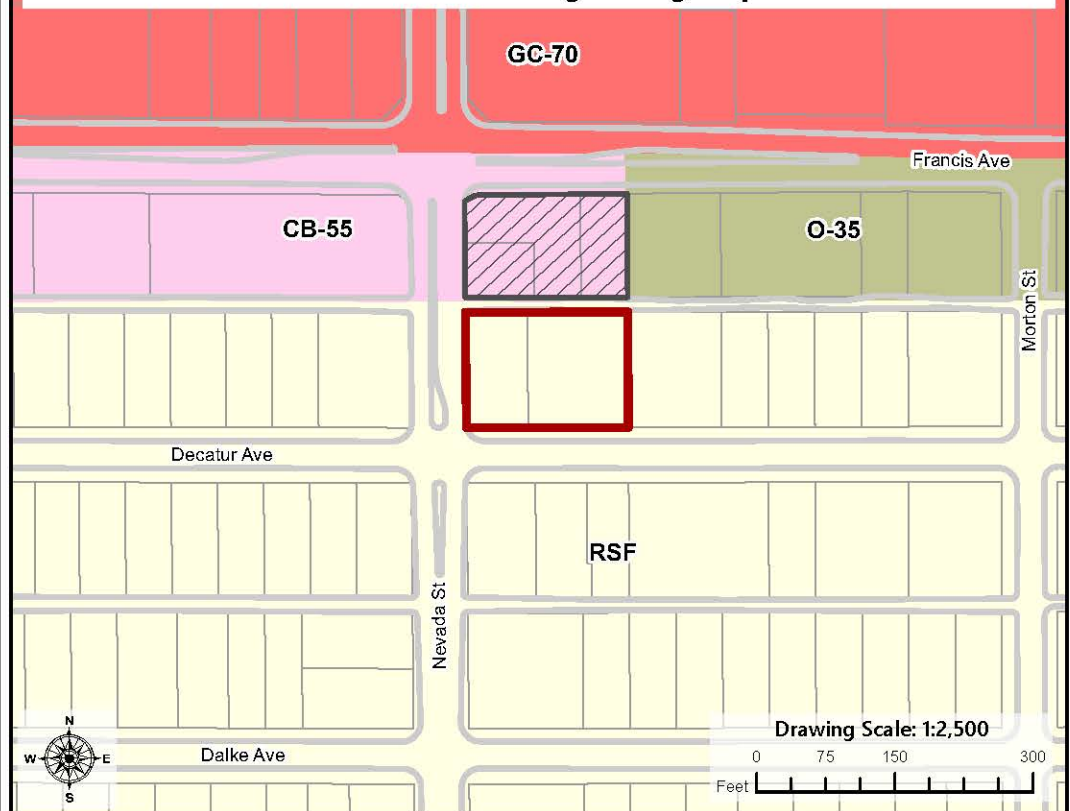
Residential Single-Family (RSF)

*Numbers after a Zone Label  
denote the height limits in that  
area.*

Acres (Proposal): 0.51

Acres (Adjacent): 0.45

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Same Owner

Curb Line

Parcel

### Proposed Zoning

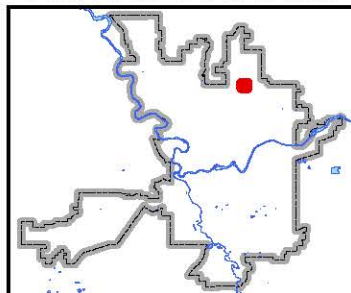
Community Business (CB)

General Commercial (GC)

Office (O)

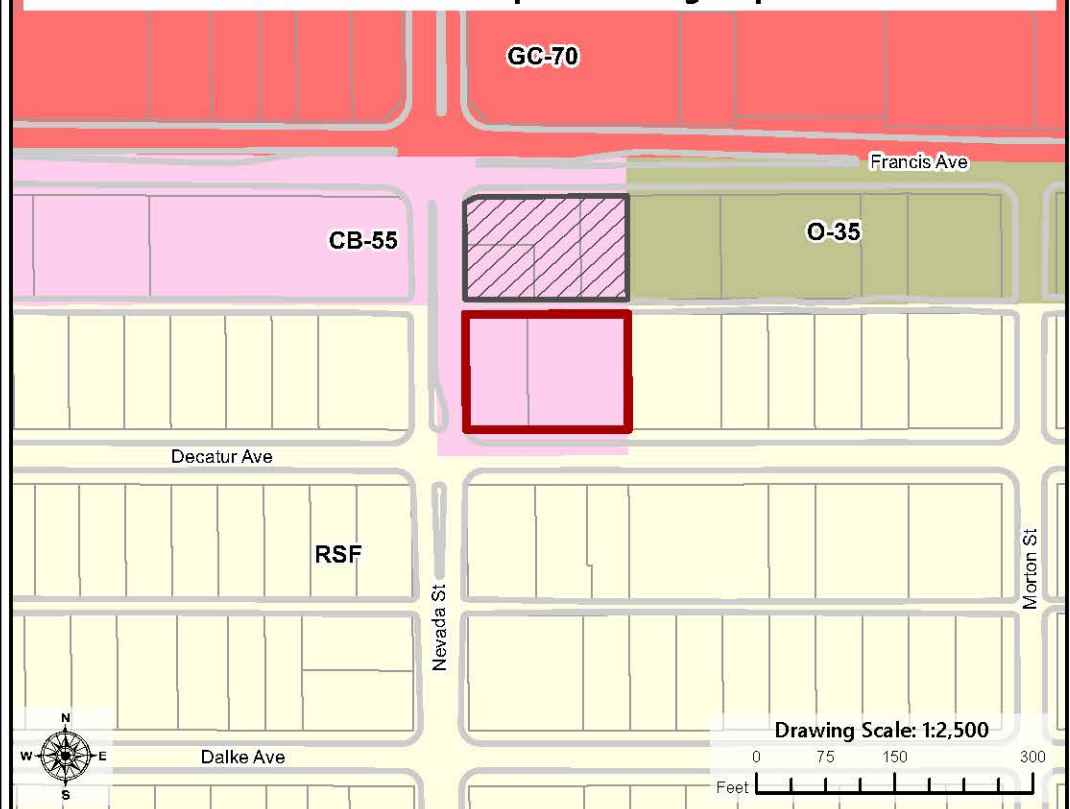
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





**Exhibit E: Legal Description**

Lots 13 through 16, Block 1, Lancaster's 2<sup>nd</sup> Addition in the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-501COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map (Map LU1) designation and zoning of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                       |
|---------------------------|-----------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 36321.0209 and 36321.0210                                             |
| <b>Address(es):</b>       | 6204 N Nevada Street and 1015 E Decatur Avenue                        |
| <b>Property Size:</b>     | 0.51 acres (area of change), 0.45 adjacent acres in common ownership. |
| <b>Legal Description:</b> | Lots 13 through 16, Block 1, Lancasters 2 <sup>nd</sup> Addition      |
| <b>General Location:</b>  | Northeast of the Intersection of N Nevada Street and E Decatur Ave.   |
| <b>Current Use:</b>       | Two single-family residences.                                         |

**II. APPLICANT SUMMARY**

|                        |                                                 |
|------------------------|-------------------------------------------------|
| <b>Agent:</b>          | Dwight Hume, Land Use Solutions and Entitlement |
| <b>Applicant:</b>      | Francis Nevada Partnership                      |
| <b>Property Owner:</b> | Francis Nevada Partnership                      |

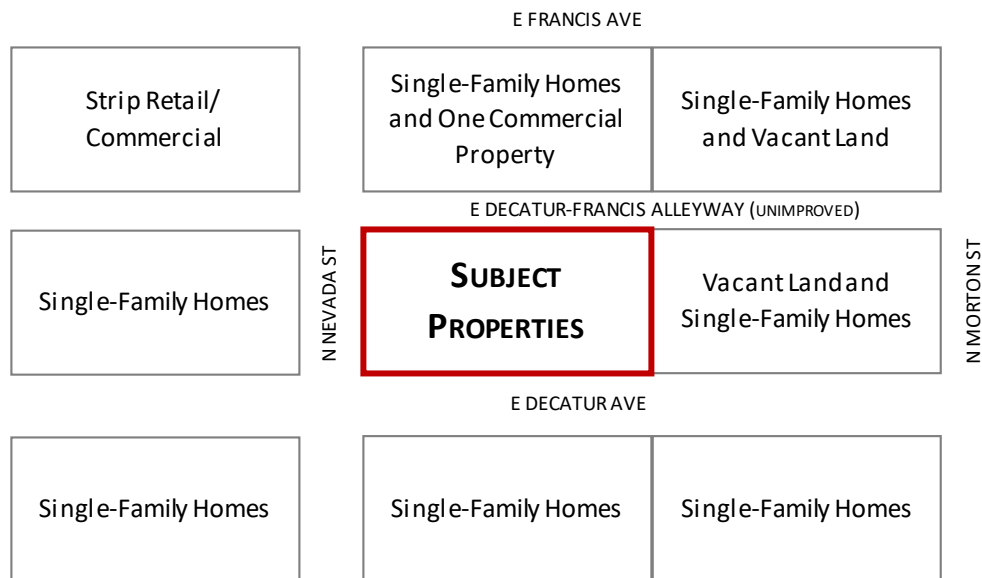
**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 DUs/Acre (R 4-10)                                                                                                          |
| <b>Proposed Land Use Designation:</b> | General Commercial (GC)                                                                                                                     |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | Community Business, 55-foot height limit (CB-55)                                                                                            |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | None                                                                                                                                        |



## IV. BACKGROUND INFORMATION

1. **General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the Nevada Heights neighborhood. The intent of the applicant is to potentially develop non-residential uses on both subject lots, all in common ownership by the applicant.
2. **Site Description and Physical Conditions:** The proposal concerns two parcels: 36321.0209 and 36321.0210. Both are located on the southern half of the block. Each of the two subject parcels contains a single-family home, currently rented out. Other site improvements are typical for residential homes, including fences, landscaping, and garden sheds. There is an unpaved, ad hoc drive aisle situated on the eastern limit of parcel 36321.0210 which provides access to the commercial properties to the north. To the north of the subject parcels are three parcels owned by the same entity, the Francis Nevada Partnership. A City-owned alley separates the two subject parcels from the three parcels to the north in common ownership.
3. **Property Ownership:** All of the subject properties are owned by the same owner, listed by the County Assessor as the “Francis Nevada Partnership”.
4. **Adjacent Property Improvements and Uses:** The proposal is surrounded by existing development of the following nature:



5. **Street Class Designations:** N Nevada Street and E Francis Avenue are designated as Major Arterials. E Decatur Avenue is designated as a local street and the alleyway to the north of the subject properties is designated as an Alleyway in the City’s Arterial Street Map. These designations match the current designation in map TR-12, the Arterial Network Map, indicating that the City does not intent for these classes to change. Likewise, no change of street class designation is proposed by the applicant.



6. **Current Land Use Designation and History:** As shown in **Exhibit A**, the subject properties are currently designated on the Land Use Plan Map as “Residential 4-10” (between 4 and 10 dwelling units per acre). The subject properties have been designated for these uses since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001.
7. **Proposed Land Use Designation:** As shown in **Exhibit B**, the proposal is to amend the Land Use Plan Map designation so that the entirety of the subject properties are designated for General Commercial uses.
8. **Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF). This zoning has been the same since the current zoning map was adopted in 2006. Historically, the subject properties have been zoned for single-family residential (or similar) since at least 1958.

Properties to the north and northwest were residentially zoned in 1958, but over time they have slowly become zoned for commercial uses. Commercial zoning along E Francis Avenue has generally progressed from Division St eastward along the length of Francis Avenue towards the subject properties, but it has not generally extended south of the alleyway. The subject properties are surrounded on three sides by single-family residential zoning—a condition that has existed for more than 60 years.

9. **Proposed Zoning:** As shown in Exhibit D, the proposal seeks to amend the zoning so that the entirety of the subject properties are zoned Community Business with a 55-foot height limit (CB-55).

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | June 10, 2020     |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |
| SEPA Determination Issued .....                               | August 24, 2020   |

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



Notice of Public Hearing Posted ..... August 26, 2020

Plan Commission Hearing Date (Scheduled) ..... September 9, 2020

- 2. Comments Received:** A request for comments was issued to City departments, local agencies, and departments within 400 feet of the proposal, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- Charles Hansen, Whitman Neighborhood Council Chair
- Cliff Winger, Shiloh Hills Neighborhood Council Chair<sup>3</sup>
- Randy Abrahamson, Spokane Tribe Historic Preservation Chair
- Joelie Eliason, Spokane Development Services Center

Mr. Hansen did not have any issues with the proposal. Mr. Winger reported following a presentation by the applicant to the neighborhood council, stating the following concerns:

1. The alleyway should remain open.
2. Vehicles accessing the site should not be allowed to exit/enter from Nevada St.

The proposal does not include the vacation of the alleyway, nor has the applicant indicated their wish to ask for a vacation in the future. Furthermore, site access would be considered if and when the applicant comes forward for building permits, if the Comprehensive Plan Amendment is approved.

Ms. Eliason communicated that the Spokane Development Services Center has no concerns or objection to the proposal. As for the Spokane Tribe, Mr. Abrahamson indicated that future development of the project would have a low probability of uncovering cultural resources. Copies of these comments are included in this staff report as **Exhibit L**.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. No additional comments were received from the public during the 60-day comment period.

- 3. Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 10, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was provided an opportunity to speak during the workshop but no public comment was taken.

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<sup>3</sup> Since this comment was received, Mr. Winger has become a member of the Spokane Plan Commission. When this comment was submitted, he was acting as neighborhood chair, not as a representative of the Plan Commission, thus his comment has been considered by staff from that framework.



## VI. APPLICATION REVIEW AND ANALYSIS

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:

- A. Keep the comprehensive plan alive and responsive to the community.
- B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
- C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
- D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
- E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
- F. Amendments to the comprehensive plan must result in a net benefit to the general public.

2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. **GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.



- C. Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency or City Department comment request or require a traffic impact analysis for the proposal. The subject properties are already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

**E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.



*Neighborhood Planning Documents Adopted after 2001.* The Nevada Heights neighborhood completed its initial neighborhood planning in 2012, when it was known as the Nevada Lidgerwood neighborhood. The neighborhood adopted four issue areas and various strategies to address them, including: neighborhood communication; neighborhood identity; non-motorized travel safety; and traffic patterns, volume, and speed. The proposed action would not conflict with any of the strategies provided in the neighborhood plan.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

See Item K.2 for an analysis and results.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: As discussed under item K.2.a below, the proposal is in conflict with the Comprehensive Plan. As a map change proposal, this application does not include any amendment to the text of the plan, including any policies with which it is in conflict.

The proposal is in conflict with this criterion.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** No evidence has been provided by any adjacent jurisdiction, including the County of Spokane, indicating this proposal would conflict with the CWPP or the plans of any neighboring jurisdiction. The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*



2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>4</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

**I. Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of a previously developed area served by the public facilities and services described in CFU 2.1. The proposed change in

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<sup>4</sup> State Environmental Policy Act



land-use designations affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

**K. Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

The proposal meets this criterion.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.8, General Commercial Uses. That policy generally calls for containment of non-residential uses to centers and corridors. The subject properties are located immediately adjacent to commercially-zoned property, but are not located in a Center or Corridor designated by the City. The policy allows for "limited expansions" of existing commercial areas outside Centers, provided the following factors are considered:

- Maintaining minimum depth from an arterial street necessary for the establishment or expansion of a general commercial neighborhood business;
- Avoiding intrusion where incompatible into established neighborhoods; and



- Implementing transitioning land uses with the intent of protecting neighborhood character.

To the first point, the northern two parcels under common ownership have already been developed with a commercial use, seeming to argue that the parcels are developable and thus additional depth is not required. The presence of the City alleyway would also serve to divide the overall group of properties. Furthermore, the proposal would effectively extend commercial use south of the alleyway when development along the south side of Francis has not crossed the alleyway elsewhere. Lastly, the proposal is for General Commercial Land Use Plan Map designation, which is the most intensive commercial use designation in the Comprehensive Plan, thus it cannot be considered to be “transitional,” as office uses generally are. The factors above may provide an argument that the proposal is not consistent with LU 1.8.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude commercial development on the site. The site is adequately served by all utilities and by a major arterial street, bus service is nearby at the intersection of Francis and Nevada, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: As discussed in item ‘a’ above, one could argue that this proposal is inconsistent with Comprehensive Plan policy.

This proposal’s consistency with this criteria is unclear.

3. **Rezoning, Land Use Plan Amendment:** *Corresponding rezoning will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property, as requested by the applicant, will change from RSF (Residential Single-Family) to CB-55 (Community Business, 55-foot height limit). The CB zone implements the General Commercial land-use designation proposed by the applicant. No policy language changes have been identified as necessary to support the proposed Land Use Plan Map amendment. The proposal meets this criterion.



## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal's relationship to the criteria set forth by SMC 17G.020 is unclear.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Because the consistency of this proposal with the location requirements of Comprehensive Plan policy is unclear, staff does not have a recommendation.

## **IX. LIST OF EXHIBITS**

- |                                  |                                           |
|----------------------------------|-------------------------------------------|
| A. Existing Land Use Plan Map    | G. Wide-Area Aerial                       |
| B. Proposed Land Use Plan Map    | H. List of Relevant Comp Plan Policies    |
| C. Existing Zoning Map           | I. Application Materials                  |
| D. Proposed Zoning Map           | J. SEPA Checklist                         |
| E. Application Notification Area | K. SEPA Determination of Non-Significance |
| F. Detail Aerial                 | L. Agency Comments                        |



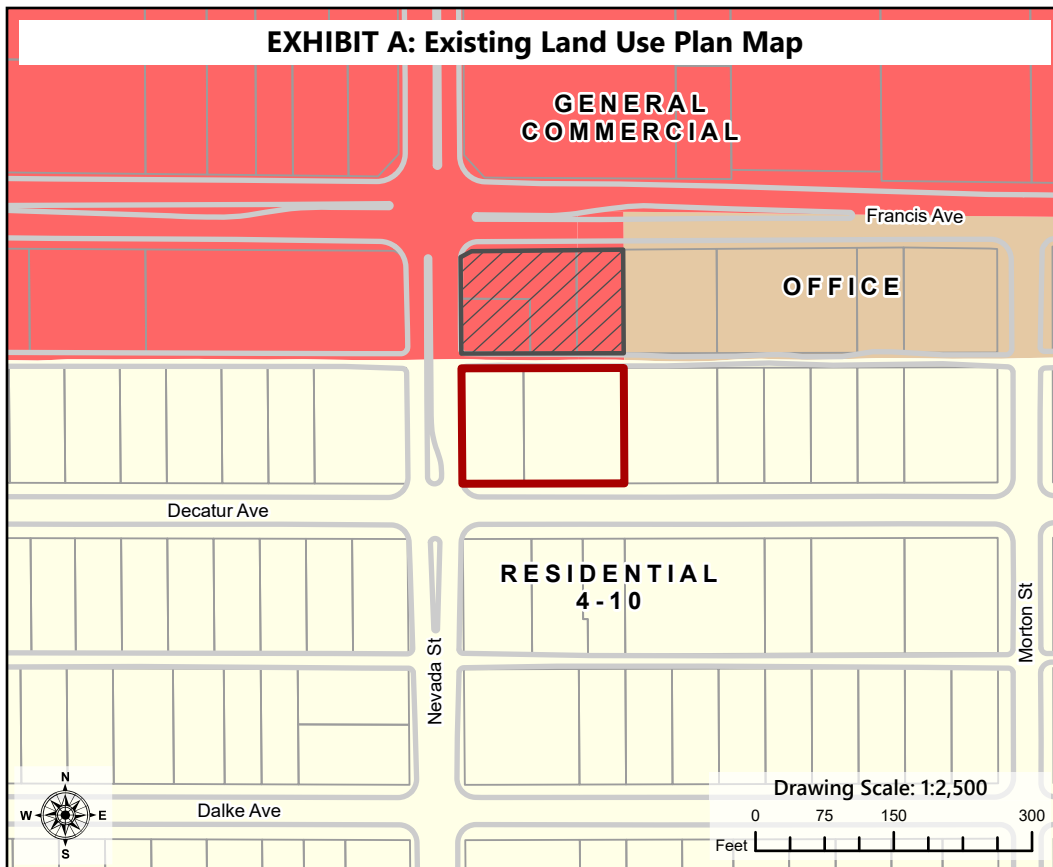


# Z19-501COMP (6204 N Nevada St & 1015 E Decatur Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

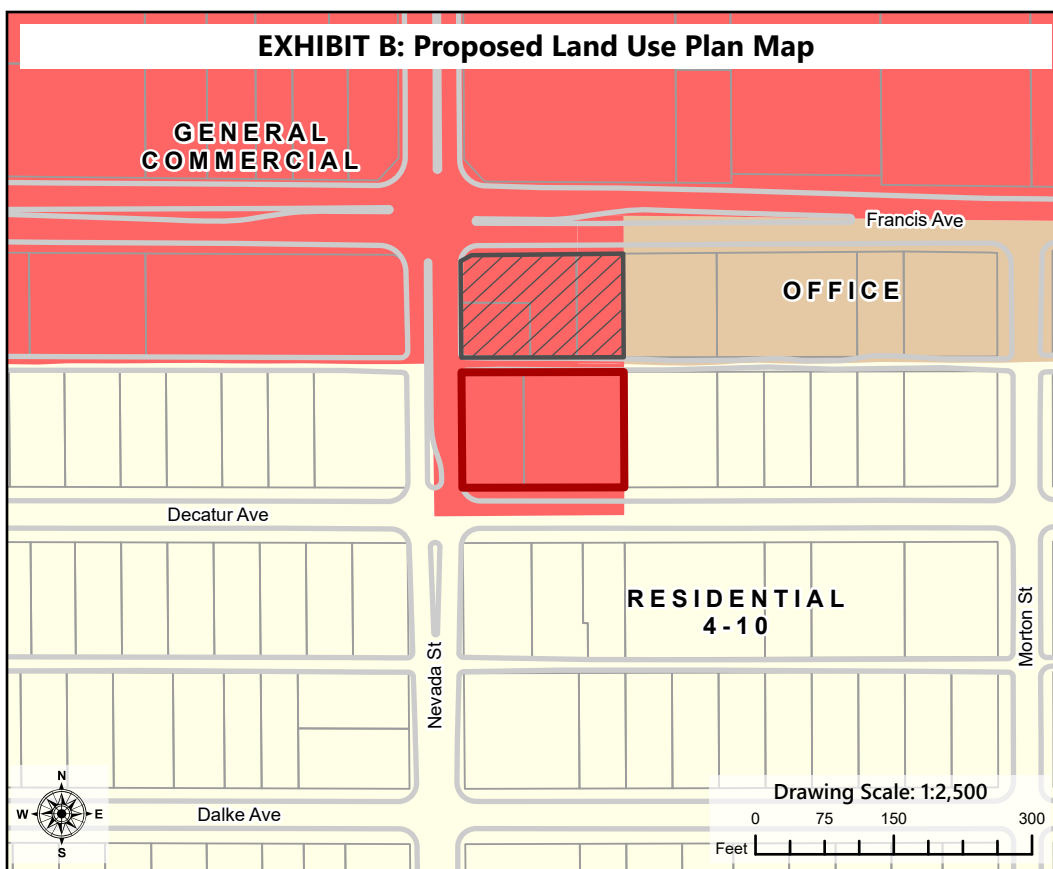
## Land Use Plan Designation

Residential 4-10

Office

General Commercial

Acres (Proposal): 0.51  
Acres (Adjacent): 0.45



## Legend

Subject Parcels

Same Owner

Parcel

Curb Line

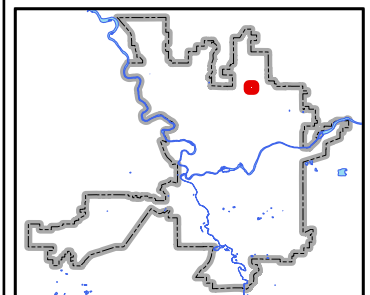
## Proposed Land Use

Residential 4-10

Office

General Commercial

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-501COMP (6204 N Nevada St & 1015 E Decatur Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

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## Legend

- Subject Parcels
- Same Owner
- Curb Line
- Parcel

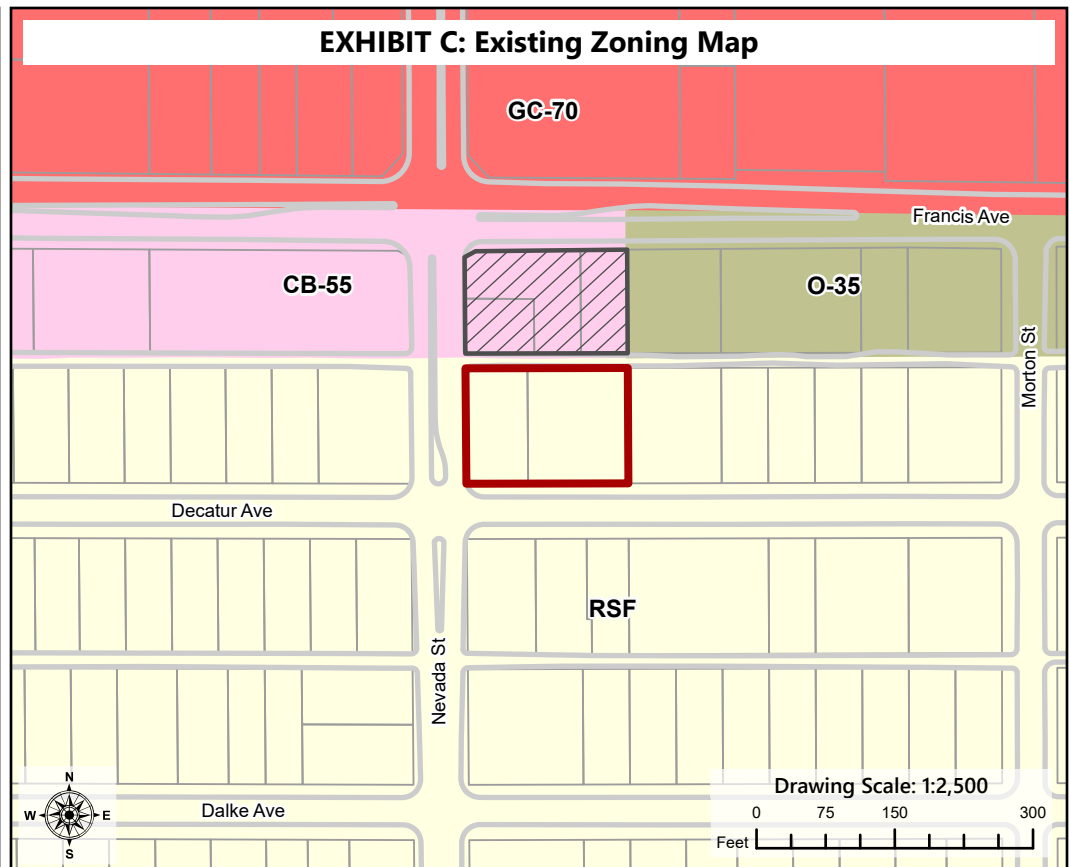
## Current Zoning

- Community Business (CB)
- General Commercial (GC)
- Light Industrial (LI)
- Office (O)
- Residential Single-Family (RSF)

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 0.51  
Acres (Adjacent): 0.45

## EXHIBIT C: Existing Zoning Map



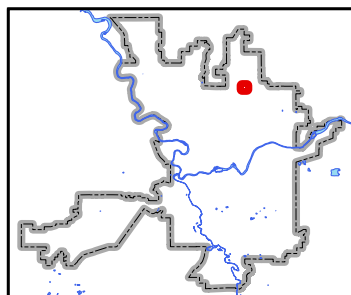
## Legend

- Subject Parcels
- Same Owner
- Curb Line
- Parcel

## Proposed Zoning

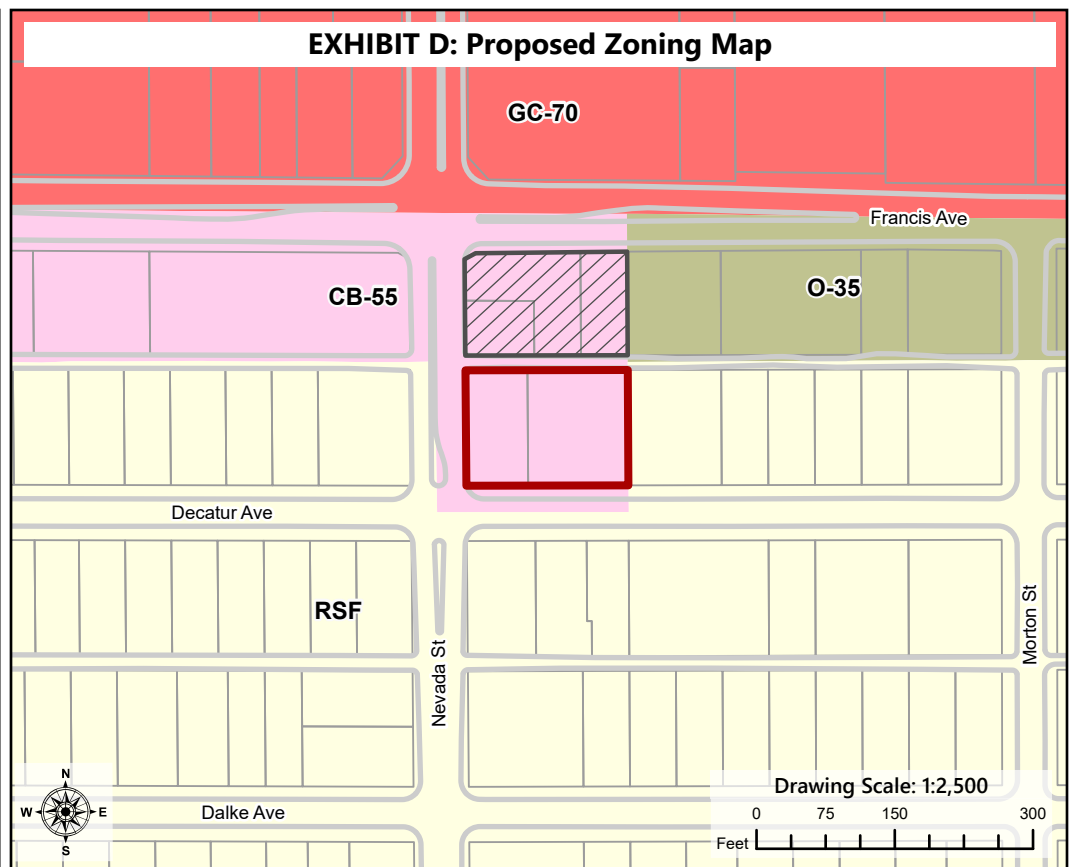
- Community Business (CB)
- General Commercial (GC)
- Office (O)
- Residential Single-Family (RSF)

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map







# Z19-501COMP

## (6204 N Nevada St & 1015 E Decatur Ave)

2019/2020 Comprehensive Plan Amendment Proposals

THIS IS NOT A LEGAL DOCUMENT: The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



### Legend

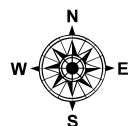
- Parcel
- Notification Boundary
- Curb Line
- Address Point

### Subject Parcels

- Subject Parcels
- Same Owner

### Application proposes to:

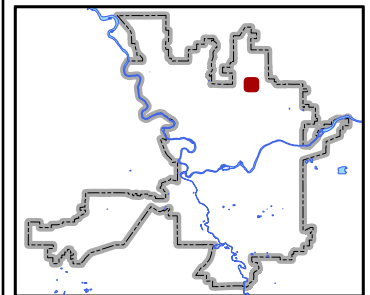
Change Land Use Designation from Residential 4-10 to General Commercial.



Project Size: 0.51 Acres (Approximate)  
Drawing Date: 12/3/2019 Drawing Scale: 1:2,800

0 75 150 300  
Feet

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# **Z19-501COMP** **(6204 N Nevada St & 1015 E Decatur Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

**THIS IS NOT A LEGAL DOCUMENT**

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT F: Detail Aerial



## Legend

Subject Parcels

Adjacent Ownership

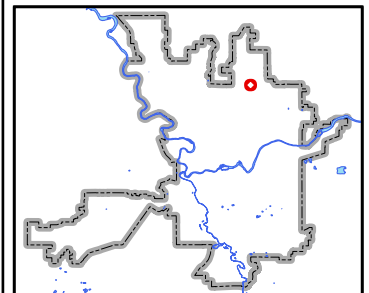
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 0.51  
Acres (Adjacent): 0.45

## EXHIBIT G: Wide-Area Aerial



## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-501COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.8 General Commercial Uses

Direct new General Commercial uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* General Commercial areas provide locations for a wide range of commercial uses. Typical development in these areas includes freestanding business sites and larger grouped businesses (shopping centers). Commercial uses that are auto-oriented and include outdoor sales and warehousing are also allowed in this designation. Land designated for General Commercial use is usually located at the intersection of or in strips along principal arterial streets. In many areas such as along Northwest Boulevard, this designation is located near residential neighborhoods.

To address conflicts that may occur in these areas, zoning categories should be implemented that limit the range of uses, and site development standards should be adopted to minimize detrimental impacts on the residential area. New General Commercial areas should not be designated in locations outside Centers and Corridors. Existing commercial strips should be contained within their current boundaries with no further extension along arterial streets allowed.

However, recognizing existing investments, and given deference to existing land-use patterns, exceptions to the containment policy may be allowed for limited expansions adjacent to existing General Commercial areas located outside Centers and Corridors. The factors to consider in such adjacent expansions include: maintaining the minimum depth from an arterial street necessary for the establishment or expansion of a general commercial neighborhood business; avoiding intrusion where incompatible into established neighborhoods; and implementing transitional land uses with the intent of protecting neighborhood character.

Areas designated General Commercial within Centers and Corridors are encouraged to be developed in accordance with the policies for Centers and Corridors. Through a neighborhood planning process



for the Center, these General Commercial areas will be designated in a land use category that is appropriate in the context of a Center and to meet the needs of the neighborhood.

Residential uses are permitted in these areas. Residences may be in the form of single-family homes on individual lots, upper-floor apartments above business establishments, or other higher density residential uses.

#### LU 3.1 Coordinated and Efficient Land Use

Encourage coordinated and efficient growth and development through infrastructure financing and construction programs, tax and regulatory incentives, and by focusing growth in areas where adequate services and facilities exist or can be economically extended.

*Discussion:* Future growth should be directed to locations where adequate services and facilities are available. Otherwise, services and facilities should be extended or upgraded only when it is economically feasible to do so.

The Centers and Corridors designated on the Land Use Plan Map are the areas of the city where incentives and other tools should be used to encourage infill development, redevelopment and new development. Examples of incentives the city could use include assuring public participation, using public facilities and lower development fees to attract investment, assisting with project financing, zoning for mixed-use and higher density development, encouraging rehabilitation, providing in-kind assistance, streamlining the permit process, providing public services, and addressing toxic contamination, among other things.

#### LU 5.3 Off-Site Impacts

Ensure that off-street parking, access, and loading facilities do not adversely impact the surrounding area.

*Discussion:* Off-street parking, access, and loading facilities are usually associated with the development of higher density residential, office, and commercial uses. These features often have major impacts on single-family residential areas. The impacts are most significant when these facilities are next to or intrude between homes. When these facilities are accessory to a higher density residential or nonresidential use, they should be developed according to the same policies and zoning regulations as govern the primary use. New parking lots should also have the same zoning classification as the primary use. In addition, these facilities should be developed to minimize adverse impacts to adjacent properties. All parking lots should be paved. Parking lots and loading areas should have appropriate buffers to fully screen them from adjacent, less intensive uses. Access to business and higher density residential sites should be controlled to avoid impacts on adjacent uses, pedestrian movement, and street functions.

#### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

### **Chapter 11—Neighborhoods**

#### N 8.4 Consistency of Plans



Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-28-19

Kevin Freibott, Planner II  
Planning & Development Services  
3<sup>rd</sup> Floor City Hall  
West 801 Spokane Falls Blvd.  
Spokane WA 99201

Francis Nevada Partnership Map Amendment Request R 4-10 to GC

Dear Kevin:

This is a continuation of an amendment filed last October and then tabled for one year by Council action, pending the adoption of new policy language for LU 1.8. As you know, the proposed language will be heard by Council on November 18<sup>th</sup> and can be used to measure compliance for this request.

I have therefore amended the Early Threshold Form to address what I felt was the theme of various iterations of this revised policy language. Perhaps the most important thing to consider is what the marketplace is demanding, namely configuration and not just square footage for expansion of existing GC sites. Such is the case here.

Please include the revised Early Threshold form as part of the request to resume our application submitted one year ago.

Regards



Dwight A Hume





Submitted 2018, Still Applies to this Application

**DESCRIPTION OF PROPOSAL:**

Map Amendment from R 4-10 to GC and zone change from RSF to CB-55

**ADDRESS OF SITE OF PROPOSAL:** (if not assigned yet, obtain address from Public Works before submitting application)

6204 N Nevada and 1015 E Decatur

**APPLICANT:**

**Name:** Francis Nevada Partnership % Sue Millersmith  
**Address:** 7624 N. Panorama Dr. Spokane WA 99208-8436  
**Phone (home):** 509-467-0671 **Phone (work):** N/A

**Email address:** hairetc@comcast.net

**PROPERTY OWNER: (Same as above)**

**Name:**

**Address:**

**Phone (home):**

**Phone (work):**

**Email address:**

**AGENT:**

**Name:** Dwight Hume dba Land Use Solutions and Entitlement  
**Address:** 9101 N Mt. View Lane

**Phone (home):** **Phone (work):** 435-3108

**Email address:**

RECEIVED

OCT 29 2018

Neighborhood and  
Planning Services

**ASSESSOR'S PARCEL NUMBERS:**

36321.0209 and 36321.0210

**LEGAL DESCRIPTION OF SITE:**

All of Lot 13, W. 33' of Lot 14, Block 1 Lancaster's 2<sup>nd</sup> Addition and the E 7' of Lot 14 and all of Lot 15, Block 1 of said Lancaster's 2<sup>nd</sup> Addition.

**SIZE OF PROPERTY:**

.50 acres (.19 ac and .31 ac)

**LIST SPECIFIC PERMITS REQUESTED IN THIS APPLICATION:**

Land Use Map Amendment with implementing zone change.



Exhibit I, p.3





# Comprehensive Plan Amendments

## Threshold Review

### Francis Neda Partnership R 4-10 to GC & RSF to CB-55

#### ***Pre-application:***

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

#### ***Description of the Proposed Amendment:***

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.

*36321.0209 and 0210. 6204 N Nevada and 1015 E Decatur. .50 acres*

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*The UDC allows for private sector request to amend site specific parcels as map amendments.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*The initial submittal in October 2018 was tabled for one year by City Council to allow staff to review Policy LU 1.8 and draft new policy language. That new language is being considered by City Council at their November 18, 2019 council meeting, presumably available for review of this request during the next amendment cycle.*
3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.  
*There are no foreseen issues that would generate extraordinary review time.*
4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby,

Planning & Development Services, 808 West Spokane Falls Boulevard, Spokane, WA 99201-3336  
[my.spokanecity.org](http://my.spokanecity.org) | Phone: 509.625.6300

(Rev Sept 2017)



similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated? *No other property is suitable for this request.*

5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC. *The proposed language of LU 1.8 eliminates traffic volume as a threshold and replaces it with policy language that minimizes the expansion to that which is necessary for the establishment or expansion.*

*In this situation several facts support this request for expansion across the alley to Decatur:*

- a) *Lineal expansion along Francis does not configure the site for today's retail market demand, hence expansion southerly enables the configuration to meet needs of the marketplace.*
- b) *The extended frontage along Nevada is still along a Principal arterial and not a local access side street. Hence, the intrusion is no different than the current depth from Francis in terms of intrusion into a neighborhood. Moreover, the applicant owns the SE corner of Decatur and Nevada as an added buffer.*
- c) *The current GC designation is to the alley and no additional expansion of the zone is possible without crossing the alley.*

*Regardless of the final verbiage adopted by Council, the expansion will be the minimum necessary to allow expansion per today's market demand. Accordingly, the request is consistent with the current comprehensive plan and therefore is consistent with Countywide Planning Policies, the GMA and other applicable state and federal regulations.*

6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated. *This is the same application as submitted last year. Council continued it to the 2020 amendment cycle pending adoption of revised policy language for LU 1.8.*
7. If this change is directed by state law or a decision of a court or administrative agency, please describe. *N/A*
8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.  
*The same proposal has been presented to the Nevada Neighborhood Council when it was submitted last year and seemed to be supported by them.*



**From:** [Dwight Hume](#)  
**To:** [Nevada Heights Neighborhood](#); [Freibott, Kevin](#)  
**Subject:** Annual Amendment Presentation  
**Date:** Tuesday, November 26, 2019 1:03:44 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Hi Mindy, I need to get on your schedule for December or January to talk about our resumption of Nevada Francis amendment. Maybe 10 minutes, if that. Let me know. Thanks and happy holidays!

*Dwight J Hume*

Land Use Solutions and Entitlement  
9101 N Mt. View Lane  
Spokane WA 99218  
509-435-3108



**From:** [Dwight Hume](#)  
**To:** [shilohhillsnc@outlook.com](mailto:shilohhillsnc@outlook.com)  
**Subject:** Request to present December meeting  
**Date:** Tuesday, November 26, 2019 1:40:28 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Mr. Knox, Per city of Spokane requirements, I need to meet with your neighborhood council and share with you a land use amendment filed with the city for their annual review and consideration. This project is located at the SE corner of Nevada and Francis (Hair Etc.) and proposes to expand south across the alley to Decator along Nevada. This is located within the Nevada Heights NC but due to its proximity to your neighborhood boundary, we must share our application with you. I do not expect this to take more than 15 minutes of your evening. Please advise if there is room on this or your January meeting.

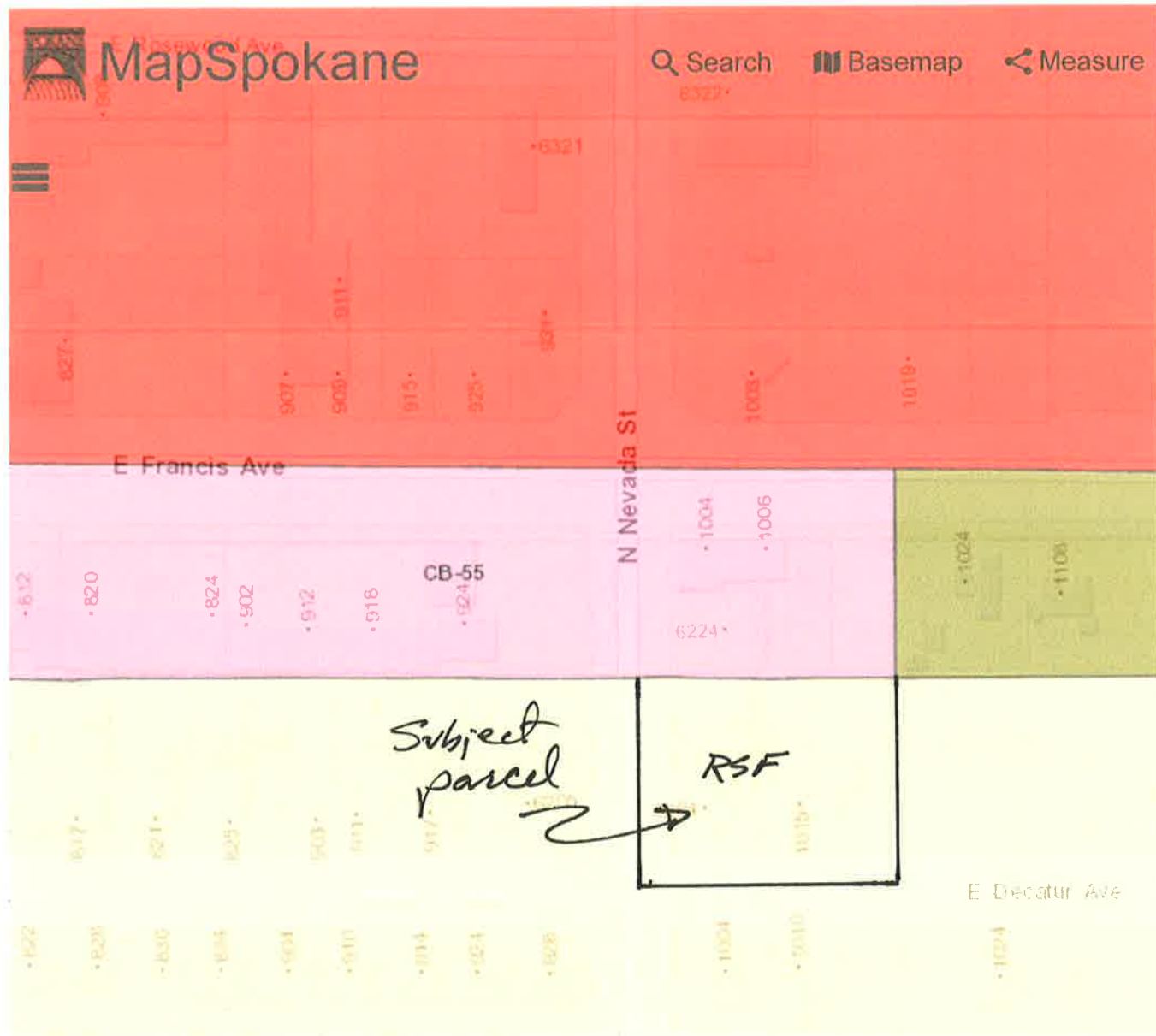
Regards

*Dwight J Hume*

Land Use Solutions and Entitlement  
9101 N Mt. View Lane  
Spokane WA 99218  
509-435-3108



Application Z19-499COMP



EXISTING ZONE

RECEIVED

OCT 29 2018

Neighborhood and Planning Services

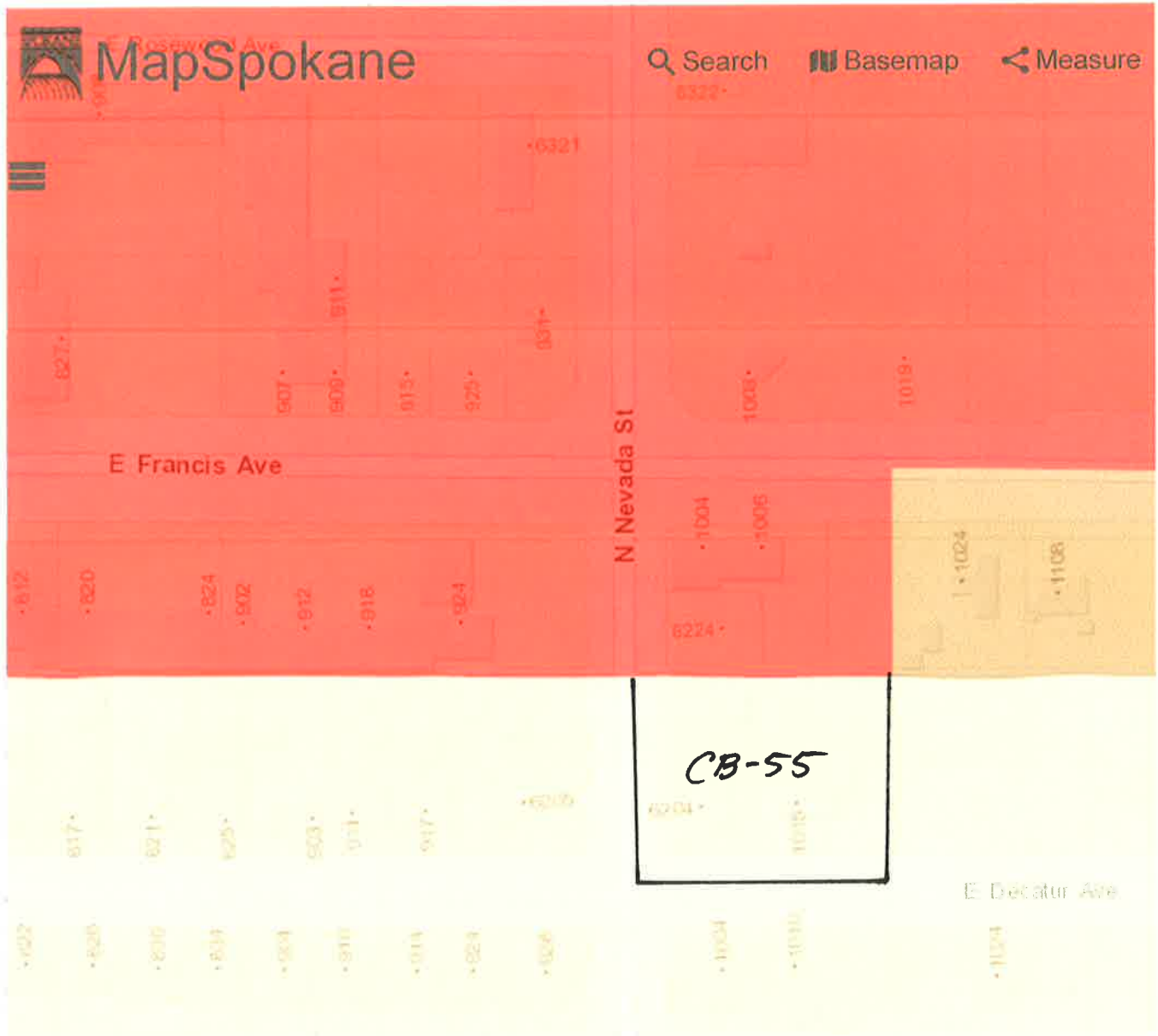


[Map Use Disclaimer](#)

0 50 100ft



Application Z19-499COMP



PROPOSED ZONE

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OCT 29 2018

Neighborhood and  
Planning Services

0 50 100ft



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## Environmental Checklist

File No. Z19-501COMP

### Purpose of Checklist:

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

### Instructions for Applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### Use of checklist for non-project proposals:

Complete this checklist for non-project proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For non-project actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**A. BACKGROUND**

1. Name of proposed project, if applicable: Z19-501COMP
2. Name of applicant: Francis Nevada Partnership
2. Address and phone number of applicant or contact person:  
Land Use Solutions & Entitlement Dwight Hume agent  
9101 N Mt. View Lane Spokane WA 99218 509-435-3108
4. Date checklist prepared: March 2020
5. Agency requesting checklist: City of  
Spokane
6. Proposed timing or schedule (including phasing, if applicable): Project is a comprehensive plan amendment, modifying the land use plan map designation and zoning of the subject properties. This action is expected to be decided late fall or winter of 2020.
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. N/A, non-project action  
b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. Yes, the adjacent .46 acres north of the alley along Francis Avenue is owned by the applicant.
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.  
  
Upon approval of the zone change, a project will have to comply with all applicable development regulations.
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.  
No.
10. List any government approvals or permits that will be needed for your proposal, if known.  
Comp Plan amendment and zone change;



11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.

The zone change will enable the site to be enlarged from its current .46 acres along Francis to approximately .96 acres and more importantly, a site configuration that allows a new building setback away from the intersection and the dangerous proximity and inclusion in vehicular accidents where vehicles often collide with the building, endangering customers and employees. The project would be site planned with building setbacks away from Nevada and Francis and allow the ingress and egress to and from the existing alley which runs east-west thru the subject site and along the north boundary of the requested amendment.

12. Location of the proposal. Give sufficient information to a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist.

The site is located at the NE corner of Decatur and Nevada and consist of two houses located on site. (6204 N Nevada and 1015 E Decatur.

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.)

Yes; Sewer Service Area: Yes; City of Spokane

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

- (1) Describe any systems, other than those designed for the disposal of sanitary waste, installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities).

N/A, non-project action

- (2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?

N/A, non-project action



- (3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems.

N/A, non-project action

- (4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater?

N/A, non-project action

b. Stormwater

- (1) What are the depths on the site to groundwater and to bedrock (if known)?

N/A, non-project action

- (2) Will stormwater be discharged into the ground? If so, describe any potential impacts?

N/A, non-project action

**TO BE COMPLETED BY APPLICANT**

**B. ENVIRONMENTAL ELEMENTS**

**1. Earth**

- a. General description of the site (circle one): **flat**, *rolling*, *hilly*, *steep slopes*, *mountains*, *other*: \_\_\_\_\_

- b. What is the steepest slope on the site (approximate percent slope)?

Unknown \_\_\_\_\_

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. N/A, non-project action

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. N/A, non-project action

- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill:

To be determined at time of construction by others \_\_\_\_\_

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

No, the project does not include and construction \_\_\_\_\_



- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? To be determined at time of construction by others.
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: To be determined at time of construction by others.

## 2. Air

- a. What type of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial, wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.  
N/A, non-project action
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. Drive-by traffic, which idles during traffic light stoppage and backs up along Nevada from Francis past Decatur.
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:  
To be determined at time of construction by others a

## 3. Water

- a. SURFACE:
- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.  
No
- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.  
None
- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.  
Non-project action
- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.  
No
- (5) Does the proposal lie within a 100-year floodplain?    If so, note location on the site plan.  
No



- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

To be determined at time of construction by others

b. GROUND:

- (1) Will groundwater be withdrawn, or will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sanitary waste treatment facility. Describe the general size of the system, the number of houses to be served (if applicable) or the number of persons the system(s) are expected to serve.

None, as the site is served with public sewer.

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

Run-off would be limited to stormwater and discharged into existing city storm drains.

- (2) Could waste materials enter ground or surface waters? If so, generally describe.

No

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water impacts, if any.

None



#### 4. Plants

- a. Check or circle type of vegetation found on the site:
- \_\_\_\_\_ **Deciduous tree:** *alder, maple, aspen, other.*
- \_\_\_\_\_ **Evergreen tree:** *fir, cedar, pine, other.*
- \_\_\_\_\_ **Shrubs**
- \_\_\_\_\_ **Grass**
- \_\_\_\_\_ Pasture
- \_\_\_\_\_ Crop or grain
- \_\_\_\_\_ Wet soil plants, *cattail, buttercup, bullrush, skunk cabbage, other.*
- \_\_\_\_\_ Water plants: *water lilly, eelgrass, milfoil, other.*
- \_\_\_\_\_ Other types of vegetation.
- b. What kind and amount of vegetation will be removed or altered? Non-project action \_\_\_\_\_
- c. List threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:  
To be determined at time of construction by others \_\_\_\_\_

#### 5. Animals

- a. Circle any birds and animals which have been observed on or near the site are known to be on or near the site:
- birds: *hawk, heron, eagle, **songbirds**, other.* Typical urban fowl
- mammals: *deer, bear, elk, beaver, other.* \_\_\_\_\_
- fish: *bass, salmon, trout, herring, shellfish, other.* \_\_\_\_\_
- other: \_\_\_\_\_
- b. List any threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- c. Is the site part of a migration route? If so, explain. Unknown \_\_\_\_\_
- d. Proposed measures to preserve or enhance wildlife, if any: \_\_\_\_\_  
None \_\_\_\_\_

#### 6. Energy and natural resources

- a. What kinds of energy (**electric, natural gas**, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.



The site contains two residential uses which are served with electrical and gas services. No additional services are anticipated. \_\_\_\_\_

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. Unknown \_\_\_\_\_
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

None \_\_\_\_\_

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. None \_\_\_\_\_
- (1) Describe special emergency services that might be required.  
No special services are needed. \_\_\_\_\_
- (2) Proposed measures to reduce or control environmental health hazards, if any:  
None \_\_\_\_\_
- b. NOISE:
- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?  
Traffic on Francis and Nevada adjoin the site on Principle Arterials
- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.  
To be determined at time of construction by others \_\_\_\_\_
- (3) Proposed measure to reduce or control noise impacts, if any:  
None \_\_\_\_\_

## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties?  
Site: 2 single family houses \_\_\_\_\_  
West: single family homes and Auto Repair \_\_\_\_\_  
East: single family homes and North: Retail and vacant \_\_\_\_\_  
South: single family homes \_\_\_\_\_
- b. Has the site been used for agriculture? If so, describe. No \_\_\_\_\_



- c. Describe any structures on the site. None
- d. Will any structures be demolished? If so, which? Both dwellings may be removed as part of future construction plans not a part of this proposal.
- e. What is the current zoning classification of the site? RSF
- f. What is the current comprehensive plan designation of the site? R 4-10
- g. If applicable, what is the current shoreline master program designation of the site?  
None
- h. Has any part of the site been classified as a critical area? If so, specify. No
- i. Approximately how many people would reside or work in the completed project?  
To be determined at time of construction by others
- j. Approximately how many people would the completed project displace? 3
- k. Proposed measures to avoid or reduce displacement impacts, if any: none
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: Compliance with all applicable development regulations

## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle or low-income housing. None
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. Two
- c. Proposed measures to reduce or control housing impacts, if any: None

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?  
To be determined at time of construction by others
- b. What views in the immediate vicinity would be altered or obstructed?  
To be determined at time of construction by others
- c. Proposed measures to reduce or control aesthetic impacts, if any:  
To be determined at time of construction by others



## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?  
To be determined at time of construction by others \_\_\_\_\_
- b. Could light or glare from the finished project be a safety hazard or interfere with views?  
To be determined at time of construction by others \_\_\_\_\_
- c. What existing off-site sources of light or glare may affect your proposal? No impacts would occur \_\_\_\_\_
- d. Proposed measures to reduce or control light and glare impacts, if any: None

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? N/A \_\_\_\_\_
- b. Would the proposed project displace any existing recreational uses? If so, describe. No \_\_\_\_\_
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: None \_\_\_\_\_

## 13. Historic and cultural preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. Unknown \_\_\_\_\_
- b. Generally describe any landmarks or evidence of historic archaeological, scientific or cultural importance known to be on or next to the site.  
None \_\_\_\_\_
- c. Proposed measures to reduce or control impacts, if any: \_\_\_\_\_  
None \_\_\_\_\_

## 14. Transportation

- a. Identify public streets and highways serving the site and describe proposed access to the existing street system. Show on site plans, if any.  
The site is served by Nevada and Decatur. \_\_\_\_\_
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?  
Yes it is served \_\_\_\_\_



- c. How many parking spaces would the completed project have? How many would the project eliminate?

To be determined at time of construction by others \_\_\_\_\_

- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets not including driveways? If so, generally describe (indicate whether public or private). No new roads or streets are needed. Improvements would be limited to curb cuts and sidewalks if any.

- e. Will the project use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. No \_\_\_\_\_

- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak would occur.

To be determined at time of construction by others \_\_\_\_\_

*(Note: to assist in review and if known indicate vehicle trips during PM peak, AM Peak and Weekday (24 hours).)*

- g. Proposed measures to reduce or control transportation impacts, if any:

None \_\_\_\_\_

## 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe. No new or expanded services would be needed or generated by this proposal \_\_\_\_\_

- b. Proposed measures to reduce or control direct impacts on public services, if any: None \_\_\_\_\_

## 16. Utilities

- a. Circle utilities currently available at the site: **electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.**

- b. Describe the utilities that are proposed for the project, the utility providing the service and the general construction activities on the site or in the immediate vicinity which might be needed.

None \_\_\_\_\_



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 \_\_\_\_\_

Signature: *Dwight J Hume*

***Please Print or Type:***

Proponent: Dwight J Hume \_\_\_\_\_ Address: 9101 N Mt. View Lane

Phone: 509.435.3108 \_\_\_\_\_ Spokane WA, 99218

Person completing form (if different from proponent): Same \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott \_\_\_\_\_

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



#### **D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS**

***(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage or release of toxic or hazardous substances; or production of noise?

The future use for retail and parking is similar to the surrounding land use pattern. No impacts are foreseen by this proposed use.

Proposed measures to avoid or reduce such increases are:

The development must comply with applicable adopted development standards.

2. How would the proposal be likely to affect plants, animals, fish or marine life?  
Future development of the site would be urban in nature, similar to the retail adjoining the subject parcels. Eventual redevelopment of the site may require the removal of on-site plants, subject to the requirements of the Spokane Municipal Code for new construction.

Proposed measures to protect or conserve plants, animals, fish or marine life are:  
None

3. How would the proposal be likely to deplete energy or natural resources?  
While some additional resources would be required during redevelopment, these would be similar to those required of any construction project.

Proposed measures to protect or conserve energy and natural resources are:  
None

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands?  
The site does not contain any sensitive areas

Proposed measures to protect such resources or to avoid or reduce impacts are:  
None

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?  
Development would comply with applicable development regulations, including landscape, screening and setbacks. Furthermore, the site is located outside any shoreline areas.



Proposed measures to avoid or reduce shoreline and land use impacts are:  
See above comment

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6. How would the proposal be likely to increase demands on transportation or public services and utilities?

There would be no extraordinary demand upon utility services and or traffic as it is a drive-by convenience site using the existing driveby traffic pattern.

Proposed measures to reduce or respond to such demand(s) are:

To be determined at time of construction by others

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment.

No impacts are foreseen



### C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 \_\_\_\_\_

Signature: *Dwight J Hume*

***Please Print or Type:***

Proponent: Dwight J Hume \_\_\_\_\_ Address: 9101 N Mt. View Lane

Phone: 509.435.3108 \_\_\_\_\_ Spokane WA, 99218

Person completing form (if different from proponent): Same \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

#### FOR STAFF USE ONLY

Staff member(s) reviewing checklist: Kevin Freibott \_\_\_\_\_

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z19-501COMP

**PROPONENT:** Sue Millersmith, Francis Nevada Partnership (Agent: Dwight Hume, Land Use Solutions and Entitlement)

**DESCRIPTION OF PROPOSAL:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the land use designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties (0.51 acres in size) located in the Nevada Heights neighborhood. No specific development proposal is being approved at this time.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns two parcels: 36321.0209 and 36321.0210. Both are located immediately northeast of the intersection of N Nevada Street and E Decatur Avenue, all on the southern half of the block. The parcels are located at 6204 N Nevada St and 1015 E Decatur Ave. Each of the two subject parcels contains a single-family home, currently rented out.

**Legal Description:** Lots 13 through 16, Block 1, Lancasters 2nd Addition to the City of Spokane in Section 32, Township 26N, Range 43E.

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

☐ There is no comment period for this DNS.

☐ This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.

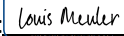
☒ This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:** 

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*



**From:** [Charles Hansen](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Re: Request for Comments for Nevada & Decatur Comp Plan Land Use Map Amendment Proposal  
**Date:** Friday, April 24, 2020 7:36:26 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Kevin While this proposal is not in my neighborhood it is an an area that serves Whitman School. I do not see any problem with extending the business classification across the alley to Decatur.

Now I must also acknowledge that I have known the family that owns the property for more than 25 years.

Charles Hansen

Whitman Chair.

On 4/24/2020 4:11 AM, Bishop, Stephanie wrote:

Good Morning,

Please find attached the Request for Comments, Maps and Environmental Checklist for the following proposed Comprehensive Plan Land Use Map Amendment:

**Location:** Nevada & Decatur  
**Permit #:** Z19-501COMP

Please direct any questions or comments to Assistant Planner II, Kevin Freibott, at [kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org).

Thank you,



**Stephanie N Bishop** | Neighborhood & Planning Services | Clerk III  
509.625.6244 | fax 509.625.6013 | [sbishop@spokanecity.org](mailto:sbishop@spokanecity.org)





**From:** [Shiloh Hills Neighborhood Council](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Bishop, Stephanie](#); [Shiloh Hills Neighborhood Council](#)  
**Subject:** FILE NO. Z19-501COMP, N Nevada & E Decatur  
**Date:** Friday, April 24, 2020 9:56:18 AM  
**Attachments:** [image.png](#)

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

FILE NO. Z19-501COMP, N Nevada & E Decatur  
(6204 N Nevada St & 1015 E Decatur Ave) 2019/2020 Comprehensive Plan Amendment  
Proposals  
Parcels: 36321.0209, 36321.0210

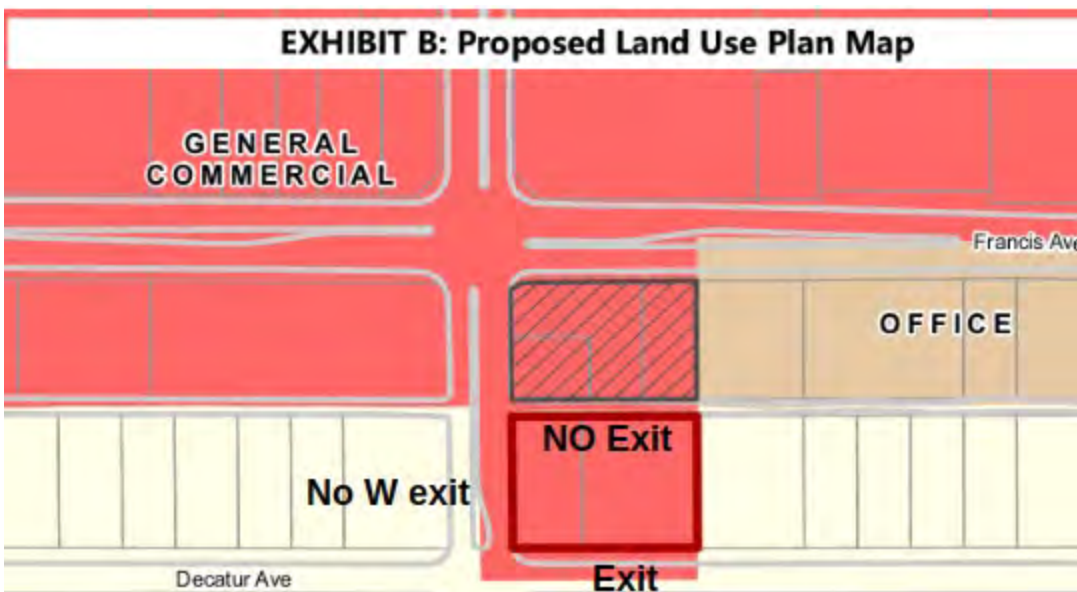
A representative for this proposal attended our Shiloh Hills Neighborhood Council meeting.

Shiloh Hills Neighborhood Council (SHNC) has two (2) comments:

1. The representative assured the SHNC that the alley way between E Francis Avenue and E Decatur Avenue would remain open for traffic east and west. SHNC approves if and only if this alley way easement is maintained.

2. SHNC very strongly recommends that in rezoning this property that no traffic from this rezoned property enter onto N Nevada Street either from the alley way between E Francis Avenue and E Decatur Avenue nor from the property itself. The only allowable exit from this property (36321.0209 & 36321.0210) would be onto E Decatur Avenue where egress traffic could go east or west on E Decatur.

On high traffic times NB traffic on N Nevada St. backs up from the signal at E Francis Ave. Compounding this traffic backlog is a STA bus stop at the corner of E Francis Ave and N Nevada St.



The undersigned attests that the representative for this proposal was told of these two (2)



conditions by the SHNC.

Thank you,

Clifford Winger

Shiloh Hills Neighborhood Council Chair

The Executive Committee of the Shiloh Hills Neighborhood Council

(509) 325-4623

[shilohhillsnc@outlook.com](mailto:shilohhillsnc@outlook.com)

<http://shilohhills.spokaneneighborhoods.org>



**From:** [Eliason, Joelle](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Brown, Eldon](#); [Nilsson, Mike](#); [Kells, Patty](#); [West, Jacque](#); [Johnson, Erik D.](#)  
**Subject:** RE: RFC Nevada & Decatur Comp Plan Land Use Amendment Z19-501COMP  
**Date:** Tuesday, May 5, 2020 3:18:50 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Kevin,

Thank you for the opportunity to review and comment on the proposed zoning change on the NE corner of E Decatur Ave and N Nevada St. Development Services has no objection to the proposed zoning change from Residential Single Family to General Commercial. Further comments regarding the design, any potential utility conflicts, stormwater management, etc. will be handled outside of this Comp Plan Amendment and associated SEPA.

Sincerely,  
Joelle Eliason

*I will be working remotely until further notice and will respond to emails as quickly as possible. Thank you for your patience!*



Joelle Eliason | City of Spokane | Development Services Center  
509.625.6385 | fax 509.625.6013 | [jeliason@spokanecity.org](mailto:jeliason@spokanecity.org)







**Spokane Tribe of Indians**  
**Tribal Historic Preservation Officer**  
P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z19-501COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-501COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to DENY the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “General Commercial” for a 0.51-acre area located at 6204 N Nevada Street and 1015 E Decatur Avenue. The implementing zoning designation requested is Community Business with 55-foot height limit (CB-55).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-501COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a 0.51-acre area located at 6204 N Nevada Street and 1015 E Decatur Avenue (the “Properties”) from “Residential 4-10” to “General Commercial” with a corresponding change in zoning from Residential Single Family (RSF) to Community Business with a 55-foot height limit (GC-55).
- E. The owner of the Properties also owns the parcels immediately to the north of the Properties, resulting in common ownership holding that spans the area between E Decatur Avenue and E Francis Avenue, save for a City alley right-of-way between the Properties and those of common ownership in the north.
- F. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- G. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- H. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, including the Application in the Work Program.
- I. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. The City received comments stating no or little concern with the proposal and one requesting conditions for site access.
- J. A Notice of Application was published on June 8, 2020 in the Spokesman Review and was mailed to all properties and owners within a 400-foot radius of the subject Properties and any adjacent



properties with the same ownership. Signs were also placed on the subject property in plain view of the public. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which a comment was received from the public, stating concerns with property value and neighborhood character impacts of the proposal.

- K. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and the Community Assembly was provided with information regarding the dates of Plan Commission workshops and hearings.
- L. On June 10, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- M. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- N. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
- O. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- P. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the property and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject property.
- Q. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis did not provide a recommendation to the Plan Commission regarding the proposal.
- R. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of the SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- S. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- T. No members of the public testified during the hearing on September 9, 2020.
- U. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.



- V. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the “Staff Report”).
- W. The Plan Commission finds that the proposal is inconsistent with Comprehensive Plan policy Land Use LU 1.8 concerning the establishment of General Commercial land uses in the City as follows:
  - 1. The proposal would place General Commercial land use outside designated Centers and Corridors and would intrude into an existing single-family residential area.
- X. The Plan Commission finds that the proposal does not meet the decision criteria established by SMC 17G.020.030, as described in the Staff Report and as described in ‘X’ above.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-501COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

- 1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Application was submitted as provided in SMC 17G.020.050(D).
- 2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
- 3. The Application is consistent with the goals and purposes of GMA.
- 4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City’s relevant six-year capital improvement plans or through enforcement of the City’s development regulations at time of development.
- 5. As outlined in above in the Findings of Fact, the Application is internally inconsistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
- 6. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
- 7. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
- 8. SEPA review was completed for the Application.
- 9. The Application will not adversely affect the City’s ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.



10. The Application proposes a land use designation that is inconsistent with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment and site is not suitable for the proposed designation.
12. The map amendment would not implement applicable comprehensive plan policies better than the current map designation.

**RECOMMENDATIONS:**

In the matter of Z19-501COMP, a request by Dwight Hume of Land Use Solutions and Entitlement on behalf of the Francis Nevada Partnership to change the land use plan designation on 0.51 acres of land from “Residential 4-10” to “General Commercial” with a corresponding change of the implementing zoning to CB-55 (Community Business, 55-foot height limit), as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **DENY** the requested amendment to the Land Use Plan Map of the City’s Comprehensive Plan with corresponding amendment to the City’s Zoning Map, and authorizes the President to prepare and sign on the Commission’s behalf a written decision setting forth the Commission’s findings, conclusions, and recommendation on the Application.

---

**Todd Beyreuther, President**

Spokane Plan Commission

October \_\_, 2020



NOTE: The following comment was received after completion of the Staff Report and after the Plan Commission closed the public record for their consideration of this proposal.

October 13, 2020

**Rezone: 6204 N. Nevada (Parcel 35321.0209)  
1015 E. Decatur (Parcel: 36321.0210) Residential lots to CB 55**

Dear City Council Members,

My name is Sue Millersmith. I grew up in the 50's in a neighborhood in Spokane, I believe in Spokane and am passionate about it. I have owned and operated Hair Etc with 10-15 employees for 45 yrs, located on the SE corner of Francis and Nevada. I have faithfully paid state and local taxes for myself and my employees. I have seen this intersection develop with a 250' depth at the Maverick Super Gas Station on the NE corner, Dutch Bros Coffee on the NW corner, and Quick Stop/Strip Mall on the SW corner. Francis & Nevada is no longer an intersection with the blinking yellow light that was there when our Dad purchased it in the late 60's. It is now a highly trafficked intersection with 20,000 cars a day with 2 major arterials. **It has changed significantly and we must rezone and change to survive.**

Last year my family again requested to begin the rezoning process of the 2 adjoining Residential lots to CB 55, only to be postponed another year. So now we're dealing with the new LU1.8 language, which **you promised would provide and allow for exceptions for high trafficked intersections just like Nevada and Francis.**

**The intent and plan of the rezone are twofold: 1. Rezone to CB 55 to match the front lots to best and highest use or respond to the market for a more desirable site. Residential zoning is obsolete at this location.** We would like to preserve housing by moving the houses to our vacant lots down the block, east on Decatur to an environmentally safer, cleaner, quieter home for families. **2. Rezone to get the existing commercial building away from the intersection.**

We've given it the test of time. For over 60 years we've tried to make this Residential Zoning work and it just hasn't worked. It's time to rezone and move the housing to safer residential lots, and provide desperately needed space to design and expand for a more desirable site.

**The re-zone would never be an intrusion into our neighborhood.** This block of East Decatur has little land use, also made up of a house and vacant lots that we own. Our neighbors support us in this re-zone as do our Neighborhood Councils. They welcome revitalizing and bringing new life to housing before it's abandoned



because it's unbearable to live in. That's what happened to 2 houses near us that were abandoned and became drug houses. Along with drug houses came the drug dealing, drug addicts that stumble thru and pass out in our yards, lots and porches, expensive break-ins, theft, crime and garbage. Or the poor guy who camps out and urinates and defecates by our garbage can or sits at the patio table at the restaurant and leaves behind his drug paraphernalia. These are the real intrusions into our neighborhood that concern us. Businesses help neighborhoods clean up and watch out for and protect neighborhoods.

**The City has widened Nevada and Francis numerous times, chiseling down these lot sizes.** We've given up our space to the City to widen Nevada & Francis, and it's now time for the City to rezone and give us some space to expand. At one time the City talked about replacing our building to keep it out of harm's way. We deal with heavy, high volume rush hour traffic, uncountable accidents that hit our building, noise, fumes, dust and vibrations. The one house (on Nevada & Decatur)... faces Nevada, unlike other houses on Nevada. The tenant never uses her front door because it is so unsafe for her and her kids. The earthquake like vibrations cause her to hold onto her dinnerplate so it doesn't vibrate off the table or have the TV volume on high to hear because of the revving cars and trucks just feet away. She can never have the doors or windows open for air because of the dust, noise and strong fumes. **This location is no longer environmentally or physically safe for a residential home. There is no question, this residential zoning has become obsolete, dangerous and hazardous to their health. It has reached the end of its life for being zoned Residential or a dream of a safe refuge for anybody.**

**Often the traffic volume along Nevada is far worse than Francis, lining up 3-4 blocks back** waiting for a red light. There is a constant flow of impatient drivers, both cars and trucks that cut thru our small parking lot like it is a detour or "Y" to go East on Francis. They even have the nerve to flip us off in our own parking lot when it's them who are illegally cutting thru. We need relief now, we need new zoning to protect our employees and customers. For over 3 wks now the traffic is down to only 1 lane going North on Nevada, causing even more congestion & back up and "cut thru" drivers.

At one time the Francis, city street sign was so close to the building it would vibrate on our window, sounding like a hungry woodpecker. During dry summer months, the traffic kicks up the dust as if a herd of horses ran thru it. Our building



is no longer a light cream color because of the exhaust emissions, fumes and dust that have discolored it permanently. Our trees and foliage have signs of distress from the deadly car exhaust. During winter months, the snow banks rob us of even more of our much needed parking. Facing Global Warming and a La Nina year, they are predicting a colder and wetter weather pattern for Spokane....translating into heavier snow fall, ice and bigger snow banks. With ice comes the sliding cars which hit our building and have broken our foundation and siding. We no longer can use the front door, because our customers fear for their lives walking so close to speeding traffic which countless times have hit our building. One truck took the corner off the building and after I chased him down, he denied hitting it, as I asked "Then why is our green roofing and our 2x4 sticking out of the side of your semitrailer?"

We have been a mainstay for over 45 years in our neighborhood. We know what would strengthen our neighborhood so we don't have to abandon it. We are like other small businesses that in these pandemic times struggle to keep their staffs so they can feed their families and pay their bills as well. **So I encourage you to vote "YES to REZONE"** and help us survive, clean up and improve our neighborhood. Join us in being a positive influence in the city we love and are proud to call home.

Thank you,

Sue Millersmith  
1004 E. Francis  
Spokane, WA 99208









**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                                |                                                    |
|--------------------------------|----------------------------------------------------|
| <b><u>Date Rec'd</u></b>       | 10/26/2020                                         |
| <b><u>Clerk's File #</u></b>   | ORD C35974                                         |
| <b><u>Renews #</u></b>         |                                                    |
| <b><u>Cross Ref #</u></b>      |                                                    |
| <b><u>Project #</u></b>        | Z19-502COMP                                        |
| <b><u>Bid #</u></b>            |                                                    |
| <b><u>Requisition #</u></b>    |                                                    |
| <b><u>Agenda Item Name</u></b> | 0650 – COMPREHENSIVE PLAN AMENDMENT – 29TH AND RAY |

**Agenda Wording**

An Ordinance relating to application Z19-502COMP by Mr. Schmelzer and Ms. Wallace, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to Office for 0.61 acres and a change to the Zoning Map from RSF to O-35.

**Summary (Background)**

The proposal concerns 3203 & 3207 E 29th Ave (pcls 35273.0219 and .0220) expanded by Council to include 2820 & 2826 S Ray St (pcls 35273.0305 and .0306). This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on Sept 8 to consider this amendment and issued a split recommendation.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |

|         |    |   |
|---------|----|---|
| Neutral | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |

**Approvals**

|                                 |                   |
|---------------------------------|-------------------|
| <b><u>Dept Head</u></b>         | MEULER, LOUIS     |
| <b><u>Division Director</u></b> | SIMMONS, SCOTT M. |
| <b><u>Finance</u></b>           | ORLOB, KIMBERLY   |
| <b><u>Legal</u></b>             | RICHMAN, JAMES    |
| <b><u>For the Mayor</u></b>     | ORMSBY, MICHAEL   |

**Council Notifications**

|                                   |                           |
|-----------------------------------|---------------------------|
| <b><u>Study Session\Other</u></b> | CC Study Session 10-29-20 |
| <b><u>Council Sponsor</u></b>     | CM Mumm                   |

**Distribution List**

|                           |
|---------------------------|
| tblack@spokanecity.org    |
| jrichman@spokanecity.org  |
| sbishop@spokanecity.org   |
| lmeuler@spokanecity.org   |
| dhume@spokane-landuse.com |

**Additional Approvals**

**Purchasing**



Ordinance No. C35974

AN ORDINANCE RELATING TO APPLICATION FILE Z19-502COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "OFFICE" FOR APPROXIMATELY 0.61 ACRES LOCATED AT 3207 AND 3203 E 29<sup>TH</sup> AVENUE AND 2820 AND 2826 S RAY STREET (PARCELS 35273.0219, 35273.0220, 35273.0305, AND 35273.0306) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "OFFICE (O-35)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-502COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-502COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 0.61 acres from "Residential 4-10" to "Office"; if approved, the implementing zoning destination requested is "Office (O-35)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-502COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-502COMP as it concerns parcels 35273.0219 and 35273.0220 is inconsistent with the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-502COMP as it concerns 35273.0305 and 35273.0306 is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend denial of the proposal as it concerns parcels 35273.0219 and 35273.0220; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend approval of the proposal as it concerns parcels 35273.0305 and 35273.0306; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-502COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "Office" for 0.61 acres, as shown in Exhibits A and B.



3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from “Residential Single Family” to “Office (O35)” as shown in Exhibits C and D.

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





## Z19-502COMP

(3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

### EXHIBIT A: Existing Land Use Plan Map



### Legend

Subject Parcels

Parcel

Curb Line

#### Land Use Plan Designation

Residential 4-10

Residential 15-30

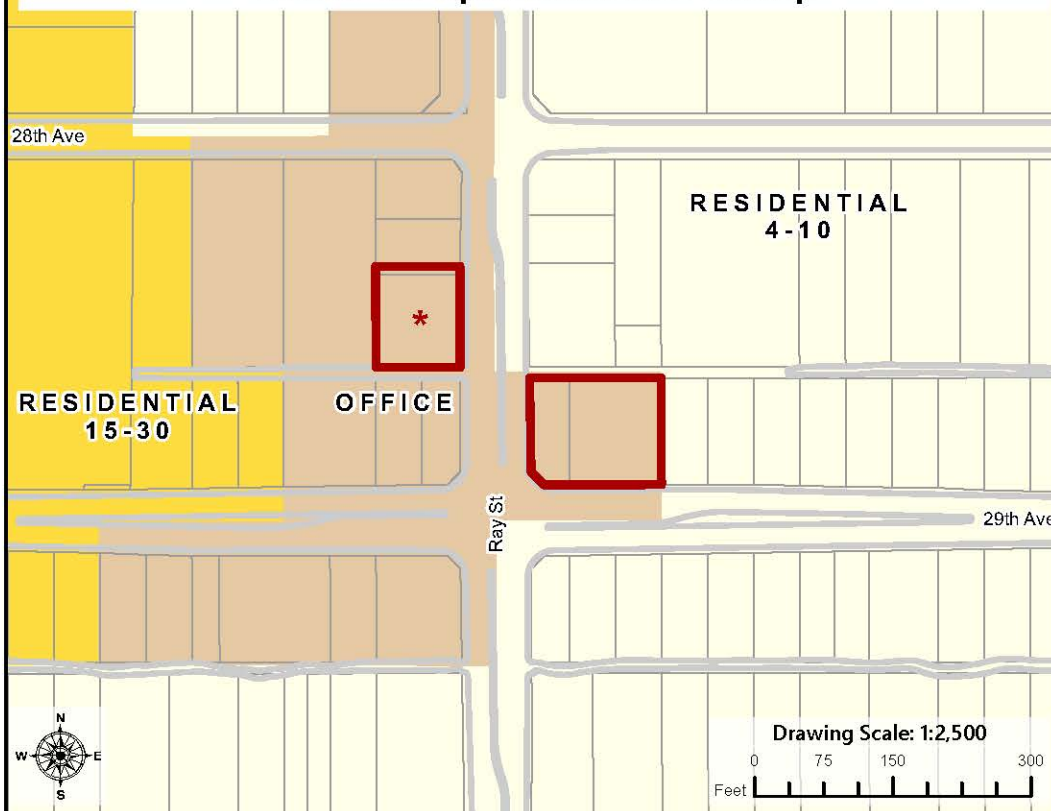
Office

CC Core

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Acres (Proposal): 0.61

### EXHIBIT B: Proposed Land Use Plan Map



### Legend

Subject Parcels

Parcel

Curb Line

#### Proposed Land Use

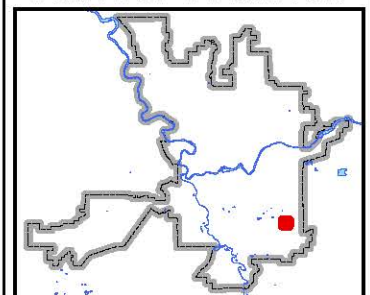
Residential 4-10

Residential 15-30

Office

CC Core

#### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# Z19-502COMP

(3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Curb Line

Parcel

### Current Zoning

Center and Corridor Type 2 (CC2)

Office (O)

Residential Multifamily (RMF)

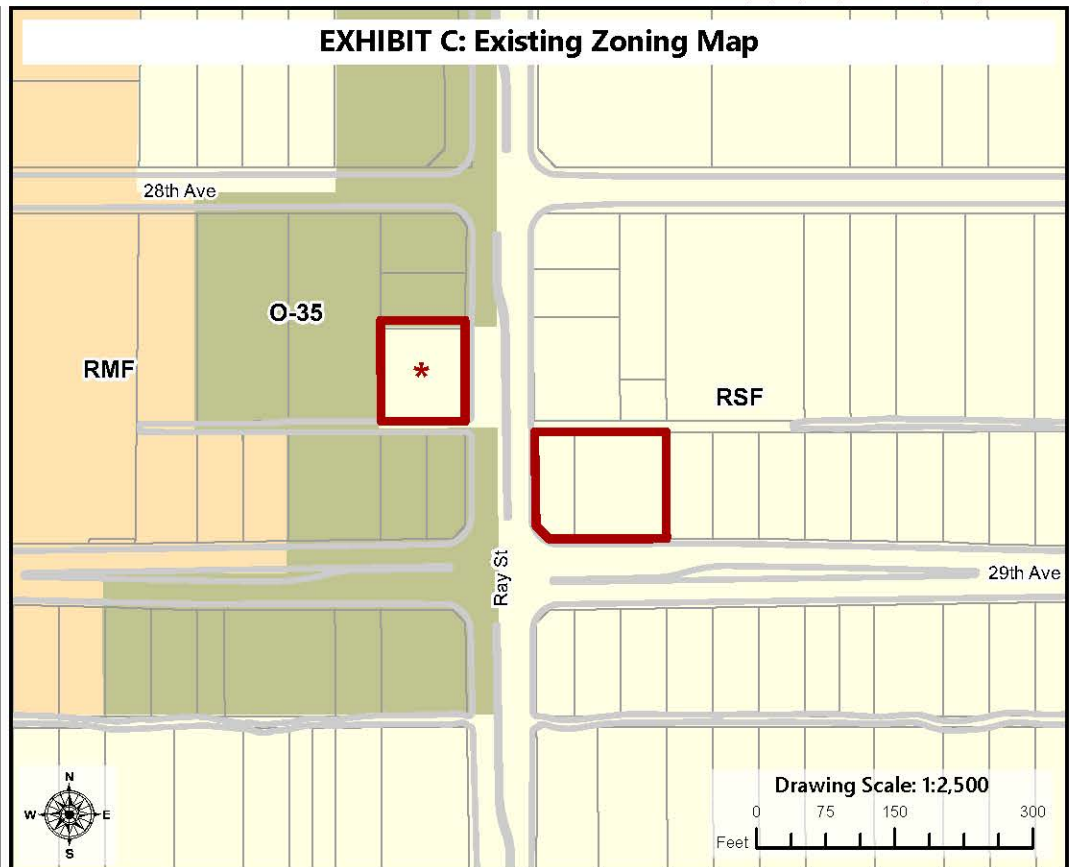
Residential Single-Family (RSF)

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 0.61

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Curb Line

Parcel

### Proposed Zoning

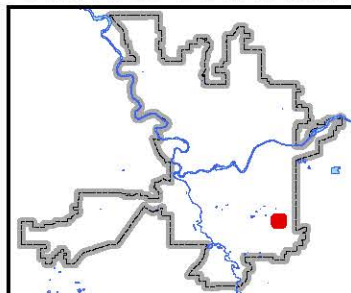
Center and Corridor Type 2 (CC2)

Office (O)

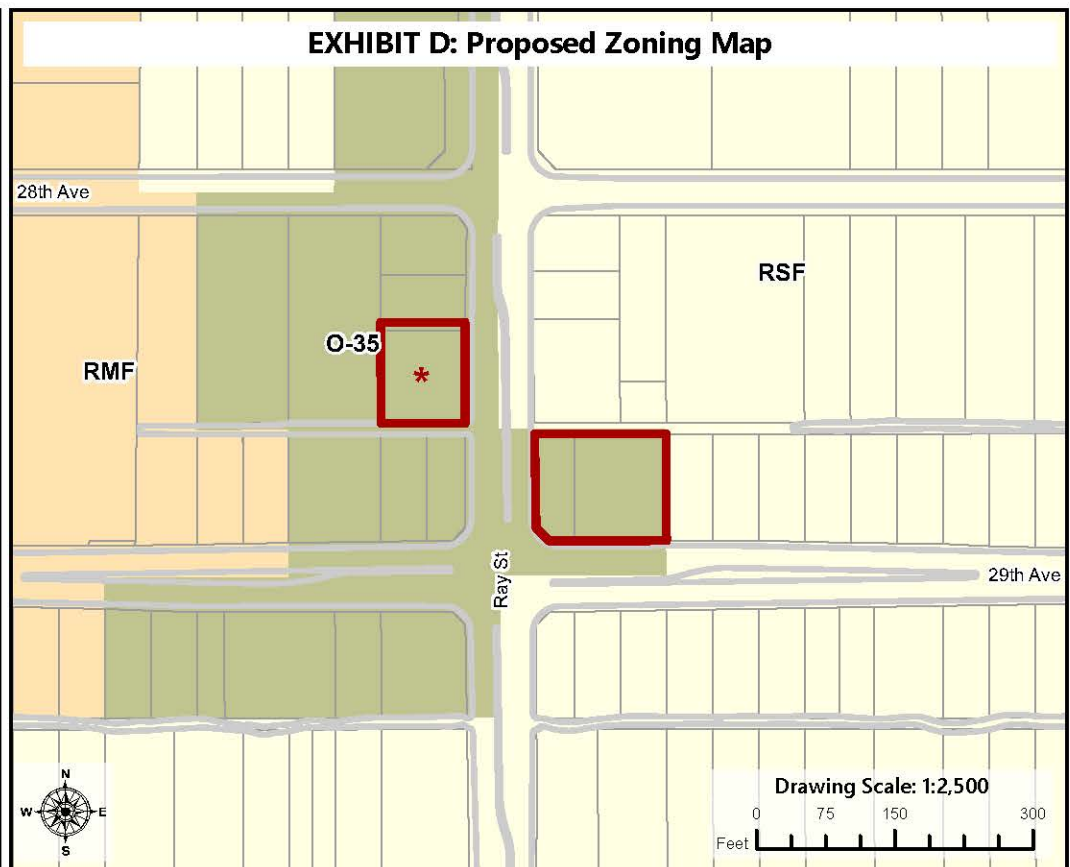
Residential Multifamily (RMF)

Residential Single-Family (RSF)

### PROJECT LOCATION



## EXHIBIT D: Proposed Zoning Map





**Exhibit E: Legal Description**

Lots 22, 23, and 24, Block 70, Lincoln Heights Addition, except streets; South 100' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. Streets; and South 60' of the north 125' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. Streets, all in the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-502COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map (LU 1) designation and a concurrent change to the zoning classification of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                                                                                                                                                                                                             |
|---------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 35273.0219, 35273.0220, 35273.0305, and 35273.0306                                                                                                                                                                                                          |
| <b>Address(es):</b>       | 3207 E 29 <sup>th</sup> Ave, 3203 E 29 <sup>th</sup> Ave, 2820 S Ray St, and 2826 S Ray St                                                                                                                                                                  |
| <b>Property Size:</b>     | 0.61 Acres                                                                                                                                                                                                                                                  |
| <b>Legal Description:</b> | Lots 22, 23, and 24, Block 70, Lincoln Heights Addition, except streets.<br>South 100' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. streets.<br>South 60' of the north 125' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. streets. |
| <b>General Location:</b>  | Northeast of the Intersection of E 29 <sup>th</sup> Avenue and S Ray Street –also-<br>100 feet north of the same intersection on the west side of S Ray Street.                                                                                             |
| <b>Current Use:</b>       | Vacant land, commercial parking lot, residential backyard.                                                                                                                                                                                                  |

**II. APPLICANT SUMMARY**

*Note that the City Council expanded the geographic scope of this application. As a result, this application has two applicants—a private applicant and the City of Spokane itself. The following information regards the original private applicant:*

|                        |                                                 |
|------------------------|-------------------------------------------------|
| <b>Agent:</b>          | Dwight Hume, Land Use Solutions and Entitlement |
| <b>Applicant:</b>      | Ryan Schmelzer and Paige Wallace                |
| <b>Property Owner:</b> | Same for parcels 35273.0219 and 35273.0220      |

*The following information regards the two properties applied for by the City:*

|                        |                                                                                     |
|------------------------|-------------------------------------------------------------------------------------|
| <b>Representative:</b> | Kevin Freibott, Neighborhood and Planning Services                                  |
| <b>Applicant:</b>      | City of Spokane                                                                     |
| <b>Property Owner:</b> | Ryan C & Melanie L Allen (Parcel 35273.0305)<br>Romney ETux, DP (Parcel 35273.0306) |



### III. PROPOSAL SUMMARY

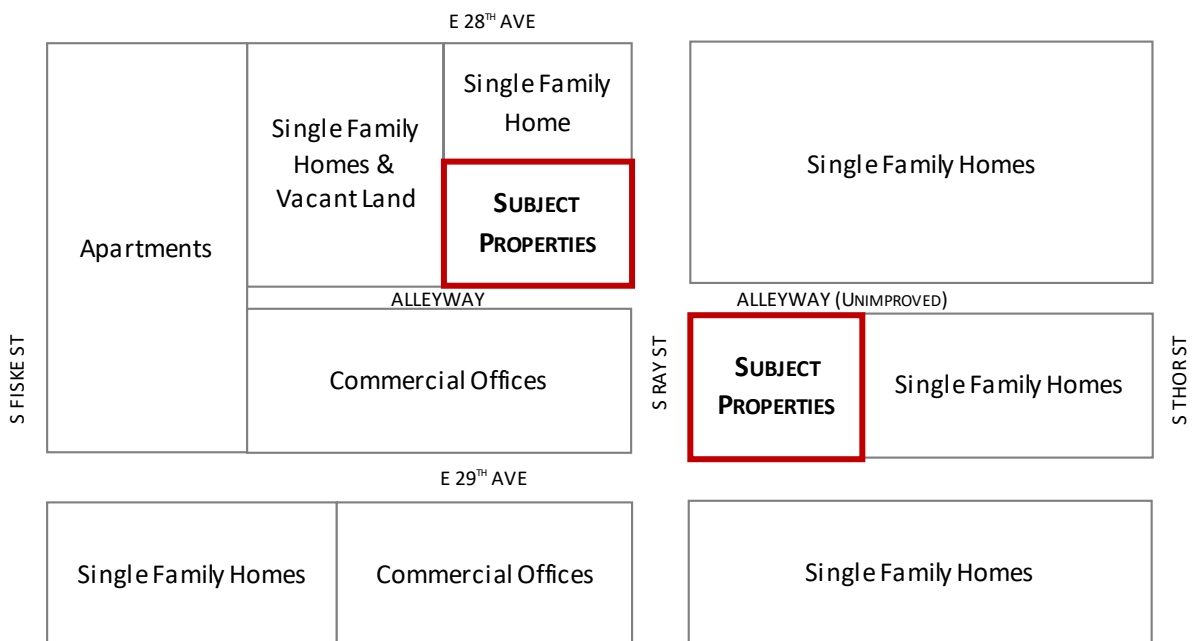
|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 DUs/Acre (R 4-10)                                                                                                          |
| <b>Proposed Land Use Designation:</b> | Office (O)                                                                                                                                  |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | Office, 35-foot height limit (O-35)                                                                                                         |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | Partial (see end of report)                                                                                                                 |

### IV. BACKGROUND INFORMATION

- 1. General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the Lincoln Heights neighborhood. The intent of the applicant is to potentially develop non-residential uses on both subject lots, all in common ownership by the applicant. During the threshold review process, the City Council added two additional properties to the proposal, both on the west side of S Ray Street. No new development is proposed or expected for those parcels—the City proposes simply to clean up zoning in these two locations.
- 2. Site Description and Physical Conditions:** The two parcels in the original private application, located northeast of the intersection of 29<sup>th</sup> Ave and Ray St, are currently vacant. A single family home shown in the aerial photographs for the site was removed (by permit) prior to this application. Parcel 35273.0306, 100 feet north of the intersection on the west side of Ray Street, contains a pre-existing commercial parking lot. As for parcel 35273.0305, only the land use of the southern 10 feet would be amended by the proposal. This portion of the parcel contains a residential back yard and part of a garage. The remainder of that parcel is already designated for office uses.
- 3. Property Ownership:** Parcels 35273.0219 and 35273.0220 are both owned by the same owners, Mr. Ryan Schmelzer and Ms. Paige Wallace. Parcel 35273.0305 (the residential backyard) is owned by Mr. and Mrs. Allen. Parcel 35273.0306 (the commercial parking lot) is owned by Romney ETux. The owners of parcels 35273.0305 and 35273.0306 were contacted via mail by the City, but they did not respond.



4. **Adjacent Property Improvements and Uses:** The proposal is in two parts—those parcels west of Ray Street were added by the City to the application, those east of Ray Street represent the original, private application. The proposal is surrounded by existing development of the following nature:



5. **Street Class Designations:** E 29<sup>th</sup> Avenue and S Ray Street are designated as Major Arterials. E 29<sup>th</sup> Avenue is designated as a local street. The alleyway that is currently improved west of Ray Street is designated as an alley on the City’s Arterial Street Map. However, the alleyway east of Ray Street is currently undesignated until approximately 175 feet east of the subject properties. No change of street class designation is proposed as part of this application. Nor does map TR-12 of the Comprehensive Plan call for a change in designation for any of these streets in the future.
6. **Current Land Use Designation and History:** As shown in Exhibit A, the subject properties are currently designated for the “Residential 4-10” (between 4 and 10 dwelling units per acre), a designation typically reserved for single-family homes. The subject properties have been designated for this use since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001.

There have been two previous proposals to amend the land use of parcels 35273.0219 and 35273.0220, one in 1985 and one in 1992. The 1985 proposal was denied by the City Council (Ordinance C27821). The proposal in 1992, itself an amendment to the now defunct Lincoln Heights Specific Plan (incorporated by reference into the Comprehensive Plan) was originally approved by the City Council, but that action was overturned by a successful appeal from local resident June Pierce and others (File 91-102-LU). In summary, neither of these proposals was successful, thus the designated land use for parcels 35273.0219 and 35273.0220 has remained Residential 4-10 (or its equivalent) since at least 1987.

The designated land use for the two parcels added to the proposal by City Council, namely parcels 35273.0305, and 35273.0306, has remained unchanged according to available records. Parcel 35273.0306 in its entirety has been designated R 4-10 or equivalent since the adoption of the first



GMA-compliant comprehensive plan in 2001. The proposal only concerns the southern nine feet of the parcel, as that is the only portion still designated for Residential 4-10. The remaining northern portion of the parcel was designated for Office uses since at least the 2001 Comprehensive Plan.

7. **Proposed Land Use Designation:** As shown in Exhibit B, the proposal is to amend the Land Use Plan Map designation so that the entirety of the subject properties are designated for Office uses.
8. **Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF). This zoning has been the same since the current zoning map was originally adopted in 2006. Historically, the subject properties have been zoned for single-family residential (or similar) since at least 1958. In 1958 the subject properties were entirely surrounded by Class I residential (single family) zoning. As time has passed, more intense zones like office and multi-family residential have been added west of Ray Street, but those changes have not extended to the subject parcels. All other properties west of Ray Street are now zoned a mix of office and multi-family zones, while the subject properties on that side of Ray are among the last few zoned for single-family residential. All properties north of 27<sup>th</sup> Avenue, east of Ray Street, and south of one half block south of 29<sup>th</sup> Street are zoned for single-family residential. As such, this area comprises the edge of commercial and multi-family zoning associated with the Lincoln Heights Center.

See item 6 above for more information on past efforts to change the land use and zoning for two of the subject parcels—those located northeast of the intersection of 29<sup>th</sup> Ave and Ray Street.

9. **Proposed Zoning:** As shown in Exhibit D, the proposal seeks to amend the zoning so that the entirety of the subject properties are zoned Office with a 35-foot height limit (O-35).

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | June 24, 2020     |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



SEPA Determination Issued ..... August 24, 2020

Notice of Public Hearing Posted ..... August 26, 2020

Plan Commission Hearing Date (Scheduled) ..... September 9, 2020

**2. Comments Received:** A request for comments was issued to City departments, local agencies, and neighborhoods within 400 feet of the proposal, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- Randy Abrahamson, Spokane Tribe Historic Preservation Officer
- Carol Tomsic, Lincoln-Heights Neighborhood Chair

Mr. Abrahamson indicated a low probability of cultural resources on the subject parcels, though he recommends the requirement for an inadvertent discovery plan for any future development on these sites. Ms. Tomsic provided several comments on the SEPA checklist, noting:

- The home previously located on the two parcels northeast of the intersection of 29<sup>th</sup> and Ray.
- The lack of any office zoning south of the subject parcels.
- The value of the parcels northeast of the intersection as a buffer between the residences and the more dense development to the northwest.
- The two previous attempts to change the land use and zoning, and the neighborhood's general opposition to those actions.
- Access limitations for the two parcels northeast of the intersection.
- The need to improve the alleyway north of the parcels northeast of the intersection.
- A reminder that traffic volumes for this intersection are available in the 29<sup>th</sup> Avenue Traffic Corridor Study

Copies of these comments are included in this staff report as **Exhibit L**.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. The following comments were received during the 60-day public comment period:

- Makaya Judge
- Lincoln Heights Neighborhood Council
- Michele Martz
- June Pierce
- Carol Tomsic, Lincoln Heights Neighborhood Chair (2 letters)

All of the commenters cited above expressed opposition to the change in designation and zoning for the two properties northeast of 29<sup>th</sup> and Ray. No comments identified any issues with the two city-



added parcels on the west side of Ray Street. Concerns raised in the letters included traffic and circulation, encroachment into an established single-family neighborhood, and the establishment of precedence that might impel more property owners on the east side of Ray St to ask for a similar designation and zoning in the future. Mrs. Pierce provided additional information regarding past efforts to make the same change to those two properties to the northeast of 29<sup>th</sup> and Ray (see discussion under item 6 above). Copies of these comments are included in this staff report as **Exhibit M**.

3. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 24, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was provided an opportunity to speak during the workshop but no public comment was taken.

## **VI. APPLICATION REVIEW AND ANALYSIS**

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:
  - A. Keep the comprehensive plan alive and responsive to the community.
  - B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
  - C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
  - D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
  - E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
  - F. Amendments to the comprehensive plan must result in a net benefit to the general public.
2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.
  - A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.



The proposal meets this criterion.

- B. GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, “Planning Goals”), and these goals guided the City’s development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. Financing:** *In keeping with the GMA’s requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency comment request or require a traffic impact analysis for the proposal. The subject property is already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

- E. Internal Consistency:**

- 1. The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:



*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.

*Neighborhood Planning Documents Adopted after 2001.* The Lincoln Heights neighborhood joined the Cliff Cannon, Manito/Cannon Hill, Rockwood, and Comstock neighborhoods to form the South Hill Coalition. These five neighborhoods combined their initial neighborhood planning funds provided by the city in order to prepare and adopt the *South Hill Coalition Connectivity and Livability Strategic Plan* (the CLSP) in 2014. Included in the priorities for Lincoln Heights in the CLSP is "Improving the interface between residential and business properties" (p. 16). Of the various projects and goals in the plan, none concerned or were located in close proximity to the subject properties. A proposed greenway was included in the projects described by the CLSP terminating at 27<sup>th</sup> Ave and Ray St, however the subject properties are unlikely to affect the eventual implementation of such a greenway. Proposed arterial streetscape improvement described by the plan to 29<sup>th</sup> Ave end west of the subject properties at Fiske St. As such, it is considered unlikely that the proposal would affect the implementation of the CLSP.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

See Item K.2 for below for analysis and results.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: As discussed under item K.2.a below, the proposal is in conflict with the Comprehensive Plan. As a map change proposal, this application does not include any amendment to the text of the plan, including any policies with which it is in conflict.

The proposal is in conflict with this criterion.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*



Staff Analysis: No evidence has been provided by any adjacent jurisdiction, including the County of Spokane, indicating this proposal would conflict with the CWPP or the plans of any neighboring jurisdiction. The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues.

The proposal meets this criterion.

**G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

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<sup>3</sup> State Environmental Policy Act



Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

- I. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of a previously developed area served by the public facilities and services described in CFU 2.1. The proposed change in land-use designations affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

**K. Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

The proposal meets this criterion.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*



- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.5, Office Uses, which directs new office uses to “Centers and Corridors designated on the Land Use Plan Map.” The subject properties are located more than 500 feet from the nearest center, the Lincoln Heights District Center. However, since the adoption of the Centers and Corridors development strategy in the 2001 Comprehensive Plan, uses have to the west of the subject parcels have slowly converted to multi-family residential and office uses. The two parcels added to the proposal by City Council are surrounded on three sides by Office designations, adopted per policy in the Comprehensive Plan that calls for greater density surrounding Centers. Conversely, the two parcels included by private application in the proposal are surrounded on three sides by single-family residential.

Policy LU 1.5 provides some opportunity for the designation of Office uses outside Centers, stating that Office uses are appropriate where it continues an “existing office development trend” and where serving as a transitional land use between the denser Center uses and lower density uses such as single-family residential. However, the policy also states, “Arterial frontages that are predominantly developed with single-family residences should not be disrupted with office uses.” This requirement directly concerns the two parcels in the original request (parcels 35273.0219 and 35273.0220), located on the east side of Ray Street. Existing development on the east side of Ray Street almost exclusively consists of single-family homes. Going north from the two parcels, non-SFR buildings begin to be seen north of 28th Avenue (representing non-residential uses allowed in that zone). However, between 28th Avenue in the north and 37th Avenue in the south, the east side of Ray Street is exclusively single-family homes and some vacant lots. Accordingly, the addition of office uses to the east side of Ray Street as proposed would appear to conflict with the requirements of Policy LU 1.5.

Designation of parcels 35273.0219 and 35273.0220 appears inconsistent with the location requirements of policy LU 1.5. Conversely, designation of parcels 35273.0305 and 35273.0306 appears consistent with the requirements of policy LU 1.5.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude physical development of office uses on the site. The site is adequately served by all utilities and by two major arterial streets, bus service is nearby, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.



Staff Analysis: As discussed in item a. above, part of the proposal appears in conflict with the requirements of the Comprehensive Plan. Additionally, there are no special factors involved that would indicate the need for additional office uses east of Ray Street.

A portion of this proposal appears to be in conflict with this criterion (see items 'a' and 'c' above).

- 3. Rezones, Land Use Plan Amendment:** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property will change from RSF (Residential Single-Family) to O-35 (Office, 35-foot height limit).

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears to be in conflict with one or more approval criteria set forth by SMC 17G.020 as it regards parcels 35273.0219 and 35273.0220.

Separately, the proposal to change the designation and zoning of parcels 35273.0305 and 35273.0306 appears consistent with the approval criteria.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Regarding the two parcels included in the original private request for an amendment—parcels 35273.0219 and 35273.0220—according to the information and analysis presented above, one could argue that the proposal to amend the Land Use Plan Map designation and zoning as of these two properties would be inconsistent with the approval criteria.

Regarding the two parcels added to the proposal by City Council—parcels 35273.0305, and 35273.0306—amendment to the Land Use Plan Map designation and zoning of the two parcels meets the requirements of the Comprehensive Plan and Spokane Municipal Code for such an amendment and staff recommends that Plan Commission and City Council approve this part of the proposal.



## **IX. LIST OF EXHIBITS**

- A. Existing Land Use Plan Map
- B. Proposed Land Use Plan Map
- C. Existing Zoning Map
- D. Proposed Zoning Map
- E. Application Notification Area
- F. Detail Aerial
- G. Wide-Area Aerial
- H. List of Relevant Comp Plan Policies
- I. Application Materials
- J. SEPA Checklist
- K. SEPA Determination of Non-Significance
- L. Agency Comments
- M. Public Comments





# Z19-502COMP

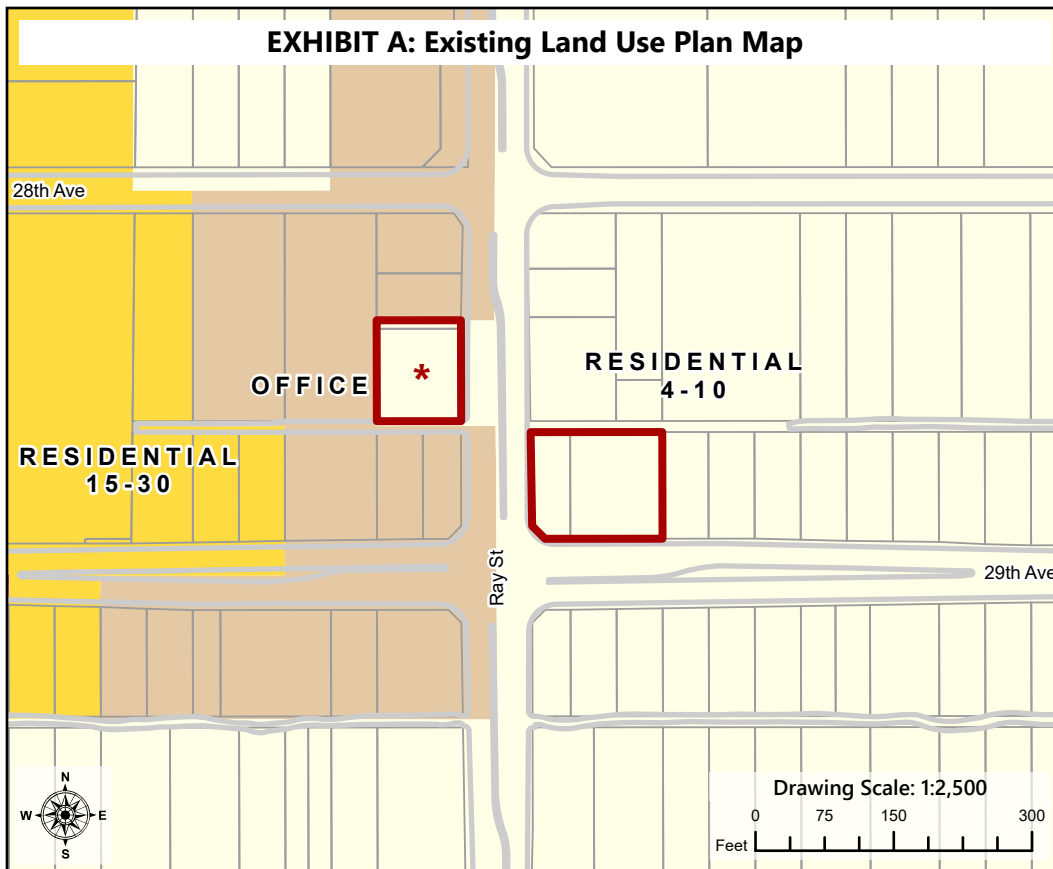
(3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



## Legend

Subject Parcels

Parcel

Curb Line

### Land Use Plan Designation

Residential 4-10

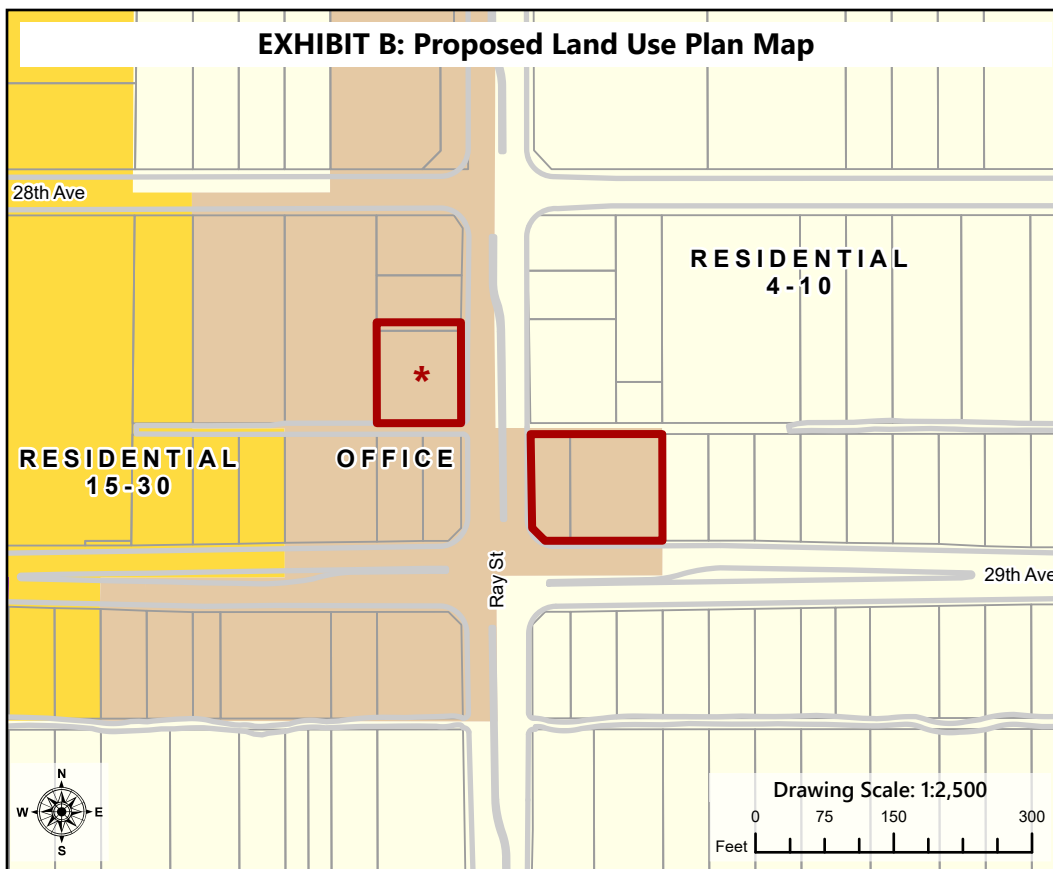
Residential 15-30

Office

CC Core

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Acres (Proposal): 0.61



## Legend

Subject Parcels

Parcel

Curb Line

### Proposed Land Use

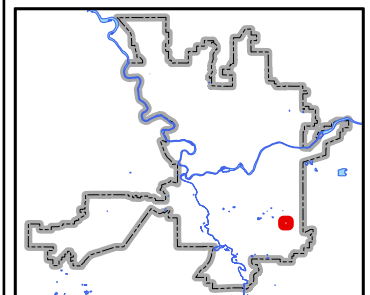
Residential 4-10

Residential 15-30

Office

CC Core

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-502COMP

(3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

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## Legend

Subject Parcels

Curb Line

Parcel

### Current Zoning

Center and Corridor Type 2 (CC2)

Office (O)

Residential Multifamily (RMF)

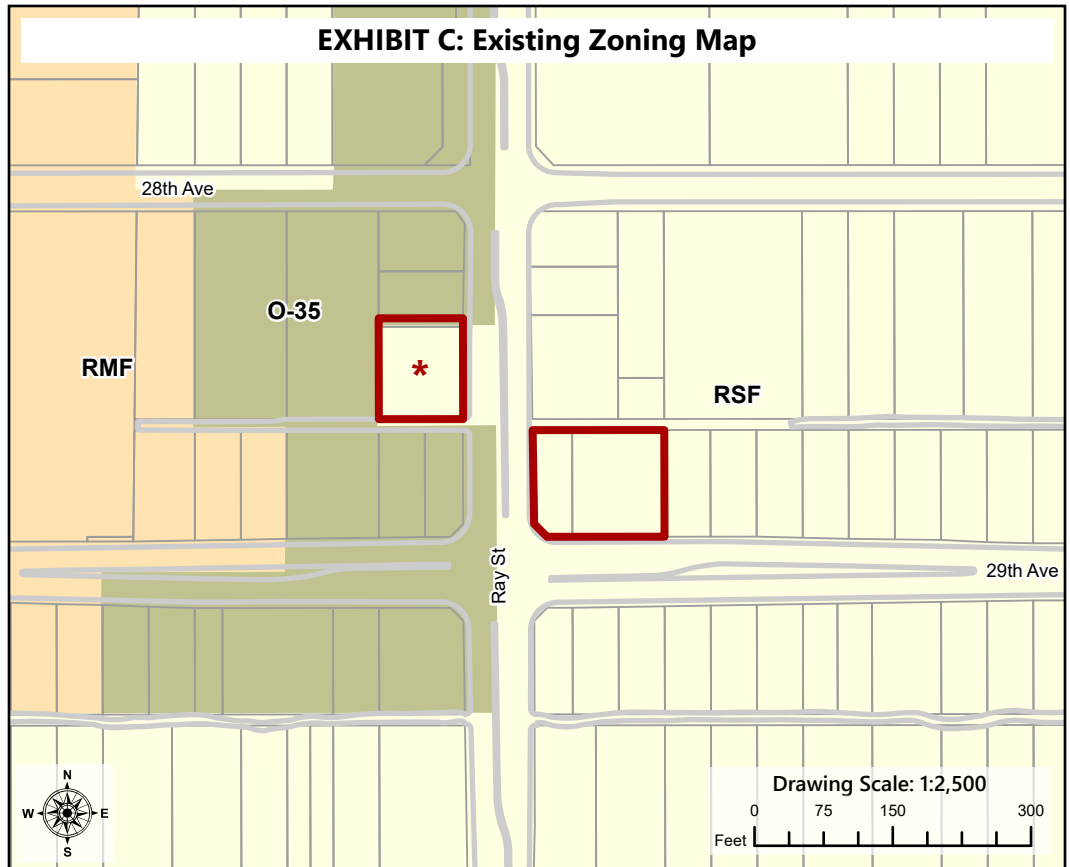
Residential Single-Family (RSF)

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 0.61

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Curb Line

Parcel

### Proposed Zoning

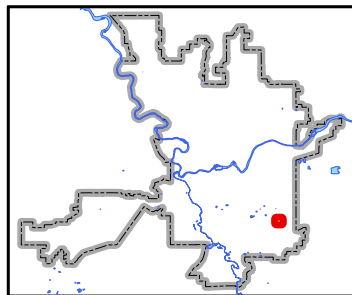
Center and Corridor Type 2 (CC2)

Office (O)

Residential Multifamily (RMF)

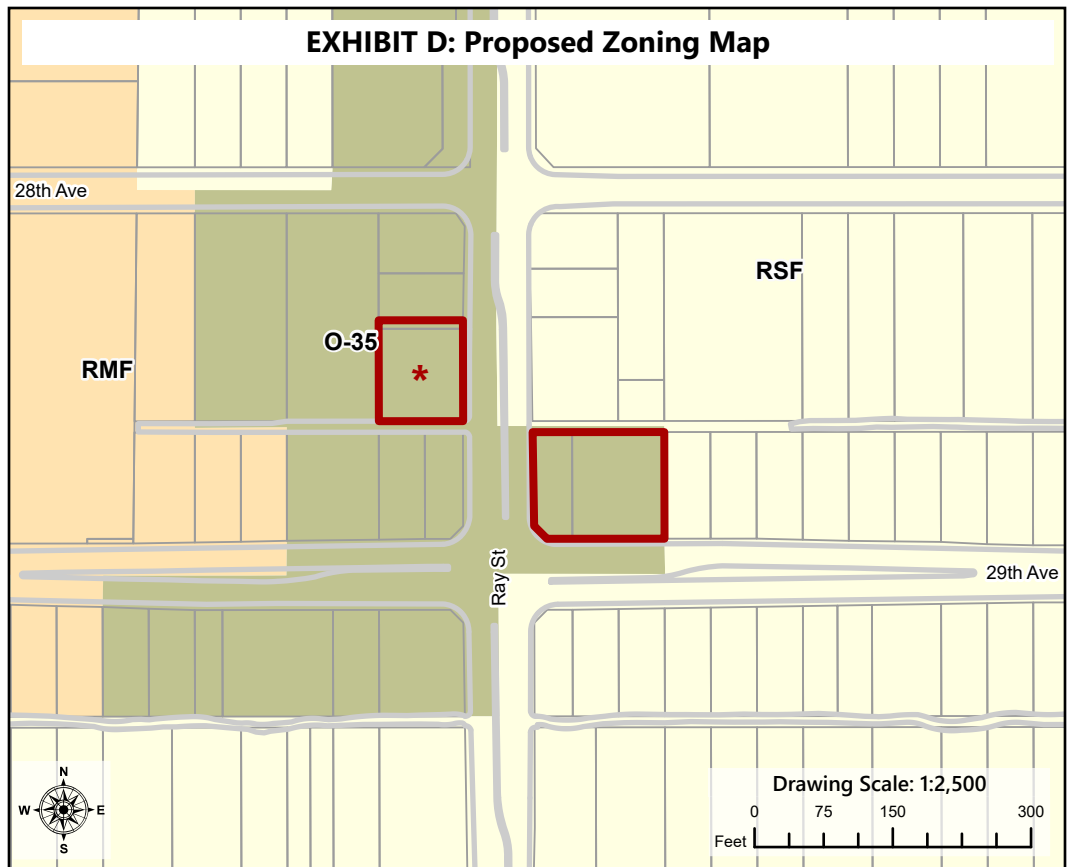
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map







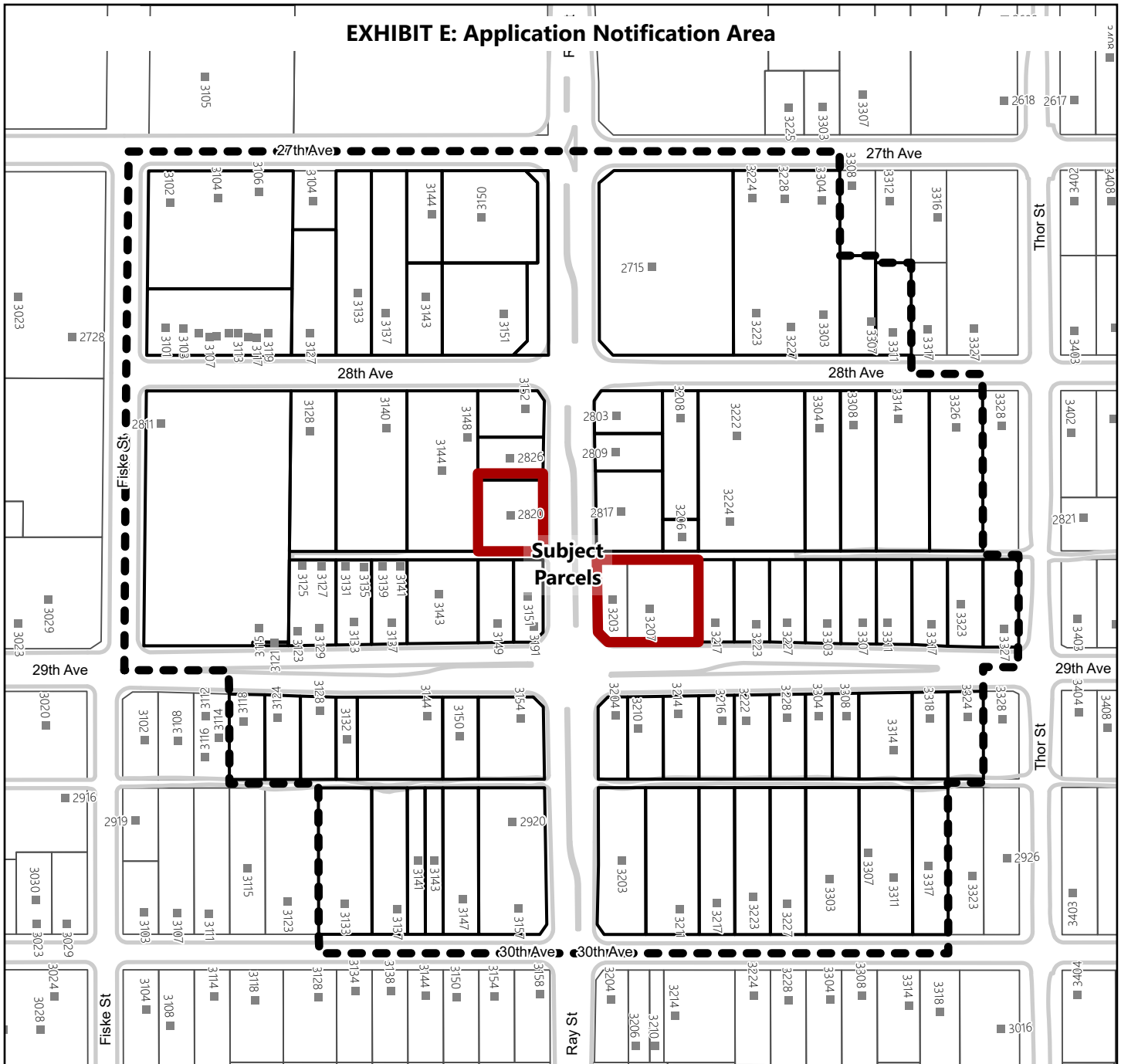
# Z19-502COMP

## (3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

THIS IS NOT A LEGAL DOCUMENT: The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

### EXHIBIT E: Application Notification Area



#### Legend

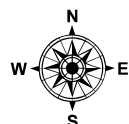
- Parcel
- Notification Boundary
- Curb Line
- Address Point

#### Subject Parcels

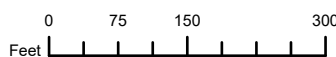
- Subject Parcels

#### Application proposes to:

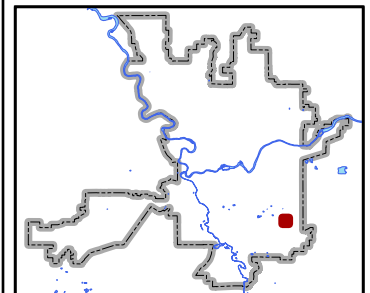
Change Land Use Designation from Residential 4-10 to Office



Project Size: 0.61 Acres (Approximate)  
Drawing Date: 3/4/2020 Drawing Scale: 1:2,500



#### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





## Z19-502COMP

(3203 & 3207 E 29th Ave and 2820 & 2826 S Ray St)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

**EXHIBIT F: Detail Aerial**



### Legend

Subject Parcels

Adjacent Ownership

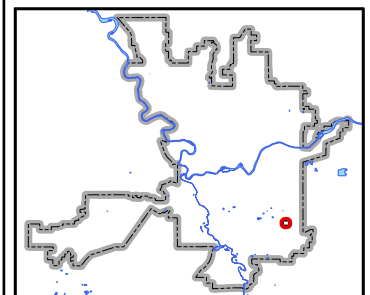
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 0.61

**EXHIBIT G: Wide-Area Aerial**



### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-502COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.5 Office Uses

Direct new office uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* Office use of various types is an important component of a Center. Offices provide necessary services and employment opportunities for residents of a Center and the surrounding neighborhood. Office use in Centers may be in multi-story structures in the core area of the Center and transition to low-rise structures at the edge.

To ensure that the market for office use is directed to Centers, future office use is generally limited in other areas. The Office designations located outside Centers are generally confined to the boundaries of existing Office designations. Office use within these boundaries is allowed outside of a Center.

The Office designation is also located where it continues an existing office development trend and serves as a transitional land use between higher intensity commercial uses on one side of a principal arterial street and a lower density residential area on the opposite side of the street. Arterial frontages that are predominantly developed with single-family residences should not be disrupted with office use. For example, office use is encouraged in areas designated Office along the south side of Francis Avenue between Cannon Street and Market Street to a depth of not more than approximately 140 feet from Francis Avenue.

Drive-through facilities associated with offices such as drive-through banks should be allowed only along a principal arterial street subject to size limitations and design guidelines. Ingress and egress for office use should be from the arterial street. Uses such as freestanding sit-down restaurants or retail are appropriate only in the Office designation located in higher intensity office areas around downtown Spokane.



Residential uses are permitted in the form of single-family homes on individual lots, upper-floor apartments above offices, or other higher density residential uses.

### LU 3.1 Coordinated and Efficient Land Use

Encourage coordinated and efficient growth and development through infrastructure financing and construction programs, tax and regulatory incentives, and by focusing growth in areas where adequate services and facilities exist or can be economically extended.

*Discussion:* Future growth should be directed to locations where adequate services and facilities are available. Otherwise, services and facilities should be extended or upgraded only when it is economically feasible to do so.

The Centers and Corridors designated on the Land Use Plan Map are the areas of the city where incentives and other tools should be used to encourage infill development, redevelopment and new development. Examples of incentives the city could use include assuring public participation, using public facilities and lower development fees to attract investment, assisting with project financing, zoning for mixed-use and higher density development, encouraging rehabilitation, providing in-kind assistance, streamlining the permit process, providing public services, and addressing toxic contamination, among other things.

### LU 3.2 Centers and Corridors

Designate Centers and Corridors (neighborhood scale, community or district scale, and regional scale) on the Land Use Plan Map that encourage a mix of uses and activities around which growth is focused.

*Discussion:* Suggested Centers are designated where the potential for Center development exists. Final determination is subject to a sub-area planning process.

...

#### DISTRICT CENTER

District Centers are designated on the Land Use Plan Map. They are similar to Neighborhood Centers, but the density of housing is greater (up to 44 dwelling units per acre in the core area of the center) and the size and scale of schools, parks, and shopping facilities are larger because they serve a larger portion of the city. As a general rule, the size of the District Center, including the higher density housing surrounding the Center, should be approximately 30 to 50 square blocks.

As with a Neighborhood Center, new buildings are oriented to the street and parking lots are located behind or on the side of buildings whenever possible. A central gathering place, such as a civic green, square, or park is provided. To identify the District Center as a major activity area, it is important to encourage buildings in the core area of the District center to be taller. Buildings up to five stories are encouraged in this area.

The circulation system is designed so pedestrian access between residential areas and the District Center is provided. Frequent transit service, walkways, and bicycle paths link District Centers and the downtown area.

The following locations are designated as District Centers on the Land Use Plan Map:



- Shadle – Alberta and Wellesley;
- Lincoln Heights– 29th and Regal;
- Southgate;
- 57th and Regal
- Grand District
- Five Mile – Francis and Ash (suggested Center, with final determination subject to a sub-area planning process described in LU 3.4); and
- NorthTown – Division and Wellesley (suggested Center, with final determination subject to a sub-area planning process described in LU 3.4).

### LU 3.5 Mix of Uses in Centers

Achieve a proportion of uses in Centers that will stimulate pedestrian activity and create mutually reinforcing land uses.

*Discussion:* Neighborhood, District, and Employment Centers are designated on the Land Use Plan Map in areas that are substantially developed. New uses in Centers should complement existing on-site and surrounding uses, yet seek to achieve a proportion of uses that will stimulate pedestrian activity and create mutually reinforcing land use patterns. Uses that will accomplish this include public, core commercial/office and residential uses.

All Centers are mixed-use areas. Some existing uses in designated Centers may fit with the Center concept; others may not. Planning for Centers should first identify the uses that do not fit and identify sites for new uses that are missing from the existing land use pattern. Ultimately, the mix of uses in a Center should seek to achieve the following minimum requirements:

| <b>Table LU 1 – Mix of Uses in Centers</b> |                            |                                       |
|--------------------------------------------|----------------------------|---------------------------------------|
| <b>Land Use</b>                            | <b>Neighborhood Center</b> | <b>District and Employment Center</b> |
| Public                                     | 10 percent                 | 10 percent                            |
| Commercial/Office                          | 20 percent                 | 30 percent                            |
| Higher-Density Housing                     | 40 percent                 | 20 percent                            |

*Note: All percentage ranges are based on site area, rather than square footage of building area.*

This recommended proportion of uses is based on site area and does not preclude additional upper floors with different uses. The ultimate mix of land uses and appropriate densities should be clarified in a site-specific planning process in order to address site-related issues such as community context, topography, infrastructure capacities, transit service frequency, and arterial street accessibility. Special care should be taken to respect the context of the site and the character of surrounding existing neighborhoods. The 10 percent public use component is considered a goal and should include land devoted to parks, plazas, open space, and public facilities.

### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

## **Chapter 11—Neighborhoods**

### N 8.4 Consistency of Plans



Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-29-19

Kevin Freibott, Planner II  
Planning & Development Services  
3<sup>rd</sup> Floor City Hall  
West 801 Spokane Falls Blvd.  
Spokane WA 99201

Ref: 29<sup>th</sup> and Ray NEC Map Amendment R 4-10 to Office

Dear Kevin:

Per requirements of the City of Spokane, I have enclosed the completed Early Threshold Review form, the General Application, signed by the Owner, maps depicting the land use designation and zone for the subject property, together with a check for the docketing process.

The subject site has been cleared of residences and posted with a For Sale sign, resulting in numerous inquiries to purchase the property, only to find that it is not zoned beyond residential low density. Hence, this request is in response to market demand and is a pre-requisite to finalizing any pending offers.

Another observation worth mentioning is the fact that the RSF zone along Ray to 17<sup>th</sup> includes 6 lineal blocks of non-residential uses immediately adjoining residential uses. Clearly, the coexistence of major non-residential uses has not impacted the remaining residential uses and/or values. These uses include 2 churches, one day care, an elementary school with parking, a fire station and neighborhood retail at 17<sup>th</sup>. So don't be deceived by the continuous RSF zone from 29<sup>th</sup> to 17<sup>th</sup>. It's not what you'd expect it to be.

Respectfully Submitted

*Dwight J Hume agent*

Dwight J Hume





**DESCRIPTION OF PROPOSAL:**

Map Amendment from R 4-10 to Office and RSF to O-35.

**ADDRESS OF SITE OF PROPOSAL:** (if not assigned yet, obtain address from Public Works before submitting application)

3203 and 3207 E 29<sup>th</sup> Avenue

**APPLICANT:**

**Name:** Ryan Schmelzer and Paige Wallace

**Address:** 3411 E 29<sup>th</sup> Avenue, Spokane WA 99223

**Phone (home):** 509-951-3553

**Phone (work):**

**Email address:** [pw724@hotmail.com](mailto:pw724@hotmail.com)

**PROPERTY OWNER:**

**Name:** Same as above

**Address:**

**Phone (home):**

**Phone (work):**

**Email address:**

**AGENT:**

**Name:** Land Use Solutions and Entitlement c/o Dwight Hume

**Address:** 9101 N Mt. View Lane Spokane WA 99218

**Phone (home):**

**Phone (work):**

435-3108

**Email address:** [dhume@spokane-landuse.com](mailto:dhume@spokane-landuse.com)

**ASSESSOR'S PARCEL NUMBERS:**

35273.0219 and 0220

**LEGAL DESCRIPTION OF SITE:**

Lots 22-24 Block 70, Lincoln Heights Addn. Except Streets.

**SIZE OF PROPERTY:**

.39 acres

**LIST SPECIFIC PERMITS REQUESTED IN THIS APPLICATION:**

Map Amendment and Zone Change









# Comprehensive Plan Amendments

## Threshold Review

### NEC 29<sup>th</sup> & Ray Map Amendment

#### **Pre-application:**

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

#### **Description of the Proposed Amendment:**

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.  
*Two vacant parcels located at the NE corner of 29<sup>th</sup> and Ray; Parcel #'s 35273.0219 and 0220 comprising .39 acres. See maps submitted herewith.*

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*The Comprehensive Plan provides guidance on the proper location of office uses. The subject site is located at an arterial intersection where the NW and SW corners are zoned Office and occupied. The request can be interpreted as "trending" in terms of land use pattern. When "trending" occurs, it is suitable for a map change within the Comprehensive Land Use Plan.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*The site is .39 acres and has limited capability for use and as stated above, the intersection has two other quadrants as office use and zoning. The request would not warrant other sub-area studies or work programs.*
3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program. *Yes, this is not an extraordinary change to the existing neighborhood, nor does it set precedence for further extension easterly.*



4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?

*As stated above, there is no purpose in expanding the Office category except as an extension of the subject property North or East. No contact has been made with either owner.*

5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC. *The annual process for amending the Comprehensive Plan is to keep the Comprehensive Plan alive and responsive to the community. As stated above, the area continues to trend toward medical and/or office services.*

*The requested amendment is consistent with the adjacent land use classification and zones and will implement many applicable Comprehensive Plan policies. The site has a full range of public services available and can accommodate a small office in close proximity to the Lincoln Heights shopping center.*

*The request is consistent with the CWPP. The CWPP encourages growth in urban areas where services and utilities already exist. When the site is further developed, the applicant or developer will be required to demonstrate that levels of service are maintained, as required by the CWPP. The CWPP also encourages the use of public transit and development where public transit is available. It is important to note that the city has adopted development regulations and policies to implement the CWPP at the City level. Thus, consistency with the CWPP is achieved.*

*The application is consistent with the goals and policies of the Growth Management Act. The GMA encourages densification, in-fill and urban development and redevelopment in areas designated for urban growth and within existing city limits. The property is within the UGA and the city limits of Spokane*

*The proposed change is consistent with the following goals of the Comprehensive Plan:*

#### **Land Use 1.5**

*The Office designation is located where it continues an existing office development trend and serves as a transitional land use.*

#### **Land Use 1.12**

*The proposed map change is consistent with LU 1.12. Existing public facilities and services are adequately available to the subject property.*



**Land Use 3.1**

*The proposed map change is consistent with LU 3.1, which encourages the efficient use of land. Under Policy LU 3.1 future growth should be directed to locations where adequate services and facilities are available.*

**Land Use 5.3**

*The Off-Site impacts are mitigated by the development standards of the city and the corner location with two access points available for ingress and egress. Accordingly, the proposed addition better ensures compliance with LU 5.3.*

**Transportation 3.1**

*Transportation and development patterns are important to support desired land uses and development patterns. This is a fully controlled arterial intersection with good visibility for non-residential uses, thus supporting office services to the community.*

**Economic Development Goal 6**

*The proposed map change is consistent with Goal ED 6, which recommends that development be located where infrastructure capacity already exist before extending infrastructure into new areas. In this case, all services are readily available.*

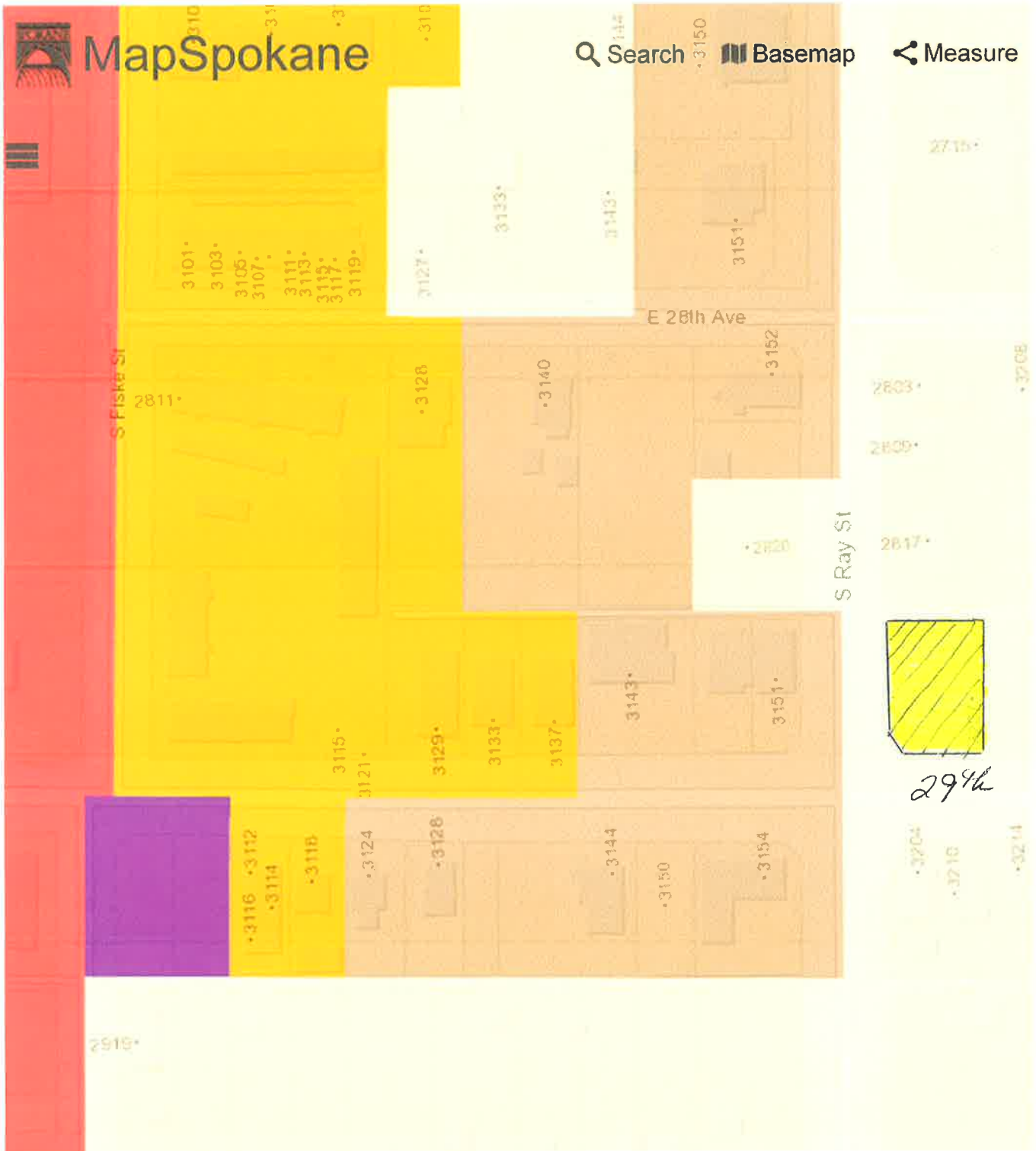
6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.  
*This is the first submittal at this corner under the adopted GMP.*
7. If this change is directed by state law or a decision of a court or administrative agency, please describe. *N/A*
8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.  
*The applicant will contact the Lincoln Heights NC to schedule a time to inform them of this request prior to Docketing.*



C

Exhibit I, p.7



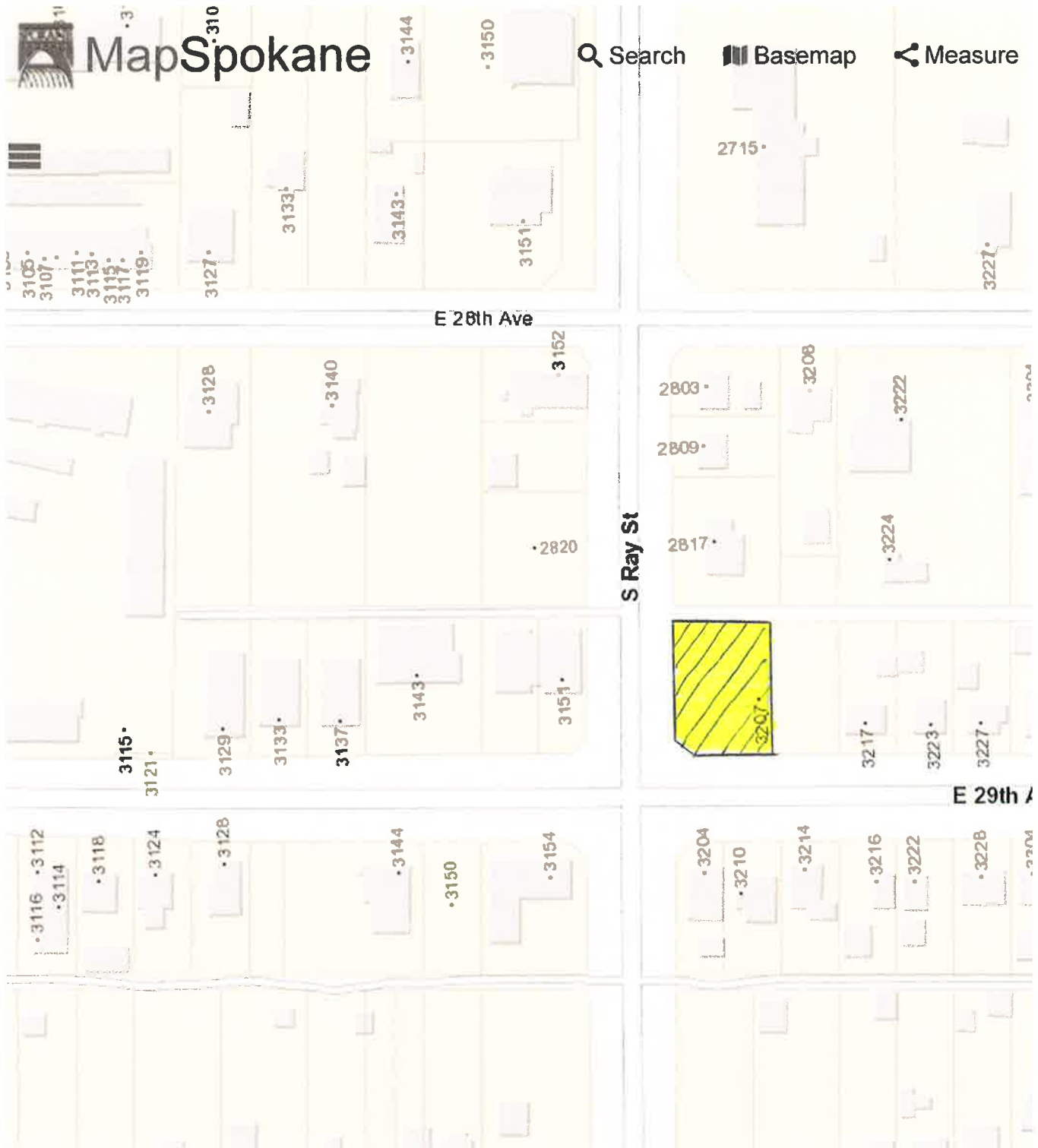


0 50 100ft



[Map Use Disclaimer](#)





[Map Use Disclaimer](#)



## Environmental Checklist

File No. Z19-502COMP

### Purpose of Checklist:

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

### Instructions for Applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### Use of checklist for non-project proposals:

Complete this checklist for non-project proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For non-project actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**Note to readers:** The Spokane City Council expanded this proposal to include additional parcels in the vicinity of the original proposal, with the same zoning and land use and similar situation/condition, pursuant to [SMC 17G.020.025](#). This proposal is now a joint private/City-sponsored application. The City has added the following properties to the proposal:

- Parcel 35273.0306 at 2820 S Ray Street, 0.21 acres in size; and
- Part of Parcel 35273.0305 at 2826 S Ray Street, 0.02 acres in size.

The following SEPA checklist was completed by the Applicant for the properties described in black below. For the properties added above, any additional information necessary for the SEPA checklist has been included in red text below.

#### A. BACKGROUND

1. Name of proposed project, if applicable: Z19-502COMP
2. Name of applicant: Ryan Schmeltzer and Paige Wallace and the City of Spokane
2. Address and phone number of applicant or contact person:  
Land Use Solutions & Entitlement Dwight Hume agent  
9101 N Mt. View Lane Spokane WA 99218 509-435-3108  
  
City Contact: Kevin Freibott, Assistant Planner, City of Spokane, 509-625-6184,  
kfreibott@spokanecity.org.
4. Date checklist prepared: March 29, 2020
5. Agency requesting checklist: City of Spokane
6. Proposed timing or schedule (including phasing, if applicable): Project is a comprehensive plan amendment, modifying the land use plan map designation and zoning of the subject properties. This action is expected to be decided late fall or winter of 2020.
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. N/A, non-project action  
  
b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. No
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Upon approval of the zone change, a project will have to comply with all applicable development regulations.



9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.  
No.
10. List any government approvals or permits that will be needed for your proposal, if known.

Comp Plan amendment and zone change

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.  
A .39 acre site to be developed for a small office use. Also, 0.23 acres currently containing a residential garage and a parking lot for an adjacent commercial use. No redevelopment or physical change to the city-added properties is anticipated.
12. Location of the proposal. Give sufficient information to a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist.  
The site is located at the NE corner of 29<sup>th</sup> and Ray and approximately 153 feet north of that intersection on the west side of Ray Street. See file for map of site.
13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.)

Yes; Sewer Service Area: Yes; City of Spokane

14. The following questions supplement Part A.
- a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)
- (1) Describe any systems, other than those designed for the disposal of sanitary waste, installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities).  
N/A, non-project action
- (2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?



N/A, non-project action

- (3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems.

N/A, non-project action

- (4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater?

N/A, non-project action

b. Stormwater

- (1) What are the depths on the site to groundwater and to bedrock (if known)?

N/A, non-project action

- (2) Will stormwater be discharged into the ground? If so, describe any potential impacts?

N/A, non-project action

**TO BE COMPLETED BY APPLICANT**

**B. ENVIRONMENTAL ELEMENTS**

**1. Earth**

- a. General description of the site (circle one): ***flat, rolling, hilly, steep slopes, mountains, other:***
- b. What is the steepest slope on the site (approximate percent slope)?  
Unknown
- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. N/A, non-project action
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. N/A, non-project action
- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill:  
To be determined at time of construction by others
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.  
No, the project does not include any construction



- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? To be determined at time of construction by others
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: To be determined at time of construction by others.

## 2. Air

- a. What type of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial, wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.  
N/A, non-project action
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.  
Drive-by traffic, minimal impacts.
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:  
To be determined at time of construction by others

## 3. Water

- a. SURFACE:
  - (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.  
No
  - (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.  
None
  - (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.  
Non project action
  - (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.  
No
  - (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.  
No



- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

To be determined at time of construction by others

b. GROUND:

- (1) Will groundwater be withdrawn, or will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sanitary waste treatment facility. Describe the general size of the system, the number of houses to be served (if applicable) or the number of persons the system(s) are expected to serve.

None as the project will be served by public sewer. No future project is proposed for the city-added sites, but they are likewise served by City sewer.

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

Run-off would be limited to stormwater and discharged into existing city storm drains

- (2) Could waste materials enter ground or surface waters? If so, generally describe.

No

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water impacts, if any.

None

4. Plants

a. Check or circle type of vegetation found on the site:

\_\_\_\_\_ Deciduous tree: *alder, maple, aspen, other.*

\_\_\_\_\_ Evergreen tree: *fir, cedar, pine, other.*

\_\_\_\_\_ Shrubs

\_\_\_\_\_ **Grass**

\_\_\_\_\_ Pasture

\_\_\_\_\_ Crop or grain

\_\_\_\_\_ Wet soil plants, *cattail, buttercup, bullrush, skunk cabbage, other.*



\_\_\_\_\_ Water plants: *water lilly, eelgrass, milfoil, other.*

\_\_\_\_\_ Other types of vegetation.

- b. What kind and amount of vegetation will be removed or altered?  
Non-project action
- c. List threatened or endangered species known to be on or near the site. None
- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:  
To be determined at time of construction by others

## 5. Animals

- a. Circle any birds and animals which have been observed on or near the site are known to be on or near the site:  
birds: *hawk, heron, eagle, songbirds, other.* Typical  
urban fowl  
mammals: *deer, bear, elk, beaver, other.* \_\_\_\_\_  
fish: *bass, salmon, trout, herring, shellfish, other.* \_\_\_\_\_  
other: \_\_\_\_\_
- b. List any threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- c. Is the site part of a migration route? If so, explain. Unknown
- d. Proposed measures to preserve or enhance wildlife, if any: None

## 6. Energy and natural resources

- a. What kinds or energy (**electric, natural gas**, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.  
The site is vacant but formerly had two single family homes on it. There is therefore adequate utility services to the property. The city-added site currently contains a commercial parking lot and part of a residential garage. No changes to those features are proposed or expected.
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. No
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: None



## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. None
- (1) Describe special emergency services that might be required.  
No special services are needed
- (2) Proposed measures to reduce or control environmental health hazards, if any:  
None
- b. NOISE:
- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?  
Traffic on 29<sup>th</sup> and Ray
- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.  
To be determined at time of construction by others
- (3) Proposed measure to reduce or control noise impacts, if any:  
None

## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties? Site: Vacant  
West: Office  
East and North: Single family  
South: Single family and Office

The city-added sites currently contain a commercial parking lot and a portion of a residential garage. To the north of the sites lies a residential home. To the west is an unimproved lot. To the south is a retail/commercial building. To the east across Ray Street is a residential home.

- b. Has the site been used for agriculture? If so, describe. No
- c. Describe any structures on the site. None
- d. Will any structures be demolished? If so, which? N/A
- e. What is the current zoning classification of the site? RSF
- f. What is the current comprehensive plan designation of the site? R 4-10



- g. If applicable, what is the current shoreline master program designation of the site? None
- h. Has any part of the site been classified as a critical area? If so, specify. No
- i. Approximately how many people would reside or work in the completed project? To be determined at time of construction by others
- j. Approximately how many people would the completed project displace? None
- k. Proposed measures to avoid or reduce displacement impacts, if any: N/A
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:  
Compliance with all applicable development regulations

## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle or low-income housing. None
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. The site is vacant No action is proposed or expected on the city-added sites.
- c. Proposed measures to reduce or control housing impacts, if any: None

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? To be determined at time of construction by others
- b. What views in the immediate vicinity would be altered or obstructed? To be determined at time of construction by others
- c. Proposed measures to reduce or control aesthetic impacts, if any: To be determined at time of construction by others

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? To be determined at time of construction by others
- b. Could light or glare from the finished project be a safety hazard or interfere with views? To be determined at time of construction by others
- c. What existing off-site sources of light or glare may affect your proposal? No impacts would occur



- d. Proposed measures to reduce or control light and glare impacts, if any: None

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? Thornton Murphy Park
- b. Would the proposed project displace any existing recreational uses? If so, describe. No
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: None

## 13. Historic and cultural preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. Unknown
- b. Generally describe any landmarks or evidence of historic archaeological, scientific or cultural importance known to be on or next to the site.  
None
- c. Proposed measures to reduce or control impacts, if any: None

## 14. Transportation

- a. Identify public streets and highways serving the site and describe proposed access to the existing street system. Show on site plans, if any.  
29<sup>th</sup> and Ray both serve the site and due to existing channelization, the site is limited to right in and right out movement. The city-added sites are served by Ray Street (parcel 35273.0306) and 28<sup>th</sup> Avenue (parcel 35273.0305). Both are existing curb-cuts and access points.
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?  
Yes it is served at 29<sup>th</sup> and Ray.
- c. How many parking spaces would the completed project have? How many would the project eliminate?  
To be determined at time of construction by others
- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets not including driveways? If so, generally describe (indicate whether public or private). No new roads or streets are needed. Improvements would be limited to curb cuts and sidewalks if any. No new improvements are proposed or expected for the city-added sites.



- e. Will the project use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. No
- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak would occur.  
To be determined at time of construction by others.

*(Note: to assist in review and if known indicate vehicle trips during PM peak, AM Peak and Weekday (24 hours).)*

- g. Proposed measures to reduce or control transportation impacts, if any:  
None

Evaluation for  
Agency Use  
Only

## 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe. No new or expanded services would be needed or generated by this proposal
- b. Proposed measures to reduce or control direct impacts on public services, if any: None

## 16. Utilities

- a. Circle utilities currently available at the site: ***electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.***
- b. Describe the utilities that are proposed for the project, the utility providing the service and the general construction activities on the site or in the immediate vicinity which might be needed.  
To be determined at time of construction by others \_\_\_\_\_



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 28, 2020 \_\_\_\_\_ Signature: *Dwight J Hume*

***Please Print or Type:***

Proponent: Dwight J Hume \_\_\_\_\_ Address: 9101 N Mt. View Lane

Phone: 509.435.3108 \_\_\_\_\_ Spokane WA, 99218

Person completing form (if different from proponent): Same \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott \_\_\_\_\_

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS**  
***(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage or release of toxic or hazardous substances; or production of noise?

The future use for office and parking is similar to the surrounding land use pattern. No impacts are foreseen by this proposed use. No physical changes to the city-added sites are proposed or expected.

Proposed measures to avoid or reduce such increases are:

None

2. How would the proposal be likely to affect plants, animals, fish or marine life?  
Future development of the site would be urban in nature, similar to the retail adjoining the subject parcels. Eventual redevelopment of the site may require the removal of on-site plants, subject to the requirements of the Spokane Municipal Code for new construction. No physical changes to the city-added sites are proposed or expected.

Proposed measures to protect or conserve plants, animals, fish or marine life are:

None

3. How would the proposal be likely to deplete energy or natural resources?  
While some additional resources would be required during redevelopment, these would be similar to those required of any construction project. No physical changes to the city-added sites are proposed or expected.

Proposed measures to protect or conserve energy and natural resources are:

None

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands?  
The site does not contain any sensitive areas. No physical changes to the city-added sites are proposed or expected.

Proposed measures to protect such resources or to avoid or reduce impacts are:

None

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?



Development would comply with applicable development regulations, including landscape, screening and setbacks. There are no shoreline areas affected by this site. All project sites, including the private proposal sites, are located outside shoreline areas.

Proposed measures to avoid or reduce shoreline and land use impacts are:  
See above comment

6. How would the proposal be likely to increase demands on transportation or public services and utilities?  
No impacts to transportation or public services and utilities. No physical changes to the city-added sites are proposed or expected.

Proposed measures to reduce or respond to such demand(s) are:

To be determined at time of construction by others

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment.  
No impacts are foreseen



C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 Signature: *Dwight J Hume*

***Please Print or Type:***

Proponent: Dwight J Hume Address: 9101 N Mt. View Lane

Phone: 509.435.3108 Spokane WA, 99218

Person completing form (if different from proponent): Same

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z19-502COMP

**PROPONENT:** Ryan Schmelzer (Agent: Dwight Hume, Land Use Solutions and Entitlement) and the City of Spokane (Contact: Kevin Freibott, Department of Neighborhood and Planning Services).

**DESCRIPTION OF PROPOSAL:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the land use designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the Lincoln Heights neighborhood. Two parcels were added to the proposal by the City of Spokane, proposed for the same action.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns two parcels, 35273.0219 and 35273.0220, located immediately northeast of the intersection of S 29<sup>th</sup> Avenue and S Ray Street as well as two additional parcels (35273.0305 and 35273.0306) located on the west side of Ray Street, approximately 180 feet north of the same intersection. The parcels are located at 3203 and 3207 E 29<sup>th</sup> Avenue as well as 2820 and 2826 S Ray Street. The entire proposal would affect an area of approximately 0.61 acres.

**Legal Description:** Lots 22, 23, and 24, Block 70, Lincoln Heights Addition, except streets. South 100' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. streets. South 60' of the north 125' of Lots 11 & 12, Block 71, Lincoln Heights Addition, exc. Streets in the City of Spokane in Section 27, Township 25 North, Range 43 East.

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

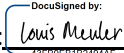
- [ ] There is no comment period for this DNS.
- [ ] This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.
- [ X ] This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:**  43F89EB1B2404AF...

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*





**Spokane Tribe of Indians**  
**Tribal Historic Preservation Officer**  
P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z19-502COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer



**From:** [Carol Tomsic](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Marilyn](#); [Sally](#); [Makaya Judge](#)  
**Subject:** Agency and City Comment on Comp Plan Land Use Map Amendment Proposal 29th & Ray  
**Date:** Sunday, May 10, 2020 8:58:55 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

B Environment Elements 8 Land and Shoreline Uses a

Site-Vacant

**Comment** - I would like to note there was a long-standing single-family house on the site that was recently demolished.

South - Single Family and Office

**Comment** - There is no office zoning south of the site. The zoning is RSF Ray Street south from 18th to 37th/Ferris High School.

B Environmental Elements 13 Historic and cultural preservation b

**Comment** - The east side of Ray Street is a historic residential buffer for the Lincoln Heights Neighborhood.

**Comment** - A proposed office zoning on the site was not supported by the residents and rejected by the city in 1985 and 1992.

B Environmental Elements 14 Transportation a

29th and Ray both serve the site and due to existing channelization, the site is limited to right in and right out movements.

**Comment** - I would like to state the limitations of movement on Ray Street will result in increase traffic and safety issues on the adjacent residential streets.

B Environment Elements 14 Transportation d

**Comment** - There is an alleyway on the north side of the property that will need to be improved. The alleyway connects to an unpaved section of Thor Street and will be a safety issue with existing residential traffic.

B Environmental Elements 14 Transportation f

**Comment** - Vehicular trip stats on 29th/Ray are in the 2019 29th Avenue Traffic Corridor Study and appendix.

Thank you  
Carol Tomsic

Kevin - Please send me an email confirmation.



**From:** [michele.martz](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** proposed plan amendments  
**Date:** Tuesday, February 25, 2020 3:02:10 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Mr. K Freibott,

I am writing to request that you please do not change the property [file # Z19-502 COMP ] 3203 & 3207 East 29 TH , from residential to office. I feel this will negatively impact traffic on 28 th Ave. Properties East of Ray street are deemed single family dwellings and this property amendment would open the door to other business wedging themselves in with private dwellings. Our single family dwellings should remain protected from this.

Sincerely,

Michele Martz

property owner 3326 E 28 th AVE



**From:** [Carol Tomsic](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Beggs, Breean](#); [Kinnear, Lori](#); [Wilkerson, Betsy](#); [Burke, Kate M.](#); [Cathcart, Michael](#); [Mumm, Candace](#); [Stratton, Karen](#); [Wittstruck, Melissa](#); [Sally Phillips](#); [DOUGLAS & MARILYN LLOYD](#)  
**Subject:** Comment on 2019/2020 Comp Plan Amendments for City Council Vote on March 2  
**Date:** Friday, February 28, 2020 12:25:00 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Lincoln Heights Proposed Change from "Residential 4-10" to "Office" - Against

I live, work and walk in the Lincoln Heights Neighborhood. I have lived two blocks from the NE corner of 29th/Ray for over 20 years. I prefer to keep the current residential zoning on the two parcels. Our city has a housing shortage and an existing house on the lot was demolished in 2019. The block is lined with single-family houses. There is a neighborhood park in walking distance.

The NE side of Ray is zoned as residential from 17th to 37th. The NE side of Ray Street has been historically designated as a residential buffer. In 1984 and 1993 the residents successfully fought against two attempts to rezone the said property to office.

The area is not trending in terms of land use. There is a church on the NE side of Ray/28th. It was built in 1959 and designed by three well-known architects; Bill Trogdon, Bruce Walker and Stan McGough. There is a church on the NE side of Ray/27th. It was built in 1953. There is a daycare on the NE side of Ray/25th that was built in 1988. An elementary school on the NE side of Ray/23rd opened in the fall of 1953. A fire station on the NE side of Ray has been in various locations on Ray since 1914. All are appropriate for a residential area.

Office zoning on small sites in or near residential areas is intended to have few detrimental impacts on the neighborhood. The proposed office zoning change will set a precedent and retail/office will creep into our residential buffer. An office use will also divert traffic into our residential neighborhood due to concrete barriers on 29th/Ray.

A 2019 29th Avenue Corridor Study online survey suggested residents "felt unsafe or uncomfortable" while walking or crossing 29th. Ray is a principal arterial. The posted speed on Ray is 30 mph. It is already unsafe to walk across four lanes of traffic on Ray, at 27th or 28th to get to work. Adding office on the NE side of Ray will only make it more dangerous.

Map TR-5 Proposed Bike Network Map Proposed Modification 5 - Support

I totally support the proposed map. I'd like to suggest adding Cook, to South Altamont Blvd, to Woodfern, to North Altamont Blvd, to the Ben Burr Trail. And, connecting the Ben Burr Trail to Thornton Murphy Park.

Thank you!

Carol Tomsic  
resident

Kevin - please send an email confirmation.



**From:** [Makaya Judge](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Plan Commission](#)  
**Subject:** Re: 29th/Ray amendment  
**Date:** Tuesday, June 16, 2020 8:41:36 AM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Dear Planning Commission,

I strongly disagree with the proposed land use change on the NE corner of 29th & Ray from residential to office use. As someone who plays with my children at the park across the street and walks that area, I feel strongly that it would change the use and feel of that area. Traditionally, that East side of Ray has been reserved for residential use. The church, daycare, and school on that side of Ray are all Buildings typical of a residential area. An office building is not. The increase of traffic, street parking, and zoning creep that will impact deep into the residential neighborhood is not worth the change. Please protect this residential area.

Thank you,  
Makaya Judge



**From:** [Carol Tomsic](#)  
**To:** [Beggs, Breean](#); [Kinnear, Lori](#); [Wilkerson, Betsy](#); [Burke, Kate M.](#); [Cathcart, Michael](#); [Mumm, Candace](#); [Stratton, Karen](#); [Freibott, Kevin](#)  
**Cc:** [Marilyn](#); [Sally](#); [Makaya Judge](#)  
**Subject:** Comment on Proposed Comp Plan Amendment Z19-502COMP 29th/Ray Residential to Office Zoning  
**Date:** Sunday, June 21, 2020 11:02:01 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Plan Commission and City Council Members.

I am against the Comp Plan Amendment Z19-502COMP - the rezoning of the northeast corner of 29th/Ray from residential to office.

I have lived two blocks from the northeast corner of 29th/Ray for 21 years. I have worked at a retail store in the Lincoln Heights Shopping Center for 20 years. I am fortunate to be able to walk to work and shop at neighborhood stores. I am also fortunate to have neighborhood residents share the history of the neighborhood with me. Most recently I learned I live where a chicken hatchery existed in the 1920s. The owner's daughter still lives in the neighborhood and she is also against the proposed zoning change.

The residents successfully fought against two attempts to rezone the said residential property to office in 1984 and 1993. A long-inhabited house on the property was demolished prior to the application being submitted. The block is lined with single-family houses and there's a neighborhood park in walking distance.

The northeast side of Ray Street is a historically designated residential buffer. It's zoned residential from 17th to 37th/Ferris High School. There is no "trending" in terms of land use on the northeast side of Ray Street. The church at 28th/Ray was built in 1959. The church at 27th/Ray was built in 1953. The daycare at 25th/Ray was built in 1988. The elementary school at 23rd/Ray opened in the fall of 1953. The fire station has been in various locations on Ray since 1914. All are appropriate for a residential area.

Office zoning on small sites in or near residential areas is intended to have few detrimental impacts on the neighborhood. The proposed rezoning will set a precedent and retail/office will creep into our historical residential buffer. An office zoning will also divert traffic into our residential neighborhood because of the limited car movements due to concrete barriers on 29th/Ray. The unwanted diverted traffic is a safety concern.

Ray Street is a principal arterial. The posted speed on Ray Street is 30 mph. A 2019 29th Corridor Study stated residents "felt unsafe or uncomfortable" while walking or crossing 29th. The proposed office zoning on the northeast side of Ray Street will only make pedestrian crossings in a residential area on 29th more dangerous.

Please protect our neighborhood and vote against the proposed Comp Plan Amendment.

Thank you!

Carol Tomsic  
resident

Kevin - please sent me an email confirmation and please send to plan commission.



June 22, 2020

Kevin Freibott  
Assistant Planner  
808 2<sup>nd</sup> Spokane Falls Blvd.  
Spokane, WA 99201

Re: Proposed Zone Change File # 219502 Comp.  
29<sup>th</sup> + Ray St - NE Corner

Dear Sir,

Thirty some years ago this neighborhood worked twice to keep that same property from being allowed to have its Classification changed from single family to office. Until last year there had been a house, continuously occupied, on the property when the house was removed.

Also last year there was a new house built in the middle of the same block and it immediately sold.

A change in Classification would be setting a precedent opening the door to future requests for zone change re classification on other properties within our zone.

With a median on both Ray and 29<sup>th</sup> streets traffic from the North would be diverted on to 28<sup>th</sup> Ave East to Union proved Thor to 29<sup>th</sup> and West to the office parking lot.

I have lived at my address for over 50 years as well as growing up in Lincoln Heights. So I have seen the many striped change on the West side of Ray St. Please consider my comments. Thank you -

Mrs. June M. Pierce

E. 3327 - 28<sup>th</sup> Ave.



**From:** [Carol Tomsic](#)  
**To:** [Freibott, Kevin](#); [Kinnear, Lori](#); [Wilkerson, Betsy](#); [Burke, Kate M.](#); [Cathcart, Michael](#); [Mumm, Candace](#); [Stratton, Karen](#); [Beggs, Breean](#)  
**Cc:** [Marilyn](#); [Sally](#); [Makaya Judge](#); [Ryan, Gabrielle](#)  
**Subject:** Lincoln Heights Neighborhood Council Comment on proposed Comp Plan Amendment Z19-502COMP 29th/Ray Residential to Office Zoning  
**Date:** Saturday, July 25, 2020 9:09:09 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

**Council Members and Plan Commission**

The Lincoln Heights Neighborhood Council executive board met via Zoom on 7/7/2020 to discuss a request by concerned residents to write a letter to the Plan Commission and City Council in support of their opposition against the proposed Comp Plan amendment to change the northeast corner of 29th/Ray from residential to office zoning. It was decided during the Zoom meeting to send an email to council members on our email list and ask if they agree or disagree with the residents request.

Email results were 7 approved and 2 opposed. A present list of concerned residents is 15 (one also sent in an email)

Therefore, the Lincoln Heights Neighborhood Council supports the concerned residents in their opposition against the proposed Comp Plan amendment to change the northeast corner of 29th/Ray from residential to office zoning.

The residents have successfully fought against two previous attempts to rezone the property to office in 1984 and 1993.

The block is lined with single-family houses and there is a neighborhood park in walking distance.

The northeast side of Ray Street is a historically designated residential buffer from 17th to 37th/Ferris High School. The church at 28th/Ray was built in 1953, the church at 27th/Ray was built in 1953, the daycare at 25th/Ray was built in 1988, the elementary school opened in the fall of 1953, a fire station has been in various locations on Ray since 1914, and all are appropriate in a residential area.

The proposed rezoning will set a precedent and retail/office will creep into the historically designated retail buffer.

Per the city's municipal code, an office zoning on small sites in or near residential areas is intended to have few detrimental impacts on the neighborhood. The proposed office zoning will divert traffic into the residential neighborhood because of limited car movements due to concrete barriers on Ray Street. The diverted traffic is a safety concern. There is a lack of sidewalks and unpaved streets.

Ray Street is a principal arterial and office zoning usually does not cross a principal arterial in to a residential neighborhood.

Carol Tomsic  
Chair, Lincoln Heights Neighborhood Council.

Kevin - Please send email confirmation.





2019/2020 Comprehensive Plan Amendments

## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-502COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE part and DENY the remainder of the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “Office” for 0.61 acres located at 3207 and 3202 E 29<sup>th</sup> Avenue and 2820 and 2826 S Ray Street. The implementing zoning designation requested is Office with a 35-foot height limit (O-35).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-502COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a 0.61-acre area located at 3207 and 3202 E 29<sup>th</sup> Avenue and 2820 and 2826 S Ray Street (the “Properties”) from “Residential 4-10” to “Office” with a corresponding change in zoning from Residential Single Family (RSF) to Office with a 35-foot height limit (O-35).
- E. The two properties on S Ray Street were added to the proposal by the City Council upon adoption of the 2020 Work Program (see ‘I’ below).
- F. The two Properties on E 29<sup>th</sup> Avenue are held in common ownership by the applicant; the property at 2820 S Ray Street is held by a second owner, unrelated to the first. The property at 2826 S Ray Street is held by a third owner, unrelated to the other two.
- G. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- H. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- I. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, including the Application in the Work Program.
- J. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. The City received comments stating no concern with the proposal and one requesting a site-specific survey for cultural resources prior to any development.



- K. The City issued a Notice of Application on June 8, 2020 by posting it in the Spokesman Review and Official Gazette and by mail to all properties and owners within a 400-foot radius of the subject Properties and any adjacent properties with the same ownership. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City received comment letters from the public in opposition to the proposal, citing potential traffic and access concerns and intrusion into a single-family neighborhood.
- L. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and the Community Assembly was provided with information regarding the dates of Plan Commission workshops and hearings.
- M. On June 24, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- N. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- O. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
- P. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- Q. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the Properties and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject property.
- R. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Application recommended approval of the City-added properties and did not provide a recommendation for the applicant-proposed Properties.
- S. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of the SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- T. Following issuance of the public hearing notice, the City received additional comment letters in opposition to the proposal from members of the public, including one who had submitted a comment letter previously during the public comment period.



- U. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- V. One member of the public testified during the hearing on September 9, 2020 in opposition to the proposal, citing concerns with site access and neighborhood intrusion impacts.
- W. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- X. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the "Staff Report").
- Y. Regarding parcels 35273.0305 and 35273.0306, those located west of S Ray Street, the Plan Commission finds that the proposal meets the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.5 concerning the establishment of Office land uses in the City.
- Z. Regarding parcels 35273.0219 and 35273.0220, those located east of S Ray Street, the Plan Commission finds that the proposal is inconsistent with the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.5 concerning the establishment of Office land uses in the City, as follows:
  - 1. The proposal would place Office land uses outside the vicinity of a Center and Corridor and would allow office uses to cross a major arterial into an existing single-family residential area, an area without an existing trend towards office development.
- AA. The Plan Commission finds that the proposal for parcel 35273.0305 and 35273.0306 meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report, while the proposal for parcels 35273.0219 and 35273.0220 does not meet the decision criteria, as described in 'AA' above.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-502COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

- 1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Application was submitted as provided in SMC 17G.020.050(D).
- 2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.



3. The Application is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Application concerning parcels 35273.0305 and 35273.0306 is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. As outlined in above in the Findings of Fact, the Application concerning parcels 35273.0219 and 35273.0220 is internally inconsistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
7. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
8. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
9. SEPA review was completed for the Application.
10. The Application will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
11. Regarding parcels 35273.0305 and 35273.0306, the Application proposes a land use designation that complies with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
12. Regarding parcels 35273.0219 and 35273.0220, the Application proposes a land use designation that is inconsistent with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
13. Subject to the foregoing, the proposed map amendment and site is suitable for the proposed designation.
14. Regarding parcels 35273.0305 and 35273.0306, the map amendment would implement applicable comprehensive plan policies better than the current map designation.
15. Regarding parcels 35273.0219 and 35273.0220, the map amendment would not implement applicable comprehensive plan policies better than the current map designation.

#### **RECOMMENDATIONS:**

In the matter of Z19-502COMP, a request by Dwight Hume of Land Use Solutions and Entitlement on behalf of the Ryan Schmelzer and Paige Wallace to change the land use plan designation on 0.61 acres



of land from “Residential 4-10” to “Office” with a corresponding change of the implementing zoning to O-35 (Office, 35-foot height limit), as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the proposed amendment for parcels 35273.0305 and 35273.0306 to the Land Use Plan Map of the City’s Comprehensive Plan with corresponding amendment to the City’s Zoning Map, by a vote of **9 to 0**, recommends City Council **DENY** the requested amendment for parcels 35273.0219 and 35273.0220 to the Land Use Plan Map, and authorizes the President to prepare and sign on the Commission’s behalf a written decision setting forth the Commission’s findings, conclusions, and recommendation on the Application.

---

**Todd Beyreuther, President**

Spokane Plan Commission

October \_\_, 2020



**From:** [Bonnie Abernethy](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: File #Z19-502COMP  
**Date:** Tuesday, September 8, 2020 9:53:23 PM

---

NOTE: The following comments were received after completion of the Staff Report. The letter from Ms. Abernethy was recieved after the public record was closed by Plan Commission, but the letter from Ms. Pierce was recieved and considered by the Plan Commission.

[CAUTION - EXTERNAL EMAIL - Verify Sender]

RE: Proposed Amendment to Comprehensive Land Use Plan

Attn: City of Spokane Planning Commission

I am writing to oppose this zoning amendment proposal. As a longtime resident of the Lincoln Heights Neighborhood, who served for four year previously as Chairperson of this neighborhood organization when in it's infancy. We, on two separate occasions, fought against any commercial development east of Ray Street on 29th Avenue. We were successful in defeating these proposals and maintaining our neighborhood without commercial intrusions.

We believed then, and I believe now, that there is an adequate number of offices buildings with empty space for lease west of Ray Street on 29th Ave. Several offer space for lease presently. To preserve the sense and feel of a single family neighborhood, we noted in the Comprehensive Plan (original Plan), that land east of Ray Street on 29th Avenue be zoned "Single Family Residential Only".

Your serious consideration of our desire to once again stand up for neighborhood preservation will be most appreciated.

Thank you.

Sincerely,

Bonnie Wilhelm-Abernethy



Kerri Frisbott  
Assistant Planner  
Spokane City Hall

Sept. 4, 2020  
RE: Property  
3203-3207 E. 29th Ave.

Dear Sir,

as a lifetime resident of the  
Lincoln Heights neighborhood I  
have been involved with many  
neighborhood projects.

In the 1980's and 90's we worked  
to keep this same piece of property  
zoned "single family" when a  
change to "office" was requested then.

Until last year when a house  
on that property was removed,  
it had been continually occupied.  
Also last year, in the center of  
that same block on 29th Ave a  
new house was built and sold.

To allow an "office" Classification  
zone change would set a precedent, open-  
ing the gates to possible zone change requests, which  
would change the flavor of our  
neighborhood.

Many of us worked on a Comprehensive  
Plan task force for the Lincoln Heights neighbor-  
hood when the east side of Ray Street  
was highly approved to keep it  
zoned "single family".

I have lived in my 28th Ave home  
for over 50 years and have seen many  
homes built in my neighborhood.

I trust the Plan Committee will  
consider our comments. Thank you,

June M. Pierce E. 3327-28th Ave.





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                              |             |
|------------------------------|-------------|
| <b><u>Date Rec'd</u></b>     | 10/26/2020  |
| <b><u>Clerk's File #</u></b> | ORD C35975  |
| <b><u>Renews #</u></b>       |             |
| <b><u>Cross Ref #</u></b>    |             |
| <b><u>Project #</u></b>      | Z19-503COMP |
| <b><u>Bid #</u></b>          |             |
| <b><u>Requisition #</u></b>  |             |

|                                  |                                                   |
|----------------------------------|---------------------------------------------------|
| <b><u>Submitting Dept</u></b>    | PLANNING                                          |
| <b><u>Contact Name/Phone</u></b> | KEVIN FREIBOTT 625-6184                           |
| <b><u>Contact E-Mail</u></b>     | KFREIBOTT@SPOKANECITY.ORG                         |
| <b><u>Agenda Item Type</u></b>   | First Reading Ordinance                           |
| <b><u>Agenda Item Name</u></b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – 53RD AVENUE |

**Agenda Wording**

An Ordinance relating to application Z19-503COMP by 3227 E 53rd Ave, LLC, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to Residential 15-30 for 10.3 acres and a change to the Zoning Map from RSF to RMF.

**Summary (Background)**

The proposal concerns 3227 E 53rd Ave (parcels 34032.9044 and .9093), expanded by Council to include 5106 S Palouse Hwy (parcel 34032.9094). This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on Sept 8 to consider this amendment and has recommended approval of the amendment.

|                                    |                   |                                     |                              |
|------------------------------------|-------------------|-------------------------------------|------------------------------|
| <b><u>Fiscal Impact</u></b>        |                   | Grant related? NO                   | <b><u>Budget Account</u></b> |
|                                    |                   | Public Works? NO                    |                              |
| Neutral                            | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| <b><u>Approvals</u></b>            |                   | <b><u>Council Notifications</u></b> |                              |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS     | <b><u>Study Session\Other</u></b>   | CC Study Session 10-29-20    |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M. | <b><u>Council Sponsor</u></b>       | CM Mumm                      |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY   | <b><u>Distribution List</u></b>     |                              |
| <b><u>Legal</u></b>                | RICHMAN, JAMES    | tblack@spokanecity.org              |                              |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL   | jrichman@spokanecity.org            |                              |
| <b><u>Additional Approvals</u></b> |                   | sbishop@spokanecity.org             |                              |
| <b><u>Purchasing</u></b>           |                   | dhume@spokane-landuse.com           |                              |
|                                    |                   | lmeuler@spokanecity.org             |                              |
|                                    |                   |                                     |                              |
|                                    |                   |                                     |                              |



Ordinance No. C35975

AN ORDINANCE RELATING TO APPLICATION FILE Z19-503COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "RESIDENTIAL 15-30" FOR APPROXIMATELY 10.3 ACRES LOCATED AT 3227 E 53<sup>RD</sup> AVENUE AND 5106 S PALOUSE HIGHWAY (PARCELS 34032.9044, 34032.9093, 34032.9094) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "RESIDENTIAL MULTI-FAMILY (RMF)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-503COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-503COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 10.3 acres from "Residential 4-10" to "Residential 15-30"; if approved, the implementing zoning destination requested is "Residential Multi-Family (RMF)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-503COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-503COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-503COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 8 to 1 to recommend approval of Application Z19-503COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-503COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "Residential 15-30" for 10.3 acres, as shown in Exhibits A and B.
3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from "Residential Single Family" to "Residential Multi-Family (RMF)," as shown in Exhibits C and D.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



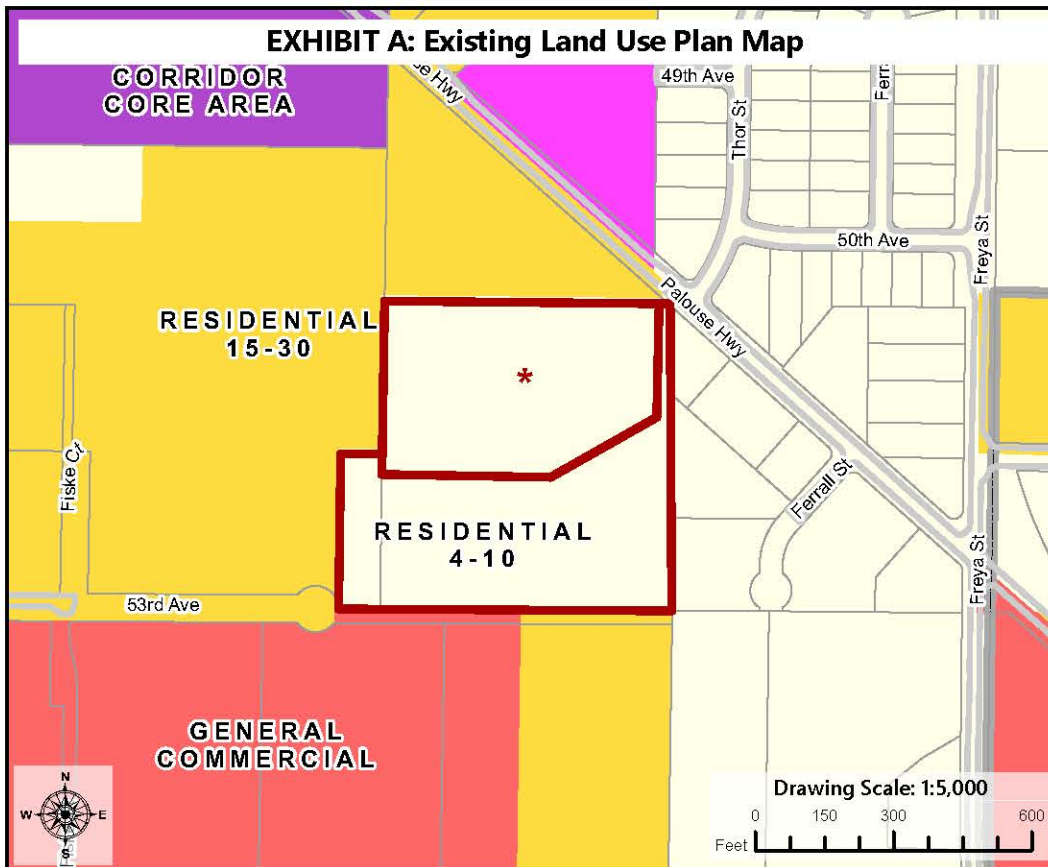


# Z19-503COMP (3227 E 53rd Ave and 5106 S Palouse Hwy) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT  
The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

### Land Use Plan Designation

Residential 4-10

Residential 15-30

CC Core

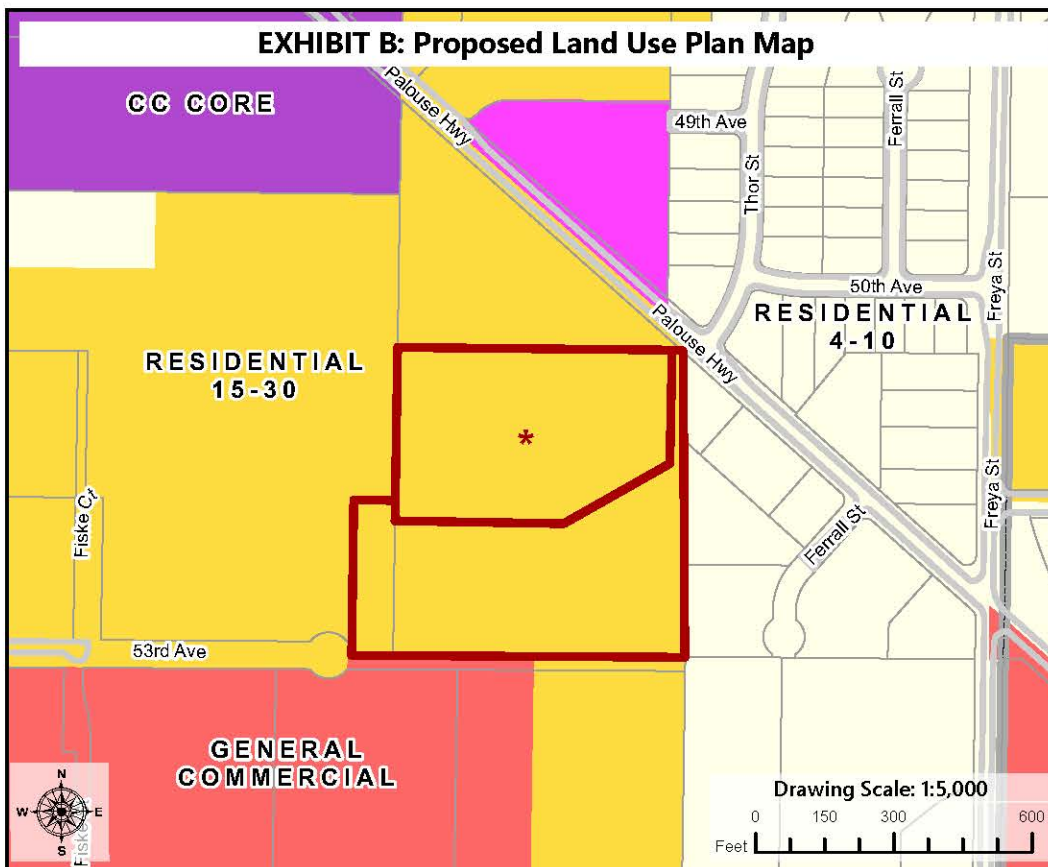
Mini Center

General Commercial

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Acres (Proposal): 10.3

## EXHIBIT B: Proposed Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

### Proposed Land Use

Residential 4-10

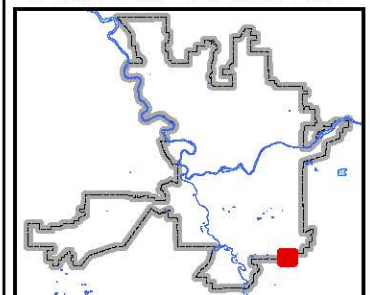
Residential 15-30

CC Core

Mini Center

General Commercial

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# Z19-503COMP (3227 E 53rd Ave and 5106 S Palouse Hwy) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT  
The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Curb Line

Parcel

### Current Zoning

Center and Corridor Type 2 (CC2)

Neighborhood Retail (NR)

Residential Multifamily (RMF)

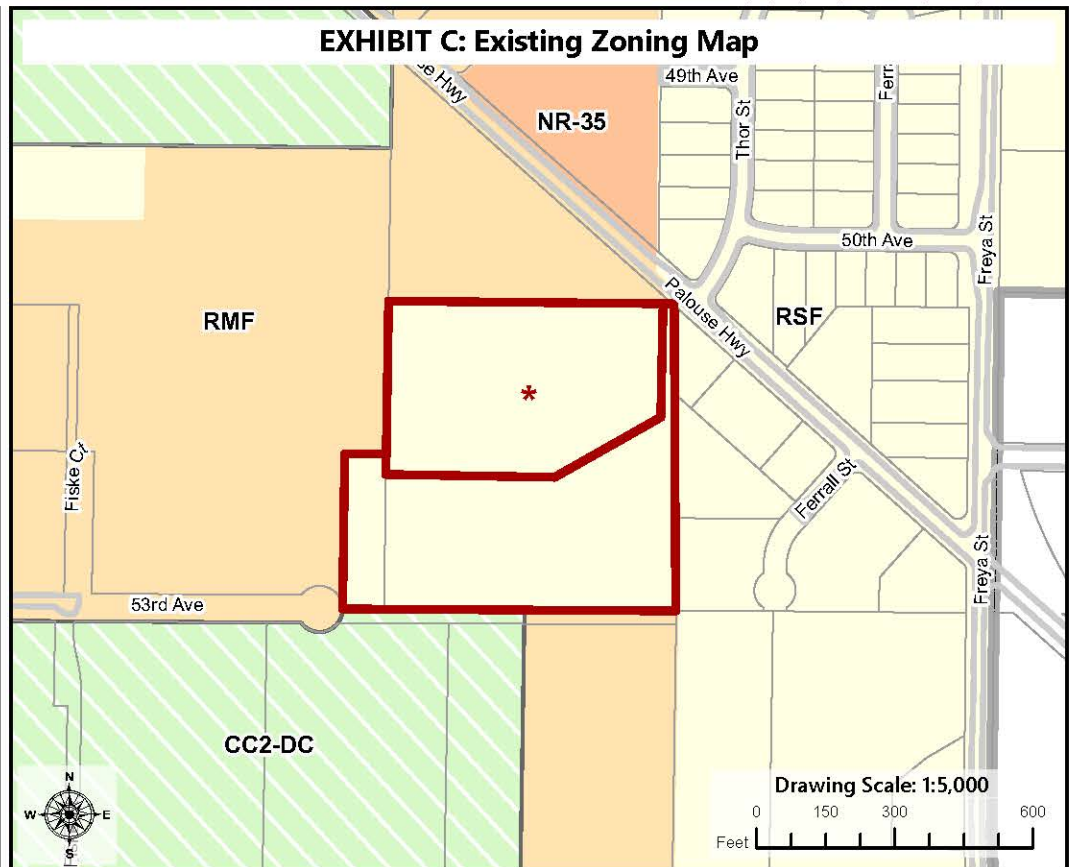
Residential Single-Family (RSF)

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 10.3

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Curb Line

Parcel

### Proposed Zoning

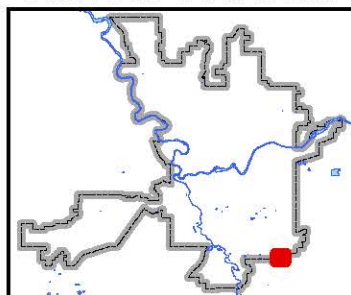
Center and Corridor Type 2 (CC2)

Neighborhood Retail (NR)

Residential Multifamily (RMF)

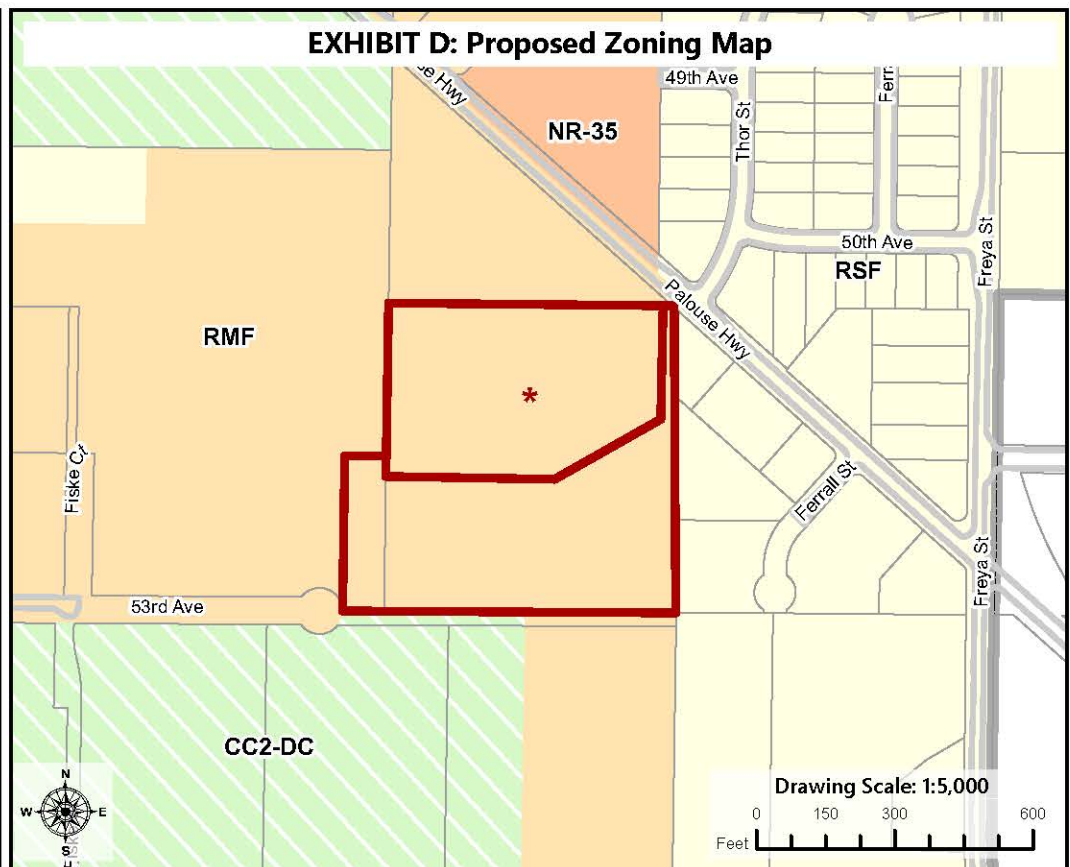
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





## **Exhibit E: Legal Description**

Parcel 1 (34032.9044)

03 24 43 E90FT OF S1/2 OF S1/2 OF L12 OF N1/2

Parcel 2 (34032.9093)

03-24-43 PORTION OF GOVERNMENT LOT 11 DESCRIBED AS FOLLOWS;  
BEGINNING AT A POINT ON THE SOUTH LINE AND 714.1 FEET WEST OF THE  
SOUTHEAST CORNER OF SAID GOVERNMENT LOT 11; THENCE WEST ALONG  
THE SOUTHLINE TO THE SOUTHWEST CORNER; THENCE NORTH ALONG THE  
WEST LINE, 660 FEET; THENCE EAST 620.2 FEET; THENCE SOUTH 660 FEET, TO  
THE POINT OF BEGINNING, EXCEPT THAT PARCEL DESCRIBED IN DOCUMENT  
NO. 78812C, FILED IN THE SPOKANE COUNTY AUDITOR'S OFFICE, DESCRIBED  
AS FOLLOWS: BEGINNING AT A POINT ON THE WEST LINE OF SAID  
GOVERNMENT LOT 11, WHICH POINT IS 286.2 FEET NORHERLY OF THE  
SOUTHWEST CORNER OF SAID GOVERNMENT LOT 11; THENCE NORHTERLY  
ALONG THE SAID WEST LINE OF GOVERNMENT LOT 11 A DISTANCE OF 373.8  
FEET; THENCE DUE EAST PARALLEL WITH THE SOUTH LINE OF SAID  
GOVERNMENT LOT 11 A DISTANCE OF 596 FEET; THENCE SOUTH 0 DEGREES  
38' EAST A DISTANCE OF 240.4 FEET; THENCE SOUTH 59 DEGREES 37' WEST A  
DISTANCE OF 263.8 FEET; THENCE DUE WEST PARALLEL WITH THE SAID  
SOUTH LINE OF GOVERNMENT LOT 11 A DISTANCE OF 370 FEET, MORE OR  
LESS, TO THE POINT OF BEGINNING.

Parcel 3 (34032.9094)

3-24-43, PTN OF SW1/4 OF GOV L11 OF N1/2 DAF: BEG AT PT ON W LN OF GOV  
L11, 286.2 FT N OF SW COR; TH N ALG SD W LN 373.8 FT; TH E PAR TO S LN OF  
SD GOV L11, 596 FT; TH S 0DEG 38MIN E, 240.4 FT; TH S 59DEG 37MIN W, 263.8  
FT; TH W PAR TO S LN SD GOV L11, 370 FT M/L TO POB;

All parcels within the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-503COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map designation and zoning of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                                                                                                                 |
|---------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 34032.9044, 34032.9093, and 34032.9094                                                                                                                          |
| <b>Address(es):</b>       | 3227 E 53 <sup>rd</sup> Avenue and 5106 S Palouse Highway                                                                                                       |
| <b>Property Size:</b>     | 10.3 acres                                                                                                                                                      |
| <b>Legal Description:</b> | On File with the City of Spokane Department of Neighborhood and Planning Services.                                                                              |
| <b>General Location:</b>  | Northeast of the eastern terminus of E 53 <sup>rd</sup> Avenue in the Southgate Neighborhood of Spokane.                                                        |
| <b>Current Use:</b>       | Single-family residential home with outbuildings (parcels 34032.9044 and 34032.9093). Radio station building and two transmission antennae (parcel 34032.9094). |

**II. APPLICANT SUMMARY**

*Note that the City Council expanded the geographic scope of this application. As a result, this application has two applicants—a private applicant and the City of Spokane itself. The following information regards the original private applicant:*

|                        |                                                       |
|------------------------|-------------------------------------------------------|
| <b>Agent:</b>          | Dwight Hume, Land Use Solutions and Entitlement       |
| <b>Applicant:</b>      | 3227 E 53 <sup>rd</sup> Ave, LLC                      |
| <b>Property Owner:</b> | Same as applicant (parcels 34032.9044 and 34032.9093) |

*The following information regards the two properties applied for by the City:*

|                        |                                                       |
|------------------------|-------------------------------------------------------|
| <b>Representative:</b> | Kevin Freibott, Neighborhood and Planning Services    |
| <b>Applicant:</b>      | City of Spokane                                       |
| <b>Property Owner:</b> | Triathlon Broadcasting of Spokane (parcel 34032.9094) |



### III. PROPOSAL SUMMARY

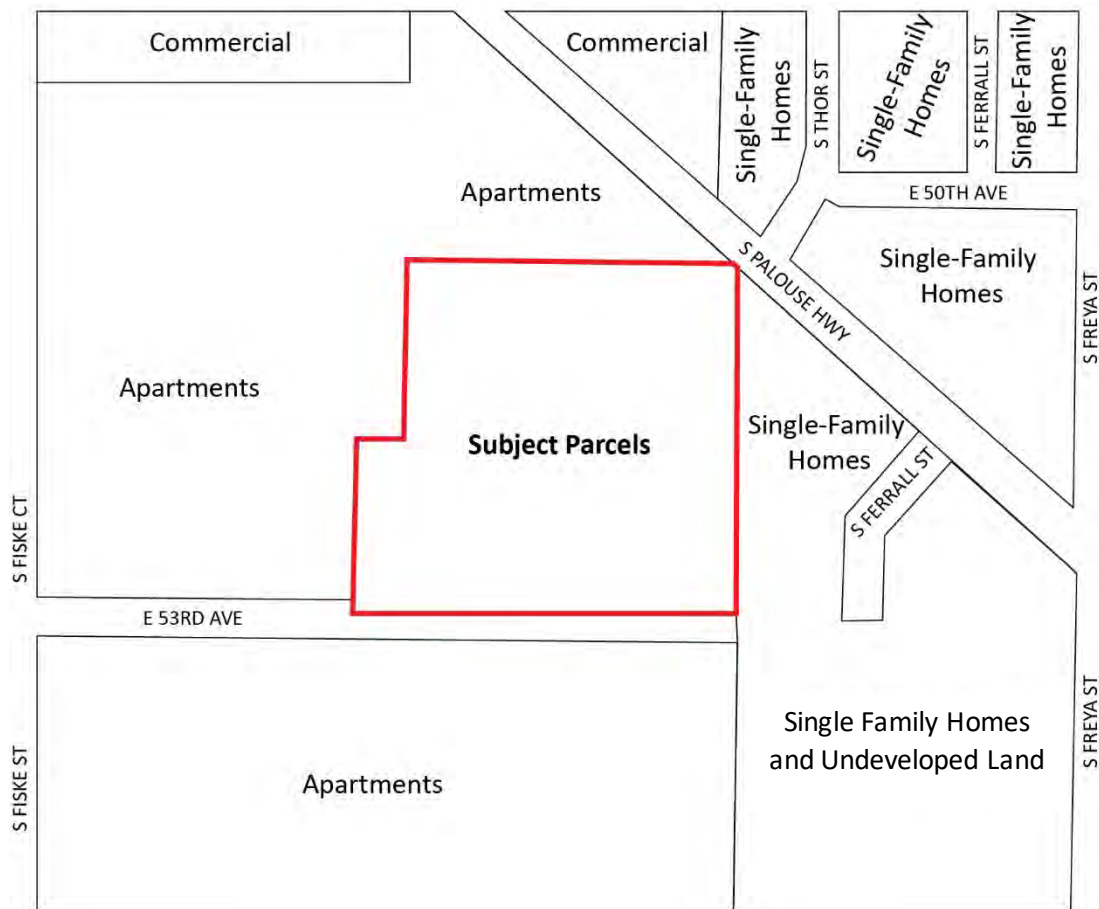
|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 DUs/Acre (R 4-10)                                                                                                          |
| <b>Proposed Land Use Designation:</b> | Residential 15-30 DUs/Acre (R 15-30)                                                                                                        |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | Residential Multi-Family (RMF)                                                                                                              |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | Recommended                                                                                                                                 |

### IV. BACKGROUND INFORMATION

- General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the Southgate neighborhood. The intent of the applicant is to potentially develop higher density residential uses on both subject lots, all in common ownership by the applicant. During the threshold review process, the City Council added one additional property (4.82 acres) to the proposal, immediately north of the original parcels, on the Palouse Highway. The owner of that additional parcel has not indicated any desire or plans for future development at this time.
- Site Description and Physical Conditions:** The two parcels in the original private application, located at the eastern terminus of E 53<sup>rd</sup> Avenue, contain a single home and some outbuildings. The majority of the two parcels remain undeveloped. The parcel added by City Council contains a radio station building, seemingly vacant, and two transmitting antennae. The majority of that parcel is undeveloped at this time as well, owing to the safety area required around the antennae. All three properties are fenced and cross-fenced.
- Property Ownership:** Parcels 34032.9044 and 34032.909 are owned by a WA-registered limited liability corporation by the name of 3227 E 53<sup>rd</sup> Ave, LLC. Parcel 34032.9094 is owned by Triathlon Broadcasting of Spokane and is operated by I Heart Radio, also of Spokane. I Heart Radio responded to inquiries by City Staff upon inclusion of that parcel by the City Council. Mr. Cal Hall, Area President for I Heart Radio, indicated verbally to Mr. Kevin Freibott of the City that his organization did not oppose their inclusion in the application.



4. **Adjacent Property Improvements and Uses:** The subject parcels are surrounded by existing development of the following nature:



5. **Street Class Designations:** E 53<sup>rd</sup> Avenue in this location is designated as a local street. The Palouse Highway is classified as a Minor Arterial. These classifications are not expected to change in the future as they match the Arterial Network Map in the Comprehensive Plan (Map TR-12). No change of street class designation is proposed as part of this application.
6. **Current Land Use Designation and History:** As shown in Exhibit A, the subject properties are currently designated on the Land Use Plan Map as “Residential 4-10” (between 4 and 10 dwelling units per acre). The subject properties have been designated for this use since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001. It’s important to note that a few Comprehensive Plan amendments have occurred in the vicinity of these parcels, namely to the northwest and southwest. These changes in land use resulted from the planning of the Southgate District Center northwest of the parcels (see Ordinance C34468) and the annexation that added properties south of 53<sup>rd</sup> Avenue to the City (see Ordinance C35359). Neither of those actions, nor any of the subsequent Comprehensive Plan amendments in the vicinity, amended the land use or zoning of these particular parcels. However, these actions in the past did result in the current situation, wherein the subject properties are surrounded on three sides by more dense uses and zoning.



7. **Proposed Land Use Designation:** As shown in Exhibit B, the proposal is to amend the land use designation so that the entirety of the subject properties are designated for “Residential 15-30” use.
8. **Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF). The zoning of these parcels has remained unchanged since their annexation into the City in 2005. As the two adjacent District Centers have been planned and annexed into the City, zoning to the north, west, and south of the subject parcels has transitioned to more dense uses.
9. **Proposed Zoning:** As shown in Exhibit D, the proposal seeks to amend the zoning so that the entirety of the subject properties are zoned Residential Multi-Family (RMF).

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | July 8, 2020      |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |
| SEPA Determination Issued .....                               | August 24, 2020   |
| Notice of Public Hearing Posted .....                         | August 26, 2020   |
| Plan Commission Hearing Date (Scheduled) .....                | September 9, 2020 |

2. **Comments Received:** A request for comments was issued to City departments, local agencies, and departments within 400 feet of the proposal, along with pertinent application details on April 24, 2020. No agency or department comments were received.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. In early July the City received a number of comment cards submitted to the Southgate Neighborhood

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



Council during the February 2, 2020 meeting when the applicant's agent presented the proposals to the neighborhood, as required by SMC 17G.020. Six such cards were received citing concerns about topics including traffic, school capacity, the placement of low-income units in the neighborhood, and parking. It's important to note that some of these issues, such as parking, are dealt with at the building permit stage, when a project has been designed and planned. At this time all that is under consideration by the City is a land use and zoning change. Copies of these cards are included in **Exhibit L** of this staff report.

3. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on July 8, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was given an opportunity to speak but was unable to participate during the workshop.

## **VI. APPLICATION REVIEW AND ANALYSIS**

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:
  - A. Keep the comprehensive plan alive and responsive to the community.
  - B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
  - C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
  - D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
  - E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
  - F. Amendments to the comprehensive plan must result in a net benefit to the general public.
2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.
  - A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.



The proposal meets this criterion.

- B. GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency comment request or require a traffic impact analysis for the proposal. The subject property is already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

- E. Internal Consistency:**

- The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:



*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.

*Neighborhood Planning Documents Adopted after 2001.* The Southgate Neighborhood Transportation & Connectivity plan was completed in September 2010. This plan included a concept for a north-south street connection on the western boundary of the subject parcels that would lead north from the terminus of E 53<sup>rd</sup> Avenue north to the Palouse Highway. However, apartments constructed by others outside the subject parcels preclude such a roadway, as numerous structures are now in the way. It's possible that E 53<sup>rd</sup> Avenue could be extended east through the southern two parcels and then north to join up with the Palouse Highway. As the current proposed does not include any development proposals and as there is no designation for a north-south roadway in either location in the City's street plan or Arterial Street Map, this is not considered a major issue for the proposed Comprehensive Plan amendments.

The Southgate Neighborhood Plan also included a "Parks and Open Space Element." This element included schematic plans for park and trail improvements throughout the neighborhood. However, it did not call for any features that would occur on or near the subject parcels.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

See Item K.2 for below for analysis and results.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: As a map change proposal, this application does not include any amendment to the text of the plan. As discussed under item K.2.a below, the proposal appears consistent with the Comprehensive Plan.

The proposal meets this criterion.



- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** No evidence has been provided by any adjacent jurisdiction, including the County of Spokane, indicating this proposal would conflict with the CWPP or the plans of any neighboring jurisdiction. The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

**Staff Analysis:** The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for map amendments, two are proposed map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

- H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle*

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<sup>3</sup> State Environmental Policy Act



*in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Mitigated Determination of Non-Significance was issued on August 24, 2020. The only mitigating requirement was to require that the property owner dedicate the northern half of 53<sup>rd</sup> Avenue along the southern boundary of parcels 35273.0219 and 35273.0220 to the City as public right-of-way at the time of future development. The southern half of the alignment has already been dedicated to the City as right-of-way by others.

The proposal meets this criterion.

- I. ***Adequate Public Facilities:*** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of an urban area already served by the public facilities and services described in CFU 2.1. The proposed change in land-use designation affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

- J. ***UGA:*** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

**K. Demonstration of Need:**

1. ***Policy Adjustments:*** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*



Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

The proposal meets this criterion.

**2. *Map Changes:*** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.4, Higher Density Residential Uses, which directs “new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.” The subject parcels are located 330 feet south of the Southgate District Center and immediately adjacent to the unnamed District Center located southwest of the subject parcels. Policy LU 1.4 calls for greater density of residential within the vicinity of Centers, confining any new multi-family residential designations outside the vicinity of Centers to locations where the existing use is already multi-family in nature. Both of the nearby Centers are designated as District Centers, described by Policy LU 3.2, Centers and Corridors, as requiring more dense development within an area of “30 to 50 square blocks.” As the proposal would increase the residential density of land adjacent to and in close proximity to two District Centers, the proposal appears consistent with the containment and density requirements of these Comprehensive Plan policies.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude physical development of office uses on the site. The site is adequately served by all utilities and by two major arterial streets, bus service is two blocks west of the properties, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: As discussed in item a. above, the proposal would implement the desire for greater residential density in the vicinity of Centers, as described in item ‘a’ above.

The proposal meets this criterion.

**3. *Rezones, Land Use Plan Amendment:*** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally*



*consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property will change concurrently from RSF (Residential Single-Family) to RMF (Residential Multi-Family).

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears consistent with the approval criteria set forth by SMC 17G.020.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Considering the above information and the whole of the administrative record, staff recommends that Plan Commission and the City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                  |                                           |
|----------------------------------|-------------------------------------------|
| A. Existing Land Use Plan Map    | G. Wide-Area Aerial                       |
| B. Proposed Land Use Plan Map    | H. List of Relevant Comp Plan Policies    |
| C. Existing Zoning Map           | I. Application Materials                  |
| D. Proposed Zoning Map           | J. SEPA Checklist                         |
| E. Application Notification Area | K. SEPA Determination of Non-Significance |
| F. Detail Aerial                 | L. Public Comments                        |





# Z19-503COMP

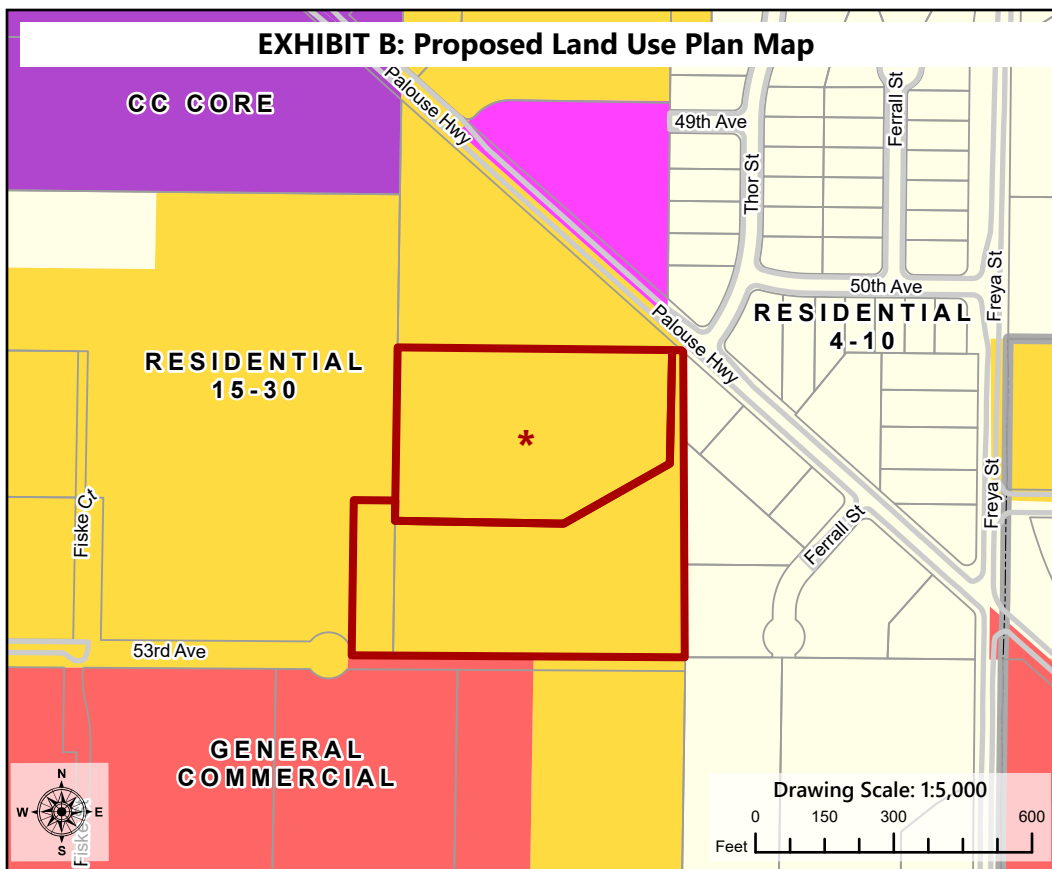
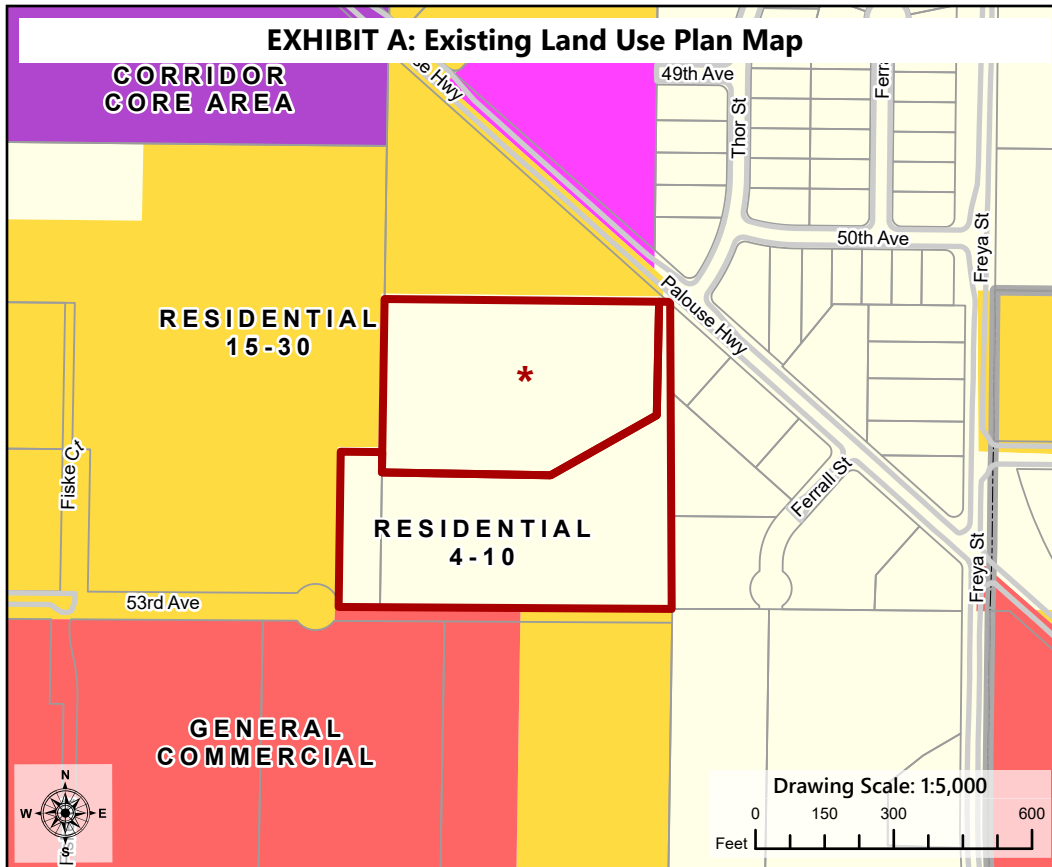
## (3227 E 53rd Ave and 5106 S Palouse Hwy)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.







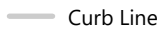
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## Legend



Subject Parcels

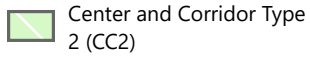


Curb Line

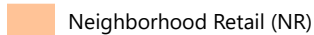


Parcel

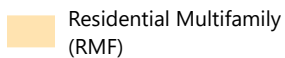
## Current Zoning



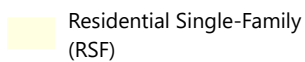
Center and Corridor Type 2 (CC2)



Neighborhood Retail (NR)



Residential Multifamily (RMF)



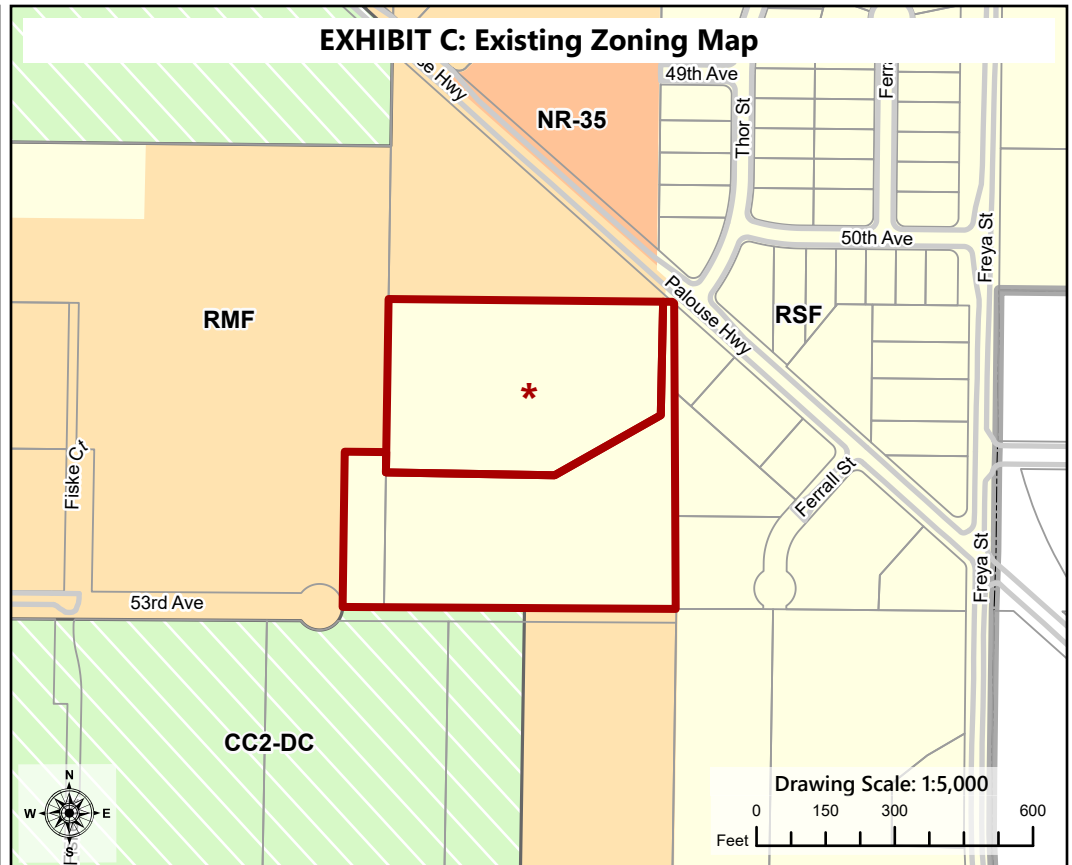
Residential Single-Family (RSF)

\* This area was added to the proposal by the City Council and comprises a City-Sponsored Application

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 10.3

## EXHIBIT C: Existing Zoning Map



## Legend



Subject Parcels

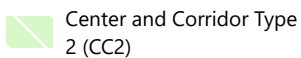


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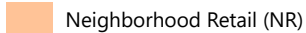


Parcel

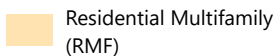
## Proposed Zoning



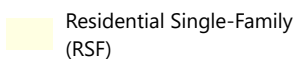
Center and Corridor Type 2 (CC2)



Neighborhood Retail (NR)

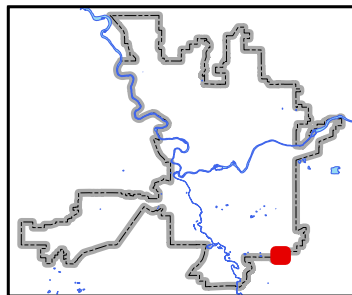


Residential Multifamily (RMF)



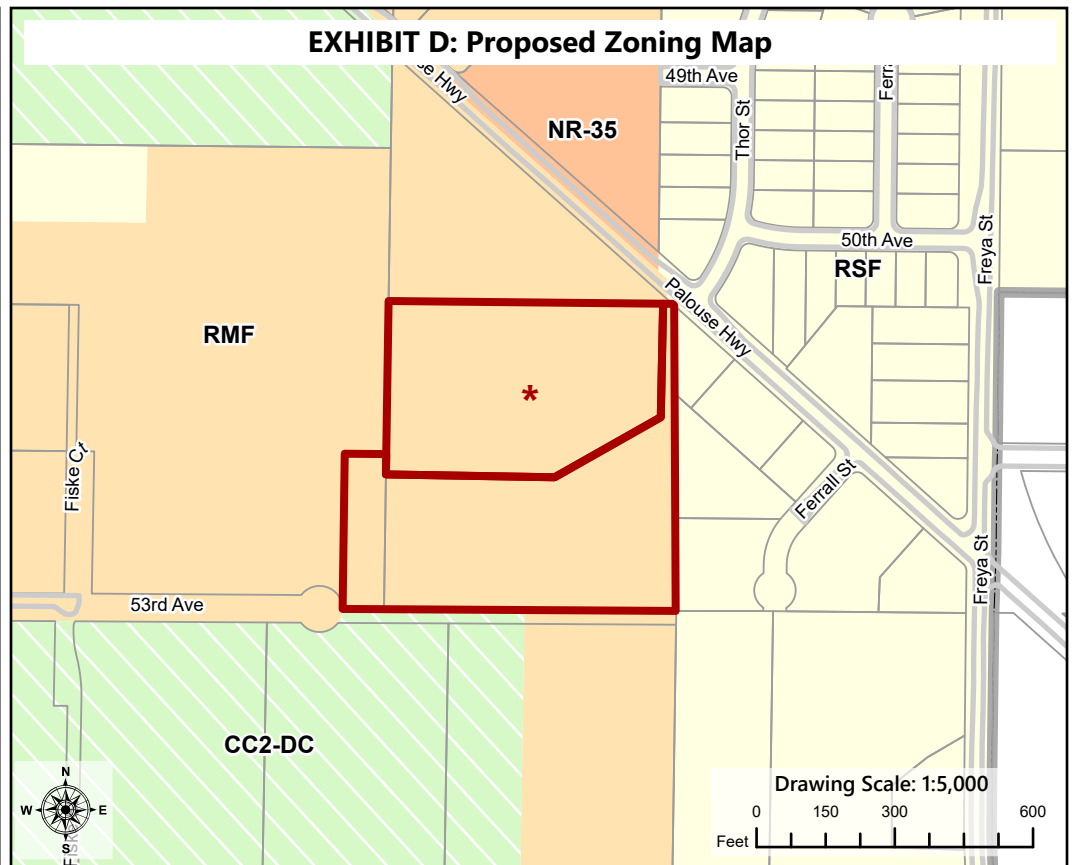
Residential Single-Family (RSF)

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





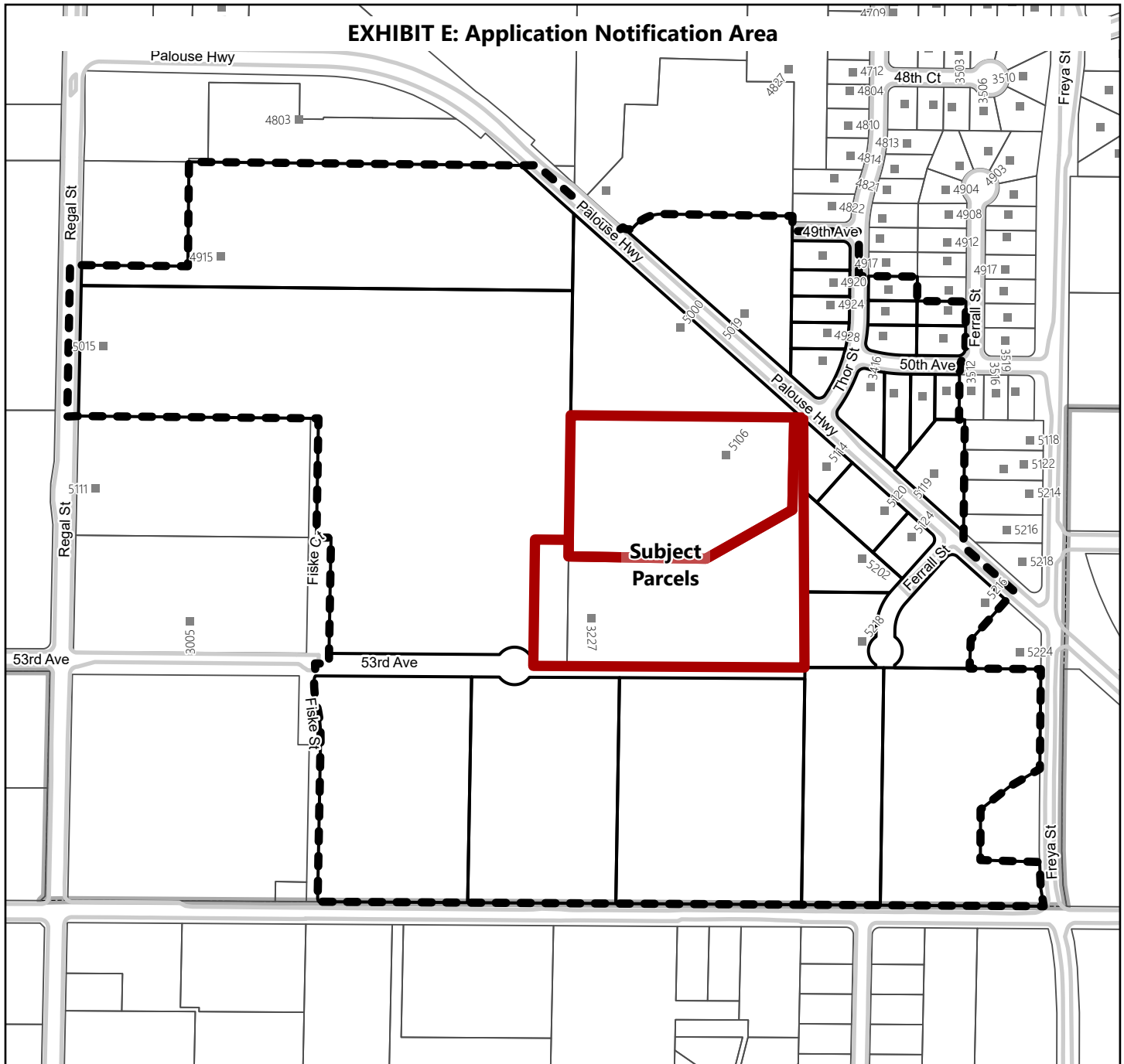


# Z19-503COMP

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2019/2020 Comprehensive Plan Amendment Proposals

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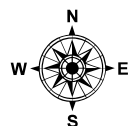
- Parcel
- Notification Boundary
- Curb Line
- Address Point

### Subject Parcels

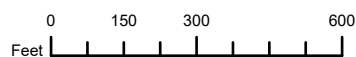
- Subject Parcels

### Application proposes to:

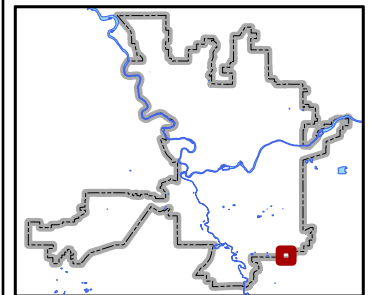
Change Land Use Designation from Residential 4-10 to Residential 15-30



Project Size: 10.3 Acres (Approximate)  
Drawing Date: 3/4/2020 Drawing Scale: 1:4,750



### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# **Z19-503COMP** **(3227 E 53rd Ave and 5106 S Palouse Hwy)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 3/4/2020

**THIS IS NOT A LEGAL DOCUMENT**

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

**EXHIBIT F: Detail Aerial**



## **Legend**

Subject Parcels

Adjacent Ownership

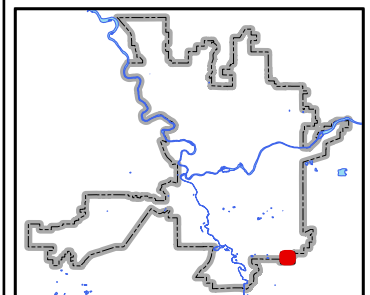
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 10.3

**EXHIBIT G: Wide-Area Aerial**



## **PROJECT LOCATION**



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-503COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.4 Higher Density Residential Uses

Direct new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* Higher density housing of various types is the critical component of a center. Without substantially increasing population in a center's immediate vicinity, there is insufficient market demand for goods and services at a level to sustain neighborhood-scale businesses. Higher density residential uses in Centers range from multi-story condominiums and apartments in the middle to small-lot homes at the edge. Other possible housing types include townhouses, garden apartments, and housing over retail space.

To ensure that the market for higher density residential use is directed to Centers, future higher density housing generally is limited in other areas. The infill of Residential 15+ and Residential 15-30 residential designations located outside Centers are confined to the boundaries of existing multi-family residential designations where the existing use of land is predominantly higher density residential.

### LU 3.1 Coordinated and Efficient Land Use

Encourage coordinated and efficient growth and development through infrastructure financing and construction programs, tax and regulatory incentives, and by focusing growth in areas where adequate services and facilities exist or can be economically extended.

*Discussion:* Future growth should be directed to locations where adequate services and facilities are available. Otherwise, services and facilities should be extended or upgraded only when it is economically feasible to do so.



The Centers and Corridors designated on the Land Use Plan Map are the areas of the city where incentives and other tools should be used to encourage infill development, redevelopment and new development. Examples of incentives the city could use include assuring public participation, using public facilities and lower development fees to attract investment, assisting with project financing, zoning for mixed-use and higher density development, encouraging rehabilitation, providing in-kind assistance, streamlining the permit process, providing public services, and addressing toxic contamination, among other things.

### LU 3.2 Centers and Corridors

Designate Centers and Corridors (neighborhood scale, community or district scale, and regional scale) on the Land Use Plan Map that encourage a mix of uses and activities around which growth is focused.

*Discussion:* Suggested Centers are designated where the potential for Center development exists. Final determination is subject to a sub-area planning process.

...

#### DISTRICT CENTER

District Centers are designated on the Land Use Plan Map. They are similar to Neighborhood Centers, but the density of housing is greater (up to 44 dwelling units per acre in the core area of the center) and the size and scale of schools, parks, and shopping facilities are larger because they serve a larger portion of the city. As a general rule, the size of the District Center, including the higher density housing surrounding the Center, should be approximately 30 to 50 square blocks.

As with a Neighborhood Center, new buildings are oriented to the street and parking lots are located behind or on the side of buildings whenever possible. A central gathering place, such as a civic green, square, or park is provided. To identify the District Center as a major activity area, it is important to encourage buildings in the core area of the District center to be taller. Buildings up to five stories are encouraged in this area.

The circulation system is designed so pedestrian access between residential areas and the District Center is provided. Frequent transit service, walkways, and bicycle paths link District Centers and the downtown area.

The following locations are designated as District Centers on the Land Use Plan Map:

- Shadle – Alberta and Wellesley;
- Lincoln Heights – 29th and Regal;
- Southgate;
- 57th and Regal
- Grand District
- Five Mile – Francis and Ash (suggested Center, with final determination subject to a sub-area planning process described in LU 3.4); and
- NorthTown – Division and Wellesley (suggested Center, with final determination subject to a sub-area planning process described in LU 3.4).

### LU 3.5 Mix of Uses in Centers



Achieve a proportion of uses in Centers that will stimulate pedestrian activity and create mutually reinforcing land uses.

*Discussion:* Neighborhood, District, and Employment Centers are designated on the Land Use Plan Map in areas that are substantially developed. New uses in Centers should complement existing on-site and surrounding uses, yet seek to achieve a proportion of uses that will stimulate pedestrian activity and create mutually reinforcing land use patterns. Uses that will accomplish this include public, core commercial/office and residential uses.

All Centers are mixed-use areas. Some existing uses in designated Centers may fit with the Center concept; others may not. Planning for Centers should first identify the uses that do not fit and identify sites for new uses that are missing from the existing land use pattern. Ultimately, the mix of uses in a Center should seek to achieve the following minimum requirements:

| Table LU 1 – Mix of Uses in Centers |                     |                                |
|-------------------------------------|---------------------|--------------------------------|
| Land Use                            | Neighborhood Center | District and Employment Center |
| Public                              | 10 percent          | 10 percent                     |
| Commercial/Office                   | 20 percent          | 30 percent                     |
| Higher-Density Housing              | 40 percent          | 20 percent                     |

*Note: All percentage ranges are based on site area, rather than square footage of building area.*

This recommended proportion of uses is based on site area and does not preclude additional upper floors with different uses. The ultimate mix of land uses and appropriate densities should be clarified in a site-specific planning process in order to address site-related issues such as community context, topography, infrastructure capacities, transit service frequency, and arterial street accessibility. Special care should be taken to respect the context of the site and the character of surrounding existing neighborhoods. The 10 percent public use component is considered a goal and should include land devoted to parks, plazas, open space, and public facilities.

#### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

### **Chapter 6 – Housing**

#### H 1.4 Use of Existing Infrastructure

Direct new residential development into areas where community and human public services and facilities are available.

*Discussion:* Using existing services and infrastructure often reduces the cost of creating new housing. New construction that takes advantage of existing services and infrastructure conserves public resources that can then be redirected to other needs such as adding amenities to these projects

#### H 1.11 Access to Transportation

Encourage housing that provides easy access to public transit and other efficient modes of transportation.



*Discussion:* Transportation is the second largest expenditure after housing and can range from 10 to 25 percent of household expenditures. Examining where housing is City of Spokane Comprehensive Plan 6-8 located and the associated transportation costs may provide a more realistic evaluation of housing affordability in the future.

#### H 2.4 Linking Housing with Other Uses

Ensure that plans provide increased physical connection between housing, employment, transportation, recreation, daily-needs services, and educational uses.

*Discussion:* The location of housing in relation to other land uses is a part of what determines the quality of housing. The desirability and viability of housing changes for different segments of the community, based on an area's mix of land uses. As complementary land uses become spread further apart, transportation options decrease while transportation costs increase. These added transportation costs reduce the amount of household income available for housing and other household needs. This affects lower-income households first. In urban areas, basic services, such as grocery stores, public transportation, and public parks, should be available within a mile walk of all housing.

### **Chapter 11—Neighborhoods**

#### N 8.4 Consistency of Plans

Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-28-19

Kevin Freibott, Planner II  
Planning & Development Services  
3<sup>rd</sup> Floor City Hall  
West 801 Spokane Falls Blvd.  
Spokane WA 99201

Ref: 3227 E 53<sup>rd</sup> LLC Map Amendment Request

Dear Kevin:

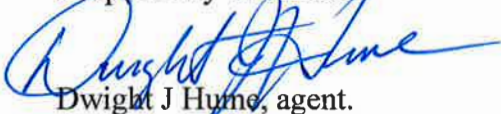
Enclosed for your review and Docketing process are the required forms, maps and fee for acceptance of this requested amendment.

While the subject site is not within the designated symbols of the adopted land use plan for District Centers, it does adjoin several other R 15-30 and a GC site which are also outside of the designated DC symbols. Accordingly, I am suggesting that the site is better interpreted as infill.

I also refer to the adjacent northerly site which is being used as a broadcasting site. In my research of this ownership, I found that the property is registered as a foreign corporation in Olympia. Nothing on the Secretary of States website indicates a local contact. I have sent a letter to the address on record for tax payments to inform them of our request.

Finally, I have sent an email to the chair of Southgate to request time on their monthly meeting to address our request.

Respectfully Submitted,



Dwight J Hume, agent.

Enclosure: Application and fee.





**DESCRIPTION OF PROPOSAL:**

Map amendment from R-<sup>4</sup>610 to R 15-30 and a zone change from RSF to RMF on 5.41 acres located along the north side of 53<sup>rd</sup> Avenue extended east of Regal Street.

**ADDRESS OF SITE OF PROPOSAL:** (if not assigned yet, obtain address from Public Works before submitting application)  
3227 E 53<sup>rd</sup> Avenue.

**APPLICANT:**

**Name:** 3227 E 53<sup>rd</sup> Ave. LLC  
**Address:** Steam Plant Square Suite 225 159 S Lincoln Spokane 99201  
**Phone (home):** **Phone (work):** N/A  
**Email address:** N/A

**PROPERTY OWNER:**

**Name:** Same as above  
**Address:**  
**Phone (home):** **Phone (work):**  
**Email address:**

**AGENT:**

**Name:** Land Use Solutions and Entitlement Dwight Hume agent  
**Address:** 9101 N Mt View Lane, Spokane WA 99218  
**Phone (home):** **Phone (work):** 435-3108  
**Email address:** dhume@spokane-landuse.com

**ASSESSOR'S PARCEL NUMBERS:**

34032.9093, 9044

**LEGAL DESCRIPTION OF SITE:**

See attached

**SIZE OF PROPERTY:**

5.41 acres

**LIST SPECIFIC PERMITS REQUESTED IN THIS APPLICATION:**

Map Amendment to Comprehensive Plan and corresponding zone change







## Legal Description

3227 E 53<sup>rd</sup> Ave LLC

### Parcel 34032.9093

03-24-43 PORTION OF GOVERNMENT LOT 11 DESCRIBED AS FOLLOWS; BEGINNING AT A POINT ON THE SOUTH LINE AND 714.1 FEET WEST OF THE SOUTHEAST CORNER OF SAID GOVERNMENT LOT 11; THENCE WEST ALONG THE SOUTHLINE TO THE SOUTHWEST CORNER; THENCE NORTH ALONG THE WEST LINE, 660 FEET; THENCE EAST 620.2 FEET; THENCE SOUTH 660 FEET, TO THE POINT OF BEGINNING, EXCEPT THAT PARCEL DESCRIBED IN DOCUMENT NO. 78812C, FILED IN THE SPOKANE COUNTY AUDITOR'S OFFICE, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE WEST LINE OF SAID GOVERNMENT LOT 11, WHICH POINT IS 286.2 FEET NORHERLY OF THE SOUTHWEST CORNER OF SAID GOVERNMENT LOT 11; THENCE NORHTERLY ALONG THE SAID WEST LINE OF GOVERNMENT LOT 11 A DISTANCE OF 373.8 FEET; THENCE DUE EAST PARALLEL WITH THE SOUTH LINE OF SAID GOVERNMENT LOT 11 A DISTANCE OF 596 FEET; THENCE SOUTH 0 DEGREES 38' EAST A DISTANCE OF 240.4 FEET; THENCE SOUTH 59 DEGREES 37' WEST A DISTANCE OF 263.8 FEET; THENCE DUE WEST PARALLEL WITH THE SAID SOUTH LINE OF GOVERNMENT LOT 11 A DISTANCE OF 370 FEET, MORE OR LESS, TO THE POINT OF BEGINNING. Containing 4.73 acres.

### Parcel 34032.9044

E 90 ft of S1/2 of S1/2 of Govt Lot 12 Containing .68 acres.





# Comprehensive Plan Amendments

## Threshold Review

### 3227 E 53<sup>rd</sup> Ave. LLC Map Amendment R 6-10 to R 15-30

#### **Pre-application:**

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

#### **Description of the Proposed Amendment:**

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.

*Parcel #'s 34032.9093 and 9044 on 5.4 acres located on the North side of 53<sup>rd</sup> Avenue extended at 3227 E 53<sup>rd</sup>. See attached maps.*

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*This is a map amendment to the land use designation of the adopted Comprehensive Plan. No other action can accomplish a change of category, therefore this is an appropriate request.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*The subject property is bounded by R 15-30 or GC designations with apartments, future retail and an existing radio broadcasting station immediately adjacent and is not, therefore appropriate for R 6-10 use. An on-going work program would not negate the obvious conclusion that the subject property is inappropriately classified amidst more intense zones.*
3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.  
*The subject site is 5.4 acres surrounded by more intense use. It can be reasonably reviewed within the normal workload of annual amendments.*



4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?

*The subject site adjoins a radio broadcasting station and if approved as requested will enclose the remaining R 6-10 site of the non-conforming radio station. An effort has been made to contact the ownership but without any reply. (They are registered with the state of Washington as a foreign corporation. Nevertheless, we would concur that it should be included based upon the same reasons this request is being made.*

5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.

*Therefore, the request is consistent with the current comprehensive plan and therefore is consistent with Countywide Planning Policies, the GMA and other applicable state and federal regulations.*

6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated. *No, this was never reviewed in the past.*
7. If this change is directed by state law or a decision of a court or administrative agency, please describe. *N/A*
8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application. *The applicant is requesting to be on the November agenda of the Southgate Neighborhood Council.*



Early Threshold Review Form Supplement

3227 E 53<sup>rd</sup> Ave. LLC

5). Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.

*The subject property is located within proximity to two District Centers designated on the adopted Land Use Plan map. To the SW, is 57<sup>th</sup> and Regal and to the NW is Southgate. Between the subject site and these symbols of District Centers are various apartment projects and a General Commercial designation zoned CC-2 DC immediately south and west across 53<sup>rd</sup> Avenue. Consequently, it serves the purpose of District Centers by providing more dense housing options within walkable distance to these services. Accordingly, this in-fill of R 15-30 against existing apartment projects, needs no further sub-area planning as suggested by LU 3.3 and LU 3.4.*

*Indeed, this infill provides furthers the provision for a compatible mix of housing and commercial uses within the Regal and 57<sup>th</sup> DC. (LU 4.2).*

*Moreover, the subject site has direct connections to both 53<sup>rd</sup> and the Palouse Highway, thus enabling a pedestrian-bicycle pathway to and from retail services and nearby-housing. (LU 4.4, TR 2.14, N 4.6)*

*H 1.9 is implemented by providing the opportunity for a range of income levels within immediate proximity to existing low- middle income housing units.*

*H 2.1 is being implemented by providing for housing options within this vicinity. It is within proximity of low-income and medium income housing options.*

*H 3.4 is implemented because of the proximity to employment and daily needs services.*

*LU 1.4 addresses infill of Residential 15-30 as confined to existing residential designations where existing use of land is predominately higher density residential. As stated before, the subject property is located adjacent to RMF and CC-2 DC*



*zoned properties and an existing non-conforming broadcasting station. It is therefor suitable for similar use and should be considered infill.*

*The request is consistent with the CWPP. The CWPP encourages growth in urban areas where services and utilities already exist. When the site is further developed, the applicant or developer will be required to demonstrate that levels of service are maintained, as required by the CWPP. The CWPP also encourages the use of public transit and development where public transit is available. It is important to note that the city has adopted development regulations and policies to implement the CWPP at the City level. Thus, consistency with the CWPP is achieved.*

*The application is consistent with the goals and policies of the Growth Management Act. The GMA encourages densification, in-fill and urban development and redevelopment in areas designated for urban growth and within existing city limits. The property is within the UGA and the city limits of Spokane*

(End of Supplement)



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-28-19

Clear Channel Broadcasting Inc  
20880 Stone Oak Parkway  
San Antonio, TX 78258

Ref: Spokane WA property at 5106 S Palouse Hwy.

To whom this may concern:

This is to inform you that the adjacent and southerly five-acre parcel is requesting a zone change from Residential Single Family (RSF) to Residential Multi-Family (RMF) similar to existing apartment zones adjacent to your west boundary. If approved, your property will be encircled by the RMF zone, as the subject property also extends along your east boundary to the Palouse Highway. (See enclosed zone map).

Under the formal procedures of the City of Spokane, the City has the discretion to include your property in this request, resulting in a change of zoning from RSF to RMF. This would not change your rights to use the site as currently intended, but simply changes the zone as stated. If for any reason, you do not wish to change your zone, a letter to me as agent for the adjoining property would be helpful so I could request that your property remain in the current RMF zone.

The timeframe to reply is on or before February 1, 2020 after which the City Council will be formally addressing our request and could possibly request that your property be included. I look forward to your response and remain available to clarify any of the above.

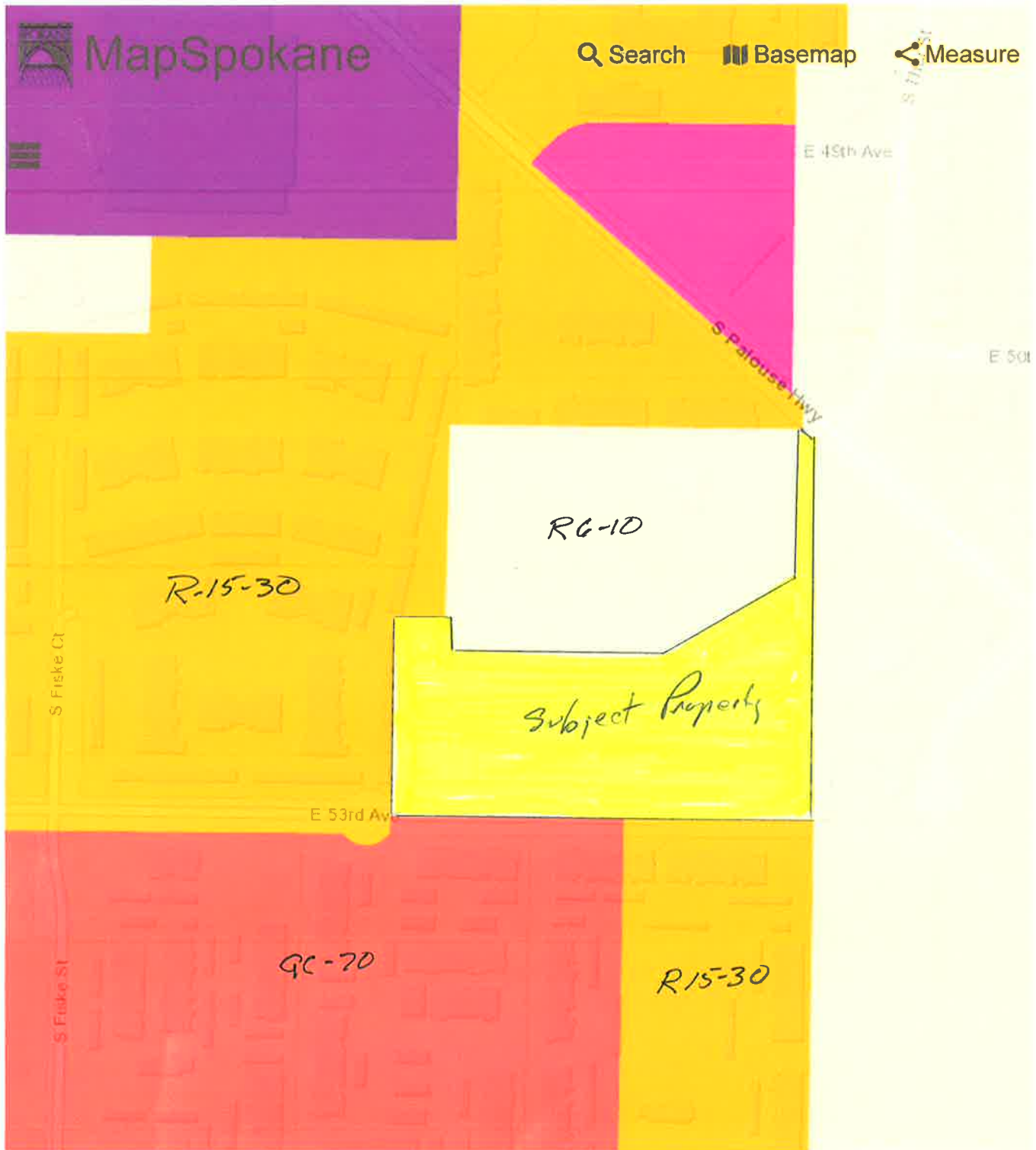
Respectfully Yours,



Dwight J Hume

Enclosure: Zone Map

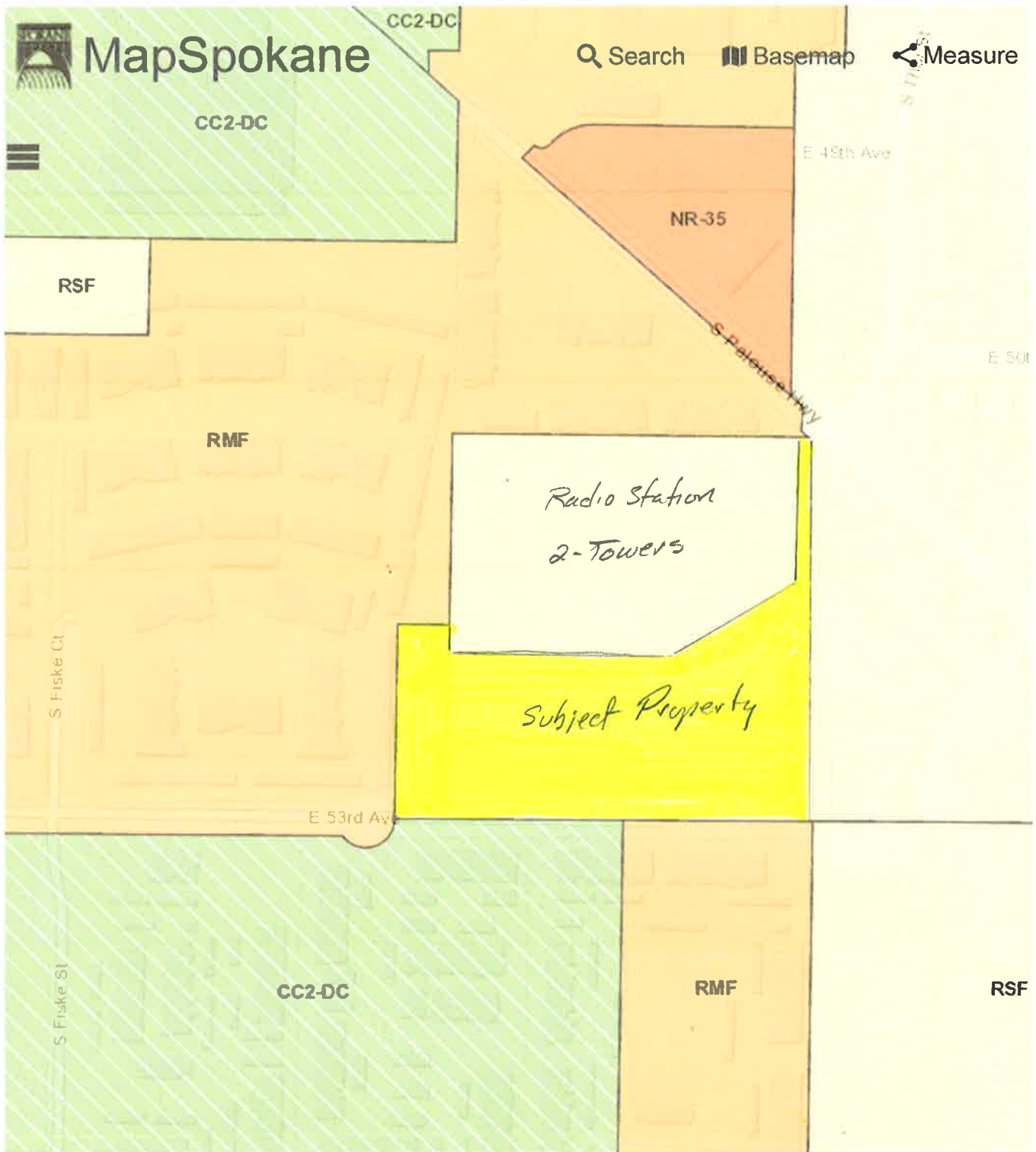




0 150 300ft

[Map Use Disclaimer](#)





0 150 300ft



[Map Use Disclaimer](#)





MapSpokane

Search

Basemap

Measure



E 49th Ave

S Palouse Hwy

E 50th Ave

Radio Station  
2-Towers

Subject Property

S Fiske Ct

S Fiske St

E 53rd Ave

0 150 300ft



[Map Use Disclaimer](#)



## Environmental Checklist

File No. Z19-503COMP

### Purpose of Checklist:

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

### Instructions for Applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### Use of checklist for non-project proposals:

Complete this checklist for non-project proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For non-project actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**Note to readers:** The Spokane City Council expanded this proposal to include an additional parcel adjacent to the original proposal, with the same zoning and land use and similar situation/condition, pursuant to [SMC 17G.020.025](#). This proposal is now a joint private/City-sponsored application. The City has added the following property to the proposal:

- Parcel 34032.9094 at 5106 S Palouse Highway, 4.82 acres in size.

The following SEPA checklist was completed by the Applicant for the property described in black below. For the property added above, any additional information necessary for the SEPA checklist has been included in red text below.

## A. BACKGROUND

1. Name of proposed project, if applicable: Z19-503COMP
2. Name of applicant: Dwight Hume of Land Use Solutions and Entitlement and the City of Spokane
3. Address and phone number of applicant or contact person:  
Land Use Solutions & Entitlement Dwight Hume agent  
9101 N Mt. View Lane Spokane WA 99218 509-435-3108  
  
City Contact: Kevin Freibott, Assistant Planner, City of Spokane, 509-625-6184,  
kfreibott@spokanecity.org.
4. Date checklist prepared: March 29 2020
5. Agency requesting checklist: City of Spokane
6. Proposed timing or schedule (including phasing, if applicable):  
Project is a comprehensive plan amendment, modifying the land use plan map designation and zoning of the subject properties. This action is expected to be decided late fall or winter of 2020.
7.
  - a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. N/A, non-project action
  - b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. No
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.  
  
Upon approval of the zone change, a project will have to comply with all applicable development regulations, including SEPA conditions if applicable.
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. No other actions are pending



10. List any government approvals or permits that will be needed for your proposal, if known.

Comp Plan amendment and zone change;

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.

A 5.4 acres with frontage along 53<sup>rd</sup> Avenue and access to the Palouse Highway. The proposed project would allow the 5.4 acres to be built out with apartments at a medium density of approximately 162 units. (5.4 x 30) This action is an amendment to the Comp Plan designation and Zone Map. Also, 4.82 acres currently containing a radio station building and two radio towers. No redevelopment or physical change to the city-added property is anticipated.

12. Location of the proposal. Give sufficient information to a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist.

The site is located south of Regal and Palouse Hwy at the east end of 53<sup>rd</sup> Avenue. It is flanked by existing apartment complexes to the west and south and adjoins a 4.2 acre site on its north boundary currently being used for a radio station. Note, the radio station site was recommended for inclusion by the Council. This was for zoning consistency to avoid an "island" of R-4-10 designation surrounded by R-15-30. The probability of its conversion to apartments is minimal since the radio broadcasting station is very viable and has no plans to be closed.

The city-added parcel is located immediately north of the properties described above.

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.)  
Yes; Sewer Service Area: Yes; City of Spokane

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

- (1) Describe any systems, other than those designed for the disposal of sanitary waste, installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of



material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities).

N/A, non-project action

- (2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?

N/A, non-project action

- (3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems.

N/A, non-project action

- (4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater?

N/A, non-project action

b. Stormwater

- (1) What are the depths on the site to groundwater and to bedrock (if known)?

Unknown

- (2) Will stormwater be discharged into the ground? If so, describe any potential impacts?

No, there is a storm water sewer system of regional scale serving this property.

**TO BE COMPLETED BY APPLICANT**

**B. ENVIRONMENTAL ELEMENTS**

**1. Earth**

- a. General description of the site (circle one): **flat, rolling, hilly, steep slopes, mountains, other:** \_\_\_\_\_
- b. What is the steepest slope on the site (approximate percent slope)?  
Unknown
- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. N/A, non-project action
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. N/A, non-project action



- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill: \_\_\_\_\_  
To be determined at time of construction by others
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. \_\_\_\_\_  
To be determined at time of construction by others
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? To be determined at time of construction by others
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: Development per applicable drainage standards and plans approved by Spokane No action is proposed or expected on the city-added sites.

## 2. Air

- a. What type of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial, wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.  
Construction equipment and activity would generate dust. After the project is completed it would be limited to traffic ingress and egress. No action is proposed or expected on the city-added sites.
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. \_\_\_\_\_  
Traffic from nearby apartment complexes and the Palouse Highway.
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:  
Paving of driving surfaces and dust abatement during construction of the site.  
No action is proposed or expected on the city-added sites.

## 3. Water

- a. SURFACE:
  - (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.  
No
  - (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.  
None



- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

None

- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No, the project site is served by City of Spokane water service

- (5) Does the proposal lie within a 100-year floodplain? \_\_\_ If so, note location on the site plan.

No

- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No

b. GROUND:

- (1) Will groundwater be withdrawn, or will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No, the site is served by City of Spokane water service

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sanitary waste treatment facility. Describe the general size of the system, the number of houses to be served (if applicable) or the number of persons the system(s) are expected to serve.

The site is served with City of Spokane Sewer service

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

No, the site will discharge storm drainage into the City of Spokane system

- (2) Could waste materials enter ground or surface waters? If so, generally describe.

No

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water impacts, if any.

None



#### 4. Plants

- a. Check or circle type of vegetation found on the site:
- \_\_\_\_\_ **Deciduous tree:** *alder, maple, aspen, other.*
- \_\_\_\_\_ **Evergreen tree:** *fir, cedar, pine, other.*
- \_\_\_\_\_ **Shrubs**
- \_\_\_\_\_ **Grass**
- \_\_\_\_\_ Pasture
- \_\_\_\_\_ Crop or grain
- \_\_\_\_\_ Wet soil plants, *cattail, buttercup, bullrush, skunk cabbage, other.*
- \_\_\_\_\_ Water plants: *water lilly, eelgrass, milfoil, other.*
- \_\_\_\_\_ Other types of vegetation.
- b. What kind and amount of vegetation will be removed or altered? To be determined at time of construction by others \_\_\_\_\_
- c. List threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:  
To be determined at time of construction by others \_\_\_\_\_

#### 5. Animals

- a. Circle any birds and animals which have been observed on or near the site are known to be on or near the site:
- birds: ***hawk, heron, eagle, songbirds***, other: urban fowl \_\_\_\_\_
- mammals: *deer, bear, elk, beaver, other.* \_\_\_\_\_
- fish: *bass, salmon, trout, herring, shellfish, other.* \_\_\_\_\_
- other: \_\_\_\_\_
- b. List any threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- c. Is the site part of a migration route? If so, explain. No \_\_\_\_\_
- d. Proposed measures to preserve or enhance wildlife, if any: \_\_\_\_\_  
None \_\_\_\_\_

#### 6. Energy and natural resources



- a. What kinds of energy (**electric, natural gas**, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.  
The site contains one dwelling unit served with all utilities. No new services are needed to serve the site. The city-added site includes a radio station and transmitting antennae but no changes are expected or proposed for the site, thus no additional energy needs are evident.
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. No
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:  
None

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. No
- (1) Describe special emergency services that might be required.  
None
- (2) Proposed measures to reduce or control environmental health hazards, if any:  
None
- b. NOISE:
- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?  
General traffic noise of the area
- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.  
To be determined at time of construction by others
- (3) Proposed measure to reduce or control noise impacts, if any:  
None



## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties?  
Site: vacant and residential rental  
West: Apartments  
East: Single family  
South: Apartments  
North: Radio Station
- North of the radio station (added to the project by the City Council) are apartments.
- b. Has the site been used for agriculture? If so, describe. No
- c. Describe any structures on the site. rental and various out buildings The city-added site includes a radio station building (commercial building) and two transmitting antennae.
- d. Will any structures be demolished? If so, which? Yes, all structures The structures on the city-added site are not proposed for demolition or reconstruction at this time.
- e. What is the current zoning classification of the site? RSF
- f. What is the current comprehensive plan designation of the site? R 4-10
- g. If applicable, what is the current shoreline master program designation of the site?  
None
- h. Has any part of the site been classified as a critical area? If so, specify. No
- i. Approximately how many people would reside or work in the completed project? To be determined at time of construction by others
- j. Approximately how many people would the completed project displace? 1 single-family unit The structures on the city-added site are not proposed for demolition or reconstruction at this time, nor do those structures currently provide any housing.
- k. Proposed measures to avoid or reduce displacement impacts, if any: none
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: Compliance with all applicable development regulations as required by a subsequent CUP approval.



## 9. Housing

**Reviewer's Note:**  
The proposal constitutes only a land use and zoning change. No additional housing is proposed or required by this proposal.

- a. Approximately how many units would be provided, if any? Indicate whether high, middle or low-income housing. 162 units maximum No construction of housing is proposed or expected on the city-added site.
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. One middle income There is no existing housing on the city-added site.
- c. Proposed measures to reduce or control housing impacts, if any: None

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? To be determined at time of construction by others. The city-added site includes two transmitting antenna, approximately 175 feet in height. These would remain under the portion of the proposal added by the City Council.
- b. What views in the immediate vicinity would be altered or obstructed? None. As the project development would be similar to the surrounding land use.
- c. Proposed measures to reduce or control aesthetic impacts, if any: N/A

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? To be determined at time of construction by others
- b. Could light or glare from the finished project be a safety hazard or interfere with views? No
- c. What existing off-site sources of light or glare may affect your proposal? None
- d. Proposed measures to reduce or control light and glare impacts, if any: None

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? SE Sports Complex and YWCA and YMCA
- b. Would the proposed project displace any existing recreational uses? If so, describe. No



- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:  
None

### 13. Historic and cultural preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. None
- b. Generally describe any landmarks or evidence of historic archaeological, scientific or cultural importance known to be on or next to the site.  
None
- c. Proposed measures to reduce or control impacts, if any: \_\_\_\_\_  
None

### 14. Transportation

- a. Identify public streets and highways serving the site and describe proposed access to the existing street system. Show on site plans, if any. The site is served by Regal to 53<sup>rd</sup> and from Palouse Hwy to site.
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?  
Yes at Regal and 53<sup>rd</sup> approximately 1/4 mile west
- c. How many parking spaces would the completed project have? How many would the project eliminate?  
To be determined at time of construction by others
- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets not including driveways? If so, generally describe (indicate whether public or private). No new roads or streets are needed, however 53<sup>rd</sup> may require widening and full improvements at the site as it becomes an unpaved road in the site frontage along 53<sup>rd</sup>. Access to and from Palouse Hwy would require a formal driveway access point. No access improvements are expected or required for the city-added site.
- e. Will the project use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. No
- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak would occur.  
To be determined at time of construction by others, based upon actual units proposed and site planning.

*(Note: to assist in review and if known indicate vehicle trips during PM peak, AM Peak and Weekday (24 hours).)*



- g. Proposed measures to reduce or control transportation impacts, if any:  
None

**15. Public services**

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe. No new or expanded services would be needed or generated by this proposal
- b. Proposed measures to reduce or control direct impacts on public services, if any: None

**16. Utilities**

- a. Circle utilities currently available at the site: ***electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.***
- b. Describe the utilities that are proposed for the project, the utility providing the service and the general construction activities on the site or in the immediate vicinity which might be needed.  
None



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 Signature: Dwight J Hume

***Please Print or Type:***

Proponent: Dwight J Hume Address: 9101 N Mt. View Lane

Phone: 509.435.3108 Spokane WA, 99218

Person completing form (if different from proponent): Same

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☐ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☒ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS**  
***(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage or release of toxic or hazardous substances; or production of noise?

No impacts are foreseen from apartment use amongst the existing apartment environment surrounding the subject property. No physical changes to the city-added sites are proposed or expected.

Proposed measures to avoid or reduce such increases are:

Compliance with applicable development standards

2. How would the proposal be likely to affect plants, animals, fish or marine life?  
Future development of the site would be urban in nature, similar to that adjoining the subject parcels. Eventual redevelopment of the site may require the removal of on-site plants, subject to the requirements of the Spokane Municipal Code for new construction. No physical changes to the city-added sites are proposed or expected.

Proposed measures to protect or conserve plants, animals, fish or marine life are:

None

3. How would the proposal be likely to deplete energy or natural resources?  
While some additional resources would be required during redevelopment, these would be similar to those required of any construction project. No physical changes to the city-added sites are proposed or expected.

Proposed measures to protect or conserve energy and natural resources are:

None

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands?

The site does not contain sensitive areas

Proposed measures to protect such resources or to avoid or reduce impacts are:

None

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?



Development would comply with applicable development regulations, including landscape, screening and setbacks. Furthermore, the site is located outside any shoreline areas. The city-added site is outside any shoreline areas or uses.

Proposed measures to avoid or reduce shoreline and land use impacts are:  
See above comment

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

The proposed use would generate more traffic load on Regal and Palouse Hwy. Schools will be impacted with more residential density on site, however utility demand is not expected to be impacted. No physical changes to the city-added sites are proposed or expected.

Proposed measures to reduce or respond to such demand(s) are:

None.

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment.

No impacts are foreseen



C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 Signature: *Dwight J Hume*

***Please Print or Type:***

Proponent: Dwight J Hume Address: 9101 N Mt. View Lane

Phone: 509.435.3108 Spokane WA, 99218

Person completing form (if different from proponent): Same

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☐ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☒ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**NONPROJECT MITIGATED DETERMINATION OF NONSIGNIFICANCE****FILE NO(S):** Z19-503COMP**PROPONENT:** 3227 E 53<sup>rd</sup> Ave, LLC (Agent: Dwight Hume, Land Use Solutions and Entitlement) and the City of Spokane (Contact: Kevin Freibott, Department of Neighborhood and Planning Services).**DESCRIPTION OF PROPOSAL:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the land use designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the Southgate neighborhood. One parcel was added to the proposal by the City of Spokane, proposed for the same action.**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns three parcels, 35273.0219 and 35273.0220, located immediately northeast of the intersection of S 29<sup>th</sup> Avenue and S Ray Street as well as one parcel immediately north of those (parcel 34032.9094). The parcels are located at 3227 E 53<sup>rd</sup> Ave and 5106 S Palouse Highway. The entire proposal would affect an area of approximately 10.3 acres.**Legal Description:** Full legal description is on file with the City of Spokane. All parcels are located in the City of Spokane in Section 3, Township 24 North, Range 43 East.**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

☐ There is no comment period for this MDNS.

☐ This DNS is issued after using the optional MDNS process in section 197-11-355 WAC. There is no further comment period on the MDNS.

☒ This MDNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the MDNS.

**Mitigating Measures:** Prior to any future development of parcels 35273.0219 and/or 35273.0220, the northern half of the alignment of an extension of 53rd Avenue along the entire southern boundary of the parcels shall be dedicated to the City of Spokane as public right-of-way for the purpose of extending street improvements along that alignment to the east of the parcels. This mitigation is required in order to provide adequate access to and from these parcels and to mitigate the additional traffic load that would result if and when parcels are redeveloped following the proposed change in Land Use Plan Map designation and zoning.

\*\*\*\*\*

**Responsible Official:** Louis Meuler**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201**Date Issued:** August 24, 2020**Signature:**  Louis Meuler

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 7, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*



Tirell,

These are comments  
we collected when  
D. Hume presented  
on Z19-503 on  
4/5/2020.

Teresa Kafentzis

**Southgate Neighborhood Council -- Comments for Comp**

Application Z19-503COMP, address is 3227 53rd

(optional) Name: Rita Connor Address: 30

What neighborhood council do you live in?

☒ Southgate ☐ Lincoln Heights

☐ Other neighborhood council, \_\_\_\_\_

☐ In county, outside neighborhood council boundaries

**Comments -- will be forwarded to Spokane City Planning**







**Southgate Neighborhood Council -- Comments for Comprehensive Plan Amendment**

Application Z19-503COMP, address is 3227 53rd

(optional) Name: Donna Becker Address: 7425 E 53rd

What neighborhood council do you live in?

☐ Southgate ☐ Lincoln Heights ☐ Comstock

☐ Other neighborhood council, \_\_\_\_\_

☒ In county, outside neighborhood council boundaries

**Comments -- will be forwarded to Spokane City Planning Department**

Traffic issues -  
low income?

**Southgate Neighborhood Council -- Comments for Comprehensive Plan Amendment**

Application Z19-503COMP, address is 3227 53rd

(optional) Name: Summer Beers Address: 4003 E. Sumac Dr.

What neighborhood council do you live in?

☒ Southgate ☐ Lincoln Heights ☐ Comstock

☐ Other neighborhood council, \_\_\_\_\_

☐ In county, outside neighborhood council boundaries

**Comments -- will be forwarded to Spokane City Planning Department**

Concerns about changing this property to high density residential:

- traffic increases in area, while we do not have appropriate sidewalks and bike lanes to allow safe transport on nearby arterials.

- Schools in area are at capacity, zoning changes will further crowd local schools



**Southgate Neighborhood Council -- Comments for Comprehensive Plan Amendment**

Application Z19-503COMP, address is 3227 53rd

(optional) Name: Stefanie E. Address: 3323 E 55<sup>th</sup> Ave

What neighborhood council do you live in?

☒ Southgate ☐ Lincoln Heights ☐ Comstock

☐ Other neighborhood council, \_\_\_\_\_

☐ In county, outside neighborhood council boundaries

**Comments -- will be forwarded to Spokane City Planning Department**

I highly oppose this plan to use this land for more apartments. This area is getting beyond FULL of apartments. Traffic on Regal is getting worse and worse. ALSO, 53<sup>rd</sup> as of now, is a dead end. Please respect the current apartment residents. Just because we live in an apartment does not mean we want neighbors out every window. We moved to this area on Bad



because there was some Space left.  
I don't live downtown for a reason.  
Even apartment dwellers like a little  
Space. I oppose this proposal.





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-503COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “Residential 15-30” for 10.3 acres located at 3227 E 53<sup>rd</sup> Avenue and 5106 S Palouse Highway. The implementing zoning designation requested is RMF (Residential Multi-Family).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-503COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a 10.3-acre area located at 3227 E 53<sup>rd</sup> Avenue and 5106 S Palouse Highway (the “Properties”) from “Residential 4-10” to “Residential 15-30” with a corresponding change in zoning from Residential Single Family (RSF) to Residential Multi-Family (RMF).
- E. The property on S Palouse Highway was added to the proposal by the City Council upon adoption of the 2020 Work Program (see ‘I’ below).
- F. The two parcels at 3227 E 53<sup>rd</sup> Avenue are held in common ownership by the applicant; the property on S Palouse Highway is held by a second owner, unrelated to the first.
- G. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- H. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- I. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, including the Application in the Work Program.
- J. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. No agency/department/neighborhood council comments were received.
- K. The City issued a Notice of Application on June 8, 2020 by posting it in the Spokesman Review and Official Gazette and by mail to all properties and owners within a 400-foot radius of the subject Properties and any adjacent properties with the same ownership. The Notice of Application



initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City received comment letters from the neighborhood council stating concerns with traffic/parking, school capacity, and the placement of low-income units in the neighborhood.

- L. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and the Community Assembly was provided with information regarding the dates of Plan Commission workshops and hearings.
- M. On July 8, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- N. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- O. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Mitigated Determination of Non-Significance (MDNS) were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
  - 2. The sole mitigation in the MDNS was to require the dedication of the northern half of E 53<sup>rd</sup> Avenue along the southern boundary of the Properties at the time of future development; a condition accepted by the applicant.
- P. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- Q. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the Properties and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject property.
- R. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Application recommended approval of the Application.
- S. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of the SEPA Mitigated Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- T. Following issuance of the public hearing notice, the City received additional comment letters in opposition to the proposal from members of the public, citing perceived ecological impacts, traffic, school capacity, impacts on public services, and increased density.



- U. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- V. One member of the public testified during the hearing on September 9, 2020 in opposition to the proposal, citing concerns with existing traffic conditions in the area.
- W. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- X. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the "Staff Report").
- Y. The Plan Commission finds that the proposal meets the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.4 concerning the establishment of higher density residential land uses in the City.
- Z. The Plan Commission finds that the proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-503COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Application was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Application is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Application is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.



7. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Application.
9. The Application will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Application proposes a land use designation that is consistent with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment and site is suitable for the proposed designation.
12. The map amendment would implement applicable comprehensive plan policies better than the current map designation.

#### **RECOMMENDATIONS:**

In the matter of Z19-503COMP, a request by Dwight Hume of Land Use Solutions and Entitlement on behalf of the 3227 E 53<sup>rd</sup> Ave, LLC to change the land use plan designation on 10.3 acres of land from "Residential 4-10" to "Residential 15-30" with a corresponding change of the implementing zoning to RMF (Residential Multi-Family), as based upon the above listed findings and conclusions, by a vote of 8 to 1, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to the Land Use Plan Map of the City's Comprehensive Plan with corresponding amendment to the City's Zoning Map, and authorizes the President to prepare and sign on the Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the Application.

---

**Todd Beyreuther, President**

Spokane Plan Commission

October \_\_, 2020



**DISSENTING OPINION RE:  
Comprehensive Plan Amendment Z19-503COMP  
(3227 E 53<sup>rd</sup> and 5106 S. Palouse Highway)**

**STATEMENT OF DISSENTING OPINION:**

This project conflicts with good city planning goals adopted by the GMA (RCW 36.70A.020). Which in part directs communities to promote concentrated urban growth, reduce sprawl, and produce affordable housing. Building high density housing in this most southerly area of Spokane City (Southgate Neighborhood; also for the North Indian Trail Neighborhood and the Shiloh Hills Neighborhood to our north City boundary) creates population sprawl. That is population density that looks more like a “barbell” of Net Residential Density<sup>1</sup> going from the northern City boundary to the southern City boundary.

**REASONS FOR DISSENSION:**

**A.** Why is this sprawl not desired? The 1990 GMA was passed to prevent this type of urban population sprawl and cities were to develop comprehensive plans to address the issue of sprawl.

Some negative outcomes of population sprawl:

- Increased traffic on Spokane north/south arterials
- Higher road maintenance because residents travel more (unnecessary) miles within the City
- More greenhouse gas emissions
- More pollution
- Longer and more frustrating commutes for citizens
- More cars on the road, necessitating poor land use in neighborhoods, work sites, and commerce establishments with parking spaces (pavement surface) increasing urban heat and water runoff.
- Higher total transportation cost for residents at City extremes, reducing true affordability
- Encourages motor vehicle use
- Reduces walkable and bikeable City routes
- Increased road rage
- Extended costs (mileage) of moving food, goods, trash, etc.

**B.** Traditionally these negative outcomes reduce commerce in city centers and eventually may destroy a once vibrant ‘downtown.’

**C.** Unfortunately approving this proposal is in conflict with RCW 36.70A.020 (2) and (10) The Comprehensive Plan needs to encourage infill and higher Net Residential Density (NRD) in Spokane within a one to two mile radius of City Hall. Another project was



rejected (W 10th Avenue) because our Comprehensive Plan does not allow a R15-30 in a R4-10 zone. This Comprehensive Plan restriction promotes sprawl rather than infilling. (Why is there no intermediate R10-15 residential zone?)

#### **CONCLUSIONS:**

A. City Council needs to address how The City of Spokane can encourage higher NRD in our City core and a lower NRD near our City boundaries in The Comprehensive Plan while keeping housing costs affordable. (*cf.* where SMC 17C.110.030 residential land uses can be aptly mixed)

**Clifford Winger**  
**Spokane City Plan Commissioner**  
**25 September, 2020**

1 Net Residential Density is calculated by taking the minimum number of planned housing units and dividing by the net acreage. Net acreage does not include land covered by wetlands, water bodies, public parks and trails, public open space, arterial road rights-of-way, and other undevelopable acres identified in or protected by local ordinances such as steep slopes.



**From:** [Heather Dakota](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Southgate Land Use Amendment - File No. Z19-503COMP  
**Date:** Wednesday, September 2, 2020 3:56:02 PM

---

NOTE: The following comments were received after completion of the Staff Report. They have been seen and considered by the Plan Commission as part of their decision.

[CAUTION - EXTERNAL EMAIL - Verify Sender]

Currently, I reside at the apartment complex directly adjacent to the proposed land use amendment. My apartment actually faces the property in question.

I understand that it is a difficult choice for city planners to add needed residences or keep green space.

However, the land in question is home to a large covey of California Quail, as well as countless other species of animals, birds, plants, and trees. Putting up apartment complexes will damage this ecosystem, which would be a shame. It's one of the reasons I moved to this particular area.

I would like to voice my concern and disapproval of the proposed land use designation change. Please keep it zoned single family RSF (4-10). Protect this green space for the environment and the beauty of Spokane.

Thank you for your consideration and taking the time to read this email.

Sincerely,  
Heather Jansen

5015 S. Regal St.  
Apt O2119  
Spokane 99223



**From:** [Laurie Nisbet](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** New apartments off palouse highway  
**Date:** Wednesday, September 9, 2020 1:13:23 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Sent from my Verizon, Samsung Galaxy smartphone

To whom it may concern,

I am so very disappointed that the city feels adding more apartments to this area of the south hill is necessary. Is it necessary to continue to overpopulate this area? Is it necessary to add more cars to already densely filled roads? Is it necessary to fill our schools with children when there is no room for them? Is it necessary to tax our police and fire fighters with additional calls to these densely thick apartments? The amount of sirens I have heard from my home over the last 2-4 years as the city added more apartments has been ridiculous, constant and never ending. People have to park on the Palouse Highway as there is not sufficient parking!

**STOP BUILDING APARTMENTS IN THIS AREA OF THE SOUTH HILL!! YOU ARE STRESSING OUR SYSTEMS!**

Laurie Nisbet

Sent from my Verizon, Samsung Galaxy smartphone





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                              |            |
|------------------------------|------------|
| <b><u>Date Rec'd</u></b>     | 10/26/2020 |
| <b><u>Clerk's File #</u></b> | ORD C35976 |
| <b><u>Renews #</u></b>       |            |

|                                  |                                                  |                             |             |
|----------------------------------|--------------------------------------------------|-----------------------------|-------------|
| <b><u>Submitting Dept</u></b>    | PLANNING                                         | <b><u>Cross Ref #</u></b>   |             |
| <b><u>Contact Name/Phone</u></b> | KEVIN FREIBOTT 625-6184                          | <b><u>Project #</u></b>     | Z19-504COMP |
| <b><u>Contact E-Mail</u></b>     | KFREIBOTT@SPOKANECITY.ORG                        | <b><u>Bid #</u></b>         |             |
| <b><u>Agenda Item Type</u></b>   | First Reading Ordinance                          | <b><u>Requisition #</u></b> |             |
| <b><u>Agenda Item Name</u></b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – 8TH AVENUE |                             |             |

**Agenda Wording**

An Ordinance relating to application Z19-504COMP by Sunset Health, LLC, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to Residential 15-30 for 2.2 acres and a change to the Zoning Map from RSF to RMF.

**Summary (Background)**

The proposal concerns 3004 W 8th Avenue (parcels 25234.0902 and .6501). This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended approval of the amendment.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |

|         |    |   |
|---------|----|---|
| Neutral | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |

|                                    |                   |                                     |                           |
|------------------------------------|-------------------|-------------------------------------|---------------------------|
| <b><u>Approvals</u></b>            |                   | <b><u>Council Notifications</u></b> |                           |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS     | <b><u>Study Session\Other</u></b>   | CC Study Session 10-29-20 |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M. | <b><u>Council Sponsor</u></b>       | CM Mumm                   |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY   | <b><u>Distribution List</u></b>     |                           |
| <b><u>Legal</u></b>                | RICHMAN, JAMES    | tblack@spokanecity.org              |                           |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL   | jrichman@spokanecity.org            |                           |
| <b><u>Additional Approvals</u></b> |                   | sbishop@spokanecity.org             |                           |
| <b><u>Purchasing</u></b>           |                   | dhume@spokane-landuse.com           |                           |
|                                    |                   | lmeuler@spokanecity.org             |                           |
|                                    |                   |                                     |                           |
|                                    |                   |                                     |                           |



Ordinance No. C35976

AN ORDINANCE RELATING TO APPLICATION FILE Z19-504COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "RESIDENTIAL 15-30" FOR APPROXIMATELY 2.2 ACRES LOCATED AT 3004 W 8<sup>TH</sup> AVENUE (PARCELS 25234.0902 AND 25234.6501) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "RESIDENTIAL MULTI-FAMILY (RMF)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-504COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-504COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 2.2 acres from "Residential 4-10" to "Residential 15-30"; if approved, the implementing zoning destination requested is "Residential Multi-Family (RMF)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-504COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-504COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-504COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend approval of Application Z19-504COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-504COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "Residential 15-30" for 2.2 acres, as shown in Exhibits A and B.
3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from "Residential Single Family" to "Residential Multi-Family (RMF)," as shown in Exhibits C and D.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

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CITY ADMINISTRATOR SIGNATURE:

---





# Z19-504COMP (3004 W 8th Ave)

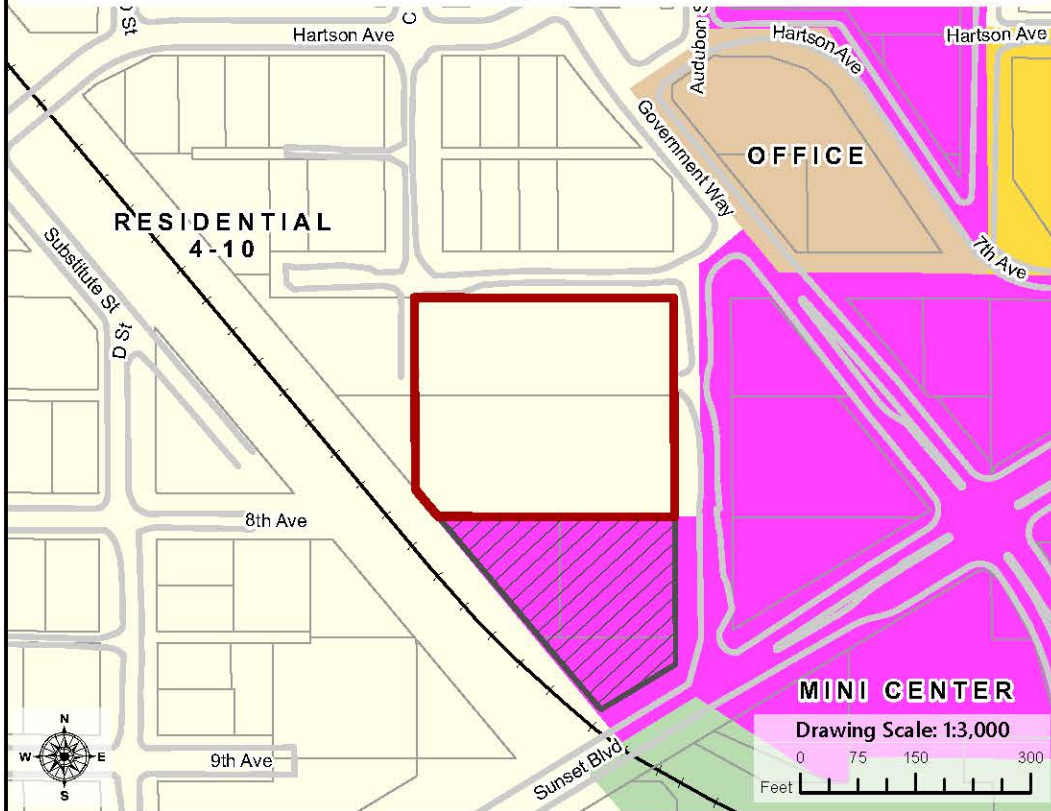
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

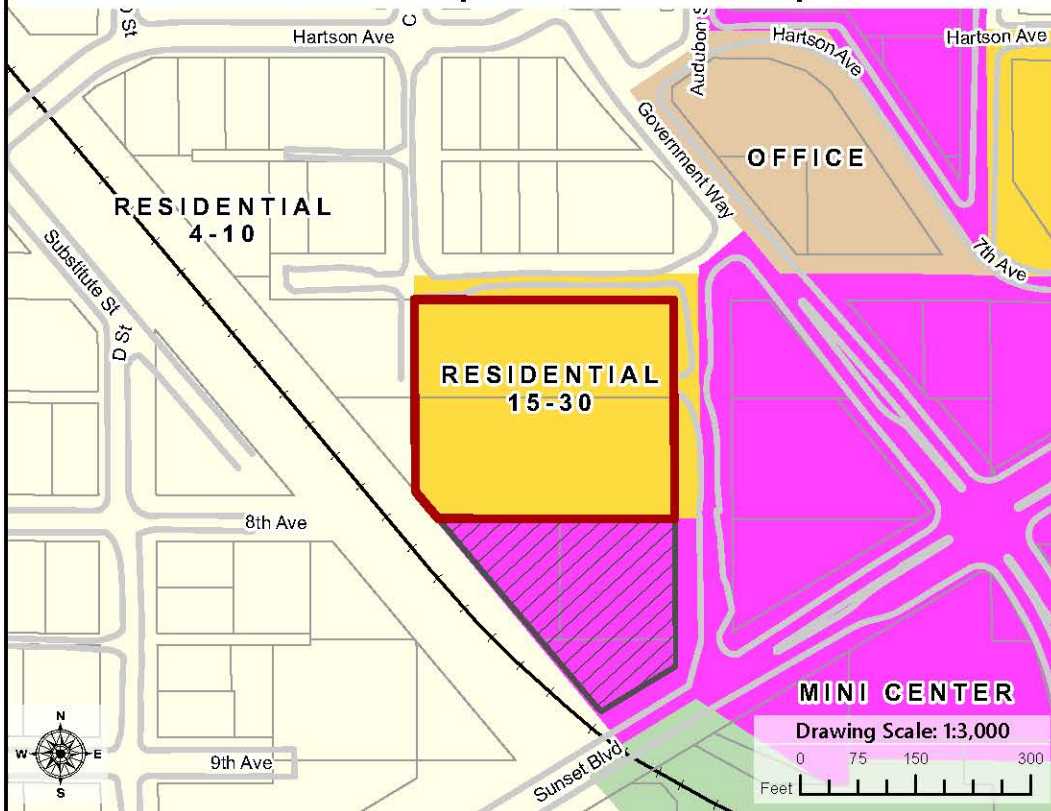
- Subject Parcels
- Same Owner
- Parcel
- Curb Line

## Land Use Plan Designation

- Conservation Open Space
- Residential 4-10
- Residential 15-30
- Office
- Mini Center

Acres (Proposal): 2.20

## EXHIBIT B: Proposed Land Use Plan Map

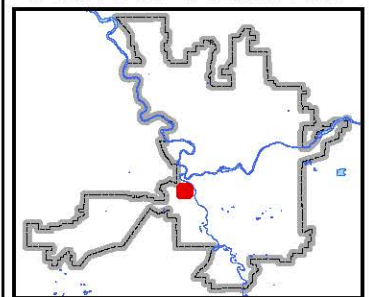


- Subject Parcels
- Same Owner
- Parcel
- Curb Line

## Proposed Land Use

- Conservation Open Space
- Residential 4-10
- Residential 15-30
- Office
- Mini Center

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# Z19-504COMP (3004 W 8th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

- Subject Parcels
- Same Owner
- Curb Line
- Parcel
- Railroad

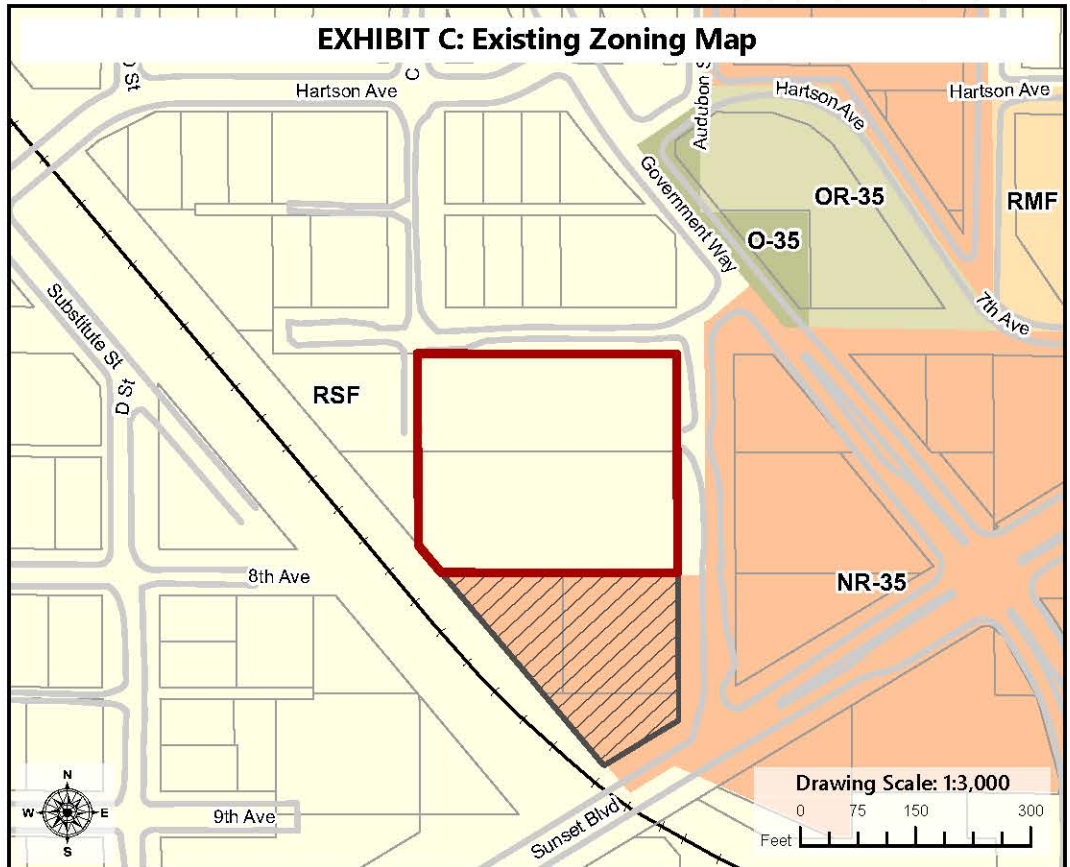
## Current Zoning

- Neighborhood Retail (NR)
- Office (O)
- Office Retail (OR)
- Residential Multifamily (RMF)
- Residential Single-Family (RSF)

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 2.20

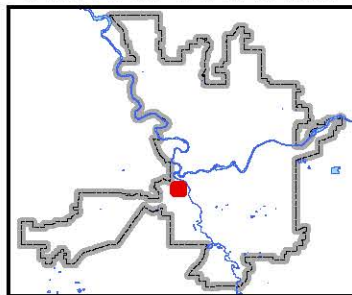
## EXHIBIT C: Existing Zoning Map



## Legend

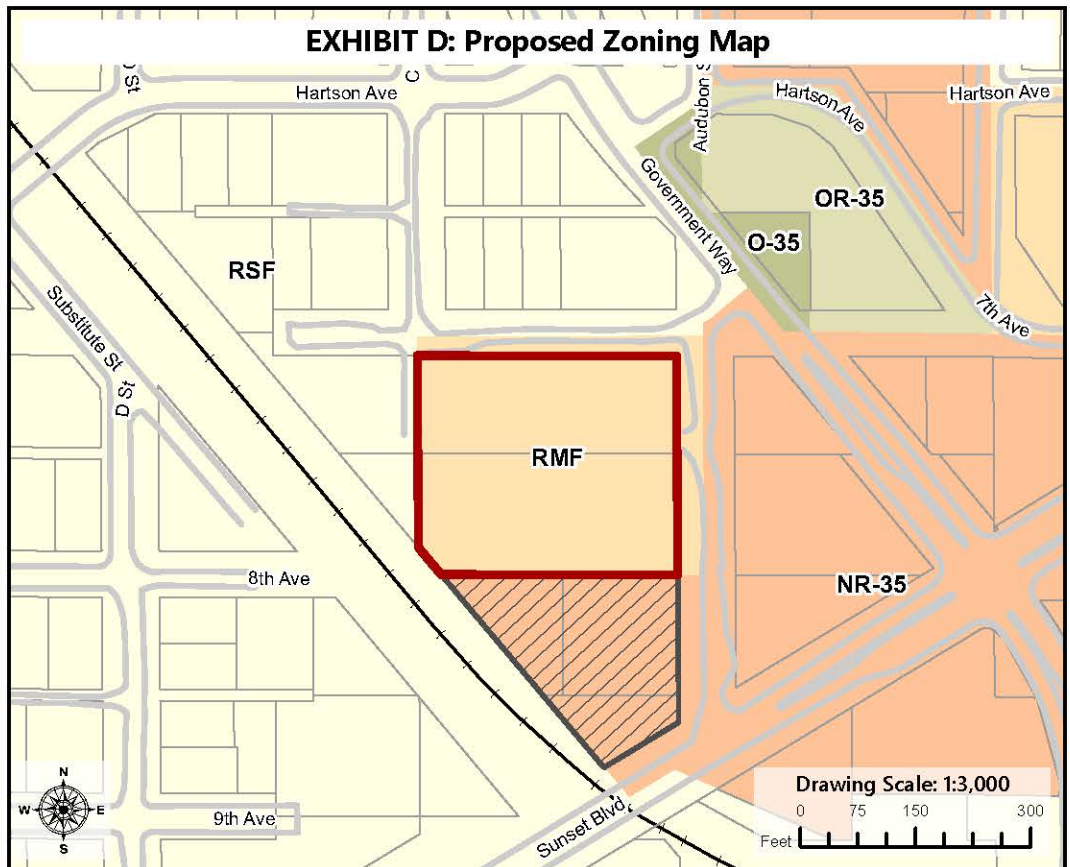
- Subject Parcels
  - Same Owner
  - Curb Line
  - Parcel
- ## Proposed Zoning
- Neighborhood Retail (NR)
  - Office (O)
  - Office Retail (OR)
  - Residential Multifamily (RMF)
  - Residential Single-Family (RSF)

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





**Exhibit E: Legal Description**

Lots 1 through 12 of Block 13, Terrace Park, Lewis & Shaw's Addition, in the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-504COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map designation and zoning of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                                |
|---------------------------|--------------------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 25234.0902 and 25234.6501                                                      |
| <b>Address(es):</b>       | 3004 W 8 <sup>th</sup> Avenue                                                  |
| <b>Property Size:</b>     | 2.2 acres                                                                      |
| <b>Legal Description:</b> | Lots 1 through 12 of Block 13, Terrace Park Lewis & Shaw's Addition            |
| <b>General Location:</b>  | Southwest of the intersection of W 7 <sup>th</sup> Street and S Audubon Street |
| <b>Current Use:</b>       | Three multi-family residences and open space.                                  |

**II. APPLICANT SUMMARY**

|                        |                                                 |
|------------------------|-------------------------------------------------|
| <b>Agent:</b>          | Dwight Hume, Land Use Solutions and Entitlement |
| <b>Applicant:</b>      | Sunset Health, LLC                              |
| <b>Property Owner:</b> | Sunset Health, LLC                              |

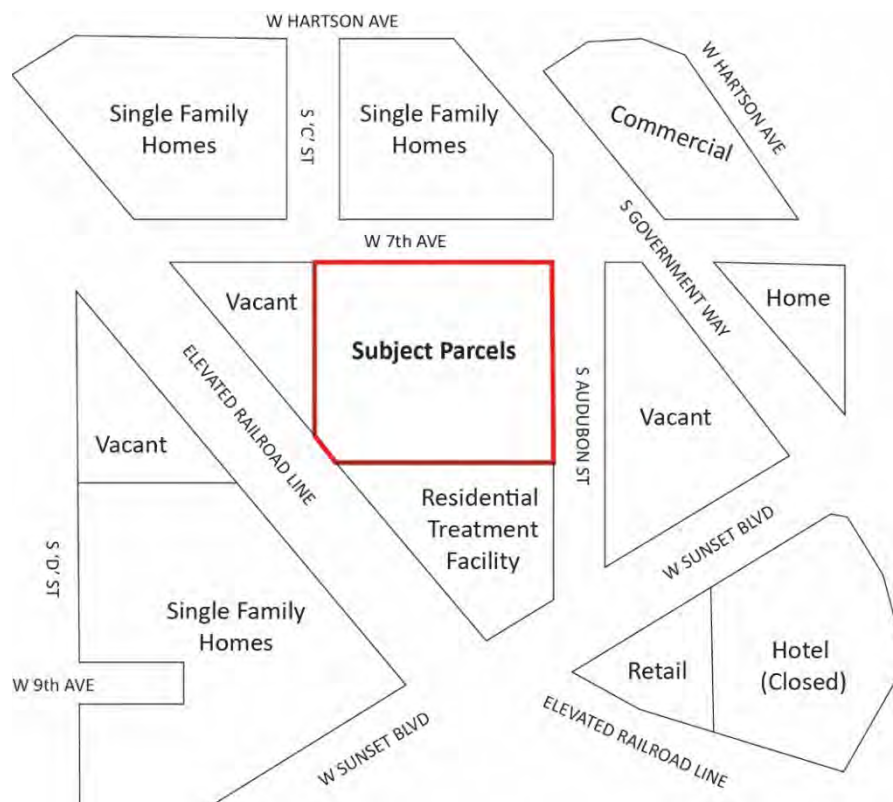
**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 DUs/Acre (R 4-10)                                                                                                          |
| <b>Proposed Land Use Designation:</b> | Residential 15-30 DUs/Acre (R 15-30)                                                                                                        |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | Residential Multi-Family (RMF)                                                                                                              |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | None                                                                                                                                        |



## IV. BACKGROUND INFORMATION

- 1. General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the West Hills neighborhood. The intent of the applicant is to redevelop the residential care facility immediately south of these parcels and to extend improvements to the subject parcels. However, no development plans have been submitted, nor are any permits or approvals for future development sought by the applicant at this time.
- 2. Site Description and Physical Conditions:** The subject parcels are largely vacant, save for three small multi-family buildings on the southern portion. The site previously contained a mobile home park under a previous owner, but those improvements were removed prior to the submission of this application. There are a number of large trees on or about the property and some remaining patches of paving.
- 3. Property Ownership:** Both parcels, as well as the three parcels located immediately south of the subject parcels, are owned by Sunset Health, LLC, a WA-registered Limited Liability Corporation.
- 4. Adjacent Property Improvements and Uses:** The subject parcels are surrounded by existing development of the following nature:



- 5. Street Class Designations:** Both of the adjacent streets, S Audubon St and W 7<sup>th</sup> Ave are designated as unimproved local streets in the Spokane arterial map. The Comprehensive Plan designates both for future paving/development as local access streets.



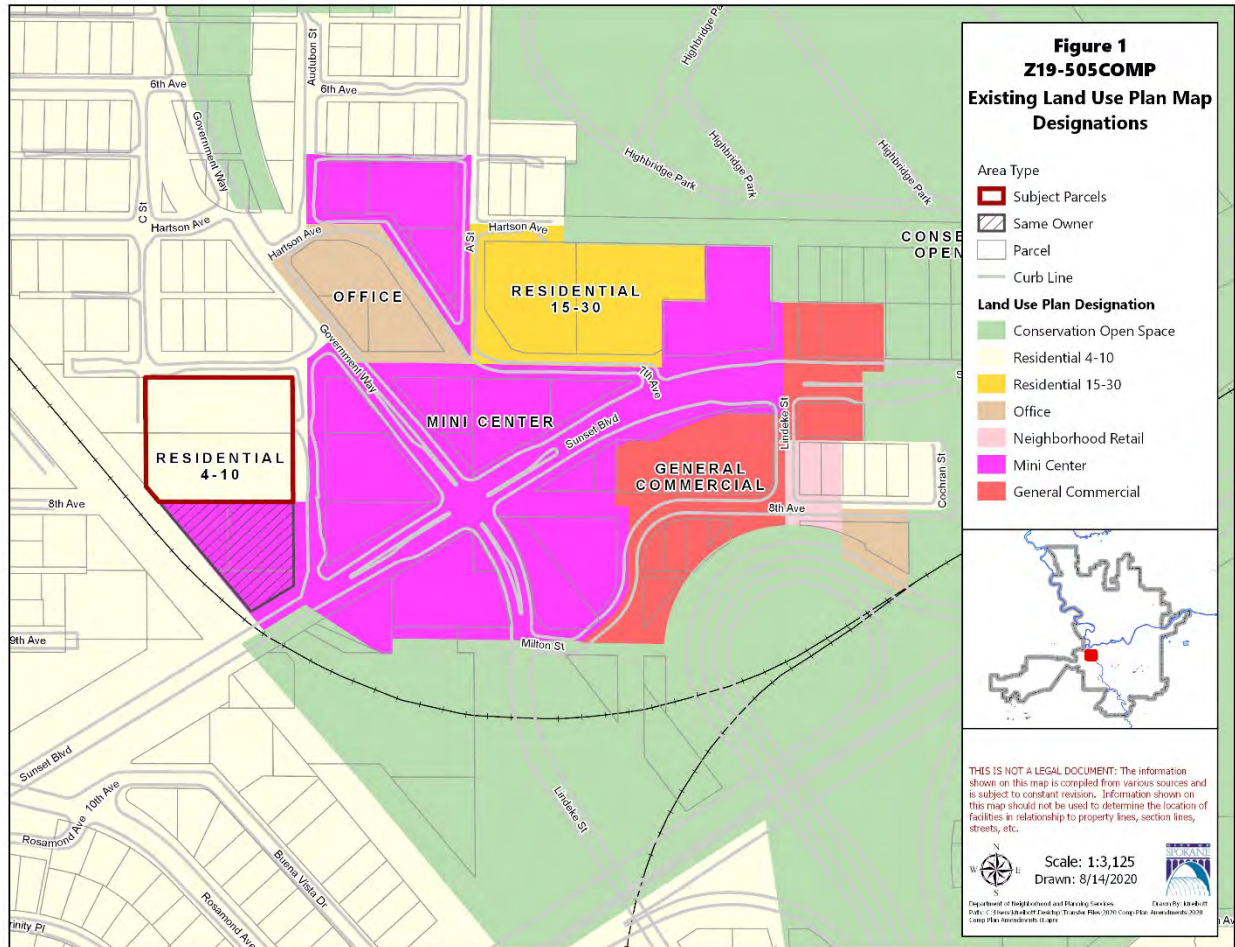
6. **Current Land Use Designation and History:** As shown in Exhibit A, the subject properties are currently designated for “Residential 4-10” (between 4 and 10 dwelling units per acre), a designation reserved for single-family homes. The subject properties have been designated for this use since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001.
7. **Proposed Land Use Designation:** As shown in Exhibit B, the proposal is to amend the Land Use Plan Map designation so that the entirety of the subject properties are designated for “Residential 15-30” use.
8. **Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF). The subject properties have changed zoning designation multiple times in the past. In 1958 the properties were zoned Class I Residential, reserved for single-family homes. By 1975 the properties were zoned B1: Local Business Zone for light intensity commercial uses. By 2006 the properties were zoned for a similarly light intensity commercial use, Neighborhood Retail.

In 2001 the City adopted the current iteration of the Comprehensive Plan. This version of the Comprehensive Plan, which is still substantially similar today, focused density and intensity of use into centers and corridors and limited it elsewhere in the City. In response to this new land use strategy, the City undertook a massive update to the zoning code and residential building standards in the City in order to ensure consistency with the new Comprehensive Plan. This action, completed in 2006, included the rezoning of numerous properties in the City, including the subject properties. In order to help focus density in centers and corridors, this effort included analysis and identification of parcels that contained different physical uses than those allowed in the previous zoning district. For instance, parcels like the subject parcels that were previously zoned for commercial uses but which were vacant or contained low-density residential uses, were identified and rezoned to match the existing use of the property. Accordingly, the subject properties were rezoned from Neighborhood Retail to Single Family Residential (SFR), as they contained a few mobile homes at that time (see Ordinance C33841). Conversely, the two properties to the south that are now in common ownership with the subject properties remained in the NR zoning district, as they contained a motel at that time and didn’t require rezone. Adoption of ordinance C33841 required significant work and included multiple workshops and outreach with the general public, the Plan Commission, and the City Council.

Of additional consideration for this application is the existence of a Neighborhood Mini Center immediately south and east of the subject parcels. This Mini Center, focused on the intersection of Sunset Blvd and Government Way, is surrounded by a complex mix of Land Use Plan Map designations and land improvements. Because this Mini Center has bearing on the policy ramifications of the proposed amendment, **Figure 1** has been provided on the next page showing the various Land Use Plan Map Designations surrounding the Mini Center.

9. **Proposed Zoning:** As shown in Exhibit D, the proposal seeks to amend the zoning so that the entirety of the subject properties are zoned Residential Multi-Family (RMF).





## V. APPLICATION PROCESS AND PUBLIC COMMENT

- Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |

<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



Plan Commission Workshop ..... July 8, 2020  
60-Day Public Comment Period Ended ..... August 7, 2020  
SEPA Determination Issued ..... August 24, 2020  
Notice of Public Hearing Posted ..... August 26, 2020  
Plan Commission Hearing Date (Scheduled) ..... September 9, 2020

- 2. Comments Received:** A request for comments was issued to City departments, local agencies, and neighborhood councils within 400 feet of the proposal, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- Randy Abrahamson, Spokane Tribe Historic Preservation Officer
- Karen Carlberg, Chair of the West Hills Neighborhood

Mr. Abrahamson expressed concerns that the project site may include cultural resources and asked that any future development conduct a cultural survey and sub-surface testing. Mrs. Carlberg provided some comments/corrections for the SEPA checklist and requested that in the future the City update its process/standards for Comprehensive Plan amendments to provide a greater level of information and coordinate with neighborhoods. Both comment letters are included in **Exhibit L** of this staff report.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. The following comments were received during the 60-day public comment period:

- Scott Kappes

Mr. Kappes, an adjacent property owner, provided several comments, including the need for paving of adjacent dirt roads, concerns with stormwater runoff, inclusion of open space/greenspace for the use of local birds and wildlife on the property, and the requirement that the project use a vegetated screen between the eventual improvements to the property and the adjacent home. It's of note that [SMC 17C.200](#) provides requirements for landscaping and screening between uses. Any future development of the site would be required to meet these standards before approval. However, the proposal does not currently include any plans or permits for the actual development/improvement of the property. Those requirements would be applied in the future, if and when the property owner decides to develop.

- 3. Public Workshop:** A public workshop with the Spokane Plan Commission was held on July 8, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was given an opportunity to speak but was unable to participate during the workshop.



## VI. APPLICATION REVIEW AND ANALYSIS

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:

- A. Keep the comprehensive plan alive and responsive to the community.
- B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
- C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
- D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
- E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
- F. Amendments to the comprehensive plan must result in a net benefit to the general public.

2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. **GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.



- C. Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency comment request or require a traffic impact analysis for the proposal. The subject property is already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

**E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.



*Neighborhood Planning Documents Adopted after 2001.* The West Hills neighborhood completed its initial neighborhood planning project in 2016. This planning effort was centered on the stretch of Fort George Wright Drive adjacent to the Spokane Falls Community College, far from the subject parcels, and would not affect or be affected by this proposal.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

See Item K.2 for below for analysis and results.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: See item K.2 below for analysis and results.

The consistency of the proposal with this criterion is unclear. See criterion K.2 below.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** No evidence has been provided by any adjacent jurisdiction, including the County of Spokane, indicating this proposal would conflict with the CWPP or the plans of any neighboring jurisdiction. The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed



transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

- I. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of an urban area already served by the public facilities and services described in CFU 2.1. The proposed change in land-use designations affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

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<sup>3</sup> State Environmental Policy Act



- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

K. **Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

The proposal meets this criterion.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.4, Higher Density Residential Uses, which directs "new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map." The subject parcels are located immediately adjacent to a designated Mini-Center on the Land Use Plan Map. Accordingly, it is also important to review the requirements of policy LU 1.7, Neighborhood Mini-Centers. According to Policy LU 1.7, mini centers could be considered to be "outside Centers and Corridors." However, LU 1.7 also states, "Mini-Center locations are encouraged to become small, mixed-use centers with higher density residential use as a major component." The current mini-center at Government Way and Sunset Blvd already includes Neighborhood Retail, Community Business, and some Residential Multi-Family zoning, all of which would allow higher density residential uses. However, this neighborhood mini-center has not seen any significant retail or commercial development since the adoption of the 2006 Comprehensive Plan.

While Policy LU 1.4 provides for some opportunity to establish new higher density residential uses *outside* centers, it generally limits such additions to areas where the predominant development character is already multi-family in nature. As the subject properties are substantially surrounded by vacant land, single-family



residential, and only limited multi-family residential, this proposal is potentially inconsistent with this policy. It is unclear if this proposal meets the policy intention of Policy LU 1.7, Neighborhood Mini Centers and Policy LU 1.4, Higher Density Residential.

This proposal's consistency with the requirements of LU 1.4 and LU 1.7 is unclear, as described above.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude physical development of office uses on the site. The site is adequately served by all utilities and by two major arterial streets, bus service is nearby, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: As discussed in item a. above, the proposal may implement the desire for greater residential density in the vicinity of mini centers.

The proposal's consistency with this criterion is unclear.

- 3. **Rezones, Land Use Plan Amendment:** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property will change concurrently from RSF (Residential Single-Family) to RMF (Residential Multi-Family).

The proposal meets this criterion.

## VII. CONCLUSION

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal's consistency with criteria for a comprehensive plan amendment as provided in SMC 17G.020.030 is unclear.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.



## **VIII. STAFF RECOMMENDATION**

Because the consistency of this proposal with a policy of the Comprehensive plan is unclear, staff does not have a recommendation regarding this application.

## **IX. LIST OF EXHIBITS**

- |                                  |                                           |
|----------------------------------|-------------------------------------------|
| A. Existing Land Use Plan Map    | H. List of Relevant Comp Plan Policies    |
| B. Proposed Land Use Plan Map    | I. Application Materials                  |
| C. Existing Zoning Map           | J. SEPA Checklist                         |
| D. Proposed Zoning Map           | K. SEPA Determination of Non-Significance |
| E. Application Notification Area | L. Agency Comments                        |
| F. Detail Aerial                 | M. Public Comments                        |
| G. Wide-Area Aerial              |                                           |





# Z19-504COMP (3004 W 8th Ave)

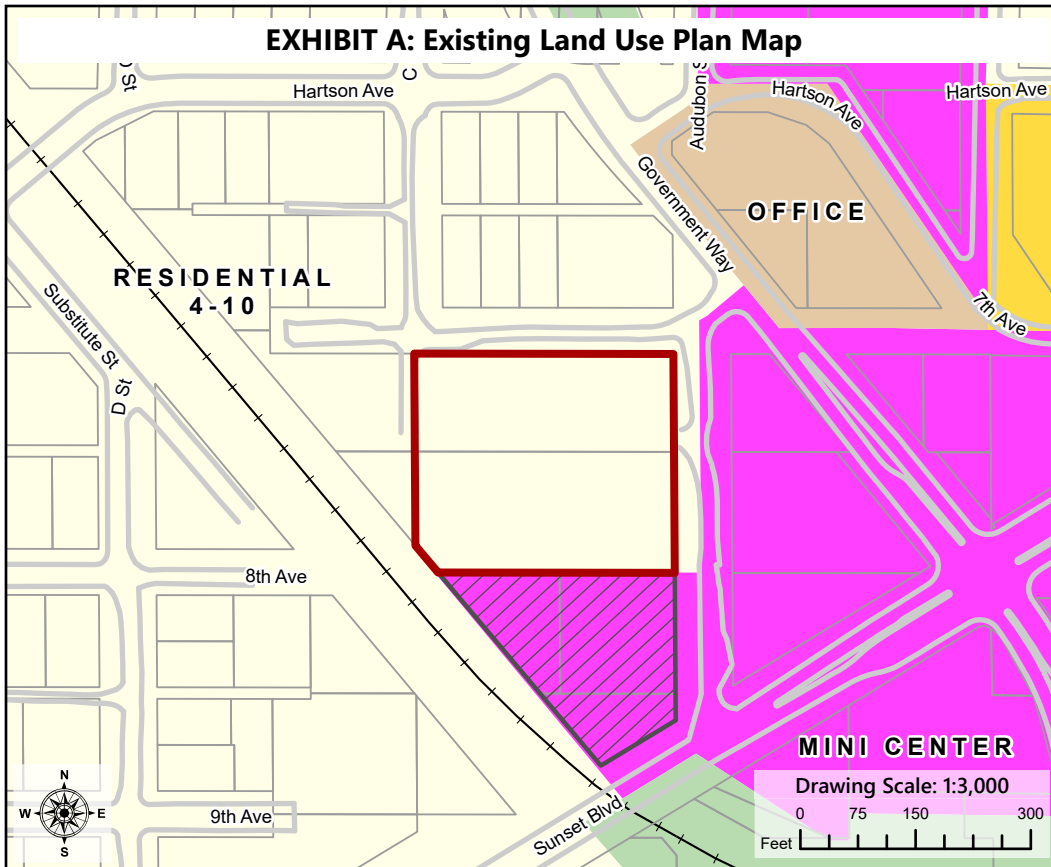
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

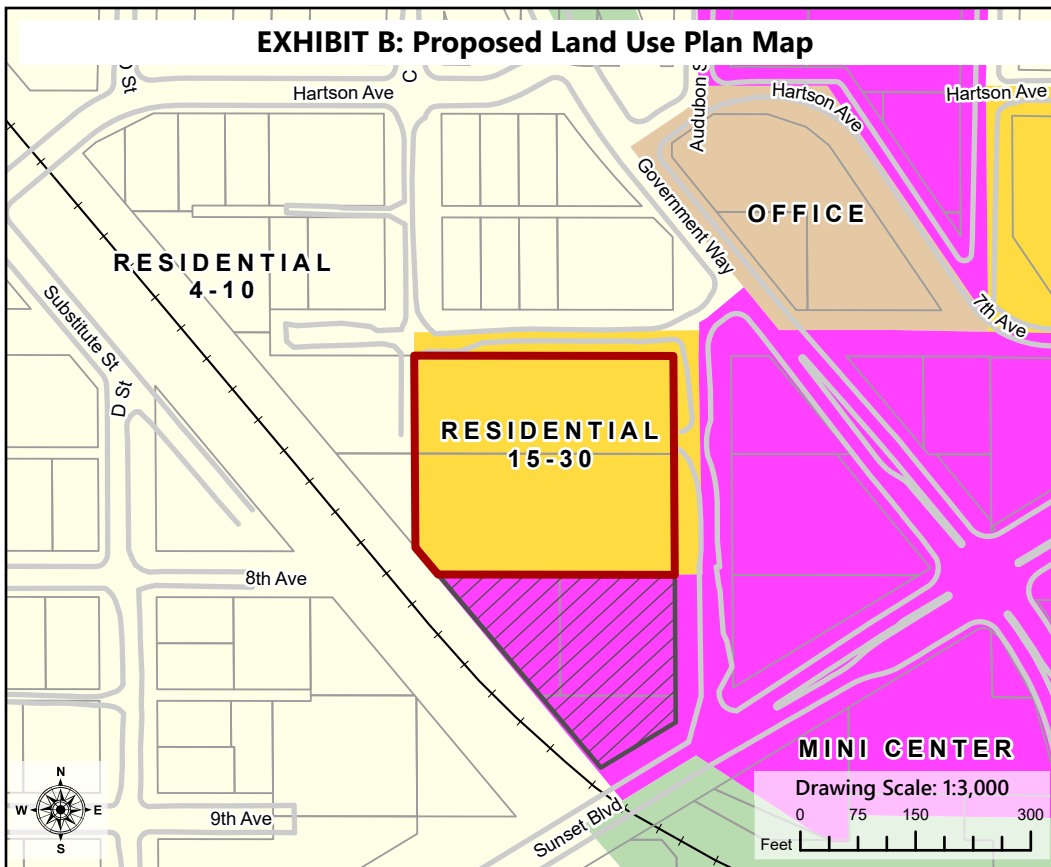
- Subject Parcels
- Same Owner
- Parcel
- Curb Line

## Land Use Plan Designation

- Conservation Open Space
- Residential 4-10
- Residential 15-30
- Office
- Mini Center

Acres (Proposal): 2.20

## EXHIBIT B: Proposed Land Use Plan Map

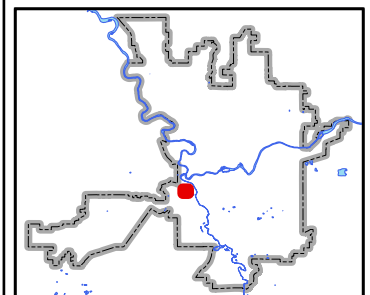


- Subject Parcels
- Same Owner
- Parcel
- Curb Line

## Proposed Land Use

- Conservation Open Space
- Residential 4-10
- Residential 15-30
- Office
- Mini Center

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-504COMP (3004 W 8th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

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## Legend

- Subject Parcels
- Same Owner
- Curb Line
- Parcel
- Railroad

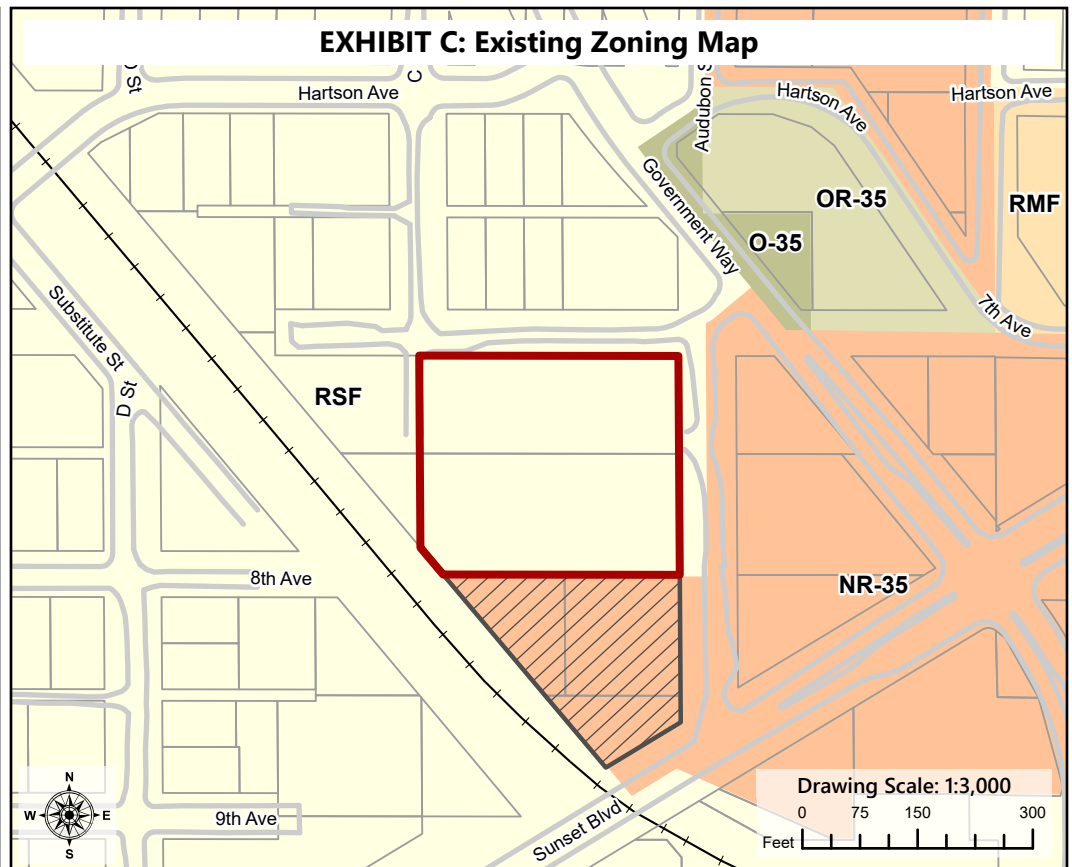
## Current Zoning

- Neighborhood Retail (NR)
- Office (O)
- Office Retail (OR)
- Residential Multifamily (RMF)
- Residential Single-Family (RSF)

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 2.20

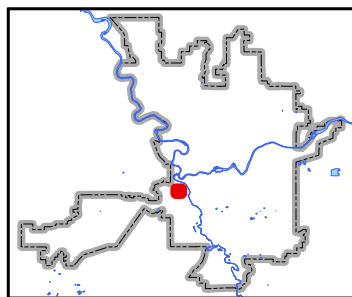
## EXHIBIT C: Existing Zoning Map



## Legend

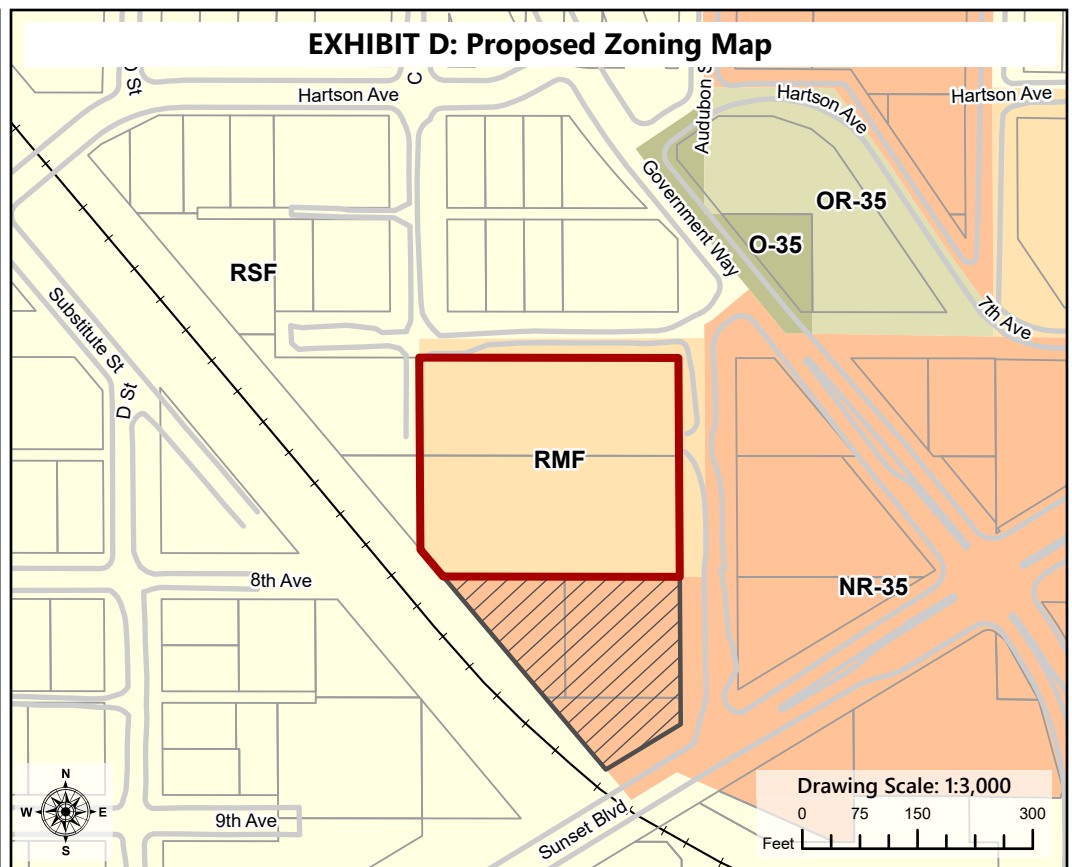
- Subject Parcels
  - Same Owner
  - Curb Line
  - Parcel
- ## Proposed Zoning
- Neighborhood Retail (NR)
  - Office (O)
  - Office Retail (OR)
  - Residential Multifamily (RMF)
  - Residential Single-Family (RSF)

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map



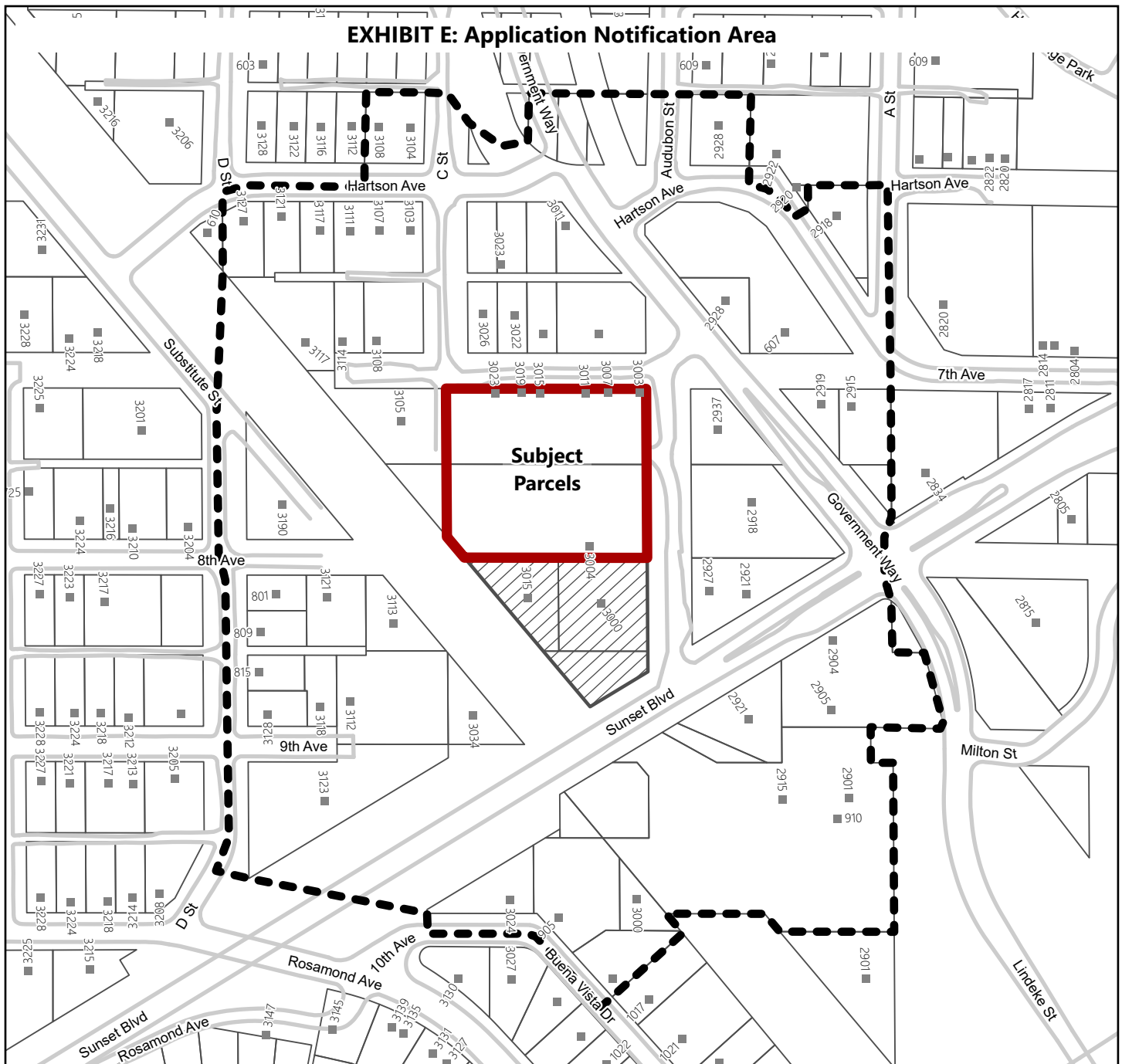




# Z19-504COMP (3004 W 8th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

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## Legend

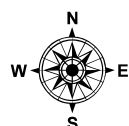
- Parcel
- Notification Boundary
- Curb Line
- Address Point

## Subject Parcels

- Subject Parcels
- Same Owner

## Application proposes to:

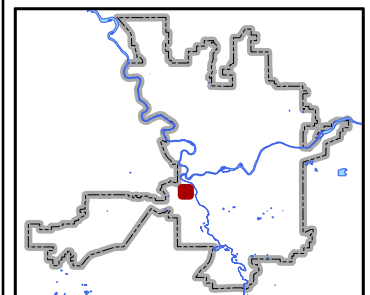
Change Land Use Designation from Residential 4-10 to Residential 15-30.



Project Size: 2.20 Acres (Approximate)  
Drawing Date: 12/3/2019 Drawing Scale: 1:3,000

0 75 150 300  
Feet

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-504COMP (3004 W 8th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

THIS IS NOT A LEGAL DOCUMENT

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## EXHIBIT F: Detail Aerial



## Legend

Subject Parcels

Adjacent Ownership

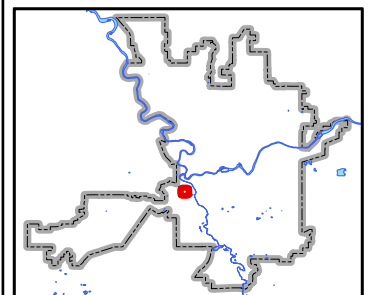
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 2.20

## EXHIBIT G: Wide-Area Aerial



## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-504COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.4 Higher Density Residential Uses

Direct new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* Higher density housing of various types is the critical component of a center. Without substantially increasing population in a center's immediate vicinity, there is insufficient market demand for goods and services at a level to sustain neighborhood-scale businesses. Higher density residential uses in Centers range from multi-story condominiums and apartments in the middle to small-lot homes at the edge. Other possible housing types include townhouses, garden apartments, and housing over retail space.

To ensure that the market for higher density residential use is directed to Centers, future higher density housing generally is limited in other areas. The infill of Residential 15+ and Residential 15-30 residential designations located outside Centers are confined to the boundaries of existing multi-family residential designations where the existing use of land is predominantly higher density residential.

### LU 1.7 Neighborhood Mini-Centers

Create a Neighborhood Mini-Center wherever an existing Neighborhood Retail area is larger than two acres.

*Discussion:* The Neighborhood Mini-Center designation recognizes the existence of small neighborhood-serving businesses in locations that are two to five acres in size that lie outside Centers and Corridors designated on the Land Use Plan Map. However, some designated Neighborhood Mini-Centers are over five acres in size because they are based on pre-existing zoning



designations. Similar to Neighborhood Retail, the Neighborhood Mini-Center designation consists of small, freestanding businesses usually sited at the intersection of or along arterial streets. Another characteristic of this designation is the greatly restricted potential for redevelopment of the surrounding area to support a full Neighborhood Center. Consequently, the Mini-Center designation limits mixed-use development to the boundaries of the existing Mini-Center designation.

Mini-Center locations are encouraged to become small, mixed-use centers with higher density residential use as a major component. Residential use adds market demand for neighborhood business and enables enhanced transit service to these locations. Shared-use parking arrangements are encouraged to increase the development intensity of the site for both residential and commercial uses.

This designation allows the same uses as the Neighborhood Retail designation. No new drive-through facilities, including gas stations and similar auto-oriented uses, should be allowed except along principal arterial streets where they should be subject to size limitations and design guidelines. Buildings should be oriented to the street to encourage walking by providing easy pedestrian connections. Parking lots should not dominate the frontage and should be located behind or on the side of buildings.

New Mini-Center locations may be established through a neighborhood planning process. They should be separated by at least one-mile from other neighborhood serving business areas and should not exceed five acres in size. To provide convenient accessibility from the surrounding neighborhood, new Mini-Centers should be located at the intersection of arterial streets.

#### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

### **Chapter 6—Housing**

#### H 1.4 Use of Existing Infrastructure

Direct new residential development into areas where community and human public services and facilities are available.

*Discussion:* Using existing services and infrastructure often reduces the cost of creating new housing. New construction that takes advantage of existing services and infrastructure conserves public resources that can then be redirected to other needs such as adding amenities to these projects

#### H 1.11 Access to Transportation

Encourage housing that provides easy access to public transit and other efficient modes of transportation.

*Discussion:* Transportation is the second largest expenditure after housing and can range from 10 to 25 percent of household expenditures. Examining where housing is City of Spokane Comprehensive Plan 6-8 located and the associated transportation costs may provide a more realistic evaluation of housing affordability in the future.



#### H 2.4 Linking Housing with Other Uses

Ensure that plans provide increased physical connection between housing, employment, transportation, recreation, daily-needs services, and educational uses.

*Discussion:* The location of housing in relation to other land uses is a part of what determines the quality of housing. The desirability and viability of housing changes for different segments of the community, based on an area's mix of land uses. As complementary land uses become spread further apart, transportation options decrease while transportation costs increase. These added transportation costs reduce the amount of household income available for housing and other household needs. This affects lower-income households first. In urban areas, basic services, such as grocery stores, public transportation, and public parks, should be available within a mile walk of all housing.

### **Chapter 11—Neighborhoods**

#### N 8.4 Consistency of Plans

Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-23-19

Kevin Freibott, Planner II  
Planning & Development Services  
3<sup>rd</sup> Floor City Hall  
West 801 Spokane Falls Blvd.  
Spokane WA 99201

Ref: Sunset Health LLC Map Amendment Request


Dear Kevin:

I am encouraged to report that the former Ventura Land Holdings application is finally on solid ground with the transfer of ownership to Empire Health Foundation and its subsidiary Sunset Health LLC, the new applicant for the amendment previously sought by Ventura Land Holdings and others. More importantly, I have learned that the former services of Ascenda have been retained, as has the long-range goal of improving the housing accommodations for existing residents of the Ascenda program, currently housed in the existing motel.

Submitted herewith is the required application forms, a map of the subject amendment, the check in the amount of \$500.00 and a copy of the minutes from the April 2019 West Hills meeting in which Ascenda attended the meeting to explain the future plans for this project, once approved.

I am confident that this application will be pursued to its completion now that the other idiosyncrasies have passed.

Respectfully Submitted



Dwight J Hume

Copy w/enclosures:

Sunset Health LLC c/o Jeff Bell, Interim President, Empire Health Foundation





**DESCRIPTION OF PROPOSAL:**

Amend Land Use Plan Map from R 4-10 to R 15-30 and zone from RSF to RMF

**ADDRESS OF SITE OF PROPOSAL:** (if not assigned yet, obtain address from Public Works before submitting application)  
3004 W 8<sup>th</sup> Avenue

**APPLICANT:**

**Name:** Sunset Health LLC, C/O Jeff Bell  
**Address:** 1020 W Riverside Ave. Spokane WA 99201  
**Phone (home):** **Phone (work):**  
**Email address:** jeff@empirehealthfoundation.org

**PROPERTY OWNER:**

**Name:** Same  
**Address:**  
**Phone (home):** **Phone (work):**  
**Email address:**

**AGENT:**

**Name:** Land Use Solutions & Entitlement C/O Dwight Hume  
**Address:** 9101 N Mt. View Lane, Spokane WA 99218  
**Phone (home):** **Phone (work):** 509-435-3108  
**Email address:** [dhume@spokane-landuse.com](mailto:dhume@spokane-landuse.com)

**ASSESSOR'S PARCEL NUMBERS:**

25234.6501 25234.0902

**LEGAL DESCRIPTION OF SITE:**

Lots 1-12, Block 13, Terrace Park Lewis and Shaws and the E 1/2 of vacated "C" street.

**SIZE OF PROPERTY:**

2.20 acres

**LIST SPECIFIC PERMITS REQUESTED IN THIS APPLICATION:**

Land Use Map Amendment and Zone Change from R 4-10 to R-15-30 and from RSF to RMF zone



**SUBMITTED BY:**

Dwight Hume  
☐ Applicant   ☐ Property Owner   ☐ Property Purchaser   ☒ Agent

In the case of discretionary permits (administrative, hearing examiner, landmarks commission or plan commission), if the applicant is not the property owner, the owner must provide the following acknowledgement:

I, Jeff Bell, managing partner of Sunset Health LLC, owners of the above-described property do hereby authorize Dwight J Hume to represent us and our interests in all matters regarding this application.

**ACKNOWLEDGMENT:**

STATE OF WASHINGTON     )  
                                          ) ss.  
COUNTY OF SPOKANE     )

On this 21<sup>st</sup> day of OCTOBER, 2019, before me, the undersigned, a Notary Public in and for the State of Washington, duly commissioned and sworn, personally appeared JEFF BELL, to me known to be the individual that executed the foregoing instrument and acknowledged the said instrument to be free and his/her free and voluntary act and deed, for the uses and purposes therein mentioned.

Witness my hand and official seal hereto affixed the day and year first above written.



Kelly Knutson  
Notary Public in and for the State of Washington,  
residing at SPOKANE, WA 99208





# Comprehensive Plan Amendments

## Threshold Review

### Sunset Health LLC Map Amendment R 6-10 to R 15-30

#### **Pre-application:**

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

#### **Description of the Proposed Amendment:**

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.  
25234.1201, 1206 and 1207. 2.2 acres bounded by 7<sup>th</sup> Avenue, Govt. Way and vacated 8<sup>th</sup> Avenue, 3004 W 8<sup>th</sup> Avenue. (See attached map).

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*The Comprehensive Plan provides guidance on the proper location of higher density residential. The subject property is located adjacent to an existing NR-35 zone which implements a mini-neighborhood center. Within the discussion of a Neighborhood Mini-Center, it mentions that higher density residential is encouraged in those areas. This is an area characterized by several apartment developments within the immediate vicinity as well as office uses.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*This is an area contemplated for future sub-area planning. However, this request deals with an urgent need for housing to accommodate an increasing demand for a special needs segment of our community currently being housed within the adjacent former motel. The approval of this request would not prejudice the outcome of future land use planning.*



3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.  
*The use of the property is for upgraded housing for residents of the existing motel complex. The tenants are recovering and/or recovered addicts and their families. This is a long-standing and successful program known as Ascenda. Ascenda has reached out to both the immediate neighbors and the West Hills Neighborhood Council and resolved many questions and concerns. We therefore do not expect much opposition, if any. Thus, the proposed amendment can be reviewed within the regular and normal time frame.*
4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?  
*As stated above, the management of Ascenda has reached out to neighboring residence but not for the purpose of considering inclusion of their property within this request.*
5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.  
*The proposal lies adjacent to an existing Mini-Center. It provides the increased housing mentioned within the policy discussions concerning Neighborhood Mini-Centers at page 3-39 where it states that higher density residential use is encouraged in these areas. LU 1.7 discusses states that residential use adds market demand for businesses and enables enhanced transit service to these areas.*  
  
*Since the proposed increase in residential density located next to an existing mini-center, the request is consistent with the current comprehensive plan and therefore is consistent with Countywide Planning Policies, the GMA and other applicable state and federal regulations.*
6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.  
*This is the third annual cycle within which this request has been made. In the previous two request the City Council and the Docketing Committee approved this request to go forward. Unfortunately, there were unforeseen circumstances that pre-empted the ability to go forward. In the interim, the property has been sold to Empire Health Foundation under the name of Sunset Health LLC and Ascenda is being retained as the manager and program operating on site. We therefore, fully expect the long range plans for resident housing to be implemented upon approval of this request.*
7. If this change is directed by state law or a decision of a court or administrative agency, please describe. N/A



8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application. *See attached minutes from April 2019. We have requested to be on their December agenda to give an update.*



## Project Narrative Summary

### Sunset Health LLC Map Amendment

Sunset Health LLC has requested to amend the comprehensive plan map from R 4-10 to R 15-30 and a corresponding zone change from RSF to RMF. The property is 2.2 acres located north and west of the S. Gov't Way and Sunset Highway intersection. The applicant owner also controls the former motel site located south of this proposed request, zoned Neighborhood Retail on an additional 1.1 acres.

The purpose of this change would allow up to 66 apartment units to be constructed adjacent to this motel site and densify the subject land adjacent to the motel and vacant land currently zoned Neighborhood Retail. If this request is approved, then the motel and vacant land would be converted to new apartment units upon the relocation of existing tenants to the subject property to enable demolition of existing motel structures and replacement with new housing thereon.

The subject site has two duplex units and one residential single-family dwelling that would be removed to accommodate the proposed apartment project. It is anticipated that construction would begin in the spring of 2021 upon approval of this request in the fall of 2020.



WEST HILLS NEIGHBORHOOD COUNCIL  
MINUTES FOR THE MEETING OF APRIL 9, 2019

ATTENDANCE

Neighbors: Walter Bible, George Bressler, Sherry Bressler, Johnna Calvert, Karen Carlberg (Chair), Rick Clapp (Vice-Chair), Kimberly Craigen, Brian Houle, Jessica Jackson, Sev Jones, Karen Jurasin (Communications Director), Mark Kieffer, Stephanie Klein, Gloria Kohn, Dave Marr, Susan Mensching, Julieann Morse, Lyle Morse, Rodney Redmond, Lisa Saddler (Ascenda Executive Director), Wes Sawyer, Brandon Swafford, Annetta Theademan, LaVerne Truman, Paula Truna, Nancy Westbrook

City staff: none

Guests: Jayne Kubasak (Ascenda Board of Directors), Kylie Nagle (Spokane Parks and Recreation), Tim Ottmar (Spokane Police), Barry Saddler (Ascenda Board of Directors), Steven Wilson (Finch Arboretum Community Garden)

MINUTES

Minutes were approved for the meeting of February 12, 2019.

NEIGHBORHOOD POLICE REPORT, Tim Ottmar, Spokane Police

Tim was asked to investigate whether Ascenda had been a source of crime since its founding in 2004. He said that he has been our neighborhood police officer for 8-9 yr and never has responded to Ascenda, or been aware of crime coming from Ascenda residents. He asked some of his fellow officers, and they were not aware of any crime at Ascenda.

He presented some crime statistics for his district, P7, the Southwest District. P7 includes areas of the city that generally are south of the Spokane River and west of Division, Grand, and Perry, but not including the downtown/Riverside area. This includes the southern 2/3 of the West Hills neighborhood, all of the Grandview-Thorpe, Latah/Hangman, Browne's Addition, and Peaceful Valley neighborhoods, and parts of Cliff/Cannon and Comstock. A map of crimes in P7 from March 31 to April 6, 2019, shows several various types of crime around the Motel 6 (near the intersection of Sunset Boulevard and S Rustle St), and nothing else in West Hills, Grandview-Thorpe, Latah/Hangman, Browne's Addition, or Peaceful Valley. A table of crimes in P7 shows numbers of violent crimes and property crimes for the last 7 days, 28 days, and year to date, without showing specifically where they were within the district. All of these are reported crimes, not arrests. These will be posted on Nextdoor.

Tim addressed homelessness in our neighborhood. If you see a camp, call 311. Current laws limit what the police can do. The priority is to direct homeless people to services where they can get help. If you have questions or concerns, email Tim at [tottmar@spokanepolice.org](mailto:tottmar@spokanepolice.org).

Tim was asked about mailbox theft. Apparently very few of these crimes have been reported to Spokane Police. It is more effective to report to Spokane Police (call Crime Check at 456-2233 or COPS Southwest at 755-2677) than to the post office. Cameras are the most effective tool (Ring and Wyze were recommended). Photos can be forwarded to Spokane Police, and if the perpetrator is familiar to them, they can act.

COMMUNITY ASSEMBLY REPORT, Paul Bundy, West Hills CA Representative

Paul was absent.

SUMMER PROGRAMS FROM SPOKANE PARKS AND RECREATION, Kylie Nagle, Spokane Parks and Recreation



Kylie told us about two free activities that will be available this summer. First, mobile rec vans will come to four city parks from June 17 to August 22, from 10:00 a.m. to 1:00 p.m. They will be at Grant Park on Mondays, Friendship Park on Tuesdays, A.M. Cannon Park on Wednesdays, and Cannon Hill Park on Thursdays. The vans will have a variety of sports and recreation equipment for children to use. All children and parents are welcome. Second, swimming is free for all children and adults at all city outdoor pools, which will be open the same dates as the mobile rec vans. Everyone needs a Splash Pass, which can be obtained for free at SpokaneRec.org, any city aquatic center, or by calling 311.

#### TRAFFIC CALMING FOR GOVERNMENT WAY, Susan Mensching, West Hills

Susan submitted a traffic calming proposal for Government Way last month. She requested several improvements to get drivers to slow down and to make it easier to cross the road. These include speed signs and crosswalks. She also requested two West Hills gateway signs. We will find out in a few months whether our request is funded.

#### NEIGHBORHOOD SIGN

Spokane Engineering Services proposed several variations of the neighborhood sign that we sent to them several months ago for installation on Sunset Boulevard. The sign will be in a landscaped area in the median of Sunset Boulevard somewhere around Assembly Street to Royal Street, visible to eastbound drivers.

We looked at five variations of the sign. Votes were 3, 16, 0, 0, 0. Our choice has a dark brown background, white lettering, and green and white trees. We will insist that the sun be changed from green to white. The winning sign will be posted on Nextdoor. (Note: Someone asked about resistance to graffiti. Karen C asked Rich Proszek, our city engineering contact, and he said that the sign will be made from wood, with a graffiti resistant coating.)

#### FINCH ARBORETUM COMMUNITY GARDEN, Nancy Westbrook and Steven Wilson

Nancy and Steve described the Community Garden at Finch Arboretum and invited anyone to participate. The garden is about 100 yards south of Woodland Center. It started in 2017 and has grown each year. This year there will be 14 plots assigned to individuals or families, as well as a community plot from which anyone can take produce. Leftover produce is donated to a worthy cause; last year about 500 pounds were donated. If you would like to apply for a plot for 2019, go to <https://www.facebook.com/groups/396273954067253>. The application fee is \$25.

#### ASCENDA, Lisa Saddler, Executive Director

Lisa told us about the history of AscendA, what is happening there these days, and what the future might hold.

History: The AscendA property previously belonged to John Coghlan. George Nossek bought the property from him in 2004. (George Nossek purchased the property and financed it with John Coghlan.) Initially George called the facility Christmas House, and used the property to house homeless people, but soon thereafter he transitioned it to a facility for sober living. Lisa joined the Board of Directors in 2006, then complained a lot about rules not being followed and poor supervision. New rules were implemented, and the name was changed to AscendA. Lisa was named Executive Director in about 2008. Also in 2008, John foreclosed on the property and took back ownership. He allowed AscendA to stay because he believed in its mission. John died recently, and now the property is owned by his heirs.

AscendA in 2019: There are 43 residents, including children. Several residents came to our meeting. Adults come to AscendA because they have been addicted, they are committed to



recovery and sober living, and they want a home that is safe and supportive for maintaining sobriety. Some people have lived there for several years. The residents become family for one another. Absolute sobriety and rigorous honesty are required. Urinalysis tests are done regularly, and if someone refuses, or fails, they must move out of the property immediately. Residents sign a contract when they move there, promising to follow the rules. Guests follow the same rules. Sex offenders are not allowed, partly because children live there. The children go to Spokane public schools. About 85% of the adults have full-time jobs. Everyone pays for rent and utilities. Rent funds the operating expenses for AscendA. There is a curfew, which is relaxed for people whose work schedule requires them to come and go at odd hours. A few residents told us about their successes in achieving sobriety, earning degrees, keeping stable jobs, earning awards at their jobs, and getting their children back. Empire Health Foundation, a Spokane philanthropic organization, provides much of the funding for AscendA.

Lisa and the residents want to be part of our neighborhood. Returning to a normal social life, and to community involvement, is part of the recovery process. All of us are welcome to attend their weekly meetings, Mondays at 5:00 p.m. They have a barbecue every summer, to which they invite the police, the nearby neighbors, and other community members; they will send us an invitation this summer. Karen J will send all AscendA residents an invitation to join our Nextdoor.

Future: The goal is to purchase the property from the Coghlan heirs. Empire Health Foundation will partner with AscendA for the purchase and development of the property. The purchase has been complicated by changing demands from the heirs. This is the reason that the zoning change request was withdrawn in both 2018 and 2019 (Lisa was not involved in the zoning change requests). Once the property is owned by AscendA, the plan is to build four 8-plexes, with four residences on each of two floors. These should look like any normal neighborhood. More 8-plexes could be built in the future. Old buildings will be removed. (AscendA wants to offer the Department of Corrections office space to make it easier for the parole officers as well as the residents on parole.) AscendA also would like to buy the property at the northwest corner of Sunset Boulevard and Government Way, which is owned by a different owner, and currently is for sale.

See [ascendaspokane.com](http://ascendaspokane.com).

8:03 - ADJOURN







## Environmental Checklist

File No.19-504COMP

### Purpose of Checklist:

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

### Instructions for Applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### Use of checklist for nonproject proposals:

Complete this checklist for nonproject proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



## A. BACKGROUND

1. Name of proposed project, if applicable: **Sunset Health Map Amendment**
2. Name of applicant: **Land Use Solutions & Entitlement**
3. Address and phone number of applicant or contact person: **Dwight Hume  
9101 N Mt View Lane Spokane WA 99218 509-435-318**
4. Date checklist prepared: **March 29 2020**
5. Agency requesting checklist: **City of Spokane**
6. Proposed timing or schedule (including phasing, if applicable): **Immediate upon approval**
7.
  - a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. **The project may be phased with approximately 60 units within the subject amendment area.**
  - b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. **Yes, the applicant owns the 1.3 acre motel site immediately south of the subject property that contains the motel and is zoned NR-35.**
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to his proposal. **Unknown**
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. **No other applications are pending**
10. List any government approvals or permits that will be needed for your proposal, if known. **Map amendment, zone change and development permits.**
11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. **The project encompasses 2.2 acres and the proposed amendment would generate up to 66 residential units.**

**Reviewer's Note:**

This checklist has been submitted as part of a Comprehensive Plan Amendment. No construction has been proposed or designed for this project, nor is any such construction required or a condition of approval for the Amendment. The City recommends that readers disregard any specific construction details herein, such as the removal of existing structures or the construction of new housing in the future.



12. Location of the proposal. Give sufficient information to a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist. **The property consist of one platted block bound by 7<sup>th</sup> and 8<sup>th</sup>, S Gov't Way and vacated "C: street. In the vicinity of Sunset Highway and S Gov't Way.**
13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.)  
**Yes; Sewer Service Area: Yes; City of Spokane**
14. The following questions supplement Part A.
- a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)
- (1) Describe any systems, other than those designed for the disposal of sanitary waste, installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities).  
**Non-Project Action, to be determined at time of development.**
- (2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?  
**Non-Project Action, to be determined at time of development.**
- (3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems.  
**Non-Project Action, to be determined at time of development.**
- (4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater?  
**Non-Project Action, to be determined at time of development.**
- b. Stormwater
- (1) What are the depths on the site to groundwater and to bedrock (if known)?  
**Non-Project Action, to be determined at time of development.**



- (2) Will stormwater be discharged into the ground? If so, describe any potential impacts?

**Non-Project Action, to be determined at time of development.**

**TO BE COMPLETED BY APPLICANT**

## **B. ENVIRONMENTAL ELEMENTS**

### **1. Earth**

- a. General description of the site (circle one): *flat, rolling, hilly, steep slopes, mountains, other:*
- b. What is the steepest slope on the site (approximate percent slope)? **N/A**
- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. **Non-Project Action, to be determined at time of development.**
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. **Non-Project Action, to be determined at time of development.**
- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill: **Non-Project Action, to be determined at time of development.**
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. **Non-Project Action, to be determined at time of development.**
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? **Non-Project Action, to be determined at time of development.**
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: **Non-Project Action, to be determined at time of development.**

### **2. Air**

- a. What type of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial, wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known. **Non-Project Action, to be determined at time of development.**



- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. **Yes, overhead railroad trestle adjacent the subject property.**
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:  
**Non-Project Action, to be determined at time of development.**

### 3. Water

a. SURFACE:

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.  
**Latah Creek is located east of the subject property approximately 1/2 mile.**
- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.  
**No**
- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.  
**None**
- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.  
**No, the site is served with City of Spokane water service**
- (5) Does the proposal lie within a 100-year floodplain? \_\_\_\_ If so, note location on the site plan.  
**No**
- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.  
**No**

b. GROUND:

- (1) Will groundwater be withdrawn, or will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.  
**No**
- (2) Describe waste material that will be discharged into the ground from septic tanks or other sanitary waste treatment facility. Describe the general size of the system, the number of houses to be served (if



applicable) or the number of persons the system(s) are expected to serve.

**None as the site is served by public sewer** \_\_\_\_\_

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

**Run-off would be limited to stormwater and discharged into existing city storm drains.** \_\_\_\_\_

- (2) Could waste materials enter ground or surface waters? If so, generally describe.

**No** \_\_\_\_\_

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water impacts, if any.

**None** \_\_\_\_\_

4. Plants

a. Check or circle type of vegetation found on the site:

\_\_\_\_ **X** \_\_\_\_ Deciduous tree: *alder, maple, aspen, other.*

\_\_\_\_ **X** \_\_\_\_ Evergreen tree: *fir, cedar, pine, other.*

\_\_\_\_ **X** \_\_\_\_ Shrubs

\_\_\_\_ **X** \_\_\_\_ Grass

\_\_\_\_\_ Pasture

\_\_\_\_\_ Crop or grain

\_\_\_\_\_ Wet soil plants, *cattail, buttercup, bullrush, skunk cabbage, other.*

\_\_\_\_\_ Water plants: *water lilly, eelgrass, milfoil, other.*

\_\_\_\_\_ Other types of vegetation.

b. What kind and amount of vegetation will be removed or altered? **Non-Project Action, to be determined at time of development.** \_\_\_\_\_

c. List threatened or endangered species known to be on or near the site.  
**None** \_\_\_\_\_

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: **None** \_\_\_\_\_

5. Animals

a. Circle any birds and animals which have been observed on or near the site are known to be on or near the site:

birds: ***hawk, heron, eagle, songbirds, other: Urban fowl*** \_\_\_\_\_

mammals: *deer, bear, elk, beaver, other:* \_\_\_\_\_



fish: *bass, salmon, trout, herring, shellfish, other:* \_\_\_\_\_  
other: \_\_\_\_\_

- b. List any threatened or endangered species known to be on or near the site.

**Unknown** \_\_\_\_\_

- c. Is the site part of a migration route? If so, explain. \_\_\_\_\_

**No**

- d. Proposed measures to preserve or enhance wildlife, if any: \_\_\_\_\_

**None** \_\_\_\_\_

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. **Non-Project Action, to be determined at time of development.**

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. \_\_\_\_\_

**Unknown**

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

**None**

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. **No** \_\_\_\_\_

- (1) Describe special emergency services that might be required.

**None**

- (2) Proposed measures to reduce or control environmental health hazards, if any:

**None**

- b. NOISE:

- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

**Train traffic from adjacent railroad overpass** \_\_\_\_\_

- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.



**Non-Project Action, to be determined at time of development.**

- (3) Proposed measure to reduce or control noise impacts, if any:  
**None** \_\_\_\_\_

**8. Land and shoreline use**

- a. What is the current use of the site and adjacent properties? **On site: 5 DU's and vacant land; North, single family and vacant lots; East: Apartment ground, retail, South: Former motel and vacant.** \_\_\_\_\_
- b. Has the site been used for agriculture? If so, describe. \_\_\_\_\_  
**No** \_\_\_\_\_
- c. Describe any structures on the site. **2 duplex buildings and one single family** \_\_\_\_\_
- d. Will any structures be demolished? If so, which? **Yes, all will be eliminated**
- e. What is the current zoning classification of the site? **RSF** \_\_\_\_\_
- f. What is the current comprehensive plan designation of the site? **Residential 4-10** \_\_\_\_\_
- g. If applicable, what is the current shoreline master program designation of the site?  
**N/a** \_\_\_\_\_
- h. Has any part of the site been classified as a critical area? If so, specify. **No**
- i. Approximately how many people would reside or work in the completed project?

**Non-Project Action, to be determined at time of development.**

- j. Approximately how many people would the completed project displace?  
**None, the apartments would be built before the existing units are removed.** \_\_\_\_\_
- k. Proposed measures to avoid or reduce displacement impacts, if any: **Build new units before removing the existing.**
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: **Non-Project Action, to be determined at time of development.** \_\_\_\_\_

**9. Housing**

- a. Approximately how many units would be provided, if any? Indicate whether high, middle or low-income housing. **66 apartment units** \_\_\_\_\_



- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. **5 units in two duplexes and one single family unit. Low income units.** \_\_\_\_\_
- c. Proposed measures to reduce or control housing impacts, if any: **Build new before demo of old.** \_\_\_\_\_

#### 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? **Non-Project Action, to be determined at time of development.** \_\_\_\_\_
- b. What views in the immediate vicinity would be altered or obstructed? **Non-Project Action, to be determined at time of development.** \_\_\_\_\_
- c. Proposed measures to reduce or control aesthetic impacts, if any: **None** \_\_\_\_\_

#### 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? **Non-Project Action, to be determined at time of development.** \_\_\_\_\_
- b. Could light or glare from the finished project be a safety hazard or interfere with views? **No** \_\_\_\_\_
- c. What existing off-site sources of light or glare may affect your proposal? **None** \_\_\_\_\_
- d. Proposed measures to reduce or control light and glare impacts, if any: **None** \_\_\_\_\_

#### 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? **Centennial Trail is SE of the subject property across Sunset Highway** \_\_\_\_\_
- b. Would the proposed project displace any existing recreational uses? If so, describe. **No** \_\_\_\_\_
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: **None** \_\_\_\_\_

#### 13. Historic and cultural preservation



- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. **No** \_\_\_\_\_
- b. Generally describe any landmarks or evidence of historic archaeological, scientific or cultural importance known to be on or next to the site.  
**None** \_\_\_\_\_
- c. Proposed measures to reduce or control impacts, if any: \_\_\_\_\_  
**None** \_\_\_\_\_

#### 14. Transportation

- a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any. **S Gov't Way and 7<sup>th</sup> Avenue** \_\_\_\_\_
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop? **No** \_\_\_\_\_
- c. How many parking spaces would the completed project have? How many would the project eliminate? **Non-Project Action, to be determined at time of development.** \_\_\_\_\_
- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets not including driveways? If so, generally describe (indicate whether public or private). **Non-Project Action, to be determined at time of development.** \_\_\_\_\_
- e. Will the project use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. **Yes, trains use an adjacent track. However there are no stops or other features that would be affected or used by the project.** \_\_\_\_\_
- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak would occur. **Non-Project Action, to be determined at time of development.** \_\_\_\_\_

*(Note: to assist in review and if known indicate vehicle trips during PM peak, AM Peak and Weekday (24 hours).)*

- g. Proposed measures to reduce or control transportation impacts, if any: **None**

#### 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe. **Yes, due to increased housing of the site**
- b. Proposed measures to reduce or control direct impacts on public services, if any: **Full compliance with applicable building and fire codes.**



**16. Utilities**

- a. Circle utilities currently available at the site: *septic* **electricity, natural gas, water, refuse service, telephone, sanitary sewer,** *system, other:*
- b. Describe the utilities that are proposed for the project, the utility providing the service and the general construction activities on the site or in the immediate vicinity which might be needed. **Non-Project Action, to be determined at time of development.**



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 3-29-20 \_\_\_\_\_ Signature: Dwight J Hume \_\_\_\_\_

***Please Print or Type:***

Proponent: Dwight J Hume \_\_\_\_\_ Address: 9101 N Mt. View Lane

Phone: 509.435.3108 \_\_\_\_\_ Spokane WA, 99218

Person completing form (if different from proponent): Same \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_  
\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott \_\_\_\_\_

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS**  
***(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage or release of toxic or hazardous substances; or production of noise?

**The proposal is to build apartments for residential use in compliance  
With all applicable development standards.**

Proposed measures to avoid or reduce such increases are:

**Same as above**

2. How would the proposal be likely to affect plants, animals, fish or marine life?

**Future development of the site would be urban in nature, similar to the uses  
adjoining the subject parcels. Eventual redevelopment of the site may require  
the removal of on-site plants, subject to the requirements of the Spokane  
Municipal Code for new construction.**

Proposed measures to protect or conserve plants, animals, fish or marine life are:

**None**

3. How would the proposal be likely to deplete energy or natural resources?

**While some additional resources would be required during redevelopment,  
these would be similar to those required of any construction project.**

Proposed measures to protect or conserve energy and natural resources are:

**None**

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands?

**There are no such areas on site**

Proposed measures to protect such resources or to avoid or reduce impacts are:

**None**

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

**If redeveloped in the future the site could change from 12 platted residential  
lots to 66 apartment units. The immediate area is a mixture of apartments,  
retail and office uses. The subject site was once a mobile home park.**



Proposed measures to avoid or reduce shoreline and land use impacts are:  
**Development per applicable development standards. Non-Project Action, to be determined at time of development.**

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

**Non-Project Action, to be determined at time of development.**

Proposed measures to reduce or respond to such demand(s) are:

**Non-Project Action, to be determined at time of development.**

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment.

**The proposal would not conflict with any applicable state or federal laws or regulations.**



C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 3-29-20 \_\_\_\_\_

Signature: *Dwight J Hume* \_\_\_\_\_

***Please Print or Type:***

Proponent: Dwight J Hume \_\_\_\_\_ Address: 9101 N Mt. View Lane

Phone: 509.435.3108 \_\_\_\_\_ Spokane WA, 99218

Person completing form (if different from proponent): Same \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott \_\_\_\_\_

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z19-504COMP

**PROPONENT:** Sunset Health, LLC (Agent: Dwight Hume, Land Use Solutions and Entitlement).

**DESCRIPTION OF PROPOSAL:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the land use designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for two properties located in the West Hills neighborhood.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns two parcels, 25234.0902 and 25234.6501, located immediately southwest of the intersection of W 7<sup>th</sup> Avenue and S Audubon Street. The parcels are located at 3004 W 8<sup>th</sup> Avenue. The proposal would affect an area of approximately 2.2 acres.

**Legal Description:** Lots 1 through 12 of Block 13, Terrace Park Lewis & Shaw's Addition to the City of Spokane in Section 23, Township 25 North, Range 42 East.

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

☐ There is no comment period for this DNS.

☐ This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.


☒ This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:** 

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*





**Spokane Tribe of Indians**  
Tribal Historic Preservation Office  
P.O. Box 100 Wellpinit WA 99040

May 5, 2020

**TO: Kevin Freibott, Assistant Planner**

**RE: Z19-504COMP**

Mr. Freibott,

Thank you for contacting the Tribe's Historic Preservation Office. We appreciate the opportunity to provide a cultural consent for your project.

We have reviewed your project forwarded to our office; we are concerned that the project area potentially contains cultural resources which would be impacted by the proposed ground disturbing activity, and is a high-risk area for archeological sites and human remains.

**Recommendation:** Cultural Survey, Sub-surface testing.

Once the survey / sub-surface testing is completed we will do more mitigation to discuss the plan of action if cultural sites are identified during the cultural survey.

However, if any artifacts or human remains are found upon excavation activity this office is to be notified and the immediate area cease

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist us in protecting our shared heritage.

If questions arise, please contact me at 258-4222

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer



**From:** [Karen Carlberg](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Mike Gifford](#); [Paul Bundy](#); [Karen Jurasin](#); [Rick Clapp](#)  
**Subject:** West Hills comments on Z19-504COMP 8th Ave  
**Date:** Sunday, May 10, 2020 7:50:15 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Kevin,

I have the following comments on the Comprehensive Plan Land Use Map Amendment Proposal for 3004 W 8<sup>th</sup> Avenue:

12.a: It is the Fish Lake Trail that is immediately SE of the property, not the Centennial Trail.

14.a: Accessing the property from W 7<sup>th</sup> Avenue would be appropriate. If there is direct access from the property to Government Way, this could create traffic problems because the intersection would be so close to the major intersection of Sunset Boulevard and Government Way.

14.b: There are STA stops on Sunset Boulevard just east of Government Way. A neighbor who used these bus stops when he was still working told us that some of the other users were Ascenda residents.

Keep in mind that I'm still figuring out how this process works. While this application includes more details than the one that Mr. Hume submitted 1+ yr ago, it still has fewer details that I think it should. If the purpose of this exercise is, in part, a thoughtful review by neighboring residents, then a lot more details are required for a meaningful review. Our neighborhood council has met with the Ascenda Executive Director several times, and with the Empire Health Foundation Interim President once. We now know quite a lot about plans for the property and have no remaining questions. But if we had not had these meetings, and were relying solely on this application, we still would not have sufficient information, and we still would have the same questions, concerns, controversy, and anger that were created when the application was submitted to us 1+ yr ago. I believe that the City of Spokane should seriously consider examining their requirements for these applications. The current requirements do not serve the intended purpose of sending the applications to neighborhoods, especially if a project has the potential for controversy or significant impact on neighboring residents.

Karen A. Carlberg  
Chair, West Hills Neighborhood Council



**From:** [Scott Kappes](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Public Comment -Z19-504COMP, 8th Ave land use change  
**Date:** Tuesday, August 4, 2020 9:37:43 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

City of Spokane & Kenvin Freibott,

I am providing comment as an adjacent land owner regarding file no. Z19-504COMP, 8th Ave land use change. I am supportive of higher density housing, however I have several issues that I feel need addressed before any land use changes. I purchased and developed my property based upon the current land use designations of the neighborhood.

The first relates to the road situation. 7th Ave and C St need to be paved and all runoff addressed. The current proposal states "run-off would be limited to stormwater and discharged into existing city storm drains." This would be an issue with less surface absorption of water with higher density development and place my existing dwelling at risk of water damage. Run-off over 7th Ave needs controlled not for only this, but an additional safety reason: runoff from the existing property washes gravel down C St and creates a liability for the city as the gravel accumulates in piles in the paved intersection of Hartson and Government Way. The only solution is paving and developing 7th Ave adjacent to the property to C St and C St between 7th and Hartson.

Paving of 7th and C St would also be necessary for increased traffic. Currently the city won't take any action to level out potholes from city garbage & recycle trucks' weekly use and a detour while repairing the Sunset Highway and Government Way intersection, along with School bus use. This needs addressed before additional traffic or services use 7th Ave and the city, with additional revenue from increased density, needs to pave and take over all maintenance of 7th Ave or will be causing harm to property values and safety of my neighbors and me. This would also include deterioration of air quality from dust if not paved.

I would like two other issues that are more personal of nature addressed before any land use changes are granted. One is that there are deer and quail populations the live in the neighborhood and some sort of green space for at least passage would be great. The other is I would like any development to require a natural screen of trees and shrubs to provide privacy from the increased density.

If the development is done right, with road improvements of paving and run-off concerns addressed, natural screening, and green space passage addressed, I think the property and proposal would be a benefit for Spokane.

Thank you for your consideration in these concerns, and please confirm you've received this public comment.

Sincerely,

Scott Kappes  
3022 W. 7th Ave.  
Spokane, Wa 99223





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-504COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “Residential 15-30” for 2.2 acres located at 3002 W 8<sup>th</sup> Avenue. The implementing zoning designation requested is RMF (Residential Multi-Family).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-504COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a 2.2-acre area located at 3004 W 8<sup>th</sup> Avenue (the “Properties”) from “Residential 4-10” to “Residential 15-30” with a corresponding change in zoning from Residential Single Family (RSF) to Residential Multi-Family (RMF).
- E. The two parcels at 3004 W 8<sup>th</sup> Avenue are held in common ownership by the applicant, as well as the three parcels immediately south of the Properties, resulting in common ownership of the entire area between W 7<sup>th</sup> Avenue in the north and W Sunset Boulevard in the south.
- F. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- G. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- H. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, including the Application in the Work Program.
- I. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. Comments were received noting certain facts for the SEPA checklist and requesting a site-specific survey for cultural resources prior to any future development.
- J. The City issued a Notice of Application on June 8, 2020 by posting it in the Spokesman Review and Official Gazette and by mail to all properties and owners within a 400-foot radius of the subject Properties and any adjacent properties with the same ownership. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City



received comments regarding requested wastewater and street improvements adjacent to the Properties.

- K. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and the Community Assembly was provided with information regarding the dates of Plan Commission workshops and hearings.
- L. On July 8, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- M. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- N. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance (DNS) were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
- O. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- P. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the Properties and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject property.
- Q. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis did not provide a recommendation to the Plan Commission regarding the proposal.
- R. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of the SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- S. Following issuance of the public hearing notice, the City received an additional comment letter in support of the proposal.
- T. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- U. No members of the public testified during the hearing on September 9, 2020.
- V. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.



- W. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the “Staff Report”).
- X. The Plan Commission finds that the proposal meets the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.4 concerning the establishment of higher density residential land uses in the City and Land Use LU 1.7 concerning Neighborhood Mini-Centers, citing the need for additional multi-family uses in the vicinity of the mini center.
- Y. The Plan Commission finds that the proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-504COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review application was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Application is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City’s relevant six-year capital improvement plans or through enforcement of the City’s development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Application is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Application.
9. The Application will not adversely affect the City’s ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.



10. The Application proposes a land use designation that is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment and site is suitable for the proposed designation.
12. The map amendment would implement applicable comprehensive plan policies better than the current map designation.

**RECOMMENDATIONS:**

In the matter of Z19-504COMP, a request by Dwight Hume of Land Use Solutions and Entitlement on behalf of Sunset Health, LLC to change the land use plan designation on 2.2 acres of land from “Residential 4-10” to “Residential 15-30” with a corresponding change of the implementing zoning to RMF (Residential Multi-Family), as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to the Land Use Plan Map of the City’s Comprehensive Plan with corresponding amendment to the City’s Zoning Map, and authorizes the President to prepare and sign on the Commission’s behalf a written decision setting forth the Commission’s findings, conclusions, and recommendation on the Application.

---

**Todd Beyreuther, President**

Spokane Plan Commission

October \_\_, 2020





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                                |                                                   |
|--------------------------------|---------------------------------------------------|
| <b><u>Date Rec'd</u></b>       | 10/26/2020                                        |
| <b><u>Clerk's File #</u></b>   | ORD C35977                                        |
| <b><u>Renews #</u></b>         |                                                   |
| <b><u>Cross Ref #</u></b>      |                                                   |
| <b><u>Project #</u></b>        | Z19-505COMP                                       |
| <b><u>Bid #</u></b>            |                                                   |
| <b><u>Requisition #</u></b>    |                                                   |
| <b><u>Agenda Item Name</u></b> | 0650 – COMPREHENSIVE PLAN AMENDMENT – 10TH AVENUE |

**Agenda Wording**

An Ordinance relating to application Z19-505COMP by Lark Homes, LLC, amending the Comprehensive Plan Land Use Plan Map from Residential 4-10 to Residential 15-30 for 0.16 acres and a change to the Zoning Map from RSF to RMF.

**Summary (Background)**

The proposal concerns 1117 W 10th Avenue (parcel 35193.1405). This Application is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. The application has fulfilled public participation and notification requirements. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended denial of the amendment.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |

|         |    |   |
|---------|----|---|
| Neutral | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |
| Select  | \$ | # |

**Approvals**

|                                 |                   |
|---------------------------------|-------------------|
| <b><u>Dept Head</u></b>         | MEULER, LOUIS     |
| <b><u>Division Director</u></b> | SIMMONS, SCOTT M. |
| <b><u>Finance</u></b>           | ORLOB, KIMBERLY   |
| <b><u>Legal</u></b>             | RICHMAN, JAMES    |
| <b><u>For the Mayor</u></b>     | ORMSBY, MICHAEL   |

**Council Notifications**

|                                   |                           |
|-----------------------------------|---------------------------|
| <b><u>Study Session\Other</u></b> | CC Study Session 10-29-20 |
| <b><u>Council Sponsor</u></b>     | CM Mumm                   |

**Distribution List**

|                           |
|---------------------------|
| tblack@spokanecity.org    |
| jrichman@spokanecity.org  |
| sbishop@spokanecity.org   |
| dhume@spokane-landuse.com |
| lmeuler@spokanecity.org   |

**Additional Approvals**

**Purchasing**



Ordinance No. C35977

AN ORDINANCE RELATING TO APPLICATION FILE Z19-505COMP AMENDING MAP LU 1, LAND USE PLAN MAP, OF THE CITY'S COMPREHENSIVE PLAN FROM "RESIDENTIAL 4-10" TO "RESIDENTIAL 15-30" FOR APPROXIMATELY 0.16 ACRES LOCATED AT 1117 W 10<sup>TH</sup> AVENUE (PARCEL 35193.1405) AND AMENDING THE ZONING MAP FROM "RESIDENTIAL SINGLE FAMILY (RSF)" TO "RESIDENTIAL MULTI-FAMILY (RMF)".

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, land use amendment application Z19-505COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Application Z19-505COMP seeks to amend the Land Use Plan Map of the City's Comprehensive Plan for 0.16 acres from "Residential 4-10" to "Residential 15-30"; if approved, the implementing zoning destination requested is "Residential Multi-Family (RMF)"; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Application Z19-505COMP reviewed all the criteria relevant to consideration of the application was published on August 25, 2020 and sent to all applicants and the Plan Commission; and



WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the application was published on August 26, 2020 and September 2, 2020; and

WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted on the property and mailed to all property owners, occupants, and taxpayers of record, as shown in the most recent Spokane County Assessor's record for all properties within 400 linear feet of any portion of the boundary of the subject properties, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Application Z19-505COMP is inconsistent with and does not implement the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Application Z19-505COMP does not meet the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 7 to 0 (with one abstention) to recommend denial of Application Z19-505COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Application. Application Z19-505COMP is approved.
2. Amendment of the Land Use Map. The Spokane Comprehensive Plan Map LU 1, Land Use Plan Map, is amended from "Residential 4-10" to "Residential 15-30" for 0.16 acres, as shown in Exhibits A and B.
3. Amendment of the Zoning Map. The City of Spokane Zoning Map is amendment from "Residential Single Family" to "Residential Multi-Family (RMF)," as shown in Exhibits C and D.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





# Z19-505COMP (1117 W 10th Ave)

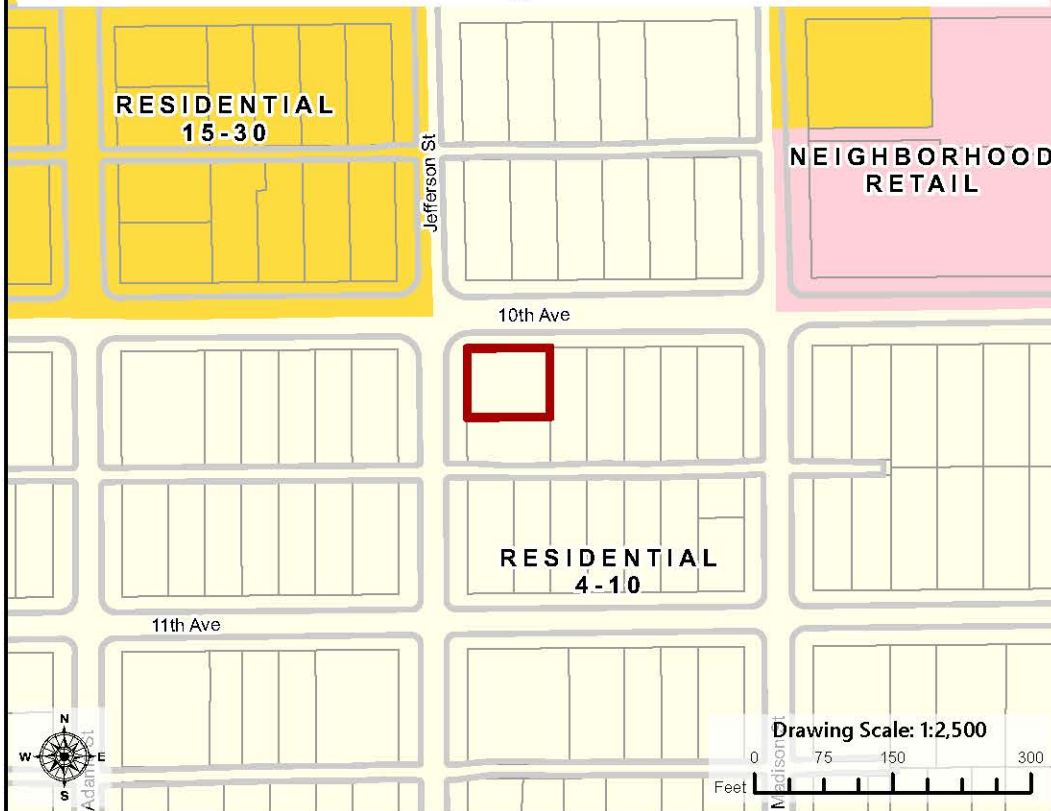
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

### Land Use Plan Designation

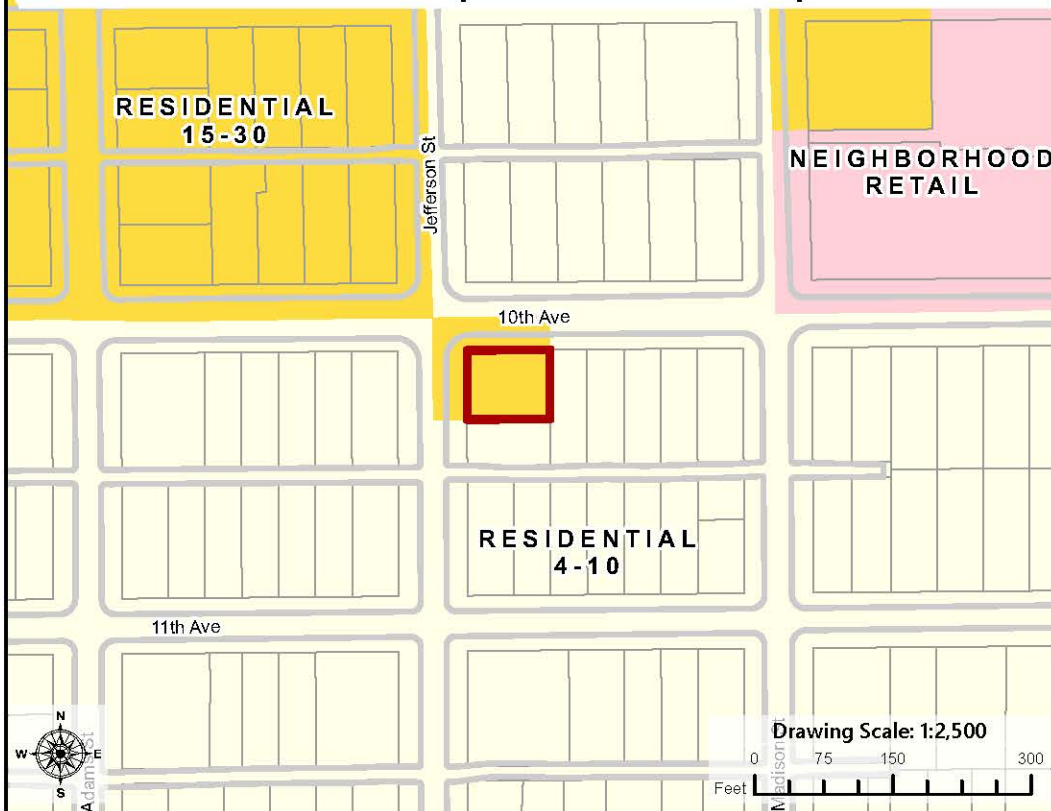
Residential 4-10

Residential 15-30

Neighborhood Retail

Acres (Proposal): 0.16

## EXHIBIT B: Proposed Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

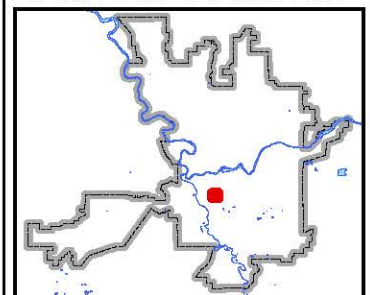
### Proposed Land Use

Residential 4-10

Residential 15-30

Neighborhood Retail

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Frebott





# Z19-505COMP (1117 W 10th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## Legend

Subject Parcels

Curb Line

Parcel

### Current Zoning

Neighborhood Retail (NR)

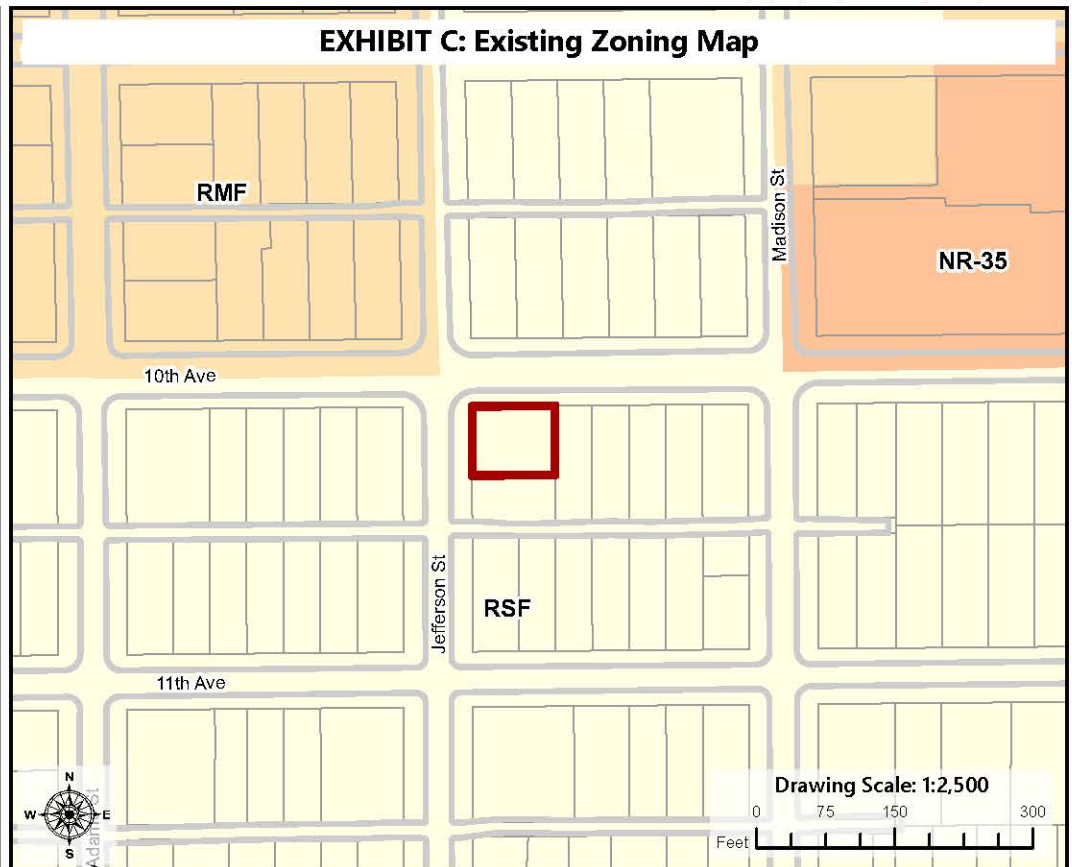
Residential Multifamily (RMF)

Residential Single-Family (RSF)

*Numbers after a Zone Label denote the height limits in that area.*

Acres (Proposal): 0.16

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Curb Line

Parcel

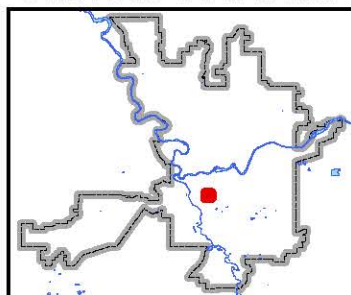
### Proposed Zoning

Neighborhood Retail (NR)

Residential Multifamily (RMF)

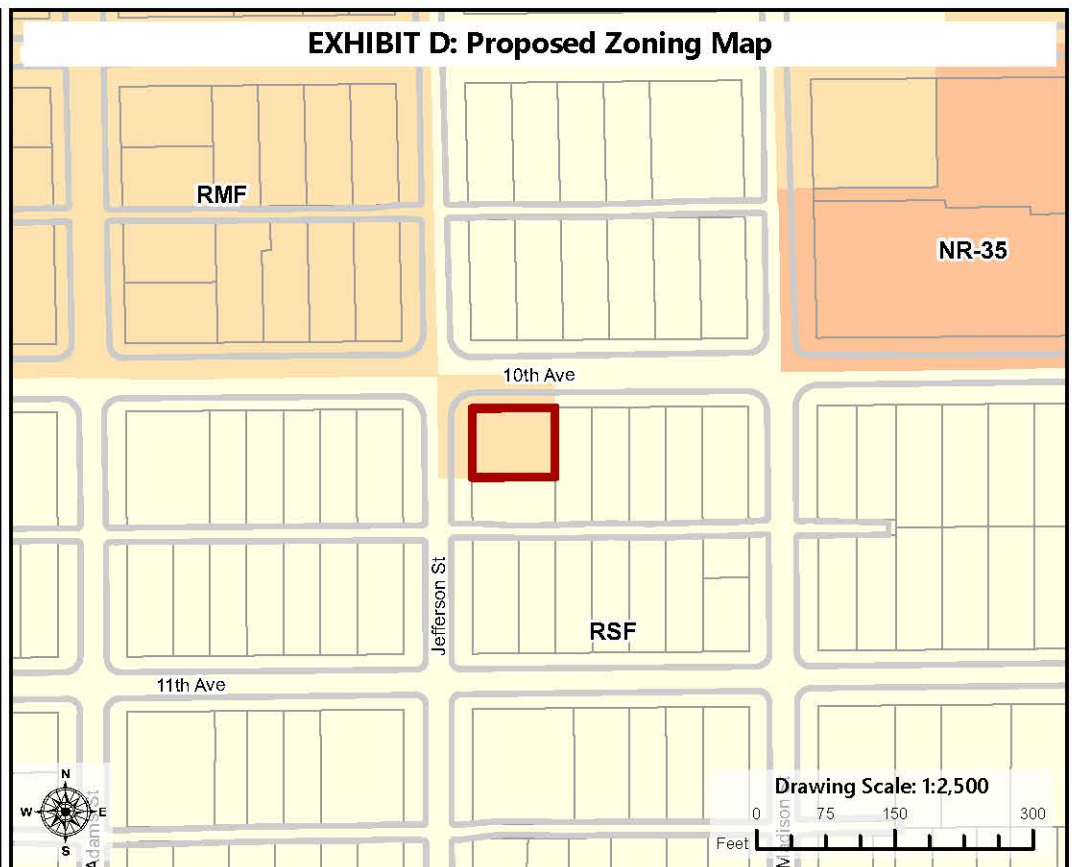
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map





**Exhibit E: Legal Description**

North 75 feet of lots 6-7, Block 2, Booges Addition, in the City of Spokane, Spokane County, Washington State.



**STAFF REPORT Z19-505COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to the Land Use Plan Map designation and zoning of one or more parcels in the City of Spokane. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                                                             |
|---------------------------|-----------------------------------------------------------------------------|
| <b>Parcel(s):</b>         | 35193.1405                                                                  |
| <b>Address(es):</b>       | 1117 W 10 <sup>th</sup> Avenue                                              |
| <b>Property Size:</b>     | 0.16 acres                                                                  |
| <b>Legal Description:</b> | North 75 feet of lots 6-7, block 2, Booges Addition                         |
| <b>General Location:</b>  | Southeast of the intersection of W 10 <sup>th</sup> Ave and S Jefferson St. |
| <b>Current Use:</b>       | Multi-Family Residence (legal, nonconforming)                               |

**II. APPLICANT SUMMARY**

|                        |                                                 |
|------------------------|-------------------------------------------------|
| <b>Agent:</b>          | Dwight Hume, Land Use Solutions and Entitlement |
| <b>Applicant:</b>      | Lark Homes, LLC                                 |
| <b>Property Owner:</b> | Lark Homes, LLC                                 |

**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | Residential 4-10 DUs/Acre (R 4-10)                                                                                                          |
| <b>Proposed Land Use Designation:</b> | Residential 15-30 DUs/Acre (R 15-30)                                                                                                        |
| <b>Current Zoning:</b>                | Residential Single-Family (RSF)                                                                                                             |
| <b>Proposed Zoning:</b>               | Residential Multi-Family (RMF)                                                                                                              |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | Not Recommended                                                                                                                             |



## IV. BACKGROUND INFORMATION

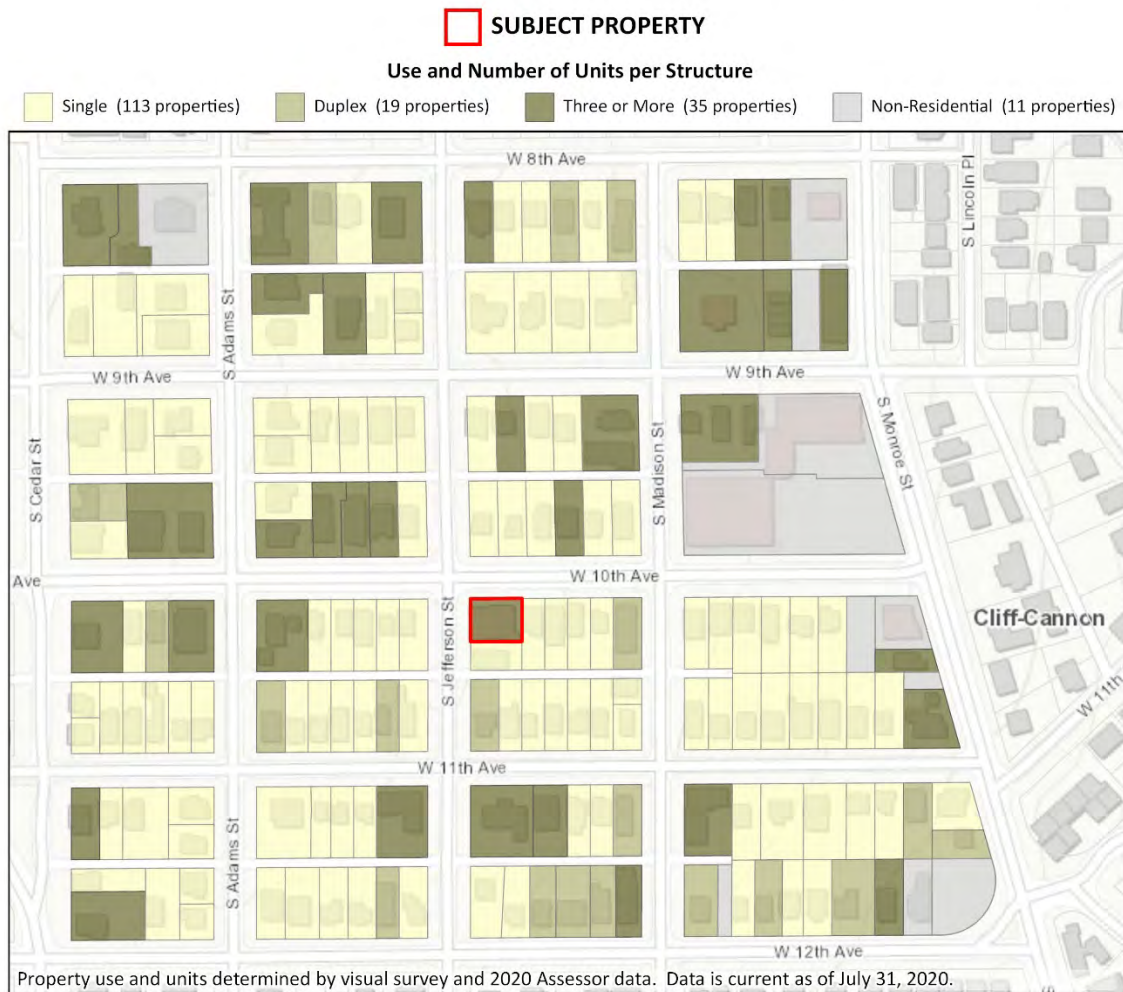
1. **General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the applicant asks the City of Spokane to amend the Land Use Plan Map designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for one property located in the Cliff Cannon neighborhood. The intent of the applicant is to maintain the existing structure(s) but to occupy them fully. Current zoning regulations prohibit this for single-family residential zones. However, no development plans have been submitted nor are any permits or approvals for future development sought by the applicant at this time.
2. **Site Description and Physical Conditions:** The subject parcel contains a large multi-family residence consisting of a home and connected additions. The remainder of the site is typical for a single-family residence in this area.
3. **Property Ownership:** The property is owned by Lark Homes, LLC, a WA-registered Limited Liability Corporation.
4. **Adjacent Property Improvements and Uses:** The subject property is entirely surrounded by residential development of varying densities and occupancy. Staff undertook a detailed survey of adjacent residential uses to determine the density of improvements adjacent to the subject property. This survey was conducted via online street view imagery, aerial photography, and County Assessor data, on a property-by-property basis.

The resulting type and density of adjacent development is shown in **Figure 1** at the top of the next page. Note that **Figure 1** indicates existing development, which is separate from zoning and Land Use Plan Map designation, which are discussed later in this report. As **Figure 1** shows, residential development within two blocks of the subject property varies in density. While the map appears to show a great number of multi-family residences, a significant majority of nearby parcels contain single-family homes. Additionally, nearly all two-family residences and more than half of all multi-family residences have the exterior characteristics of a single-family home. A number of traditional “apartment” style buildings have been constructed nearby, but most multi-family dwellings in this area were constructed as large single family homes and later converted to multi-family uses. Note that these changes were allowed within the zoning at the time (see item 8 below).

5. **Street Class Designations:** Both adjacent streets, W 10<sup>th</sup> Ave and S Jefferson St, are designated as local streets in the Spokane arterial map. The Comprehensive Plan designates both as local access streets as well, indicating that no future change in status for these streets is likely. This application does not propose to change the designation of any streets.
6. **Current Land Use Designation and History:** As shown in Exhibit A, the subject property is currently designated for “Residential 4-10” (between 4 and 10 dwelling units per acre), a designation reserved for single-family homes. The subject property has been designated for this use since the original adoption of the Growth Management Act (GMA) compliant Comprehensive Plan in 2001.
7. **Proposed Land Use Designation:** As shown in Exhibit B, the proposal is to amend the Land Use Plan Map designation so that the property is designated for “Residential 15-30” uses.



**FIGURE 1: Improvement and Use of Nearby Properties by Number of Dwelling Units**



- 8. Current Zoning and History:** The current zoning of the subject properties is Residential Single-Family (RSF). As the growth of Spokane has continued, the zoning of the subject property has changed over time. In 1958 the subject property, along with all properties on both sides of W 10<sup>th</sup> Avenue, was zoned Class 2 residential. As Spokane only had two classes of residential zoning at the time, Class 2 was the densest residential zoning, allowing everything from single-family homes to high-density apartments and multi-family dwellings.

By 1975 the commercial uses east of Madison St and north of 10<sup>th</sup> Ave had been developed. The remaining neighborhood around this location was zoned R3: Multi-Family Residence Zone. By 1975 most of the surrounding properties were developed with single-family homes and a few apartment buildings consistent with this higher density zoning. At this time, R3 was not the highest density residential—representing an equivalent density to the City’s current Residential Multi-Family Zoning. In 2006 the subject property and all the properties around it were zoned R4: Multi-family Residential. This zoning represented a step higher in density from the R3 zoning of the 1970s.



In 2001 the City adopted the current iteration of the Comprehensive Plan. This version of the Comprehensive Plan, which remains substantially similar today, focused density and intensity of use into centers and corridors and limited it elsewhere in the City. In response to this new land use strategy, the City undertook a massive update to the zoning code and residential building standards in the City in order to ensure consistency with the new Comprehensive Plan. This action, completed in 2006, included the rezoning of numerous properties in the City, including the subject property and those around it. In order to help focus density in centers and corridors, this effort included analysis and identification of parcels that contained different physical uses than those allowed in the previous zoning district. For instance, parcels like the subject parcel that were previously zoned for commercial uses but which were vacant or contained low-density residential uses were identified and rezoned to match the existing use of the property. This was done in large groups rather than parcel-by-parcel in order to provide for orderly control of density and land use as called for in the Comprehensive Plan.

Accordingly, during this effort the subject properties and most properties south of 10<sup>th</sup> Ave were rezoned from R4 to Single Family Residential (SFR), as they contained only few multifamily dwellings at that time (see Ordinance C33841). This effort also rezoned the properties west of S Jefferson Street and North of W 10<sup>th</sup> Ave for Residential Multi-Family. As such, at this intersection only one of the four corner properties is zoned for multi-family residential uses. Adoption of ordinance C33841 required significant work and included multiple workshops and outreach with the general public, the Plan Commission, and the City Council.

9. **Proposed Zoning:** As shown in **Exhibit D**, the proposal seeks to amend the zoning so that the entirety of the subject property is zoned Residential Multi-Family (RMF).

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | July 8, 2020      |

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



60-Day Public Comment Period Ended ..... August 7, 2020

SEPA Determination Issued ..... August 24, 2020

Notice of Public Hearing Posted ..... August 26, 2020

Plan Commission Hearing Date (Scheduled) ..... September 9, 2020

2. **Comments Received:** A request for comments was issued to City departments, local agencies, and neighborhood councils within 400 feet of the proposal, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- **Randy Abrahamson**, Spokane Tribe Historic Preservation Officer

Mr. Abrahamson indicated that there was a low probability of cultural resources on the subject property and that he had no additional concerns. He requested that any eventual development of the site include an Inadvertent Discovery Plan in the event that cultural resources are unearthed at that time. The proposal does not include any physical changes to the site at this time. Mr. Abrahamson's letter is attached to this staff report as **Exhibit L**.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. The following comments were received during the 60-day public comment period:

- **Seth Knutson, Applicant:** Mr. Knutson asserted his willingness to sign a development agreement limiting the use of the site to "senior assisted living care" and to keep the building envelope the same.
- **Milton Roland, Law Office of Milton G. Rowland, PLLC:** speaking for his clients, the Landry's, Mr. Roland expressed concerns about neighborhood character, the intention of the City to keep the zoning low density, on-street parking capacity, the ability of 10<sup>th</sup> Avenue to carry necessary traffic loads, the condition of the improvements on the property, the possibility of failure of the enterprise, and quality of life in the surrounding neighborhood.
- **Glen Landry:** Mr. Landry mentioned adjacent non-rental uses, the existing use that includes all ages, his assertion that the applicant had mentioned using the property as a "halfway house" in the past, and parking.
- **Wai Landry:** Mrs. Landry commented on the unsuitability of the property for senior living, including the size of the property, the perceived lack of any outdoor space, the proximity of the entrance to the street, and parking.
- **A Petition:** A petition stating opposition to the project due to unnamed impacts to the neighborhood was signed by 37 individuals.
- **Tom May:** Mr. May expressed concerns about impacts to the surrounding neighborhood including changes in neighborhood character, historic homes in the vicinity, parking, and nearby property value impacts.



- **Judy Madden:** Ms. Madden expressed concerns about the condition of the improvements on the property, financing concerns for future improvements, the interior condition of the structure, the presence of “too many” multi-family dwellings in the neighborhood, and concerns about neighborhood character.
- **Alison Johnston:** Ms. Johnston opposed the proposal, mentioned concerns about increased multi-family uses in the neighborhood, parking capacity, impacts on existing property values, and potential increases in crime.
- **Deanna Murdy:** Ms. Murdy expressed a general opposition to the project and concerns for property values.
- **Jerry Widing:** Mr. Widing expressed confusion about the land use designation for the home, perceived wishes of the applicant to only raise the value of his property for sale, and concerns about parking.
- **Austin LaRue:** Mr. LaRue expressed a general opposition to the project due to the historic character of the neighborhood, the condition of the property, increased traffic, and parking.
- **Anne Putney:** Mrs. Putney expressed concerns about this change leading to a trend for more multi-family conversions in the area, potential impacts if the owner sells the property and a new owner decides to construct an apartment building on the site, the perceived poor condition of the improvements on-site, and impacts to property values and parking in the vicinity.
- **Damian Putney:** Mr. Putney shared his history in the neighborhood and his business in construction before expressing concerns with the applicant’s stated financial resources to renovate the property properly and potential impacts to the neighborhood’s single-family character.
- **Roger Takiguchi<sup>3</sup>:** Mr. Takiguchi expressed concerns about parking capacity if the is fully occupied, pointing out the congestion already caused by the nearby shopping center, potential health concerns of allowing more people to reside on the property, the perceived poor condition of the property and improvements, and the potential change to the predominantly “family” character of the neighborhood.
- **Katherine Widing:** Mrs. Widing expressed concerns that the applicant is seeking to create a “halfway house,” that parking is insufficient for a more dense use, a perceived desire by the applicant to raise the eventual sale price of his property, and the perceived poor condition of improvements on site.

The majority of public comments on this proposal can be grouped into several factors. These include concerns about parking on streets already impacted by the nearby commercial uses on Monroe, changes in existing neighborhood character, what local residents feel is the poor condition of the current improvements on the property, and a general concern for the suitability of the site as a senior care facility. Other concerns have been raised about the owner’s possible intention to sell

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<sup>3</sup> Note Mr. Takiguchi submitted two identical emails in comment—only one is included in **Exhibit M**.



the property—thought the City has no concrete proof that such is the case. Copies of all public comments received on this proposal are attached to this staff report as **Exhibit M**.

Regarding development/redevelopment impacts such as parking or the condition of the property/improvements, the Spokane Municipal Code requires that these issues be resolved to the satisfaction of the code if and when the property owner seeks building permits for future work. However, this Comprehensive Plan amendment proposal does not currently seek any permits or approvals for physical changes to the property or improvements. Regarding the historic nature of the neighborhood, or the impacts associated with multi-family residential uses in a predominantly single-family area, see the discussion under decision criteria K.2.a below. The analysis presented in this staff report, including the conclusions as to the decision criteria below, considers the proposed land use and zoning change and the types of development and use that area allowed in general under those uses/zones, not a specific development or redevelopment that may or may not occur.

3. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on July 8, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was given an opportunity to speak but was unable to participate during the workshop. However, the agent for the applicant provided written testimony regarding the project proposal, attached to this staff report as **Exhibit N**.

In that written testimony, the agent outlined the history of the structure on the property, stating that the multi-family nature of the building was initiated in 1956 with permits from the City. This use was legal under non-conforming rights even after multiple rezones of the property, the testimony asserts, but those rights were lost when the previous owner stopped using the property as a nursing home more than 12 months before the current owner purchased it. The agent continued, stating that parking concerns raised by some would be mitigated by the nature of future occupants, who will be less ambulatory than typical renters and unlikely to use/own personal vehicles. Finally, the agent communicated the applicant's acceptance of a possible development agreement that would restrict future redevelopment/use of the property as a full-density multi-family residential use.

## **VI. APPLICATION REVIEW AND ANALYSIS**

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:
  - A. Keep the comprehensive plan alive and responsive to the community.
  - B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
  - C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
  - D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.



E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.

F. Amendments to the comprehensive plan must result in a net benefit to the general public.

2. **Review Criteria:** SMC 17G.020.030 establishes the approval criteria for Comprehensive Plan amendments, including Land Use Plan Map amendments. In order to approve a Comprehensive Plan Land Use Plan Map amendment request, the decision-making authority shall make findings of fact based on evidence provided by the applicant that demonstrates satisfaction of all the applicable criteria. The applicable criteria are shown below in *italic* print. Following each criterion is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. **GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. **Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency comment request or require a traffic impact analysis for the proposal. The subject property is already served by water, sewer, nearby transit service, and adjacent existing City streets. Furthermore, under State and local laws, any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020.

The proposal meets this criterion.



- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

The proposal meets this criterion.

**E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Development Regulations.* As a non-project proposal, there are no specific plans for development of this site. Additionally, any future development on this site will be required to be consistent with the current development regulations at the time an application is submitted. The proposal does not result in any non-conforming uses or development and staff finds no reason to indicate that the proposed Comprehensive Plan Land Use Plan Map and zone change would result in a property that cannot be reasonably developed in compliance with applicable regulations.

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.

*Neighborhood Planning Documents Adopted after 2001.* The Cliff Cannon neighborhood joined the Lincoln Heights, Manito/Cannon Hill, Rockwood, and Comstock neighborhoods to form the South Hill Coalition. These five neighborhoods combined their initial neighborhood planning funds provided by the city in order to prepare and adopt the *South Hill Coalition Connectivity and Livability Strategic Plan* (the CLSP) in 2014. The priorities for Cliff Cannon in the document included traffic calming on major north-south streets through the neighborhood, preservation of existing trees, and additional connections between the historic Cannon's Addition and downtown uses and along 14<sup>th</sup> Avenue. None of these priorities is in the vicinity of the subject property. Of the various projects and goals in the plan, none concerned or were located in close proximity to the subject property either. As such, it is considered unlikely that the proposal would affect the implementation of the CLSP.



The City is currently considering the Cannon Streetcar Suburb Historic District. The subject parcel would be located within this District if formed.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in Exhibit H of this report. Further discussion of these policies is provided under section K.2 below.

See Item K.2 for below for analysis and results.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: As discussed under item K.2.a below, the proposal is in conflict with the Comprehensive Plan. Furthermore, this application does not include any amendment to the text of the plan, including any policies with which it is in conflict. Therefore, the proposal also does not conform to this criterion.

The proposal appears inconsistent with this criterion.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** No evidence has been provided by any adjacent jurisdiction, including the County of Spokane, indicating this proposal would conflict with the CWPP or the plans of any neighboring jurisdiction. The proposed change in land use designations affects a relatively small area within an existing urbanized area, with no foreseeable implications to regional or inter-jurisdictional policy issues.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed



transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>4</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

1. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would change the land-use designation of an urban area already served by the public facilities and services described in CFU 2.1. The proposed change in land-use designation affects a relatively small area and does not measurably alter demand for public facilities and services in the vicinity of the site or on a citywide basis. Any subsequent development of the site will be subject to a concurrency determination pursuant to SMC 17D.010.020, thereby implementing the policy set forth in CFU 2.2.

The proposal meets this criterion.

---

<sup>4</sup> State Environmental Policy Act



- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

K. **Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

The proposal meets this criterion.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The primary Comprehensive Plan policy which applies to the proposal is Land Use LU 1.4, Higher Density Residential Uses, which directs “new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.” The subject parcel is not located within the vicinity of any designated center or corridor, as shown on Map LU 1 of the Comprehensive Plan. The nearest center is the downtown, which is 0.44 miles away. Policy LU 1.4 goes on to say that any infill of higher density residential designations is limited to the “boundaries of existing multi-family residential designations where the existing use of land is predominantly higher density residential.” The definition of “predominantly” is not included in the policy. As shown in the existing zoning map (see **Exhibit C**), the subject parcel is not enclosed in a larger area of multi-family residential uses—rather it is located catty-corner across from a multi-family area. Furthermore, while significant amounts of multi-family zoning exist northwest of the subject parcel, the predominant improvement type in the vicinity is single-family homes (see **Figure 1** under discussion 4 above).

Of further consideration is policy LU 1.3, Single-Family Residential Area, which guides the application of single-family land use and zoning in the city. According to policy LU 1.3, the City should “protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and



Corridors.” Because this parcel is not encompassed by a larger area of multi-family land use designations, it is well outside any center or corridor uses, and the neighborhood is not predominantly multi-family in nature, the proposal appears to be inconsistent with Policy LU 1.4.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: There exist no physical features of the site or its surroundings that would preclude physical development of office uses on the site. The site is adequately served by all utilities and by two major arterial streets, bus service is nearby, and the site is generally level and devoid of critical areas.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: As discussed in item a. above, the proposal does not seem to provide greater/more effective implementation of comprehensive plan policy, as it appears inconsistent with the siting requirements for higher density residential uses.

The proposal appears inconsistent with this criterion.

- 3. **Rezones, Land Use Plan Amendment:** Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.

Staff Analysis: If the Land Use Plan Map amendment is approved as proposed, the zoning designation of the subject property will change concurrently from RSF (Residential Single-Family) to RMF (Residential Multi-Family).

The proposal meets this criterion.

## VII. CONCLUSION

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears inconsistent with the approval criteria set forth by SMC 17G.020, specifically to policies pertaining to the placement of higher-density residential uses in the City outside designated centers and corridors.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City’s Comprehensive Plan.



## **VIII. STAFF RECOMMENDATION**

Considering the above information and the whole of the administrative record, staff does not recommend that Plan Commission and the City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                        |                                           |
|----------------------------------------|-------------------------------------------|
| A. Existing Land Use Plan Map          | I. Application Materials                  |
| B. Proposed Land Use Plan Map          | J. SEPA Checklist                         |
| C. Existing Zoning Map                 | K. SEPA Determination of Non-Significance |
| D. Proposed Zoning Map                 | L. Agency Comments                        |
| E. Application Notification Area       | M. Public Comments                        |
| F. Detail Aerial                       | N. Agent Communication Regarding PC       |
| G. Wide-Area Aerial                    | Workshop                                  |
| H. List of Relevant Comp Plan Policies |                                           |





# Z19-505COMP (1117 W 10th Ave)

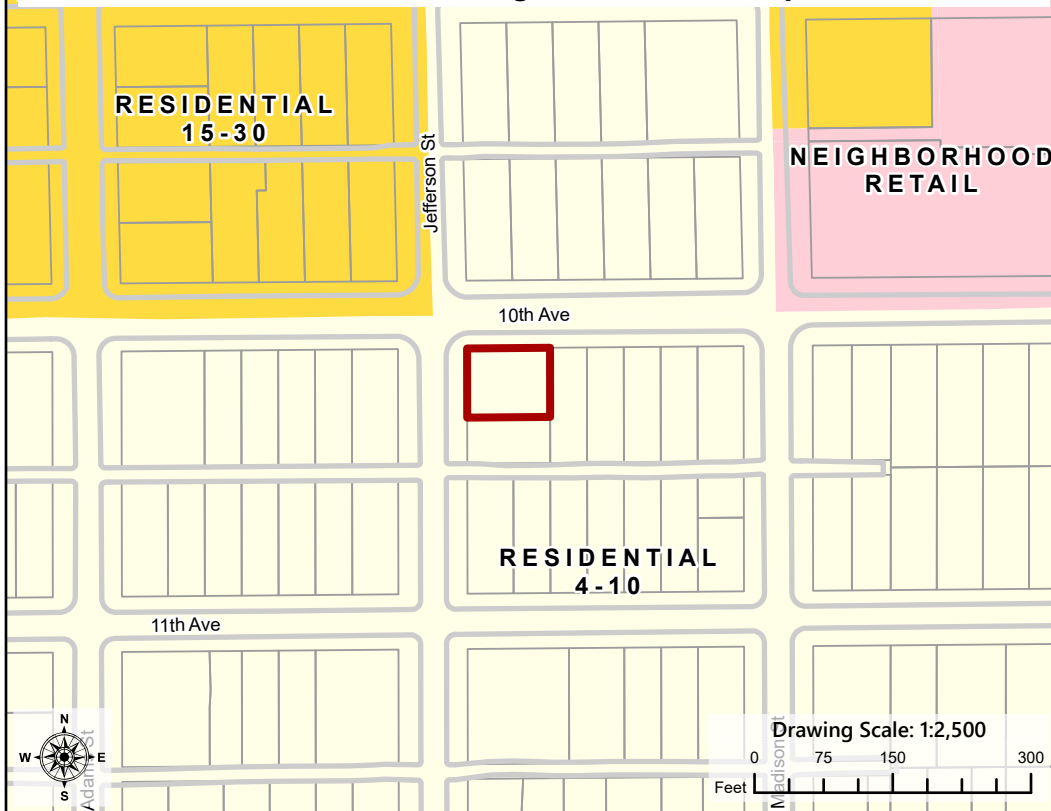
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT A: Existing Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

### Land Use Plan Designation

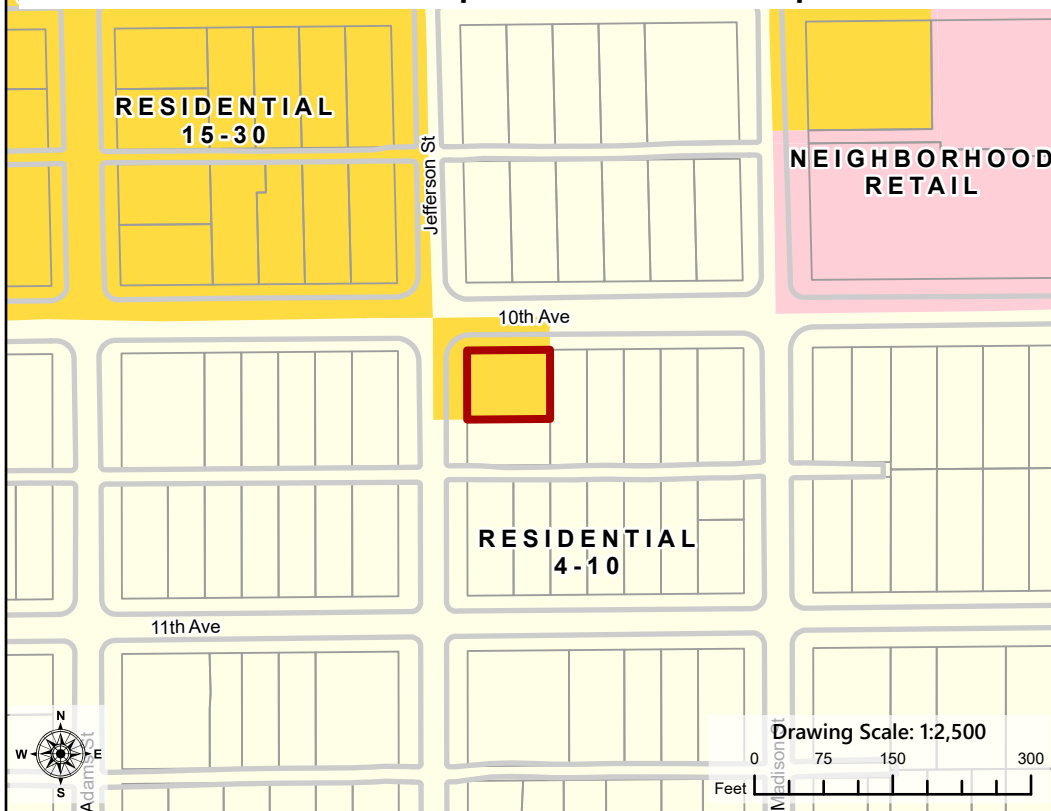
Residential 4-10

Residential 15-30

Neighborhood Retail

Acres (Proposal): 0.16

## EXHIBIT B: Proposed Land Use Plan Map



## Legend

Subject Parcels

Parcel

Curb Line

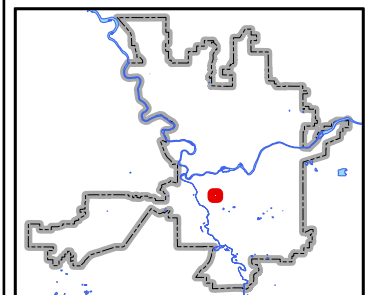
### Proposed Land Use

Residential 4-10

Residential 15-30

Neighborhood Retail

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-505COMP (1117 W 10th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 11/27/2019

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## Legend

Subject Parcels

Curb Line

Parcel

### Current Zoning

Neighborhood Retail (NR)

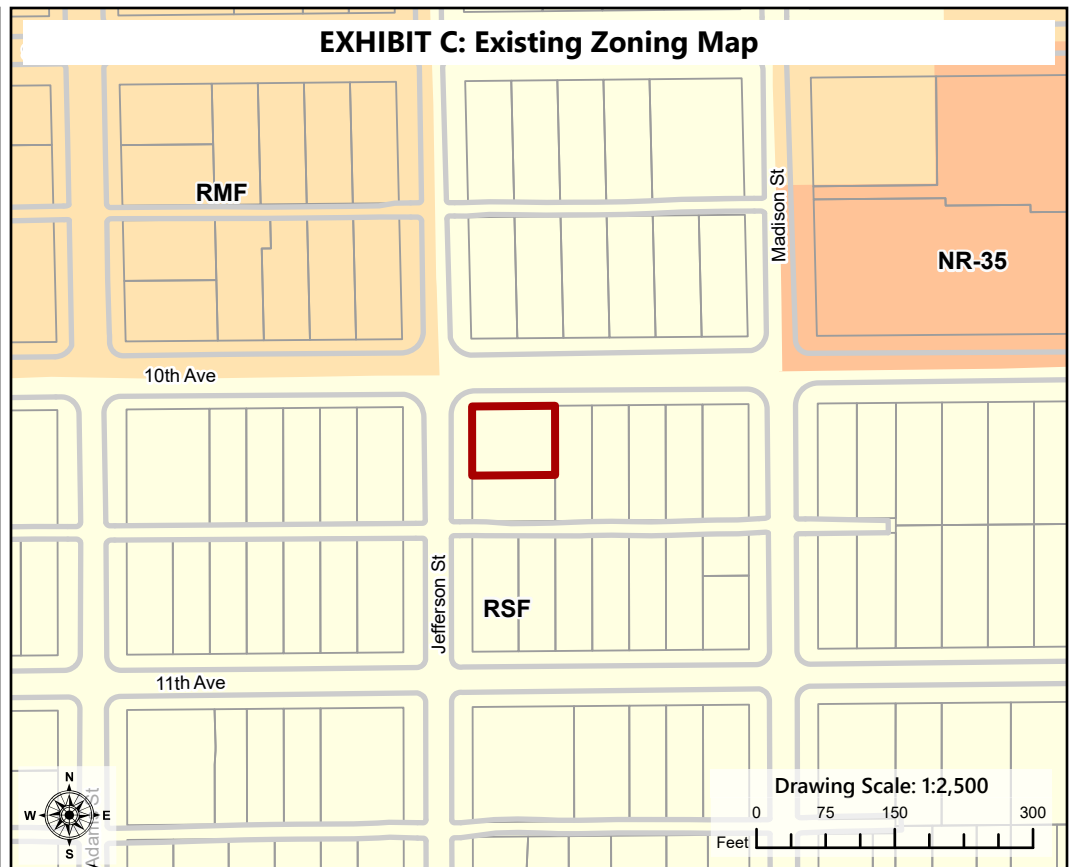
Residential Multifamily (RMF)

Residential Single-Family (RSF)

Numbers after a Zone Label denote the height limits in that area.

Acres (Proposal): 0.16

## EXHIBIT C: Existing Zoning Map



## Legend

Subject Parcels

Curb Line

Parcel

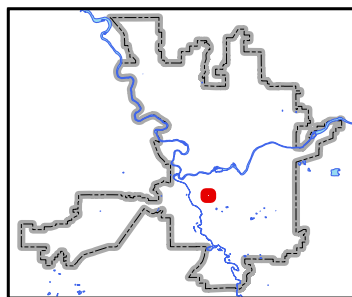
### Proposed Zoning

Neighborhood Retail (NR)

Residential Multifamily (RMF)

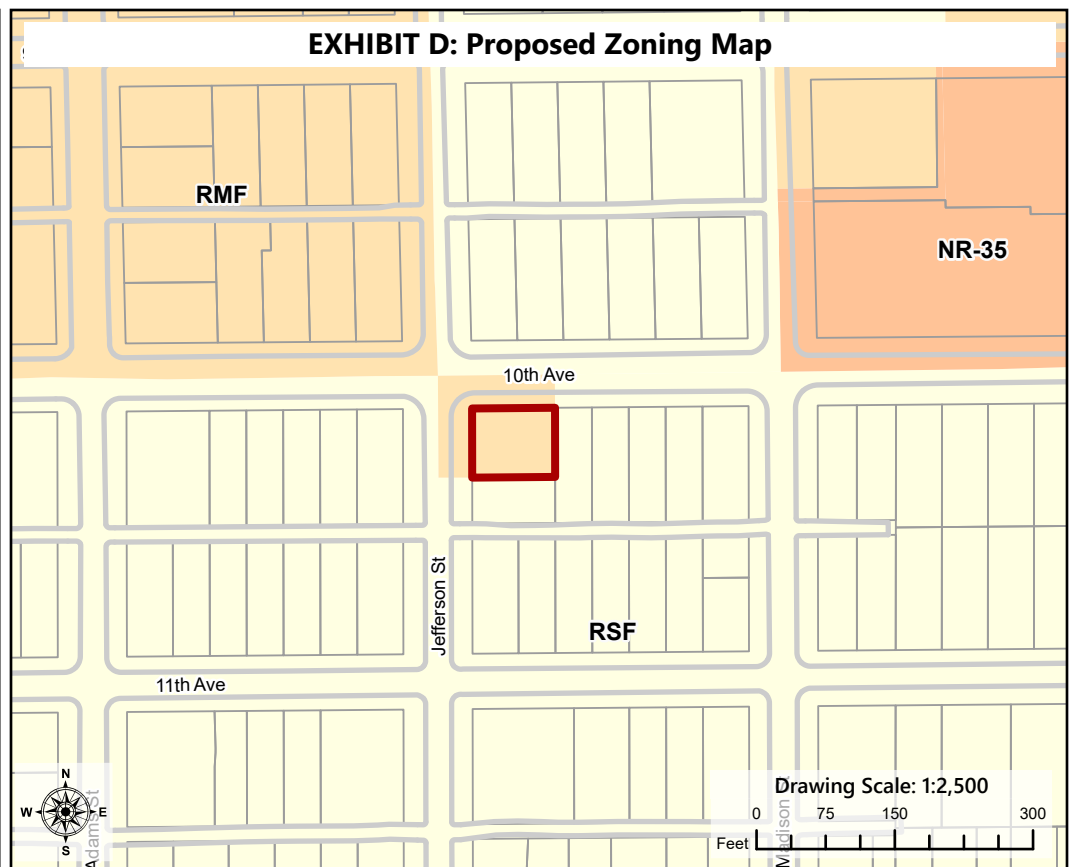
Residential Single-Family (RSF)

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

## EXHIBIT D: Proposed Zoning Map







# Z19-505COMP (1117 W 10th Ave)

2019/2020 Comprehensive Plan Amendment Proposals

THIS IS NOT A LEGAL DOCUMENT: The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT E: Application Notification Area



### Legend

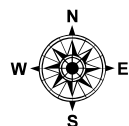
- Parcel
- Notification Boundary
- Curb Line
- Address Point

### Subject Parcels

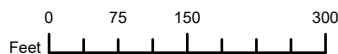
- Subject Parcels

### Application proposes to:

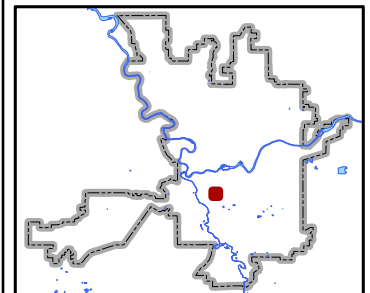
Change Land Use Designation from Residential 4-10 to Residential 15-30.



Project Size: 0.16 Acres (Approximate)  
Drawing Date: 12/3/2019 Drawing Scale: 1:2,500



### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





# Z19-505COMP (1117 W 10th Ave)

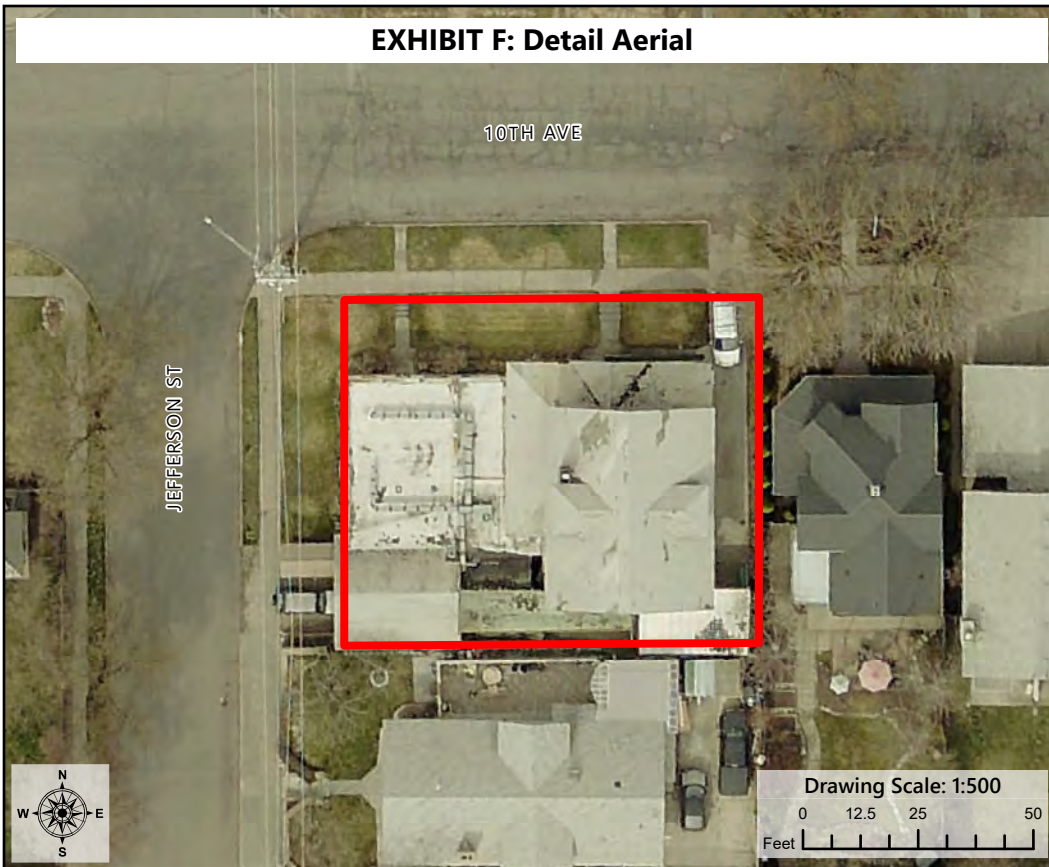
2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 12/5/2019

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

## EXHIBIT F: Detail Aerial



## Legend

Subject Parcels

Adjacent Ownership

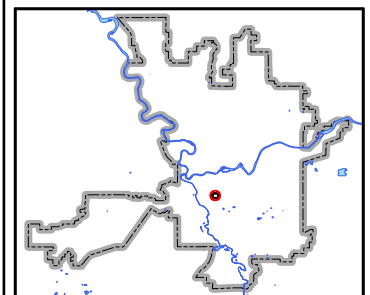
Aerial Photograph Taken  
on 4/3/2018

Acres (Proposal): 0.16

## EXHIBIT G: Wide-Area Aerial



## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





The following policies of the Comprehensive Plan relate to application Z19-505COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 3—Land Use**

### LU 1.3 Single-Family Residential Areas

Protect the character of single-family residential neighborhoods by focusing higher intensity land uses in designated Centers and Corridors.

*Discussion:* The city's residential neighborhoods are one of its most valuable assets. They are worthy of protection from the intrusion of incompatible land uses. Centers and Corridors provide opportunities for complementary types of development and a greater diversity of residential densities. Complementary types of development may include places for neighborhood residents to work, shop, eat, and recreate. Development of these uses in a manner that avoids negative impacts to surroundings is essential. Creative mechanisms, including design standards, must be implemented to address these impacts so that potential conflicts are avoided.

### LU 1.4 Higher Density Residential Uses

Direct new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.

*Discussion:* Higher density housing of various types is the critical component of a center. Without substantially increasing population in a center's immediate vicinity, there is insufficient market demand for goods and services at a level to sustain neighborhood-scale businesses. Higher density residential uses in Centers range from multi-story condominiums and apartments in the middle to small-lot homes at the edge. Other possible housing types include townhouses, garden apartments, and housing over retail space.

To ensure that the market for higher density residential use is directed to Centers, future higher density housing generally is limited in other areas. The infill of Residential 15+ and Residential 15-30 residential designations located outside Centers are confined to the boundaries of existing multi-family residential designations where the existing use of land is predominantly higher density residential.

### LU 5.5 Compatible Development

Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

## **Chapter 6—Housing**

### H 1.4 Use of Existing Infrastructure



Direct new residential development into areas where community and human public services and facilities are available.

*Discussion:* Using existing services and infrastructure often reduces the cost of creating new housing. New construction that takes advantage of existing services and infrastructure conserves public resources that can then be redirected to other needs such as adding amenities to these projects

#### H 1.7 Socioeconomic Integration

Promote socioeconomic integration throughout the city.

*Discussion:* Socioeconomic integration includes people of all races, color, religion, sex, national origin, handicap, disability, economic status, familial status, age, sexual orientation, or other arbitrary factors. Often, housing affordability acts as a barrier to integration of all socioeconomic groups throughout the community.

#### H 1.11 Access to Transportation

Encourage housing that provides easy access to public transit and other efficient modes of transportation.

*Discussion:* Transportation is the second largest expenditure after housing and can range from 10 to 25 percent of household expenditures. Examining where housing is City of Spokane Comprehensive Plan 6-8 located and the associated transportation costs may provide a more realistic evaluation of housing affordability in the future.

#### H 1.18 Distribution of Housing Options

Promote a wide range of housing types and housing diversity to meet the needs of the diverse population and ensure that this housing is available throughout the community for people of all income levels and special needs.

*Discussion:* A variety of housing types should be available in each neighborhood. Diversity includes styles, types, size, and cost of housing. Many different housing forms can exist in an area and still exhibit an aesthetic continuity. Development of a diversity of housing must take into account the context of the area and should result in an improvement to the existing surrounding neighborhood.

#### H 1.19 Senior Housing

Encourage and support accessible design and housing strategies that provide seniors the opportunity to remain within their neighborhoods as their housing needs change.

*Discussion:* Accessory dwelling units, condominiums, and existing home conversions within centers are examples of other arrangements that reduce maintenance worries and increase access to services.

#### H 1.22 Special Needs Housing

Encourage the retention, inclusion, and development of special needs and assisted living housing.



*Discussion:* Both the Growth Management Act and Countywide Planning Policies require that essential public facilities be fairly and equitably distributed. This applies within jurisdictions, as well as between neighboring jurisdictions. This policy does not apply to criminal or prerelease transitional housing.

### H 2.3 Housing Preservation

Encourage preservation of viable housing.

*Discussion:* Housing that is susceptible to redevelopment is often serving lower income households and is an important part of the housing mix within the city. Future sub-area plans should preserve existing viable housing outside of designated center or corridor environments where redevelopment and intensification are encouraged. Often the housing that is destroyed cannot be replaced by new housing elsewhere at the same cost level. Sub-area plans should permit the transfer of unused development rights from low-income housing to eligible sites elsewhere in the planning area or the city as a preservation strategy.

Information about soon-to-be-demolished housing should be made available to the public, such as on the internet, so that concerned housing-related groups can determine if there are alternatives to demolition when the structure is worth preserving. Options might include purchase of the property or relocation of the housing.

## **Chapter 11—Neighborhoods**

### N 2.1 Neighborhood Quality of Life

Ensure that neighborhoods continue to offer residents transportation and living options, safe streets, quality schools, public services, and cultural, social, and recreational opportunities in order to sustain and enhance the vitality, diversity, and quality of life within neighborhoods.

*Discussion:* Spokane enjoys a rich variety of living opportunities within its individual neighborhoods, each with its unique character. Maintaining and enhancing our neighborhood assets is key to providing stability within neighborhoods and Spokane citizens with a prolonged sense of pride.

### N 2.3 Special Needs

Ensure that neighborhood-based services are available for special needs and located in proximity to public transit routes in order to be accessible to local residents.

*Discussion:* Special needs services can include child/adult care services, long-term care for special needs, special needs housing, and other related services which recognize self-direction and participation by all residents and/or recipients of the services.

### N 2.4 Neighborhood Improvement

Encourage revitalization and improvement programs to conserve and upgrade existing properties and buildings.

## **Chapter 11—Neighborhoods**

### N 8.4 Consistency of Plans



Maintain consistency between neighborhood planning documents and the comprehensive plan.

*Discussion:* Neighborhood planning shall be conducted within the framework of the comprehensive plan, and further, the Growth Management Act requires that these plans be consistent with the comprehensive plan.



## Land Use Solutions & Entitlement

### Land Use Planning Services

9101 N. MT. VIEW LANE Spokane, WA 99218

509-435-3108 (V)

dhume@spokane-landuse.com

10-29-19

Kevin Freibott, Planner II  
Planning & Development Services  
3<sup>rd</sup> Floor City Hall  
West 801 Spokane Falls Blvd.  
Spokane WA 99201

Ref: 10<sup>th</sup> and Jefferson SEC R 4-10 to R-30

Dear Kevin:

The applicant, Lark Homes LLC is requesting the change of density in order to utilize an existing nursing home for its maximum capacity of 16 assisted living occupants. This was a nursing home, first established in the mid-fifties when the property was zoned R4. It was operated as such through the mid 80's and eventually sold to Lark Homes LLC, the applicant.

The non-conforming rights no longer apply and an RMF zone is needed in order to accommodate the existing living space to full capacity of 16 assisted living occupants. Lark Homes is currently renting rooms to 6 separate occupants per provisions of the current zone in which 6 unrelated individuals constitutes a family unit as allowed by the current RSF zone.

With the increasing need for assisted living, it is imperative that the City approve the request simply to enable optimum use of this facility. To that end, we are open to entering into a development agreement to assure the neighbors and the City that an approved amendment will not be precedent setting and/or allow apartment use.

Lark Homes has contacted the surrounding neighbors and they are supportive of his plans, presumably because of the historic nature of the building and land use as a nursing home. Lark Homes has also met with the Cliff Cannon Neighborhood Council and shared his plans and they are supportive of his proposal.

Respectfully Submitted



Dwight J Hume, agent





**DESCRIPTION OF PROPOSAL:**

Map amendment from R 4-10 to R 15-30 and a zone change from RSF to RMF on property located at the SEC of Jefferson and 10<sup>th</sup> Avenue.

**ADDRESS OF SITE OF PROPOSAL:** (if not assigned yet, obtain address from Public Works before submitting application)

1117 W 10<sup>th</sup> Avenue

**APPLICANT:**

**Name:** Lark Homes LLC  
**Address:** 8805 N Cedar Rd Spokane WA 99208  
**Phone (home):** **Phone (work):**  
**Email Address:** spy.pawn007@gmail.com

**PROPERTY OWNER:**

**Name:** Same  
**Address:** **Phone (work):**  
**Phone (home):**  
**Email address:**

**AGENT:**

**Name:** Land Use Solutions & Entitlement C/O Dwight Hume  
**Address:** 9101 N Mt. View Lane,  
**Phone (home):** **Phone (work):** 435-3108  
**Email address:** dhume@spokane-landuse.com

**ASSESSOR'S PARCEL NUMBERS:**

35193.1405

**LEGAL DESCRIPTION OF SITE:**

N 75 ft of Lots 6-7 Block 2, Booges Addition

**SIZE OF PROPERTY:**

6750 sf

**LIST SPECIFIC PERMITS REQUESTED IN THIS APPLICATION:**

Annual Map Amendment and Zone Change



**SUBMITTED BY:**

Dwight Hume

☐ Applicant   ☐ Property Owner   ☐ Property Purchaser   ☒ Agent

In the case of discretionary permits (administrative, hearing examiner, landmarks commission or plan commission), if the applicant is not the property owner, the owner must provide the following acknowledgement:

I, Seth Knutson, owner of the above-described property do hereby authorize Land Use Solutions & Entitlement c/o Dwight Hume to represent me and my interests in all matters regarding this application.

[Signature]

**ACKNOWLEDGMENT:**

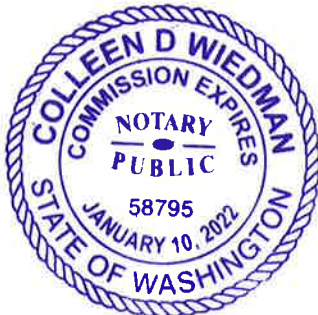
STATE OF WASHINGTON    )

) ss.

COUNTY OF SPOKANE        )

On this 16<sup>th</sup> day of October, 2019, before me, the undersigned, a Notary Public in and for the State of Washington, duly commissioned and sworn, personally appeared Seth Knutson, to me known to be the individual that executed the foregoing instrument and acknowledged the said instrument to be free and his/her free and voluntary act and deed, for the uses and purposes therein mentioned.

Witness my hand and official seal hereto affixed the day and year first above written.



Colleen D. Wiedman

Notary Public in and for the State of Washington,

residing at Spokane, WA



## Project Narrative Summary

### Lark Homes LLC Map Amendment

Lark Homes LLC request a map amendment from R 4-10 to R 15-30 to simply enable the efficient use of an existing group home facility, formerly a nursing home established in 1956 when the property was then zoned R-4, Multi-Family. It is located at the SEC of Jefferson and 10<sup>th</sup> Avenue within one block of a grocery store (Huckleberry's) and a hardware (Ace Hardware) at 9<sup>th</sup> and Monroe.

The area is a mix of conforming and non-conforming apartment uses due to the fact that the subject site is adjacent to the RMF zone but now within the RSF zone, whereas it was formerly an R4 zone which allowed apartments, nursing homes, lodges, motels and hotels, medical and dental offices. Some of which were established within this current down zoned area.

As stated above, in 1956, there was the conversion of two adjoining residences into a common facility for use as a state regulated nursing home. The nursing home changed hands four times and eventually closed. In the interim, the zone changed from R4 multi-family to RSF single family and the then current non-conforming rights to a nursing home expired upon lapse of one year's vacancy.

The Applicant/Owner, Lark Homes LLC, purchased the property with these expired rights and needs to bring the facility into compliance with current city and state codes as they relate to Group Living and Assisted Living. Currently, the owner is renting to 6 non-related adults as temporary housing. In compliance with the restrictions of the RSF zone for single family occupancy.

It is the intent of this applicant to bring the facility up to code and accommodate sixteen (16) assisted living spaces, whereas the current RSF zone only allows the facility to be under-utilized with just ten (10) assisted living spaces. There are no other means of bringing this facility into compliance but for a land use plan map amendment and zone change to R 15-30 and the RMF zone.

As of this submittal, Lark Homes LLC has met with the neighborhood council and explained the proposal to them. They were very supportive of his plans and offered written support. He has also met with each adjacent homeowner and explained his long-range intentions and received no negative responses. Presumably, this request should be supported as proposed.

To conclude, we would suggest that the City Council approve this with the added restriction of a development agreement to ensure the limitation of use as proposed.





# Comprehensive Plan or Land Use Code Amendment

Pre-Application

Rev.20180102

## DESCRIPTION OF THE PROPOSED AMENDMENT:

(Please check the appropriate box(es) (701-707 S Sherman Map Amendment)

- |                                                         |                                                                 |
|---------------------------------------------------------|-----------------------------------------------------------------|
| <input type="checkbox"/> Comprehensive Plan Text Change | <input checked="" type="checkbox"/> Land Use Designation Change |
| <input type="checkbox"/> Regulatory Code Text Change    | <input type="checkbox"/> Area-Wide Rezone                       |

**Please respond to these questions on a separate piece of paper. Incomplete answers may jeopardize your application's chances of being reviewed during this amendment cycle.**

### 1. General Questions *(for all proposals):*

- a. Summarize the general nature of the proposed amendment.  
*A Map amendment from R 4-10 to R 15-30 and RSF to RMF on 6750 sf at the SEC of 10<sup>th</sup> Avenue and Jefferson.*
- b. Why do you feel this change is needed?  
*The area is not scheduled for a sub-area plan update; the existing and historical use has been group living as a nursing home since 1956 when the site was zoned R4 and permitted nursing homes. The applicant purchased the facility several years after it was vacated as a nursing home and thus inherited an expired non-conforming use. The current facility can accommodate 16 assisted living units whereas the zone allows 10 but can only do so if the land use designation is changed to R 15-30 and RMF zoning, together with the approval of a Conditional Use permit for group living. No other options are available in the MC or Development Code.*
- c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?  
*The request is similar to the fundamental concepts in the comprehensive plan because the area is adjacent to R-15-30 zoning at the NWC of 10<sup>th</sup> and Jefferson diagonally across the intersection, therefore it is similar to adjacent land use designations. Apartments also exist within 100 feet NE of the subject along 10<sup>th</sup> as non-conforming uses within the RSF zone.*
- d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal? *N/A*
- e. For map amendments:
  1. What is the current Land Use designation and zoning for each affected parcel? *R 4-10 and RSF*
  2. What is the requested Land Use designation and zoning for each affected parcel? *R-15-30 and RMF*
  3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc. *Site: Nursing Home; East: S/F; South: S/F; West S/F and apartments; North: Apartments and S/F.*
- f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal? *No*
- g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood



planning, public input on new regulations, etc.)? *There are no other sub-area plans scheduled for this area. More importantly, there are no other means of enabling 16 assisted living patients except by a change of land use and zone.*

- h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?  
☐ Yes                      ☒ No

- i. If yes, please answer the following questions:

1. When was the amendment proposal submitted?
2. Was it submitted as a consistent amendment or an inconsistent amendment?
3. What were the Plan Commission recommendation and City Council decision at that time?
4. Describe any ways that this amendment proposal varies from the previously considered version.

Development Services Center 808 West Spokane Falls Boulevard, Spokane, WA 99201-3336  
[my.spokanecity.org](http://my.spokanecity.org) | Phone: 509.625.6300 | Fax: 509.625.6822



## Early Threshold Review

Lark Homes LLC 10<sup>th</sup> & Jefferson Map Amendment

**Description of Proposed Amendment:** Land Use Map change from R 4-10 to R 15-30 and a zone change from RSF to RMF on 6750 sf.

The request would allow an existing nursing home to accommodate 16 assisted living patients within the existing facility. The current zone only allows 10 assisted living patients, whereas this enables maximum utilization of the existing facility per adopted regulations for Group Living. (17C.330.120)

### **SMC 17G.025.010**

1. **Describe how the proposed amendment is appropriately addressed as a Unified Development Code Amendment.**

The UDC allows for private sector request on individual ownerships, in-lieu-of a city-wide update to the comprehensive plan or a sub-area plan. Neither of these options are available, leaving the private sector request as the only reasonable option.

2. **The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.**

As stated above, neither a Citywide update nor a sub-area plan are available to this area and request, nor are they timely for the applicants needs.

3. **The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.**

The proposal is reasonable because it is the perpetuation of a longstanding neighborhood land use. Public records show that the two residences were combined into a single nursing home in 1956. It changed names and ownerships four times prior to the current ownership, all of which were under various nursing home names.

The owner/applicant has met with the Cliff Cannon NC to share his intent to optimize assisted living within the existing facility. To which they expressed their support. In addition, he has met with the surrounding neighbors and



shared his plans. Accordingly, we do not expect any significant neighborhood opposition to this proposal, thus a normal process of review and comments is expected.

4. **Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.**

The annual process for amending the Comprehensive Plan is to keep the Comprehensive Plan alive and responsive to the community. Upon adoption of land use categories, it is then implemented through adopted zone and development regulations. In this case, the neighborhood has coexisted with this site's use as a nursing home since 1956, when the two residences were combined into one nursing home facility. It is the adopted Municipal Code at Chapter 17 that addresses the proposed use and requires an R-15-30 designation to allow full use of the existing facility for assisted living. (Note, the facility had been vacant for several years, thus losing its non-conforming right to 16 occupants. The applicant was the innocent purchaser of this property and its lapse of non-conforming rights. No other provisions of the MC enable this use without the R-15-30 designation.

The request is consistent with the CWPP. The CWPP encourages growth in urban areas where services and utilities already exist. When the site is further developed, the applicant or developer will be required to demonstrate that levels of service are maintained, as required by the CWPP. The CWPP also encourages the use of public transit and development where public transit is available. (Route 42 serves the subject property). It is important to note that the city has adopted development regulations and policies to implement the CWPP at the City level. Thus, consistency with the CWPP is achieved.

The application is consistent with the goals and policies of the Growth Management Act. The GMA encourages densification, in-fill and urban development and redevelopment in areas designated for urban growth and within existing city limits. The property is within the UGA and the city limits of Spokane.

The proposed change is consistent with the following goals of the Comprehensive Plan:



**Land Use 1.4** Higher Density Residential Uses.

*Direct new higher density residential uses to Centers and Corridors designated on the Land Use Plan Map.*

The subject property is located adjacent to and diagonally across from a large area currently designated R 15-30 and RMF zone. Notwithstanding, while currently designated R 4-10, it is within 100 ft. of an existing three-story apartment located NE of the subject facing 10<sup>th</sup> Avenue and across from several apartment conversions within the immediate neighborhood lying north and west of the subject property within this RMF zone. In other words, there is a mix of apartment uses throughout the immediate neighborhood, thus creating a land use trend and/or mix of housing.

Moreover, the subject property was originally within an R-4 Multi-Family Residence zone at the time of its conversion to a nursing home in 1956. A zone which not only allowed apartments, but hotels, lodges, hospitals, medical and dental offices. Hence the hodge-podge sprinkling of today's non-conforming uses within the current RSF zone. While some of these non-conforming uses cease to exist, they nonetheless, leave behind facilities that were altered for those uses and cannot be easily reformed into the primary uses of an RSF single-family zone.

Such is the case for the subject property, which has been used as a nursing home since 1956 and could be converted to a sixteen-bed assisted living facility within its four walls, but for the restrictions of the current zone, which only allows ten beds and/or occupants.

Accordingly, we would recommend that the request be tied to a Development Agreement to ensure the existing character of the neighborhood is preserved and thereby using the R 15-30 designation as a necessary tool to enable these additional 6 oc

**Land Use 1.12**

The proposed map change is consistent with LU 1.12.

*Ensure that public facilities and services systems are adequate to accommodate proposed development before permitting development to occur.*

Existing public facilities and services are adequately available to the subject property. The proposed assisted living use has minimal impacts on roads, transit, or trip generations. Nor are there significant impacts upon utilities and/or public facilities.



**LU 3 EFFICIENT LAND USE** Goal: *Promote the efficient use of land by the use of incentives, density and mixed-use development in proximity to retail businesses, public services, places of work, and transportation systems.*

The approval of this request enables the optimum utilization of an existing assisted living facility, whereas the current designation and zone limits the use to an occupancy at two-thirds its capability, (from 10 to 16 occupants). Thus, the approval promotes the efficient use of land.

**LU 5 DEVELOPMENT CHARACTER** Goal: *Promote development in a manner that is attractive, complementary, and compatible with other land uses.*

The subject site will be substantially maintained as is. Thus, no visual character changes will occur in terms of bulk, scale or use customarily associated with this site.

**Economic Development Goal 6**

The proposed map change is consistent with Goal ED 6, which recommends that development be located where infrastructure capacity already exist before extending infrastructure into new areas. In this case, all services are readily available.

5. **The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.** N/A, the proposal has not been submitted in the past.
6. **If this change is directed by state law or a decision of a court or administrative agency, please describe.** N/A

**End of Form**



**From:** [Dwight Hume](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Fwd: January meeting  
**Date:** Tuesday, November 26, 2019 1:27:02 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Request for 1117 W 10th Z19-505COMP

I will attend their nc meetings despite the circumstances, unique as they are.

Regards

*Dwight J Hume*

Land Use Solutions and Entitlement  
9101 N Mt. View Lane  
Spokane WA 99218  
509-435-3108

----- Forwarded message -----

From: **Dwight Hume** <[dhume@spokane-landuse.com](mailto:dhume@spokane-landuse.com)>  
Date: Tue, Nov 26, 2019 at 1:22 PM  
Subject: January meeting  
To: <[Patricia@pahansen.com](mailto:Patricia@pahansen.com)>, <[spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)>

Patricia, I have filed the annual amendment application for Seth Kenudson at 10th and Jefferson. I know he met with you earlier this fall, however per process of the City, we must come and share the application information with you. Can you schedule this for your January 7th meeting? Please advise as to when you can schedule me in.

Regards

*Dwight J Hume*

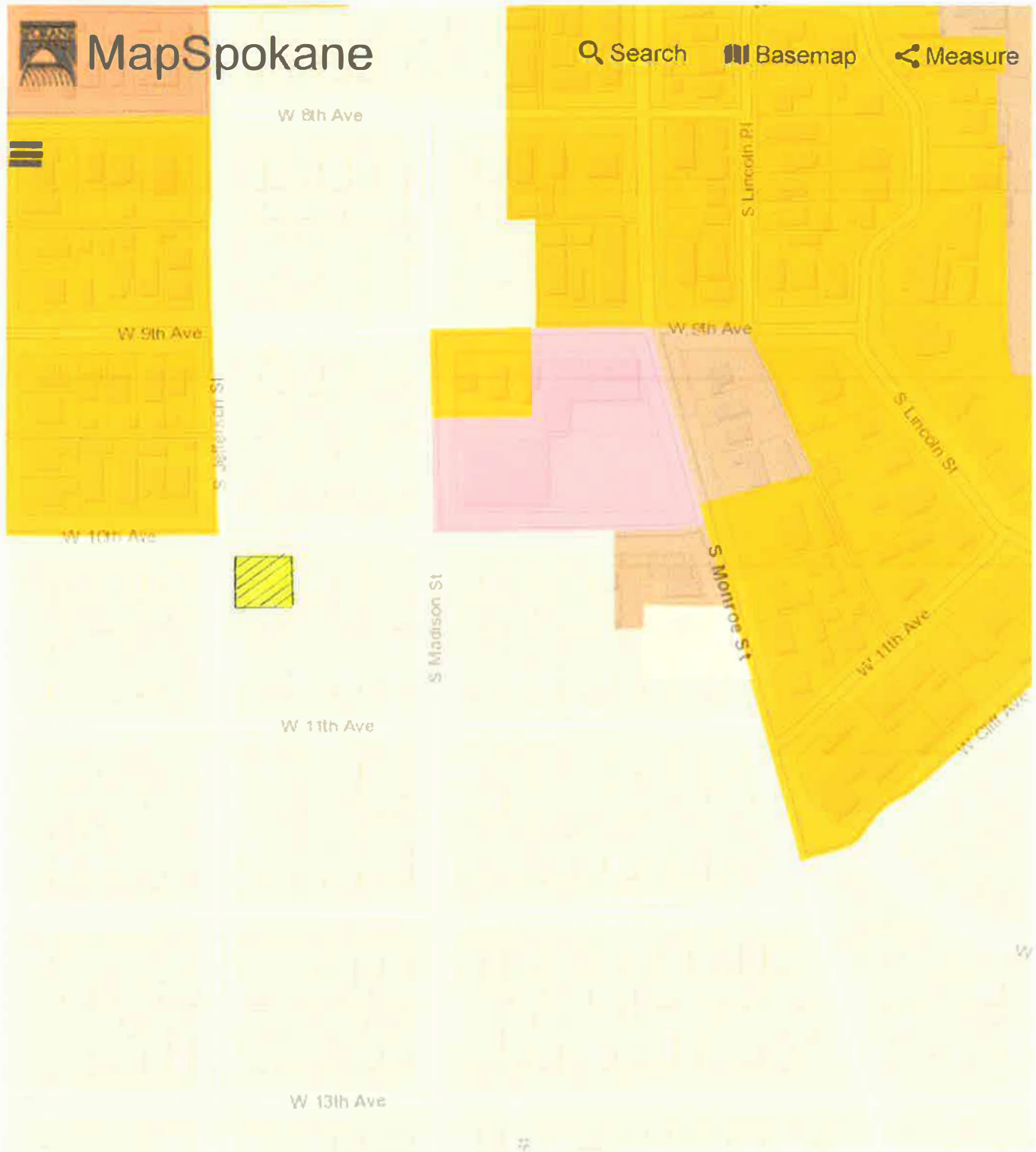
Land Use Solutions and Entitlement  
9101 N Mt. View Lane  
Spokane WA 99218  
509-435-3108





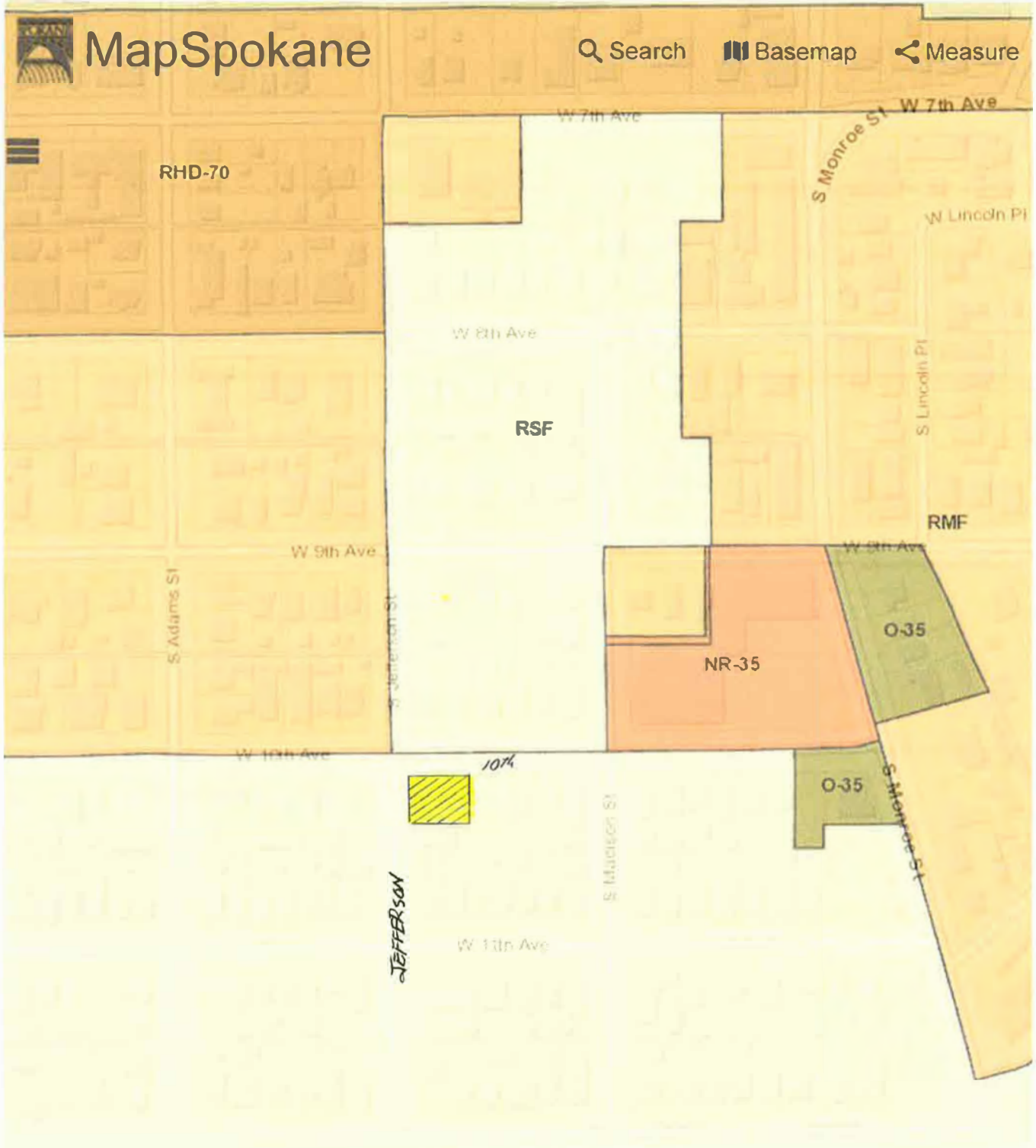
[Map Use Disclaimer](#)





[Map Use Disclaimer](#)





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[Map Use Disclaimer](#)



## Environmental Checklist

File No. Z19-505COMP

### Purpose of Checklist:

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

### Instructions for Applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

### Use of checklist for non-project proposals:

Complete this checklist for non-project proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For non-project actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**A. BACKGROUND**

1. Name of proposed project, if applicable: Z19-505COMP
2. Name of applicant: Dwight Hume of Land Use Solutions and Entitlement
2. Address and phone number of applicant or contact person:  
Land Use Solutions & Entitlement Dwight Hume agent  
9101 N Mt. View Lane Spokane WA 99218 509-435-3108
4. Date checklist prepared: March 29 2020
5. Agency requesting checklist: City of Spokane
6. Proposed timing or schedule (including phasing, if applicable):  
  
Project is a comprehensive plan amendment, modifying the land use plan map designation and zoning of the subject properties. This action is expected to be decided late fall or winter of 2020.
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. N/A, non-project action  
  
b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. No
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.  
In the event of any future development or renovation, the project will have to comply with applicable development regulations.
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. No other actions are pending
10. List any government approvals or permits that will be needed for your proposal, if known.  
Comp Plan amendment and zone change;
11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.  
This is an existing facility that was historically used as a nursing home. This existing facility is now planned to have 16 senior living quarters in the same space formerly used for the nursing home. No new structures are planned for the site. The development is contingent upon this request to amend the comp plan and zone change.



12. Location of the proposal. Give sufficient information to a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist.

The site is located at the SE corner of Jefferson and 10<sup>th</sup> Avenue.

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.)  
City of Spokane

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

- (1) Describe any systems, other than those designed for the disposal of sanitary waste, installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities).

N/A, non-project action

- (2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?

N/A, non-project action

- (3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems.

None

- (4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater?

None

b. Stormwater

- (1) What are the depths on the site to groundwater and to bedrock (if known)?

Unknown

- (2) Will stormwater be discharged into the ground? If so, describe any potential impacts?



Storm water is discharged to City of Spokane storm drains

**TO BE COMPLETED BY APPLICANT**

## **B. ENVIRONMENTAL ELEMENTS**

### **1. Earth**

- a. General description of the site (circle one): **flat**, *rolling, hilly, steep slopes, mountains, other:* \_\_\_\_\_
- b. What is the steepest slope on the site (approximate percent slope)?  
Unknown \_\_\_\_\_
- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. N/A, non-project action
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. No \_\_\_\_\_
- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill:  
N/A, non-project action \_\_\_\_\_
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.  
N/A, non-project action \_\_\_\_\_
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? N/A, reuse of existing facility \_\_\_\_\_
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: None \_\_\_\_\_

### **2. Air**

- a. What type of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial, wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.  
N/A, non-project action \_\_\_\_\_
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. \_\_\_\_\_  
None \_\_\_\_\_
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:  
None \_\_\_\_\_



### 3. Water

#### a. SURFACE:

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

No \_\_\_\_\_

- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

None \_\_\_\_\_

- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Non-project action \_\_\_\_\_

- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No, the project is served by City of Spokane water service \_\_\_\_\_

- (5) Does the proposal lie within a 100-year floodplain? \_\_\_\_ If so, note location on the site plan. \_\_\_\_\_

No \_\_\_\_\_

- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No \_\_\_\_\_

#### b. GROUND:

- (1) Will groundwater be withdrawn, or will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No \_\_\_\_\_

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sanitary waste treatment facility. Describe the general size of the system, the number of houses to be served (if



applicable) or the number of persons the system(s) are expected to serve.

The site is served by City of Spokane sewer service \_\_\_\_\_

c. WATER RUNOFF (INCLUDING STORMWATER):

(1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.  
Storm water will drain to the City of Spokane storm drain inlet

(2) Could waste materials enter ground or surface waters? If so, generally describe.

No \_\_\_\_\_

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water impacts, if any.

None

4. Plants

a. Check or circle type of vegetation found on the site:

\_\_\_\_\_ **Deciduous tree:** *alder, maple, aspen, other.*

\_\_\_\_\_ **Evergreen tree:** *fir, cedar, pine, other.*

\_\_\_\_\_ **Shrubs**

\_\_\_\_\_ **Grass**

\_\_\_\_\_ Pasture

\_\_\_\_\_ Crop or grain

\_\_\_\_\_ Wet soil plants, *cattail, buttercup, bullrush, skunk cabbage, other.*

\_\_\_\_\_ Water plants: *water lilly, eelgrass, milfoil, other.*

\_\_\_\_\_ Other types of vegetation.

b. What kind and amount of vegetation will be removed or altered? No changes due to use of existing improvements \_\_\_\_\_

c. List threatened or endangered species known to be on or near the site.

Unknown \_\_\_\_\_

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

To be determined at time of construction by others \_\_\_\_\_



## 5. Animals

- a. Circle any birds and animals which have been observed on or near the site are known to be on or near the site:  
birds: *hawk, heron, eagle, **songbirds**, other:* urban fowl \_\_\_\_\_  
mammals: *deer, bear, elk, beaver, other:* \_\_\_\_\_  
fish: *bass, salmon, trout, herring, shellfish, other:* \_\_\_\_\_  
other: \_\_\_\_\_
- b. List any threatened or endangered species known to be on or near the site.  
Unknown \_\_\_\_\_
- c. Is the site part of a migration route? If so, explain. Unknown \_\_\_\_\_
- d. Proposed measures to preserve or enhance wildlife, if any: \_\_\_\_\_  
None \_\_\_\_\_

## 6. Energy and natural resources

- a. What kinds of energy (**electric, natural gas**, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.  
The site contains an existing nursing home which is served with electrical and gas services. No additional services are anticipated \_\_\_\_\_
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. No \_\_\_\_\_
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:  
To be determined at time of construction \_\_\_\_\_

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. No \_\_\_\_\_
- (1) Describe special emergency services that might be required.  
None \_\_\_\_\_
- (2) Proposed measures to reduce or control environmental health hazards, if any:  
None \_\_\_\_\_
- b. NOISE:
- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?



None

- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

None

- (3) Proposed measure to reduce or control noise impacts, if any:

None

## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties? Site: Rental former nursing home  
West: Single family and apartments  
East: Single family and apartments  
South: Single family and apartments
- b. Has the site been used for agriculture? If so, describe. No
- c. Describe any structures on the site. The site has an existing blend of two former houses into one circa 1956.
- d. Will any structures be demolished? If so, which? No
- e. What is the current zoning classification of the site? RSF
- f. What is the current comprehensive plan designation of the site? R 4-10
- g. If applicable, what is the current shoreline master program designation of the site?  
N/A
- h. Has any part of the site been classified as a critical area? If so, specify. No
- i. Approximately how many people would reside or work in the completed project?  
Non project action
- j. Approximately how many people would the completed project displace?  
None
- k. Proposed measures to avoid or reduce displacement impacts, if any: none
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: Compliance with all applicable development regulations if required by a subsequent CUP approval.



## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high-, middle- or low-income housing. None
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. None
- c. Proposed measures to reduce or control housing impacts, if any: None

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?  
N/A existing structure to remain
- b. What views in the immediate vicinity would be altered or obstructed?  
None
- c. Proposed measures to reduce or control aesthetic impacts, if any:  
None

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?  
Existing lighting only inside.
- b. Could light or glare from the finished project be a safety hazard or interfere with views?  
No
- c. What existing off-site sources of light or glare may affect your proposal?  
None
- d. Proposed measures to reduce or control light and glare impacts, if any: None

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? None
- b. Would the proposed project displace any existing recreational uses? If so, describe. No
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:  
None

## 13. Historic and cultural preservation



- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. No
- b. Generally describe any landmarks or evidence of historic archaeological, scientific or cultural importance known to be on or next to the site.  
None
- c. Proposed measures to reduce or control impacts, if any: \_\_\_\_\_  
None

#### 14. Transportation

- a. Identify public streets and highways serving the site and describe proposed access to the existing street system. Show on site plans, if any. 10<sup>th</sup> and Jefferson
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?  
Yes it is served one block to east at 10<sup>th</sup> and Madison
- c. How many parking spaces would the completed project have? How many would the project eliminate?  
To be determined at time of CUP submittal
- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets not including driveways? If so, generally describe (indicate whether public or private). No new roads or streets are needed.
- e. Will the project use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. No
- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak would occur.  
Most senior residents will not drive. Staff would be the normal additional traffic on three shifts 24-7.  
  
*(Note: to assist in review and if known indicate vehicle trips during PM peak, AM Peak and Weekday (24 hours).)*
- g. Proposed measures to reduce or control transportation impacts, if any:  
None

#### 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe. No new or expanded services would be needed or generated by this proposal



- b. Proposed measures to reduce or control direct impacts on public services, if any: None

**16. Utilities**

- a. Bold existing utilities currently available at the site: ***electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.***
- b. Describe the utilities that are proposed for the project, the utility providing the service and the general construction activities on the site or in the immediate vicinity which might be needed.  
None



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020 Signature: Dwight J Hume

***Please Print or Type:***

Proponent: Dwight J Hume Address: 9101 N Mt. View Lane

Phone: 509.435.3108 Spokane WA, 99218

Person completing form (if different from proponent): Same

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS**  
***(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage or release of toxic or hazardous substances; or production of noise?

No impacts from Assisted Living are foreseen

Proposed measures to avoid or reduce such increases are:

Compliance with applicable development standards at the time of renovation

2. How would the proposal be likely to affect plants, animals, fish or marine life?

No impacts to natural flora and fauna since this is an urban site.

Proposed measures to protect or conserve plants, animals, fish or marine life are:

None

3. How would the proposal be likely to deplete energy or natural resources?

While some additional resources would be required during redevelopment, these would be similar to those required of any construction project.

Proposed measures to protect or conserve energy and natural resources are:

None

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands?

No impact

Proposed measures to protect such resources or to avoid or reduce impacts are:

None

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

Development would comply with applicable development regulations, as imposed by development regulations. This is not affected by shoreline management.

Proposed measures to avoid or reduce shoreline and land use impacts are:

See above comment

6. How would the proposal be likely to increase demands on transportation or public services and utilities?



There would be no extraordinary demand upon utility services \_\_\_\_\_

Proposed measures to reduce or respond to such demand(s) are:

To be determined at time of construction \_\_\_\_\_

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment.

No impacts are foreseen \_\_\_\_\_



C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Non-significance that it might issue in reliance upon this checklist.

Date: March 29, 2020

Signature: *Dwight J*

*Hume*

**Please Print or Type:**

Proponent: Dwight J Hume Address: 9101 N Mt. View Lane

Phone: 509.435.3108 Spokane WA, 99218

Person completing form (if different from proponent): Same

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

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- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z19-505COMP

**PROPONENT:** Lark Homes, LLC (Agent: Dwight Hume, Land Use Solutions and Entitlement).

**DESCRIPTION OF PROPOSAL:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the land use designation (Map LU-1 of the Comprehensive Plan) and zoning designation (Official Zoning Map of the City of Spokane) for one property located in the Cliff Cannon neighborhood.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal concerns a single parcel (Parcel 35193.1405), located immediately southeast of the intersection of W 10<sup>th</sup> Ave and S Jefferson St. The proposal would affect an area of approximately 0.16 acres. The parcel is located at 1117 W 10<sup>th</sup> Avenue.

**Legal Description:** North 75 feet of lots 6-7, block 2, Booges Addition to the City of Spokane in Section 19, Township 25 North, Range 43 East.

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

☐ There is no comment period for this DNS.

☐ This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.

☒ This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:** 

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

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**Spokane Tribe of Indians  
Tribal Historic Preservation Officer**

P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z19-505COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer





# LAW OFFICE OF MILTON G. ROWLAND, PLLC

ATTORNEY AND COUNSELOR AT LAW

July 20, 2020

Kevin Freibott  
Department of Neighborhood and Planning Services  
City of Spokane  
808 W Spokane Falls Boulevard  
Spokane, WA 99201

VIA Email and United States Mail

Re: Response and comments regarding Project Z19-505COMP

Dear Mr. Freibott:

I am writing on behalf of Mr. and Mrs. Landry, who live directly south of the subject property, adjacent to it. This letter is intended to voice some of my clients' objections to Project Z19-505COMP ("the Project"). Before the deadline stated for submittal of comments (August 7, 2020), we will very likely submit further comments, which are intended to be cumulative in nature. This letter is intended to reach Commission members prior to the July 22 meeting.

We would like to begin by pointing out that what my clients really want, after living on 11<sup>th</sup>-10<sup>th</sup> and Jefferson for over 30 years, is for the young families in the neighborhood to enjoy what my clients enjoyed—a quiet, neighborly residential neighborhood with amenities close by and low enough density that their children can play in the yard without fear or distraction. My clients are perhaps the oldest and longest-tenured neighbors. They raised children in their now-restored property on Jefferson, and hope that the quiet, residential character of the neighborhood can be maintained.

The City Council apparently agreed; its 2006 downzone in the neighborhood set the limit of density in the neighborhood to the density already existing in 2006. It is rare to see a neighborhood downzoned, like ours was in 2006. The change must be seen as intentional, and the intention was clearly to maintain the quiet residential character of this neighborhood.

Further, there are few garages in the neighborhood, which means that many if not most families in our neighborhood rely on on-street parking. The proposed changes will make parking very difficult, if not impossible, for many of our neighbors.



Nearby homes are *not* multi-family. While some homes in our neighborhood have been converted into "flat" or apartment-like rentals, most have not, including the closest neighbors to this Project. The two closest properties, including ours have been restored, at substantial expense. The home next door to the Project is on the historic preservation list.

10<sup>th</sup> Avenue is not an arterial. It *is* a bus route, but it is not an arterial, and has never been a busy street. That will of course change if 35 people can live there, on the Project site.

Similarly, the Project property has not been a "nursing home" in a quarter century. The landowner has rented rooms, and let the property deteriorate. My clients have had to trim back the 5' high weeds on their side of the property.

Most nearby owners are *not* in favor of this change. While my clients did not voice objections while they were visited, out of courtesy, they have spoken to several neighbors with the same concerns as are voiced here.

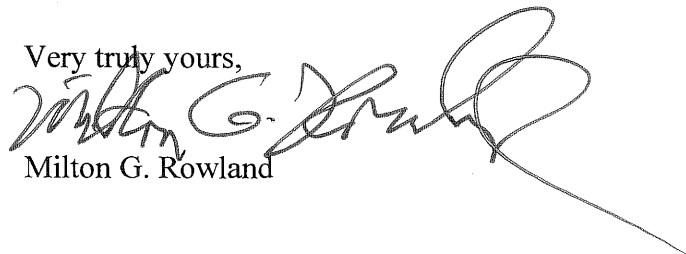
My clients are suspicious of the motives of the owner. This is not because my clients believe that the owners are bad people. It's just that the modern nursing home (see <https://www.phcconline.org/facility-services>) relies on economies of scale that cannot be matched on the Project property, 1/16 of an acre.

As stated, my clients restored their home, at substantial expense. They are very concerned that their investment in their home, and the neighborhood, could be lost if the Project is approved.

Finally (for now), nursing home residents do not contribute to the quality of life in a neighborhood like ours. They will not meet for coffee at Huckleberries or the Rocket Bakery. We will not see them on the street to talk about the weather or the pandemic. They should be housed with greater comfort than this property could allow, with more assistance than this property could sustain. Instead of contributing to the neighborhood, they will simply (and without any intent to do so), place additional strains on the neighborhood in terms of parking, traffic, and noise. It is not fair to them, and it is not fair to us, to upzone this property to allow 35 people to be packed into a property that really should be home to a family.

Thank you for your courteous attention to this matter.

Very truly yours,

A handwritten signature in black ink, appearing to read 'Milton G. Rowland', is written over the typed name. The signature is fluid and cursive, with a long horizontal stroke extending to the right.

Milton G. Rowland



July 19, 2020

Kevin Freibott

**Department of Neighborhood and Planning Services**  
City of Spokane  
808 W Spokane Falls Boulevard  
Spokane, WA 99201

VIA Email and United States Mail

Re: Response and comments regarding Project Z19-505COMP

Dear Mr. Freibott:

My name is Glen Landry and I live at 1011 South Jefferson which adjoins the south side of the subject property.

My wife and I listened in to your meeting a few weeks ago. Here are a few thoughts regarding what was said.

1. Regarding rental properties in the area, neither adjoining home has ever been or was used as a rental. This goes back to 1904. We have completely restored our home and the house on the east side of the subject property was restored and put on the historic registry.
2. There seems to be some misinformation regarding senior living on the property. This property has not been used for senior living for over 20 years. I have met four guys living there and the oldest is around 61. I would guess that the youngest is less than 30.
3. The petitioner told me a couple of years ago that he was going to make a halfway house out of the property. I expressed my distress over this and he no longer uses the term "halfway house". It is my opinion that the petitioner uses the term "senior" to curry favor with you and the neighborhood.
4. No one spoke about the possibility that the new zone change could open the pathway to a halfway house where we would have no input.

I know of three cars parked on the property and I cannot see the north side where there may be more. Additional tenants will mean more cars. I question whether his current rentals comply the parking ration right now. There is no room for parking.

Thanks for this opportunity to voice my concerns. I understand from your meeting that my comments will be forwarded ahead of the next meeting on Tuesday.

Respectfully,

/s/  
Glen M. Landry



To: Spokane Plan Commission and the Spokane City Council

Re: Z19-505COMP

From: Wai Landry, property owner of 1011 S Jefferson St., Spokane WA 99204

I have been a volunteer for Hospice of Spokane since retirement in 2015.

As a volunteer, I visit clients and provide emotional support for them. In the past five years, I had the opportunity to visit a few assisted living homes as well as nursing homes where my clients resided. They range from large facilities to family run home with as few as 4 patients.

What stands out most with the smaller facilities (comparable to the size of the building at 1117 W 10<sup>th</sup>) are space and location. They are usually on the outskirts of town and on acreage. They all have an outdoor area for their patients. It might be a deck, a patio, a courtyard or a full backyard. Some of them even have a view of the trees and mountains. Another common feature is a gated or fenced property.

In my opinion, the property at 1117 W 10<sup>th</sup> Ave is less than ideal as an assisted living/senior home for the following reasons:

1. There is no space to provide a grassy backyard, court yard, outdoor patio or balcony for the residents to take in some fresh air and sunshine when weather permits.
2. Patient's mobility is limited to walking from the bedroom to the dining room or sitting room. For the unfortunate bed ridden patients, the only view they see is the siding of another house through their bedroom window.
3. The entrance to the building is only a few feet from the street. If a resident happened to get out, he or she would be on the street in no time flat.
4. Parking is a big problem. There is only a driveway alongside the building with no parking lot. Where are the visitors, staff and delivery persons going to park their vehicles?

In summary, I think there are better places to operate an assisted/senior home than at 1117 W 7<sup>th</sup> Ave.

/s/ Wai Landry



**OPPOSITION TO COMPREHENSIVE PLAN  
AND ZONING AMENDMENT**

We the undersigned residents of the neighborhood in which Z19-505 COMP is currently before the Planning Commission, oppose the project for its impact on our neighborhood.

| Name             | Address                      | Signature         | Date         |
|------------------|------------------------------|-------------------|--------------|
| Katherine Widing | 1215 W 10 <sup>th</sup>      | K. W              | 17 July 2020 |
| Barbara Stepien  | 1117 S. Jefferson            | Barbara Stepien   | 7/17/20      |
| Rory Crowley     | 1216 W. 11 <sup>th</sup>     | Rory Crowley      | 2-17-20      |
| Laura Crowley    | 1211 W 11 <sup>th</sup>      | Laura Crowley     | 7/17/20      |
| Geraldyn Dust    | 1215 W 11 <sup>th</sup>      | Geraldyn Dust     | 7-17-20      |
| Suzanne Levitt   | 1323 W 11 <sup>th</sup>      | Suzanne Levitt    | 17 Jul 2020  |
| [Signature]      | 1720 W 11 <sup>th</sup>      | Nicholas Reynolds | 7/17/2020    |
| [Signature]      | 1212 W <sup>th</sup> Ave     | David Johnson     | 7/17/20      |
| Judy Madden      | 1204 W. 11 <sup>th</sup> Ave | Judy Madden       | 7/17/20      |
| Alex Scott       | 1124 W 11 <sup>th</sup> Ave  | [Signature]       | 7/18/20      |
| Hedra C. Progne  | 1112 W. 11 <sup>th</sup>     | Hedra C. Progne   | 7/18/2020    |
| Carissa Landry   | 1048 W 11 <sup>th</sup>      | Carissa Landry    | 7/18/2020    |
| Don Craig        | 1039 W 10 <sup>th</sup>      | Don Craig         | 7/18/2020    |
| [Signature]      | 1031 W 10 <sup>th</sup> Ave  | [Signature]       | 18 July 2020 |
| Suzanne Chen     | 1037 W 10 <sup>th</sup> Ave  | Suzanne Chen      | 18 July 2020 |
| Meagan Ciesla    | 1107 W. 10 <sup>th</sup> Ave | Meagan Ciesla     | 7/18/20      |
| Ferdinand Gerson | 1128 W 10 <sup>th</sup> Ave  | [Signature]       | 7/18/20      |



**OPPOSITION TO COMPREHENSIVE PLAN  
AND ZONING AMENDMENT**

We the undersigned residents of the neighborhood in which Z19-505 COMP is currently before the Planning Commission, oppose the project for its impact on our neighborhood.

| Name              | Address          | Signature         | Date      |
|-------------------|------------------|-------------------|-----------|
| Jack M. Koentopp  | 1207 W 10th Ave  | Jack M. Koentopp  | 7-19-2020 |
| James C. Bergdahl | 917 S. Adams     | James C. Bergdahl | 7/19/2020 |
| James C. Bergdahl | 1228 W. 11th Ave | James C. Bergdahl | 7/19/2020 |
| Sherwood Ort      | 1219 W. 9th Ave  | Sherwood Ort      | 7/19/2020 |
| James Barz        | 1211 W. 9th Ave  | James Barz        | 7/19/2020 |
| Roger Takiguchi   | 1129 W 9th Ave   | Roger Takiguchi   | 7/19/20   |
| AUSTIN LARUE      | 1118 W 10TH AVE  | Austin Larue      | 7-19-20   |
| Eileen Martin     | 1124 W. 10th     | Eileen Martin     | 7/19/20   |
| JACK ELLIOTT      | 1204 W 10TH      | Jack Elliott      | 7/19/20   |
| Ben Kempf         | 1130 W 11th Ave  | Ben Kempf         | 7/19/20   |
| Allison Johnston  | 1120 W. 11th Ave | Allison Johnston  | 7/20/20   |
| Brenda Garberg    | 1108 W. 11th     | Brenda Garberg    | 7-20-20   |
| Sam Van Vleet     | 1040 W. 11th     | Sam Van Vleet     | 7/20/2020 |
| Jack Mear         | 1016 S. Madison  | Jack Mear         | 7/20/2020 |
| Amanda Renaud     | 1111 W. 10th     | Amanda Renaud     | 7/20/20   |
| Brittany O'Neal   | 1115 W 10th Ave  | Brittany O'Neal   | 7/20/20   |
| Dewsh Sirhan      | 1207 W 9th Ave   | Dewsh Sirhan      | 7/20/20   |



We the undersigned residents of the neighborhood in which Z19-505 COMP is currently before the Planning Commission, oppose the project for its impact on our neighborhood.

Date \_\_\_\_\_

7/20/2020

7/20/2020

7/20/2020.



**From:** [Tom P. May](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Comment Land use change Ref #: Z19-505Comp  
**Date:** Monday, July 20, 2020 5:37:34 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Re: 1117 W 10<sup>th</sup> /Z19-505COMP

Hello Mr. Freibott:

Living 40 years at my address on 9<sup>th</sup> avenue I have witnessed the neighborhood become more single family friendly. This comment is in opposition to the pending application for the residence/business on 10<sup>th</sup> avenue. To grant the requested change will significantly and adversely affect our neighborhood. My understanding is that the petition for Comp. Plan/zone change is primarily aimed at increasing the re-sale price of this property. Granting the petition will be contrary to the current neighborhood zoning and Comprehensive Plan and to the best interests of the surrounding property owners, many of whom purchased their homes based on the historic designations and the changes in zoning toward single family residences. The parking and traffic on 10<sup>th</sup> will be hurt by increasing the residence allowance by 200%. Plus my opinion is that to grant the petition will reduce the property values of the surrounding single family homes.

Thank you,  
Tom P. May

**Tom P. May, Attorney at Law**  
**1117 West 9th Ave.**  
**Spokane, WA 99204**  
**(509) 981-3779**



**From:** [Judy Madden](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Z19-505COMP - Opposition to zoning change at 1117 W. 10th Avenue  
**Date:** Tuesday, July 21, 2020 1:10:52 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Hello Kevin -

My husband (Tom Sutherland) and I live at 1204 W. 11th Avenue, a block away from the property at 1117 W. 10th Avenue, requesting a zoning change from RSF 4-10 to RMF 15-30. We are opposing this zoning change for several reasons. I would like to comment that I think Seth has good intentions, but may be in a bit over his head and also maybe a little naive. I also think an eldercare facility in this neighborhood isn't a bad idea, just not at that particular property. I don't know if Seth has ever owned a 100+ year old home, but our home was built in 1906 and we have done extensive restoration and remodeling and it is not inexpensive nor easy, and it is always going to cost more (and maybe a lot more) than you anticipate. Our home has "good bones" and is worth the investment; I do not believe that the 1117 W. 10th property falls into that same category, just by looking at the exterior.

- Seth Knutson has indicated he wants to transform this property into an eldercare facility - we have the following concerns and questions:
  - Parking for staff and visitors - most people on that block already park on the street. There is a three car stacked or tandem parking which is not practical. Seth has said he envisions neighbors walking or biking to the facility to visit their loved ones which is an unreasonable assumption.
  - Seth has said he plans to get a bank loan when/if he receives the zoning change, for \$100,000. Anybody who has an old house knows that is a way too low estimate - I would think he would need to pay maybe as much as \$500,000. Just by looking at the house, you can tell it is in terrible disrepair. Seth has indicated he doesn't not want to get any preliminary bids prior to a zoning approval. I honestly don't know what bank would give him a loan for the amount he needs to turn that property into an eldercare facility. Rather than spending that much money to transform the property into an eldercare facility, I would think it would be better just to tear it down and rebuild. All of us have older homes - we know that once you start a project, you find a dozen more and before you know it, the cost has doubled or tripled!
  - I assume an eldercare home is considered a medical facility and along with that designation comes a lot of regulations. Although I have not been in the property, others who have tell me it is in terrible shape. Seth claims he has made improvements on the inside, but I don't know of anybody who can vouch to that claim.
  - In the seven years that Seth has owned this property, he has made no



improvements. He does maintain the lawn which we appreciate. He could have done some of the required work such as scraping paint and re-painting.

- What we (and many of our neighbors) would like to happen:
  - This neighborhood already has too many multi-family dwellings.
  - In the ten years we have been in our home, we have been thrilled to see many of the single family homes remodeled, and also some flipped and re-sold. It is nice to see families with small children moving into the neighborhood.
  - We would like to see either Seth sell the home or "flip it" and sell it as a single family home. The flat-roofed addition could be removed for a garage, yard or garden. I think he could make a good profit by following that approach, similar homes in the neighborhood have been flipped and the developers have made money.

I think that's it - thanks for your attention to this matter and please let me know if you have any questions.

Judy Madden

509-808-3857



From: Ali Johnston <alisonkatejohnston@gmail.com>  
Sent: Monday, August 3, 2020 1:33 PM  
To: Freibott, Kevin <kfreibott@spokanecity.org>  
Subject: z19-505comp

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Reference: z19-505comp

Hello Kevin!

I am the owner of 1120 W. 11th Ave, Spokane, WA 99204. I am writing in regards to the proposed amendment of land use from Residential 4-10 to Residential 15-30 for the address 1117 W10th Ave, Spokane, WA 99204. I strongly oppose the proposed change—there are already many apartments and multi-family properties in this neighborhood, causing an issue with parking and making single family residences' value decrease (like mine). These properties also have brought an increase in crime in the area.

Thank you for your time. Please let me know if you have any questions.

Alison Johnston

P.S. In the letter I received, it stated that comments written with be made part of the public record. Would you please redact my address from that? Thank you.



**From:** [deanna.murdy](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** 1117 W 10th  
**Date:** Monday, August 3, 2020 7:40:15 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Just signed a petition regarding the situation happening in our neighborhood. My response is No, No, No. We have been hit hard with the whole 5G installed lowering our property values. This is a beautiful neighborhood and that is why I moved here. Please!!! My address is 1220 S Adams and our neighborhood feels strongly against what this man is trying to do.

Regards, Deanna Murdy



**From:** [Jerry Widing](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Reference Z19-505COMP  
**Date:** Thursday, August 6, 2020 9:04:08 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Regarding the zoning change at 1117 W. 10th Avenue, Reference Z19-505COMP

We live just 3 houses away from this property. We are concerned about a number of things if this is given a greater multi family zoning. I say greater since it has been multi family since the current owners bought the property. Shortly after purchasing it, saying they were going to set up a retirement/nursing home, they started advertising rooms for rent for \$300. This to me is not a single family home, so I am confused as to why it seems to be currently listed as such.

If the current owner is simply trying to change the zoning to increase the value of the property, that is unfair to the entire neighborhood.

This neighborhood already has a parking problem, and this zoning change would just make it worse.

This would be a very negative change for the neighborhood.

Thanks,

Jerry Widing

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**“Travel is fatal to prejudice, bigotry, and narrow-mindedness, and many of our people need it sorely on these accounts. Broad, wholesome, charitable views of men and things cannot be acquired by vegetating in one little corner of the earth all one's lifetime.” — Mark Twain**



**From:** [Austin LaRue](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** 1117 W 10th Ave rezoning  
**Date:** Friday, August 7, 2020 4:51:09 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Sir,

In regards to the rezoning of this address from a single family to a multi-family home. I, 1118 W 10th ave, respectfully disagree with the proposal. This is a historic area to lower south hill and deserves to look like it. The house hasn't seen improvements since I moved onto the block, and with a higher headcount in the home I feel the property will only become more tattered. Adding assisted care will only increase road and visitor traffic, leaving parking shorter than it already is. I would hate to see my neighbors and my own property value decline because of this action.

Very respectfully,

Austin LaRue



**From:** [Anne Putney](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Opposition to zoning change at 1117 W. 10th Ave.  
**Date:** Friday, August 7, 2020 4:52:57 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

To whom it may Concern,

I am writing in opposition to the zoning change for the following property:

Address: 1117 W. 10<sup>th</sup> Ave

Parcel: 35193.1405

Application/Permit Number: Z19-505COMP

My name is Anne Putney. My husband Damian and I have lived in the neighborhood for 16 years and our house is 2 blocks from the above-mentioned property. The Cliff-Cannon neighborhood is a very special place and in the time we have lived here, it has been truly amazing to see the transformation and restoration that has happened here. Personally, we have fixed up many properties and have been very instrumental in the revitalization of this neighborhood. We are deeply invested here and it is our vision to continue to help with the restoration of the Cliff Cannon neighborhood. That is why when we saw the notice of application for the zoning change at 1117 W. 10<sup>th</sup>, we, along with most of our neighbors, were extremely concerned.

Our first concern is that if this property is allowed to be re-zoned for 15-30 occupants, then what is to stop the next property from trying to do the same? Many years ago, this portion of the neighborhood was rezoned to prevent and protect this from happening. If this goes through, it is my understanding that this house would then be grandfathered in, so if the current owner decided to sell, it could one day have the potential to be a huge detriment to our neighborhood.

We are also very concerned for the plans the owner has for this property. The property is in very poor condition and in my professional opinion, does not have very suitable living conditions, especially for, as his plan states, the elderly. In the 7 years that they have owned it, they have done little or nothing to improve it, which to those of us who live here is very frustrating. We are also concerned about the impact that having a 24 hour facility will have on the neighbors well being, property values, parking etc.

I believe that the majority of the neighbors that live near this property have signed a petition in opposition to this zoning change, and we all have signed this for good reason. We are not against change and progress, but this is not the type of proposal that is going to help improve the neighborhood and make it a better place to live. I hope you take time to consider all of these concerns as if you were a neighbor who lived near this property. Please take this into consideration when making your decision and thank you for your time.

Sincerely,



Anne Putney  
Putney Building Company  
509-280-4134  
[www.putneybuildingcompany.com](http://www.putneybuildingcompany.com)



**From:** [Damian Putney](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Letter of Opposition to Permit #: Z19-505COMP  
**Date:** Friday, August 7, 2020 5:00:18 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Address: Regarding 1117 W. 10th  
Parcel #: 35193:1405  
Permit #: Z19-505COMP

Hi, my name is Damian Putney and I am against the proposed rezoning of this property.

**About Me:**

I have lived in this neighborhood for 17 years with my wife Anne and have raised 3 children: Max 13, Miles 11, and my daughter Grace 8. In 2003, we bought a 1905 craftsman home on 12<sup>th</sup> and Adams, which had been turned into an 8 unit apartment in the 1940's and the landlord had lost control of the property, tenants were not paying rent, squatting, doing drugs etc. and the property had fallen into major disrepair and neglect. But we knew the property had potential and fell in love with the neighborhood, old houses, mature trees, and nearby parks, coffee shops and restaurants and made the decision to purchase the property and restore it to its original glory. We also got our home registered on the Historic Registry with the help of Linda Yeomans. It was a lot of work, but we love this neighborhood and it has been absolutely worth the efforts.

**Our Business:**

My wife Anne and I own and operate a construction company, Putney Building Company specializing in high end remodel work and new construction and we do literally all of our business on the South Hill of Spokane. Our depth and understanding of old houses, buildings, and how to restore them is virtually unparalleled. We are also proponents of development where it makes sense. For instance, we purchased a 4 plex, which had extensive damage from a fire in one of the units and had the opportunity to purchase it via short sale from the bank. After discovering the extent of the damage caused by the fire, we decided it would be best to demolish the building and build something in its place that fit the neighborhood and honored the architecture and style of the neighboring properties and built it to be as close to a turn of the century home as possible. The property is located at 1110 S. Adams St. and has been regarded by many to be one of the best new homes ever built on the South Hill. Folks with the Building and Planning Department actually use a picture of the home in their slide shows as a representation of how new homes can be built with taste and craftsmanship in an effort to fit in and blend in with their surroundings.

**My Knowledge of the Building at 1117 W. 10<sup>th</sup> Ave.**

I had an opportunity to tour this building when it was on the market 10 years ago. And my professional opinion of the building is that it needs \$300,000 in improvement to be a great building with solid mechanicals, finishes, etc., regardless of use. The owner has mentioned that he thinks he can fix it up for \$100,000, which is severely underestimated.

**Why I am Against Rezoning of this Property**



I am against the rezoning of this property because it's use does not fit in with the single family use of the neighboring properties. The only person who benefits from the rezoning is the owner, and nobody else in the neighborhood supports it being rezoned.

Thank You,

Damian Putney



**From:** [rogertak@earthlink.net](mailto:rogertak@earthlink.net)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Eileen Martin: tmwiseman@earthlink.net](#)  
**Subject:** Z19-505COMP Zoning 1117W 10th Ave  
**Date:** Friday, August 7, 2020 7:41:19 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Mr Kevin Freibott  
Assistant Planner  
City of Spokane

I am property owner of 1129 W 9th Ave parcel 35193.1005 with concerns for zone change at 1117 W 10th Ave parcel 35193.1405 Z19-505COMP. My property is one block north of the zone change at 1117 W 10th Ave. My concerns:

- 1.) Increase RSF to RMF, there are 10 multi resident buildings within one block of 1117 W 10th Ave. The limited off street parking for each multiple resident building has the area congested. Recent renovations of single family residence to multiple residents housing has not required adequate off street parking; increasing more street parking in this residential area. An increase at 1117 W 10th would create more street parking either for resident, visitor or employee parking within a one block radius; note the shopping center at Huckleberries, Ace Hardware, etc is within a block of 1117 W 10th Ave and employee use street parking during the day time.
- 2.) The increase for more than 20 residents at the address should have the consideration of the planned usage of the building; ie., nursing home, individual room rental, or interim housing(half-way residence). At this time, health concerns should be an important factor on the use at the address, the adjacent area has more than 10 single family residences with school age children.
- 3.) The current condition of the building is in need of maintenance; roof needs repair, repairs to the structure, general building maintenance. I purchased my property in 2011 and have replaced the roof, installed new yard fencing, 2 years of building repairs/maintenance, extensive landscaping and garden plantings. The owner of 1117 W 10th Ave has done limited maintenance and if an increase of residents at the address is approved will there be improvements to the property that should have been done as an ongoing maintenance program.

I am concerned in regards to the proposed use of the property and the effect on the residential area and street parking. This South Hill area has slowly become more family oriented over the past several years, with prior years having experienced drug problems and issues with the multi-residents properties(drug traffic, etc). Hopefully the planning commission will consider the concerns of the residents and the actual use for the property with its effect on the area.

Thank You for Your Consideration  
Roger Takiguchi  
1129 W 9th Ave Parcel 35193.1005  
Spokane WA 99204  
[rogertak@earthlink.net](mailto:rogertak@earthlink.net)  
(509) 714-2691



**From:** [Katherine Widing](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** ref: Z19-505COMP HOUSE@1117 W 10th Ave, Spokane WA 99204  
**Date:** Friday, August 7, 2020 11:13:22 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

\*\*\* I am re-sending this email. I sent it on Thursday 6th August around 2pm, and it came back to me tonight as "undeliverable". I have no idea what happened, but I realize now that the deadline passed a few hours ago. I hope you will still accept my comments. I apologize for this cyberspace glitch.

Dear Mr Freibott,

I am a neighbor of the house at 1117 W. 10th Ave where an application for a multi-family dwelling permit has been requested. I am most concerned for many reasons.

Firstly, about 5 years ago, I met the owner who said he intended to turn the property into a nursing home. He said that he had plans in place and then a few days later there was a sign in front of the property "Rooms for Rent: \$300 per month". I was surprised, and since then there has been a steady stream of "renters". I wonder now, why he is applying for the multi-family status, and if it is granted if indeed it will become a nursing home, or some sort of halfway house which is inappropriate for a residential, and very family oriented area such as this.

I am concerned about the parking situation for the property. Currently there is a parking problem on the streets around the property. I live on 10th and there are too many cars parked on the street as it is, that we, in the block west (at 1215 W 10th, between Jefferson and Adams), can almost never park in front of our house. If our friends come to visit they have to park at least a block away. This is an issue, but the main problem that frequently occurs is that people are inconsiderate to our driveway parameters and they park partially in front of our driveway, or ignore our driveway altogether, and hence we are often blocked in and have been delayed in leaving for appointments and exiting the driveway. I would like to know how they plan to provide parking, plus what the city requires for off street parking spaces for a property such as this.

My other question is why after approx 5 years is the owner applying for the multi family status? Does he really intend to alter the property to renovate it to become a nursing home, or is he planning to sell it and this status will garner a higher price? If sold, then we begin this battle with a new owner, or can we? The property is in a poor state of disrepair and requires a substantial amount of money to upgrade to meet (what I expect) would be the level needed to attain the permits to reach the required standards and codes to meet approval for said type of property.

This is a residential area and inappropriate for such a commercial venture, and for this



dwelling to be a multi-family property. Thank you for considering the objections of our neighborhood.

Please confirm that this email was received prior to deadline. (this should now read - Please confirm that you will accept my email, which is just a few hours late due to no fault of my own)

With thanks,  
Katherine Widing  
1215 W. 10th Ave,  
Spokane, WA 99204  
chocovelo@hotmail.com



**From:** [Watkins, Kandace](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: Comment Letter on Comp Plan Amendments?  
**Date:** Thursday, February 27, 2020 2:31:12 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Candace

Thank you for taking the time to reply. I did know the address referenced was not recommended. This application has support of the members of cliff/cannon executive committee. I am willing to do a development agreement to limit use to senior assisted living care and keep the envelope of the building the same size. There are no other planning mechanisms to fully utilize the historic use of the building. It has been senior care since the 1950s. I look forward speaking in person on Monday. Thank you again for your time and service.

Seth Knutson

> On Feb 24, 2020, at 9:23 PM, Mumm, Candace <[cmumm@spokanecity.org](mailto:cmumm@spokanecity.org)> wrote:

>

> Hi Seth,

>

> I typically don't meet with folks on individual comp plan amendments.

> We have a specific public process and it's important our staff, plan commissioners and Council have access to all comments and concerns.

> That being said, the address you referenced is not one the Council Docketing committee is recommending.

> (See documents in our upcoming agenda at website below)

>

> We will likely be voting on which Comp Plan Amendments to move forward (or not) for full Plan Commission review and recommendation the night of Monday, March 2nd. You are welcome to come down and speak to all of us when that item comes up for a vote. We will only be voting on which amendments to have public hearings on in the future. Our final decision would not come until likely next fall. You are also welcome to write us an email in support or opposition and I'm happy to share that with other council members.

>

> You can see which ones we are recommending move forward at:

> <https://static.spokanecity.org/documents/citycouncil/advance-agendas/2>

> 020/03/city-council-advance-agenda-2020-03-02.pdf

>

> Hope this helps.

>

> Candace Mumm



> Docketing Chair  
> Spokane City Council District #3  
>  
> -----Original Message-----  
> From: [spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com) <[spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)>  
> Sent: Wednesday, February 12, 2020 4:22 PM  
> To: Mumm, Candace <[cmumm@spokanecity.org](mailto:cmumm@spokanecity.org)>  
> Subject: 1117 w 10th  
>  
> [CAUTION - EXTERNAL EMAIL - Verify Sender]  
>  
> Candace  
>  
> I would like to have a conversation about your concerns/problems with the comp. plan amendment at 1117 W 10th. Please let me know when you have some time that is convenient for you. Thank you for your time and service.  
>  
> Seth Knutson

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**From:** Freibott, Kevin

**Sent:** Thursday, February 13, 2020 2:42 PM

**To:** Watkins, Kandace <[kwatkins@spokanecity.org](mailto:kwatkins@spokanecity.org)>

**Subject:** Comment Letter on Comp Plan Amendments?

Good afternoon, Kandace. Thanks for getting me the signed findings from the Threshold meeting. On an aside, during that meeting CM Mumm mentioned a comment letter she had received on one of the applications. Could you find out about that and see if she's willing to send me a copy? I'd like to add it to OnBase so it's part of the official record. Thanks!

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)







## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z19-505COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to DENY the Comprehensive Plan Amendment application seeking to amend the land use plan map designation from “Residential 4-10” to “Residential 15-30” for 0.16 acres located at 1117 W 10<sup>th</sup> Avenue. The implementing zoning designation requested is RMF (Residential Multi-Family).**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment application Z19-505COMP (the “Application”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Application seeks to amend the land use plan map designation for a .016-acre area located at 1117 W 10<sup>th</sup> Avenue (the “property”) from “Residential 4-10” to “Residential 15-30” with a corresponding change in zoning from Residential Single Family (RSF) to Residential Multi-Family (RMF).
- E. Annual amendment applications were subject to a threshold review process to determine whether the applications will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- F. On February 6, 2020, an Ad Hoc City Council Committee reviewed the applications that had been timely submitted, and forwarded its recommendation to City Council regarding the applications.
- G. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, including the Application in the Work Program.
- H. Thereafter, on May 11, 2020, staff requested comments from agencies, departments, and neighborhood councils. Comments were received noting certain facts for the SEPA checklist and requesting a site-specific survey for cultural resources prior to any future development.
- I. The City issued a Notice of Application on June 8, 2020 by posting it in the Spokesman Review and Official Gazette and by mail to all properties and owners within a 400-foot radius of the subject properties and any adjacent properties with the same ownership. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City received multiple comments in opposition to the proposal, stating concerns with parking, traffic, impacts to neighborhood character, and the condition of the property.



- J. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Application, and the Community Assembly was provided with information regarding the dates of Plan Commission workshops and hearings.
- K. On July 8, 2020, the Spokane City Plan Commission held a workshop to study the Application.
- L. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Application.
- M. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance (DNS) were issued for the Application. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Application was published in the Official Gazette on September 2 and 9, 2020.
- N. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- O. On August 24, 2020, Notice of Public Hearing and SEPA Determination was posted on the property and mailed to all property owners and taxpayers of record, as shown by the most recent Spokane County Assessor's record, and occupants of addresses of property located within a four-hundred-foot radius of any portion of the boundary of the subject property.
- P. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Application, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Application did not recommend approval of the Application.
- Q. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of the SEPA Checklist and Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- R. Following issuance of the public hearing notice, the City received additional comment letters in opposition to the proposal from members of the public, citing similar concerns to the previously-submitted concerns.
- S. The City also received a comment letter from the applicant in rebuttal to opposition comments received.
- T. On September 9, 2020, the Plan Commission held a public hearing on the Application, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- U. Multiple members of the public testified at the public hearing, most in opposition along themes previously communicated by comment letters and by the property owner in support of the proposal, stating that assisted living beds are needed in Spokane.



- V. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- W. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Application (the "Staff Report").
- X. The Plan Commission finds that the proposal is inconsistent with the intent and requirements of the Comprehensive Plan, most specifically policy Land Use LU 1.4 concerning the establishment of higher density residential land uses in the City.
- Y. The Plan Commission finds that the proposal does not meet the decision criteria established by SMC 17G.020.030, as described in the Staff Report and in 'Y' above.

## **CONCLUSIONS:**

Based upon the application materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding application File No. Z19-505COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Application was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review application was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Application is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Application will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Application is internally inconsistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Application is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Application has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Application.



9. The Application will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Application proposes a land use designation that is inconsistent with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment and site is not suitable for the proposed designation.
12. The map amendment would not implement applicable comprehensive plan policies better than the current map designation.

#### **RECOMMENDATIONS:**

In the matter of Z19-505COMP, a request by Dwight Hume of Land Use Solutions and Entitlement on behalf of Lark Homes, LLC to change the land use plan designation on 0.16 acres of land from "Residential 4-10" to "Residential 15-30" with a corresponding change of the implementing zoning to RMF (Residential Multi-Family), as based upon the above listed findings and conclusions, by a vote of **7 to 0 and 1 abstention**, the Spokane Plan Commission recommends City Council **DENY** the requested amendment to the Land Use Plan Map of the City's Comprehensive Plan with corresponding amendment to the City's Zoning Map, and authorizes the President to prepare and sign on the Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the Application.

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**Todd Beyreuther, President**

Spokane Plan Commission

October \_\_, 2020



**From:** [Britt O'Neal](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Re: Opposition to Z19-505COMP  
**Date:** Thursday, August 6, 2020 2:16:49 PM

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NOTE: The following comment was received by staff during the public comment period but left out of the staff report due to a clerical error. The Plan Commission was given this letter separately and they had time to consider it before their hearing on the item.

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

*Dear Council,*

I am writing to express my strong opposition in reference to Z19-505COMP: the proposed rezoning at 1117 W 10th Ave. If approved, the local community may be unable to prevent commercial development, that in itself will be detrimental to the area. Nearly all residents in the Boogie's Addition neighborhood are completely opposed to the addition of multi-family housing that will likely exacerbate current traffic and safety problems. As owner of one of a handful historically registered homes within the Boogie's Addition, I feel this would greatly impact the intrinsic and property value of the existing historical community.

It is deeply unwise to consider increasing the residence capacity on our block. Overflow parking from Huckleberries, lack of off-street parking for homeowners and many pre-existing renters means: we do not believe that the proposed rezoning will cause a problem, **but greatly exasperate a problem that currently exists**. Our street is fraught with speeding and reckless drivers. My partner and I work from home. We were heartbroken to discover our dear cat was hit by a speeding car and killed, only a month after moving into our first home here on 10th. After talking to our neighbors, we learned that this is not uncommon on our block. With small children playing on the corner and a very pet friendly neighborhood, we need to work harder to minimize the existing traffic safety issues, before we can allow for the potential to increase household capacity.

It must be noted that we are currently living through a Pandemic. Covid is known to negatively impact our elders to a greater extent. Covid is also known to travel asymptotically regardless of age. It seems irresponsible to open a senior care facility at the heart of an already overcrowded neighborhood. What Seth wants to do is a great service to our greater community, but better served in a less dense and overly populated part of Spokane. If Covid is changing society in any way, it's about spreading out. The priority can no longer be to maximize space and density. We must take note of greater trends in societal behavior when making decisions about our local community.

Aside from one casual conversation, we have seen no proof of building plans from Seth. Seth has quoted to us that restoration would cost \$100K to bring the 5800sqft property back to a functioning senior care facility. The restoration of our 2600sqft single family home cost over 140k in 2007. With no written plan offered, we have no assurance that he intends to follow through with his development plans once he's been granted the rezoning.

In the event rezoning was approved and Seth could not finance the full restoration and instead decided to change the intended purpose or sell the rezoned property, we would have no control over the intended use of this property. Some community members have said that if Seth's dream of restoring it to an elder care facility falls through, he will consider turning it into a halfway home for recovering addicts and/or ex-convicts on Federal Grant Assistance. Again, I am not against providing these incredibly important and valuable services to our community, but we must consider where is the best location and whether this corner is that place. We are encouraging families to set roots here. As a community, transitional homes, senior facilities and more rental complexes do not align with our greater goals.

As the owners of 1115 W 10th Ave, we are **one of two** homeowners that share a property line with the property in question. Our quality of life will be among the most gravely impacted by this proposed rezoning. Please I urge you to disapprove the proposed rezoning, and from recent meetings and discussions with my neighbors, I know my opinions are shared with many in the community.

Thank you for your continued service and support of our communities.

Best regards,

Brittany O'Neal

510-703-6040  
[onealbsn@gmail.com](mailto:onealbsn@gmail.com)



1115 W 10th Ave  
Spokane WA, 99204



**From:** [John Schram](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Kinneer, Lori](#); [Wilkerson, Betsy](#)  
**Subject:** File No. Z19-505COMP, 10th Avenue  
**Date:** Monday, August 31, 2020 12:58:20 PM

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NOTE: The following comments were received by the City after completion of the Staff Report. They were provided separately to the Plan Commission for their consideration prior to their hearing on the item.

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Planning Services Department,

In reference to the recent mailed notice of land use amendment proposal for the property located at 1117 W 10th Avenue (parcel# 35193.1405). I own and work out of my business property several blocks away from the parcel.

I will voice my wholehearted approval and encourage your approval of the project for several reasons.

- 1) Cliff/Cannon is a vibrant multi use neighborhood with essential services such as grocery and personal needs shopping, dining, medical care, financial and insurance services as well as home based businesses. Further integration of essential services such as this should be encouraged. This property is only one block away from a neighborhood hub of essential shopping and other business services. Less than one block away is a bus stop which connects to downtown and our local hospital/medical center zone.
- 2) This project fits generally within the City of Spokane's own infill housing strategies parameters by allowing full utilization of a given property that are within centers and corridors impact areas. The current use and restrictions on this property do not allow for full utilization.
- 3) Elder Care Facilities located in neighborhoods are a preferred and cost effective solution to caring for our elderly population especially those with more limited resources.
- 4) In my personal experience, having an elder care facility across the street from my personal residence, parking availability was never an issue and in this case the property is located on a large corner lot which allows for ample on street parking. I have never witnessed any parking congestion on that section of 10th avenue as I have on many other nearby streets. Unlike multi unit single family house conversions or apartment complexes, elder care facilities by their nature do not lend themselves to many, if any, vehicles owned and parked outside by residents. Visitors are typically far and few between (unfortunately) and staff can typically be accommodated by on site parking.
- 5) I have known the owner, Seth Knudson, personally for many years through our mutual involvement in the Cliff/Cannon Neighborhood Council and can attest to his deep love and concern for the betterment of the entire neighborhood. He has voiced his commitment to the improvement of the property and within the precepts of the proposed historic district standards.



In your service,  
John A. Schram, CFP®  
917 S. Monroe St.  
Spokane, WA 99204  
509.328.5627  
509.328.4634 (f)



**From:** [Freibott, Kevin](#)  
**To:** [spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)  
**Cc:** [Dwight Hume](#)  
**Subject:** RE: Z19-505Comp  
**Date:** Wednesday, September 9, 2020 11:32:00 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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I will forward this to the Plan Commission as well. Thanks, Seth.

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



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**From:** spy.pawn007@gmail.com <spy.pawn007@gmail.com>  
**Sent:** Wednesday, September 9, 2020 11:11 AM  
**To:** Freibott, Kevin <kfreibott@spokanecity.org>  
**Cc:** Dwight Hume <dhume@spokane-landuse.com>  
**Subject:** Z19-505Comp

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Kevin

Please include this email as part of the record.

So plan commission is aware of it. I believe the petition was miss represented. The email below supports this. At no time have I stated a halfway house as an option. I see concerns in three categories.

1. Parking: I have the 1 to 4 parking ratio required by the city. Few assisted living residents drive. 2 of 85 at Fairwood. My staff could be required to park at park and ride. There is a stop at the end of the block. Currently there are 6 non-related adults with 6 car parked the majority of the day. If my application proceeds I could see a reduction of street congestion.

2. Property value/Improvements: A large investment in the property would allow the improvements all agree is need. This investment would most like increase the value of the property and others in the area.

3. Miss-information/misunderstanding: I have never stated or planned a "halfway house". I am willing to do a development agreement to ensure it becomes elder care assisted living. Some comments refer to 15-30 residents. I believe this is a misunderstanding 15-30 in zoning is a reference to number of units per acre not residents allowed.



Seth Knutson

Begin forwarded message:

**From:** [spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)  
**Date:** September 8, 2020 at 4:10:56 PM PDT  
**To:** Dwight Hume <[dhume@spokane-landuse.com](mailto:dhume@spokane-landuse.com)>  
**Subject:** Fwd: your property on W 10th

Dwight

Those signatures were gathered with a false pretense. That I was going to make it a halfway home. Here is email I received from a resident

Seth Knutson

Begin forwarded message:

**From:** Diane Benson <[dcb1810@yahoo.com](mailto:dcb1810@yahoo.com)>  
**Date:** July 19, 2020 at 1:16:32 PM PDT  
**To:** "[spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)" <[spy.pawn007@gmail.com](mailto:spy.pawn007@gmail.com)>  
**Subject:** your property on W 10th

Hi, Seth...

A woman just came around, saying that she thinks you intend to turn the old assisted living place into a halfway house. She is collecting signatures to fight the zoning change.

I was under the impression that you were intending to use it as an assisted living facility or nursing home of some sort. We have no objections to elderly folks living there. But, there is already so much petty crime in our area. We would definitely be against turning it into a halfway house.

Can you please explain what you intend to do with the property and provide a rough timeline?

Thanks,

Diane Benson  
1217 W 10th Ave.  
916-402-5224





# LAW OFFICE OF MILTON G. ROWLAND, PLLC

ATTORNEY AND COUNSELOR AT LAW

---

September 9, 2020

Kevin Freibott  
Department of Neighborhood and Planning Services  
City of Spokane  
808 W Spokane Falls Boulevard  
Spokane, WA 99201

VIA Email to Louis Mueller at [plancommission@spokanecity.org](mailto:plancommission@spokanecity.org) and to K. Freibott at [Kfreibott@spokanecity.org](mailto:Kfreibott@spokanecity.org)

Re: Response and comments regarding Project Z19-505COMP

Dear Mr. Freibott and Mr. Mueller:

I represent Mr. and Mrs. Landry, who live at 1011 S. Jefferson, directly south of the subject parcel. I previously filed a letter with you in July 2020, and this letter is intended to supplement the same.

My clients wish you to know, for example, that this property has been subject to neglect for years. As one client stated:

In the years that the current owner has had the facility, no improvements have been made to the exterior. I have paid to get the weeds cut which are a fire hazard between my house and the property. The south gable end of the roof is hanging and will fall at any time. The kitchen door is below this hazard. Someone will get hurt. This hazard has been reported to the city within the last year but no action was taken by them. The 1950s cinder block addition on the west side of the main building is so degraded that you can see into the structure. Watering is little or none so the landscaping is pitiful. The roof is so far gone that the integrity of the framing of the roof may be in question. Clearly the owner has taken revenues from the property and has put little or nothing back.

Another problem is that the property is wholly unsuitable for a nursing home of care facility. As one client stated:



... I have seen a senior home function in this facility. These people are deserving of a facility that can accommodate their needs. We do not need a 1950s style facility in a house that is isolated from a comprehensive or regional plan for assisted care. Seniors deserve better. I saw the senior care facility function for about 10 years. It contributed nothing to the neighborhood. None of the seniors ventured out of the building unless they were lost. Several ended up on our doorstep. You might get a glimpse of a new tenant as they moved in and again on a gurney when they left. In the 21<sup>st</sup> century this is just not right. This facility has not been used for a senior home for over 20 years.

The facility is currently being used a boarding house and not as a “senior home”. If the applicant is allowed to expand from the six people he currently renting to (there may be more than six) to 15 or more (up to 30 would be allowed), then the following will happen:

1. The central kitchen could not accommodate all parties getting an opportunity to prepare food.
2. The parking would be impossible for the tenants and tenant guests. Most nearby homes were built without garages or offstreet parking. The boarding house tenants and all the guests will take up far too much parking on the street.
3. In our experience over the last 30 years in the neighborhood, there have been no domestic violence calls to homes either on 10<sup>th</sup> or Jefferson and in the immediate area. There have, however, been calls to the larger apartment rentals on 11<sup>th</sup> and 10<sup>th</sup>. This boarding house will result in domestic friction as a result of overcrowding and we can expect plenty of police calls.

Can Applicant point to any community vision that intends to reduce the quality of living for the residents in the neighborhood? This zone change proposal does not fit the comprehensive plan and should not be approved. It is not in the neighborhood center or corridor.<sup>1</sup>

We agree with the staff conclusion regarding this application overall (application should be denied). We do not however agree with staff's conclusion that the SEPA checklist met all non-project requirements. RCW 43,21C.450 provides for SEPA exemptions in several narrow classes of cases, but this is not one of those cases. It is obvious that the whole point of this application is to dramatically increase the density of this neighborhood. That would be categorically detrimental to the quality of the neighborhood.

City Planning rezoned the property back to single family. The new applicant purchased the property with this knowledge. Improvements to the properties immediately to the east, south, and west have had in excess of \$250,000 worth of improvements to them. Adjoining to the east, the home has been put on the historic register. None of these three are rentals nor have they ever been rentals.

---

<sup>1</sup> Applicant's initial submittals at para. 1.4, assert that this property is on the “centers and corridors” map. This is false. We are at a loss to understand why applicant made such an egregious error.



Now the neighborhood has changed to younger families with children. Parking is a very big problem for tenants of the property and this is not considering guests trying to find a place to park.

The lot footprint is very small. It is unlikely that you could find another lot of this size that would get this zoning approval. Parking will always be a problem unless the property becomes a very tall structure with parking on the bottom. Congestion from additional cars would be a hazard since the parcel is not on an arterial. It is not within an urban growth area.

We submitted scores of signatures on the equivalent of a petition to deny the requested changes. This application has a neighborhood united against the proposed change. We urge the decisionmaking body to follow staff recommendations and deny.

Thank you for your courteous attention to this matter.

Very truly yours,

/s/

Milton G. Rowland





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                       |             |
|-----------------------|-------------|
| <b>Date Rec'd</b>     | 10/26/2020  |
| <b>Clerk's File #</b> | ORD C35978  |
| <b>Renews #</b>       |             |
| <b>Cross Ref #</b>    |             |
| <b>Project #</b>      | Z20-019COMP |
| <b>Bid #</b>          |             |
| <b>Requisition #</b>  |             |

|                           |                                                     |
|---------------------------|-----------------------------------------------------|
| <b>Submitting Dept</b>    | PLANNING                                            |
| <b>Contact Name/Phone</b> | KEVIN FREIBOTT 625-6184                             |
| <b>Contact E-Mail</b>     | KFREIBOTT@SPOKANECITY.ORG                           |
| <b>Agenda Item Type</b>   | First Reading Ordinance                             |
| <b>Agenda Item Name</b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – BIKE MAP TR-5 |

**Agenda Wording**

An Ordinance relating to proposal Z20-019COMP by the City, amending the Comprehensive Plan Proposed Bike Network Map in various locations throughout the city and text in Appendix D, Transportation.

**Summary (Background)**

The proposal concerns amendments to Map TR-5 in 13 locations throughout the City, updated terminology in Appendix D, and corrected map references in Appendix D. This is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. Public participation and notification requirements are complete. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended approval of the amendment.

|                                    |                   |                                     |                              |
|------------------------------------|-------------------|-------------------------------------|------------------------------|
| <b><u>Fiscal Impact</u></b>        |                   | Grant related? NO                   | <b><u>Budget Account</u></b> |
|                                    |                   | Public Works? NO                    |                              |
| Neutral                            | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| Select                             | \$                |                                     | #                            |
| <b><u>Approvals</u></b>            |                   | <b><u>Council Notifications</u></b> |                              |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS     | <b><u>Study Session\Other</u></b>   | CC Study Session 10-29-20    |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M. | <b><u>Council Sponsor</u></b>       | CM Mumm                      |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY   | <b><u>Distribution List</u></b>     |                              |
| <b><u>Legal</u></b>                | RICHMAN, JAMES    | tblack@spokanecity.org              |                              |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL   | jrichman@spokanecity.org            |                              |
| <b><u>Additional Approvals</u></b> |                   | sbishop@spokanecity.org             |                              |
| <b><u>Purchasing</u></b>           |                   | dhume@spokane-landuse.com           |                              |
|                                    |                   | lmeuler@spokanecity.org             |                              |
|                                    |                   |                                     |                              |
|                                    |                   |                                     |                              |





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



Ordinance No. C35978

AN ORDINANCE RELATING TO PROPOSAL FILE Z20-019COMP AMENDING COMPREHENSIVE PLAN MAP TR-5, PROPOSED BIKE NETWORK MAP, IN VARIOUS LOCATIONS, AND AMENDING THE TEXT OF APPENDIX D TO THE COMPREHENSIVE PLAN TO UPDATE TERMINOLOGY RELATING TO PROTECTED BIKE LANES AND TO UPDATE MAP REFERENCES.

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, Proposal File Z20-019COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Proposal Z20-019COMP seeks to amend Comprehensive Plan Map TR-5, Proposed Bike Network, in 13 locations throughout the City and amending the text of Appendix D to update terminology relating to protected bike lanes and update map references; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Proposal Z20-019COMP reviewed all the criteria relevant to consideration of the proposal was published on August 25, 2020 and sent to all applicants and the Plan Commission; and

WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the proposal was published on August 26, 2020 and September 2, 2020; and



WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted online and with the Spokesman Review, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing, including the taking of public testimony, on September 9, 2020, during which the public record was closed; and

WHEREAS, the Spokane Plan Commission held a public hearing, during which they deliberated this and all other Comprehensive Plan Amendment applications, on September 23, 2020; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-019COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-019COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 8 to 0 to recommend approval of Proposal Z20-019COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Proposal. Proposal Z20-019COMP is approved.
2. Amendment of the Proposed Bike Network Map. The Spokane Comprehensive Plan Map TR-5, Proposed Bike Network Map, is amended as shown in Exhibit A.
3. Amendment of Appendix D. Page 25 of Part 6 of Appendix D to the Comprehensive Plan is amended as follows:

*~~((Cycle Tracks))~~ Protected Bike Lanes*

*A ~~((cycle-track))~~ protected bike lane is an exclusive bike facility that combines the user experience of a separated path with the on-street infrastructure of a conventional bike lane. A ~~((cycle-track))~~ protected bike lane is physically separated from motor traffic and distinct from the sidewalk. ~~((Cycle-tracks))~~ Protected bike lanes have different forms but all share common elements—they provide space that is intended to be exclusively or*



*primarily used for bicycles, and are separated from motor vehicle travel lanes, parking lanes, and sidewalks. In situations where on-street parking is allowed, ~~((cycle tracks))~~ protected bike lanes are located to the curb-side of the parking (in contrast to bike lanes).*

*~~((Cycle tracks))~~ Protected bike lanes may be one-way or two-way, and may be at street level, at sidewalk level, or at an intermediate level. If at sidewalk level, a curb or median separates them from motor traffic, while different pavement color/texture separates the ~~((cycle track))~~ protected bike lanes from the sidewalk. If at street level, they can be separated from motor traffic by raised medians, on-street parking, or bollards. These design features do raise different considerations – such as driveway conflicts, driver expectations, and maintenance issues that need to be addressed. By separating cyclists from motor traffic, ~~((cycle tracks))~~ protected bike lanes can offer a higher level of security than bike lanes and are attractive to a wider spectrum of the public. Routes classified as future bike lanes in this plan may be considered for protected bike lane designs following additional assessment and review. Further network-level planning will be required to identify a system of routes best suited to these designs.*

***Figure 5. Examples of potential ~~((cycle track))~~ protected bike lane designs.***

4. Amendment of Appendix D. Page 31 of Part 6 of Appendix D to the Comprehensive Plan is amended as follows:

### ***BIKEWAY NETWORK MAPS***

*Spokane's bicycle facilities network includes protected bicycle lanes, bike lanes, shared-use paths, neighborhood greenways, shared roadways, and bike-friendly routes. The development of bicycle facilities is expected to take place over the course of the next 20 years. A number of unforeseen circumstances may affect the way that Spokane's bike network will develop. The Bicycle Facility Network Development Maps are not intended to define a specific time frame for the development of bike facilities within the city. These maps represent how the network may develop over time recognizing that the network cannot be created immediately. If an opportunity to develop any of the facilities on the map arises, that opportunity should be pursued. The bikeway network is shown in Map TR-5 in Comprehensive Plan Chapter 4: Transportation.*

***~~((Existing Bikeway Network Map~~***



~~Map BMP 1 shows all of the existing bicycle facilities in Spokane at the time of the adoption of the Bike Master Plan.~~

~~**Future Bikeway Network Map**~~

~~Map BMP 2 (Map TR 5) shows all the proposed bicycle facilities for the City.))~~

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date



# EXHIBIT A

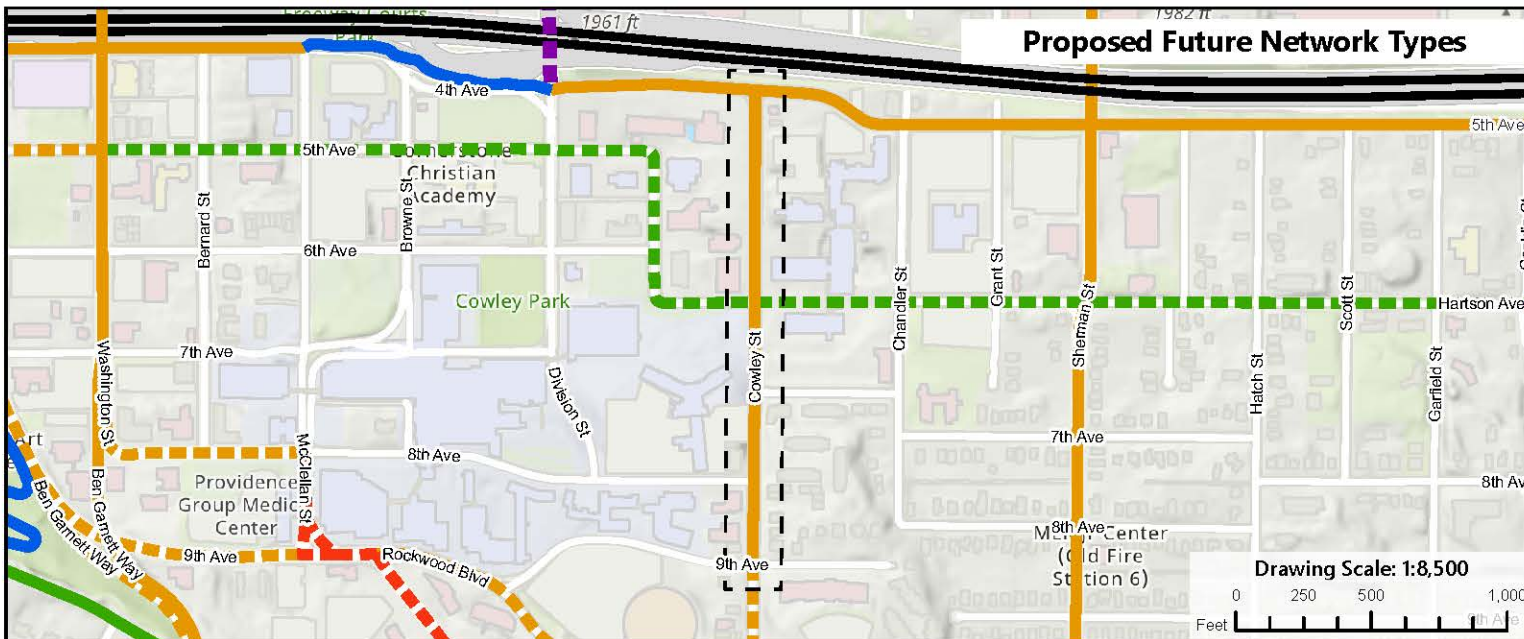
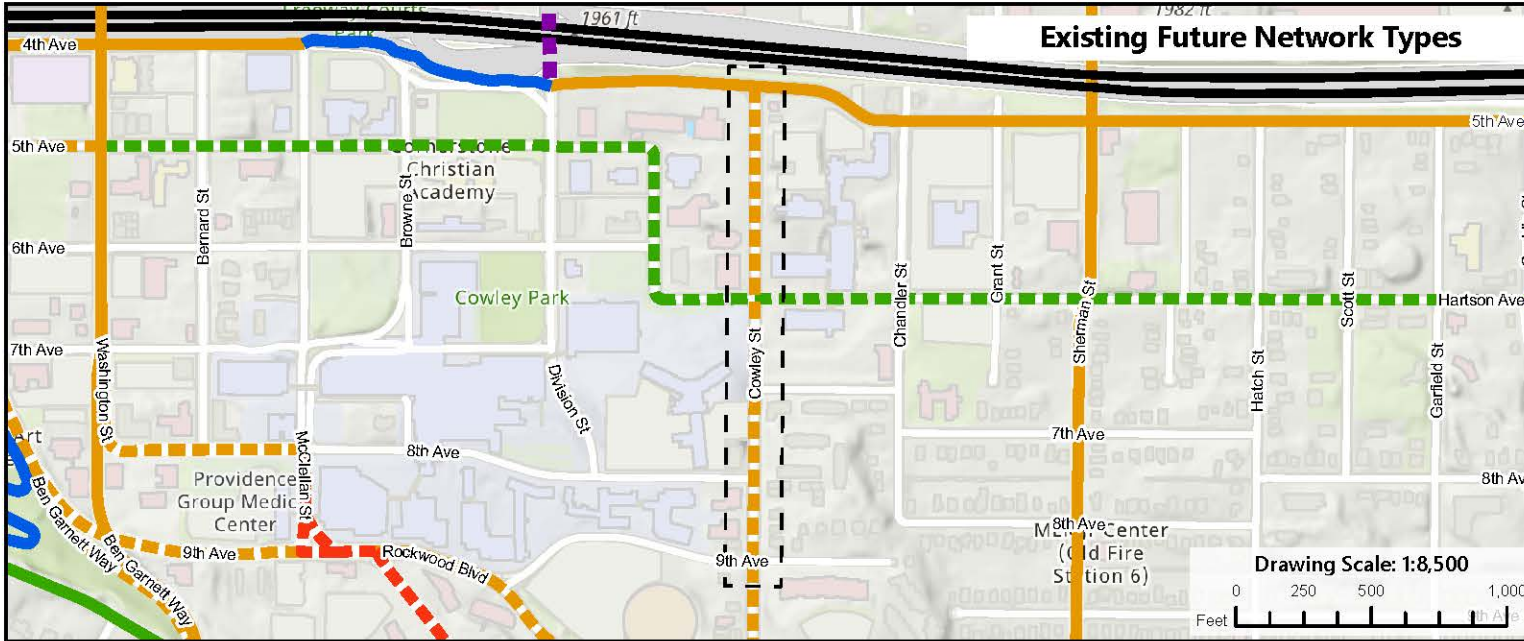


## Z20-019COMP: Map TR-5, Proposed Modification 1 (Cowley St between 4th Ave and 9th Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



### Legend

[ ] Area of Proposed Change

#### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.33 Miles

#### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



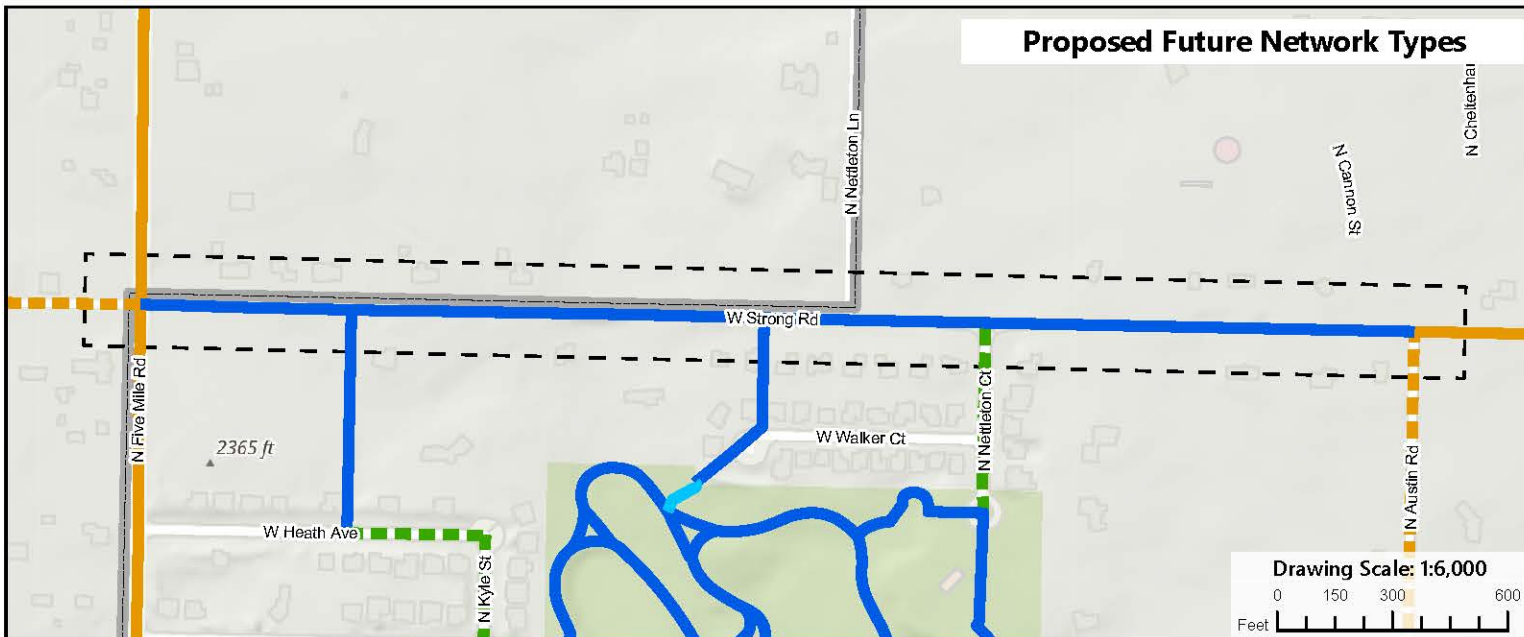
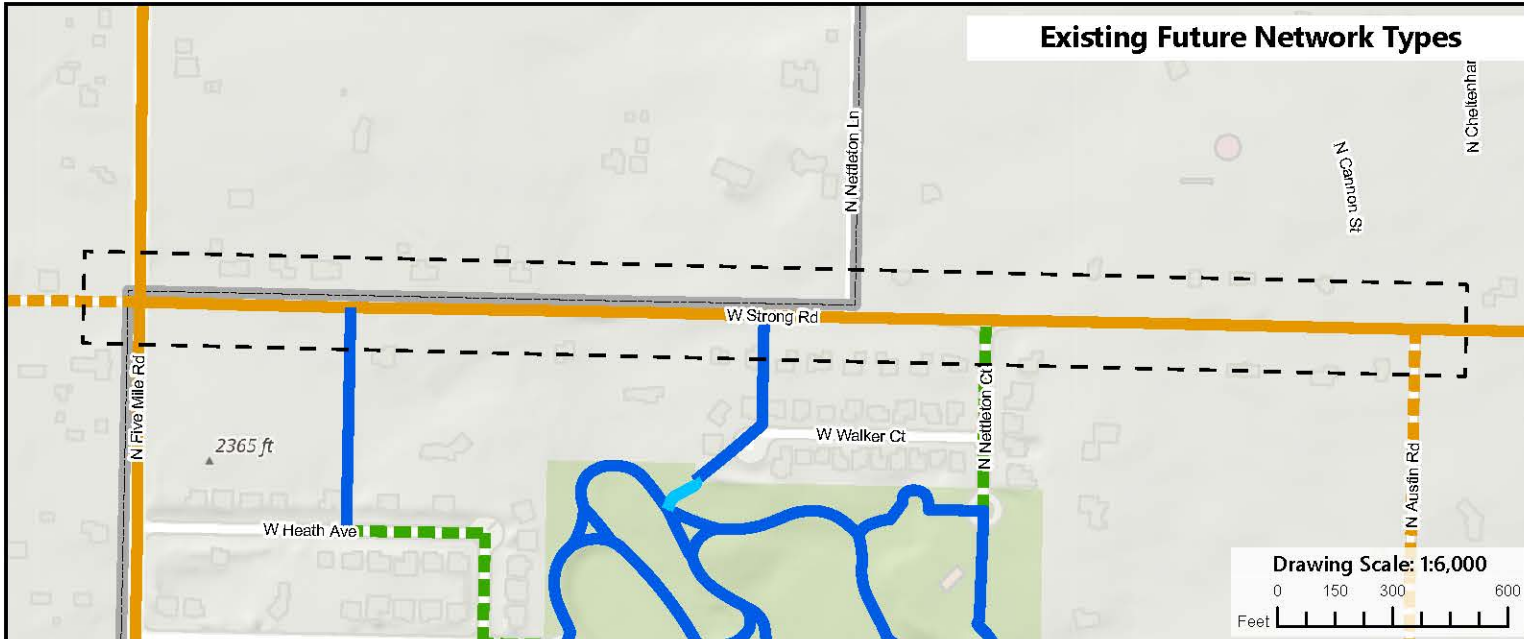


# **Z20-019COMP: Map TR-5, Proposed Modification 2 (Revised June 8)** **(Strong Rd between Five Mile Rd and Austin Rd)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 6/8/2020

THIS IS NOT A LEGAL DOCUMENT

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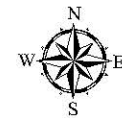


## Legend

[ ] Area of Proposed Change

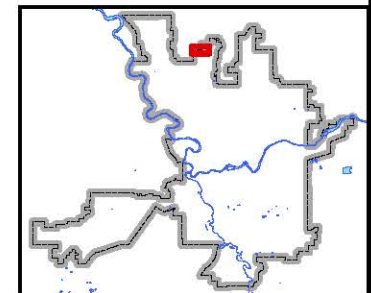
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.63 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



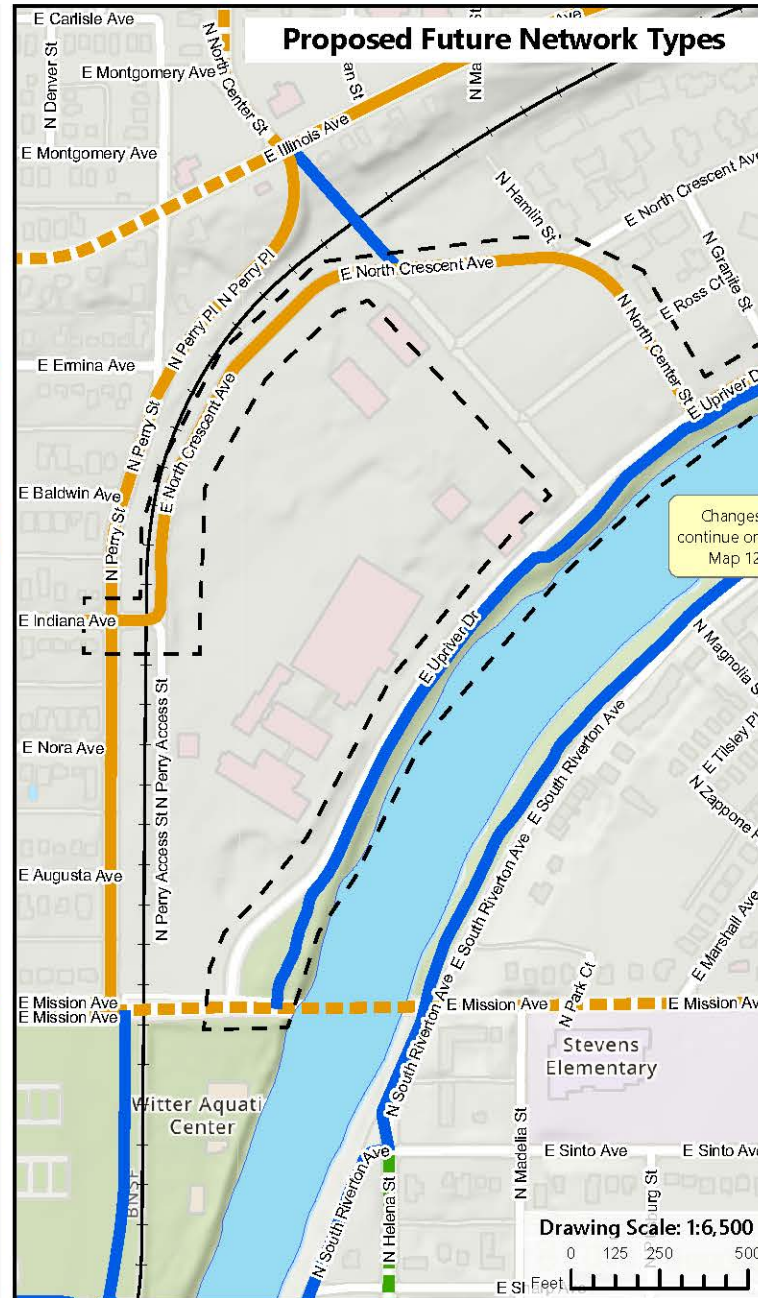
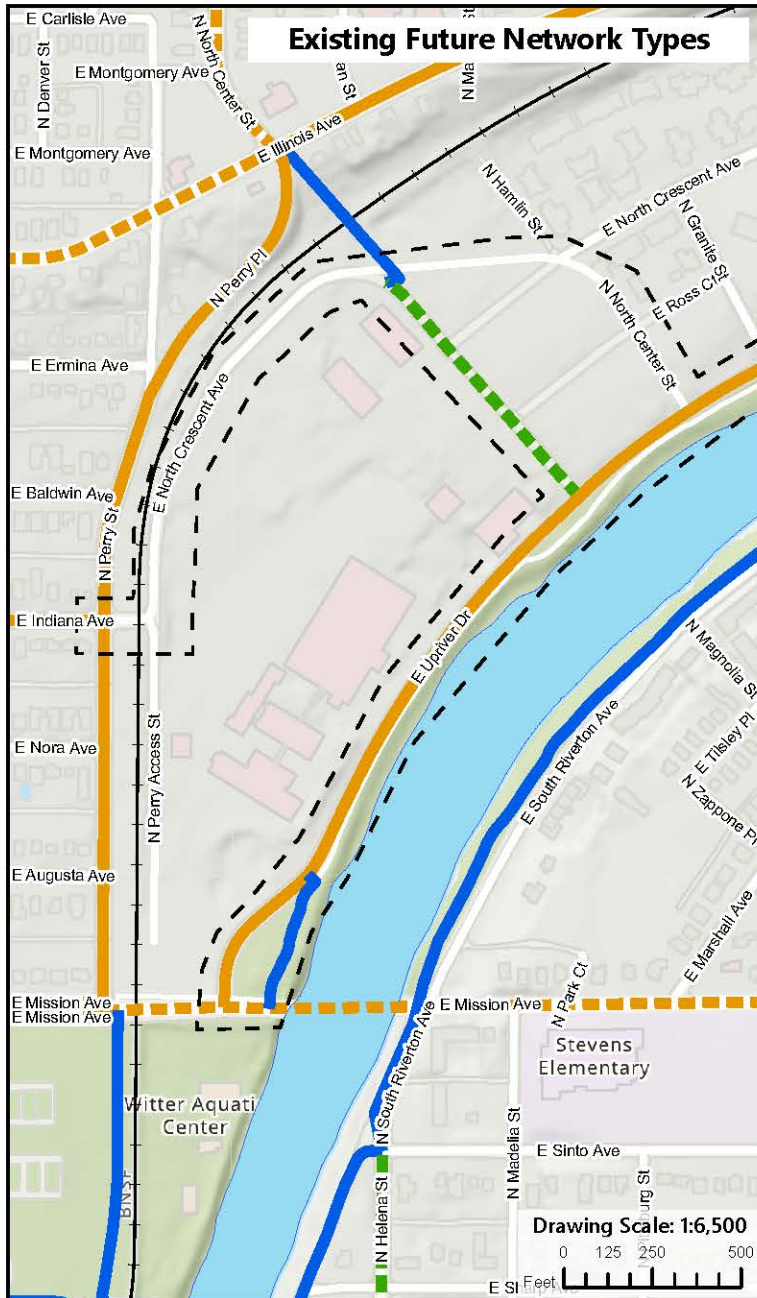


# Z20-019COMP: Map TR-5, Proposed Modification 3 (Avista Headquarters Vicinity) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/9/2020

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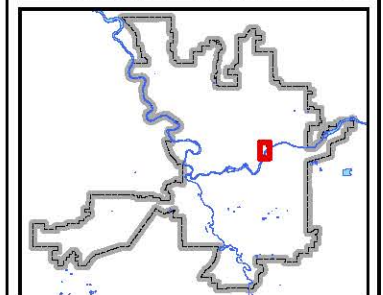
## Legend

- [ ] Area of Proposed Change
- Railroad
- Waterbody
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 1.06 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



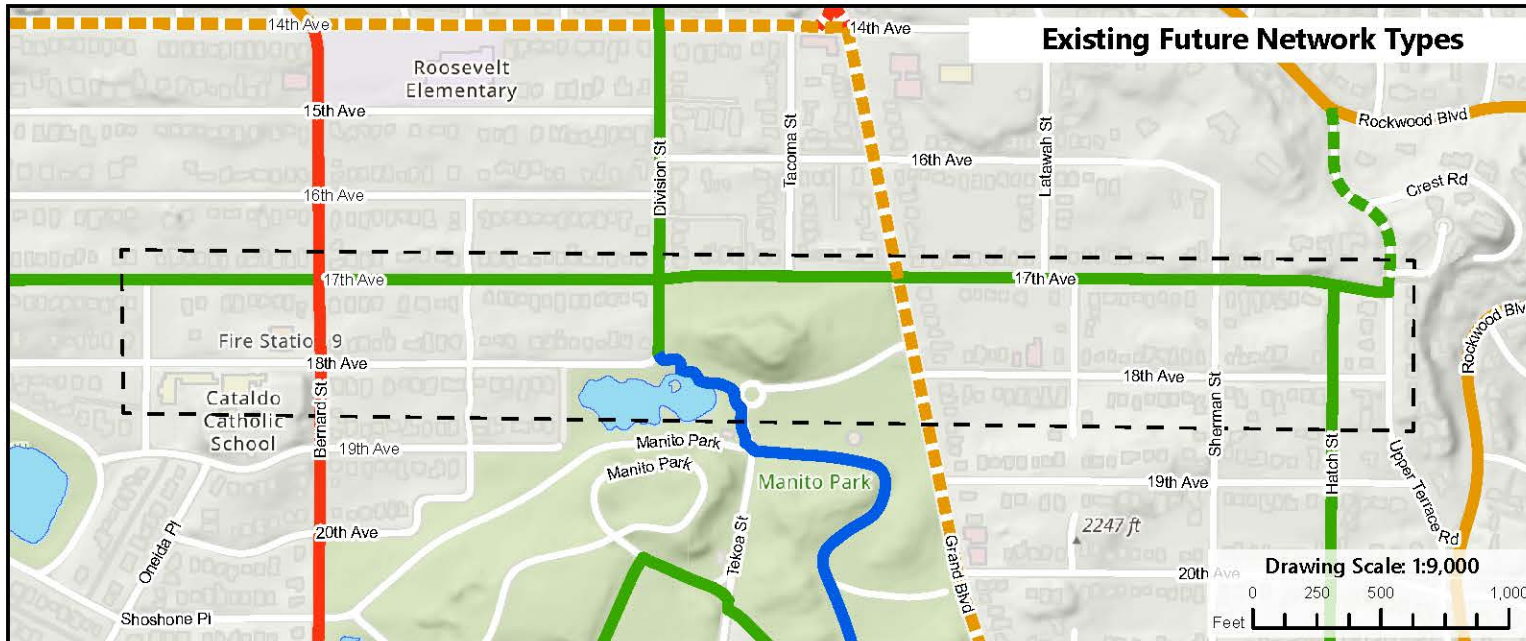


# **Z20-019COMP: Map TR-5, Proposed Modification 4** **(17th Ave between Stevens St and Hatch St)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

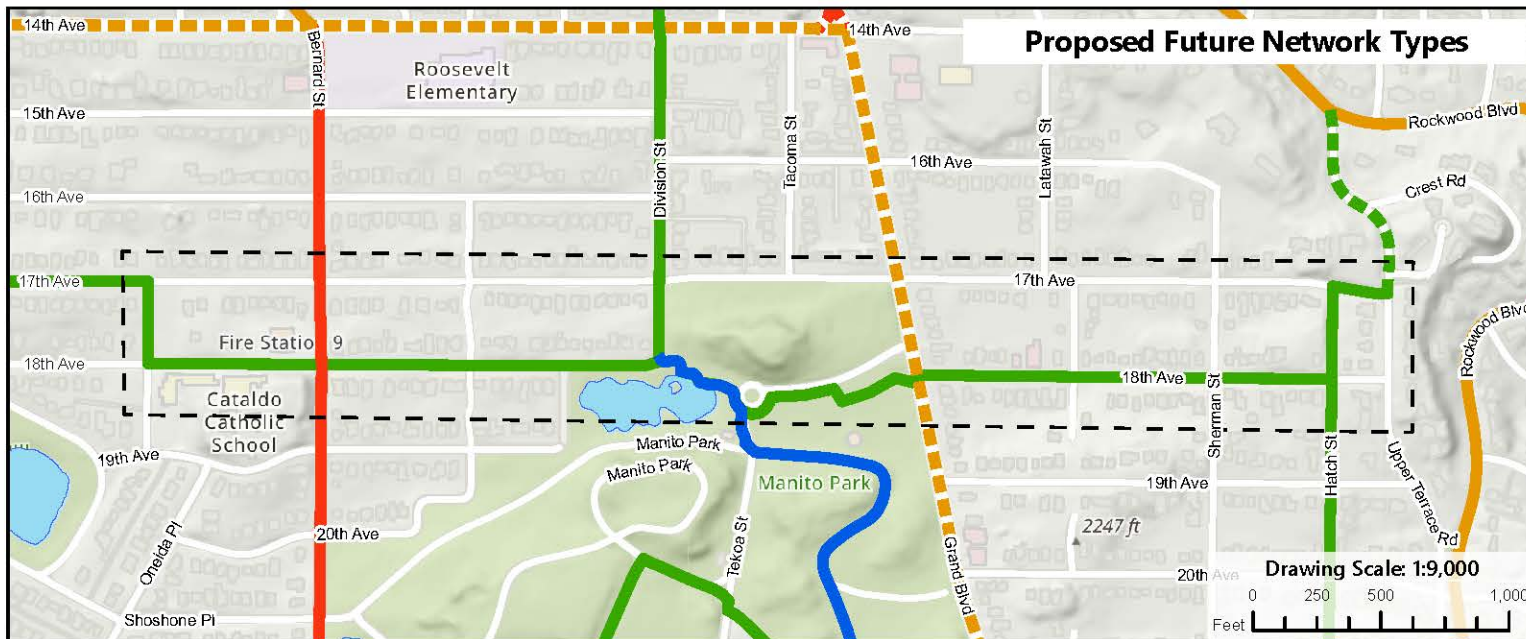
THIS IS NOT A LEGAL DOCUMENT

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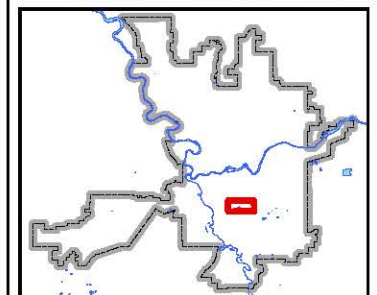
## Legend

- Waterbody
- Area of Proposed Change
- Current Bikeway Network**
  - Bike Friendly Route
  - Closed to Bike
  - Difficult Connection
  - High Traffic (Bike Lane)
  - High Traffic (Shared)
  - Moderate Traffic (Bike Lane)
  - Moderate Traffic (Shared)
  - Neighborhood Greenway
  - Shared Use Path
  - Soft Surface Path



Length of Change: 0.92 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



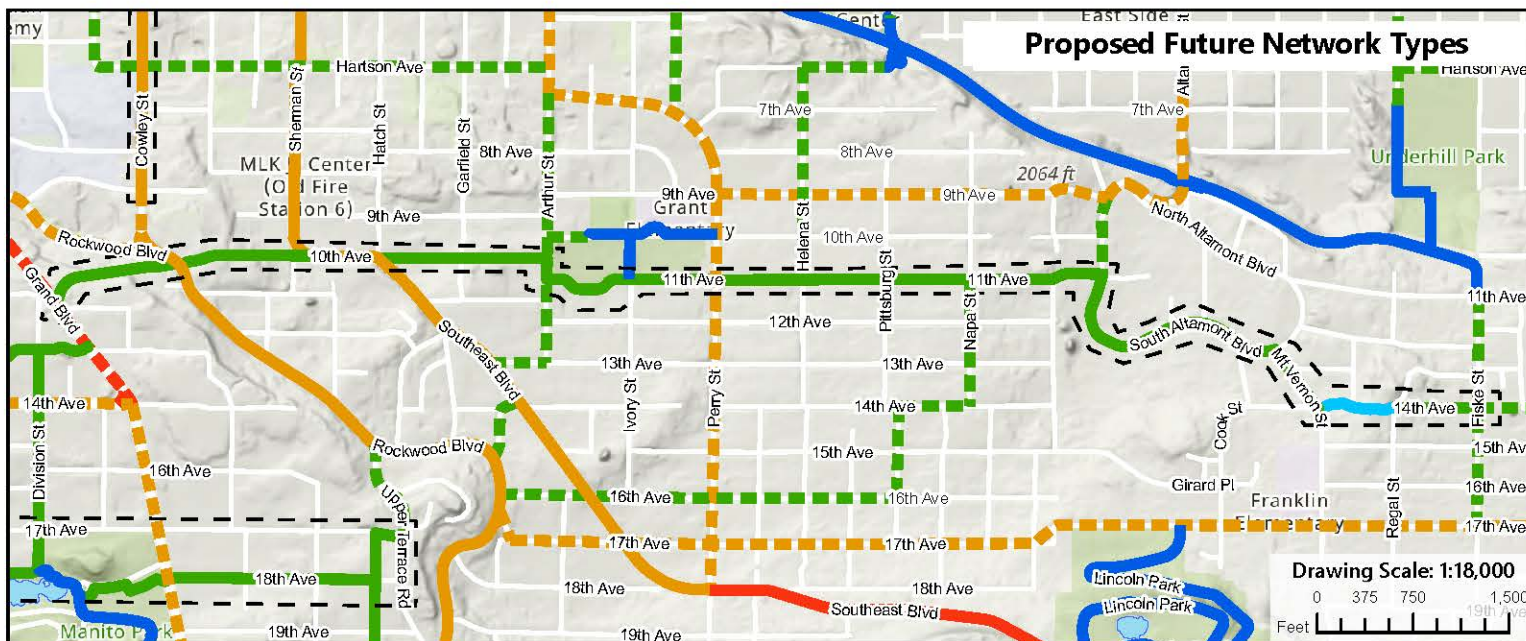
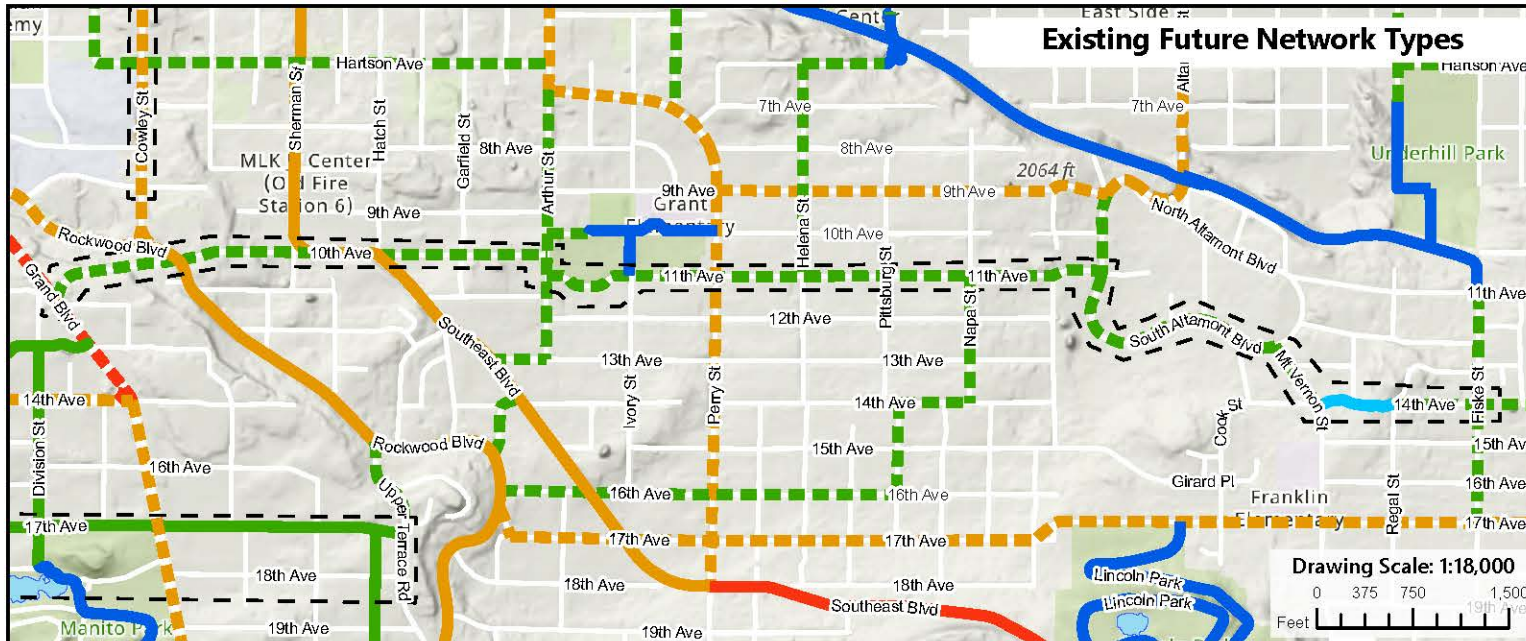


# **Z20-019COMP: Map TR-5, Proposed Modification 5** **(Sumner Ave, 10th Ave, 11th Ave, Altamont Blvd, Mt Vernon St, 14th Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

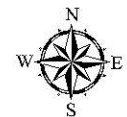
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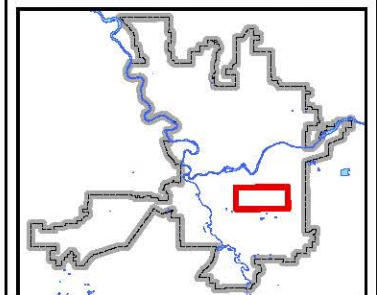
## Legend

- Waterbody
- Area of Proposed Change
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 3.26 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



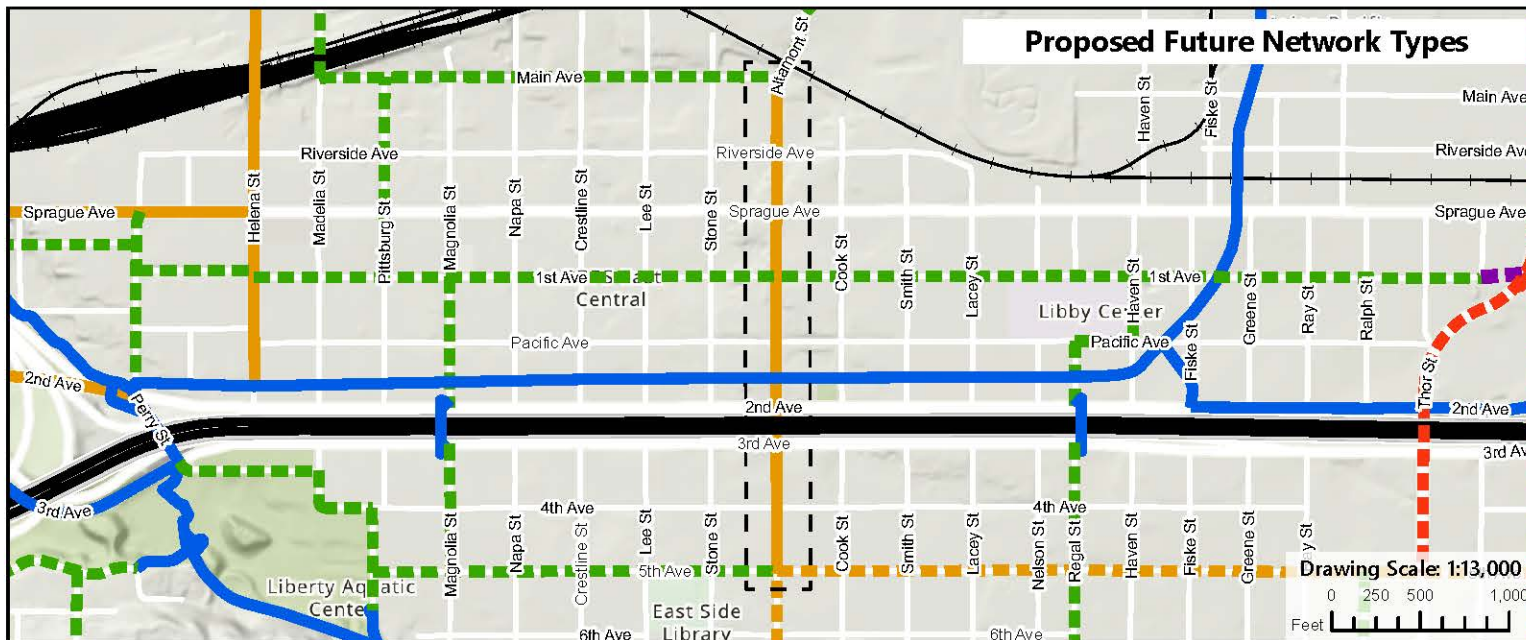
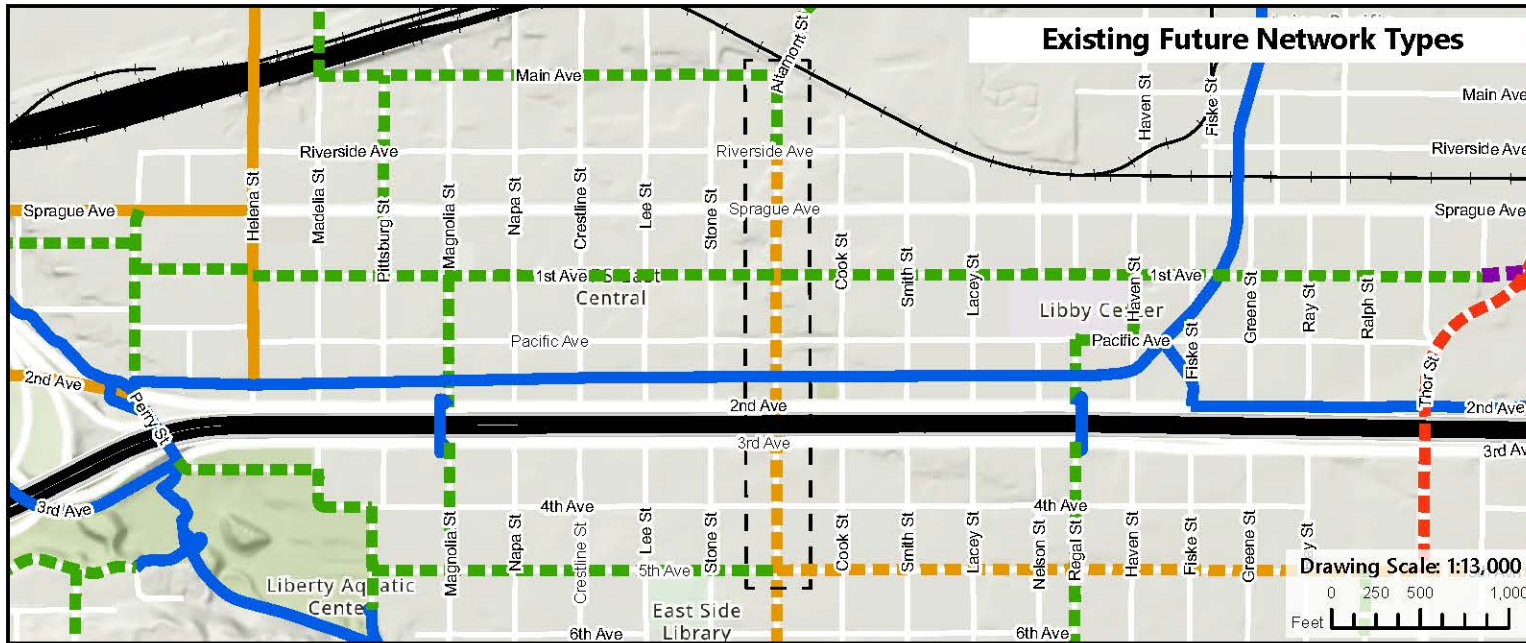


# Z20-019COMP: Map TR-5, Proposed Modification 6 (Altamont St between 5th Ave and Main Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

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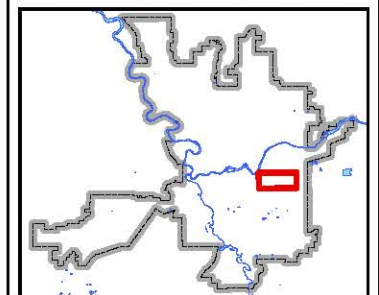
## Legend

- Railroad
- Area of Proposed Change
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.53 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



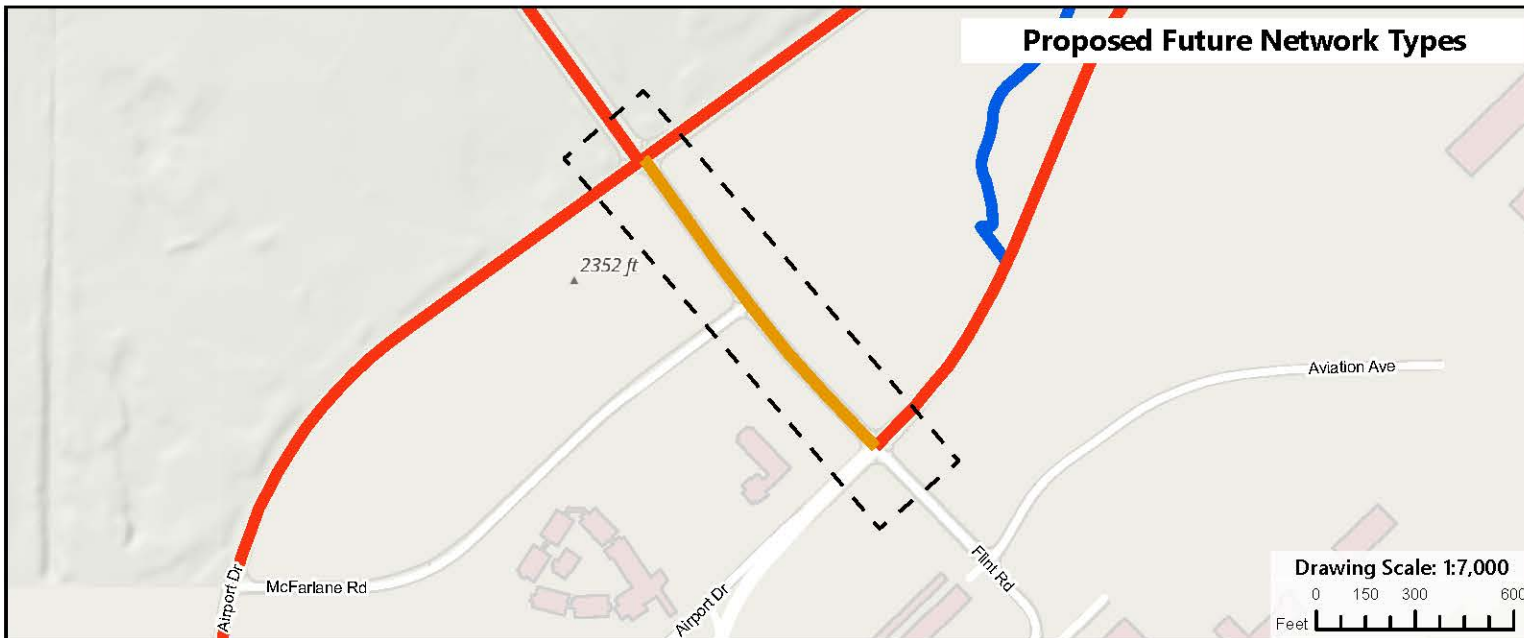


# Z20-019COMP: Map TR-5, Proposed Modification 7 (Flint Rd between Airport Dr and Airport Dr) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

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## Legend

[ ] Area of Proposed Change

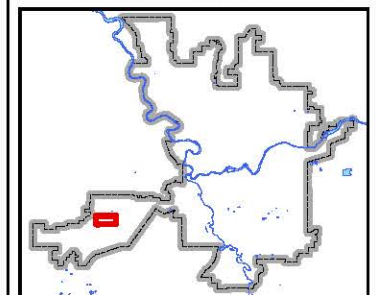
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.21 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



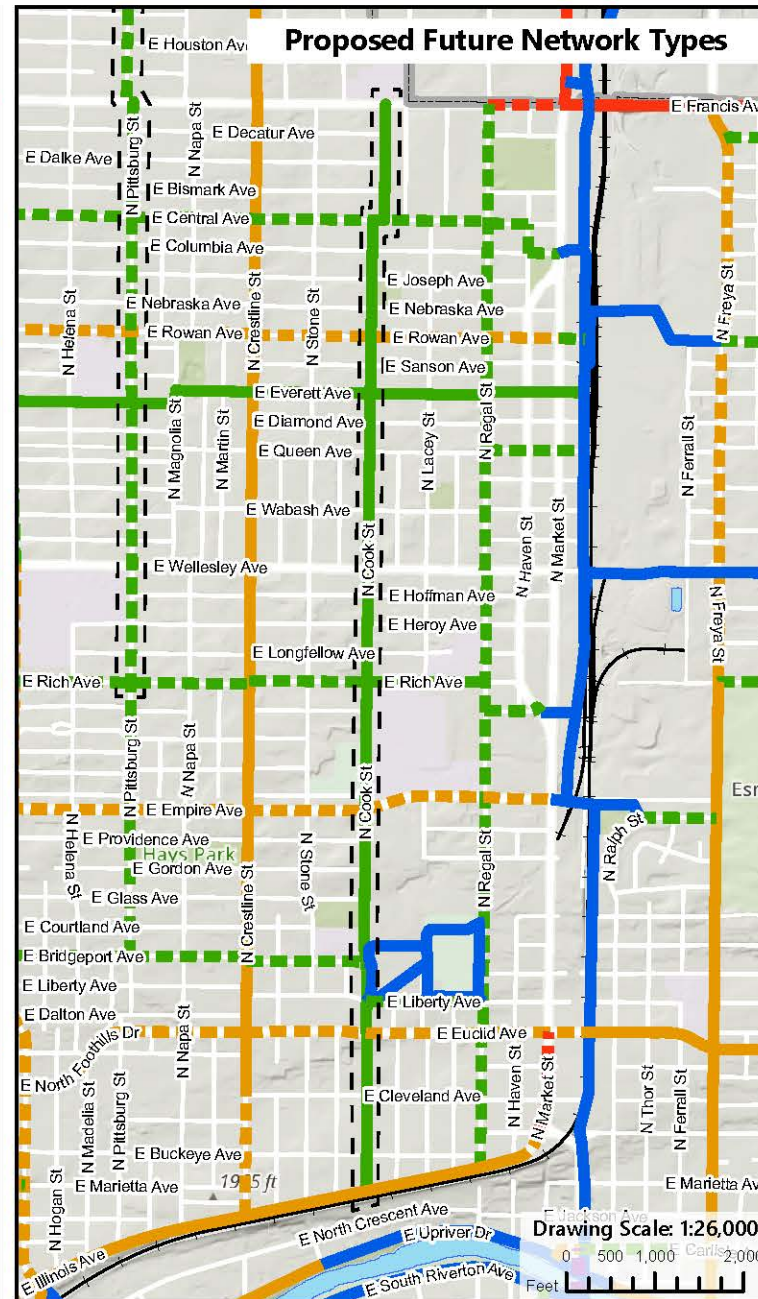
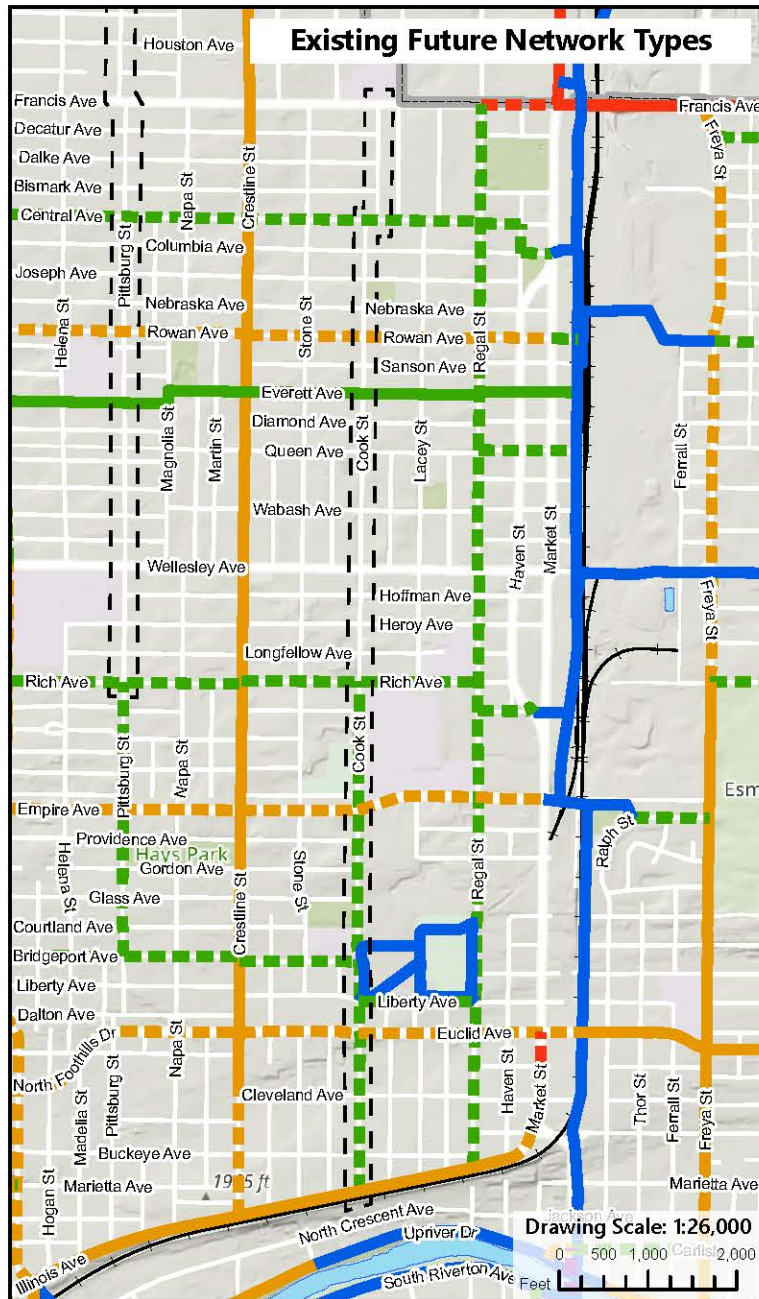


# **Z20-019COMP: Map TR-5, Proposed Modification 8** **(Cook St between Francis Ave and Illinois Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/14/2020

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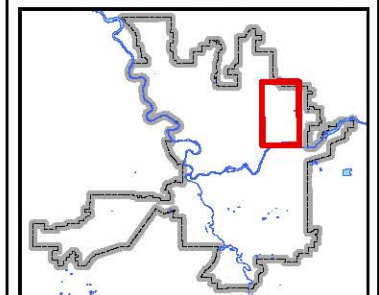
## Legend

- Railroad
- Waterbody
- Area of Proposed Change
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 2.35 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



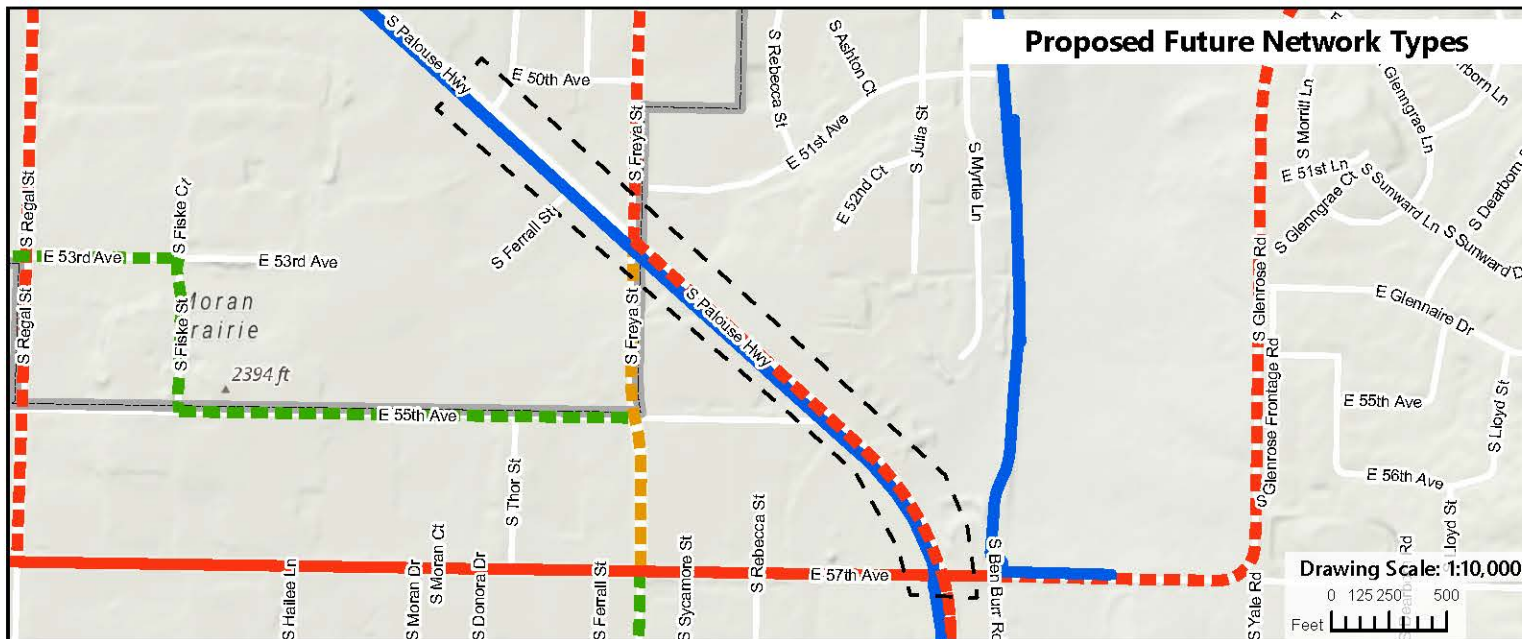
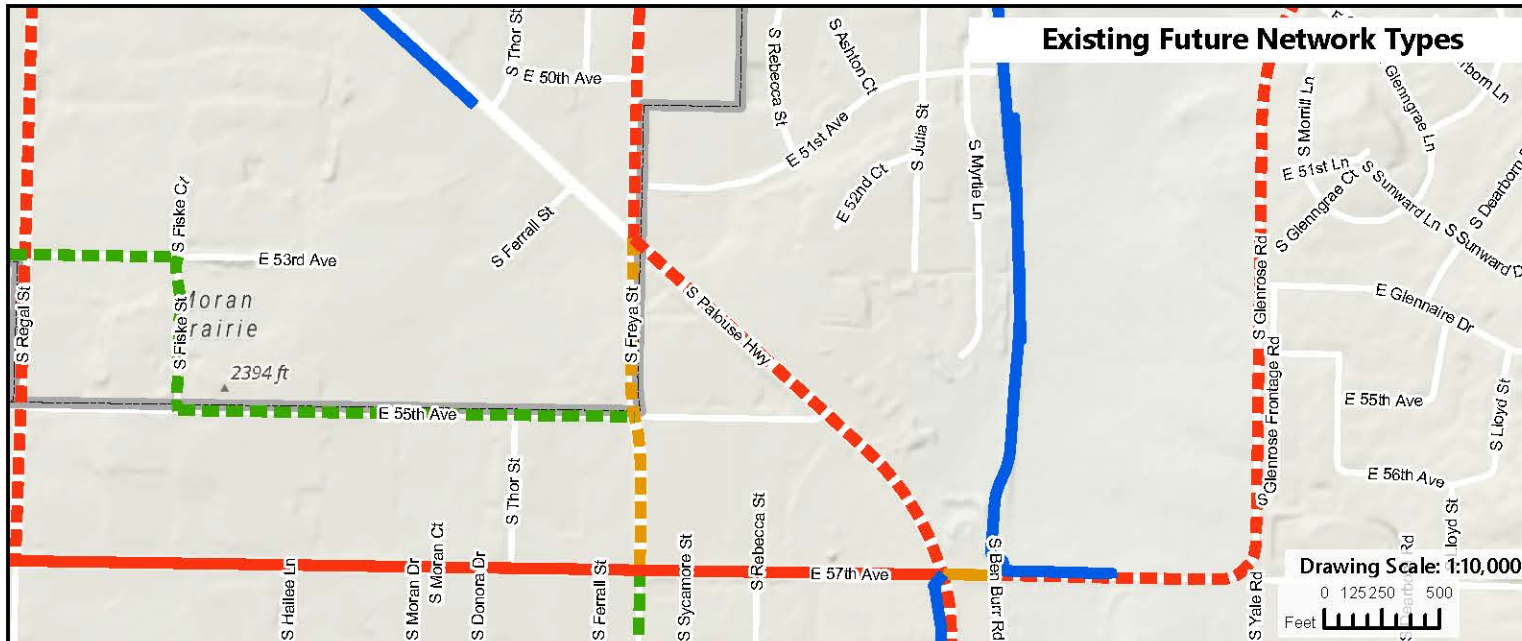


# **Z20-019COMP: Map TR-5, Proposed Modification 9** **(Palouse Hwy between Thor St and 57th Avenue)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/7/2020

THIS IS NOT A LEGAL DOCUMENT

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## Legend

[ ] Area of Proposed Change

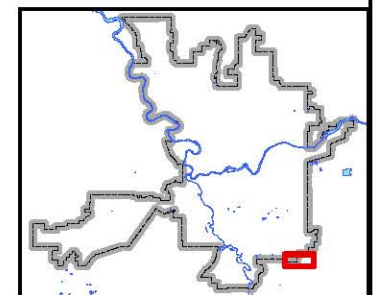
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.75 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



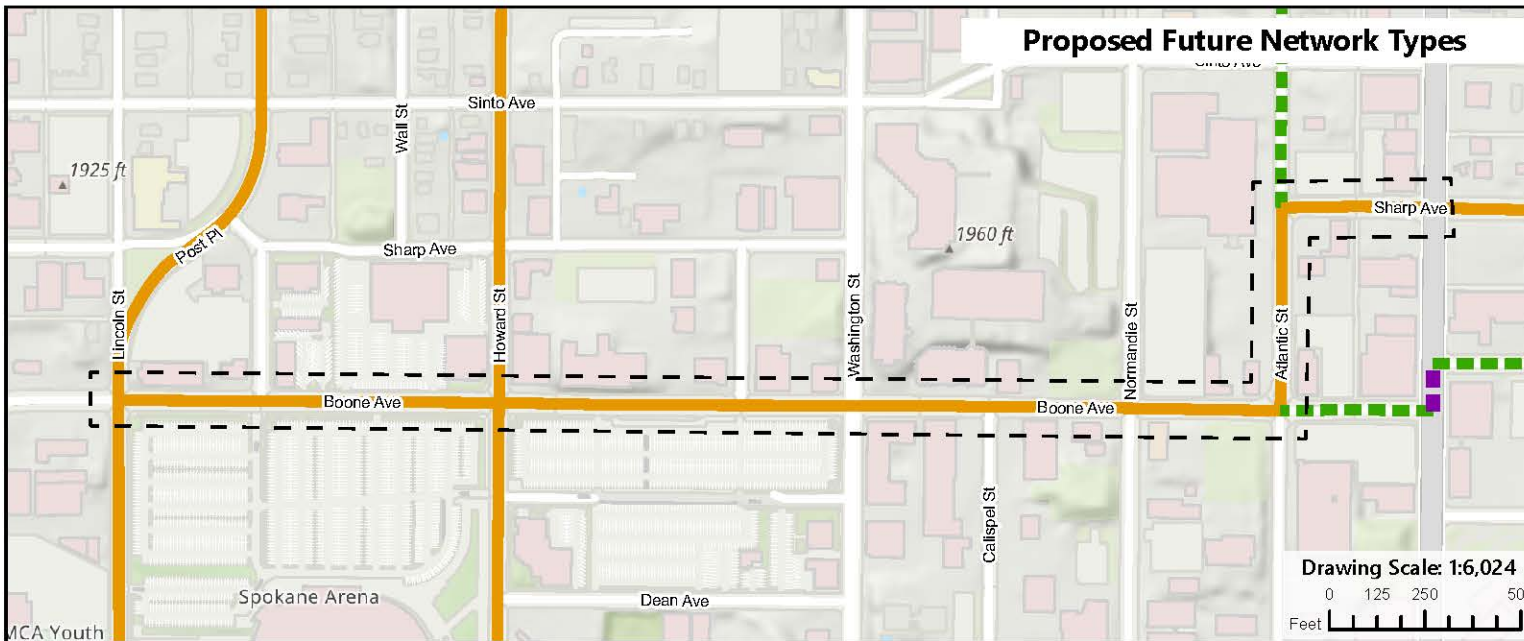
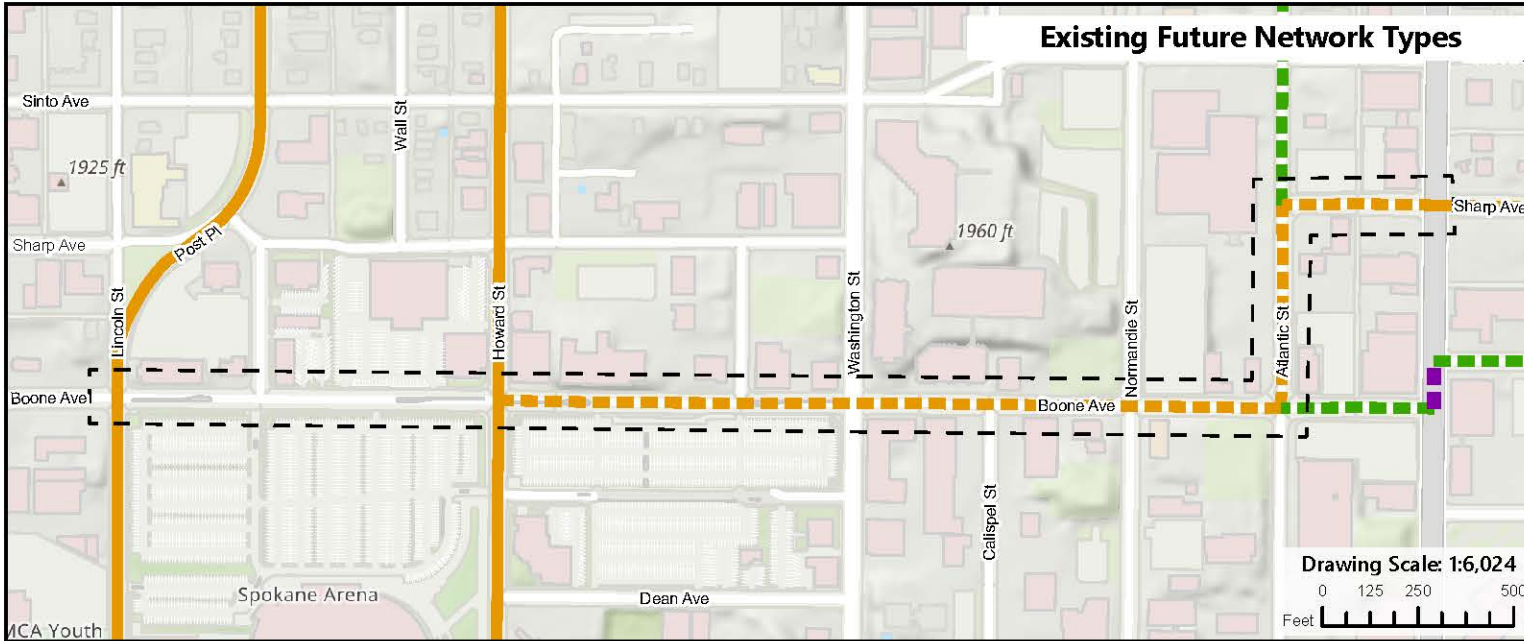


# Z20-019COMP: Map TR-5, Proposed Modification 10 (Boone Ave, Atlantic St, & Sharp Ave between Lincoln St and Division St) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



Length of Change: 0.75 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





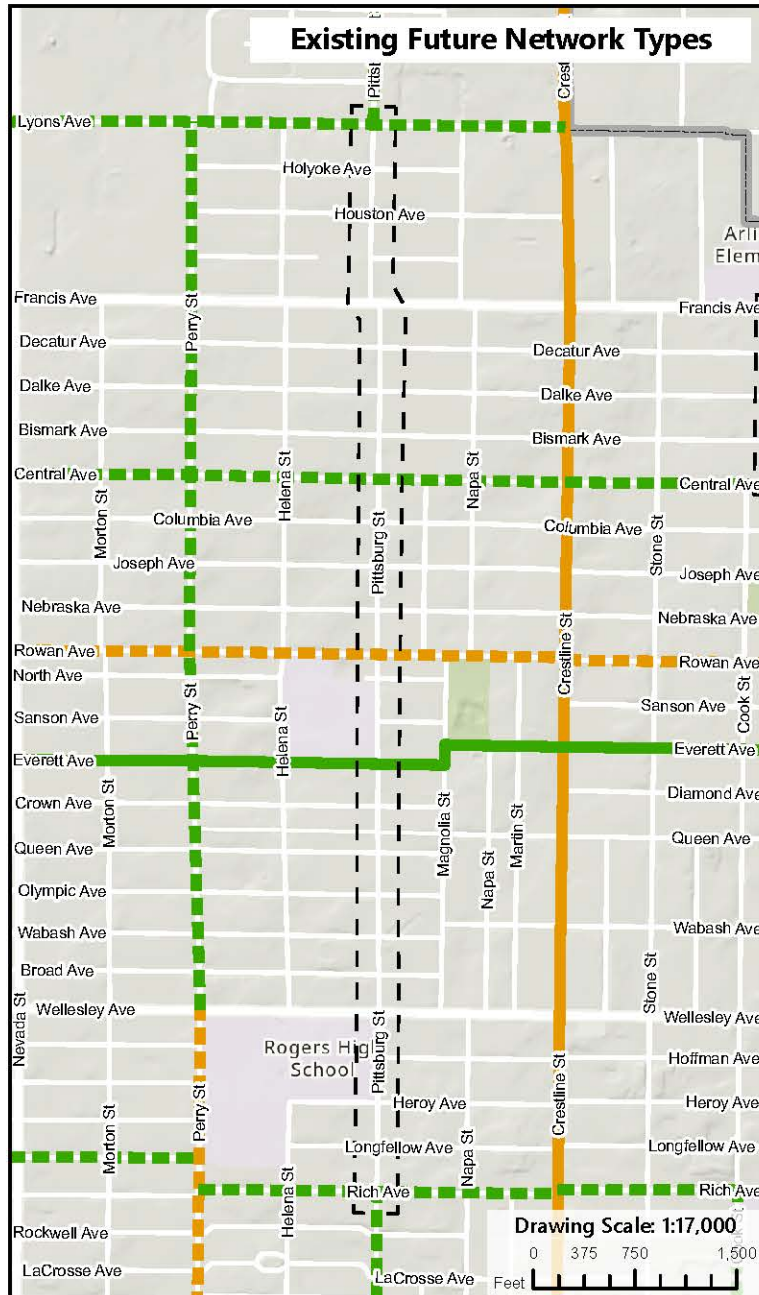
# Z20-019COMP: Map TR-5, Proposed Modification 11 (Pittsburg St between Lyons Ave and Rich Ave)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

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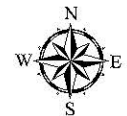


## Legend

[ ] Area of Proposed Change

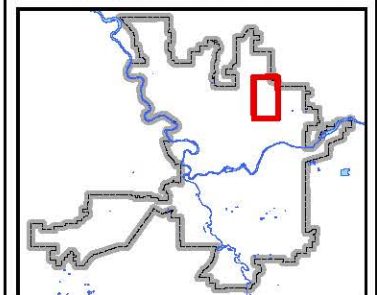
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 1.51 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





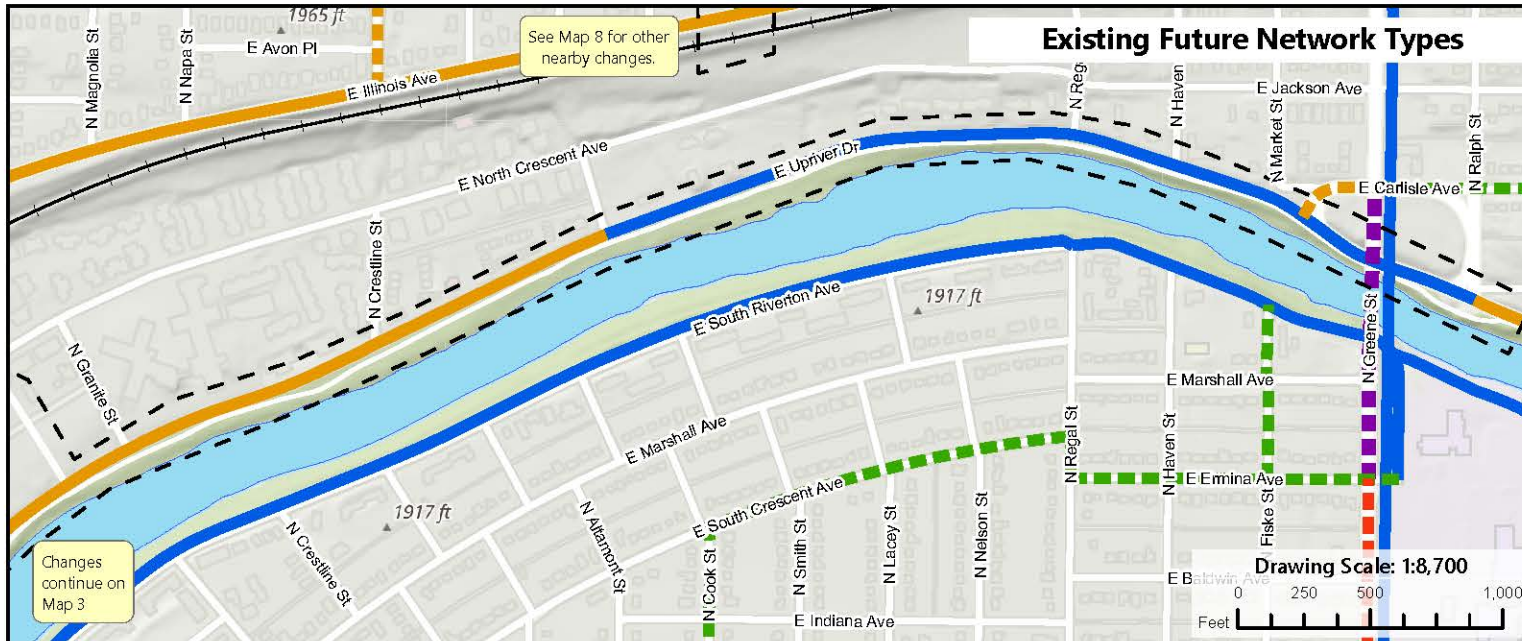
# Z20-019COMP: Map TR-5, Proposed Modification 12 (Upriver Drive and Centennial Trail)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/17/2020

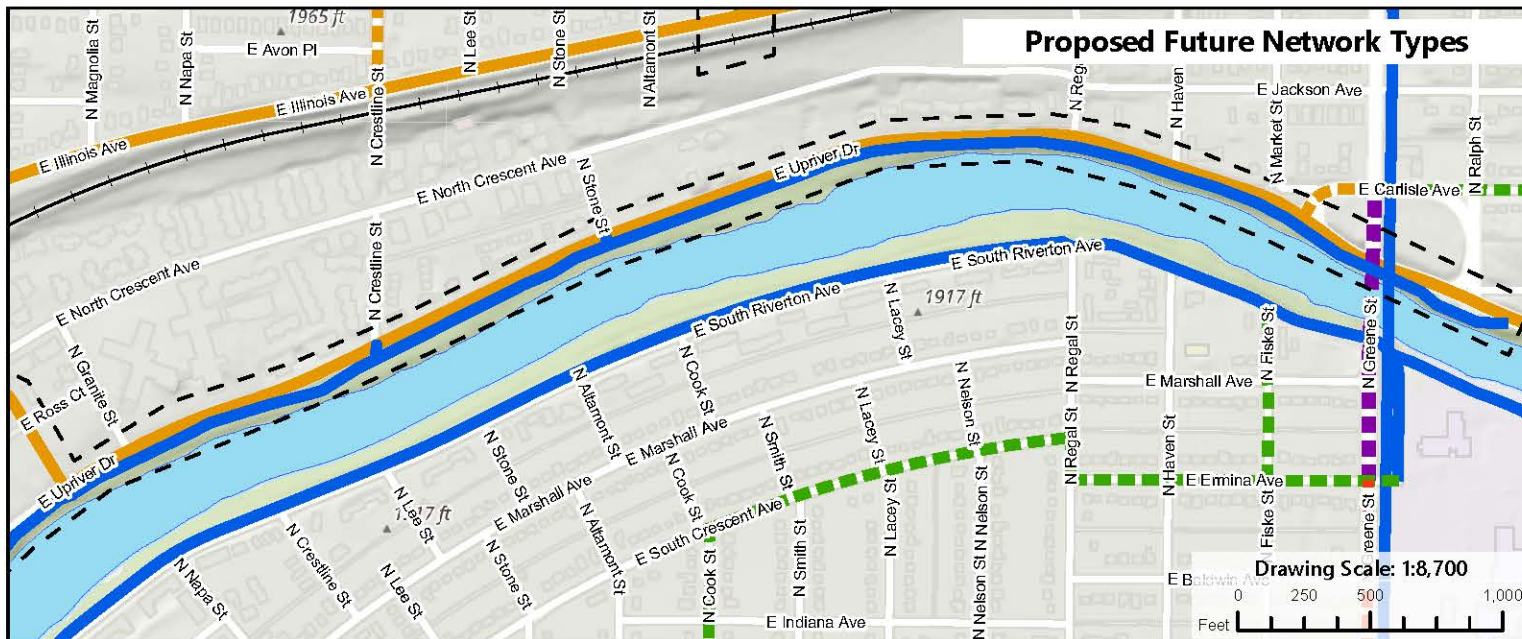
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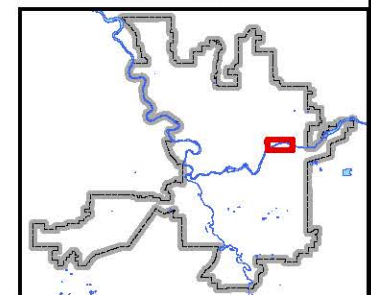
## Legend

- Area of Proposed Change
- Railroad
- Waterbody
- Current Bikeway Network**
  - Bike Friendly Route
  - Closed to Bike
  - Difficult Connection
  - High Traffic (Bike Lane)
  - High Traffic (Shared)
  - Moderate Traffic (Bike Lane)
  - Moderate Traffic (Shared)
  - Neighborhood Greenway
  - Shared Use Path
  - Soft Surface Path



Length of Change: 1.06 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



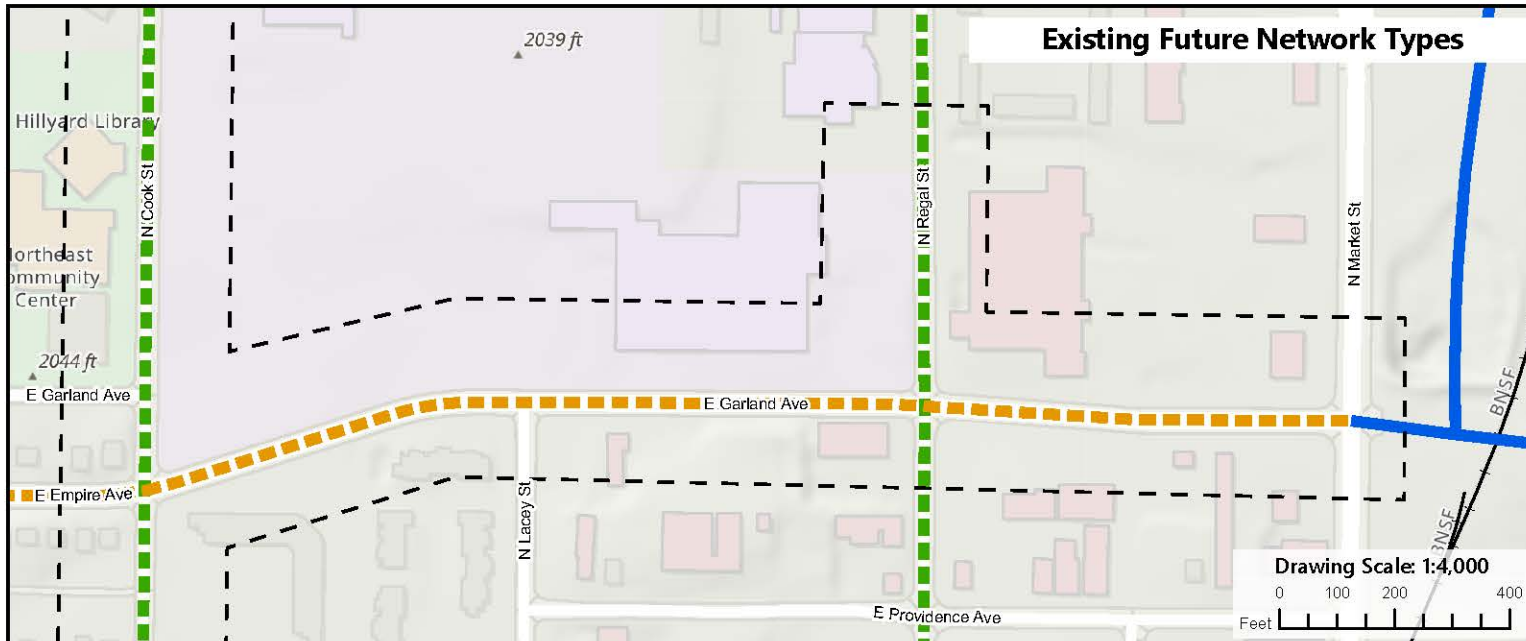


# **Z20-019COMP: Map TR-5, Proposed Modification 13** **(Garland Ave between Cook St and Market St)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/29/2020

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The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.



## **Legend**

[ ] Area of Proposed Change

— Railroad

### **Current Bikeway Network**

— Bike Friendly Route

— Closed to Bike

— Difficult Connection

— High Traffic (Bike Lane)

— High Traffic (Shared)

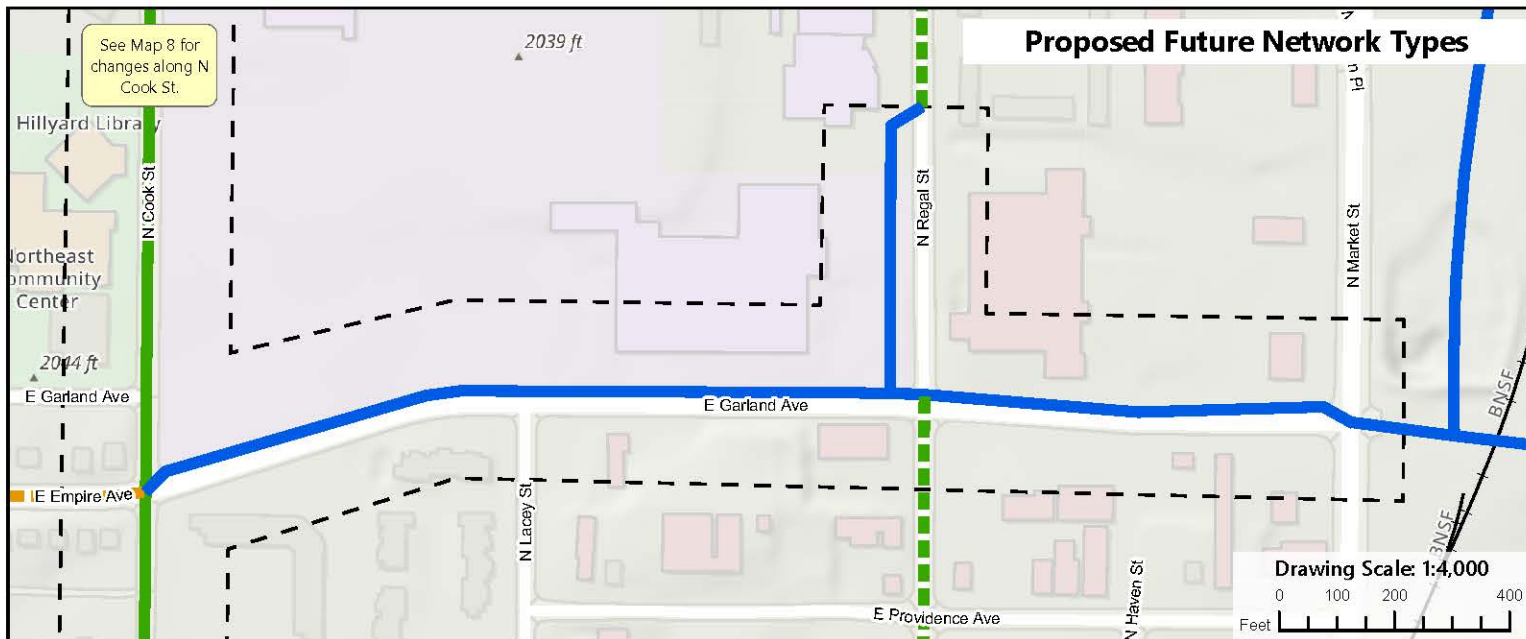
— Moderate Traffic (Bike Lane)

— Moderate Traffic (Shared)

— Neighborhood Greenway

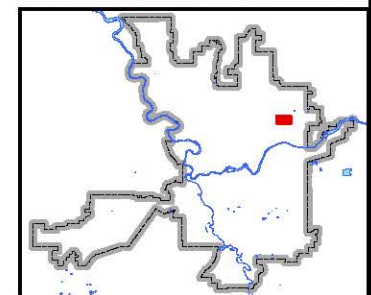
— Shared Use Path

— Soft Surface Path



Length of Change: 0.51 Miles

## **PROJECT LOCATION**



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



**STAFF REPORT Z20-019COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed Comprehensive Plan Amendment to the current Comprehensive Plan for the City of Spokane. The proposal constitutes a requested change to Map TR5 of the Comprehensive Plan in Chapter 4: Transportation, and related text amendments to the City of Spokane Bicycle Master Plan, located in Appendix D of the Comprehensive Plan. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                 |
|---------------------------|---------------------------------|
| <b>Parcel(s):</b>         | NA - Various locations citywide |
| <b>Address(es):</b>       | NA – Various locations citywide |
| <b>Property Size:</b>     | Not applicable                  |
| <b>Legal Description:</b> | Not applicable                  |
| <b>General Location:</b>  | Public rights-of-way            |
| <b>Current Use:</b>       | Bicycle facilities              |

**II. APPLICANT SUMMARY**

|                        |                                                                                                                    |
|------------------------|--------------------------------------------------------------------------------------------------------------------|
| <b>Staff contact:</b>  | Colin Quinn-Hurst, Assistant Planner, <a href="mailto:cquinnhurst@spokanecity.org">cquinnhurst@spokanecity.org</a> |
| <b>Applicant:</b>      | City of Spokane                                                                                                    |
| <b>Property Owner:</b> | City of Spokane                                                                                                    |

**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                             |
|---------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | NA                                                                                                                                          |
| <b>Proposed Land Use Designation:</b> | NA                                                                                                                                          |
| <b>Current Zoning:</b>                | NA                                                                                                                                          |
| <b>Proposed Zoning:</b>               | NA                                                                                                                                          |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 14, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                           |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                              |
| <b>Staff Recommendation:</b>          | Recommended                                                                                                                                 |



## IV. BACKGROUND INFORMATION

1. **General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the City of Spokane proposes to amend Map TR-5 in Chapter 4 of the Comprehensive Plan to update and keep current planned bikeway facility designations, and to amend text in Appendix D of the Comprehensive Plan to be consistent with proposed map amendments. The proposal seeks to update future facility designations for segments of the planned bikeway network to be consistent with available right-of-way, engineering assessment, neighborhood plans and proposals, and community feedback.
2. **Site Description and Physical Conditions:** The proposal concerns changes to planned bikeway facilities, as defined in Map TR5, in various locations citywide. A total of thirteen locations are addressed by these changes, concerning segments of Cowley Street, Strong Road, Upriver Drive, 17<sup>th</sup> Avenue, 10<sup>th</sup> Ave/11<sup>th</sup> Ave/Altamont Boulevard, Altamont Street, Flint Road, Cook Street, Palouse Highway, Boone Avenue, Atlantic Street, Sharp Avenue, Pittsburg Street, and Garland Avenue.
3. **Property Ownership:** All proposed changes are within City right-of-way.
4. **Adjacent Property Improvements and Uses:** Property uses are of various types citywide, including residential, industrial and commercial uses.
5. **Street Class Designations:** The streets addressed by this change are of various street class designations as follows:
  - a. **Cowley Street** – Urban Major Collector
  - b. **Strong Road** – Urban Major Collector
  - c. **Upriver Drive** – Urban Minor Arterial
  - d. **17<sup>th</sup> Avenue** – Urban Minor Collector
  - e. **10<sup>th</sup> Ave/11<sup>th</sup> Ave/Altamont Boulevard** – Urban Local Access
  - f. **Altamont Street** – Urban Minor Arterial
  - g. **Flint Road** – Urban Major Collector
  - h. **Cook Street** – Urban Local Access
  - i. **Palouse Highway** – Urban Minor Arterial
  - j. **Boone Avenue** – Urban Minor Arterial
  - k. **Atlantic Street** – Urban Minor Arterial
  - l. **Sharp Avenue** – Urban Minor Arterial
  - m. **Pittsburg Street** – Urban Local Access
  - n. **Garland Avenue** – Urban Minor Arterial
6. **Current Land Use Designation and History:** N/A
7. **Proposed Land Use Designation:** N/A
8. **Current Zoning and History:** N/A
9. **Proposed Zoning:** N/A



## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | June 24, 2020     |
| Public Workshop .....                                         | July 29, 2020     |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |
| SEPA Determination Issued .....                               | August 24, 2020   |
| Notice of Public Hearing Posted .....                         | August 26, 2020   |
| Plan Commission Hearing Date (Scheduled) .....                | September 9, 2020 |

2. **Comments Received:** A request for comments was issued to City departments, local agencies, and departments along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from the following:

- Randy Abrahamson – Spokane Tribe of Indians
- Joeline Eliason – City of Spokane Development Services Center
- Dave Kokot – City of Spokane Fire Department
- Bobby Halbig – City of Spokane Streets Department
- Inga Note – City of Spokane, Integrated Capital Management

No concerns or objections were identified in these comments. Comments from City of Spokane Integrated Capital Management identified the need to change the future facility type for Garland Avenue in order to be consistent with the City of Spokane's Six-Year Streets Plan, and to clarify in the text of the Bicycle Master Plan, an appendix to the Comprehensive Plan, how protected bike lane facilities relate to the facilities identified in Map TR 5. Following additional review, those changes and a correction to the extent of Modification 2 for W. Strong Road were added to the proposal, and a revised Notice of Application was released on June 9, 2020.

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 in the Spokesman Review. The following comments were received during the 60-day public comment period:

- Karen Carlberg
- Carol Tomsic
- Patrick McCann
- Eileen Hyatt
- Linda Carroll
- Wyatt Schroeder
- Kevin Flatt
- Laurie Fleming
- Justin Haller
- Melvin Neil
- Erik Powell
- Tim Shauvin
- Cindie Smith
- Jessica Engelman
- Spokane Public Facilities District

- 3. Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 10, 2020, during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The applicant was provided an opportunity to speak during the workshop but no public comment was taken. No changes were proposed at the workshop.

An online public workshop for the general public was held on July 30, 2020. Questions were answered and comments received. No changes were proposed at the workshop.

During the public comment period, presentations were also provided to the Bicycle Advisory Board on June 16, 2020, to the Pedestrian Transportation and Traffic Committee of the Community Assembly on June 23, 2020, and to the Plan Commission Transportation Subcommittee on July 7, 2020.

## **VI. APPLICATION REVIEW AND ANALYSIS**

- 1. Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:

- A.** Keep the comprehensive plan alive and responsive to the community.
- B.** Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
- C.** Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.



- D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
  - E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
  - F. Amendments to the comprehensive plan must result in a net benefit to the general public.
2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested. The applicable criteria are shown below in *italic print*. Following each requirement is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. **GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. **Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: The City did not require, nor did any Agency comment request or require a traffic impact analysis for the proposal. There will be no immediate impact to the city budget and it is expected that state and federal grants will support these improvements within the next 20 years.

The proposal meets this criterion.



- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

**E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal.

*Neighborhood Planning Documents Adopted after 2001.* The proposal is consistent with the goals and policies of affected neighborhood plans. Proposed changes are consistent with the bicycle facility recommendations in the following neighborhood plans:

- *Five Mile Prairie Neighborhood Plan for Bicycle and Pedestrian Improvements, Figure E-1*
- *South Hill Coalition Connectivity and Livability Strategic Plan – Lincoln Heights, Manito/Cannon Hill, and Rockwood Neighborhoods - Project Map, pg. 41*
- *Greater Hillyard North-East Planning Alliance Report and Final Proposals – Bemiss, Hillyard and Whitman Neighborhoods - Objective 6.1, 6.4, 6.5*

The proposed amendments do not conflict with the neighborhood planning documents for each neighborhood in which a proposed amendment is located:

- *Logan Neighborhood Form-Based Code Subarea Plan*
- *East Central Ben Burr Trailhead Planning*



- *Southgate Neighborhood Transportation & Connectivity Element* – Page 5, Major Organizing Concepts, Pages 7 and 8 – Green Ring and Ben Burr Trail Extension
- *Nevada Lidgerwood Neighborhood Planning – Phase II Summary, Non-motorized Travel Safety, and Traffic Patterns – Findings and Implications*

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in **Exhibit D** of this report. Further discussion of these policies is provided under section K.2 below.

The proposal meets this criterion.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: The proposal is generally consistent with current Comprehensive Plan policies, as described in further detail in the staff analysis of Criterion K.2 below and other criteria in this report. Therefore, no amendment to policy wording is necessary and this criterion does not apply to the subject proposal.

The proposal meets this criterion.

- F. **Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** The proposed change in facility designations are consistent with regional transportation plans and countywide planning policies (CWPP), updating future facility designations on selected street segments already identified as bicycle corridors in regional transportation plans and aligning with transportation plans of adjacent jurisdictions. No comments have been received from any agency, City department, or neighboring jurisdiction which would indicate that this proposal is not regionally consistent.

The proposal meets this criterion.

- G. **Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*



2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

**I. Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would not impact the City's ability to provide transportation facilities at the planned level of service.

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<sup>3</sup> State Environmental Policy Act



The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

K. **Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: Not applicable.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: Not applicable.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: The proposed adjustments to Map TR-5 better carry out Comprehensive Plan policies TR 1 - Transportation Network for All Users, TR 5 - Active Transportation, and TR 7 - Neighborhood Access. These adjustments better achieve these policies by correcting inaccuracies to align with existing facilities and upgrading bikeway facility recommendations to be consistent subarea plans, neighborhood council recommendations, and current local, regional and national design standards for given roadway conditions.

3. **Rezones, Land Use Plan Amendment:** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally*



*consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: Not applicable.

The proposal meets this criterion.

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal is consistent with the approval criteria set forth by SMC 17G.020.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to the Land Use Plan map of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Considering the above information and the whole of the administrative record, staff recommends that Plan Commission and the City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                        |                                           |
|----------------------------------------|-------------------------------------------|
| A. Proposed Map Amendments             | F. SEPA Checklist                         |
| B. Proposed Text Amendment             | G. SEPA Determination of Non-Significance |
| C. Currently Adopted Map TR-5          | H. Agency Comments                        |
| D. List of Relevant Comp Plan Policies | I. Public Comments                        |
| E. Application Materials               |                                           |



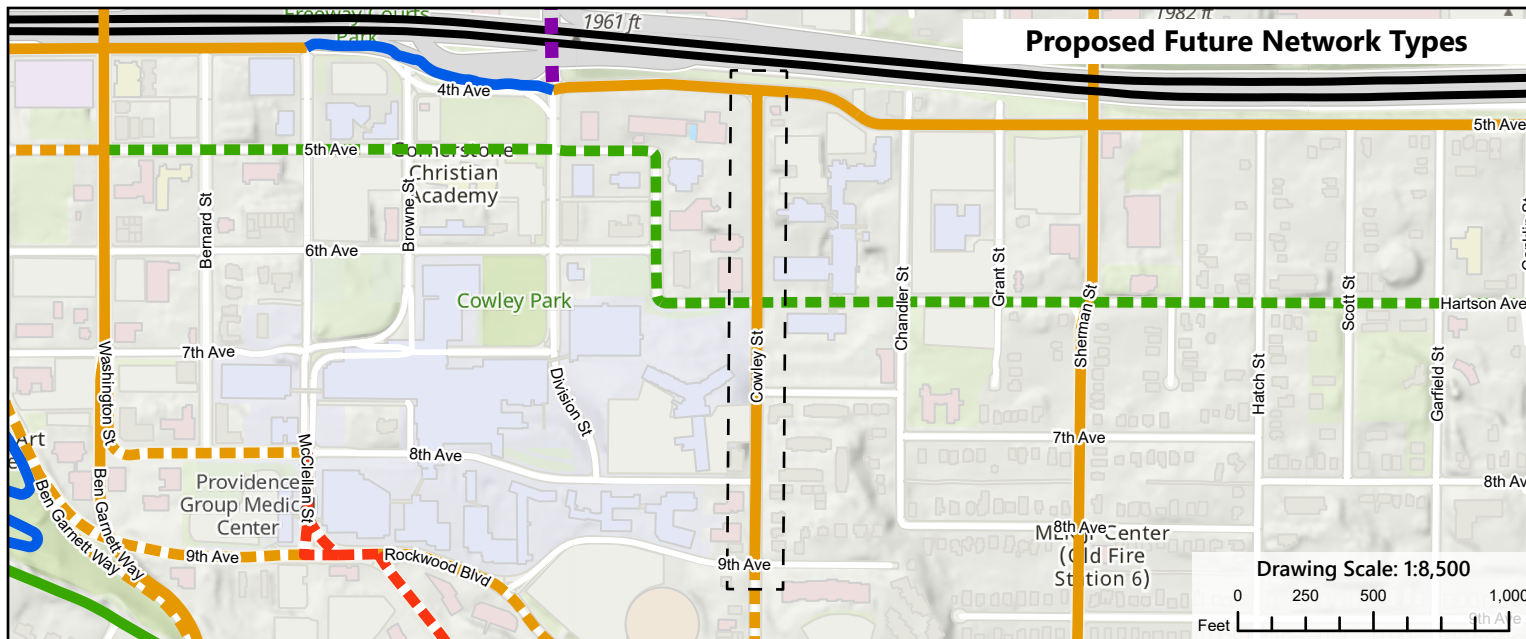
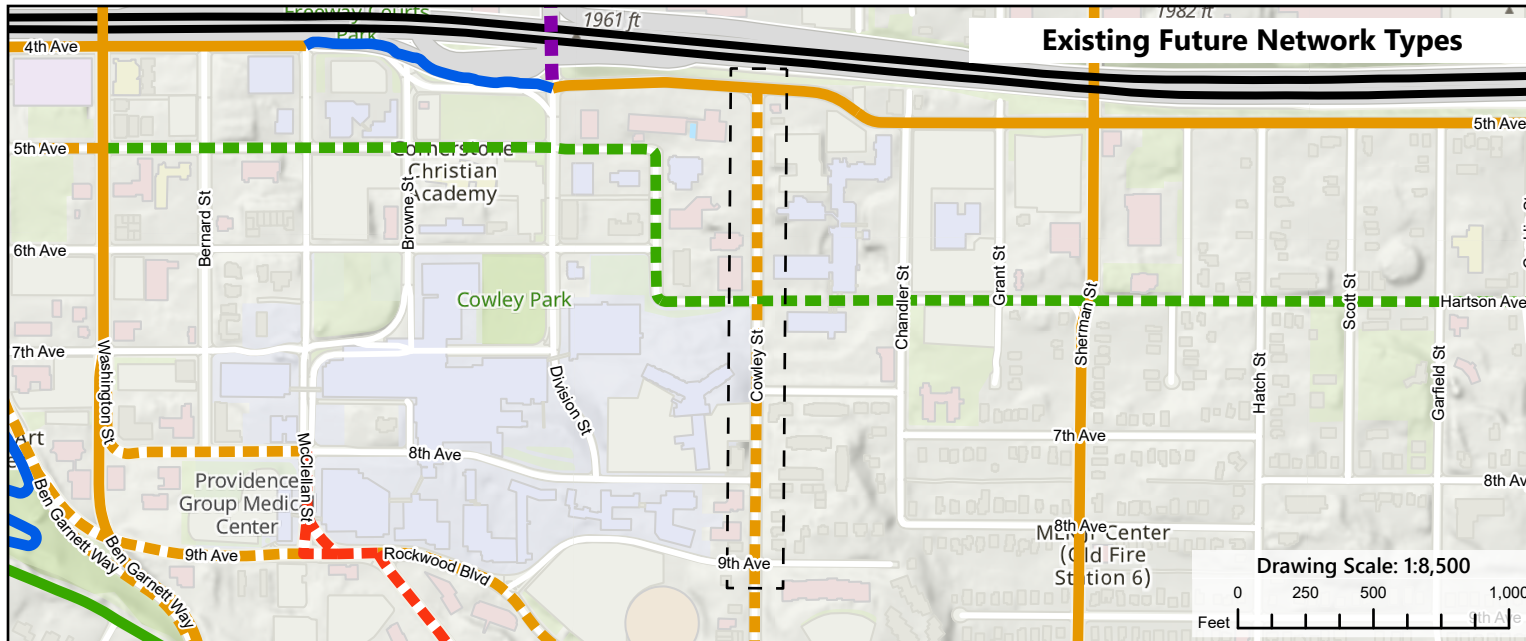


# Z20-019COMP: Map TR-5, Proposed Modification 1 (Cowley St between 4th Ave and 9th Ave) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

THIS IS NOT A LEGAL DOCUMENT

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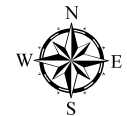


## Legend

[ ] Area of Proposed Change

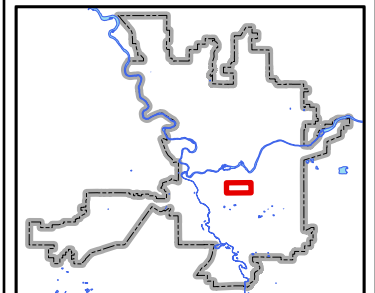
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.33 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



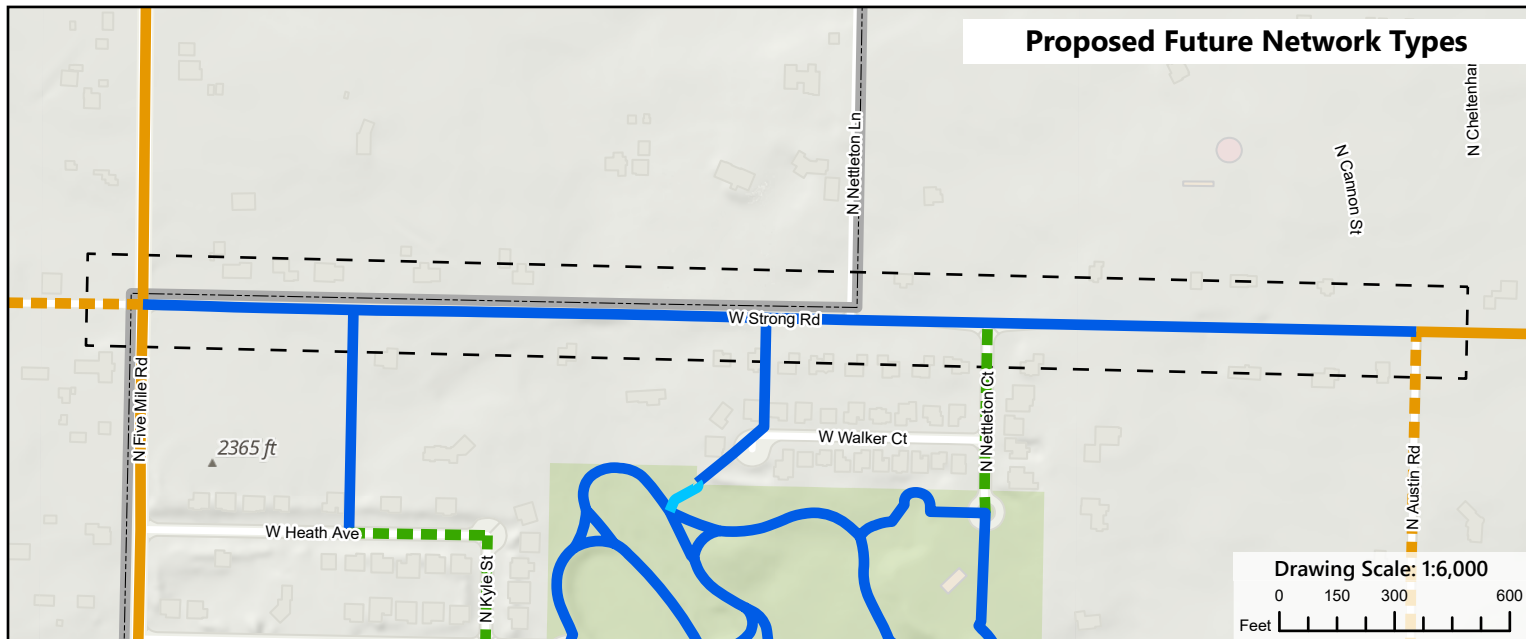
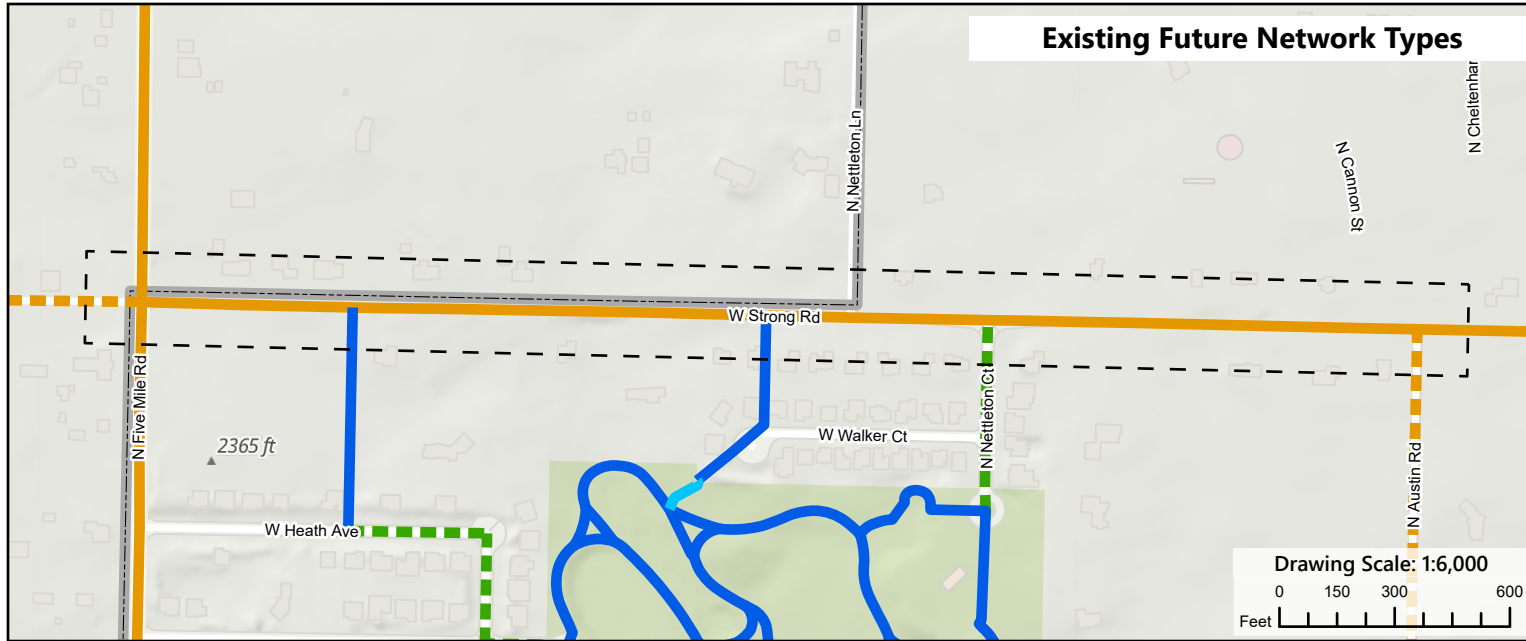


# **Z20-019COMP: Map TR-5, Proposed Modification 2 (Revised June 8)** **(Strong Rd between Five Mile Rd and Austin Rd)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 6/8/2020

THIS IS NOT A LEGAL DOCUMENT

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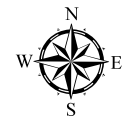


## Legend

[ ] Area of Proposed Change

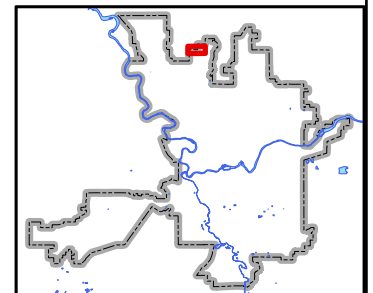
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.63 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



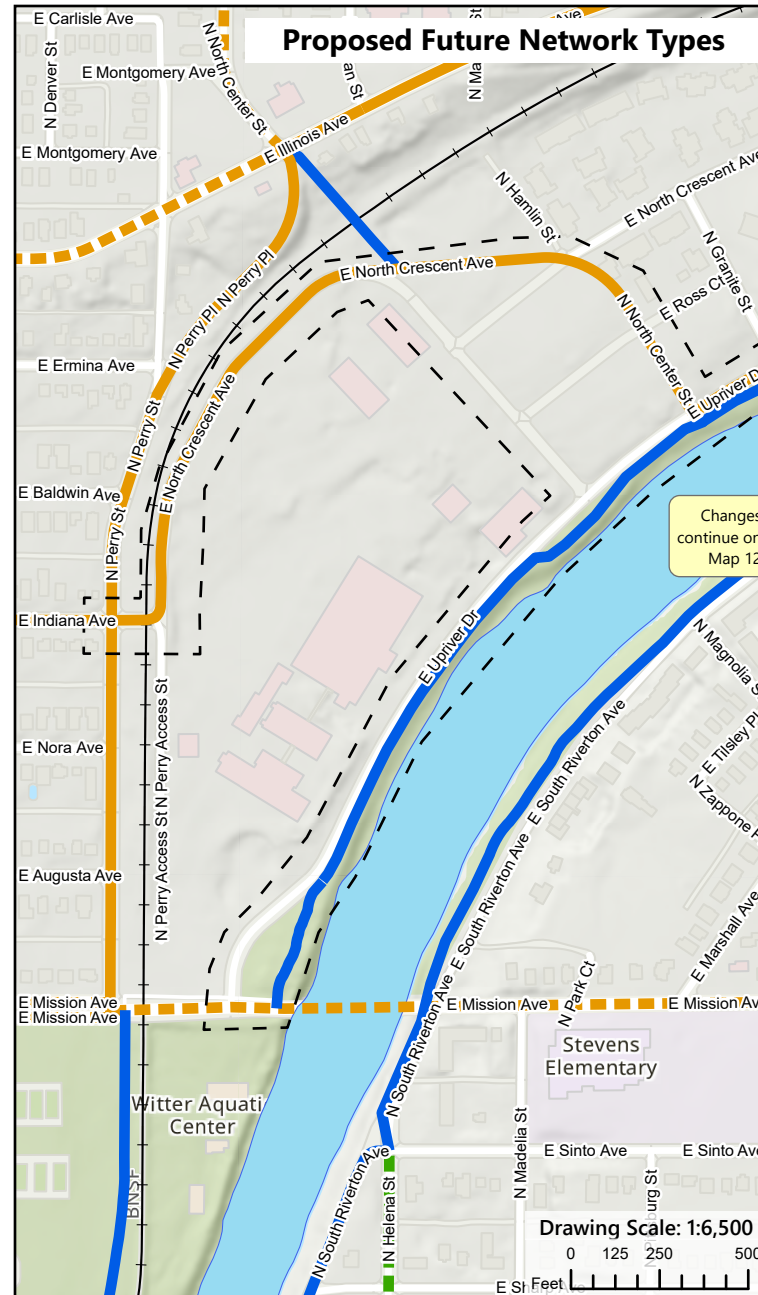
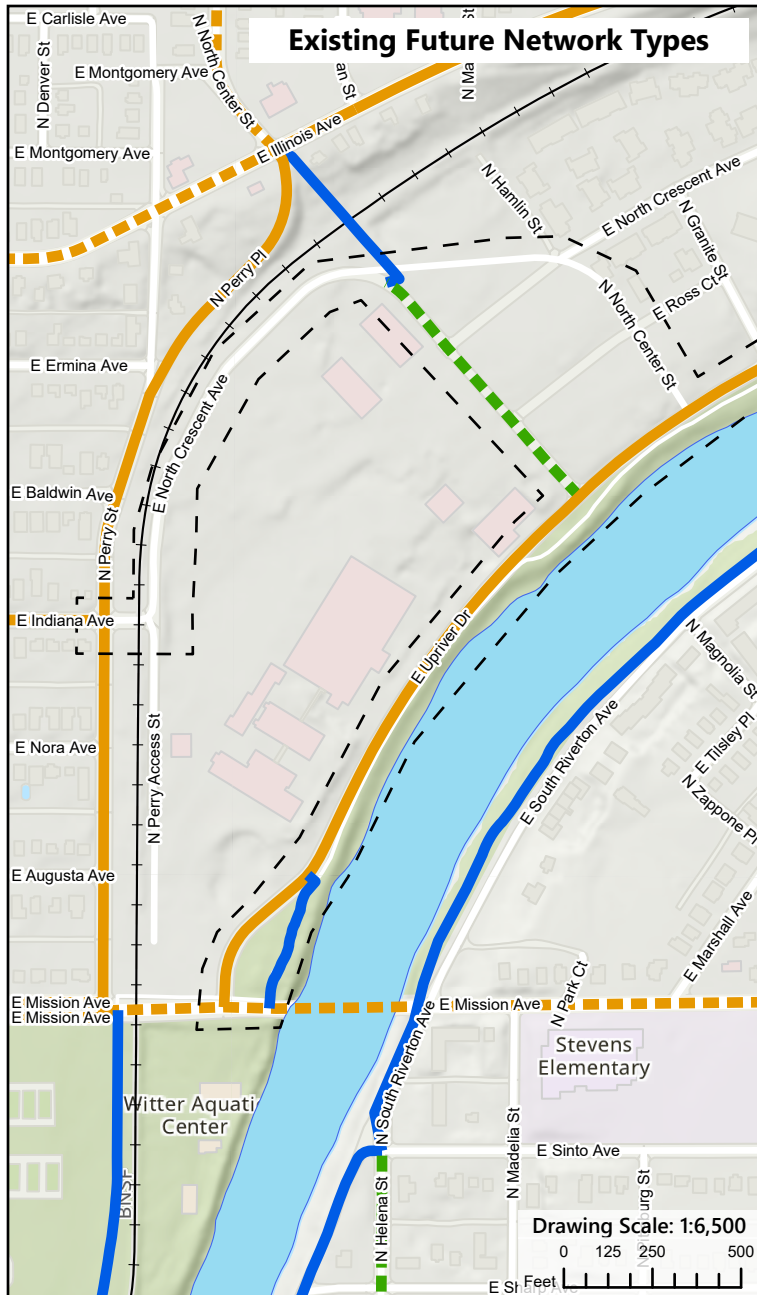


# **Z20-019COMP: Map TR-5, Proposed Modification 3** **(Avista Headquarters Vicinity)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/9/2020

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## Legend

[ ] Area of Proposed Change

— Railroad

Waterbody

### Current Bikeway Network

Bike Friendly Route

Closed to Bike

Difficult Connection

High Traffic (Bike Lane)

High Traffic (Shared)

Moderate Traffic (Bike Lane)

Moderate Traffic (Shared)

Neighborhood Greenway

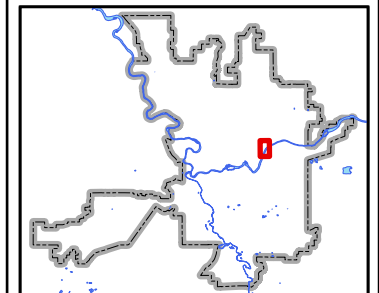
Shared Use Path

Soft Surface Path



Length of Change: 1.06 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

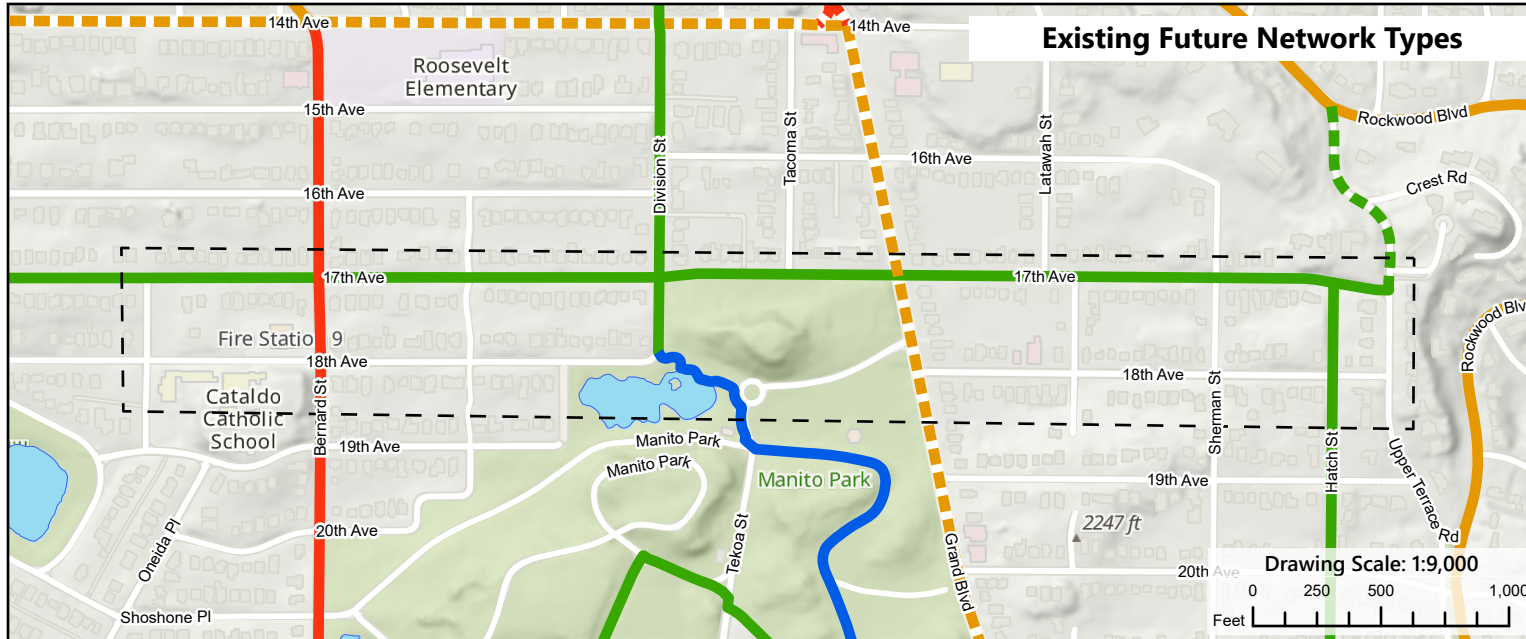




# **Z20-019COMP: Map TR-5, Proposed Modification 4** **(17th Ave between Stevens St and Hatch St)** 2019/2020 Comprehensive Plan Amendment Proposals

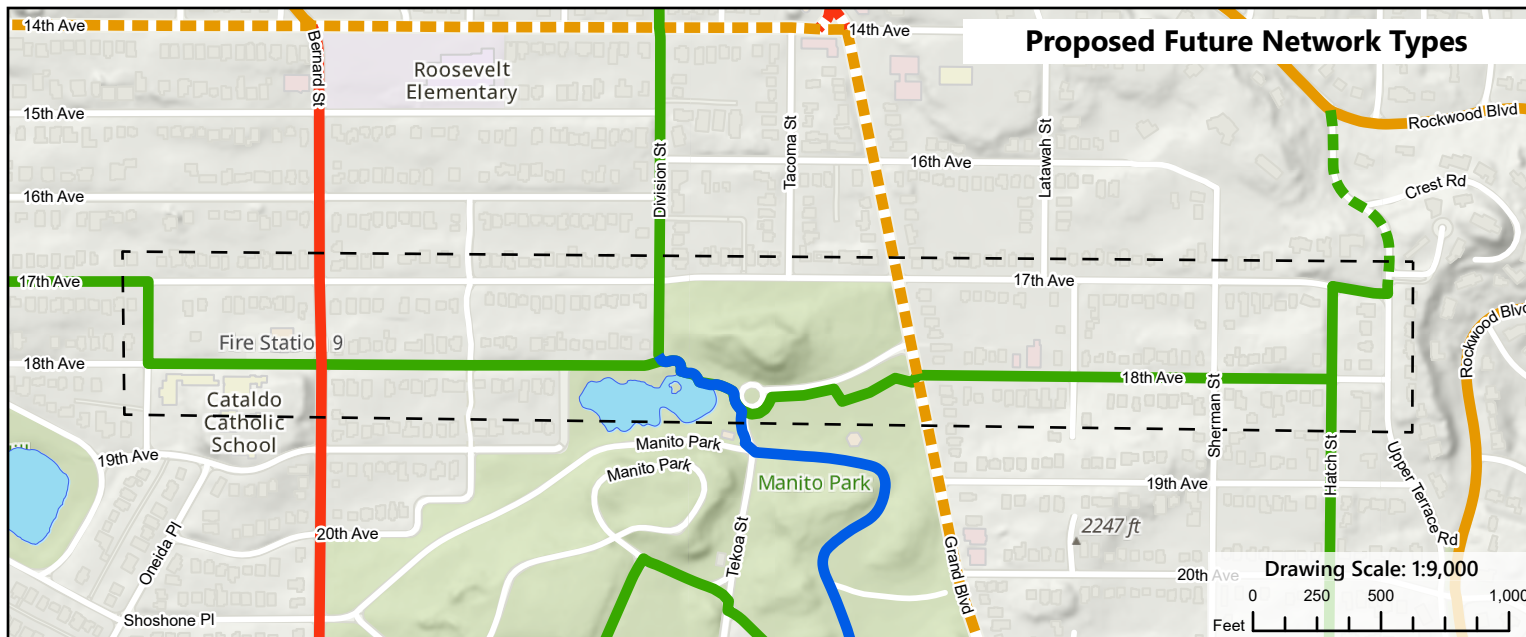
Drawn: 2/11/2020  
**THIS IS NOT A LEGAL DOCUMENT**

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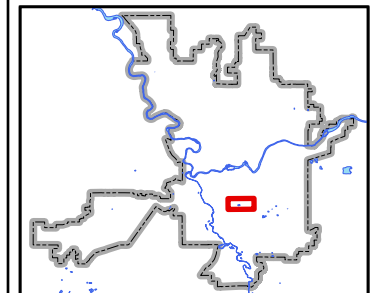
## **Legend**

- Waterbody
- Area of Proposed Change
- Current Bikeway Network**
  - Bike Friendly Route
  - Closed to Bike
  - Difficult Connection
  - High Traffic (Bike Lane)
  - High Traffic (Shared)
  - Moderate Traffic (Bike Lane)
  - Moderate Traffic (Shared)
  - Neighborhood Greenway
  - Shared Use Path
  - Soft Surface Path



Length of Change: 0.92 Miles

## **PROJECT LOCATION**



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

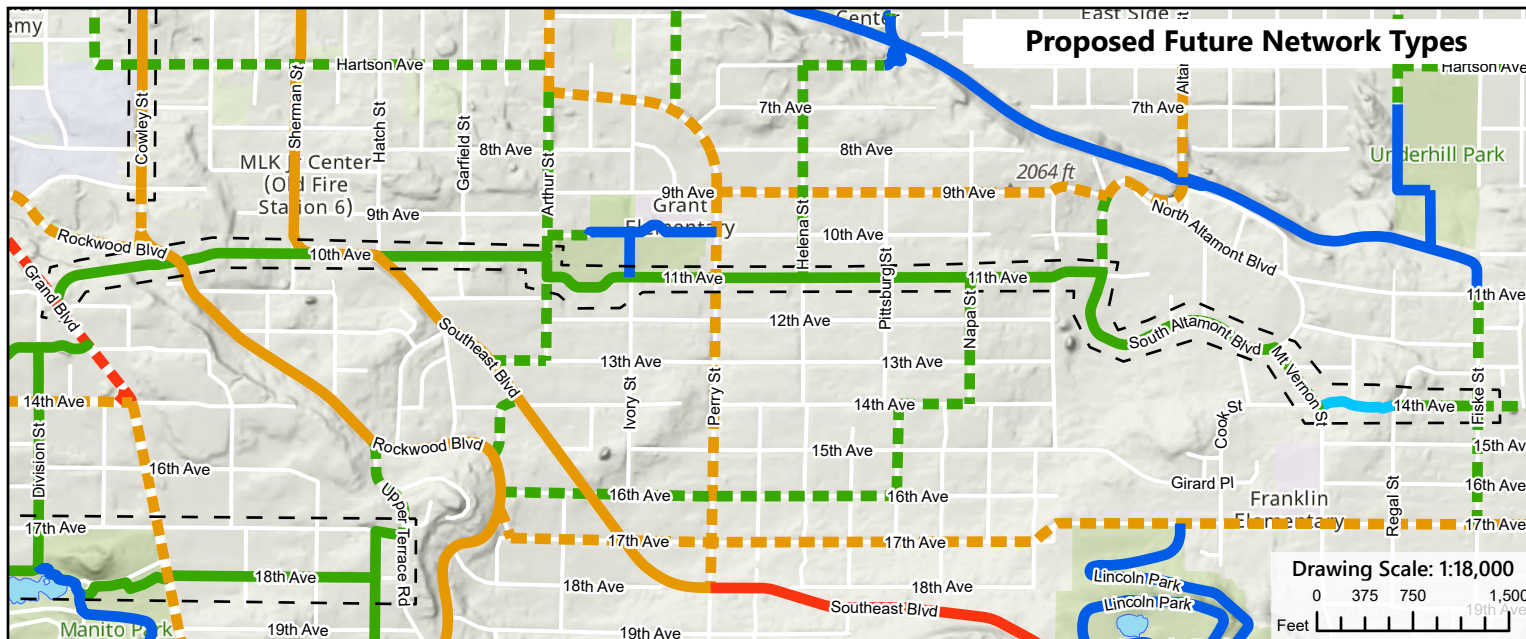
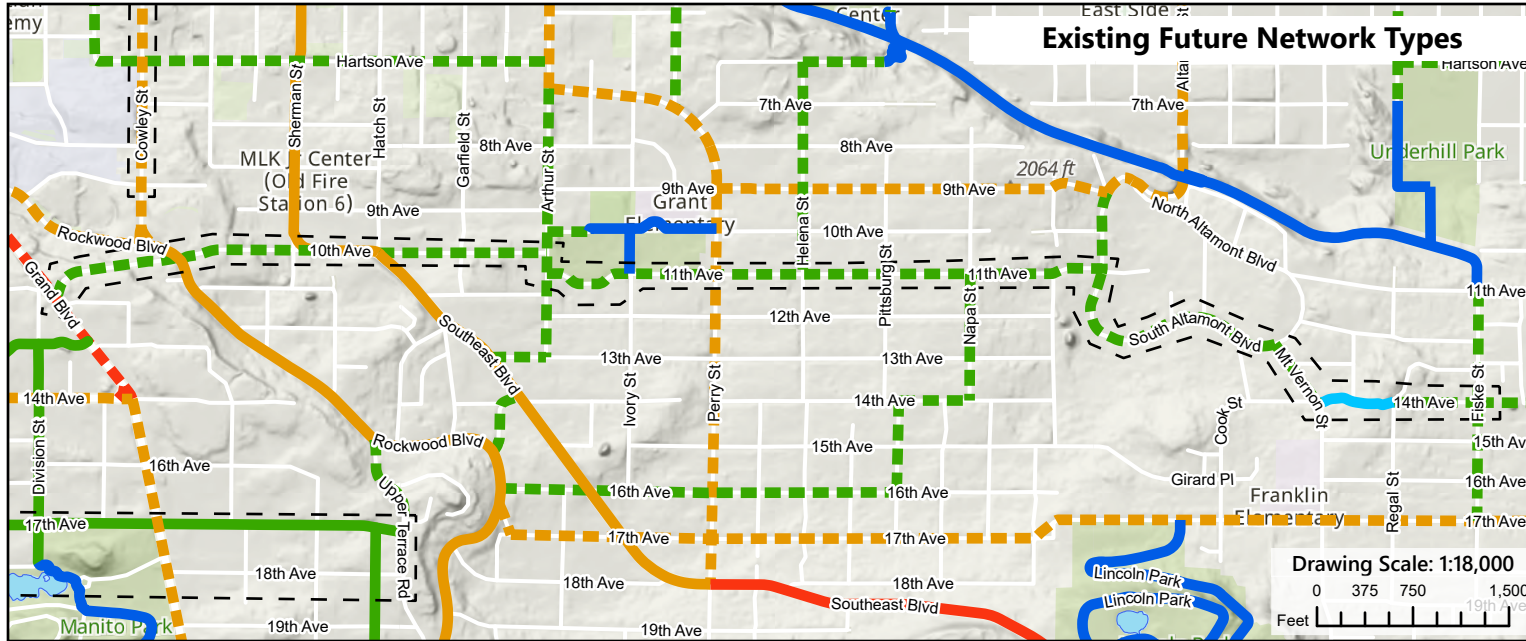




# **Z20-019COMP: Map TR-5, Proposed Modification 5** **(Sumner Ave, 10th Ave, 11th Ave, Altamont Blvd, Mt Vernon St, 14th Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

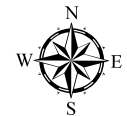
Drawn: 2/11/2020  
THIS IS NOT A LEGAL DOCUMENT

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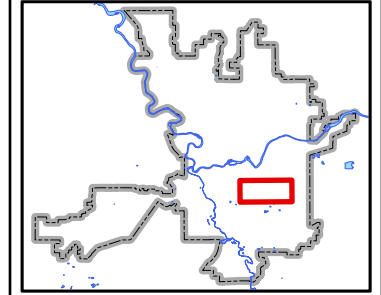
## Legend

- Waterbody
- Area of Proposed Change
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 3.26 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

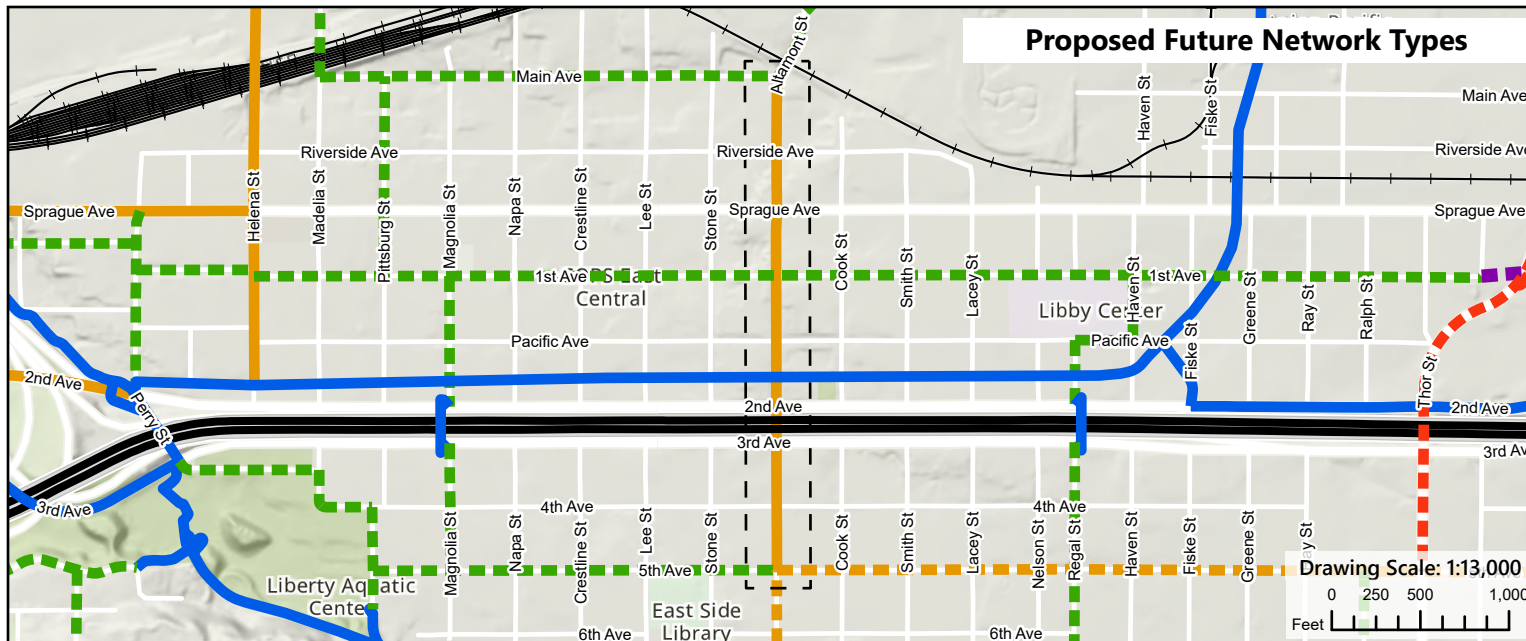
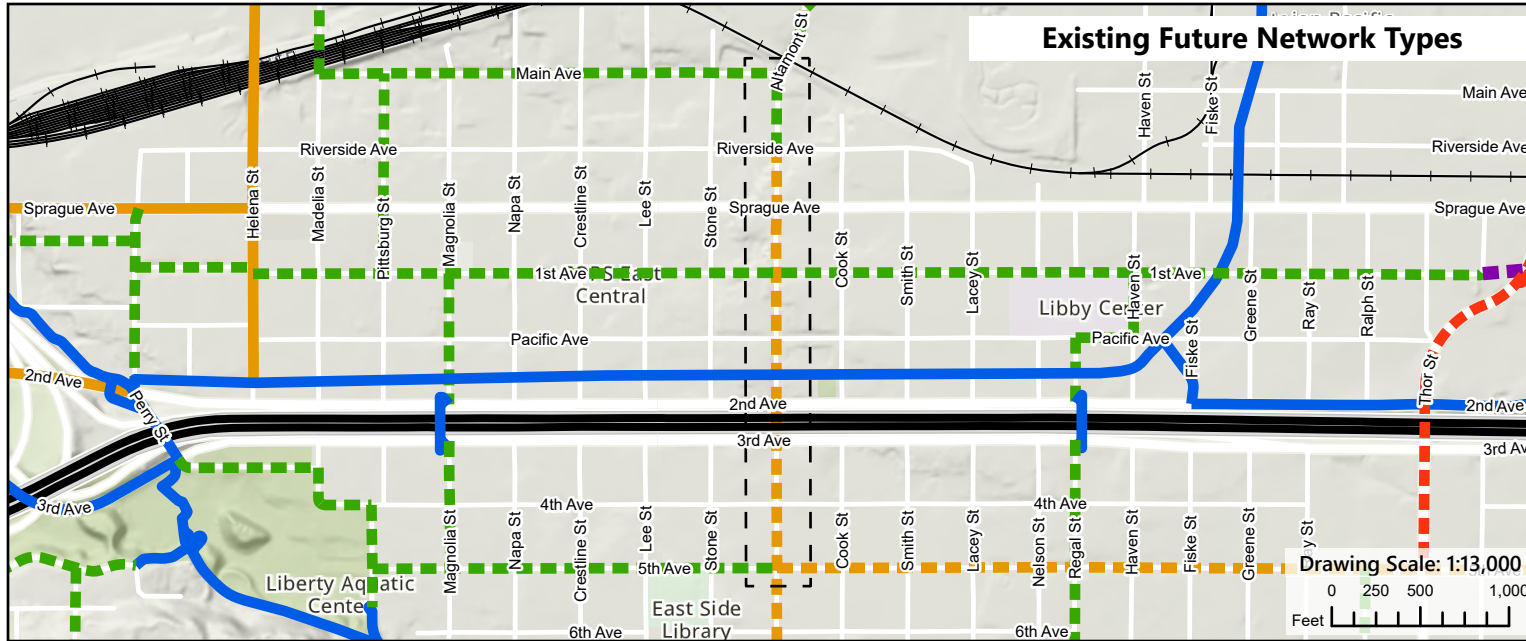




# **Z20-019COMP: Map TR-5, Proposed Modification 6** **(Altamont St between 5th Ave and Main Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020  
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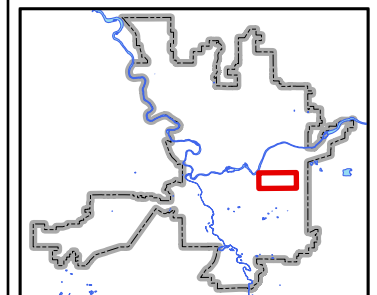
## Legend

- Railroad
- Area of Proposed Change
- Current Bikeway Network**
- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.53 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



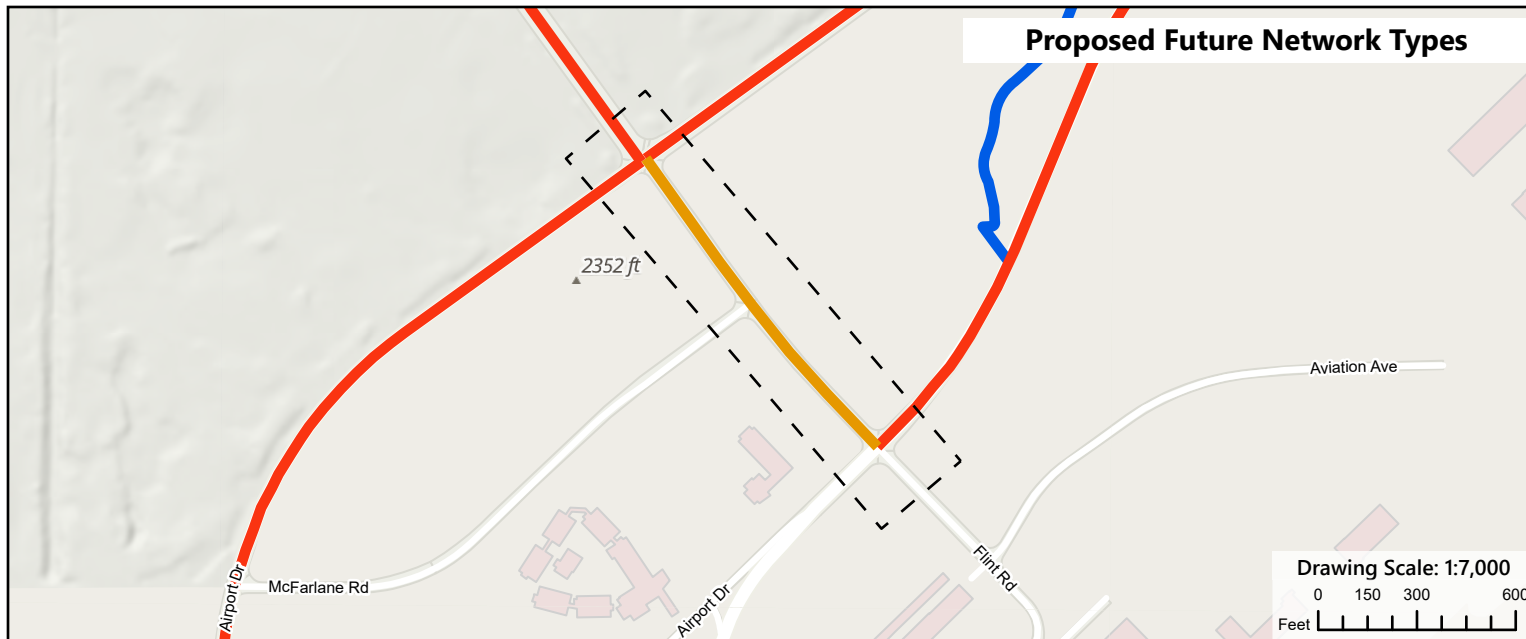
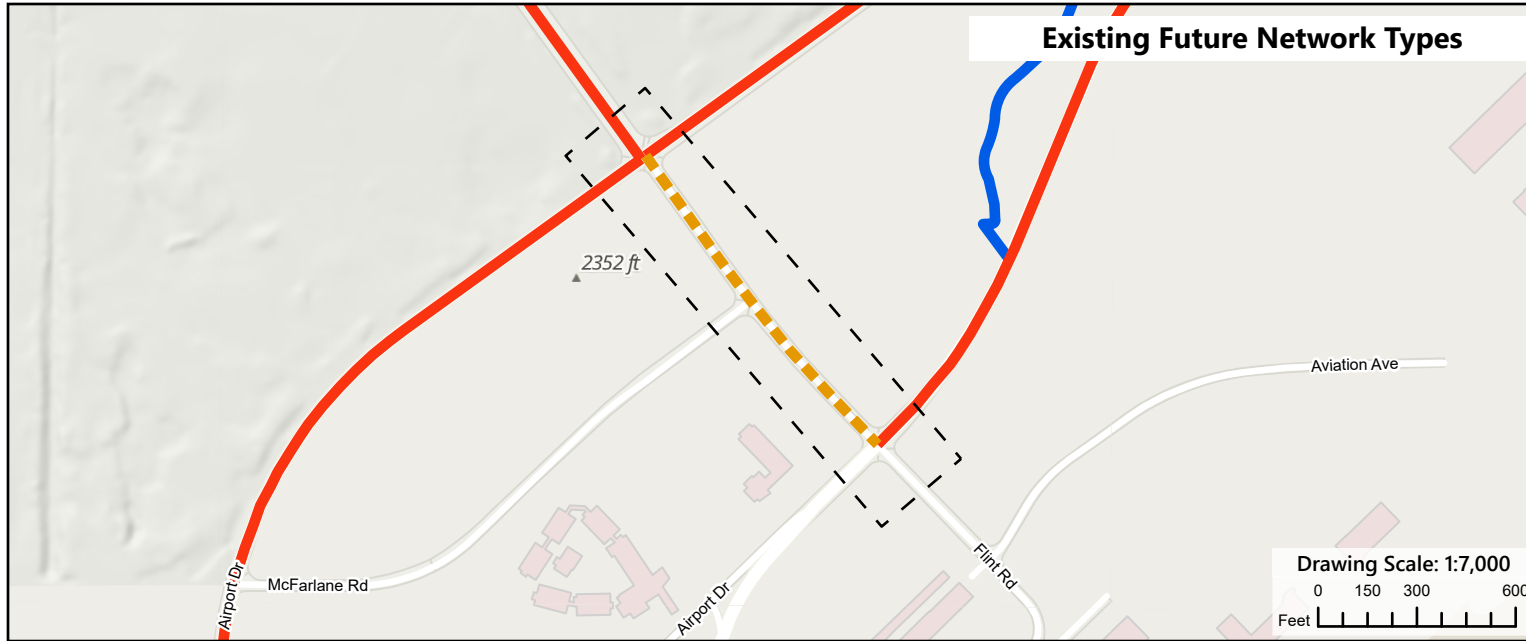


# Z20-019COMP: Map TR-5, Proposed Modification 7 (Flint Rd between Airport Dr and Airport Dr) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020

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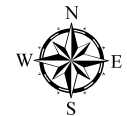


## Legend

[ ] Area of Proposed Change

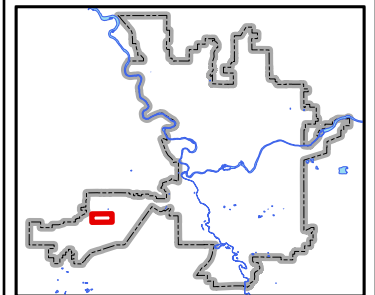
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



Length of Change: 0.21 Miles

### PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott

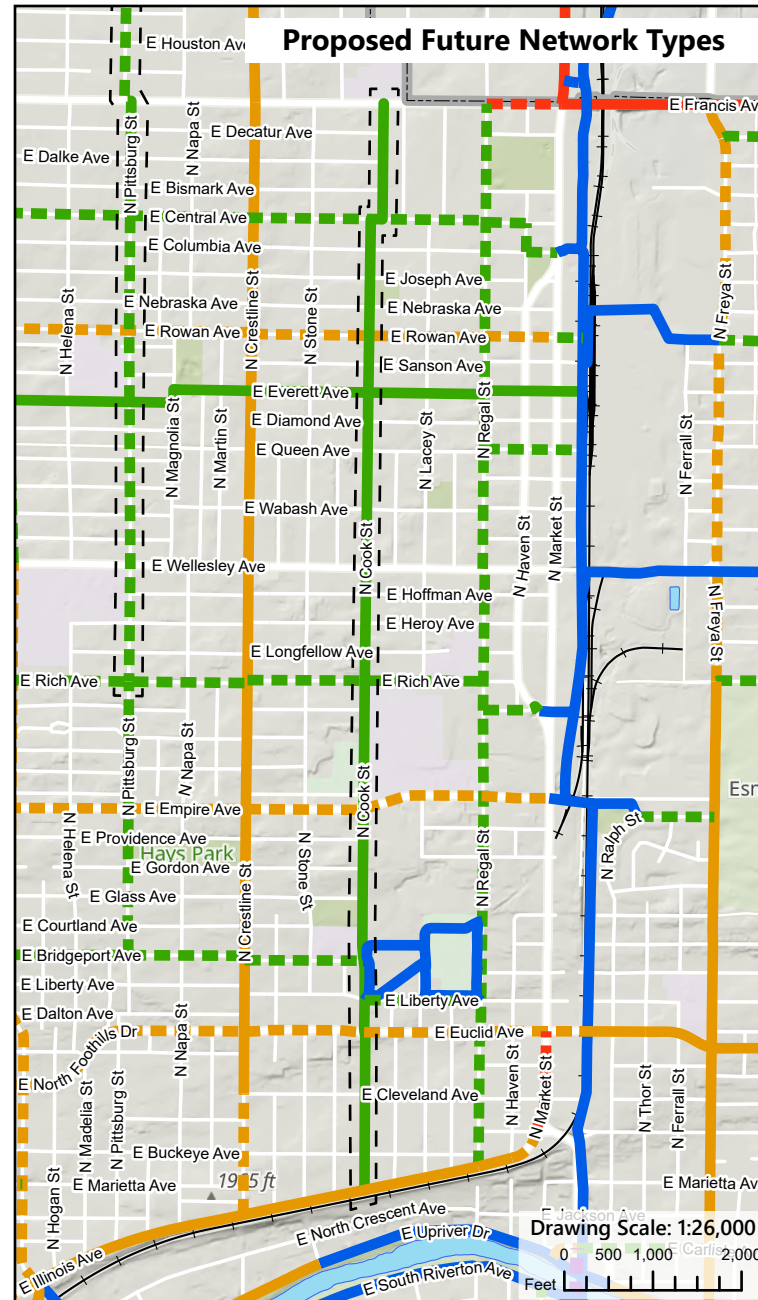
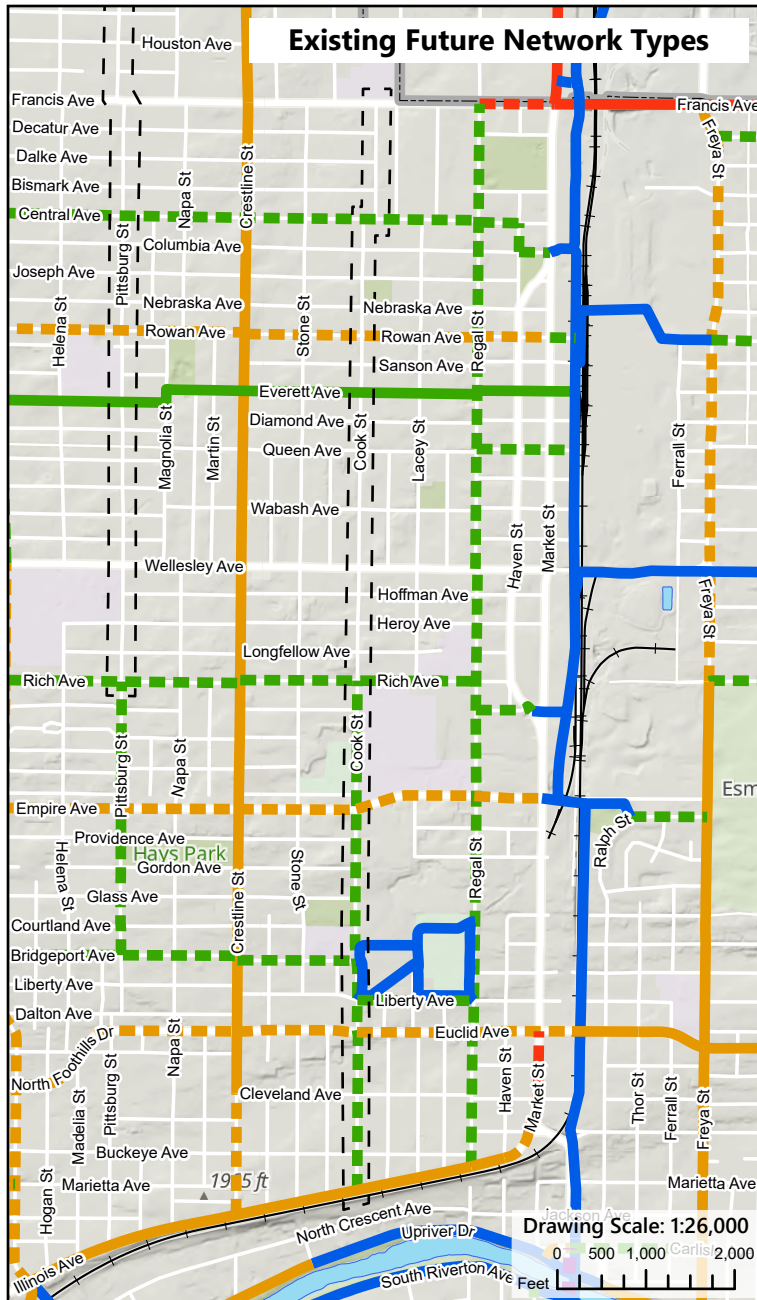




# **Z20-019COMP: Map TR-5, Proposed Modification 8** **(Cook St between Francis Ave and Illinois Ave)** 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/14/2020  
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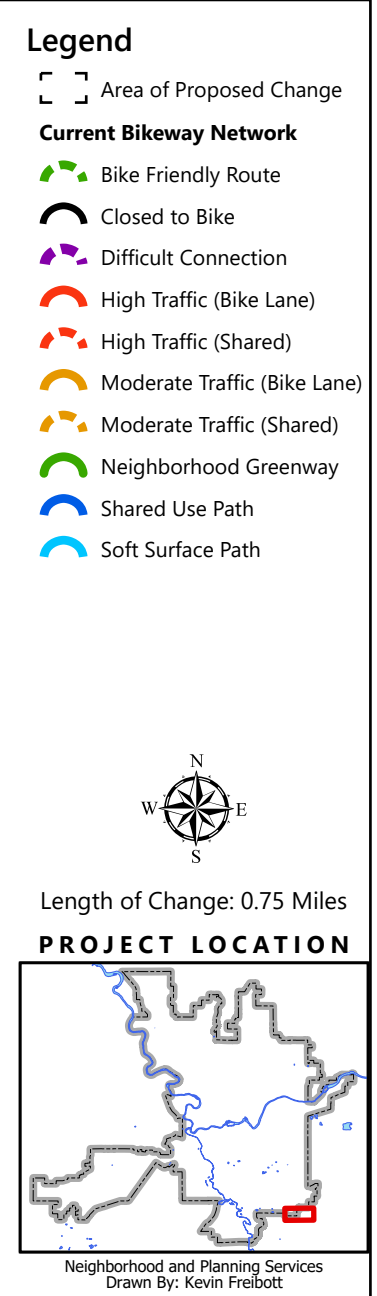
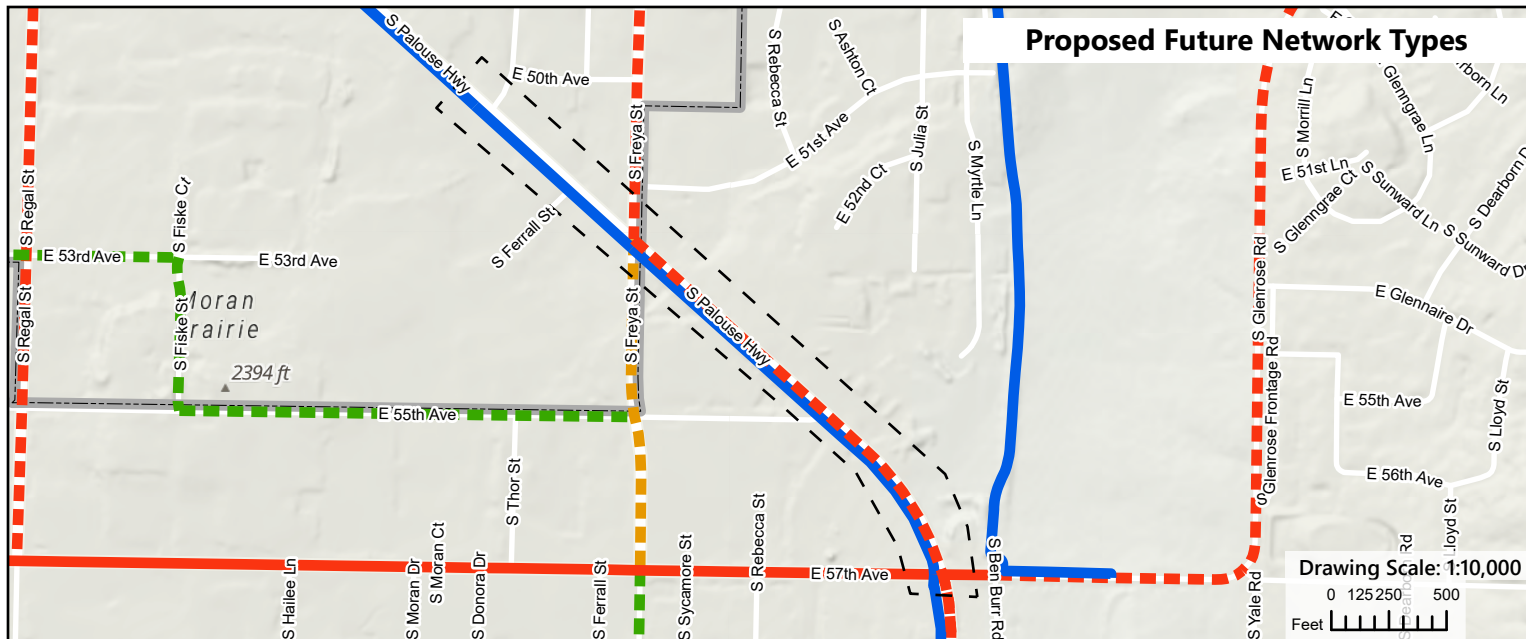
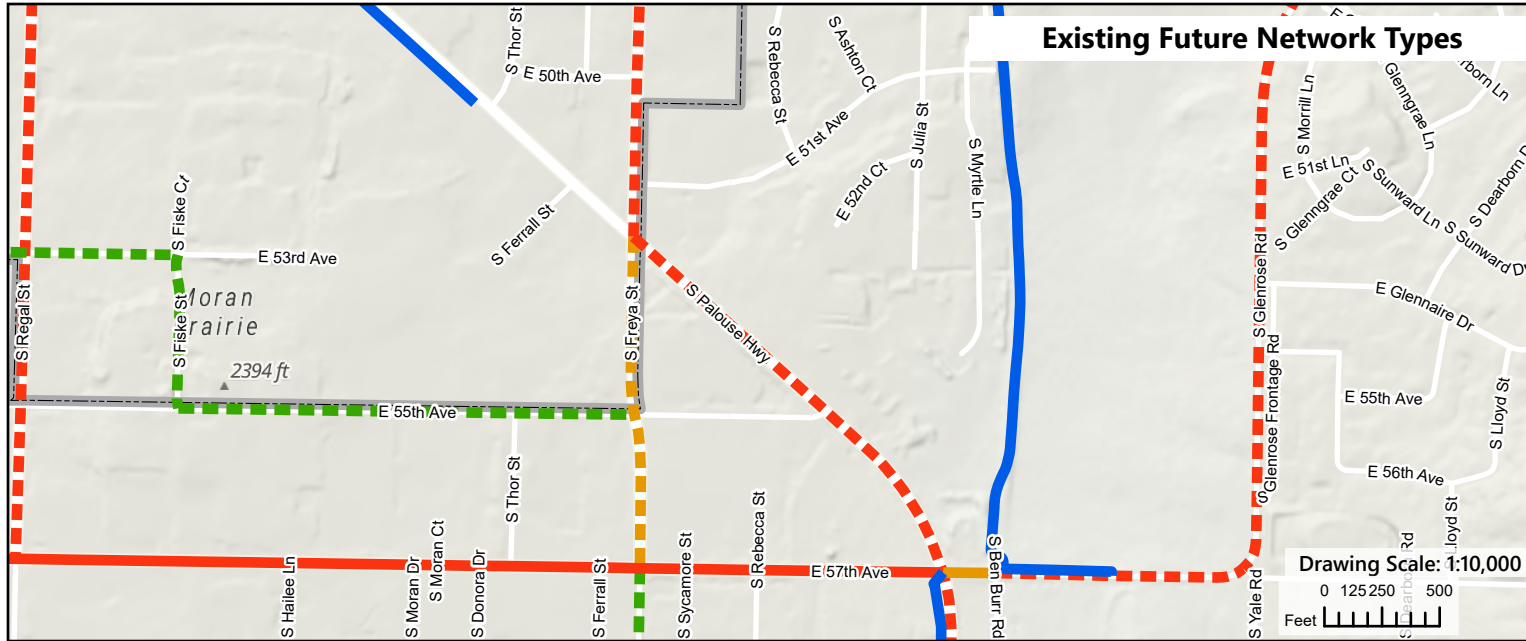
# **Z20-019COMP: Map TR-5, Proposed Modification 9** **(Palouse Hwy between Thor St and 57th Avenue)**

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/7/2020

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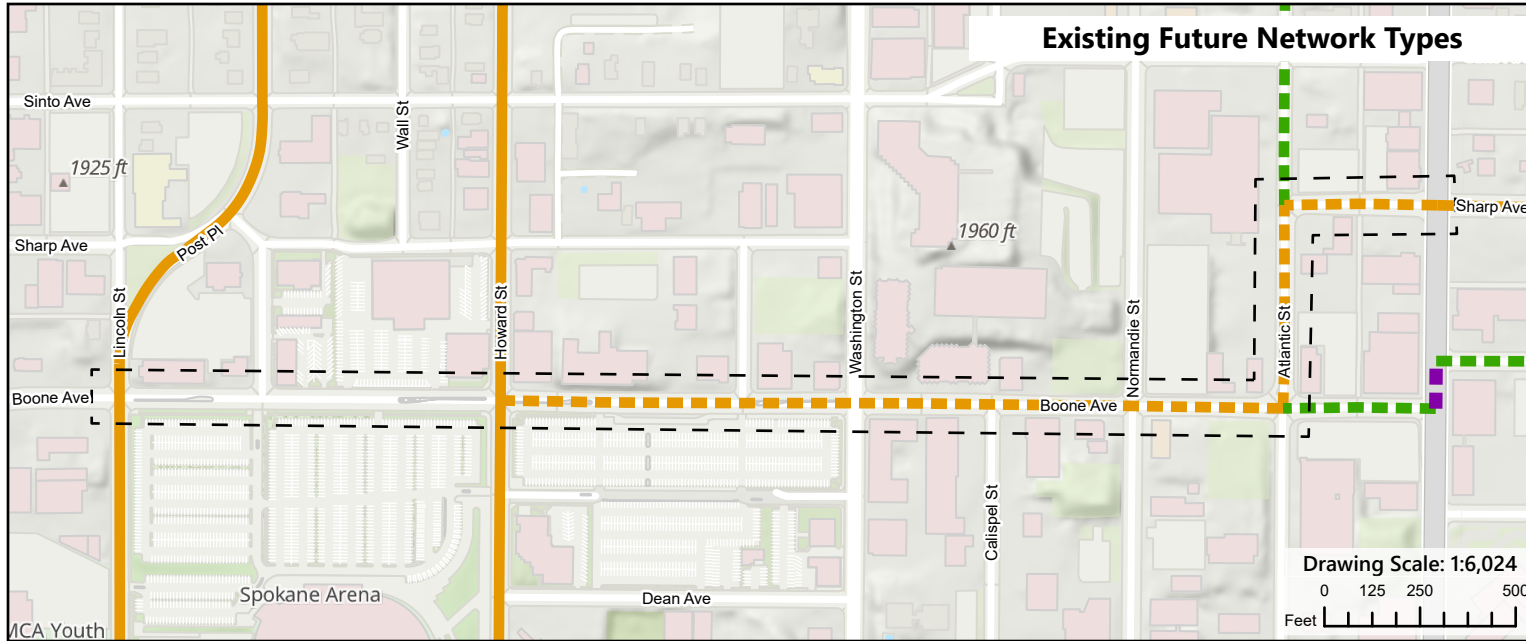




# Z20-019COMP: Map TR-5, Proposed Modification 10 (Boone Ave, Atlantic St, & Sharp Ave between Lincoln St and Division St) 2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 2/11/2020  
THIS IS NOT A LEGAL DOCUMENT

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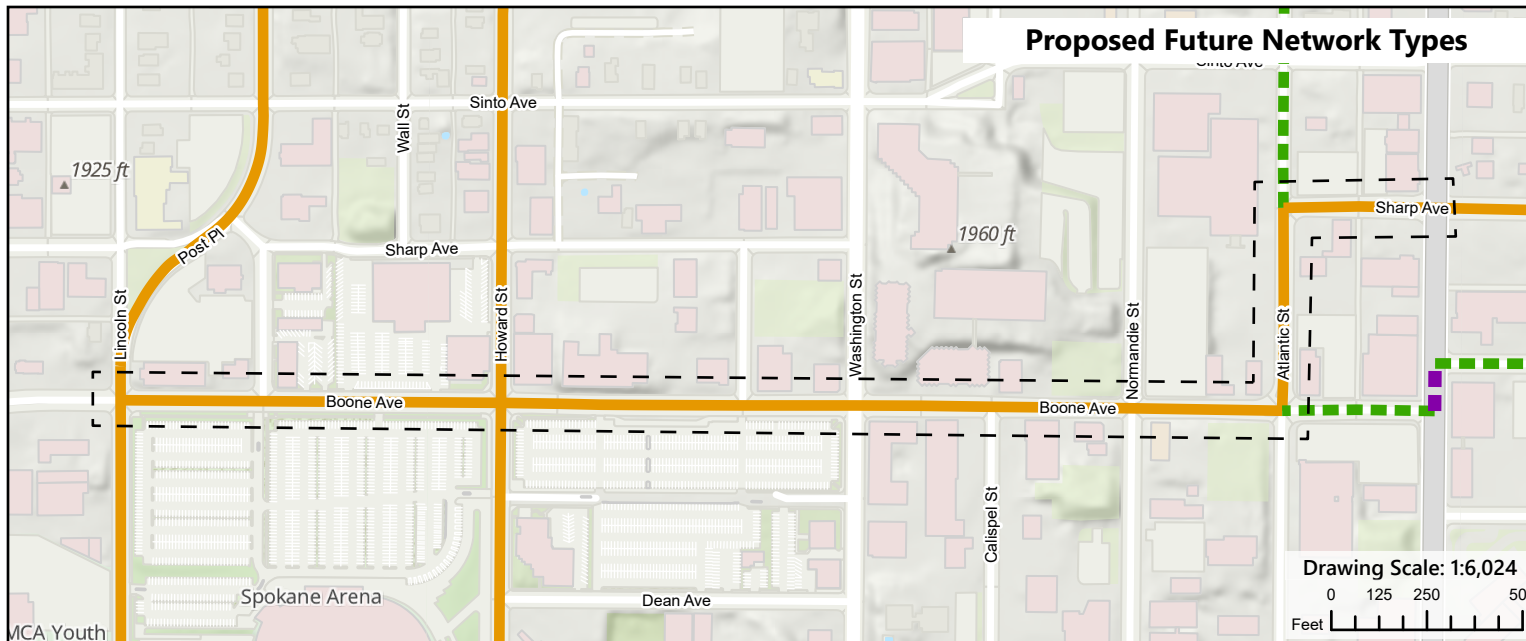


## Legend

[ ] Area of Proposed Change

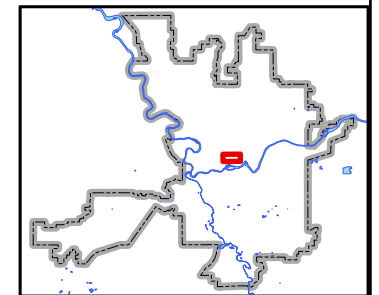
### Current Bikeway Network

- Bike Friendly Route
- Closed to Bike
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path



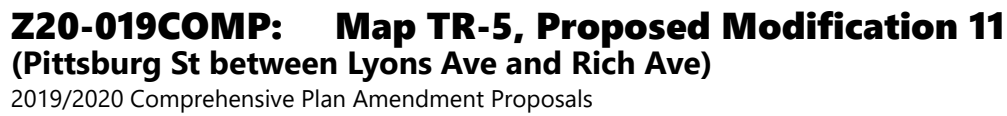
Length of Change: 0.75 Miles

## PROJECT LOCATION

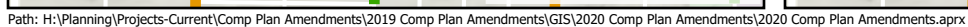
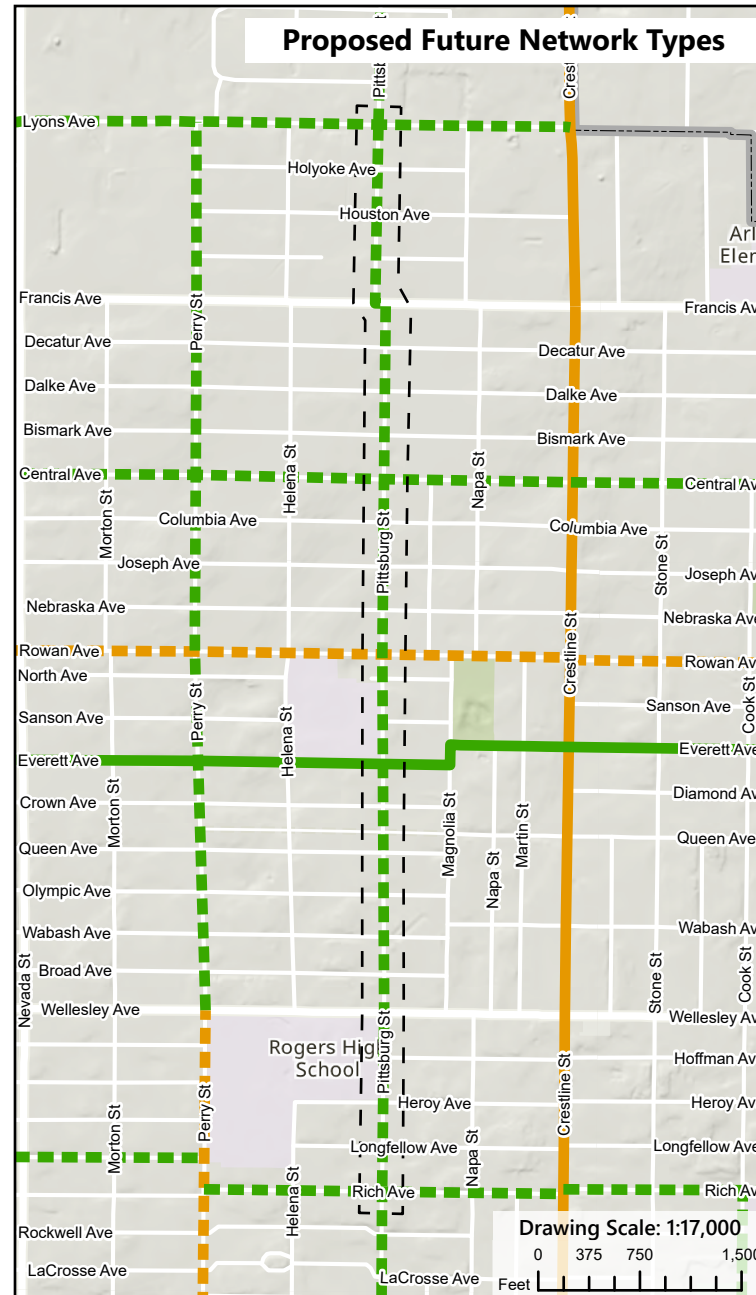


Neighborhood and Planning Services  
Drawn By: Kevin Freibott





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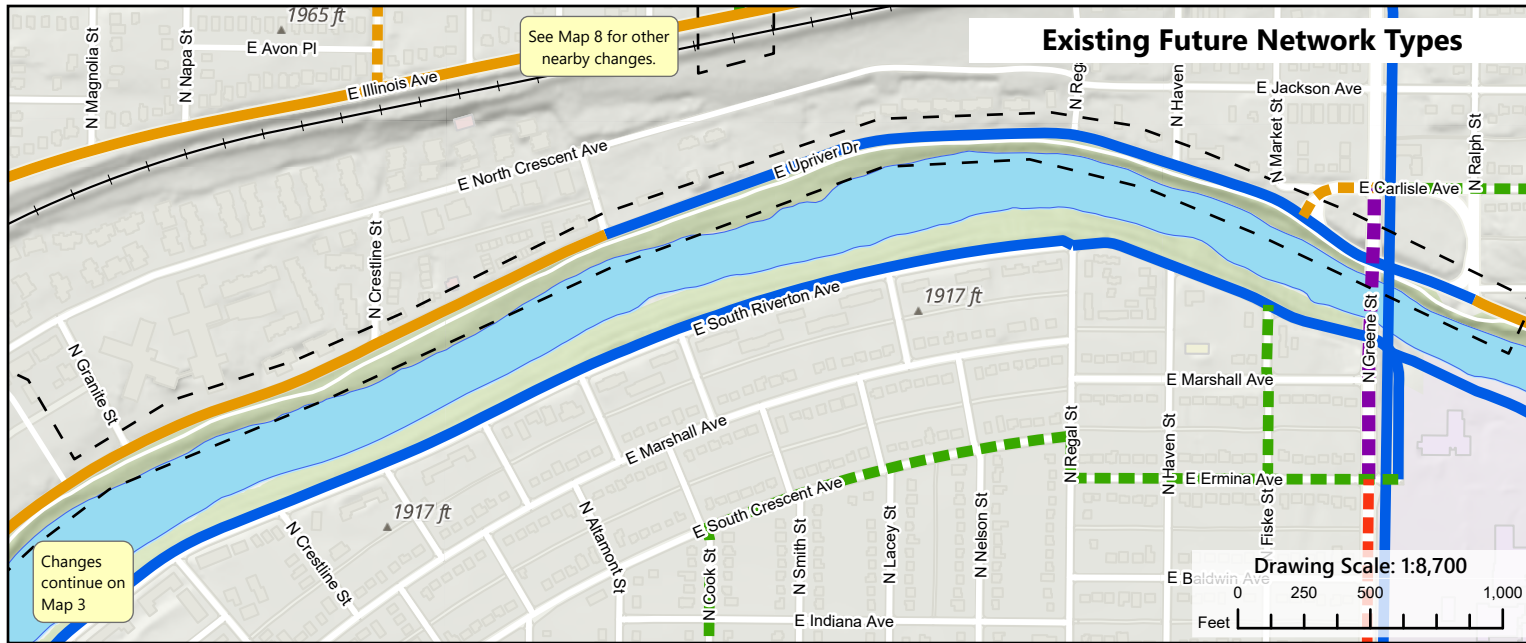
# Z20-019COMP: Map TR-5, Proposed Modification 12 (Upriver Drive and Centennial Trail)

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/17/2020

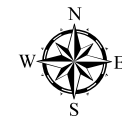
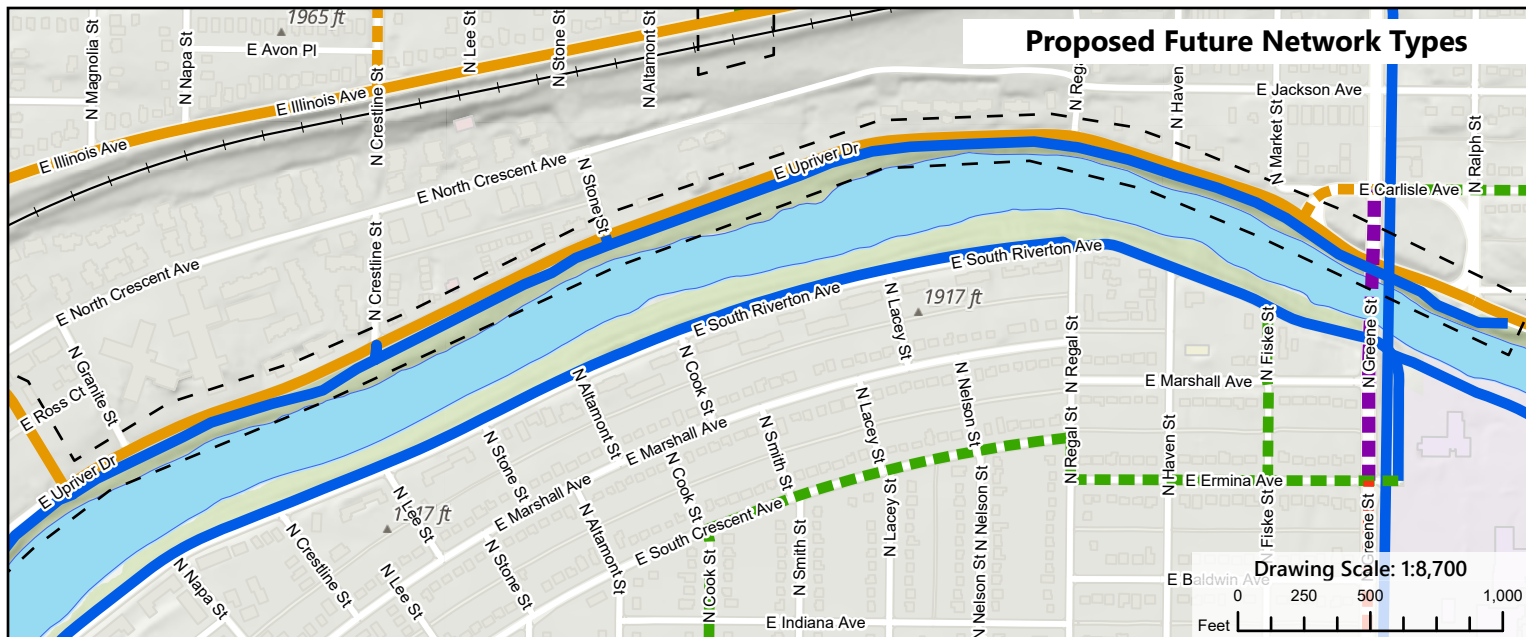
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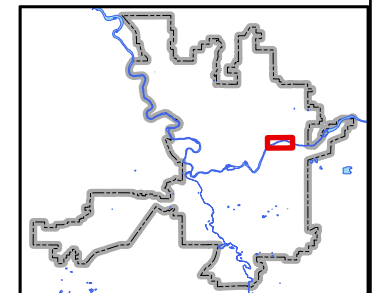
## Legend

- Area of Proposed Change
- Railroad
- Waterbody
- Current Bikeway Network**
  - Bike Friendly Route
  - Closed to Bike
  - Difficult Connection
  - High Traffic (Bike Lane)
  - High Traffic (Shared)
  - Moderate Traffic (Bike Lane)
  - Moderate Traffic (Shared)
  - Neighborhood Greenway
  - Shared Use Path
  - Soft Surface Path



Length of Change: 1.06 Miles

## PROJECT LOCATION



Neighborhood and Planning Services  
Drawn By: Kevin Freibott





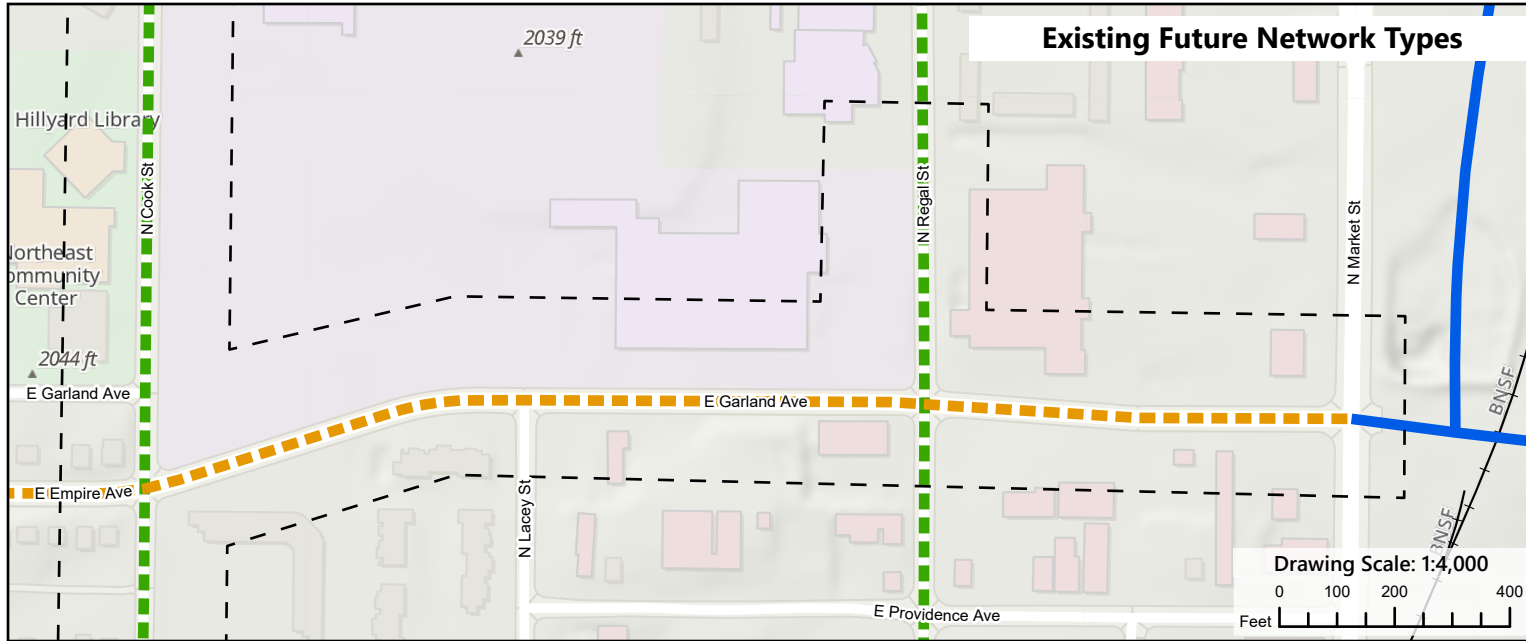
# **Z20-019COMP: Map TR-5, Proposed Modification 13** **(Garland Ave between Cook St and Market St)**

2019/2020 Comprehensive Plan Amendment Proposals

Drawn: 4/29/2020

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## **Legend**

[ ] Area of Proposed Change

—+— Railroad

### **Current Bikeway Network**

— Bike Friendly Route

— Closed to Bike

— Difficult Connection

— High Traffic (Bike Lane)

— High Traffic (Shared)

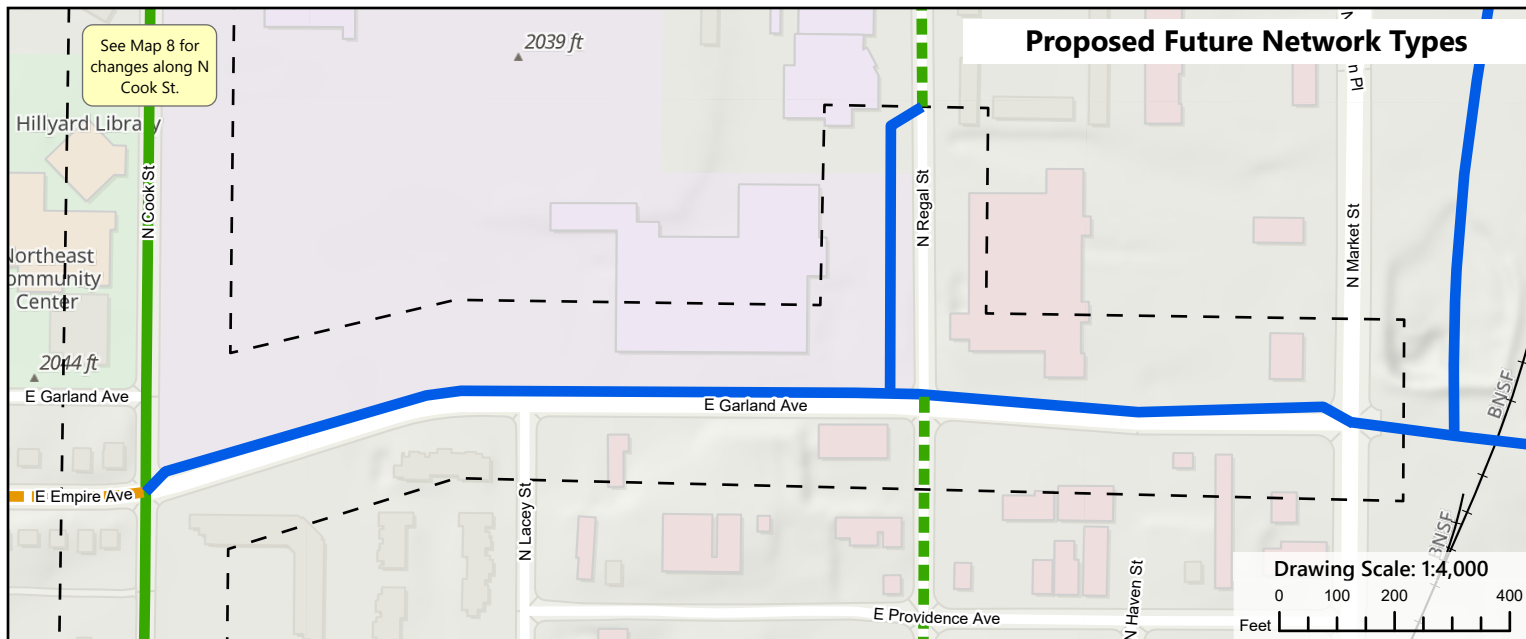
— Moderate Traffic (Bike Lane)

— Moderate Traffic (Shared)

— Neighborhood Greenway

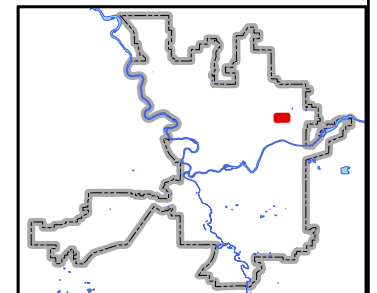
— Shared Use Path

— Soft Surface Path



Length of Change: 0.51 Miles

## **PROJECT LOCATION**



Neighborhood and Planning Services  
Drawn By: Kevin Freibott



## Exhibit B: Proposed Text Amendments to Appendix D of the Comprehensive Plan

### Cycle-Tracks Protected Bike Lanes

A cycle-track protected bike lane is an exclusive bike facility that combines the user experience of a separated path with the on-street infrastructure of a conventional bike lane. A cycle-track protected bike lane is physically separated from motor traffic and distinct from the sidewalk. Cycle-tracks Protected bike lanes have different forms but all share common elements—they provide space that is intended to be exclusively or primarily used for bicycles, and are separated from motor vehicle travel lanes, parking lanes, and sidewalks. In situations where on-street parking is allowed, cycle-tracks protected bike lanes are located to the curb-side of the parking (in contrast to bike lanes).

Cycle-tracks Protected bike lanes may be one-way or two-way, and may be at street level, at sidewalk level, or at an intermediate level. If at sidewalk level, a curb or median separates them from motor traffic, while different pavement color/texture separates the cycle-track-protected bike lanes from the sidewalk. If at street level, they can be separated from motor traffic by raised medians, on-street parking, or bollards. These design features do raise different considerations – such as driveway conflicts, driver expectations, and maintenance issues that need to be addressed. By separating cyclists from motor traffic, cycle-tracks protected bike lanes can offer a higher level of security than bike lanes and are attractive to a wider spectrum of the public. Routes classified as future bike lanes in this plan may be considered for protected bike lane designs following additional assessment and review. Further network-level planning will be required to identify a system of routes best suited to these designs.

Figure 5. Examples of potential cycle-track protected bike lane designs





## Protected Intersections:

A protected intersection is an at-grade road junction in which cyclists and pedestrians are separated from cars. Vehicles turning right (in countries driving on the right, or left in countries driving on the left) are separated by a car length from crossing cyclists and pedestrians, providing increased reaction times and visibility. Drivers looking to turn right have better visibility to cyclists and pedestrians as they can look to the side for conflicts instead of over their shoulders.



## BIKEWAY NETWORK MAPS

Spokane's bicycle facilities network includes protected bicycle lanes, bike lanes, shared-use paths, neighborhood greenways, shared roadways, and bike-friendly routes. The development of bicycle facilities is expected to take place over the course of the next 20 years. A number of unforeseen circumstances may affect the way that Spokane's bike network will develop. The Bicycle Facility Network Development Maps are not intended to define a specific time frame for the development of bike facilities within the city. These maps represent how the network may develop over time recognizing that the network cannot be created immediately. If an opportunity to develop any of the facilities on the map arises, that opportunity should be pursued. [The bikeway network is shown in Map TR-5 in Comprehensive Plan Chapter 4: Transportation.](#)

### Existing Bikeway Network Map

~~Map BMP 1 shows all of the existing bicycle facilities in Spokane at the time of the adoption of the Bike Master Plan.~~

### Future Bikeway Network Map

~~Map BMP 2 (Map TR 5) shows all the proposed bicycle facilities for the City.~~



# Proposed Bike Network Map

Map TR 5

## Legend

### Proposed Bike Network

- Closed to Bikes
- Difficult Connection
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Bike Friendly Route
- Neighborhood Greenway
- Shared Use Path
- Soft Surface Path

### Base Map Layers

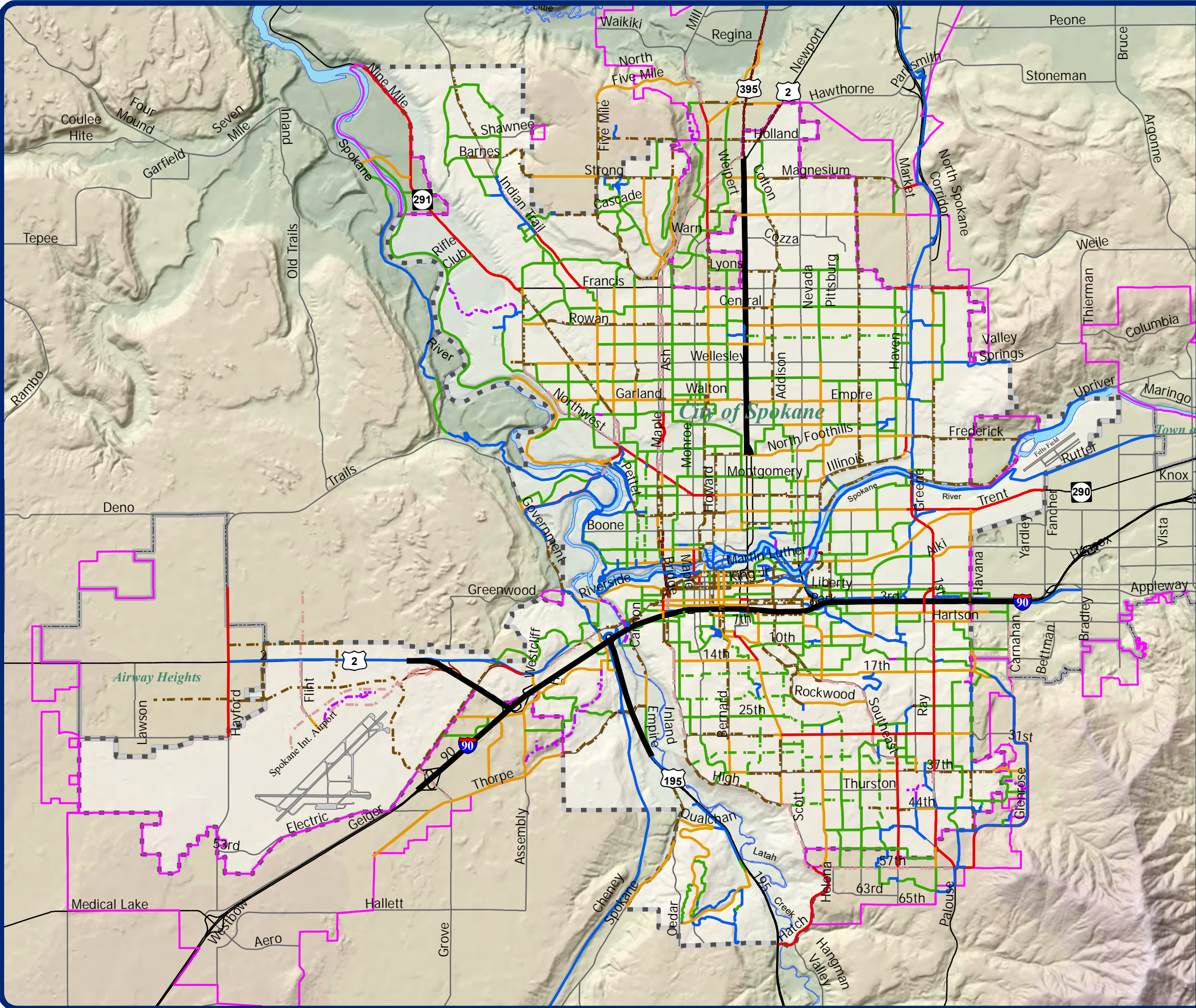
- County Adopted Urban Growth Area
- Municipal Boundary
- County Boundary
- Rivers
- State Routes
- Arterials
- Future North/South Corridor

1 0.5 0 1 2 Miles

Source: GIS  
Date: 07/2017



**THIS IS NOT A LEGAL DOCUMENT:**  
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The following policies of the Comprehensive Plan relate to application Z20-019COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 4—Transportation**

### **TR Goal B: Provide Transportation Choices**

Meet mobility needs by providing facilities for transportation options – including walking, bicycling, public transportation, private vehicles, and other choices.

**INTENT** The objective is to support the desires of the community to have transportation options by providing options for commuting, recreation and short trips using transit and active modes like walking and biking, as well as other choices such as rideshare, carpooling, taxi/for hire services, and private vehicles. Traditional transportation activities focus on the design and construction of facilities—yet travel behavior and mode choice are determined by a broader set of factors. The city shall continue to create new, and improve the existing multi-modal system, in order to accommodate the safe and efficient movement of all people. Effective transportation system management measures should be utilized to support safe and efficient travel for all users.

### **TR Goal C: Accommodate Access to Daily Needs and Priority Destinations**

Promote land use patterns and construct transportation facilities and other urban features that advance Spokane’s quality of life.

**INTENT** Land use type, mix, intensity, and distribution - as a result of on-going development of the city - greatly influences travel choices and decisions on connectivity, placement and investments of transportation facilities. Harmonize the key relationship between the places where people live, work, learn, access essential services, play, and shop and their need to have access to these places. Transportation investments should help drive economic development, energize activity centers, provide greater food security for residents, and produce quality places/neighborhoods/communities that retain value through time. Creating prosperous and walkable neighborhoods that offer opportunities for people to meet and connect means thinking of streets as people places as much as vehicle spaces. Spokane recognizes that transportation needs and travel choices may change over time as new alternatives become available. Other modes become viable when land uses are planned in a way that connects to multiple travel options and the distance between daily needs are closer. Coordinating appropriate transportation options and land uses is important. Transportation facilities should be maintained and improved in a manner that equitably serves Spokane.

### **TR Goal F: Enhance Public Health & Safety**

Promote healthy communities by providing and maintaining a safe transportation system with viable active mode options that provides for the needs of all travelers, particularly the most vulnerable users.

**INTENT** Promote healthy communities in Spokane by implementing a transportation system that provides for the ability to reduce auto mode share, increases the number of active travelers and



transit riders of all ages and abilities, and improves safety in all neighborhoods. Work with the Spokane Regional Health District and other agencies to promote active lifestyles through educational and encouragement programs and safe and accessible routes for active travelers of all ages and abilities in all neighborhoods. Consider the needs of all roadway users when applying traffic calming measures. Implementing safety efforts should be done in a comprehensive manner to safeguard against shifting traffic problems from one neighborhood to another. Spokane will seek to improve safety through the use of supporting federal and state programs, documents, and policies such as: FHWA Towards Zero Deaths (TZD), the FHWA Highway Safety Improvement Program (HSIP), and Washington State Department of Transportation's (WSDOT) Target Zero: Strategic Highway Safety Plan. Spokane recognizes the importance of evaluating transportation projects using objective criteria to reflect community standards. An environmental justice approach strives to avoid decisions that can have a disproportionate adverse effect on the environmental and human health of traditionally underserved neighborhoods and vulnerable populations compared to the population as a whole.

#### **TR 1 – Transportation Network For All Users**

Design the transportation system to provide a complete transportation network for all users, maximizing innovation, access, choice, and options throughout the four seasons. Users include pedestrians, bicyclists, transit riders, and persons of all abilities, as well as freight, emergency vehicles, and motor vehicle drivers. Guidelines identified in the Complete Streets Ordinance and other adopted plans and ordinances direct that roads and pathways will be designed, operated, and maintained to accommodate and promote safe and convenient travel for all users while acknowledging that not all streets must provide the same type of travel experience. All streets must meet mandated accessibility standards. The network for each mode is outlined in the Master Bike Plan, Pedestrian Master Plan, Spokane Transit's Comprehensive Plan, and the Arterial Street map.

#### **Key Actions**

- a. Make transportation decisions based upon the adopted policies, plans, design standards and guidelines, taking into consideration seasonal needs of users, system wide integration, and impacts on the relevant transportation planning decisions of neighboring jurisdictions.
- b. Utilize relevant performance measures and adopted level of service standards to track the city's progress in developing the transportation network for all users.
- c. Recognize and accommodate the special transportation needs of the elderly, children, and persons with disabilities in all aspects of, transportation planning, programming, and implementation.
  - i. Address the community's desire for a high level of accommodation for persons with disabilities by using the applicable and context sensitive local, state, or federal design standards in all projects within the city's right-of-way. City of Spokane Comprehensive Plan 4-20
  - ii. Implement the city's ADA Transition Plan, Pedestrian Plan and Bicycle Plan with a new focus on broader user groups



## TR 5 – Active Transportation

Identify high-priority active transportation projects to carry on completion/ upgrades to the active transportation network.

### Key Actions

- a. Ensure that the pedestrian and bicycle networks provide direct connections between major activity centers and transit stops and stations.
- b. The planning, design and construction of transportation projects should maintain or improve the accessibility and quality of existing and planned pedestrian and bicycle facilities.
- c. Implement a network of low vehicle volume, bike-friendly routes throughout the city.
- d. Support the development of a bike-share program within the city core.
- e. Seek grant funding for projects and programs such as Safe Routes to School, Transportation Alternatives, and other active transportation initiatives.
- f. Utilize the Bicycle Plan and the Pedestrian Plan to guide the location and type of bicycle and pedestrian facilities developed in Spokane to:
  - i. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages to transit stops and stations.
  - ii. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages between major activity areas where features that act as barriers prevent safe and convenient access.
  - iii. Provide safe, attractive, convenient and quality pedestrian and bicycle facilities and an aesthetically pleasing environment on bridges.
  - iv. Enhance the pedestrian and bicycle environment along routes to schools to provide a safe walking and riding environment for children. Means of accomplishing this include:
    - encouraging school routes not to cross arterials;
    - having user-activated signals at arterial intersections;
    - implementing safety patrols with traffic-control signs at busy intersections;
    - working with schools to promote walking groups; and
    - strengthening and enforcing pedestrian right-of-way laws.
  - v. Enhance the pedestrian, bicycle and transit environment along routes to desirable destinations for seniors.
  - vi. Enhance the pedestrian, bicycle and transit environment along routes in communities with a high percentage of underserved populations.
  - vii. Provide safe bicycle and pedestrian access to city parks from surrounding neighborhoods.
- g. Provide viable facilities for active transportation modes as alternatives to driving.
  - i. Ensure gaps in the bicycle network are identified and prioritized to complete and expand the connected bicycle network.



- ii. Ensure sidewalk gaps are not present and provide for safe pedestrian circulation within the city. Wherever possible, this should be in the form of sidewalks with a pedestrian buffer strip or other separation from the street.
- iii. Use pedestrian safety strategies on high bicycle and pedestrian traffic corridors.
- iv. Establish and maintain crosswalks at key locations where active transportation facilities cross collector and arterial roadways.
- h. Provide secure parking for bicyclists at key destinations (i.e. Downtown, identified Centers and Corridors, schools and universities, community centers, key transit locations) and ensure future developments include bicycle parking on site that adheres to city-established design and siting standards.
- i. Work with local and regional partners to implement the “Spokane County Wayfinding and Gateway Feature Placement & Design Plan”.
- j. Coordinate with other departments and partner agencies to combine related projects for the purpose of cost-sharing.

#### **TR 6 – Commercial Center Access**

Improve multi-modal transportation options to and within designated district centers, neighborhood centers, employment centers, corridors, and downtown as the regional center.

#### **Key Actions**

- a. Maintain Street Design Standards and Guidelines to support pedestrian activity and pedestrian-supportive amenities such as shade trees, multimodal design, street furniture, and other similar amenities.
- b. Maintain street design guidelines reflecting best practices to implement designs that effectively manage traffic flow within designated Centers and Corridors while ensuring designs correspond to and support local context.
- c. Designate and develop neighborhood greenways and low vehicle volume bicycle routes that parallel major arterials through designated Centers and Corridors.
- d. Establish and maintain bicycle parking guidelines and standards for Centers and Corridors to provide sufficient and appropriate short- and long-term bicycle parking.
- e. Provide transit supportive features (e.g. sidewalks, curb ramps, transit benches, etc.) in support with STA

#### **TR 9 – Promote Economic Opportunity**

Focus on providing efficient and affordable multi-modal access to jobs, education, and workforce training to promote economic opportunity in the city’s designated growth areas, develop “Great Streets” that enhance commerce and attract jobs.

#### **Key Actions**

- a. Ensure street designs support business activity-and thus jobs creation-to ensure that travelers feel comfortable to stop and shop.



- b. Coordinate closely with STA and area colleges and universities to provide convenient, cost-efficient transit service for students.
- c. Use new technology when feasible to increase efficiency in all transportation modes, such as:
  - i. Intelligent feedback to users;
  - ii. Dynamic traffic signals;
  - iii. Priority transit routes and signaling; and,
  - iv. Information sharing about capacity.
- d. Coordinate closely with STA to identify opportunities for service improvements in designated land use areas.
- e. Coordinate with Visit Spokane and other relevant groups to support and promote bicycle tourism in the city and region.
- f. Partner with business entities and organizations to educate them and their members on the economic benefits of transit and active transportation oriented development.
- g. Implement the city's bicycle master plan for improved city-wide mobility.

#### TR 20 – Bicycle/Pedestrian Coordination

Coordinate bicycle and pedestrian planning to ensure that projects are developed to meet the safety and access needs of all users.

#### **Key Actions**

- a. Coordinate City of Spokane departments and other agencies to efficiently provide transportation alternatives and facilitate the accomplishment of the city's transportation priorities.
- b. Incorporate bicycle/pedestrian facilities as early as possible into development and roadway plans to reduce costs and take advantage of cooperative opportunities.
- c. Seek funding sources for active transportation projects.
- d. Maintain Street Design Standards and Guidelines to ensure that public and private developments meet a variety of transportation needs. Refer to national references (such as NACTO) for facilities design when updating the standards and guidelines.
- e. Develop transportation-related educational programs for both nonmotorized and motorized transportation users.
- f. Consistently update and implement the pedestrian and bicycle master plans for active transportation users.





# General Application

Rev.20180104

## DESCRIPTION OF PROPOSAL

Map amendments to the Bicycle Master Plan Map TR-5 in order to show newly-built bikeways and to reflect minor adjustments to planned bikeways.

Address of Site Proposal (if not yet assigned, obtain address from Public Works before submitting application):

Multiple locations and street segments. Please see attached list.

## APPLICANT

Name: Colin Quinn-Hurst, Project Planner - Pedestrian and Bicycle

Address: Neighborhood and Planning Services, Rm. 610, 808 W. Spokane Falls Blvd.

Phone: (509) 625-6804 Email: cquinnhurst@spokanecity.org

## PROPERTY OWNER

Name: City of Spokane Public Right-of-Way

Address: 808 W. Spokane Falls Blvd.

Phone: (509) 625-6804 Email: cquinnhurst@spokanecity.org

## AGENT

Name: Not Applicable

Address:

Phone: Email:

Assessor's Parcel Numbers: Various Public Right-of-Ways

Legal Description of Site:



List Specific Permits Requested in this Application: Adjustments to Map BMP 2 (Map TR 5).

☒ Applicant      ☐ Property Owner      ☐ Property Purchaser      ☐ Agent

I, Not Applicable, owner of the above-described property, do hereby authorize Not Applicable to represent me and my interests in all matters regarding this application.

[illegible]

Witness my hand and official seal hereto affixed the day and year first above written.

Not Applicable

Notary Public in and for the State of Washington, residing at



## Comprehensive Plan Amendments: Bicycle Master Plan Map TR-5 Adjustments - 2020

|    | Street                                                         | From            | To                | Description                                                                 |
|----|----------------------------------------------------------------|-----------------|-------------------|-----------------------------------------------------------------------------|
|    | <b>Updates to Map BMP 1 (Map TR-5) - Existing Bike Network</b> |                 |                   |                                                                             |
| 1  | Walnut St.                                                     | 6th Ave.        | 10th Ave.         | Update from shared to bike lane designation                                 |
| 2  | Maple St.                                                      | 6th Ave.        | 8th Ave.          | Update from shared to bike lane designation                                 |
| 3  | Cowley St.                                                     | 4th Ave.        | 9th Ave.          | Update from shared to bike lane designation                                 |
| 4  | Strong Rd.                                                     | Five Mile Rd.   | Nettleton Ln.     | Update from bike lane to shared use path                                    |
| 5  | Indiana Rd.                                                    | Perry St.       |                   | Update to bike lane designation                                             |
|    |                                                                |                 |                   |                                                                             |
|    | <b>Updates to Map BMP 2 (Map TR-5) - Future Bike Network</b>   |                 |                   |                                                                             |
| 1  | 17th Ave.                                                      | Division St.    | Upper Terrace Dr. | Shift greenway designation from 17th to 18th Ave along this stretch         |
| 2  | 11th/Altamont/14th Bike-Friendly Route                         | Grand Blvd.     | Fiske St.         | Update bike-friendly route designation to neighborhood greenway designation |
| 3  | 1st Ave.                                                       | Bernard St.     | Riverside Ave.    | Consolidate to one street                                                   |
| 4  | Sprague Ave.                                                   | Bernard St.     | Riverside Ave.    |                                                                             |
| 5  | Altamont St.                                                   | 5th Ave.        | Main Ave.         | Bike Lane                                                                   |
| 6  | Flint                                                          | Airport Rd.     | Hwy 2             | Designate as bike lane                                                      |
| 7  | Longfellow Ave                                                 | Perry St.       | Pittsburg St.     | Shift bike route to alley connection                                        |
| 8  | Cook St.                                                       | Francis Ave.    | Central Ave.      | Designate as Neighborhood Greenway                                          |
| 9  | Palouse Hwy Path                                               | Palouse Highway | Benn Burr         | Shared Use Path connection                                                  |
| 10 | Boone Ave.                                                     | Monroe St.      | Sharp Ave.        | Bike lane designation                                                       |





# Comprehensive Plan or Land Use Code Amendment

Pre-Application

Rev.20180102

## DESCRIPTION OF THE PROPOSED AMENDMENT:

*(Please check the appropriate box(es))*

- |                                                                    |                                                      |
|--------------------------------------------------------------------|------------------------------------------------------|
| <input checked="" type="checkbox"/> Comprehensive Plan Text Change | <input type="checkbox"/> Land Use Designation Change |
| <input type="checkbox"/> Regulatory Code Text Change               | <input type="checkbox"/> Area-Wide Rezone            |

**Please respond to these questions on a separate piece of paper. Incomplete answers may jeopardize your application's chances of being reviewed during this amendment cycle.**

### 1. General Questions *(for all proposals)*:

- a. Summarize the general nature of the proposed amendment.
- b. Why do you feel this change is needed?
- c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?
- d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?
- e. For map amendments:
  1. What is the current Land Use designation and zoning for each affected parcel?
  2. What is the requested Land Use designation and zoning for each affected parcel?
  3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc.
- f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?
- g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?
- h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?
 

|                              |                             |
|------------------------------|-----------------------------|
| <input type="checkbox"/> Yes | <input type="checkbox"/> No |
|------------------------------|-----------------------------|
- i. If yes, please answer the following questions:
  1. When was the amendment proposal submitted?
  2. Was it submitted as a consistent amendment or an inconsistent amendment?
  3. What were the Plan Commission recommendation and City Council decision at that time?
  4. Describe any ways that this amendment proposal varies from the previously considered version.



## Comprehensive Plan Amendment – Pre-Application Bicycle Master Plan Map Adjustments – 2020

### 1. General Questions:

- a. Summarize the general nature of the proposed amendment.

This proposed amendment would adjust the Bicycle Master Plan, specifically Map BMP 1 – Existing Bikeway Network Map and Map BMP 2 – Future Bikeway Network Map, also modifying Map TR-5 of the Comprehensive Plan, to acknowledge recently-constructed bikeways and minor adjustments to the routing and designations of planned bikeways.

- b. Why do you feel this change is needed?

This change is needed to maintain the accuracy of Map BMP 1 – Existing Bikeway Network and Map TR-5 of the Comprehensive Plan to accurately represent the current bikeway network, including recently-constructed bikeways. This change is also needed to maintain the accuracy of Map BMP 2 – Future Bikeway Network and Map TR-5 of the Comprehensive Plan to represent community plans, incorporating community and staff feedback recommending minor adjustments to the planned bikeway network in the context of changing development patterns, land uses, and travel patterns.

- c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?

This proposal is consistent with the fundamental concepts contained in the Comprehensive Plan, in that recently-built bikeways incorporated into Map BMP 1 – Existing Bikeway Network were planned and constructed based on the route alignments and types shown in Map BMP 2- Future Bikeway Network and Map TR-5 of the Comprehensive Plan. This proposal is also consistent with the concepts of the Comprehensive Plan in that proposed minor adjustments to Map BMP 2 – Future Bikeway Network and Map TR-5 of the Comprehensive Plan are based on the same fundamental goals of connectivity and safety, and seek to maintain the integrity of these goals by shifting route alignments and types to achieve these goals in the face of changing development patterns, land use patterns and travel patterns.

- d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?

This proposal does not change goals, policies or regulations, but does change other documents, specifically Map BMP 1 – Existing Bikeway Network, Map BMP 2 – Future Bikeway Network, and Map TR-5.

- e. For map amendments:

1. What is the current Land Use designation and zoning for each affected parcel? Not Applicable
2. What is the requested Land Use designation and zoning for each affected parcel? Not Applicable
3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc.  
Not Applicable



- f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?

Yes, other studies, plans and other documents support this proposal. These include:

- The Shadle Area Master Plan
- City Council Resolution No. 2019-0098 Requesting Designation of Boone Avenue as a Designated Bicycle Route
- The Spokane Downtown Plan Update – Underway
- The South University District Sub-Area Plan – Underway

- g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?

We are pursuing a Comprehensive Plan amendment because the Bicycle Master Plan maps and Map TR-5 function as the primary reference for bikeway status and plans in the City of Spokane. These maps continuously reconcile the recommendations from various neighborhoods and City staff into a single document, accounting for both ongoing City initiatives and construction projects as well as neighborhood feedback and recommendations.

- h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?

No, these specific changes have not been considered in a prior Comprehensive Plan amendment proposal. While City staff regularly undertake this type of comprehensive plan amendment in order to maintain the accuracy of the Bicycle Master Plan and make minor adjustments, these specific adjustments represent newly-proposed modifications.

- i. If yes, please answer the following questions: Not Applicable

1. When was the amendment proposal submitted?
2. Was it submitted as a consistent amendment or an inconsistent amendment?
3. What were the Plan Commission recommendation and City Council decision at that time?
4. Describe any ways that this amendment proposal varies from the previously considered version.



## Comprehensive Plan Amendments – Threshold Review: Bicycle Master Plan Map Adjustments – 2020

### ***Description of the Proposed Amendment:***

This proposed amendment would adjust the Bicycle Master Plan, specifically Map BMP 1 – Existing Bikeway Network Map and Map BMP 2 – Future Bikeway Network Map, to acknowledge recently-constructed bikeways and minor adjustments to the routing and designations of planned bikeways.

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.

This proposed amendment would materially alter Map BMP 1 and Map BMP 2 as they appear in the Appendix D: Transportation of the Approved 2017 Comprehensive Plan.

2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.

Yes

3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.

Yes

4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?

Not Applicable

5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.

This proposed amendment is consistent with the Policies of the adopted Bicycle Master Plan as incorporated into Appendix D of the approved 2017 Comprehensive Plan. These policies include: 1) Continually increase the bicycle mode share for all trips, 2) Complete and maintain bikeways that provide safe transportation for Spokane cyclists throughout the City.

6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.



This proposed amendment is not the same or substantially similar to a proposal that was considered in the previous year's threshold review process.

7. If this change is directed by state law or a decision of a court or administrative agency, please describe.

Not Applicable

8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.

Documentation will be provided on a project-by-project basis through agendas and minutes from the relevant Neighborhood Councils, the Bicycle Advisory Board, and the Community Assembly's Pedestrian Transportation and Traffic sub-committee.



**State Environmental Policy Act (SEPA)  
ENVIRONMENTAL CHECKLIST**

**File No. Z20-019COMP**

**PLEASE READ CAREFULLY BEFORE COMPLETING THE CHECKLIST!**

**Purpose of Checklist:**

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

**Instructions for Applicants:**

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

**Use of checklist for nonproject proposals:**

Complete this checklist for nonproject proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**A. BACKGROUND**

1. Name of proposed project: City of Spokane Comprehensive Plan Map Amendments to Bicycle Master Plan Map TR-5
2. Applicant: City of Spokane
3. Address: 808 W. Spokane Falls Blvd.  
City/State/Zip: Spokane, WA 99201 Phone: 509-625-6804  
Agent or Primary Contact: Colin Quinn-Hurst  
Address: 808 W. Spokane Falls Blvd.  
City/State/Zip: Spokane, WA 99201 Phone: 509-625-6804  
Location of Project: Various Locations Citywide  
Address: \_\_\_\_\_  
Section: \_\_\_\_\_ Quarter: \_\_\_\_\_ Township: \_\_\_\_\_ Range: \_\_\_\_\_  
Tax Parcel Number(s) \_\_\_\_\_
4. Date checklist prepared: 3/26/2020
5. Agency requesting checklist: City of Spokane, Washington
6. Proposed timing or schedule (including phasing, if applicable): A Plan Commission hearing on this proposal will be requested to be held in the third quarter of 2020. Then the Plan Commission will make a recommendation to the City Council. Then the amendments must be approved by City Council and signed by the Mayor if they are to be adopted. The projects called for by the Bicycle Master Plan may be implemented over the course of the next 20 years.
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. Yes, minor updates are anticipated on an annual basis as City projects and private developments alter land use and transportation patters. A broader, comprehensive review of the Bicycle Master Plan is anticipated as part of the City of Spokane Comprehensive Plan update, due to be completed by 2025.  
b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. \_\_\_\_\_  
Most of the facilities involved in this proposal are within City rights-of-way or are on or adjacent to land owned by the City of Spokane
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. None that is directly related to this proposal. The Six-Year



Comprehensive Program for Streets have associated SEPA Checklists adopted with the program on an annual basis. They are available upon request. At the time of this checklist no technical reports are required or expected as a result of this proposal.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. None.
10. List any government approvals or permits that will be needed for your proposal, if known. The proposed amendments to the City of Spokane Comprehensive Plan require approval of the Spokane City Council and Mayor. For any new construction projects involving proposals within the B, proper permits will need to be obtained.
11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. This proposed amendment would amend the Bicycle Master Plan in Map TR-5 of the Comprehensive Plan, to acknowledge recently-constructed bikeways and minor adjustments to the routing and designations of planned bikeways. Individual facilities will be added with future construction projects where a particular roadway is widened or reconstructed, street signs or on-street markings are added, or new off-street paths are constructed, depending on the type of facility designated on the map.
12. Location of the proposal: Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist. Affected facilities are located in the City of Spokane and within its Urban Growth Area
13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.) Yes, all of the above.



14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

(1) Describe any systems, other than those designed for the disposal of sanitary waste installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities). Not applicable, this is a non-project action. Appropriate disposal of stormwater will be addressed for new projects at the time of construction.

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(2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored?

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Not applicable, this is a non-project action.

(3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems. Not applicable, this is a non-project action. Bicycle lanes and other facilities will be analyzed for their consistence with the City of Spokane Critical Aquifer Recharge Area Aquifer Protection Code, Chapter 17E.010 SMC, as well as other local, state and federal regulations at the time of development, per Spokane Municipal Code requirements.

(4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater? Not applicable, this is a non-project action. Storage, handling and use will be addressed when each project is designed and constructed.

b. Stormwater

(1) What are the depths on the site to groundwater and to bedrock (if known)? The depth to groundwater varies, depending on location within the Urban Growth Area.



(2) Will stormwater be discharged into the ground? If so, describe any potential impacts. Not applicable, this is a non-project action.

## **B. ENVIRONMENTAL ELEMENTS**

### **1. Earth**

a. General description of the site (check one):

☐ Flat   ☐ Rolling   ☐ Hilly   ☐ Steep slopes   ☐ Mountainous

Other: Varies.

b. What is the steepest slope on the site (approximate percent slope)?

c. Varies. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

Varies.

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. \_

Not applicable. This is a non-project action.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill:

Not applicable. This is a non-project action.

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

Not applicable. This is a non-project action.

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt, or buildings)?

Not applicable. This is a non-project action.



- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any:

Not applicable. This is a non-project action.

## **2. Air**

- a. What type of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

Not applicable. This is a non-project action.

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

Not applicable. This is a non-project action.

- c. Proposed measures to reduce or control emissions or other impacts to air, if any:

Not applicable. This is a non-project action.

## **3. Water**

- a. SURFACE WATER:

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

Not applicable. This is a non-project action.

- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

Not applicable. This is a non-project action.

- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Not applicable. This is a non-project action.



- (4) Will the proposal require surface water withdrawals or diversions? If yes, give general description, purpose, and approximate quantities if known.

Not applicable. This is a non-project action.

- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

Not applicable. This is a non-project action.

- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

Not applicable. This is a non-project action.

b. GROUNDWATER:

- (1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

Not applicable. This is a non-project action.

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

Not applicable. This is a non-project action.

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

Not applicable. This is a non-project action.



(2) Could waste materials enter ground or surface waters? If so, generally describe.

Not applicable. This is a non-project action.

(3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

Not applicable. This is a non-project action.

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any.

Not applicable. This is a non-project action.

#### 4. Plants

a. Check the type of vegetation found on the site: Not applicable. This is a non-project action.

Deciduous tree: ☐ alder ☐ maple ☐ aspen

Other: \_\_\_\_\_

Evergreen tree: ☐ fir ☐ cedar ☐ pine

Other: \_\_\_\_\_

☐ Shrubs ☐ Grass ☐ Pasture ☐ Crop or grain

☐ Orchards, vineyards or other permanent crops

Wet soil plants: ☐ cattail ☐ buttercup ☐ bullrush ☐ skunk cabbage

Other: \_\_\_\_\_

Water plants: ☐ water lily ☐ eelgrass ☐ milfoil

Other: \_\_\_\_\_

Other types of vegetation: \_\_\_\_\_

b. What kind and amount of vegetation will be removed or altered?

Not applicable. This is a non-project action.

c. List threatened and endangered species known to be on or near the site.

Not applicable. This is a non-project action.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:



Not applicable. This is a non-project action.

- e. List all noxious weeds and invasive species known to be on or near the site.

Not applicable. This is a non-project action.

## 5. Animals

- a. Check and List any birds and other animals which have been observed on or near the site or are known to be on or near the site: Not applicable. This is a non-project action.

Birds: ☐ hawk ☐ heron ☐ eagle ☐ songbirds

Other: \_\_\_\_\_

Mammals: ☐ deer ☐ bear ☐ elk ☐ beaver

Other: \_\_\_\_\_

Fish: ☐ bass ☐ salmon ☐ trout ☐ herring ☐ shellfish

Other: \_\_\_\_\_

Other (not listed in above categories): \_\_\_\_\_

- b. List any threatened or endangered animal species known to be on or near the site.

Not applicable. This is a non-project action.

- c. Is the site part of a migration route? If so, explain.

Not applicable. This is a non-project action.

- d. Proposed measures to preserve or enhance wildlife, if any:

Not applicable. This is a non-project action.

- e. List any invasive animal species known to be on or near the site.

Not applicable. This is a non-project action.

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.



Future bicycle infrastructure that includes lighting would require electrical energy in limited amounts. No other energy sources are expected to be required.

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

No. Bicycle facilities typically are at ground-level and do not include structures that could shade solar power generation.

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

Not applicable. This is a non-project action.

## **7. Environmental health**

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. \_

Not applicable. This is a non-project action.

- (1) Describe any known or possible contamination at the site from present or past uses.

Not applicable. This is a non-project action.

- (2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

Not applicable. This is a non-project action.

- (3) Describe any toxic or hazardous chemicals/conditions that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

Not applicable. This is a non-project action.

- (4) Describe special emergency services that might be required.

Not applicable. This is a non-project action.

- (5) Proposed measures to reduce or control environmental health hazards, if any:

Not applicable. This is a non-project action.

- b. NOISE:



- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

Most bicycle facilities are located on or near roadways, subject to typical street noise.

- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

Typical pedestrian and bicycle traffic noises, largely limited to conversation and similar noise.

- (3) Proposed measure to reduce or control noise impacts, if any:

Noise generated during construction or use of bicycle facilities would be restricted by Spokane Municipal Code requirements under SMC Section 10.08D.070 Maximum Permissible Environmental Sound Levels.

## **8. Land and shoreline use**

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

Bicycle facilities are to be located mostly on city rights-of-way that contain streets and sidewalks. Adjacent land uses are of all types, including residential, commercial, industrial and open space uses.

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

No.

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

Not applicable, this is a non-project action.

- c. Describe any structures on the site.

Sites designated for bicycle infrastructure by nature are free from structures.

- d. Will any structures be demolished? If so, which?



None are expected to be demolished (see "c" above).

- e. What is the current zoning classification of the site?

Zoning varies, based on the adjacent land use. See answer "a" above.

- f. What is the current comprehensive plan designation of the site?

Land Use designation varies.

- g. If applicable, what is the current shoreline master program designation of the site?

Some bicycle facilities designated on map TR-5 lie within shoreline designations. Future development of bicycle infrastructure in those locations is subject to SMC 17E.060.600 Transportation Facilities.

- h. Has any part of the site been classified as a critical area by the city or the county? If so, specify.

Not applicable, this is a non-project action.

- i. Approximately how many people would reside or work in the completed project?

None. Bicycle facilities do not typically employ persons.

- j. Approximately how many people would the completed project displace?

None, as no structures would be demolished and projects are usually restricted to City rights-of-way.

- k. Proposed measures to avoid or reduce displacement impacts, if any:

None.

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

None.

- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any:

None



## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

None.

- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing.

None.

- c. Proposed measures to reduce or control housing impacts, if any:

None.

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Typical bicycle facilities are located at ground level. Some signage or lighting could be installed above ground but would be limited in height, subject to the requirements of the SMC.

- b. What views in the immediate vicinity would be altered or obstructed?

None.

- c. Proposed measures to reduce or control aesthetic impacts, if any:

Urban Design staff and the City's Design Review Board would be consulted on any projects involving vertical elements, curblane changes or landscaping.

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

Lighting may be installed that provides for the light necessary to provide for safe use of the facilities. This lighting would operate from dusk to dawn in most cases.

- b. Could light or glare from the finished project be a safety hazard or interfere with views? No, subject to the requirements of the SMC.

- c. What existing off-site sources of light or glare may affect your proposal?



None.

- d. Proposed measures to reduce or control light and glare impacts, if any:

None.

## **12. Recreation**

- a. What designated and informal recreational opportunities are in the immediate vicinity?

Various parks and recreation facilities.

- b. Would the proposed project displace any existing recreational uses? If so, describe.

No. The proposed improvements would support recreational uses.

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

None.

## **13. Historic and cultural preservation**

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe.

None.

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

None known. Future construction is subject to SMC requirements for the discovery and protection of these resources.

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archaeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

None known. Future construction is subject to SMC requirements for the discovery and protection of these resources.



- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required  
None known. Future construction is subject to SMC requirements for the discovery and protection of these resources.

#### 14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

Various.

- b. Is site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop

Yes, the sites across the City are served by various stops and routes.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

None and none.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

Bicycle facilities called for in the proposal are typically located on streets and pedestrian/bicycle facilities. As such, the proposal calls directly for improvement to these resources.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe.

No.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and non-passenger vehicles). What data or transportation models were used to make these estimates?

None.

*(Note: to assist in review and if known, indicate vehicle trips during PM peak, AM Peak, and Weekday (24 hours).)*



- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, general describe.

Not applicable, this is a non-project action.

- h. Proposed measures to reduce or control transportation impacts, if any:

None.

## **15. Public services**

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

No, as the proposal generates no new residents or employees in the City.

- b. Proposed measures to reduce or control direct impacts on public services, if any:

None.



**16. Utilities**

a. Check utilities currently available at the site: Varies.

- ☐ electricity
- ☐ natural gas
- ☐ water
- ☐ refuse service
- ☐ telephone
- ☐ sanitary sewer
- ☐ septic system

Other: \_\_\_\_\_  
\_\_\_\_\_

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed:

Varies. In some cases, lighting may be installed that requires electrical energy



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the agency must withdraw any determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 3/26/2020

Signature: \_\_\_\_\_



***Please Print or Type:***

Proponent: City of Spokane

Address: 801 W. Spokane Falls Blvd.

Phone: 509-625-6804

Person completing form (if different from proponent): Colin Quinn-Hurst

Phone: 509-625-6804 Address: 808 W. Spokane Falls Blvd. Spokane, WA 99201

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



#### D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

*(Do not use this sheet for project actions)*

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise? The proposal would not directly increase any of these elements, save for the use of typical hazardous substances and generating typical noise related to construction. This would be commensurate with similar construction projects and would be temporary in nature and consistent with the Spokane Municipal Code requirements for such emissions/use. As part of the Master Bike Plan, the proposed routes are intended to reduce automobile traffic and encourage non-motorized transportation, thus having a beneficial effect on air emissions..

Proposed measures to avoid or reduce such increases are: Traffic impacts would be evaluated at the time specific improvements are designed and before projects are implemented to ensure that the addition of bicycle facilities would not lead to auto traffic congestion.

2. How would the proposal be likely to affect plants, animals, fish or marine life? Most of the proposed projects would likely not affect plants, animals, fish or marine life. For any project requiring a newly constructed path or wider roadway, an environmental review would take place to evaluate these impacts per SMC Section 17E.060.600 and SMC Section 17E.020.050.

Proposed measures to protect or conserve plants, animals, fish or marine life are: Environmental reviews of projects at the time of project design and permitting would ensure that each bike project would enact measures to protect and conserve plants, animals, fish and marine life that are affected.

3. How would the proposal be likely to deplete energy or natural resources? The proposal promotes bicycling as a utilitarian transportation option, likely reducing the overall use of motorized travel in the vicinity of these improvements and a commensurate reduction in fossil fuel use. In cases where lighting is installed as a component of implementing projects, minor amounts of electrical energy



would be required for operation. Proposed measures to protect or conserve energy and natural resources are: None required.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands? This proposed Comprehensive Plan amendments will not directly affect environmentally sensitive areas. Full implementation of the Bicycle Master Plan will promote access to some parks, wilderness, rivers, historic or cultural sites, etc. New construction will be subject to the Shoreline and critical area standards of the Spokane Municipal Code.

Proposed measures to protect such resources or to avoid or reduce impacts are: Specific measures as required would be implemented for implementing projects that could affect these resources, including the possible use of permeable surfaces, to be determined during the design and permitting stage of any proposed improvements. Path placement and road adjustments would be sensitive to the preservation of parks, rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans? Future implementation projects constructed under the proposed amendments are required to meet the development regulations adopted under the Comprehensive Plan and, where applicable, shoreline development standards.

Proposed measures to avoid or reduce shoreline and land use impacts are: No additional measures are proposed.

6. How would the proposal be likely to increase demands on transportation or public services and utilities? The proposal incrementally enhances a transportation system that supports non-motorized transportation options by adding or altering planned bikeways in about 12 locations. As such, the projects described by the proposal are expected to ultimately reduce the demand on existing transportation infrastructure and public services.

Proposed measures to reduce or respond to such demand(s) are: None.

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment. The proposal would not conflict with local, state or federal laws or requirements for the protection of the environment.

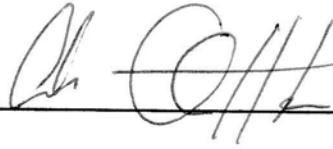


C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the agency may withdraw any Determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: March 26, 2020

Signature: \_\_\_\_\_



**Please Print or Type:**

Proponent: City of Spokane Address: 808 W. Spokane Falls  
Blvd.

Phone: 509-625-6804 Spokane, WA 99201-3329

Person completing form (if different from proponent): Colin Quinn-Hurst

Phone: 509-625-6804 Address: 808 W. Spokane Falls Blvd.

Spokane, WA 99201-  
3329

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- A. ☒ there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- B. ☐ probable significant adverse impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- C. ☐ there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**NONPROJECT DETERMINATION OF NONSIGNIFICANCE****FILE NO(S):** Z20-019COMP**PROPONENT:** City of Spokane

**DESCRIPTION OF PROPOSAL:** Amendment of Map TR-5, "Proposed Bike Network Map", of Chapter 4, Transportation, of the Comprehensive Plan to modify the proposed bike network in various locations throughout the City. Map TR-5 identifies the proposed future bike facilities expected to be constructed during the lifetime of the Comprehensive Plan. No actual construction is proposed at this time.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal is a city-wide map amendment and would affect the proposed bike facilities that may be installed in 13 locations throughout the City as well as a minor text amendment to the Bike Master Plan (an appendix to the Comprehensive Plan). The specific locations and changes proposed are available at the website identified below:

<http://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/>

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

☐ There is no comment period for this DNS.

☐ This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.

☒ This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201**Date Issued:** August 24, 2020**Signature:**

DocuSigned by:  
Louis Meuler

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*





**Spokane Tribe of Indians**  
**Tribal Historic Preservation Officer**  
P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z20-019COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer



**From:** [Eliason, Joelle](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Brown, Eldon](#); [Nilsson, Mike](#); [Kells, Patty](#); [Johnson, Erik D.](#); [West, Jacque](#)  
**Subject:** RFC Z20-019COMP TR-5 Map Amendment  
**Date:** Thursday, April 30, 2020 7:52:33 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Kevin,

Thank you for the opportunity to review and comment on the proposed bike network map amendment. Development Services has no objection to the proposed bike network map amendment. Further comments regarding the design, any potential utility conflicts, stormwater management, etc. will be handled outside of this Comp Plan Amendment and associated SEPA.

Sincerely,  
Joelle Eliason

*I will be working remotely until further notice and will respond to emails as quickly as possible. Thank you for your patience!*



Joelle Eliason | City of Spokane | Development Services Center  
509.625.6385 | fax 509.625.6013 | [jeliason@spokanecity.org](mailto:jeliason@spokanecity.org)





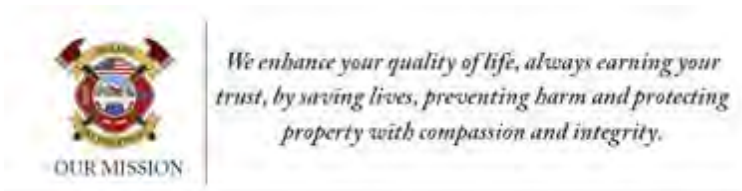
**From:** [Kokot, Dave](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: Addendum: Request for Comments for Proposed Bike Network Map Amendment Proposal  
**Date:** Tuesday, May 5, 2020 2:55:52 PM  
**Attachments:** [image001.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[image007.png](#)  
[image008.png](#)  
[image009.png](#)

---

Fire has no comments on this proposal.

**I am currently working remotely and will respond to emails as soon as possible. Your patience is appreciated.**

**David F. Kokot, P.E.** | Spokane Fire Department | Fire Protection Engineer  
509.625-7056 | fax 509.625.7006 | [dkokot@spokanefire.org](mailto:dkokot@spokanefire.org) | [spokanefire.org](http://spokanefire.org)





**From:** [Halbig, Bobby](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Eveland, Marcus](#); [Melvin, Val](#); [Okihara, Gerald](#)  
**Subject:** RE: REVISED Request For Comments - Proposed Bike Network Map Amendment  
**Date:** Tuesday, June 16, 2020 12:25:52 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Hello Kevin,  
The Street Department has reviewed the proposal and has no comments.  
Best regards,



**Bobby Halbig** | City of Spokane | Engineering Technician III, Traffic Operations  
509.232-8846 | fax 509.232.8830 | [bhalbig@spokanecity.org](mailto:bhalbig@spokanecity.org) | [spokanecity.org](http://spokanecity.org)





## Quinn-Hurst, Colin

---

**To:** Ball, John; Note, Inga  
**Cc:** Freibott, Kevin  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

Thanks John. Inga, we will show the path along Garland from Market to Cook, and the spur up Regal

---

**From:** Ball, John <jball@spokanecity.org>  
**Sent:** Monday, April 27, 2020 3:32 PM  
**To:** Note, Inga <inote@spokanecity.org>  
**Cc:** Freibott, Kevin <kfreibott@spokanecity.org>; Quinn-Hurst, Colin <cquinnhurst@spokanecity.org>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

Inga, Here was the most recent option. Let me know if you need any others.

They can be found here:

B:\8 - Reference-Study-Report Data\AutoCad Drawings\Internal Request\Inga\Garland - Shaw Middle



**John Ball** | City of Spokane | Public Works  
509.625-6344 | fax 509.625.6822 | [jball@spokanecity.org](mailto:jball@spokanecity.org) | [spokanecity.org](http://spokanecity.org)



---

**From:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 3:19 PM  
**To:** Ball, John <[jball@spokanecity.org](mailto:jball@spokanecity.org)>  
**Cc:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

John,  
Can you send a drawing of the Garland pathway project that we worked on? This is from Cook to the NSC trail at Market. I can't figure out where we saved them. Hopefully you remember. ☺  
Thanks,  
Inga

---

**From:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:59 PM  
**To:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>; Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

It can be approximate, I just need to know about where the route will be and what classification. Thanks!



Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services  
509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



*Please note that in compliance with the State of Washington guidelines on social distancing and the Stay Home, Stay Safe order, I will be working remotely for the foreseeable future. Messages left on my phone (see number above) will be forwarded to me, following which I will call you back. Thanks for understanding, and stay healthy!*

---

**From:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:57 PM  
**To:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

Thanks Kevin, that is great.

Inga would you mind sending the drawings with the general alignment, if available, knowing it may shift somewhat?

Thanks,

Colin

---

**From:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:54 PM  
**To:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Cc:** Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

We can add it now and I can send it to the agencies/departments as a special addition. Colin, can you give me an idea what the scope of this is and I'll start working on the map?



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services  
509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



*Please note that in compliance with the State of Washington guidelines on social distancing and the Stay Home, Stay Safe order, I will be working remotely for the foreseeable future. Messages left on my phone (see number above) will be forwarded to me, following which I will call you back. Thanks for understanding, and stay healthy!*

---

**From:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:52 PM



**To:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Cc:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

It just came up during PIEs.

---

**From:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:51 PM  
**To:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

You are right, we need to add it. Kevin, is this something we can add following the current comment period?

Thank you,

Colin

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**From:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Sent:** Monday, April 27, 2020 2:37 PM  
**To:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Cc:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Miller, Katherine E <[kemiller@spokanecity.org](mailto:kemiller@spokanecity.org)>  
**Subject:** RE: Request for Comments for Proposed Bike Network Map Amendment Proposal

Colin,  
I thought we had talked about adding the proposed Garland pathway between Cook and the Children of the Sun Trail to the bike plan. I don't see it in the maps on the webpage.  
Thanks  
Inga



**From:** [Karen Carlberg](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Bike lane for Upriver Drive  
**Date:** Tuesday, June 9, 2020 7:19:27 PM

---

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Mr. Freibott,

I have one comment on the proposed changes to the Bike Master Plan:

I strongly support the addition of a bike lane to Upriver Drive. This will make cyclists feel safer, and is particularly important because this section of Upriver Drive is part of the Centennial Trail.

Karen Carlberg



**From:** [Carol Tomsic](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Beggs, Breean](#); [Kinnear, Lori](#); [Wilkerson, Betsy](#); [Burke, Kate M.](#); [Cathcart, Michael](#); [Mumm, Candace](#); [Stratton, Karen](#); [Wittstruck, Melissa](#); [Sally Phillips](#); [DOUGLAS & MARILYN LLOYD](#)  
**Subject:** Comment on 2019/2020 Comp Plan Amendments for City Council Vote on March 2  
**Date:** Friday, February 28, 2020 12:25:00 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Lincoln Heights Proposed Change from "Residential 4-10" to "Office" - Against

I live, work and walk in the Lincoln Heights Neighborhood. I have lived two blocks from the NE corner of 29th/Ray for over 20 years. I prefer to keep the current residential zoning on the two parcels. Our city has a housing shortage and an existing house on the lot was demolished in 2019. The block is lined with single-family houses. There is a neighborhood park in walking distance.

The NE side of Ray is zoned as residential from 17th to 37th. The NE side of Ray Street has been historically designated as a residential buffer. In 1984 and 1993 the residents successfully fought against two attempts to rezone the said property to office.

The area is not trending in terms of land use. There is a church on the NE side of Ray/28th. It was built in 1959 and designed by three well-known architects; Bill Trogdon, Bruce Walker and Stan McGough. There is a church on the NE side of Ray/27th. It was built in 1953. There is a daycare on the NE side of Ray/25th that was built in 1988. An elementary school on the NE side of Ray/23rd opened in the fall of 1953. A fire station on the NE side of Ray has been in various locations on Ray since 1914. All are appropriate for a residential area.

Office zoning on small sites in or near residential areas is intended to have few detrimental impacts on the neighborhood. The proposed office zoning change will set a precedent and retail/office will creep into our residential buffer. An office use will also divert traffic into our residential neighborhood due to concrete barriers on 29th/Ray.

A 2019 29th Avenue Corridor Study online survey suggested residents "felt unsafe or uncomfortable" while walking or crossing 29th. Ray is a principal arterial. The posted speed on Ray is 30 mph. It is already unsafe to walk across four lanes of traffic on Ray, at 27th or 28th to get to work. Adding office on the NE side of Ray will only make it more dangerous.

Map TR-5 Proposed Bike Network Map Proposed Modification 5 - Support

I totally support the proposed map. I'd like to suggest adding Cook, to South Altamont Blvd, to Woodfern, to North Altamont Blvd, to the Ben Burr Trail. And, connecting the Ben Burr Trail to Thornton Murphy Park.

Thank you!

Carol Tomsic  
resident

Kevin - please send an email confirmation.



**From:** [pcmckann\\_](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Bike Network amendments  
**Date:** Wednesday, February 26, 2020 7:29:22 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Hi Kevin, our neighborhood council has made me aware that you are updating the bike network map. I saw some changes that included South Altamont Blvd. I think this is a good first step.

But, I think that the entire boulevard should be included, since people biking downhill from Lincoln Park and 17th will take Cook to South Altamont Blvd, to Woodfern, to North Altamont Blvd, to the Ben Burr trail. That is the shortest route to get on the Ben Burr from Lincoln Park/17th. I live on North Altamont Blvd, and see bike commuters every day in front of my house.

Also, we would like to see the Ben Burr trail connected to Thornton Murphy Park. This would involve extending the bike route up Fiske to where it dead ends into city land, and could follow city land all the way to Thornton Murphy.

Please consider these positive changes to our bike network.

Thank you,

Patrick McKann



**From:** [Eileen](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Changes to bikeway planning  
**Date:** Tuesday, June 9, 2020 3:22:21 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Kevin,

I am commenting on the use of West 18th Avenue for a neighborhood greenway and eliminating West 17th from the bikeway plan.

I was a resident of 17th Avenue For 30 years. I commuted by bike to the far north side and rode my bikes recreationally. I served on the Bicycle Advisory Board in the 1990s, and also served as chairperson. I authored the state Bicycle Traffic Skills Curriculum for middle schools in use today and trained teachers around the state as part of DOT and WSDOT funded grant programs.

While the West 18th Avenue would be a beautiful hilly greenway to Manito Park, just a block away on West 17th Avenue is a much more bike friendly flat route that has naturally evolved over the years to be a popular commuter and recreational route. If you rode it yourself you would see that this straight, flat route is much preferable to a bicyclist than the new hilly 18th Ave proposal with its many twists and turns. The 17th Avenue route would serve the children and staff at Cataldo School as their playground court area touches 17th Avenue. The crossing at 17th and Bernard is a flat area with good sight lines. At Grand Avenue, the new proposal would require adult commuters to use twisting sidewalks to continue their trip.

The existing crosswalk at 18th Avenue and Bernard is not enough to overcome the topography that creates a better route for bicyclists on West 17th Avenue. From 17th Avenue a bicyclist going west can turn toward Manito Park, or toward 14th Avenue at McClellan Street. During busy traffic hours I often use the traffic light at 14th Avenue to safely cross Grand Blvd.

Thank you for working to improve Spokane's bicycle network.

Eileen Hyatt  
509-475-9328



**From:** [Linda Carroll](#)  
**To:** [North Bank Bikeways DL](#)  
**Subject:** support for Boone-Sharp bike lanes  
**Date:** Tuesday, July 21, 2020 10:10:14 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

As a citizen of Spokane who travels extensively via bicycle (I do not own a car) and particularly in this neighborhood (I live near Corbin Park), I strongly support the conversion of two lanes of Boone-Sharp to bicycle lanes. This new configuration will make travel by bike safer not only on that thoroughfare but also on the north-south streets that cross it. The reason for the latter is that the Boone-Sharp thoroughfare is currently treated as a drag strip/raceway by many of the cars that use it, making it hazardous for bicyclists attempting to cross it. Converting two of its lanes to bicycle lanes will change that dynamic. It would be made even safer if the traffic light cycle always turned red for Boone. It currently does so only if there is a car on the north-south street (bicycles are too light to activate the sensor and the curb cuts are angled toward Boone, so a cyclist on the marked bike path on Howard has to get off their bike, hump it over the curb, hit the pedestrian cross button and then get the bike back in the street and get back on it and get going fast enough to make the light. Very dangerous.)

Thank you for this proposal.

Linda Carroll

Sent from my iPhone



**From:** [wyattschroeder@gmail.com](mailto:wyattschroeder@gmail.com)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Beggs, Breean](#)  
**Subject:** Master Bike Plan Update  
**Date:** Monday, July 20, 2020 1:27:20 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

I was excited to see the request for feedback on cycling in Spokane. I have only lived here three years, but ride quite a bit. I moved here from Vancouver, WA and they had a fairly good grid system with arterials both north/south and east/west, about 12 to 15 blocks. I'm learning, and we need a grid system. East/west so far I like Wellesley, Garland, and Trent (starting eastbound at Hamilton). North/south am leaning towards either Monroe or Post (parallel). Post has an easier grade to the Garland District. Alberta is another great bike arterial for North/South which needs bike lanes, and I look forward to the Cincinnati corridor. We also need to think of a parallel route to the North/south freeway (maybe we are). A good model is Trent between Freya and Havana, with protective bike lanes when possible.

Connecting neighborhoods is a must. Sprague Street needs a corridor parallel to connect people from east Central, to Perry Street, to downtown and Kendall Yards. Garland District needs connection to downtown and points North.

The bike lanes on Indiana must extend west, from Lidgerwood to Northwest Blvd. Northwest Blvd. needs protected bike lanes all the way north to the Indian Trails neighborhood.

Finally, we need clean bike lines, with a dedicated crew that follows bike routes. Currently, when they clean a street they don't get the bike lane (4 feet). Also, a bike lane debris button on the 311 line to easily report glass and other debris.

Wyatt Schroeder  
360.241.3365

Sent from my iPhone



**From:** [Home](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** master plan  
**Date:** Friday, July 17, 2020 2:00:46 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

I would encourage that class 1 and 2 ebikes are allowed anywhere that regular pedal bikes can go. Class 3 speed makes me want to discourage that class as I think 28 mph is too fast.

Thanks,

Kevin Flatt  
11517 S Elk Run  
Spokane WA



**From:** [Laurie Fleming](#)  
**To:** [North Bank Bikeways DL](#)  
**Subject:** Comment on the New east-west bike lane connection primarily on Boone Ave  
**Date:** Thursday, July 30, 2020 7:38:59 PM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Hi,

When I was commuting to work from the Downtown Bus Plaza, I would use Howard and then turn on Indiana to get to Hamilton Street. I would have to use the sidewalk west of Division. This is not a very friendly way for biking. Leaving work, I would use Indiana and then go south on Division riding on the sidewalk until I got towards Boone. I then would turn to get on Boone, which had less traffic to take me downtown. Since there are now bike paths from Indiana to the Gonzaga area, the proposed bike route would be a much better alternative than what is available right now.

Laurie Fleming  
2724 E 44<sup>th</sup> Ave  
Spokane, WA 99223



**From:** [Justin Haller](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: No more bike lanes!  
**Date:** Wednesday, July 29, 2020 10:12:54 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Well I'm certainly not concerned about zoning changes as those don't affect my life nearly as much as you guys wasting money on bike Lanes. Fix existing potholes! To be clear, yes I don't want any more bike Lanes! I ride my bike a lot in this city yet I don't want bike Lanes either.

[Sent from Yahoo Mail on Android](#)

On Wed, Jul 29, 2020 at 10:00 AM, Freibott, Kevin  
<[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)> wrote:

Thanks for your comments, Justin. Just to be certain, these are comments on the proposed amendments to the bike master plan, correct? Thanks!

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



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**From:** Justin Haller <[justinhallerphoto@yahoo.com](mailto:justinhallerphoto@yahoo.com)>  
**Sent:** Tuesday, July 28, 2020 8:05 PM  
**To:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>  
**Subject:** No more bike lanes!

[CAUTION - EXTERNAL EMAIL - Verify Sender]



I live in District 1 and I don't want any more bike Lanes let's fix all the potholes before you even entertain any bike Lanes! I ride my bicycle all the time even more than the so-called city council members that claim they ride their bikes. Why is it you never see the city council members riding their bikes and taking public transportation will clamoring for more bike Lanes fix the potholes first! Also stop building roundabouts are complete waste of money and big rigs can't get past them easily.

[Sent from Yahoo Mail on Android](#)



**From:** [MELVIN NEIL](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Bike Lanes  
**Date:** Thursday, July 9, 2020 11:40:32 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Hello Kevin;

I am the Vice Chair for the North Indian Trail Neighborhood. I read with interest your article in the paper about requesting input for the bike routes in the city.

The Indian Trail area is in the very northwest corner of the city and we have no good bike route out of here coming into town. We have talked about this before at neighborhood meetings and a plan was proposed that I think is a good one, if we could make it work.

The south end of Pamala Street dead ends next to the fence for the North Landfill property. This empty land is presently owned by Harlan Douglas. But if the city could move the landfill fence over about ten feet, there would be room to make a bike trail that could go from that point next to Indian Trail Road (on city property), and get the trail down to the start of the four lane section on Indian Trail Road.

From that point south to Francis Street, sometime in the future maybe, it would be nice to get rid of the five foot grass strip at that point (and any other place in the city) and continue the trail on the west side of Indian Trail Road south.

This could make a good bike route from Barnes Road south. It could be named Pamala Bike Trail?

This could be done with minimal money and give a safe bike route south out of our neighborhood.

I hope you will consider this for a future project and I hope this input helps you with your route planning.

Mr. Melvin Neil  
NITNC Vice Chairman



**From:** [MELVIN NEIL](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: Bike Lanes  
**Date:** Thursday, July 16, 2020 10:39:45 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Hello again Kevin;

I have had a couple new ideas on the bike route out of Indian Trail Area.

On my earlier plan I said it could run on the old dump site south along Indian trail Road to the four lanes and then come back out to Indian Trail south to Francis. I think I have a better idea.

How about staying on the dump site at that point and going south about a block more and coming out around Pamela Ct. and then staying on the street that is a block west of Indian Trail Rd. and going south all the way down to Yokes store that is close to Francis.

Also that route could branch southwest and stay on the dump site along the fence and come out down close to the Rifle Club Rd. and that would lead over to Riverside State Park Rd.

All of this could be done on city property and be fairly safe route from traffic.

Mr. Melvin Neil



**From:** [Erik Powell](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** Bike Plans for Spokane  
**Date:** Monday, July 6, 2020 8:54:42 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Good Morning,

I just read the encouraging article in today's Spokesman Review regarding the expansion of bike lanes in Spokane. While we have made great progress, we definitely need more safe lanes for bikes. I love the lanes along the South Hill, for example, and can ride safely for miles; it would be wonderful to see lanes throughout the city as well.

Have a great day,

Erik Powell



**From:** [tim shauvin](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** bike rules  
**Date:** Monday, July 6, 2020 7:24:19 AM

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**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

Sure would be nice if more drivers were educated on when to stop for a bike rider trying to cross the street. When I pull up to a stop sign to wait for traffic to clear so I can proceed sometimes a driver stops and disrupts the flow of traffic, angers drivers behind and coming from the other direction so I can cross when I know they do not have to stop for me unless I was off my bike and pushing it. That is to me very annoying because I've had police cars keep on driving just like I was an automobile or motorcyclist so drivers seem a bit confused about a bike rider not being the same as a pedestrian crossing at a crosswalk. I have even attempted to illegally cross mission by st als church in mid block and they stop and wave me thru come on dumb drivers I can wait for traffic to clear then go like I should! Yes I know crossing midblock is wrong but I would only do it if traffic was light so most of the time best i cross at traffic light intersection just make sure you let driver who may be turning right see you by making eye contact. I love riding my bike but stupid drivers can make what seems like such a simple procedure as difficult as finding a cure for corona virus!!

Sent from [Mail](#) for Windows 10



**From:** [Gwinn, Nathan](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** FW: Bicycle plans  
**Date:** Monday, July 6, 2020 2:43:04 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

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**From:** Cindie Smith <[smithcindie8@gmail.com](mailto:smithcindie8@gmail.com)>  
**Sent:** Monday, July 6, 2020 2:22 PM  
**To:** Gwinn, Nathan <[ngwinn@spokanecity.org](mailto:ngwinn@spokanecity.org)>  
**Subject:** Re: Bicycle plans

[CAUTION - EXTERNAL EMAIL - Verify Sender]

I appreciate your reply Mr Gwinn. This is an important issue for me.  
Sincerely  
Cindie Smith

On Mon, Jul 6, 2020, 1:47 PM Gwinn, Nathan <[ngwinn@spokanecity.org](mailto:ngwinn@spokanecity.org)> wrote:

Thank you for your comment, Ms. Smith. I will forward it to my colleague, Kevin Freibott, who is collecting comments for this year's City Comprehensive Plan amendments.

Sincerely,



**Nathan Gwinn, AICP** | Assistant Planner | Planning & Development  
509.625.6893 | [ngwinn@spokanecity.org](mailto:ngwinn@spokanecity.org) | [www.spokanecity.org](http://www.spokanecity.org)



**Schedule:** This week, I am working from home Monday through Friday, July 6-10.

**ADVISORY:** Please be advised the City of Spokane is required to comply with the Public Records Act Chapter 42.56 RCW. This act establishes a strong state mandate in favor of disclosure of public records. As such, the information you submit to the City via email, including personal information, may ultimately be subject to disclosure as a public record.

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**From:** Cindie Smith <[smithcindie8@gmail.com](mailto:smithcindie8@gmail.com)>  
**Sent:** Monday, July 6, 2020 11:59 AM  
**To:** Gwinn, Nathan <[ngwinn@spokanecity.org](mailto:ngwinn@spokanecity.org)>  
**Subject:** Fwd: Bicycle plans



[CAUTION - EXTERNAL EMAIL - Verify Sender]

Good Morning Mr. Gwinn,

I am writing regarding the Bicycle plan for Spokane and Spokane County. After reading an article in the online Review this morning I had to find out how to comment and after researching I found your name and email. I want to thank you in advance for reading my story and suggestions.

I have been a bike rider in my younger days and it was a great way to get around. I now live south of Spokane off of Valley Chapel Road. My mind is now changing about bike riders. As you might know Valley Chapel is a curvey country road with no shoulders. Too many times during the spring, summer and fall I have almost been hit and almost hit oncoming cars or a bike rider. There are only tiny gravel shoulders on this road and the bicyclists ride 2 to 4 abreast and continue to ride in the middle of the road not allowing cars to pass.

Someone will be killed on this road!

My suggestions are this: 1- Spokane/Spokane County MUST designate roads like Valley Chapel as "NO Bike" roads, Roads such as Valley Chapel are not safe, period! 2- ALL bicyclists that ride on city/county streets must be licensed after passing a bike riders rules and safety class, 3- Bicycles must be licensed. These monies then can be used to create and maintain bike paths. 4- Bicycles are allowed on safe bike paths only.

I hope these suggestions can be considered/added to the comprehensive bicycle plan for Spokane.

Thank you

Sincerely

Cindie A Smith

Valleyford WA



**From:** [Jessica Engelman](#)  
**To:** [Freibott, Kevin](#)  
**Cc:** [Quinn-Hurst, Colin](#)  
**Subject:** Proposed Bike Network Map amendments  
**Date:** Friday, August 7, 2020 12:27:29 PM

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

Hi Kevin,

I wanted to voice my support for the various proposed amendments to the Bike Network Map and other documents.

"Protected bike lane" is definitely the term most widely used and understood, not only locally but elsewhere in the region and country. Also relating to protected bike lanes, we should not only be considering all bike lanes for receiving protection, but make physical protection our default treatment for on-street separated bicycle facilities moving forward, for the sake of making our cycling network more welcoming and accessible to all.

I also agree with the various proposed changes to individual bike route classifications. These proposals appear to reflect current conditions, best opportunities, and neighborhood input and support. If anything I was hoping to see a few more routes make the cut to be upgraded to "greenway" status, such as current "bike-friendly" routes that already serve as critical connections in our cycling network, as well as routes that have been identified by neighborhoods as desired shared cycling facilities.

Finally, I would like to voice especially strong support for the proposed protected bike lane on the Boone/Atlantic/Sharp corridor in the North Bank. This proposal was the result of a thorough consideration of the needs and challenges in the North Bank transportation network, and many conversations involving a wide variety of stakeholders. It was a response to a grassroots campaign led by Spokane residents concerned about the disruption the vacation of Cataldo to construct the Sportsplex would have on an already fragmented, auto-oriented area of our transportation system. The North Bank serves as the gateway between downtown and the northern neighborhoods, as well as a connection for the southern Logan, Emerson-Garfield, and West Central neighborhoods, yet has a disconnected street grid that provides few safe and comfortable routes to North Bank destinations and beyond. The state of the area's transportation network for individuals with mobility impairments is especially lacking, and was a major motivation behind the campaign to secure a safe, accessible alternative east-west route following the loss of Cataldo. The addition of separated bicycle facilities will also traffic-calm an overbuilt corridor and provide more transportation options for people attending events at the Sportsplex, Arena, and Riverfront Park.

Thank you for consideration of my comments.

Sincerely,  
Jessica Engelman



**From:** [Michael Gaffaney](#)  
**To:** [Monte Koch](#); [Note, Inga](#)  
**Cc:** [Matt Meyer](#); [Stephanie Curran](#); [Andrew Young - CSC](#); [Quinn-Hurst, Colin](#)  
**Subject:** RE: Review Boone and Mallon Traffic Control  
**Date:** Thursday, July 30, 2020 11:52:20 AM  
**Attachments:** [image002.png](#)

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

My only comment is could a left turn arrow light at Washington and Boone N/S be a be installed to help the situation (since you mention light timing).

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**From:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Sent:** Thursday, July 30, 2020 9:59 AM  
**To:** 'Note, Inga' <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

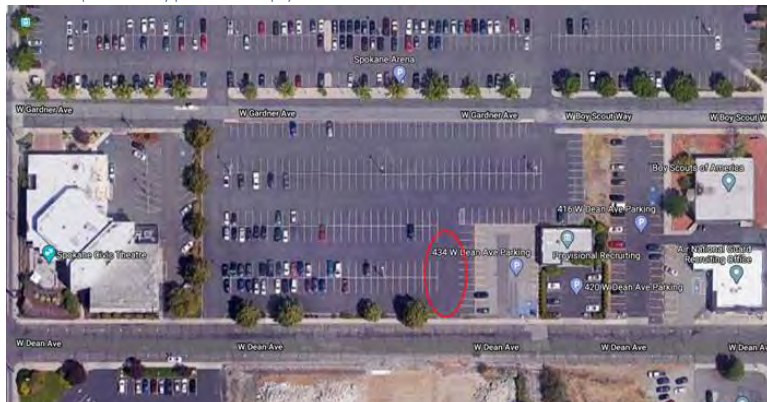
Not as a part of the Sportsplex project. We may collect fees for the south lot via pay-by-phone or by means of a kiosk when the venue opens next year.

**Monte Koch**, Director of Facilities & Operations  
Spokane Public Facilities District  
[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)  
Phone 509.279.7169 Mobile 509.951.6969  
720 West Mallon Avenue • Spokane • WA 99201

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**From:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Sent:** Thursday, July 30, 2020 9:51 AM  
**To:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

One more question. Any plans to add a pay booth at the Dean Avenue access?



---

**From:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Sent:** Thursday, July 30, 2020 9:35 AM  
**To:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** FW: Review Boone and Mallon Traffic Control

[CAUTION - EXTERNAL EMAIL - Verify Sender]

Inga,  
Below is input from Tom Malone, Diamond Parking Manager for the District properties.

**Monte Koch**, Director of Facilities & Operations  
Spokane Public Facilities District  
[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)  
Phone 509.279.7169 Mobile 509.951.6969  
720 West Mallon Avenue • Spokane • WA 99201

---

**From:** Tom Malone <[Tom.Malone@DiamondParking.com](mailto:Tom.Malone@DiamondParking.com)>  
**Sent:** Thursday, July 30, 2020 9:31 AM  
**To:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

Monte,

Average transaction time is 15seconds cash, and around 30 seconds for credit card. This is really dependent on the person paying and as a person may write a check or realize they can't



find there payment. We do have two lanes so we can handle 2 vehicles per 15 second transaction time cash or per 30 seconds if they are using cards.

In reading over the analysis, it seems they are admitting this plan will not work even at existing Arena traffic levels, there are multiple factors that they are not considering.

Other factors to consider are multiple events or STA parkers where the exiting vehicles and the entering vehicles may be encountering each other, Imagine Garth Brooks with only the two lanes on Boone. The Sports plex will definitely impact traffic in the area during an Arena event has the current traffic lights are not enough for the current traffic for the past Arena events. We currently have the majority of Transit parkers exiting lots C&D between 430pm and 6pm and this is the time that a weekday concert is also beginning to arrive, with the Sportsplex they may have an event arriving or exiting at this same time adding to the vehicles in the area. Boone restrictions would only compound this issue.


The current real issue is the vehicles trying to enter the Washington street entrance(From the Northbound lanes)trying to cross South Bound traffic have to wait for a break with the Boone/Washington intersection. This back up will then meet up with those traveling North Bound trying to using the Boone/Washington intersection where sometimes only 1 Or 2 vehicles are able to take a left onto Boone. The current back ups that occur are very rarely caused at the parking payment booth most occur due to the street lights and vehicles trying to cross lanes. The other issue is vehicles traveling west bound on Boone trying to take a left onto Howard, this light currently has the same issue even with the police directing only a couple vehicles are to make the left hand turn onto Howard per light.

Now I understand typical daily traffic may be fine for the lane reduction however the area around the Arena is being developed so past traffic usage really isn't accurate for the future demands we will see. Projects like Sportsplex, LBstone development, the Wonder Building and other upcoming projects, will impact the traffic and can result in frustrating traffic situations are large delays for entering or exiting the facilities.

**Thomas Malone**  
City Manager / Spokane / Diamond Parking Services  
Office (509)747-8144 / Cell (509)723-7652

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**From:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Sent:** Tuesday, July 28, 2020 3:12 PM  
**To:** Tom Malone <[Tom.Malone@DiamondParking.com](mailto:Tom.Malone@DiamondParking.com)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>  
**Subject:** FW: Review Boone and Mallon Traffic Control

**Caution:** This email originated from outside **Diamond Parking Email systems**. Do not click or open attachments unless you recognize the sender and are confident the content is safe.  
Tom,  
Here is the Traffic Impact Analysis Report that we spoke of today. Your input is appreciated.

**Monte Koch**, Director of Facilities & Operations  
Spokane Public Facilities District  
[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)  
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---

**From:** Note, Inga <[ingnote@spokanecity.org](mailto:ingnote@spokanecity.org)>  
**Sent:** Tuesday, July 28, 2020 12:49 PM  
**To:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>; Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>  
**Cc:** Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

To the PFD staff,  
Attached is my draft memorandum on the impacts of the proposed Boone Avenue protected bike lane. Can you please review my writeup and images depicting the current event traffic control? I'm hoping to finalize this before end of the day Thursday.  
If you have any information on the average time it takes to collect parking payment, that would be helpful.  
Thanks  
Inga

-----Original Appointment-----  
**From:** Quinn-Hurst, Colin  
**Sent:** Tuesday, July 7, 2020 12:18 PM  
**To:** Quinn-Hurst, Colin; Note, Inga; Michael Gaffaney; [scurran@spokanepfd.org](mailto:scurran@spokanepfd.org); [mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org); [mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)  
**Cc:** Andrew Young  
**Subject:** Review Boone and Mallon Traffic Control  
**When:** Wednesday, July 15, 2020 3:00 PM-4:00 PM (UTC-08:00) Pacific Time (US & Canada).  
**Where:** Webex - <https://spokanecity.webex.com/spokanecity/j.php?MTID=m315d3ba19e15e77245ece38502ff9f5b>

Switching this to an online meeting:

**Meeting Information**  
Meeting link:  
<https://spokanecity.webex.com/spokanecity/j.php?MTID=m315d3ba19e15e77245ece38502ff9f5b>  
Meeting number:



146 468 3588

Password:

ZPv5Uy2w3Pa

Host key:

741744

#### More ways to join

Join by video system

Dial [1464683588@spokanecity.webex.com](tel:1464683588)

You can also dial 173.243.2.68 and enter your meeting number.

Join by phone

+1-408-418-9388 United States Toll

Access code: 146 468 3588

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## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z20-019COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment proposal seeking to amend Map TR5 of the Comprehensive Plan in Chapter 4: Transportation, and related text amendments to the City of Spokane Bicycle Master Plan, located in Appendix D of the Comprehensive Plan.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment proposal Z20-019COMP (the “Proposal”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Proposal seeks to amend Map TR5 of the Comprehensive Plan to update facility type designations for thirteen segments of the City’s planned bicycle network to reflect updates in transportation patterns, land use and development patterns and design standards as well as related text amendments seek to update bikeway facility type descriptions in the City of Spokane Bicycle Master Plan, located in Appendix D of the Comprehensive Plan.
- E. Included in the proposed amendments is a change to the bicycle facility designation for the bike route segment on Boone Avenue between Howard Street and Atlantic Street, on Atlantic Street between Boone Avenue and Sharp Avenue, and on Sharp Avenue between Atlantic Street and Division Street in conformance with City Council Resolution 2019-0098.
  - 1. The potential traffic impacts that might occur from the installation of bike facilities on Boone Avenue were addressed in a traffic analysis memo prepared by the Integrated Capital Management department.
- F. Annual amendment proposals are subject to a threshold review process to determine whether the proposals will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- G. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, and including the Proposal in the Work Program.



- H. Thereafter, on April 24, 2020, staff requested comments from agencies, departments, and neighborhood councils. The City received comments stating no concerns and one requesting an adjustment to the future facility identification for Garland Avenue to be consistent with the City of Spokane's Six-Year Streets Plan and a modification to related text in the Bicycle Master Plan, located in Appendix D of the Comprehensive Plan.
- I. On June 9, 2020, staff reissued the request for comments from agencies, departments, and neighborhood councils with the adjustment of Modification 2 for W Strong Rd to extend east to N Austin Road, and with the addition of text amendments related to the proposed facility designation adjustments.
- J. A Notice of Application was published on June 8, 2020 by posting it in the Spokesman Review. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City received comments that included support for the proposed amendments, requests for reconsideration of specific proposals, requests for additional changes and minor adjustments to the proposals, and requests that would be appropriate for consideration in future Comprehensive Plan amendments.
- K. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Proposal, and was provided with information regarding the dates of Plan Commission workshops and hearings.
- L. On June 10, 2020, the Spokane City Plan Commission held a workshop to study the Proposal.
- M. On June 16, 2020, the Bicycle Advisory Board received a presentation regarding the Proposal.
- N. On June 23, 2020, the Pedestrian Transportation and Traffic Committee of the Community Assembly received a presentation regarding the Proposal.
- O. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Proposal.
- P. On July 7, 2020, the Plan Commission Transportation Subcommittee received a presentation regarding the Proposal.
- Q. On July 30, 2020, an online public workshop specific to the bicycle- and transportation-related amendments was provided for the general public to answer questions and receive comments on the proposed amendments.
- R. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Proposal. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the application was published in the Official Gazette on September 2 and 9, 2020.
- S. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.



- T. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of a SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- U. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Proposal, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Proposal recommended approval of the Proposal.
- V. On September 9, 2020, the Plan Commission held a public hearing on the Proposal, including the taking of verbal testimony, and closed the public record, postponing deliberations until the following hearing date.
- W. All comments received prior to the close of the public record were forwarded to the Plan Commission by City staff.
- X. Members of the public testified both in opposition and in support of certain proposed future bikeway designation updates.
- Y. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- Z. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Proposal (the "Staff Report").
- AA. The Plan Commission finds that the Proposal meets the intent and requirements of the Comprehensive Plan.
- BB. The Plan Commission finds that the Proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.
- CC. The Plan Commission notes that design considerations of the eventual implementation of improvements to Boone Ave (shown as Modification 10 in the maps of the Staff Report) should consider and reduce any traffic or safety impacts arising on that road as they relate to special events at the Arena or Podium facilities, perhaps through the use of electronic signage or a modular design that can be changed during events to accommodate greater vehicle loads.

## **CONCLUSIONS:**

Based upon the Proposal materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding Proposal File No. Z20-019COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

- 1. The Proposal was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Proposal was submitted as provided in SMC 17G.020.050(D).



2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Proposal is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Proposal will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Proposal is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Proposal is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Proposal has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Proposal.
9. The Proposal will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Proposal proposes a map amendment that is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment is suitable for the proposed designation.
12. The map amendment would implement applicable comprehensive plan policies better than the current map designation.

## **RECOMMENDATIONS:**

In the matter of Z20-019COMP, a request by City of Spokane staff to amend Map TR5 in Chapter 4: Transportation of the Comprehensive Plan to update selected future bikeway designations within the planned citywide bicycle network as well as corresponding text amendments within the City of Spokane Bicycle Master Plan, located in Appendix D of the Comprehensive Plan, as based upon the above listed findings and conclusions, by a vote of **8 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to the Map TR5 in Chapter 4: Transportation of the City's Comprehensive Plan and related text amendments within the City's Bicycle Master Plan, and authorizes the President to prepare and sign on the Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the Proposal.





---

**Todd Beyreuther, President**

Spokane Plan Commission

October 21, 2020



**From:** [Freibott, Kevin](#)  
**To:** [Raychel Callary](#); [North Bank Bikeways DL](#)  
**Cc:** [Quinn-Hurst, Colin](#); [Black, Tirrell](#)  
**Subject:** RE: Support for bike lanes on Boone  
**Date:** Monday, September 14, 2020 10:11:00 AM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

NOTE: The following comment was received after completion of the Staff Report and after the Plan Commission closed the public record for their consideration of this proposal.

Thank you for your comment, Raychel. Because the Plan Commission closed the public record for application Z20-019COMP during the September 9 hearing, I will hold this email until after they make their decision. After that, I will make sure to give it to City Council for their consideration during the final hearing for the application—tentatively scheduled in October or November. Thanks again and have a great day!

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



**From:** Raychel Callary <[raychelcallary@gmail.com](mailto:raychelcallary@gmail.com)>  
**Sent:** Thursday, September 10, 2020 2:17 PM  
**To:** North Bank Bikeways DL <[northbankbikeways@spokanecity.org](mailto:northbankbikeways@spokanecity.org)>  
**Subject:** Support for bike lanes on Boone

**[CAUTION - EXTERNAL EMAIL - Verify Sender]**

The addition of bike lanes on Boone between Howard and Washington would greatly enhance the pedestrian experience. The sidewalk on the north side of Boone is very narrow, and is uncomfortably close to traffic. A rock wall between Howard and Stevens prevents pedestrians from stepping off of the sidewalk to prevent a conflict if necessary. Many people with disabilities utilize services within a few blocks (Lilac Services for the Blind, DVR, Aging and Long Term Care, Nexus Inland NW, etc.). Bike lanes would also provide a much-needed space for scooters and other vehicles that often otherwise use the sidewalk.

When vacating Cataldo was discussed by city council, the need for bicycle access was discussed at length with a result that bike lanes would be installed on Boone (and perhaps other areas, but I was following the Boone discussion most carefully). If that is disregarded, a valuable opportunity for connectivity would be lost. As an "amatuer" cyclist, I would not want to ride on that section of Boone but would love to be able to.

Thank you for reading my comments!

Raychel Callary





## MEMO

### **Z20-019COMP – Proposed Amendments to Map TR-5, Proposed Bike Network Map**

October 26, 2020

To: Council President and Council Members

From: Colin Quinn-Hurst, Planning Services Department

**Re: Additional Packet Information for File Z20-019COMP TR5 Bikeway Amendments, Modification 10 – Boone, Atlantic and Sharp Ave.**

In evaluating proposed amendments to the Bicycle Master Plan included in proposed Comprehensive Plan amendment Z20-019COMP, proposing amendments to Bicycle Master Plan Map TR-5, it was apparent that accommodating a painted or physically-separated bicycle facility along Boone Street from Howard Street to Atlantic Street would require either converting existing automotive travel lanes to on-street bicycle facilities or converting adjacent private property to bikeways along the Spokane Arena street frontage.

Although including this project now on Bicycle Master Plan Map TR-5 is a policy-level decision and does not make project level decisions or commit to specific designs, layouts or treatments, additional assessment was conducted to provide additional background information related to this modification.

City staff conducted initial assessment in early July of 2020 potential impacts on automotive travel, turning movements, and adjacent properties. City staff held an online meeting on the afternoon of Wednesday July 15, 2020 with the Public Facilities District (PFD), the entity responsible for managing the Spokane Arena and adjacent public properties and facilities. PFD staff and associated parking management company staff from Diamond Parking subsequently submitted comments via email addressing private property impacts and event parking and traffic flows, included here. The traffic assessment memo was finalized on July 30, 2020, and the final draft of the assessment is included here, as well as email communication from the Public Facilities District regarding this matter.





## INTEGRATED CAPITAL MANAGEMENT

**DATE:** July 30th, 2020

**TO:** Project File - Staff Report

**FROM:** Inga Note, P.E., Integrated Capital Management

**CC:** Colin Quinn-Hurt; Kevin Freibott; Katherine Miller

**SUBJECT:** Comprehensive Plan Amendment – Z20-019COMP  
*Boone Avenue – Atlantic Street – Sharp Avenue bike lane addition*

This memorandum summarizes the traffic impacts of the proposed amendments to TR-5 Bike Network Map. It specifically focuses on the impacts of adding a bike lane to Boone Avenue from Howard to Atlantic, and on Sharp Avenue between Atlantic and Lidgerwood. Establishment of bike lanes in these areas would require reducing the overall road section from five vehicle lanes to three vehicle lanes. The proposed bike lane would be a protected with paint buffers, flexible bollards, planters or curbs.

*Figure 1. Proposed Buffered Bike Lane - Howard to Washington*



*Figure 2. Proposed Buffered Bike Lane – Washington to Atlantic*





*Figure 3. Proposed Buffered Bike Lane – Atlantic to Ruby*



### **Daily Traffic Volumes**

In 2019 this section of Boone Avenue from Howard to Washington carried an average of 11,000 vehicles per day (vpd). East of Washington the volumes drop to 8,000 vpd and continue to drop to around 6,000 vpd as the route wraps around to Sharp Avenue. These volumes are well within the range of traffic that can be accommodated with a three-lane road. Sharp Avenue already transitions to three lanes east of Lidgerwood.

### **Spokane Arena Event Traffic**

The bigger concern with this proposal is the impact to event traffic entering and exiting the Spokane Arena parking lots. It is well known that evening events at the arena have a large impact on local through traffic movement on Boone Avenue, Washington Street, Howard Street, Lincoln Street, Mallon Avenue and sometimes Monroe Street.

#### *Event Traffic Arrival*

During large events at the Spokane Arena people start arriving about 1.5 hours prior to the show. Attendees can enter the parking lots at several points but must stop at a pay booth. Payment is handled via cash or card. Cash transactions average 15 seconds per vehicle and credit cards take 30 seconds.

*Figure 4. Pay stations and parking lot entry points*



Because of the pay booths it is common for queues to form outside the entry points. These include the left turn from Boone Avenue to Howard Street (figure 2) and also the eastbound curb lane on Boone east of Lincoln Street (figure 3). The second through lane in both of these conditions allows for non-event traffic to continue to use Boone Avenue. The pay booths cannot be required to relocate unless the PFD was seeking a permit to make changes to the site. Queuing is also common for northbound Washington Street where the drivers are making



a left turn into the parking lot (Boy Scout Way). The queuing on Washington is caused more by a lack of gaps in southbound traffic than delays at the pay booth.

With removal of the second through lane, non-event traffic is likely to drive illegally in the center turn lane to bypass the queued vehicles. This will create additional conflicts with vehicles turning in and out of driveways in the block.

*Figure 5. Existing traffic pattern during event arrival*



*Figure 6. Existing traffic pattern during event arrival*





### Event Traffic Departure

Clearing out the parking lots after an event takes about 30-45 minutes. Traffic may exit from multiple locations and the dual lanes on Boone are used to accommodate this. Traffic control is handled by a combination of Spokane Arena staff and the police department.

*Figure 7. Existing traffic pattern during event departure*



Loss of the second travel lane on Boone will slow down the egress rate from the Spokane Arena parking lots. Event attendees will need to plan for a longer departure travel time or park in a different location. The impact on local traffic will be more limited since these events often end in the late evening when volumes are down.

### **Signal Impacts**

If the comprehensive plan amendment is approved, an analysis of the signalized intersections will be required during the design phase. Depending on the turn volumes it may be beneficial to keep right-turn pockets and share that space with the bikes. Other options at the intersections could include widening or using space behind the curb for the bike lane. Queue lengths and available storage should be evaluated between Division and Browne to ensure the change will not result in queues exceeding the distance between the intersections. This can sometimes be addressed through signal timing adjustments, but not always without adding delay to the through traffic on Browne and Division.

### **Multiple Events**

Consideration should also be given to the close proximity of venues and potential for multiple events at the same time. The Spokane Arena, Civic Theater, Riverfront Park, and Sportsplex all utilize the same access roads and parking lots. It is common for events at the Arena and the Theater to overlap. The northeast parking lot is also used as a park and ride for downtown commuters. So there is potential for arriving event-goers to conflict with commuters departing from the lot between 4:30 and 6:00 pm on weekdays.

### **Emergency Response**

Traffic prior to and following an event creates challenges for emergency response. Current practice is to re-route around the area during times of congestion. However the loss of the additional traffic lane on Boone would create challenges for response to the adjacent businesses, Spokane Arena, Civic Theater or the Sportsplex.



### **Pathway Option from Howard to Washington**

The biggest area of concern for repurposing vehicular travel lanes to bike lanes is on Boone between Howard and Washington. One alternative would be to maintain the lanes to Washington and provide a shared-use pathway between the existing sidewalk and parked vehicles in the lot. This would require the PFD to reconfigure the parking lot and may result in a loss of parking spaces. Implementation of this concept would require further discussion and cooperation with the PFD as the city cannot require this change.

*Figure 8. Shared-use Pathway Option*



### **Requirements for Implementation**

There are several treatments that should be considered if the Boone Avenue comprehensive plan amendment is approved and the protected bike lane project moves forward. These treatments are focused on the street right-of-way.

- Lengthen the westbound left turn pocket at Boone/Howard so that more entering vehicles can queue in the center lane. This will require removal of the concrete island and two trees.
- Conduct further evaluation of signalized intersections to determine if keeping a right-turn lane as a shared facility with bikes is needed, or using space beyond the curb for the bike lane. Also evaluate the signal timing plans at Division/Sharp and Ruby/Sharp using the reduced street section.
- Addition of north-south protected left turn phasing at Boone/Washington would help to alleviate the queuing on Washington prior to an event.
- Further explore the Howard to Washington parking lot pathway concept with the PFD.



**From:** [Michael Gaffaney](#)  
**To:** [Monte Koch](#); [Note, Inga](#)  
**Cc:** [Matt Meyer](#); [Stephanie Curran](#); [Andrew Young - CSC](#); [Quinn-Hurst, Colin](#)  
**Subject:** RE: Review Boone and Mallon Traffic Control  
**Date:** Thursday, July 30, 2020 11:52:20 AM  
**Attachments:** [image002.png](#)

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[CAUTION - EXTERNAL EMAIL - Verify Sender]

My only comment is could a left turn arrow light at Washington and Boone N/S be a be installed to help the situation (since you mention light timing).

---

**From:** Monte Koch <[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)>  
**Sent:** Thursday, July 30, 2020 9:59 AM  
**To:** 'Note, Inga' <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanebfd.org](mailto:mgaffaney@spokanebfd.org)>; Matt Meyer <[mmeyer@spokanebfd.org](mailto:mmeyer@spokanebfd.org)>; Stephanie Curran <[scurran@spokanebfd.org](mailto:scurran@spokanebfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

Not as a part of the Sportsplex project. We may collect fees for the south lot via pay-by-phone or by means of a kiosk when the venue opens next year.

**Monte Koch**, *Director of Facilities & Operations*  
Spokane Public Facilities District  
[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)  
Phone 509.279.7169 Mobile 509.951.6969  
720 West Mallon Avenue • Spokane • WA 99201

---

**From:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Sent:** Thursday, July 30, 2020 9:51 AM  
**To:** Monte Koch <[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanebfd.org](mailto:mgaffaney@spokanebfd.org)>; Matt Meyer <[mmeyer@spokanebfd.org](mailto:mmeyer@spokanebfd.org)>; Stephanie Curran <[scurran@spokanebfd.org](mailto:scurran@spokanebfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

One more question. Any plans to add a pay booth at the Dean Avenue access?



---

**From:** Monte Koch <[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)>  
**Sent:** Thursday, July 30, 2020 9:35 AM  
**To:** Note, Inga <[inote@spokanecity.org](mailto:inote@spokanecity.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanebfd.org](mailto:mgaffaney@spokanebfd.org)>; Matt Meyer <[mmeyer@spokanebfd.org](mailto:mmeyer@spokanebfd.org)>; Stephanie Curran <[scurran@spokanebfd.org](mailto:scurran@spokanebfd.org)>; Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>  
**Subject:** FW: Review Boone and Mallon Traffic Control

[CAUTION - EXTERNAL EMAIL - Verify Sender]

Inga,  
Below is input from Tom Malone, Diamond Parking Manager for the District properties.

**Monte Koch**, *Director of Facilities & Operations*  
Spokane Public Facilities District  
[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)  
Phone 509.279.7169 Mobile 509.951.6969  
720 West Mallon Avenue • Spokane • WA 99201

---

**From:** Tom Malone <[Tom.Malone@DiamondParking.com](mailto:Tom.Malone@DiamondParking.com)>  
**Sent:** Thursday, July 30, 2020 9:31 AM  
**To:** Monte Koch <[mkoch@spokanebfd.org](mailto:mkoch@spokanebfd.org)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanebfd.org](mailto:mgaffaney@spokanebfd.org)>; Stephanie Curran <[scurran@spokanebfd.org](mailto:scurran@spokanebfd.org)>; Matt Meyer <[mmeyer@spokanebfd.org](mailto:mmeyer@spokanebfd.org)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

Monte,

Average transaction time is 15seconds cash, and around 30 seconds for credit card. This is really dependent on the person paying and as a person may write a check or realize they can't



find there payment. We do have two lanes so we can handle 2 vehicles per 15 second transaction time cash or per 30 seconds if they are using cards.

In reading over the analysis, it seems they are admitting this plan will not work even at existing Arena traffic levels, there are multiple factors that they are not considering.

Other factors to consider are multiple events or STA parkers where the exiting vehicles and the entering vehicles may be encountering each other, Imagine Garth Brooks with only the two lanes on Boone. The Sports plex will definitely impact traffic in the area during an Arena event has the current traffic lights are not enough for the current traffic for the past Arena events. We currently have the majority of Transit parkers exiting lots C&D between 430pm and 6pm and this is the time that a weekday concert is also beginning to arrive, with the Sportsplex they may have an event arriving or exiting at this same time adding to the vehicles in the area. Boone restrictions would only compound this issue.

The current real issue is the vehicles trying to enter the Washington street entrance(From the Northbound lanes)trying to cross South Bound traffic have to wait for a break with the Boone/Washington intersection. This back up will then meet up with those traveling North Bound trying to using the Boone/Washington intersection where sometimes only 1 Or 2 vehicles are able to take a left onto Boone. The current back ups that occur are very rarely caused at the parking payment booth most occur due to the street lights and vehicles trying to cross lanes. The other issue is vehicles traveling west bound on Boone trying to take a left onto Howard, this light currently has the same issue even with the police directing only a couple vehicles are to make the left hand turn onto Howard per light.

Now I understand typical daily traffic may be fine for the lane reduction however the area around the Arena is being developed so past traffic usage really isn't accurate for the future demands we will see. Projects like Sportsplex, LBstone development, the Wonder Building and other upcoming projects, will impact the traffic and can result in frustrating traffic situations are large delays for entering or exiting the facilities.

**Thomas Malone**  
City Manager / Spokane / Diamond Parking Services  
Office (509)747-8144 / Cell (509)723-7652

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**From:** Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>  
**Sent:** Tuesday, July 28, 2020 3:12 PM  
**To:** Tom Malone <[Tom.Malone@DiamondParking.com](mailto:Tom.Malone@DiamondParking.com)>  
**Cc:** Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>  
**Subject:** FW: Review Boone and Mallon Traffic Control

**Caution:** This email originated from outside **Diamond Parking Email systems**. Do not click or open attachments unless you recognize the sender and are confident the content is safe.  
Tom,  
Here is the Traffic Impact Analysis Report that we spoke of today. Your input is appreciated.

**Monte Koch**, Director of Facilities & Operations  
Spokane Public Facilities District  
[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)  
Phone 509.279.7169 Mobile 509.951.6969  
720 West Mallon Avenue • Spokane • WA 99201

---

**From:** Note, Inga <[ingate@spokanecity.org](mailto:ingate@spokanecity.org)>  
**Sent:** Tuesday, July 28, 2020 12:49 PM  
**To:** Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>; Michael Gaffaney <[mgaffaney@spokanepfd.org](mailto:mgaffaney@spokanepfd.org)>; Stephanie Curran <[scurran@spokanepfd.org](mailto:scurran@spokanepfd.org)>; Monte Koch <[mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org)>; Matt Meyer <[mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)>  
**Cc:** Andrew Young - CSC <[ayoung@csc-usa.com](mailto:ayoung@csc-usa.com)>  
**Subject:** RE: Review Boone and Mallon Traffic Control

To the PFD staff,  
Attached is my draft memorandum on the impacts of the proposed Boone Avenue protected bike lane. Can you please review my writeup and images depicting the current event traffic control? I'm hoping to finalize this before end of the day Thursday.  
If you have any information on the average time it takes to collect parking payment, that would be helpful.  
Thanks  
Inga

-----Original Appointment-----  
**From:** Quinn-Hurst, Colin  
**Sent:** Tuesday, July 7, 2020 12:18 PM  
**To:** Quinn-Hurst, Colin; Note, Inga; Michael Gaffaney; [scurran@spokanepfd.org](mailto:scurran@spokanepfd.org); [mkoch@spokanepfd.org](mailto:mkoch@spokanepfd.org); [mmeyer@spokanepfd.org](mailto:mmeyer@spokanepfd.org)  
**Cc:** Andrew Young  
**Subject:** Review Boone and Mallon Traffic Control  
**When:** Wednesday, July 15, 2020 3:00 PM-4:00 PM (UTC-08:00) Pacific Time (US & Canada).  
**Where:** Webex - <https://spokanecity.webex.com/spokanecity/j.php?MTID=m315d3ba19e15e77245ece38502ff9f5b>

Switching this to an online meeting:

#### Meeting Information

Meeting link:  
<https://spokanecity.webex.com/spokanecity/j.php?MTID=m315d3ba19e15e77245ece38502ff9f5b>

Meeting number:



146 468 3588

Password:

ZPv5Uy2w3Pa

Host key:

741744

#### More ways to join

Join by video system

Dial [1464683588@spokanecity.webex.com](tel:1464683588)

You can also dial 173.243.2.68 and enter your meeting number.

Join by phone

+1-408-418-9388 United States Toll

Access code: 146 468 3588

[Global call-in numbers](#)



**From:** [Pfister, Terri](#)  
**To:** [Price, Laura](#); [Williams, Stephen](#)  
**Subject:** FW: Additional Comment Letter - Z20-019COMP - Bike Network Map, Comp Plan Amendment  
**Date:** Tuesday, November 10, 2020 6:58:29 PM  
**Attachments:** [image003.png](#)  
[image004.png](#)  
[image005.png](#)  
**Importance:** High

---

Please add to Current / Advance packets under ORD C35978.

---

**From:** Freibott, Kevin <kfreibott@spokanecity.org>  
**Sent:** Tuesday, November 10, 2020 10:14 AM  
**To:** Pfister, Terri <tpfister@spokanecity.org>  
**Cc:** Black, Tirrell <tblack@spokanecity.org>  
**Subject:** FW: Additional Comment Letter - Z20-019COMP - Bike Network Map, Comp Plan Amendment  
**Importance:** High

Just FYI, we received an additional comment on ORD C35978 which was then forwarded to the Council. See below. Thanks.

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



---

**From:** Freibott, Kevin  
**Sent:** Monday, November 9, 2020 3:08 PM  
**To:** Beggs, Breean <[bbeggs@spokanecity.org](mailto:bbeggs@spokanecity.org)>; Stratton, Karen <[kstratton@spokanecity.org](mailto:kstratton@spokanecity.org)>; Mumm, Candace <[cmumm@spokanecity.org](mailto:cmumm@spokanecity.org)>; Burke, Kate M. <[kateburke@spokanecity.org](mailto:kateburke@spokanecity.org)>; Cathcart, Michael <[mcathcart@spokanecity.org](mailto:mcathcart@spokanecity.org)>; Wilkerson, Betsy <[bwilkerson@spokanecity.org](mailto:bwilkerson@spokanecity.org)>; Kinnear, Lori <[kinnear@spokanecity.org](mailto:kinnear@spokanecity.org)>  
**Cc:** Allers, Hannahlee <[hallers@spokanecity.org](mailto:hallers@spokanecity.org)>; Estaris, Lara <[lestaris@spokanecity.org](mailto:lestaris@spokanecity.org)>; Blackwell, Shae <[sblackwell@spokanecity.org](mailto:sblackwell@spokanecity.org)>; Carlos, Mark <[mcarlos@spokanecity.org](mailto:mcarlos@spokanecity.org)>; Byrd, Giacobbe <[gbyrd@spokanecity.org](mailto:gbyrd@spokanecity.org)>; Watkins, Kandace <[kwatkins@spokanecity.org](mailto:kwatkins@spokanecity.org)>; Overbust, Kyle <[koverbust@spokanecity.org](mailto:koverbust@spokanecity.org)>; Black, Tirrell <[tblack@spokanecity.org](mailto:tblack@spokanecity.org)>  
**Subject:** Additional Comment Letter - Z20-019COMP - Bike Network Map, Comp Plan Amendment  
**Importance:** High

Good afternoon, Council President, members of the Council. Please see the email chain below as it



pertains to ORD C35978 (File Z20-019COMP), the proposed amendments to the Bike Network Map in the Comprehensive Plan. This is the only comment we have received since the Staff Reports and Ordinances were provided to you in On Base. Thanks and see you this afternoon during briefing.

Kevin



**Kevin Freibott** | Planner II | City of Spokane - Planning and Development Services

509.625-6184 | <mailto:kfreibott@spokanecity.org> | [spokanecity.org](http://spokanecity.org) | [spokaneplanning.org](http://spokaneplanning.org)



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**From:** Olsen, Eric <[eolsen@spokanepolice.org](mailto:eolsen@spokanepolice.org)>

**Sent:** Monday, November 9, 2020 12:57 PM

**To:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>; Quinn-Hurst, Colin <[cquinnhurst@spokanecity.org](mailto:cquinnhurst@spokanecity.org)>

**Cc:** Gately, John <[jgately@spokanepolice.org](mailto:jgately@spokanepolice.org)>; Lundgren, Justin <[jclundgren@spokanepolice.org](mailto:jclundgren@spokanepolice.org)>

**Subject:** FW: Spokane City Council Current Agenda for Monday, November 9, 2020, and Advance Agenda for Monday, November 16, 2020

**Importance:** High

Mr. Freibott and Mr. Quinn-Hurst,

It appears there is an item on the Advanced City Council agenda for today relating to the possibility of creating a bike lane on Boone near Washington. I thought I may have seen this was sent to Sgt. Reisenauer for the Police perspective. Sgt. Reisenauer has not been in a position to comment on behalf of Police for several years. I have asked Special Events Sergeant John Gately to review and have attached his comments.

Please cc him on any further projects.

Thank you,  
Eric

Major Eric Olsen | Investigation and Administration | Spokane Police Department  
Desk 509-835-4505 | Cell 509-951-7371 | [eolsen@spokanepolice.org](mailto:eolsen@spokanepolice.org)

---

**From:** Gately, John <[jgately@spokanepolice.org](mailto:jgately@spokanepolice.org)>

**Sent:** Monday, November 9, 2020 10:52 AM

**To:** Olsen, Eric <[eolsen@spokanepolice.org](mailto:eolsen@spokanepolice.org)>

**Subject:** RE: Spokane City Council Current Agenda for Monday, November 9, 2020, and Advance Agenda for Monday, November 16, 2020



Major,

In looking at the map and plan. It appears they do not take into consideration the impacts further out. The day and evening events at the Arena cause traffic congestion north and south on Monroe from Maxwell to Summit Blvd. Boone impacts at time reach out to Maple and Ruby. One of the bigger traffic issues which is only slightly addressed in the report is Washington from Maxwell to North River Dr. North River Dr also backs up to Division.

The report talks about most events at the Arena are in the evening. However prior to 2020 the Arena had started booking more daytime event as which will be the same for the new Sportplex.

Howard has already been reduced to 3 lanes from Mallon to Indiana. During events Howard backs all the way up to Maxwell for the go in and to Indiana for the out go.

The report also talks about the adjustments Emergency Vehicles have to make during events and states that they move to other arterials. I believe they are talking about Mission/Maxwell however they do not mention that Mission/Maxwell has also been reduced to 3 lanes from Washington to Monroe making Indiana the furthest south arterial with 5 lanes.

Hope this gives some thoughts to ask questions or offer concerns.

Thank you,  
John

---

**From:** Olsen, Eric <[eolsen@spokanepolice.org](mailto:eolsen@spokanepolice.org)>

**Sent:** Monday, November 9, 2020 8:16 AM

**To:** Gately, John <[jgately@spokanepolice.org](mailto:jgately@spokanepolice.org)>

**Subject:** FW: Spokane City Council Current Agenda for Monday, November 9, 2020, and Advance Agenda for Monday, November 16, 2020

Good morning John,

Will you please look at the proposed bike lane addition info in the advanced packet? It looks like it would really impact traffic around the Arena.

Thanks,  
Eric

Major Eric Olsen | Investigation and Administration | Spokane Police Department  
Desk 509-835-4505 | Cell 509-951-7371 | [eolsen@spokanepolice.org](mailto:eolsen@spokanepolice.org)

---

**From:** Price, Laura <[lprice@spokanecity.org](mailto:lprice@spokanecity.org)>

**Sent:** Friday, November 6, 2020 4:43 PM

**Subject:** Spokane City Council Current Agenda for Monday, November 9, 2020, and Advance Agenda



for Monday, November 16, 2020

The City Council Current Agenda for Monday, November 9, 2020, is available and can be accessed at: [11-9-2020 Current Packet](#). (Internal link)

The Advance Agenda for Monday, November 16, 2020, is available and can be accessed at: [11-16-2020 Advance Packet](#). (Internal link)

You also can view the agendas/packets online on the City's website at:  
<https://my.spokanecity.org/citycouncil/documents/>.

**Note:** For those whose name appears in red on the Agenda, you will be receiving the WebEx meeting access information from Hannahlee Allers so that you may report on your item virtually during the 3:30 p.m. Briefing Session on **November 9, 2020**. If for some reason you have not received the information by **12:00 p.m. Monday (November 9)**, please contact Hannahlee Allers at [hallers@spokanecity.org](mailto:hallers@spokanecity.org) or 509-625-6715. If you are unable to attend the Briefing Session virtually, contact [Mike Ormsby](#) (x6287) and advise him of who will be taking your place. Please also inform Hannahlee Allers as to who will be taking your place so that she may provide the WebEx meeting information to that person.

Contact the [City Clerk's Office](#) (x6350) if you have any questions.

Thank you,



**Laura Price | City of Spokane | Clerk II**  
**808 W. Spokane Falls Blvd. Spokane, WA 99201**  
**| 509-625-6304 | fax 509.625.6217 | [lprice@spokanecity.org](mailto:lprice@spokanecity.org)**

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# Bicycle Advisory Board - Draft Minutes

February 18, 2020

City Council Briefing Center

Meeting Minutes: Meeting called to order at 6:00 PM by Grant Shipley

## Attendance:

- **Board Members Present:** Grant Shipley (Chair), Rhonda Young, Jessica Engelman (Vice-Chair), Harrison Husting, Charlie Greenwood, Pablo Monsivais, Mike Bjordahl,
- **Board Members Not Present:**
- **Quorum Present:** Yes
- **Staff Members Present:** Colin Quinn-Hurst, Kara Mowery

## Public Comment:

- Sally Phillips (Spokane Bicycle Club) - Announced that the May Bike Everywhere month is coming up. This is an event put on by the Spokane Bicycle Club to encourage people to ride their bikes not only for recreation but also for transportation.
- **Jerry Compton (WSDOT Eastern Region)** - Washington Bike Walk and Roll Summit is coming in April. Registration is currently open online at [Cascade.org/summit](https://Cascade.org/summit).
- **Rhonda Young** - Transportation Student Club at Gonzaga is doing a ride and bike drive on May 2<sup>nd</sup>.

## Briefing Session:

Minutes from the January 21, 2020 meeting approved unanimously.

### 1. Liaison Report -

- Jessica Engelman reported that the sidewalk on North River Drive, and 57th and Hatch intersection projects are now included on the 6 Year Plan. Correction: North River Drive sidewalk was previously included on the 6 Year Capital Improvement Program.
- Rhonda Young reported that there are various transportation studies going on: reimagined Division, 195/I-90, and Grand Avenue study.
- Jessica Engelman reported that the PeTT Committee has not met but reported on Traffic Calming. The process for traffic calming is changing in 2021. Neighborhoods need to have their applications in by April 1st.

### 2. Chair Report - Grant Shipley

- None

### 3. Staff Report - Colin Quinn-Hurst

- Comprehensive Plan Amendment Process is starting. There are 11 proposed amendments to fine tune the bike plan. The amendments were discussed.

## Workshops:

### 1. Post Street Bridge Construction and Centennial Trail Detour

- Presentation provided by Colin Quinn-Hurst
- Questions asked and answered
- Discussion ensued

*\*\* Jessica Engelman motioned that on the Post Street Bridge, we would like to see a protected bike lane that continues south after the path veers off to the Centennial Trail for the use of bicycle traffic*

*Note: Minutes are summarized by staff. A recording of the meeting is on file with Planning Services.*



*trying to get to City Hall and Spokane Falls Blvd heading west. Motion Seconded. Motion passed unanimously\*\**

*\*\* Motion 2A: Jessica Engelman motioned that for the Post Street Bridge Detour we want to see a route that is physically protected, I would say the entire stretch, and will accommodate all ages and abilities. Specifically relating to the parking lot, we do not want to be routed through an active parking lot and the detour surface to be fully rideable, walkable, roll-able. Motion seconded. Motion Passed unanimously. \*\**

*\*\*Motion 2B: Jessica Engelman motioned that detours for walking, cycling, and micro-mobility traffic should be safe, comfortable, intuitive, and accessible for all ages and abilities. We at the BAB request city staff look into the issue of detours and create new language requiring closures of walking and cycling facilities include robust accommodations for those on foot, bike and other forms of micro mobility. Motion Seconded. Motion Passed Unanimously. \*\**

**Meeting Adjourned at 7:32 PM**

Next Bicycle Advisory Board Meeting scheduled for Tuesday, March 17, 2020



# Bicycle Advisory Board Minutes

June 16, 2020

Teleconference

Meeting Minutes: Meeting called to order at 6:05 PM by Grant Shipley

## Attendance:

- *Board Members Present:* Grant Shipley (Chair), Jessica Engelman (Vice-Chair), Charlie Greenwood, Pablo Monsivais, Rhonda Young, Jason Oestreicher, Mike Bjordahl, Taylor Stevens
- *Board Members Not Present:* Harrison Husting
- *Quorum Present:* Yes
- *Staff Members Present:* Colin Quinn-Hurst, Jackie Churchill

## Public Comment:

None

## Briefing Session:

Minutes from the April 21, 2020 meeting approved unanimously.

### 1. Liaison Report -

- NONE

### 2. Chair Report -

- None

### 3. Staff Report -

- Colin Quinn-Hurst gave a preview of the July BAB meeting topic in which staff from Integrated Capital Management will discuss how bike projects are added to the 6 Year Plan. They will look at factors such as grant criteria, the bike master plan, and location studies, and request feedback from the BAB.
- He reported on some Amendments to the Master Bike Plan including the Upriver Drive vacation and a bike lane connection along Garland and Cook. There is also updated language in Master Bike Plan changing the term Cycle Track to Protected Bike Lane and specifying that routes with planned bike lanes are eligible for protected bike lane designs pending additional evaluation.
- Colin talked about the Restaurant and Retail Expansion Program reporting that there are currently four applications to temporarily use public right-of-way next to restaurants to expand outside sitting.
- Colin reported that the Centennial Trail detour for the Post Street Bridge closure is being finalized. The Bosch Lot has been paved to create a better connection for that portion of the Centennial Trail detour.
- Wheel Share will be returning June 29th with bikes returning on July 13th.
- The City is gathering information on ways to address the Pandemic and Equity in Biking and is looking for feedback from the BAB.

## Workshops:

### 1. Fish Lake Trail Connection Study

- Presentation provided by Nathan Anunson
- Questions asked and answered
- Discussion ensued

*Note: Minutes are summarized by staff. A recording of the meeting is on file with Planning Services.*



## 2. Subcommittee Formation

- Presentation provided by Colin Quinn-Hurst
- Questions asked and answered
- Discussion ensued
  - Proposed Subcommittees: Street Design Standards, Active Transportation Responses: Equity and COVID-19, Protected Bike Lane/Mobility Lane Planning, Neighborhood Greenways planning, Bike Parking Code Update.

### **Meeting Adjourned at 7:30 PM**

Next Bicycle Advisory Board Meeting scheduled for Tuesday, July 21, 2020

Chat Record from meeting:

from Colin Quinn-Hurst to everyone:

from Colin Quinn-Hurst to everyone:

Feel free to move to approve/not approve here in the chat as well

from Jessica Engelman to everyone:

FYI my internet has been slow recently so I'm keeping my video off. I'm still here!

from Pablo Monsivais to everyone:

Charlie you're volume is very faint

from Jeff Sevela to everyone:

Agree on not xing 195. Also, WSDOT is looking at modifying 195 access to increase auto safety and can't plan anything now.

from Jessica Engelman to everyone:

Question: do you have an idea of what each route option would cost? Gov Way seems the cheapest, especially since the street is wider than traffic volumes warrant, and even the Riverside Bridge isn't that narrow (35' at its narrowest?) which at 10' lanes each direction (~2000 VPD) still provides 5' for a buffer and 10' for the facility itself.

from Shrinerdude to everyone:

Charlie, can you move closer to your mic?

from Pablo Monsivais to everyone:

Thanks for the presentation Nathan. Does the decision process take into account the neighborhoods/residential parcels served by the trail?

from Pablo Monsivais to everyone:

Follow-up question: So is this trail conceived of as a recreational trail mainly? or also intended to serve as a transportation corridor?

from Pablo Monsivais to everyone:

*Note: Minutes are summarized by staff. A recording of the meeting is on file with Planning Services.*



Wayfinding signage to links to other parts of the bike network? Lighting?

from Jeff Sevela to everyone:

Option 2 has advantage of allowing access for Inland Empire Way area and side path on east side of 195 to Cheney Spokane Rd (going through neighborhood)

from Jason Oestreicher to everyone:

I think Pablo is making a great point right now.

from Taylor Stevens to everyone:

Would Option 3 consider some opportunity to relate to the existing raised bikeway down Sunset Blvd?

from Pablo Monsivais to everyone:

Thank you Nathan. The more the "missing link" can also double as a safe, attractive corridor for western/southwestern neighborhoods into central Spokane the better.

from Jessica Engelman to everyone:

General statement: if the funds are coming out of separate pots that's one thing, but if primarily recreational cycling facilities and primarily transportation cycling facilities are competing for the same pile of funds, we should have a conversation about budgeting priorities.

from Jessica Engelman to everyone:

Thanks Nathan!

from Jessica Engelman to everyone:

is the Garland one an off-street path or in-street PBL?

from Jessica Engelman to everyone:

And is WSDOT paying for it?

from Jessica Engelman to everyone:

Question #2: is the PBL option also available for streets listed as shared facilities in the BMP?

from Shrinerdude to everyone:

Thank you for the explanation of details.

from Mike Bjordahl to everyone:

Were lane closures ever looked at downtown?

from Pablo Monsivais to everyone:

Everyone mute please

from Pablo Monsivais to everyone:

How is the construction traffic around the former Y building going to impact/intersect with this detour?

from Jason Oestreicher to everyone:

*Note: Minutes are summarized by staff. A recording of the meeting is on file with Planning Services.*



Is this detour going to be well signed? It has been confusing getting through Riverfront Park the last few years.

from Jessica Engelman to everyone:

any update on ped recall?

from Shrinerdude to everyone:

I am interested in tomorrow's webinar. Please send info.

from Pablo Monsivais to everyone:

I'm interested in 1, 3, 4.

from Jessica Engelman to everyone:

I'm already working on topics 1-4, so sign me up for all of them.

from Rhonda Young to everyone:

I'm interested in 1,2, or 3

from Shrinerdude to everyone:

I'm interested in 4 & 5

from Jason Oestreicher to everyone:

2,3,4 for me

from Mike Bjordahl to everyone:

I am interested in 4 or 5

from Taylor Stevens to everyone:

Interested in 4,5

from Jeff Sevela to everyone:

1,2,3 & 4

from Jessica Engelman to everyone:

Colin, when you send out emails, can you ask whether the people interested in that subcommittee would prefer daytime or evening meetings? Will make scheduling easier I think.

from Jessica Engelman to everyone:

why PBL on the south side for SFB?

from Pablo Monsivais to everyone:

Let me type

from Pablo Monsivais to everyone:



I am wondering if there is any news about infrastructure projects that may be changed or threatened because of budget shortfalls

from Pablo Monsivais to everyone:

Thanks

from Jessica Engelman to everyone:

I'd like to look at the SFB plans. Don't have to be part of a meeting necessarily, but I think most use will be Cincinnati-->Bridge/Downtown, so a north-side path would be better for that.

from Pablo Monsivais to everyone:

Thank you Colin.

from Rhonda Young to everyone:

I prefer daytime meetings these days

from Jessica Engelman to everyone:

Same

from Jessica Engelman to everyone:

Ha, I was hoping to talk six year plan

from Jessica Engelman to everyone:

in our remaining time

from Jessica Engelman to everyone:

since City Council vote is upcoming

from Pablo Monsivais to everyone:

A regular digest would be great.

from Rhonda Young to everyone:

Move to adjourn

from Pablo Monsivais to everyone:

Did we welcome Jason?

from Pablo Monsivais to everyone:

Welcome Jason!

from Shrinerdude to everyone:

second

from Jason Oestreicher to everyone:

Thanks everyone!

*Note: Minutes are summarized by staff. A recording of the meeting is on file with Planning Services.*



from Mike Bjordahl to everyone:

Thanks everyone!

from Pablo Monsivais to everyone:

Thank you everyone. Great to see your heads!

from Jessica Engelman to everyone:

Bye all, good seeing you again

from Taylor Stevens to everyone:

Thank you!



**Agenda Sheet for City Council Meeting of:**

11/16/2020

**Date Rec'd**

10/26/2020

**Clerk's File #**

ORD C35979

**Renews #****Submitting Dept**

PLANNING

**Cross Ref #****Contact Name/Phone**

KEVIN FREIBOTT 625-6184

**Project #**

Z20-042COMP

**Contact E-Mail**

KFREIBOTT@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

First Reading Ordinance

**Requisition #****Agenda Item Name**

0650 – COMPREHENSIVE PLAN AND SMC 12.080.040 AMENDMENT – ARTERIAL MAP

**Agenda Wording**

Ordinance relating to proposal Z20-042COMP by the City, amending the Comprehensive Plan Arterial Network Map in various locations throughout the city and amending the Official Arterial Street Map in SMC 12.080.040 accordingly.

**Summary (Background)**

The proposal concerns amendments to various streets in Map TR-12 throughout the City and concurrent amendment to the Official Arterial Street Map in SMC 12.08.040. This is being considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. Public participation and notification requirements are complete. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended approval of the amendment.

**Fiscal Impact**

Grant related? NO

Public Works? NO

**Budget Account**

Neutral \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

MEULER, LOUIS

**Study Session\Other**

CC Study Session 10-29-20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

CM Mumm

**Finance**

ORLOB, KIMBERLY

**Distribution List****Legal**

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tblack@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

jrichman@spokanecity.org

**Additional Approvals**

sbishop@spokanecity.org

**Purchasing**

dhume@spokane-landuse.com

lmeuler@spokanecity.org





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



Ordinance No. C35979

AN ORDINANCE RELATING TO PROPOSAL FILE Z20-042COMP AMENDING COMPREHENSIVE PLAN MAP TR-12, ARTERIAL NETWORK MAP, IN VARIOUS LOCATIONS THROUGHOUT THE CITY TOGETHER WITH CORRESPONDING CHANGES TO THE OFFICIAL ARTERIAL STREET MAP IN SMC 12.08.040.

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, Proposal File Z20-042COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Proposal Z20-042COMP seeks to amend Comprehensive Plan Map TR-12, Arterial Network Map, in various locations throughout the City; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Proposal Z20-042COMP reviewed all the criteria relevant to consideration of the proposal was published on August 25, 2020 and sent to all applicants and the Plan Commission; and

WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the proposal was published on August 26, 2020 and September 2, 2020; and



WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted online and with the Spokesman Review, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing on September 9, 2020, continued to September 23, 2020, during which they took public testimony on this proposal and deliberated this and all other Comprehensive Plan Amendment applications; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-042COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-042COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend approval of Proposal Z20-042COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Proposal. Proposal Z20-042COMP is approved.
2. Amendment of the Arterial Network Map. The Spokane Comprehensive Plan Map TR-12, Arterial Network Map, is amended as shown in Exhibit A.
3. Amendment of the Official Arterial Street Map. The Official Arterial Street map delineated in Spokane Municipal Code (SMC) 12.08.040 is amended as follows, and shown in Exhibit B:
  - a. Change the designation of Walnut Street from 4<sup>th</sup> Avenue to 5<sup>th</sup> Avenue to “Urban Principal Arterial.”
  - b. Remove the vacated Upriver Drive from N Center Street to Mission Avenue from the map.
  - c. Change the designation of Main Avenue from Napa Street to Altamont Street to “Urban Minor Collector.”
  - d. Change the designation of Altamont Street from Main Avenue to Sprague Avenue to “Urban Minor Collector.”



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date



# 2020 Comprehensive Plan Amendment

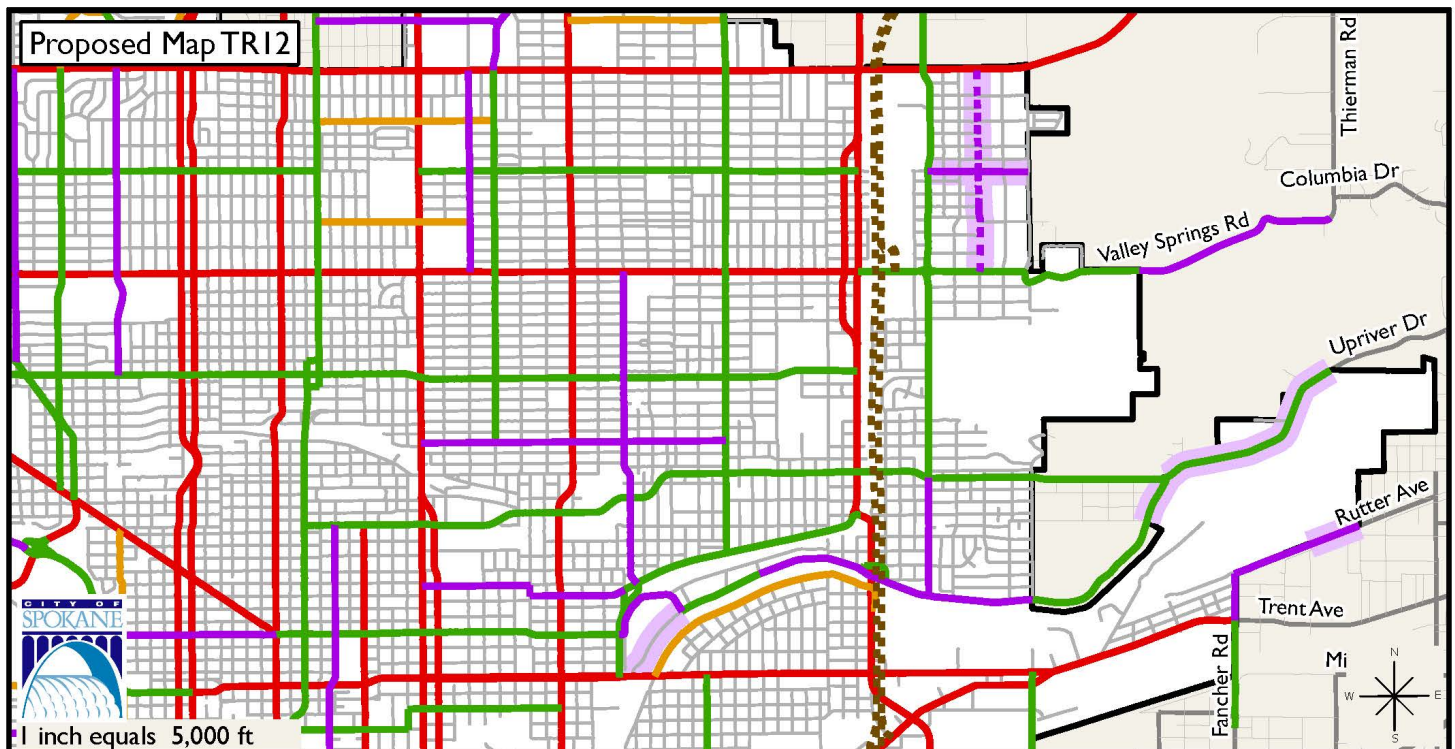
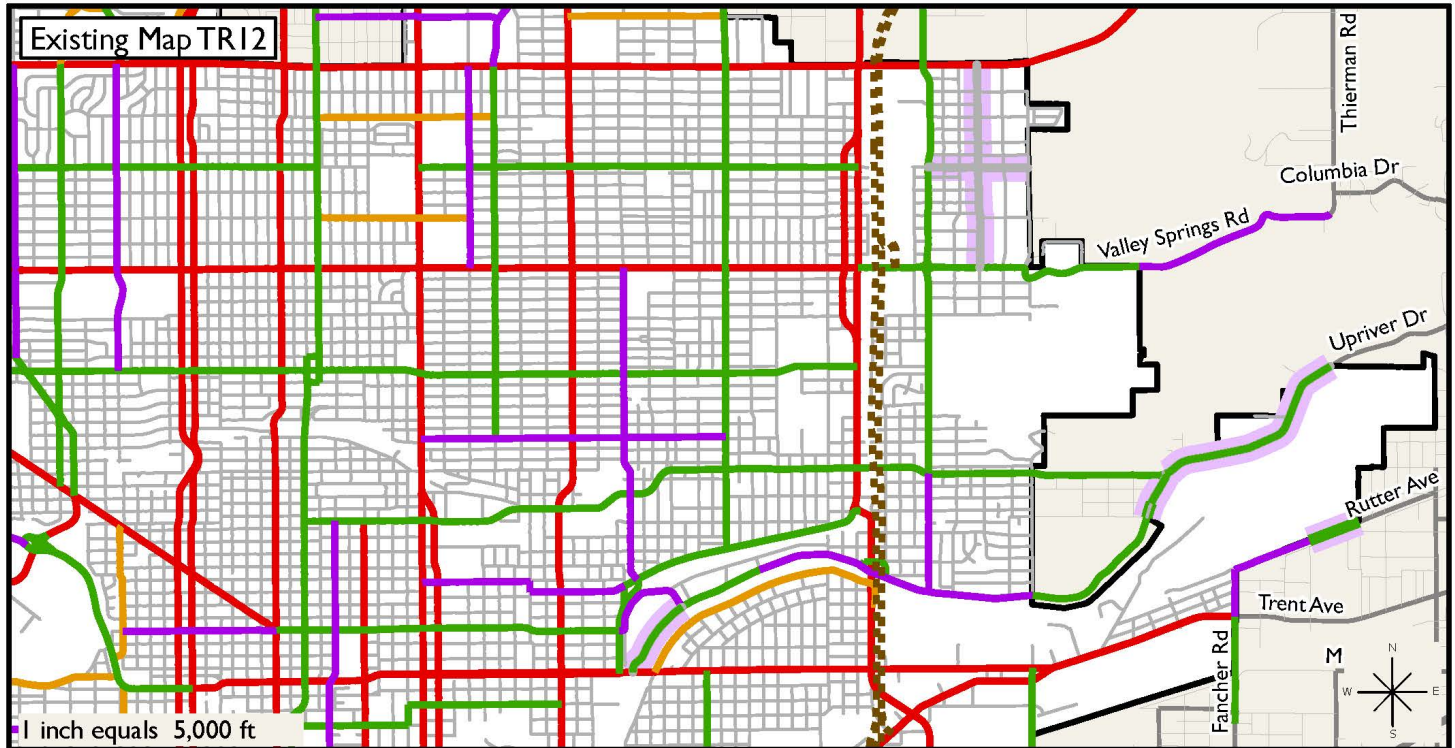
Change Request:  
East

Revised  
Date: July 2020

## EXHIBIT A

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |





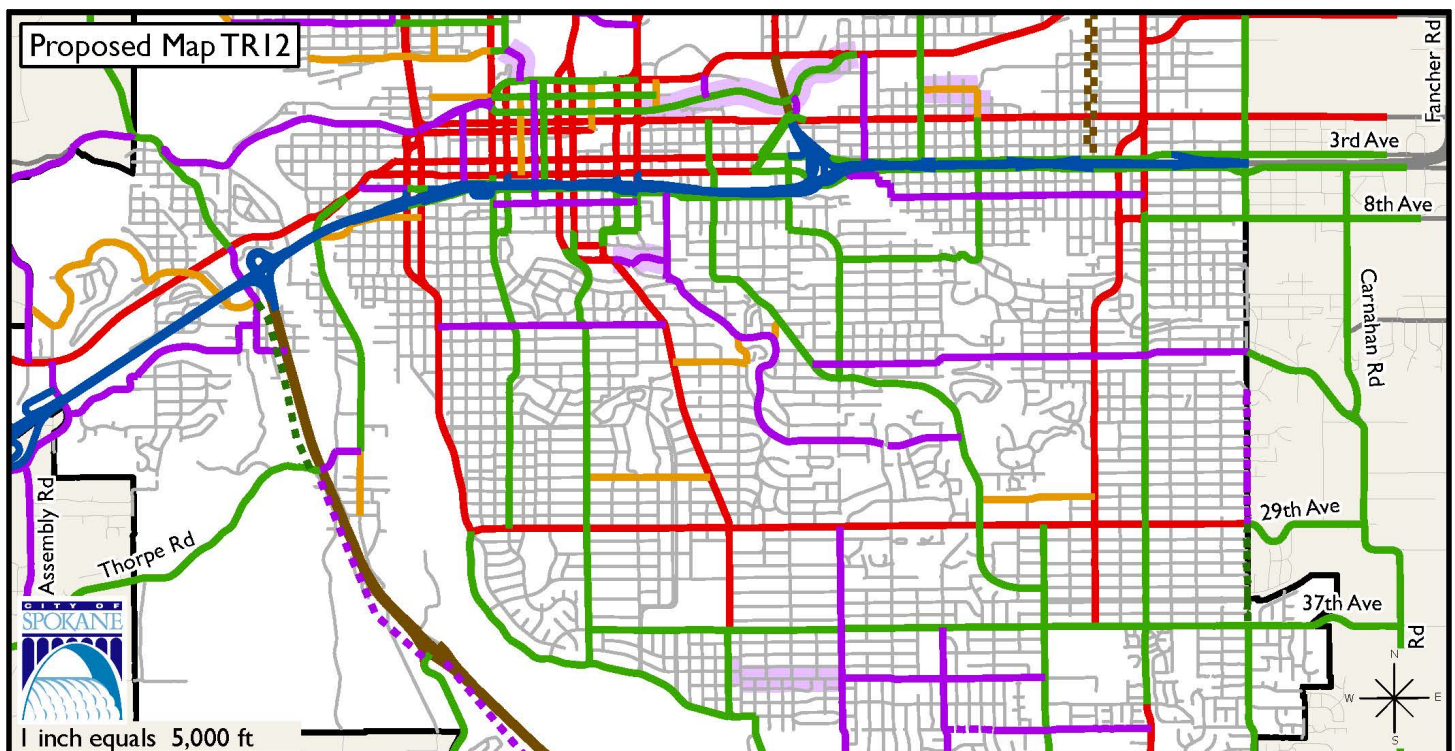
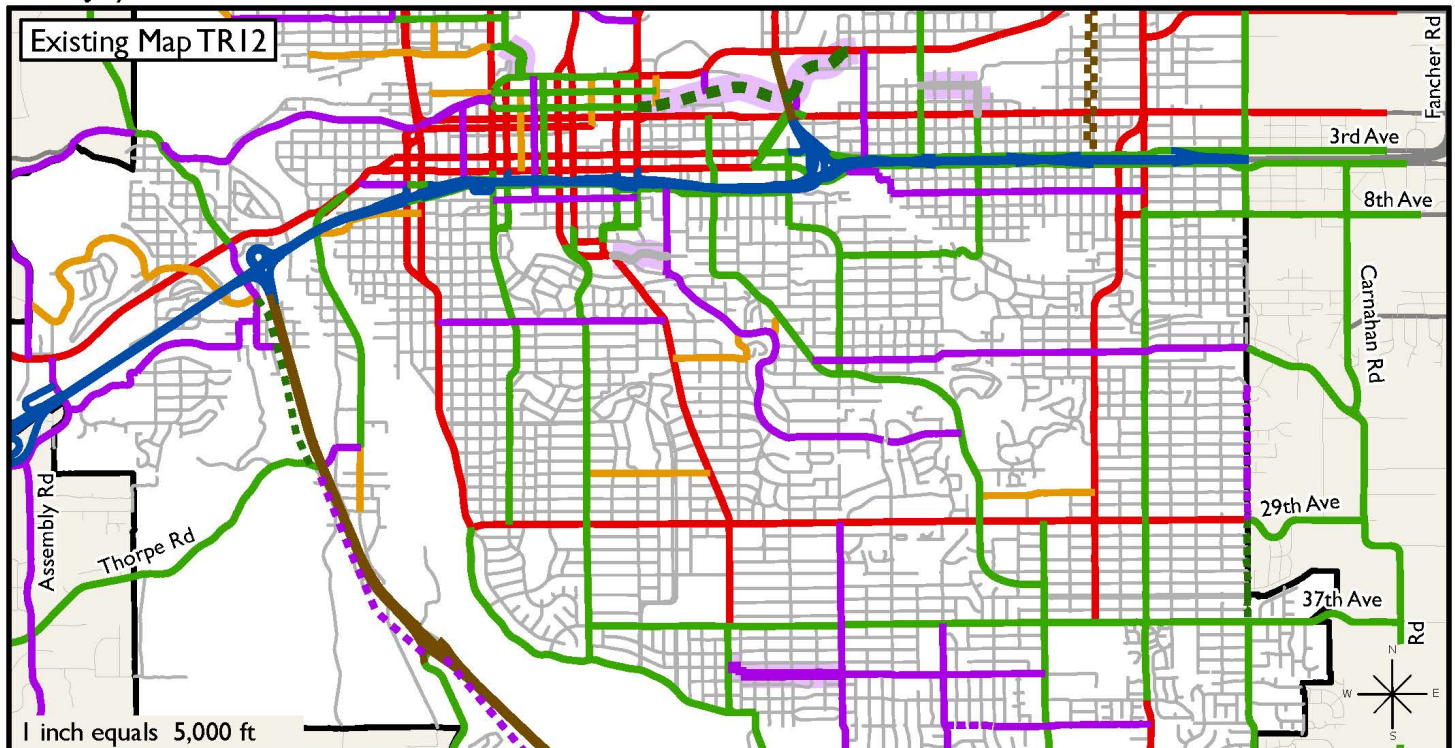
# 2020 Comprehensive Plan Amendment

Change Request:  
South

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020





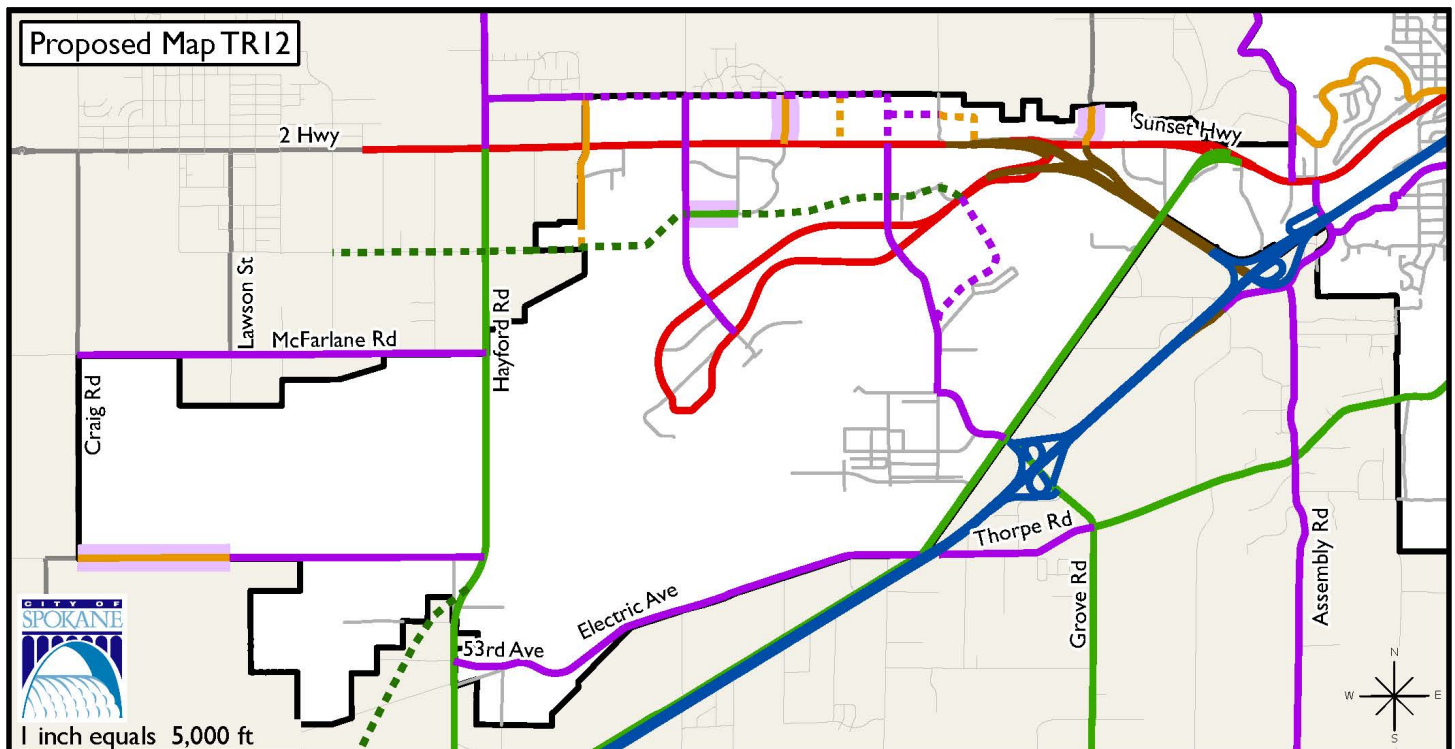
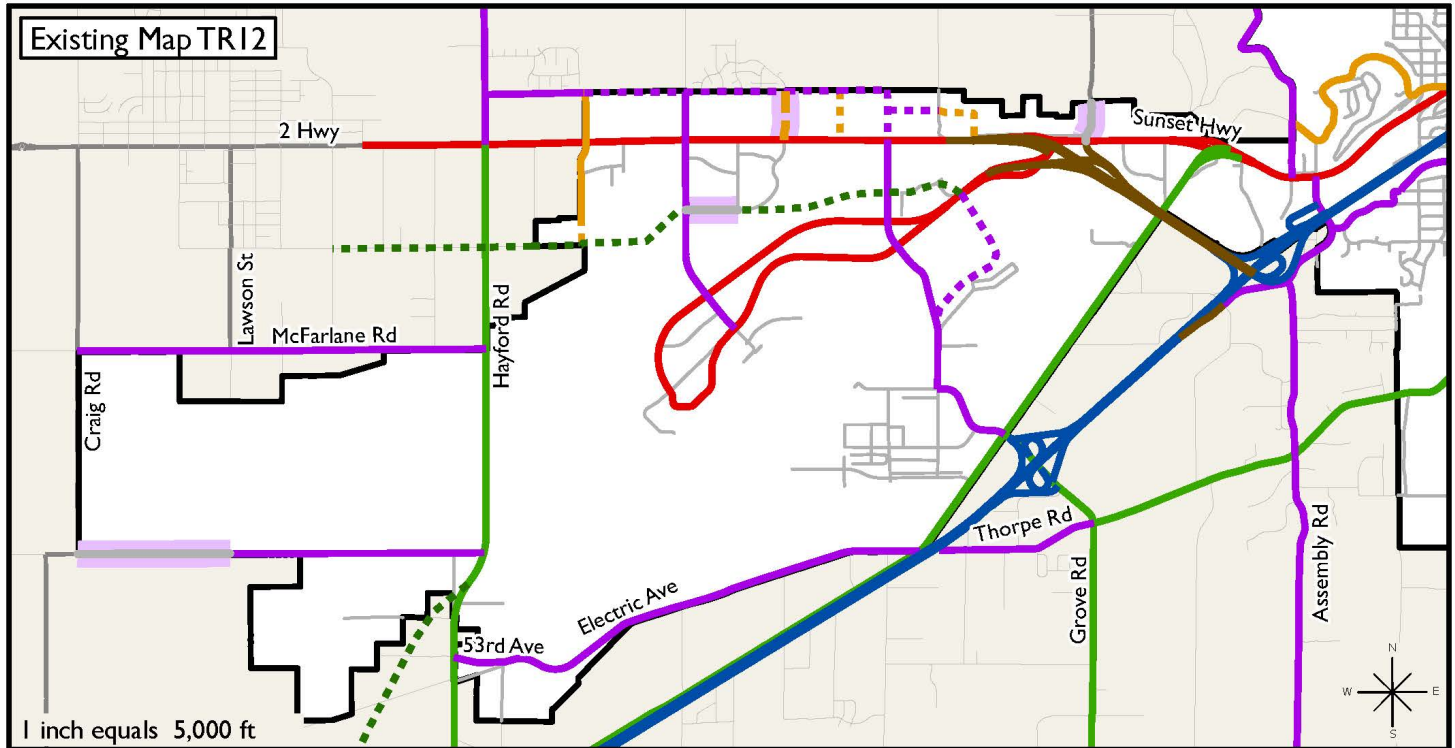
# 2020 Comprehensive Plan Amendment

Change Request:  
West Plains

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020





**STAFF REPORT Z20-042COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed amendment to the City's Comprehensive Plan. The proposal is to amend the Arterial Network Map (Map TR-12) in Chapter 4 (Transportation) of the City's Comprehensive Plan. This amendment is proposed to modify the classification of several arterial streets. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPOSAL SUMMARY**

| Street                              | Limits                       | Current Classification         | Proposed Classification        |
|-------------------------------------|------------------------------|--------------------------------|--------------------------------|
| Thorpe Rd                           | Craig to Lawson              | local                          | Urban Minor Collector          |
| Granite Road (18 <sup>th</sup> Ave) | Flint to Campus              | Local                          | Urban Minor Arterial           |
| Campus Road                         | 12 <sup>th</sup> Ave to US 2 | Proposed Urban Minor Collector | Urban Minor Collector          |
| Grove Road                          | City limit to Sunset Hwy     | Local                          | Urban Minor Collector          |
| Post Street Bridge                  | Summit Pkwy to SFB           | Urban Minor Arterial           | Urban Major Collector          |
| 9 <sup>th</sup> Ave - Rockwood      | Grand to Cowley              | Local                          | Urban Major Collector          |
| Martin Luther King Jr. Way          | Division to Trent            | Proposed Urban Minor Arterial  | Urban Minor Arterial           |
| Thurston Ave                        | Grand to Perry               | Urban Major Collector          | local                          |
| Rutter Parkway                      | Eastern to Bradley           | Urban Minor Arterial           | Urban Major Collector          |
| Upriver Drive                       | N. Center to Mission         | Urban Minor Arterial           | local                          |
| Upriver Drive                       | Buckeye to east city limit   | local                          | Urban Minor Arterial           |
| Barnes Road                         | Phoebe to Strong             | Proposed Urban Major Collector | Urban Major Collector          |
| Rowan Avenue                        | Freya to Havana              | Local                          | Urban Major Collector          |
| Myrtle Street                       | Francis to Wellesley         | Local                          | Proposed Urban Major Collector |
| Main Avenue                         | Napa to Altamont             | Local                          | Urban Minor Collector          |
| Altamont Street                     | Main to Sprague              | Local                          | Urban Minor Collector          |



## II. APPLICANT SUMMARY

|                             |                 |
|-----------------------------|-----------------|
| <b>City Representative:</b> | Inga Note       |
| <b>Applicant:</b>           | City of Spokane |
| <b>Property Owner:</b>      | City of Spokane |

## III. PROPOSAL SUMMARY

|                                      |                                                                                                                                            |
|--------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Location of Proposal:</b>         | City rights-of-way                                                                                                                         |
| <b>SEPA Status:</b>                  | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 7, 2020. |
| <b>Plan Commission Hearing Date:</b> | September 9, 2020.                                                                                                                         |
| <b>Staff Contact:</b>                | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                             |
| <b>Staff Recommendation:</b>         | Recommended                                                                                                                                |

## IV. BACKGROUND INFORMATION

- General Proposal Description:** Pursuant to the procedures established by SMC 17G.020, enabled by RCW 36.70A.130, the City proposes a change to the Arterial Network Map (Map TR-12) in Chapter 4 (Transportation) of the City's Comprehensive Plan, as described in section 'I' above.
- Site Description and Physical Conditions:** The proposal concerns sixteen street segments throughout the city. All are paved with the exception of Myrtle Street and parts of Rowan Avenue.
- Property Ownership:** City right-of-way
- Adjacent Property Improvements and Uses:** Adjacent property uses vary throughout the city including industrial, residential and commercial.

| Street                              | Limits                       | Adjacent Land Use                                                       |
|-------------------------------------|------------------------------|-------------------------------------------------------------------------|
| Thorpe Rd                           | Craig to Lawson              | Light Industrial                                                        |
| Granite Road (18 <sup>th</sup> Ave) | Flint to Campus              | Light Industrial                                                        |
| Campus Road                         | 12 <sup>th</sup> Ave to US 2 | Light Industrial                                                        |
| Grove Road                          | City limit to Sunset Hwy     | Light Industrial, Low Density Residential                               |
| Post Street Bridge                  | Summit Pkwy to SFB           | Downtown, Open Space                                                    |
| 9 <sup>th</sup> Ave - Rockwood      | Grand to Cowley              | Residential High Density, Community Business, Office, Residential 10-20 |
| Martin Luther King Jr. Way          | Division to Trent            | Institutional, Downtown, Heavy Industrial, General Commercial           |



|                 |                            |                                       |
|-----------------|----------------------------|---------------------------------------|
| Thurston Ave    | Grand to Perry             | Residential 4-10                      |
| Rutter Parkway  | Eastern to Bradley         | Light Industrial, General Commercial  |
| Upriver Drive   | N. Center to Mission       | Light Industrial                      |
| Upriver Drive   | Buckeye to east city limit | Residential 4-10                      |
| Barnes Road     | Phoebe to Strong           | Residential 4-10                      |
| Rowan Avenue    | Freya to Havana            | Heavy and Light Industrial            |
| Myrtle Street   | Francis to Wellesley       | Heavy and Light Industrial            |
| Main Avenue     | Napa to Altamont           | Light Industrial                      |
| Altamont Street | Main to Sprague            | Light Industrial, Center and Corridor |

## 5. Street Class Designations:

| Street                              | Limits                       | Classification on TR 12 Map    | New Classification             | Reason                               |
|-------------------------------------|------------------------------|--------------------------------|--------------------------------|--------------------------------------|
| Thorpe Rd                           | Craig to Lawson              | local                          | Urban Minor Collector          | Error correction                     |
| Granite Road (18 <sup>th</sup> Ave) | Flint to Campus              | Local                          | Urban Minor Arterial           | Consistency with SMC 12.08.040       |
| Campus Road                         | 12 <sup>th</sup> Ave to US 2 | Proposed Urban Minor Collector | Urban Minor Collector          | constructed                          |
| Grove Road                          | City limit to Sunset Hwy     | Local                          | Urban Minor Collector          | Error correction                     |
| Post Street Bridge                  | Summit Pkwy to SFB           | Urban Minor Arterial           | Urban Major Collector          | Consistency with SMC 12.08.040       |
| 9 <sup>th</sup> Ave - Rockwood      | Grand to Cowley              | Local                          | Urban Major Collector          | Error correction                     |
| Martin Luther King Jr. Way          | Division to Trent            | Proposed Urban Minor Arterial  | Urban Minor Arterial           | constructed                          |
| Thurston Ave                        | Grand to Perry               | Urban Major Collector          | local                          | Consistency with SMC 12.08.040       |
| Rutter Parkway                      | Eastern to Bradley           | Urban Minor Arterial           | Urban Major Collector          | Error correction                     |
| Upriver Drive                       | N. Center to Mission         | Urban Minor Arterial           | local                          | Vacated by Ordinance C35824          |
| Upriver Drive                       | Buckeye to east city limit   | local                          | Urban Minor Arterial           | Error correction                     |
| Barnes Road                         | Phoebe to Strong             | Proposed Urban Major Collector | Urban Major Collector          | constructed                          |
| Rowan Avenue                        | Freya to Havana              | Local                          | Urban Major Collector          | Increased volume, future development |
| Myrtle Street                       | Francis to Wellesley         | Local                          | Proposed Urban Major Collector | Future development                   |
| Main Avenue                         | Napa to Altamont             | Local                          | Urban Minor Collector          | Increased volume                     |
| Altamont Street                     | Main to Sprague              | Local                          | Urban Minor Collector          | Increased volume                     |



6. **Current Land Use Designation and History:** Adjacent land uses are shown in the table above.

7. **Proposed Land Use Designation:** n/a

8. **Current Zoning and History:** n/a

9. **Proposed Zoning:** n/a

## V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |
| Notice of Application Posted .....                            | June 8, 2020      |
| Plan Commission Workshop .....                                | June 24, 2020     |
| Public Workshop .....                                         | July 29, 2020     |
| 60-Day Public Comment Period Ended .....                      | August 7, 2020    |
| SEPA Determination Issued .....                               | August 24, 2020   |
| Notice of Public Hearing Posted .....                         | August 26, 2020   |
| Plan Commission Hearing Date (Scheduled) .....                | September 9, 2020 |

**Comments Received:** A request for comments was issued to City departments, local agencies, and departments within 400 feet of the proposal, along with pertinent application details on April 24, 2020. Project changes made during the public comment period necessitated two revised requests for agency/department comment, one on June 9, 2020 and one on July 28, 2020. By the close of agency comment on May 11, comments were received from the following:

- Randy Abrahamson, Spokane Tribe Historic Preservation Officer
- Bobby Halbig, City of Spokane Streets Department

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



Mr. Abrahamson indicated a low potential for cultural resources along the identified routes, and asked that any future project development include an Inadvertent Discovery Plan. Mr. Halbig indicated that the Streets Department had no direct comments regarding the proposal.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also in the Spokesman Review. No public comments were received during the 60-day public comment period.

2. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 24th, 2020 during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. The project representative, Inga Note, was provided an opportunity to speak during the workshop but no public comment was taken.

## VI. APPLICATION REVIEW AND ANALYSIS

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:

- A. Keep the comprehensive plan alive and responsive to the community.
- B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
- C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
- D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
- E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
- F. Amendments to the comprehensive plan must result in a net benefit to the general public.

2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state,



or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, “Planning Goals”), and these goals guided the City’s development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. Financing:** *In keeping with the GMA’s requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: There will be no immediate impact to the city budget. The only immediate physical change to any of the streets would be installation of stop signs on Main Avenue, which can be handled within the Streets Department maintenance budget.

The proposal meets this criterion.

- D. Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

- E. Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

Staff Analysis: The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-



project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal until the City seeks grant funding for construction.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in **Exhibit A** of this report. Further discussion of these policies is provided under section K.2 below.

The proposal meets this criterion.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

Staff Analysis: The proposal is generally consistent with current Comprehensive Plan policies, as described in further detail in the staff analysis of Criterion K.2 below and other criteria in this report. Therefore, no amendment to policy wording is necessary and this criterion does not apply to the subject proposal.

The proposal meets this criterion.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** The proposed text amendment is consistent with the railroad and roadway elements of the regional transportation plan.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*
2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor



do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

1. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

Staff Analysis: The proposal would not impact the City's ability to provide transportation facilities at the planned level of service.

The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

---

<sup>3</sup> State Environmental Protection Act



The proposal meets this criterion.

**K. Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The proposal does not include a policy adjustment, thus this criteria does not apply.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: The designation of arterial classifications like those in Map TR-12 have been prepared according to the requirements of Comprehensive Plan policies listed in **Exhibit E**.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: As this is a non-project proposal, the physical characteristics of the various road alignments will be analyzed for their physical limitations if and when future improvements are considered.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: Consistent and periodic update of the arterial designations in the Comprehensive Plan allow for the document to adjust over time to up to date conditions and requirements, and to allow for the implementation of the comprehensive plan to be dynamic and responsive.

The proposal meets the criterion.

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears to comply with the considerations for a comprehensive plan amendment as provided in SMC 17G.020.030.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to



make a recommendation to City Council for approval or denial of the requested amendment to Chapter 4 of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Staff recommends that the Plan Commission and City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                        |                                           |
|----------------------------------------|-------------------------------------------|
| A. Maps of Proposed Amendments         | D. SEPA Checklist                         |
| B. List of Relevant Comp Plan Policies | E. SEPA Determination of Non-Significance |
| C. Application Materials               | F. Agency Comments                        |



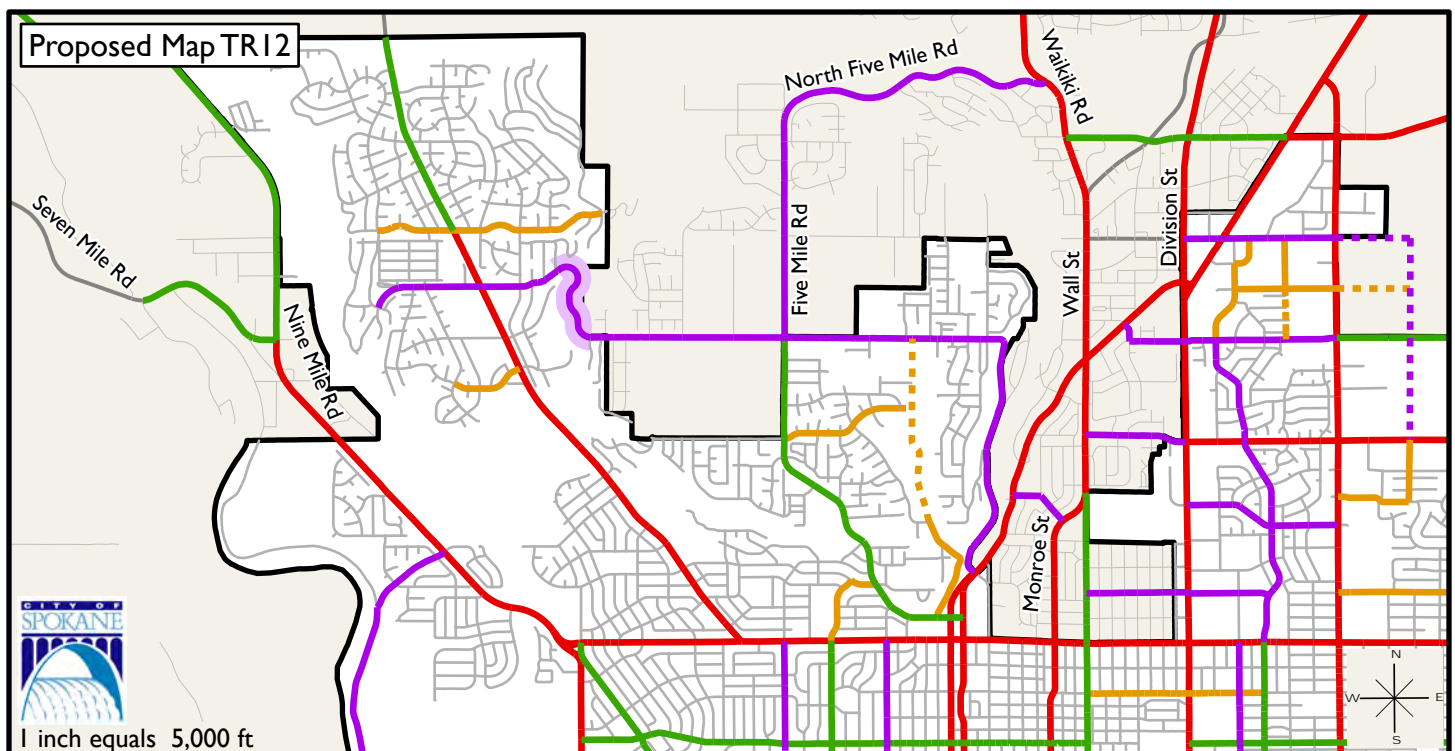
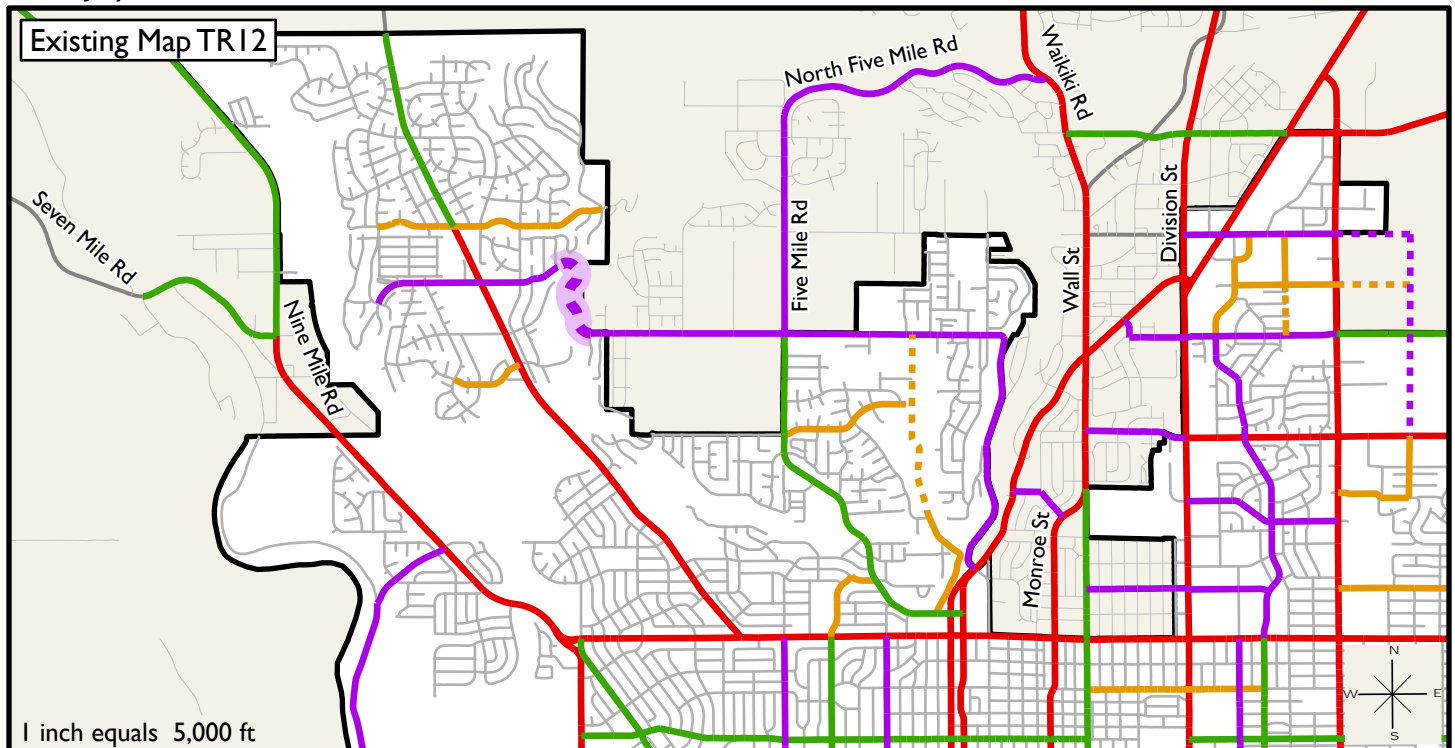
# 2020 Comprehensive Plan Amendment

Change Request:  
North

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020





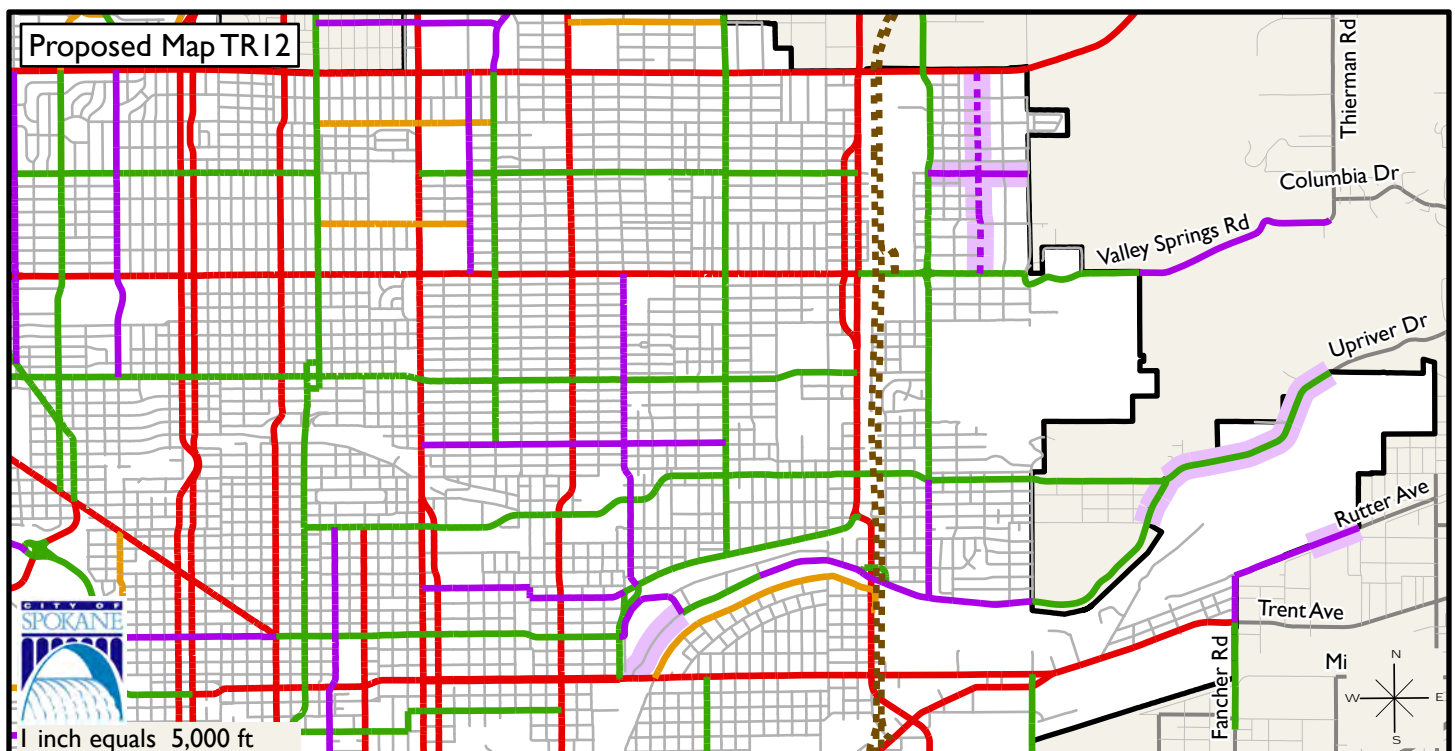
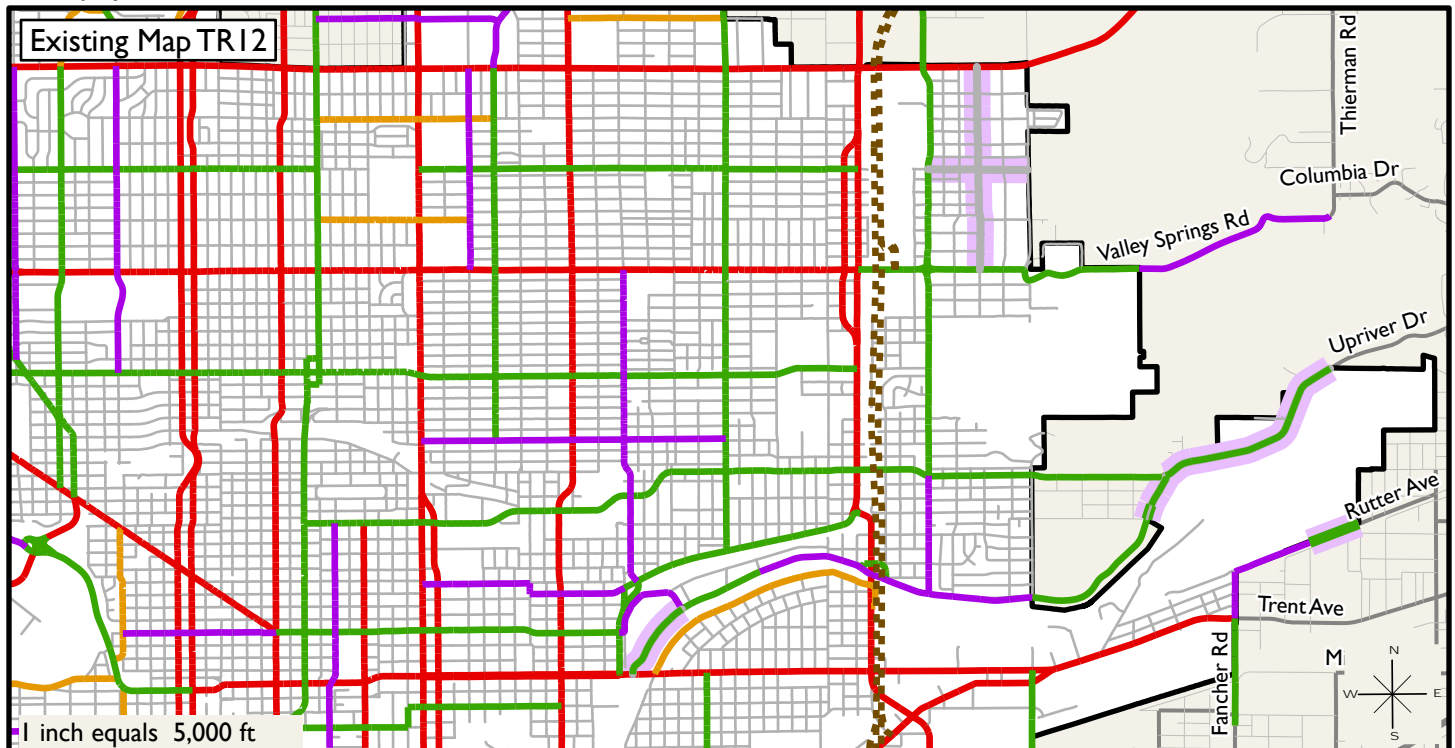
# 2020 Comprehensive Plan Amendment

Change Request:  
East

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020





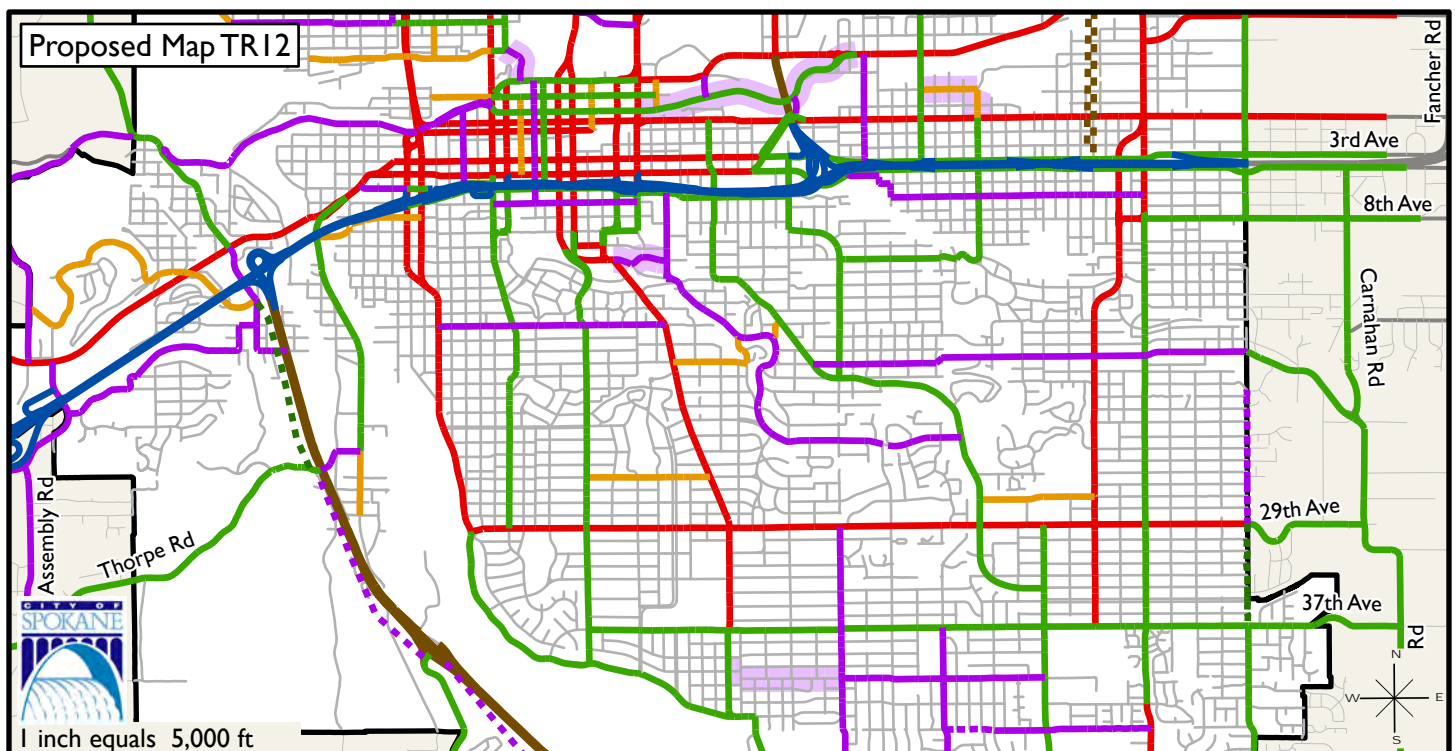
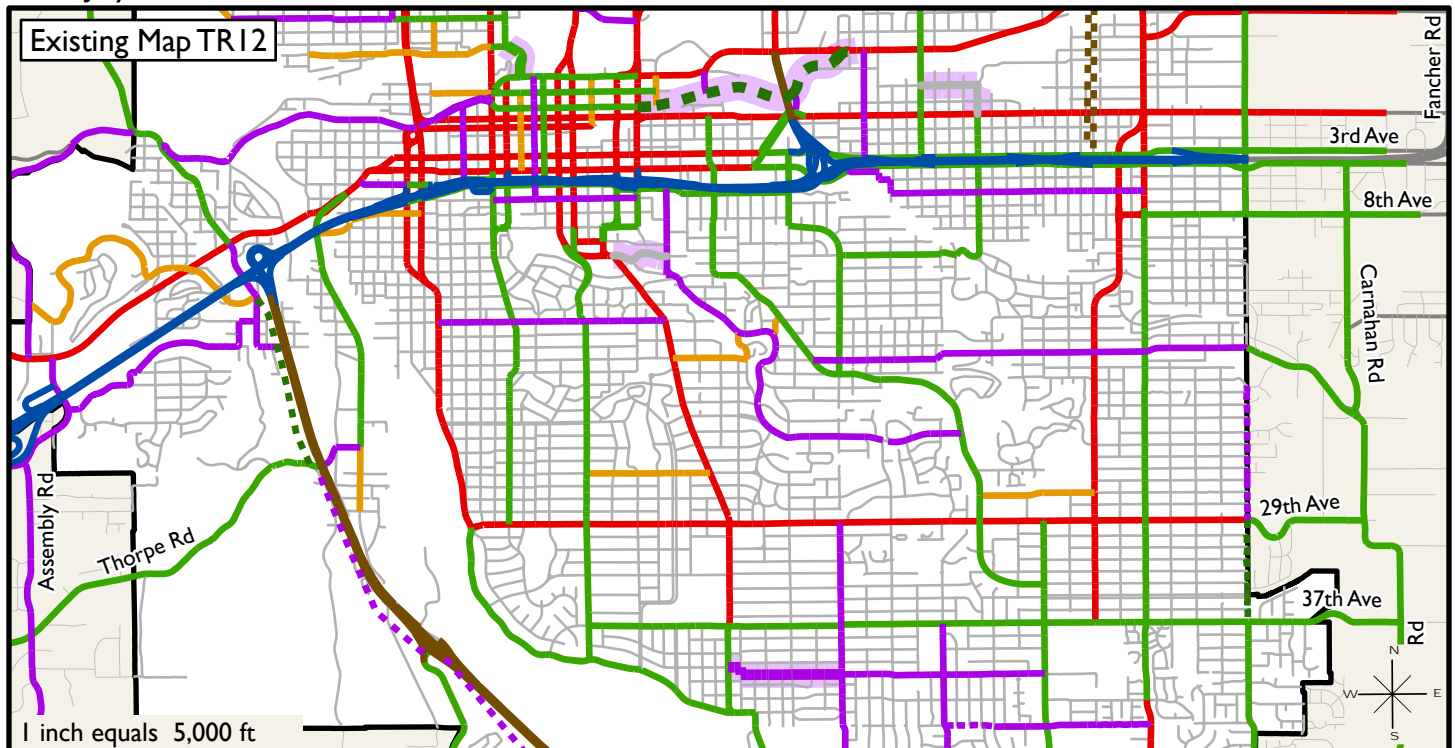
# 2020 Comprehensive Plan Amendment

Change Request:  
South

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020





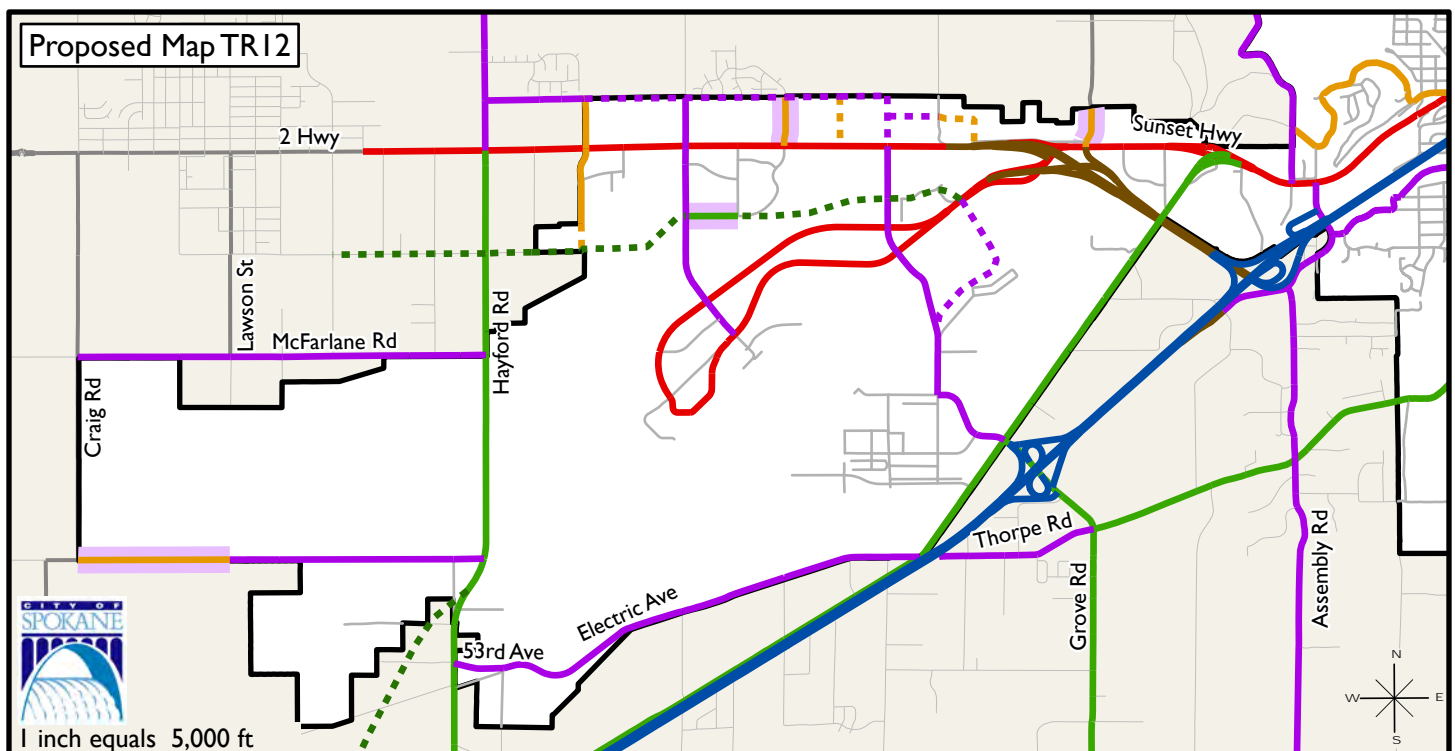
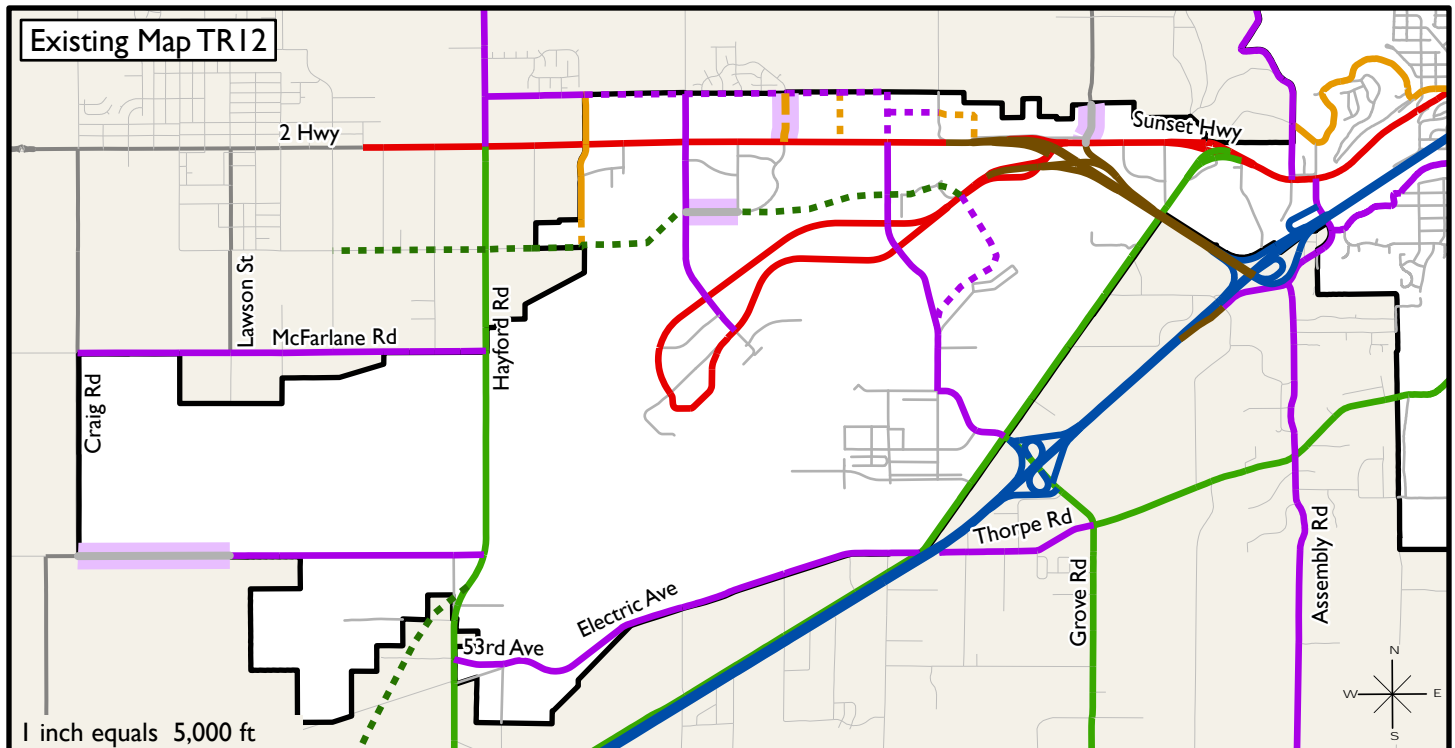
# 2020 Comprehensive Plan Amendment

Change Request:  
West Plains

## Legend

- |                                               |                          |
|-----------------------------------------------|--------------------------|
| Proposed Urban Other Freeways and Expressways | Urban Interstate         |
| Proposed Urban Principal Arterial             | Urban Principal Arterial |
| Proposed Urban Minor Arterial                 | Urban Minor Arterial     |
| Proposed Urban Major Collector                | Urban Major Collector    |
| Proposed Urban Minor Collector                | Urban Minor Collector    |
| Urban Other Freeways and Expressways          | Urban Local Access       |

Revised  
Date: July 2020







The following policies of the Comprehensive Plan relate to application Z20-042COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

### **Chapter 3—Land Use**

#### LU 4.3 Neighborhood Through-Traffic

Create boundaries for new neighborhoods through which principal arterials should not pass.

*Discussion:* Principal arterials that bisect neighborhoods create undesirable barriers to pedestrian circulation and adversely impact adjoining residences. Whenever possible, principal arterials should be located on the outer edge of neighborhoods.

#### LU 4.4 Connections

Form a well-connected network which provides safe, direct and convenient access for all users, including pedestrians, bicycles, and automobiles, through site design for new development and redevelopment.

#### LU 4.5 Block Length

Create a network of streets that is generally laid out in a grid pattern that features more street intersections and shorter block lengths in order to increase street connectivity and access.

*Discussion:* Excessively long blocks and long local access residential streets result in fewer alternative routes for pedestrian and vehicle travel and generally result in increased vehicle speeds. A grid pattern featuring more street intersections and shorter blocks provides more alternative routes for pedestrian and vehicle travel and tends to slow traffic. Block lengths of approximately 250 to 350 feet on average are preferable, but should not exceed 660 feet in length (per Spokane Municipal Code). Environmental conditions such as topography or rock outcroppings might constrain these shorter block lengths in some areas.

### **Chapter 4—Transportation**

#### TR 2 Transportation Supporting Land Use

Maintain an interconnected system of facilities that allows travel on multiple routes by multiple modes, balancing access, mobility and place-making functions with consideration and alignment with the existing and planned land use context of each corridor and major street segment.

##### *Key Actions:*

- a. Establish and maintain Street Design Standards and Guidelines reflecting best practices to implement designs that effectively support multi-modal transportation while supporting local context and existing and planned land uses.
- b. Develop transportation decisions, strategies and investments in coordination with land use goals that support the Land Use Plan and Center and Corridor strategy.



- c. Require a transportation plan (which includes connectivity and circulation) as part of any subdivision, Planned Unit Development (PUD), institutional master plan, or other major land use decision – Conduct transportation plans when needed for larger developments or other land uses of appropriate size.

#### TR 5 Active Transportation

Identify high-priority active transportation projects to carry on completion/ upgrades to the active transportation network.

##### *Key Actions*

- a. Ensure that the pedestrian and bicycle networks provide direct connections between major activity centers and transit stops and stations.
- b. The planning, design and construction of transportation projects should maintain or improve the accessibility and quality of existing and planned pedestrian and bicycle facilities.
- c. Implement a network of low vehicle volume, bike-friendly routes throughout the city.
- d. Support the development of a bike-share program within the city core.
- e. Seek grant funding for projects and programs such as Safe Routes to School, Transportation Alternatives, and other active transportation initiatives.
- f. Utilize the Bicycle Plan and the Pedestrian Plan to guide the location and type of bicycle and pedestrian facilities developed in Spokane to:
  - i. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages to transit stops and stations.
  - ii. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages between major activity areas where features that act as barriers prevent safe and convenient access.
  - iii. Provide safe, attractive, convenient and quality pedestrian and bicycle facilities and an aesthetically pleasing environment on bridges.
  - iv. Enhance the pedestrian and bicycle environment along routes to schools to provide a safe walking and riding environment for children. Means of accomplishing this include:
    - encouraging school routes not to cross arterials;
    - having user-activated signals at arterial intersections;
    - implementing safety patrols with traffic-control signs at busy intersections;
    - working with schools to promote walking groups; and
    - strengthening and enforcing pedestrian right-of-way laws.
  - v. Enhance the pedestrian, bicycle and transit environment along routes to desirable destinations for seniors.



- vi. Enhance the pedestrian, bicycle and transit environment along routes in communities with a high percentage of underserved populations.
- vii. Provide safe bicycle and pedestrian access to city parks from surrounding neighborhoods.
- g. Provide viable facilities for active transportation modes as alternatives to driving.
  - i. Ensure gaps in the bicycle network are identified and prioritized to complete and expand the connected bicycle network.
  - ii. Ensure sidewalk gaps are not present and provide for safe pedestrian circulation within the city. Wherever possible, this should be in the form of sidewalks with a pedestrian buffer strip or other separation from the street.
  - iii. Use pedestrian safety strategies on high bicycle and pedestrian traffic corridors.
  - iv. Establish and maintain crosswalks at key locations where active transportation facilities cross collector and arterial roadways.
- h. h. Provide secure parking for bicyclists at key destinations (i.e. Downtown, identified Centers and Corridors, schools and universities, community centers, key transit locations) and ensure future developments include bicycle parking on site that adheres to city-established design and siting standards.
- i. Work with local and regional partners to implement the “Spokane County Wayfinding and Gateway Feature Placement & Design Plan”.
- j. Coordinate with other departments and partner agencies to combine related projects for the purpose of cost-sharing.

#### TR 8 Moving Freight

Identify a freight network that respects needs of businesses as well as neighborhoods. Maintain an appropriate arterial system map that designates a freight network that enhances freight mobility and operational efficiencies, and increases the city’s economic health. The needs for delivery and collection of goods at businesses by truck should be incorporated into the freight network, and the national trend of increased deliveries to residences anticipated.

##### *Key Actions*

- a. Designate truck freight routes through the city that provide appropriate access without compromising neighborhood safety and livability.
- b. Periodically work with commercial freight mapping services to update their truck route information.
- c. Provide an easy to find freight map on the city’s website.
- d. Explore establishing delivery time designations/restrictions in specified areas.



#### TR 10 Transportation System Efficiency & Innovation

Develop and manage the transportation system to function as efficiently as possible while exploring innovative opportunities and technologies.

##### *Key Actions*

- a. Develop Access Management Strategies for arterials.
- b. Ensure coordinated, efficient and safe movement of all roadway users through proper signal spacing traffic control timing, and other intersection controls such as roundabouts and new traffic control coordinating technology where appropriate.
- c. Implement Intelligent Transportation System (ITS) improvements as identified by the Spokane Regional Transportation Management Center (SRTMC).
- d. Work with WSDOT to implement TDM, ITS, and transportation system management strategies developed through the Corridor Sketch Initiative (CSI).

#### TR 12 Prioritize & Integrate Investments

Prioritize investments based on the adopted goals and priorities outlined in the comprehensive plan.

##### *Key Actions:*

- a. Maintain and update as needed the metrics tied to the long range transportation prioritization matrix used to help determine transportation system capital investments.
- b. Link transportation investments with investments made under the Integrated Clean Water Plan to manage stormwater and wastewater.
- c. Utilize a least-cost planning approach in prioritizing and integrating the city's investments in infrastructure.

#### TR 19 Plan Collaboratively

Work with partner agencies to achieve a regional transportation plan that meets the goals and requirements of the Growth Management Act (GMA) but also reflects the visions and values of the City of Spokane.

##### *Key Actions:*

- a. Coordinate with SRTC and neighboring jurisdictions on transportation planning, projects and policies to ensure efficient, multi-modal transportation of people and goods between communities regionally. City of Spokane Comprehensive Plan 4-30
- b. Coordinate the setting and maintaining of transportation level of service standards with other agencies and private providers of transportation to ensure coordination and consistency when possible.
- c. Coordinate with WSDOT in areas where Highways of Statewide Significance (HSS) intersect/impact the local roadway network.



- d. Use the adopted Countywide Planning Policies (CWPP) as additional guidance for transportation planning.
- e. Protect the operations of Fairchild Air Force Base, Spokane International Airport and Felts Field with compatible land use regulations and ensure planning is coordinated and consistent with the airfields' respective Master Plans.
- f. Share information between transportation entities on a regular basis and during appropriate phases of projects and comprehensive plan updates and amendments.
- g. Coordinate with Spokane Transit Authority to ensure and support an efficient transit system.





# General Application

Rev.20180104

## DESCRIPTION OF PROPOSAL

Map amendments to the Arterial Network Map TR 12 in order to correct  
errors and omissions.

Address of Site Proposal (if not yet assigned, obtain address from Public Works before submitting application):

Multiple locations – see attached list.

## APPLICANT

Name: Inga Note, Senior Traffic Planning Engineer

Address: \_\_\_\_\_

Phone: 509-625-6331 Email: inote@spokanecity.org

## PROPERTY OWNER

Name: City of Spokane public streets

Address: 808 W Spokane Falls Blvd

Phone: \_\_\_\_\_ Email: \_\_\_\_\_

## AGENT

Name: Not applicable

Address: \_\_\_\_\_

Phone: \_\_\_\_\_ Email: \_\_\_\_\_

Assessor's Parcel Numbers: \_\_\_\_\_

Legal Description of Site: \_\_\_\_\_



---

**Page 2**  
Exhibit C, p.2





# Comprehensive Plan Amendments

## Threshold Review

### **Pre-application:**

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

### **Description of the Proposed Amendment:**

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*This proposed amendment would adjust the Arterial Network Map, TR 12, to correct errors discovered since the last update. This cannot be corrected through any other action.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*There is no work program currently planned to update the map.*
3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.  
*Yes it can.*
4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?  
*Not applicable.*
5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.  
*Not applicable.*



6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in the previous year's threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.

*Not applicable.*

7. If this change is directed by state law or a decision of a court or administrative agency, please describe.

*Not applicable.*

8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.

*This has been discussed with Council during the past six months as the SMC Arterial Street Map update was going through approval. I told them we had some corrections to make on map TR 12.*

Planning & Development Services, 808 West Spokane Falls Boulevard, Spokane, WA 99201-3336  
[my.spokanecity.org](http://my.spokanecity.org) | Phone: 509.625.6300

(Rev Sept 2017)





# Comprehensive Plan or Land Use Code Amendment

Pre-Application

Rev.20180102

## DESCRIPTION OF THE PROPOSED AMENDMENT:

*(Please check the appropriate box(es))*

- |                                                                          |                                                      |
|--------------------------------------------------------------------------|------------------------------------------------------|
| <input checked="" type="checkbox"/> Comprehensive Plan <b>MAP</b> Change | <input type="checkbox"/> Land Use Designation Change |
| <input type="checkbox"/> Regulatory Code Text Change                     | <input type="checkbox"/> Area-Wide Rezone            |

**Please respond to these questions on a separate piece of paper. Incomplete answers may jeopardize your application's chances of being reviewed during this amendment cycle.**

### 1. General Questions *(for all proposals)*:

- a. Summarize the general nature of the proposed amendment.
- b. Why do you feel this change is needed?
- c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?
- d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?
- e. For map amendments:
  1. What is the current Land Use designation and zoning for each affected parcel?
  2. What is the requested Land Use designation and zoning for each affected parcel?
  3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc.
- f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?
- g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?
- h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?  
☐ Yes ☐ No
- i. If yes, please answer the following questions:
  1. When was the amendment proposal submitted?
  2. Was it submitted as a consistent amendment or an inconsistent amendment?
  3. What were the Plan Commission recommendation and City Council decision at that time?
  4. Describe any ways that this amendment proposal varies from the previously considered version.



## Comprehensive Plan Amendment – Pre-Application Arterial Network Map Adjustments – 2020

### 1. General Questions:

a. Summarize the general nature of the proposed amendment.

*This proposed amendment would adjust the Arterial Network Map, TR 12, to correct errors discovered since the last update.*

b. Why do you feel this change is needed?

*This change is needed to maintain the accuracy of Map TR 12 – Arterial Network Map of the Comprehensive Plan.*

c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?

*Not applicable.*

d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?

*This proposal does not change goals, policies or regulations, but does change other documents, specifically Map TR 12 – Planned Arterial Network.*

e. For map amendments:

1. What is the current Land Use designation and zoning for each affected parcel? *Not Applicable*

2. What is the requested Land Use designation and zoning for each affected parcel? *Not Applicable*

3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc. *Not Applicable*

f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?

*Yes, the Spokane Official Arterial Street Map SMC 12.08.040 which represents the existing conditions on the street network.*

g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?

*Not applicable.*

h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?  
*No.*

i. If yes, please answer the following questions: *Not Applicable*

1. When was the amendment proposal submitted?

2. Was it submitted as a consistent amendment or an inconsistent amendment?

3. What were the Plan Commission recommendation and City Council decision at that time?

4. Describe any ways that this amendment proposal varies from the previously considered version.



## Comprehensive Plan Map TR 12 modifications

| Street                              | Limits                       | Classification on TR 12 Map    | New Classification    | Reason                         | Proposed by |
|-------------------------------------|------------------------------|--------------------------------|-----------------------|--------------------------------|-------------|
| Thorpe Rd                           | Craig to Lawson              | local                          | Urban Minor Collector | Error correction               | ICM         |
| Granite Road (18 <sup>th</sup> Ave) | Flint to Campus              | Local                          | Urban Minor Arterial  | Consistency with SMC 12.08.040 | ICM         |
| Campus Road                         | 12 <sup>th</sup> Ave to US 2 | Proposed Urban Minor Collector | Urban Minor Collector | constructed                    | ICM         |
| Grove Road                          | City limit to Sunset Hwy     | Local                          | Urban Minor Collector | Error correction               | ICM         |
| Post Street Bridge                  | Summit Pkwy to SFB           | Urban Minor Arterial           | Urban Major Collector | Consistency with SMC 12.08.040 | ICM         |
| 9 <sup>th</sup> Ave - Rockwood      | Grand to Cowley              | Local                          | Urban Major Collector | Error correction               | ICM         |
| Martin Luther King Jr. Way          | Division to Trent            | Proposed Urban Minor Arterial  | Urban Minor Arterial  | constructed                    | ICM         |
| Thurston Ave                        | Grand to Perry               | Urban Major Collector          | local                 | Consistency with SMC 12.08.040 | ICM         |
| Rutter Parkway                      | Eastern to Bradley           | Urban Minor Arterial           | Urban Major Collector | Error correction               | ICM         |
| Upriver Drive                       | N. Center to Mission         | Urban Minor Arterial           | local                 | Vacated by Ordinance C35824    | ICM         |
| Upriver Drive                       | Buckeye to east city limit   | local                          | Urban Minor Arterial  | Error correction               | ICM         |
| Barnes Road                         | Phoebe to Strong             | Proposed Urban Major Collector | Urban Major Collector | constructed                    | ICM         |



**State Environmental Policy Act (SEPA)  
ENVIRONMENTAL CHECKLIST****File No. Z20-042COMP****PLEASE READ CAREFULLY BEFORE COMPLETING THE CHECKLIST!****Purpose of Checklist:**

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

**Instructions for Applicants:**

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

**Use of checklist for nonproject proposals:**

Complete this checklist for nonproject proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**A. BACKGROUND**

1. Name of proposed project: **TR-12 Arterial Network Map Amendments (Comp Plan Amendment)**
2. Applicant: **Inga Note**
3. Address: **808 W Spokane Falls Blvd**  
 City/State/Zip: **Spokane, WA 99201**  
 Phone: **509-625-6331**  
 Agent or Primary Contact: **same**  
 Address: \_\_\_\_\_  
 City/State/Zip: \_\_\_\_\_ Phone: \_\_\_\_\_  
 Location of Project: **This project would affect arterial designations throughout the City**  
 Address: **n/a**  
 Section: \_\_\_\_\_ Quarter: \_\_\_\_\_ Township: \_\_\_\_\_ Range: \_\_\_\_\_  
 Tax Parcel Number(s) **None (affects City Rights-of-Way)**
4. Date checklist prepared: **4/13/2020**
5. Agency requesting checklist: **City of Spokane**
6. Proposed timing or schedule (including phasing, if applicable): **Comprehensive plan amendments are expected to be completed by December 2020.**
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. **While the proposal would amend the arterial designation for several routes within the City, no immediate future construction or reconstruction is planned at this time. Physical modification of streets designated on the map will be analyzed for their environmental effects at the time of design and construction.**  
 b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. **No**
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. **No specific studies or analyses have been prepared.**
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. **None.**
10. List any government approvals or permits that will be needed for your proposal, if known. **City Council approval of a Comprehensive Plan Amendment.**



11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. **The proposal consists of various amendments to Map TR-12, Arterial Network Map, in Chapter 4, Transportation, of the Spokane Comprehensive Plan. These amendments would modify whether certain portions of streets in the City are designated as arterials, collectors, local streets, or other classifications. No immediate or near-term physical changes to those streets are proposed at this time, as this map indicates the expected final condition of these streets within 20 years. Future construction or re-construction of streets in Spokane will be subject to additional SEPA review at the time of design.**

12. Location of the proposal: Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist. **Various locations throughout the City. The current list of locations is available at the following website: <https://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/tr-12-arterial-network-map/>**

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.) **Yes, the proposed amended streets are all located within the ASA, sewer service area, and the City of Spokane.**

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

(1) Describe any systems, other than those designed for the disposal of sanitary waste installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities). **None at this time.**



(2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored? **N/A, Non-Project Action (map change).**

(3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems. **N/A, Non-Project Action (map change).**

(4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater? **N/A, Non-Project Action (map change).**

b. Stormwater

(1) What are the depths on the site to groundwater and to bedrock (if known)? **Varies throughout the City.**

(2) Will stormwater be discharged into the ground? If so, describe any potential impacts. **N/A, Non-Project Action (map change).**

## B. ENVIRONMENTAL ELEMENTS

### 1. Earth

a. General description of the site (check one):

☐ Flat ☐ Rolling ☐ Hilly ☐ Steep slopes ☐ Mountainous

Other: **Varies throughout the City.**

b. What is the steepest slope on the site (approximate percent slope)? **Varies throughout the City.**

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. **Varies throughout the City.**

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. **N/A, Non-Project Action (map change).**



- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill: **N/A, Non-Project Action (map change).**
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. **No, Non-Project Action (map change).**
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt, or buildings)? **N/A, Non-Project Action (map change).**
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: **None.**

## 2. Air

- a. What type of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. **Vehicles utilizing streets in the city emit typical exhaust gases from vehicle engines. As the proposed streets are all existing streets at this time, the proposal is not expected to result in increased emissions.**
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. **No.**
- c. Proposed measures to reduce or control emissions or other impacts to air, if any: **None, Non-Project Action (map change).**

## 3. Water

### a. SURFACE WATER:

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. **N/A, Non-Project Action (map change). Future construction will be analyzed for effects to surface water at the time of design and development.**



- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. **N/A, Non-Project Action (map change).**
- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. **N/A, Non-Project Action (map change).**
- (4) Will the proposal require surface water withdrawals or diversions? If yes, give general description, purpose, and approximate quantities if known. **N/A, Non-Project Action (map change).**
- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. **Varies throughout the City.**
- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. **No. Non-Project Action (map change).**

b. GROUNDWATER:

- (1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. **No.**
- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. **None, Non-Project Action (map change).**

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. **N/A, Non-Project Action (map change).**



(2) Could waste materials enter ground or surface waters? If so, generally describe. **No, Non-Project Action (map change).**

(3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. **N/A, Non-Project Action (map change).**

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any. **None. Non-Project Action (map change).**

#### 4. Plants

a. Check the type of vegetation found on the site:

Deciduous tree: ☐ alder ☐ maple ☐ aspen

Other: **Various street trees.**

Evergreen tree: ☐ fir ☐ cedar ☐ pine

Other: **Various street trees.**

☐ Shrubs ☐ Grass ☐ Pasture ☐ Crop or grain

☐ Orchards, vineyards or other permanent crops

Wet soil plants: ☐ cattail ☐ buttercup ☐ bullrush ☐ skunk cabbage

Other: \_\_\_\_\_

Water plants: ☐ water lily ☐ eelgrass ☐ milfoil

Other: \_\_\_\_\_

Other types of vegetation: \_\_\_\_\_

b. What kind and amount of vegetation will be removed or altered? **N/A, Non-Project Action (map change).**

c. List threatened and endangered species known to be on or near the site. **None. All locations are paved streets.**

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: **None.**



- e. List all noxious weeds and invasive species known to be on or near the site. **N/A, Non-Project Action (map change).**

## 5. Animals

- a. Check and List any birds and other animals which have been observed on or near the site or are known to be on or near the site:

Birds: ☐ hawk ☐ heron ☐ eagle ☐ songbirds

Other: \_\_\_\_\_

Mammals: ☐ deer ☐ bear ☐ elk ☐ beaver

Other: \_\_\_\_\_

Fish: ☐ bass ☐ salmon ☐ trout ☐ herring ☐ shellfish

Other: \_\_\_\_\_

Other (not listed in above categories): **Typical urban wildlife may exist on various sites within landscaping and street trees.**

- b. List any threatened or endangered animal species known to be on or near the site.  
**None.**

- c. Is the site part of a migration route? If so, explain. **Unknown.**

- d. Proposed measures to preserve or enhance wildlife, if any: **None.**

- e. List any invasive animal species known to be on or near the site. **None.**

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. **N/A, Non-Project Action (map change).**

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. **N/A, Non-Project Action (map change).**



- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: **None.**

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. **City streets are used by public and private vehicles that emit exhaust that is known to be hazardous to health in sufficient concentrations. However, as all proposed map amendments concern existing streets the change is expected to be negligible even after construction of any new features.**

(1) Describe any known or possible contamination at the site from present or past uses. **None.**

(2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. **N/A, Non-Project Action (map change).**

(3) Describe any toxic or hazardous chemicals/conditions that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. **N/A, Non-Project Action (map change).**

(4) Describe special emergency services that might be required. **None.**

(5) Proposed measures to reduce or control environmental health hazards, if any:  
**None.**

## b. NOISE:

(1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? **Common traffic noise from existing roadways.**

(2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site. **Common traffic noise from roadways.**



(3) Proposed measure to reduce or control noise impacts, if any: **None.**

## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. **All sites consist of existing City Rights of Way serving nearby properties with access.**
- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? **No.**
- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: **N/A, Non-Project Action (map change).**
- c. Describe any structures on the site. **None.**
- d. Will any structures be demolished? If so, which? **No.**
- e. What is the current zoning classification of the site? **N/A, City streets are not zoned.**
- f. What is the current comprehensive plan designation of the site? **N/A, City streets have no designated land use.**
- g. If applicable, what is the current shoreline master program designation of the site? **N/A, Non-Project Action (map change).**
- h. Has any part of the site been classified as a critical area by the city or the county? If so, specify. **No.**
- i. Approximately how many people would reside or work in the completed project? **None.**
- j. Approximately how many people would the completed project displace? **None.**
- k. Proposed measures to avoid or reduce displacement impacts, if any: **None.**



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- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: **None. Project is consistent.**
- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any: **None.**

**9. Housing**

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. **None.**
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. **None.**
- c. Proposed measures to reduce or control housing impacts, if any: **None.**

**10. Aesthetics**

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? **N/A, Non-Project Action (map change).**
- b. What views in the immediate vicinity would be altered or obstructed? **N/A, Non-Project Action (map change).**
- c. Proposed measures to reduce or control aesthetic impacts, if any: **None.**

**11. Light and Glare**

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? **N/A, Non-Project Action (map change).**
- b. Could light or glare from the finished project be a safety hazard or interfere with views? **No.**
- c. What existing off-site sources of light or glare may affect your proposal? **None.**
- d. Proposed measures to reduce or control light and glare impacts, if any: **None.**

**12. Recreation**



- a. What designated and informal recreational opportunities are in the immediate vicinity? **Varies throughout City.**
- b. Would the proposed project displace any existing recreational uses? If so, describe. **No.**
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: **None.**

### 13. Historic and cultural preservation

- a. Are there any buildings, structures, or sites, located on or near the sited that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. **N/A, Non-Project Action (map change).**
- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. **N/A, Non-Project Action (map change).**
- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archaeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. **None.**
- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. **None.**

### 14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. **Varies. See <https://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/tr-12-arterial-network-map/> for the location of streets affected by the proposal.**
- b. Is site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop **Many routes within the City utilize City streets.**
- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? **N/A, Non-Project Action (map change).**



- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). **The proposal would amend the classification of several streets throughout the City, which may result in future improvements to those streets. All such improvements would be to public streets.**
- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. **Varies. See <https://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/tr-12-arterial-network-map/> for the location of streets affected by the proposal.**
- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and non-passenger vehicles). What data or transportation models were used to make these estimates? **None.**

*(Note: to assist in review and if known, indicate vehicle trips during PM peak, AM Peak, and Weekday (24 hours).)*

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, general describe. **No.**
- h. Proposed measures to reduce or control transportation impacts, if any: **None.**

## **15. Public services**

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. **No.**
- b. Proposed measures to reduce or control direct impacts on public services, if any: **None.**



## 16. Utilities

a. Check utilities currently available at the site:

☒ electricity

☒ natural gas

☒ water

☒ refuse service

☒ telephone

☒ sanitary sewer

☐ septic system

Other: \_\_\_\_\_  
\_\_\_\_\_

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed: **None.**



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/15/2020

Signature:  A1675FE85B1948D...

***Please Print or Type:***

Proponent: City of Spokane  
**Primary Staff Contact: Inga Note, Integrated Capital Management**

Address: 808 W Spokane Falls Blvd

Phone: 509-625-6331

Person completing form (if different from proponent):

Phone: \_\_\_\_\_ Address: \_\_\_\_\_  
 \_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS*****(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise? **Future reconstruction of these streets may generate temporary construction noise. Also, streets create normal traffic noise during operation—although as these are existing City streets any increase in traffic noise would be negligible before and after reconstruction.**

Proposed measures to avoid or reduce such increases are: **None.**

2. How would the proposal be likely to affect plants, animals, fish or marine life? **As these streets already exist as paved roadways, no effect is expected.**

Proposed measures to protect or conserve plants, animals, fish or marine life are: **None.**

3. How would the proposal be likely to deplete energy or natural resources? **The project is not expected to deplete these resources.**

Proposed measures to protect or conserve energy and natural resources are: **None.**

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands? **As these streets already exist as paved roadways, no effect is expected.**

Proposed measures to protect such resources or to avoid or reduce impacts are: **None.**

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans? **As these streets already exist as paved roadways, no effect is expected.**

Proposed measures to avoid or reduce shoreline and land use impacts are: **None.**



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6. How would the proposal be likely to increase demands on transportation or public services and utilities? **The project concerns transportation facilities directly and would not adversely affect them. A more efficient transportation system will support other services like emergency response and transit.**

Proposed measures to reduce or respond to such demand(s) are: **None.**

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment. **The proposal is not expected to conflict with any local, state, or federal laws.**



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### C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/15/2020

Signature: \_\_\_\_\_

DocuSigned by:

*Inga Note*

A1675FE85B1948D...

***Please Print or Type:***

Proponent: **City of Spokane**

**Primary Staff Contact: Inga Note, Integrated Capital Management**

Address: **808 W Spokane Falls Blvd**

Phone: **509-625-6331**

Person completing form (if different from proponent): \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

### FOR STAFF USE ONLY

Staff member(s) reviewing checklist: Kevin Freibott, Neighborhood and Planning Services

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- A. ☒ there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- B. ☐ probable significant adverse impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- C. ☐ there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z20-042COMP

**PROPONENT:** City of Spokane

**DESCRIPTION OF PROPOSAL:** Amendment of Map TR-12, "Arterial Network Map", of Chapter 4, Transportation, of the Comprehensive Plan to modify the proposed street network in various locations throughout the City. Map TR-12 identifies the proposed future arterial classification for various streets throughout the City. No actual construction is proposed at this time.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal is a city-wide map amendment and would affect various locations throughout the City. The specific locations and changes proposed are available at the website identified below:

<http://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/>

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

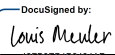
- ☐ There is no comment period for this DNS.
- ☐ This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.
- ☒ This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:** 

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*





**Spokane Tribe of Indians  
Tribal Historic Preservation Officer**

P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z20-042COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer



**From:** [Halbig, Bobby](#)  
**To:** [Freibott, Kevin](#)  
**Subject:** RE: REVISED Request For Comments - Arterial Network Map Amendment Proposal  
**Date:** Tuesday, June 16, 2020 12:25:26 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Hello Kevin,  
The Street Department has reviewed the proposal and has no comments.  
Best regards,



**Bobby Halbig** | City of Spokane | Engineering Technician III, Traffic Operations  
509.232-8846 | fax 509.232.8830 | [bhalbig@spokanecity.org](mailto:bhalbig@spokanecity.org) | [spokanecity.org](http://spokanecity.org)



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**From:** Bishop, Stephanie <sbishop@spokanecity.org>  
**Sent:** Tuesday, June 9, 2020 4:42 PM  
**To:** 92CES.CEN.CommunityProjCoord@us.af.mil; Abrahamson, Randy <randya@spokanetribe.com>; Barlow, Lori <lbarlow@spokanevalley.org>; Basinger, Mike <mbasinger@spokanevalley.org>; Becker, Kris <kbecker@spokanecity.org>; Brown, Eldon <ebrown@spokanecity.org>; Buller, Dan <dbuller@spokanecity.org>; Byus, Dave <dave.byus@avistacorp.com>; Coster, Michael <mcoster@spokanecity.org>; Crago, Wes <wcrago@spokanecity.org>; Davis, Marcia <mdavis@spokanecity.org>; Dept. of Archaeology and Historic Preservation <sepa@dahp.wa.gov>; DNR Aquatics <dnrreaqualeasingrivers@dnr.wa.gov>; Duvall, Megan <mduvall@spokanecity.org>; Eliason, Joelle <jeliason@spokanecity.org>; Environmental Review <SEPAUNIT@ECY.WA.GOV>; Figg, Greg <FiggG@wsdot.wa.gov>; Graff, Joel <jgraff@spokanecity.org>; Greene, Barry <BGreene@spokanecounty.org>; Gunderson, Dean <dgunderson@spokanecity.org>; Halbig, Bobby <bhalbig@spokanecity.org>; Hanson, Tonilee <sajbinfo@gmail.com>; Harris, Clint E. <ceharris@spokanecity.org>; Harsh, Dave <dave.harsh@dnr.wa.gov>; Howell, Gordon <ghowell@spokanetransit.com>; Hughes, Rick <rhughes@spokanecity.org>; Istrate, David <dcistrate@spokanecounty.org>; Jeff Lawlor <jeffrey.lawlor@dfw.wa.gov>; jhacker-brumley@spokanecity.org; John Conklin <jconklin@spokanecleanair.org>; Johnson, Candy <CandyJ@spokaneschools.org>; Johnson, Erik D. <edjohnson@spokanecity.org>; Jones, Garrett <gjones@spokanecity.org>; Jordan, Jess <dale.j.jordan@usace.army.mil>; Kaehler, Gretchen <gretchen.kaehler@DAH.P.wa.gov>; Kay, Char <kayc@wsdot.wa.gov>; Kegley, Daniel <dkegley@spokanecity.org>; Kells, Patty <pkells@spokanecity.org>; Kokot, Dave <dkokot@spokanecity.org>; Leslie King <leslie.king@dfw.wa.gov>; Limon, Tara <tlimon@spokanetransit.com>; Martin, Greg <gmartin@spokanecity.org>; McCann, Jacob <jmca461@ecy.wa.gov>; McClure, Jeff <Jmclure@cheneysd.org>; Melvin, Val



<vmelvin@spokanecity.org>; Meyer, Eric <emeyer@srhd.org>; Miller, Katherine E <kemiller@spokanecity.org>; Moore, David <dmoo461@ecy.wa.gov>; Moore, Michael <michael.s.moore@williams.com>; Morris, Mike <mmorris@spokanecity.org>; Murphy, Dermott G. <dgmurphy@spokanecity.org>; Myhre, Randy <randy.myhre@avistacorp.com>; Neighborhood Services <Neigh.Svcs@SpokaneCity.org>; Nilsson, Mike <mnilsson@spokanecity.org>; Note, Inga <inote@spokanecity.org>; Nyberg, Gary <GNYBERG@spokanecounty.org>; Owen, Melissa <mowen@spokanecity.org>; Palmquist, Tami <tpalmquist@spokanecity.org>; Peacock, William <wpeacock@spokanecity.org>; Pederson, John <JPederson@spokanecounty.org>; Pruitt, Larissa <larissa.pruitt@avistacorp.com>; Quinn-Hurst, Colin <cquinnhurst@spokanecity.org>; Raymond, Amanda <arraymond@bpa.gov>; Reisenauer, Chuck <creisenauer@spokanepolice.org>; Renee Kinnick <Renee.Kinnick@dfw.wa.gov>; Richman, James <jrichman@spokanecity.org>; Robertson, Renee <rrobertson@spokanecity.org>; Sakamoto, James <jsakamoto@spokanecity.org>; Savage, Paul <psavage@srhd.org>; SEPA Center <sepacenter@dnr.wa.gov>; SEPA Register <separegister@ecy.wa.gov>; Sherve, Jon <jsherve@srhd.org>; Simmons, Scott M. <smsimmons@spokanecity.org>; Spokane Library <dtcirc@spokanelibrary.org>; Steele, David <dsteeler@spokanecity.org>; Stewart, Ryan <rstewart@srtc.org>; Treasury Accounting <treasuryaccounting@spokanecity.org>; Warfield, Paul <pwarfield@spokanecity.org>; Weinand, Kathleen <kweinand@spokanetransit.com>; Weingart, LuAnn <luann.weingart@avistacorp.com>; Wendle, Ned <ned.wendle@mead354.org>; Westby, April <awestby@spokanecleanair.org>; Windsor, Scott <swindsor@spokanecity.org>; Wright, Phil <philw@spokaneschools.org>  
**Cc:** Churchill, Jackie <jchurchill@spokanecity.org>; Freibott, Kevin <kfreibott@spokanecity.org>  
**Subject:** REVISED Request For Comments - Arterial Network Map Amendment Proposal

Good Afternoon,

Please find attached the REVISED Request for Comments and Environmental Checklist for the following proposal:

**Project Name:** TR-12 Map Amendment - Arterial Network Map Amendment Proposal

**Permit #:** Z20-042COMP

Please direct any questions or comments to Assistant Planner II, Kevin Freibott, at [kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org).

Thank you,



Stephanie N Bishop | Neighborhood & Planning Services | Clerk III  
509.625.6244 | fax 509.625.6013 | [sbishop@spokanecity.org](mailto:sbishop@spokanecity.org)



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**From:** [Halbig, Bobby](#)  
**To:** [Bishop, Stephanie](#); [Churchill, Jackie](#)  
**Cc:** [Freibott, Kevin](#); [Okihara, Gerald](#); [Eveland, Marcus](#); [Melvin, Val](#)  
**Subject:** RE: 2nd REVISED Request for Comments for Arterial Network Map Amendment Proposal  
**Date:** Thursday, August 6, 2020 12:59:01 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)

---

Hi Stephanie,

The plans have been reviewed and the Street Department has no comments.

Best regards,



**Bobby Halbig** | City of Spokane | Engineering Technician III, Traffic Operations  
509.232-8846 | fax 509.232.8830 | [bhalbig@spokanecity.org](mailto:bhalbig@spokanecity.org) | [spokanecity.org](http://spokanecity.org)



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**From:** Bishop, Stephanie <[sbishop@spokanecity.org](mailto:sbishop@spokanecity.org)>  
**Sent:** Tuesday, July 28, 2020 11:05 PM  
**To:** Churchill, Jackie <[jchurchill@spokanecity.org](mailto:jchurchill@spokanecity.org)>  
**Cc:** Freibott, Kevin <[kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org)>  
**Subject:** 2nd REVISED Request for Comments for Arterial Network Map Amendment Proposal

Good Evening,

Please find attached the 2<sup>nd</sup> REVISED Request for Comments for the following proposal:

**Project Name:** TR-12 Map Amendment - Arterial Network Map Amendment Proposal

**Permit #:** Z20-042COMP

Please direct any questions or comments to Assistant Planner II, Kevin Freibott, at [kfreibott@spokanecity.org](mailto:kfreibott@spokanecity.org).

Thank you,



**Stephanie N Bishop** | Neighborhood & Planning Services | Clerk III  
509.625.6244 | fax 509.625.6013 | [sbishop@spokanecity.org](mailto:sbishop@spokanecity.org)



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## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z20-042COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment proposal seeking to amend Map TR 12 of the Comprehensive Plan in Chapter 4: Transportation, relating to arterial street classifications on various streets throughout the City of Spokane.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment Proposal Z20-042COMP (the “Proposal”) was submitted in a timely manner for review during the City’s 2019/2020 amendment cycle.
- D. The Proposal seeks to amend Map TR 12 of the Comprehensive Plan to update arterial street classifications of various street segments in the City of Spokane.
- E. Annual amendment proposals are subject to a threshold review process to determine whether the proposals will be included in the City’s Annual Comprehensive Plan Amendment Work Program.
- F. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, and including the Proposal in the Work Program.
- G. Thereafter, on April 24, 2020, staff requested comments from agencies, departments, and neighborhood councils. Comments were received from the City Streets Department and the Spokane Tribe, stating no significant concerns with the Proposal.
- H. On June 9, 2020 and again on July 28, 2020, staff reissued the request for comments from agencies, departments, and neighborhood councils due to the addition of a few new street segments to the proposed amendments. An additional two-weeks was provided each time for agencies, departments, and neighborhoods to comment on these revised notices. No new comments were received during the revised agency comment periods.
- I. A Notice of Application was published on June 8, 2020 in the Spokesman Review. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City did not receive any comments on the Proposal from the public.



- J. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Proposal, and was provided with information regarding the dates of Plan Commission workshops and hearings.
- K. On June 10, 2020, the Spokane City Plan Commission held a workshop to study the Proposal.
- L. On June 23, 2020, the Pedestrian Transportation and Traffic Committee of the Community Assembly received a presentation regarding the Proposal.
- M. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Proposal.
- N. On July 21, 2020, the Plan Commission Transportation Subcommittee received a presentation regarding the Proposal.
- O. On July 30, 2020, an online public workshop specific to the bicycle- and transportation-related amendments was provided for the general public to answer questions and receive comments on the proposed amendments.
- P. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Proposal. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Proposal was published in the Official Gazette on September 2 and 9, 2020.
- Q. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- R. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of a SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- S. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Proposal, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Proposal recommended approval of the Proposal.
- T. On September 23, 2020, the Plan Commission held a public hearing on the Proposal, including the taking of verbal testimony, and closed the public record at that time. No members of the public testified during the hearing.
- U. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- V. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Proposal (the "Staff Report").



- W. The Plan Commission finds that the Proposal meets the intent and requirements of the Comprehensive Plan.
- X. The Plan Commission finds that the Proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.

## **CONCLUSIONS:**

Based upon the Proposal materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding Proposal File No. Z20-042COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Proposal was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Proposal was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Proposal is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Proposal will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Proposal is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Proposal is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Proposal has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Proposal.
9. The Proposal will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Proposal proposes a map amendment that is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g., compatibility with neighboring land uses, proximity to arterials, etc.).
11. The proposed map amendment is suitable for the proposed designation.



12. The map amendment would implement applicable comprehensive plan policies better than the current map designation.

#### **RECOMMENDATIONS:**

In the matter of Z20-042COMP, a request by City of Spokane staff to amend Map TR 12 in Chapter 4: Transportation of the Comprehensive Plan, as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to Map TR5 in Chapter 4: Transportation of the City's Comprehensive Plan, and authorizes the President to prepare and sign on the Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the Proposal.

---



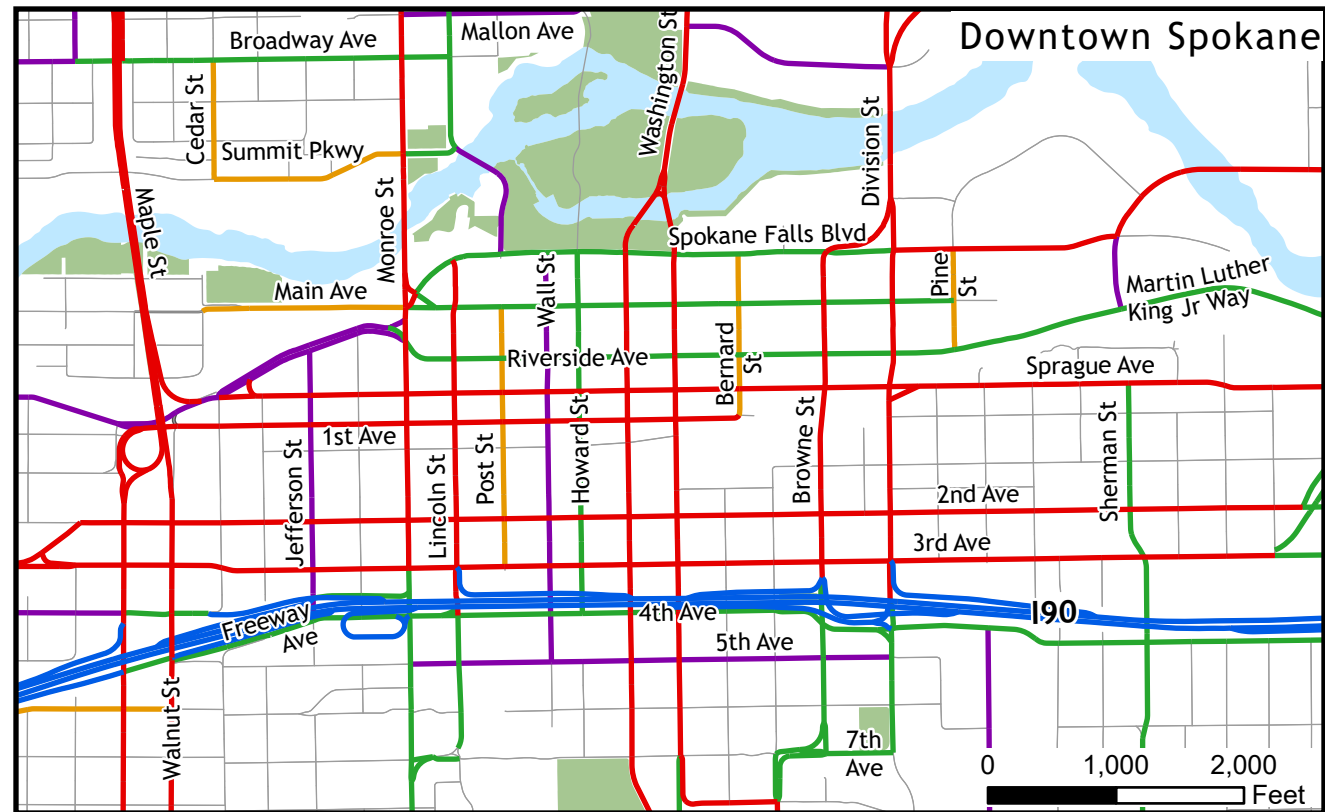
**Todd Beyreuther, President**

Spokane Plan Commission

October 21, 2020



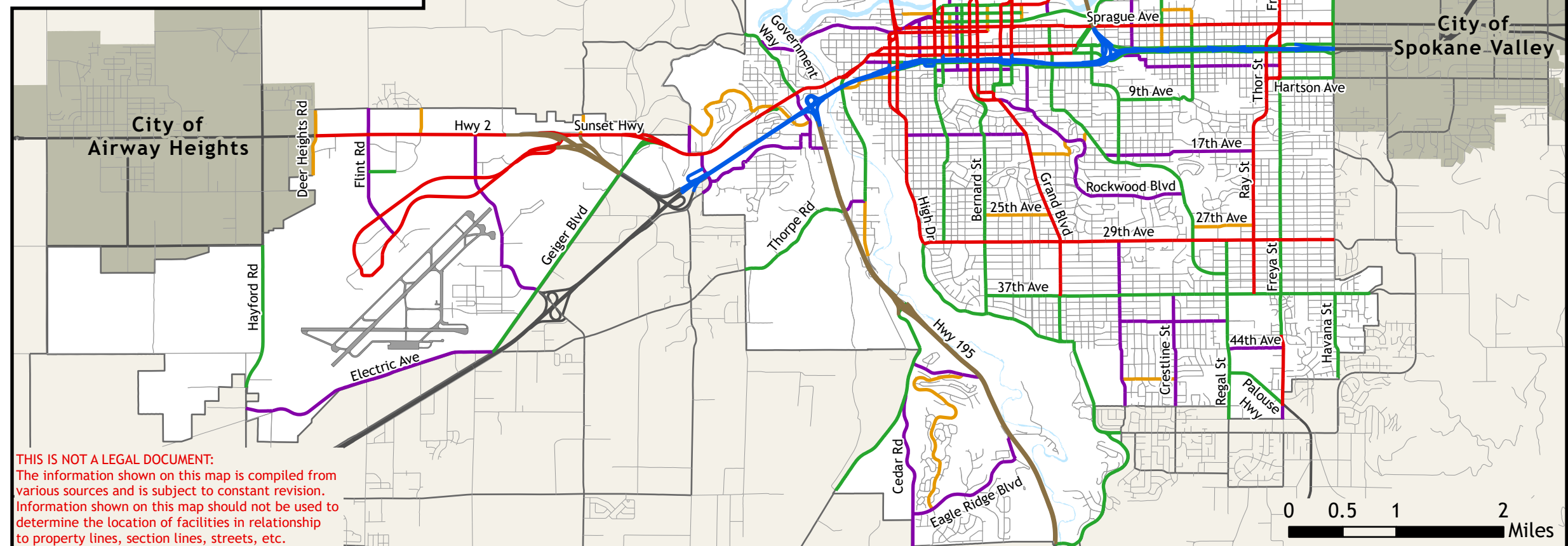
# City of Spokane Arterial Street Map SMC 12.08.040



## Legend

- Urban Interstate
- Urban Other Freeways and Expressways
- Urban Principal Arterial
- Urban Minor Arterial
- Urban Major Collector
- Urban Minor Collector
- Urban Local Access
- City of Spokane
- Stream or River

Date: October 2020



**THIS IS NOT A LEGAL DOCUMENT:**  
The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.





**Agenda Sheet for City Council Meeting of:**  
11/16/2020

|                       |             |
|-----------------------|-------------|
| <b>Date Rec'd</b>     | 10/26/2020  |
| <b>Clerk's File #</b> | ORD C35980  |
| <b>Renews #</b>       |             |
| <b>Cross Ref #</b>    |             |
| <b>Project #</b>      | Z20-045COMP |
| <b>Bid #</b>          |             |
| <b>Requisition #</b>  |             |

|                           |                                                     |
|---------------------------|-----------------------------------------------------|
| <b>Submitting Dept</b>    | PLANNING                                            |
| <b>Contact Name/Phone</b> | KEVIN FREIBOTT 625-6184                             |
| <b>Contact E-Mail</b>     | KFREIBOTT@SPOKANECITY.ORG                           |
| <b>Agenda Item Type</b>   | First Reading Ordinance                             |
| <b>Agenda Item Name</b>   | 0650 – COMPREHENSIVE PLAN AMENDMENT – RAILROAD TEXT |

**Agenda Wording**

Ordinances relating to proposal Z20-045COMP by the City, amending Chapter 4, Transportation, of the Comprehensive Plan to account for safety needs for at-grade railroad crossings.

**Summary (Background)**

The proposal concerns text amendments to Chapter 4, Transportation, accounting for safety improvements that may be necessary for at-grade railroad crossings in the City. This is considered concurrently through the annual Comprehensive Plan Amendment cycle as required by the Growth Management Act. Public participation and notification requirements are complete. The Plan Commission held a Public Hearing on September 8 to consider this amendment and has recommended approval of the amendment.

|                                    |                                       |                                                             |
|------------------------------------|---------------------------------------|-------------------------------------------------------------|
| <b><u>Fiscal Impact</u></b>        | Grant related? NO<br>Public Works? NO | <b><u>Budget Account</u></b>                                |
| Neutral \$                         |                                       | #                                                           |
| Select \$                          |                                       | #                                                           |
| Select \$                          |                                       | #                                                           |
| Select \$                          |                                       | #                                                           |
| <b><u>Approvals</u></b>            | <b><u>Council Notifications</u></b>   |                                                             |
| <b><u>Dept Head</u></b>            | MEULER, LOUIS                         | <b><u>Study Session\Other</u></b> CC Study Session 10-29-20 |
| <b><u>Division Director</u></b>    | SIMMONS, SCOTT M.                     | <b><u>Council Sponsor</u></b> CM Mumm                       |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY                       | <b><u>Distribution List</u></b>                             |
| <b><u>Legal</u></b>                | RICHMAN, JAMES                        | tblack@spokanecity.org                                      |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL                       | jrichman@spokanecity.org                                    |
| <b><u>Additional Approvals</u></b> |                                       | sbishop@spokanecity.org                                     |
| <b><u>Purchasing</u></b>           |                                       | dhume@spokane-landuse.com                                   |
|                                    |                                       | lmeuler@spokanecity.org                                     |
|                                    |                                       |                                                             |
|                                    |                                       |                                                             |



Ordinance No. C35980

AN ORDINANCE RELATING TO PROPOSAL FILE Z20-045COMP AMENDING THE TEXT OF CHAPTER 4, TRANSPORTATION, OF THE COMPREHENSIVE PLAN TO DISCUSS SAFETY NEEDS FOR AT-GRADE RAILROAD CROSSINGS.

WHEREAS, the Washington State Legislature passed the Growth Management Act (GMA) in 1990, requiring among other things, the development of a Comprehensive Plan (RCW 36.70A); and

WHEREAS, the City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act; and

WHEREAS, the Growth Management Act requires continuing review and evaluation of the Comprehensive Plan and contemplates an annual amendment process for incorporating necessary and appropriate revisions to the Comprehensive Plan; and

WHEREAS, Proposal File Z20-045COMP was submitted in a timely manner for review during the City's 2019/2020 Comprehensive Plan amendment cycle; and

WHEREAS, Proposal Z20-045COMP seeks to amend Chapter 4, Transportation, of the Comprehensive Plan to add language relating to safety for at-grade railroad crossings; and

WHEREAS, staff requested comments from agencies and departments on May 11, 2020, and a public comment period ran from June 8, 2020 to August 7, 2020; and

WHEREAS, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice before adoption of proposed changes to the Comprehensive Plan on August 25, 2020; and

WHEREAS, a State Environmental Policy Act (SEPA) Determination of Non-Significance was issued on August 24, 2020 for the amendment to the Comprehensive Plan, the comment period for which ended on September 14, 2020; and

WHEREAS, a staff report for Proposal Z20-045COMP reviewed all the criteria relevant to consideration of the proposal was published on August 25, 2020 and sent to all applicants and the Plan Commission; and

WHEREAS, notice of the SEPA Checklist and Determination and announcement of the Plan Commission Hearing for the proposal was published on August 26, 2020 and September 2, 2020; and



WHEREAS, Notice of Plan Commission Public Hearing and SEPA Determination was posted online and with the Spokesman Review, pursuant to Spokane Municipal Code 17G.020.070, on August 26, 2020; and

WHEREAS, the Spokane Plan Commission held a public hearing on September 9, 2020, continued to September 23, 2020, during which they took public testimony on this proposal and deliberated this and all other Comprehensive Plan Amendment applications; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-045COMP is consistent with and implements the Comprehensive Plan; and

WHEREAS, the Spokane Plan Commission found that Proposal Z20-045COMP meets the decision criteria for Comprehensive Plan Amendments delineated in Spokane Municipal Code 17G.020.030; and

WHEREAS, the Spokane Plan Commission voted 9 to 0 to recommend approval of Proposal Z20-045COMP; and

WHEREAS, the City Council adopts the recitals set forth herein as its findings and conclusions in support of its adoption of this ordinance and further adopts the findings, conclusions, and recommendations from the Planning Services Staff Report and the City of Spokane Plan Commission for the same purposes; --

NOW, THEREFORE, THE CITY OF SPOKANE DOES ORDAIN:

1. Approval of the Proposal. Proposal Z20-045COMP is approved.
2. Amendment of Chapter 4, Transportation. The following text is appended to the end of Chapter 4 of the Spokane Comprehensive Plan:

**Railroad Crossing Projects**

There are many at-grade rail crossings within the city. Most of these already have warning devices and gates installed to provide increased protection for vehicles, cyclists, and pedestrians on the road. However, there are a few arterial crossing locations that could use further improvements, as funding becomes available. These locations are already equipped with warning lights and bells. However, due to increased traffic on the roadway or on the rail line, the locations listed in Table TR-9 would benefit from additional safety measures.

| <b><u>TABLE TR 9 – RAILROAD CROSSING PROJECT LIST</u></b> |                                   |
|-----------------------------------------------------------|-----------------------------------|
| <b><u>Project Name</u></b>                                | <b><u>Needed Improvements</u></b> |



|                                                               |                                                                                                                                               |
|---------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------|
| <u>Havana Street crossing of UP<br/>(n/o Sprague Avenue)</u>  | <u>Widen crossing for sidewalk, install<br/>gates, update preemption equipment<br/>and track circuit for the adjacent<br/>traffic signal.</u> |
| <u>Freya Street crossing of UP<br/>(n/o Sprague Avenue)</u>   | <u>Install gates, update preemption<br/>equipment and track circuit for the<br/>adjacent traffic signal.</u>                                  |
| <u>Mission Street crossing of BNSF<br/>(e/o Perry Street)</u> | <u>Install gates, update preemption<br/>equipment and track circuit for the<br/>adjacent traffic signal.</u>                                  |

Notes: UP = Union Pacific Railroad. BNSF = Burlington Northern and  
Santa Fe Railroad.

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_, 2020.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department: N/A

Approving Supervisor: N/A

Amount of Proposed Expenditure: N/A

Funding Source: N/A

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



**STAFF REPORT Z20-045COMP**

Department of Neighborhood and Planning Services

The following staff report concerns a proposed amendment to the City's Comprehensive Plan. The proposal is to amend the text of Chapter 4, Transportation, to include language pertaining to at-grade railroad crossing safety. Amendments to the Comprehensive Plan are enabled by Spokane Municipal Code (SMC) 17G.020 and Revised Code of Washington (RCW) 36.70A.130.

**I. PROPERTY SUMMARY**

|                           |                                 |
|---------------------------|---------------------------------|
| <b>Parcel(s):</b>         | NA - Various locations citywide |
| <b>Address(es):</b>       | NA - Various locations citywide |
| <b>Property Size:</b>     | Not applicable                  |
| <b>Legal Description:</b> | Not applicable                  |
| <b>General Location:</b>  | City rights-of-way              |
| <b>Current Use:</b>       | Arterial Streets                |

**II. APPLICANT SUMMARY**

|                        |                 |
|------------------------|-----------------|
| <b>Staff Contact:</b>  | Inga Note       |
| <b>Applicant:</b>      | City of Spokane |
| <b>Property Owner:</b> | City of Spokane |

**III. PROPOSAL SUMMARY**

|                                       |                                                                                                                                            |
|---------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Current Land Use Designation:</b>  | n/a                                                                                                                                        |
| <b>Proposed Land Use Designation:</b> | n/a                                                                                                                                        |
| <b>Current Zoning:</b>                | n/a                                                                                                                                        |
| <b>Proposed Zoning:</b>               | n/a                                                                                                                                        |
| <b>SEPA Status:</b>                   | A SEPA threshold Determination of Non-Significance (DNS) was made on August 24, 2020. The appeal deadline is 5:00 PM on September 7, 2020. |
| <b>Plan Commission Hearing Date:</b>  | September 9, 2020                                                                                                                          |
| <b>Staff Contact:</b>                 | Kevin Freibott, Assistant Planner II, <a href="mailto:kfreibott@spokanecity.org">kfreibott@spokanecity.org</a>                             |
| <b>Staff Recommendation:</b>          | Recommended                                                                                                                                |



#### IV. BACKGROUND INFORMATION

1. **General Proposal Description:** Pursuant to the procedures established by SMC 17G.060, enabled by RCW 36.70A.130, the proposer asks the City of Spokane to amend the text of Chapter 4 to include language regarding at-grade railroad crossing safety improvements, shown in **Exhibit A**.
2. **Site Description and Physical Conditions:** The proposal concerns text highlighting railroad safety, highlighting three existing arterial railroad crossing locations: the Havana Street crossing of the Union Pacific line, the Freya Street crossing of the Union Pacific line and the Mission Street crossing of the BNSF line. Crossing safety signage and other improvements exist already at these locations, though there are currently no crossing gates.
3. **Property Ownership:** All designated locations are City rights-of-way.
4. **Adjacent Property Improvements and Uses:** Adjacent property uses are primarily industrial and commercial along Freya and Havana. Property uses along Mission are a park and the Avista headquarters office building.
5. **Street Class Designations:** Mission Avenue is designated as a Principal Arterial. Freya Street is designated as a Principal Arterial. Havana Street is a Minor Arterial. No change of street class designation is proposed as part of this application, nor is any change called for in Map TR-12 of the Comprehensive Plan (the Arterial Network Map).
6. **Current Land Use Designation and History:** n/a
7. **Proposed Land Use Designation:** n/a
8. **Current Zoning and History:** n/a
9. **Proposed Zoning:** n/a

#### V. APPLICATION PROCESS AND PUBLIC COMMENT

1. **Key Steps:** The application is being processed according to SMC 17G.060, including the following steps:

|                                                               |                   |
|---------------------------------------------------------------|-------------------|
| Application Submitted .....                                   | October 29, 2019  |
| Threshold Application Certified Complete .....                | November 27, 2019 |
| Council Threshold Subcommittee Established <sup>1</sup> ..... | January 13, 2020  |
| Council Threshold Subcommittee Met .....                      | February 6, 2020  |
| Annual Work Program Set <sup>2</sup> .....                    | March 2, 2020     |
| Agency/Department Comment Period Ended .....                  | May 11, 2020      |

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<sup>1</sup> Spokane City Council Resolution 2020-0002

<sup>2</sup> Spokane City Council Resolution 2020-0014



|                                                |                   |
|------------------------------------------------|-------------------|
| Notice of Application Posted .....             | June 8, 2020      |
| Plan Commission Workshop .....                 | June 24, 2020     |
| 60-Day Public Comment Period Ended .....       | August 7, 2020    |
| SEPA Determination Issued .....                | August 24, 2020   |
| Notice of Public Hearing Posted .....          | August 26, 2020   |
| Plan Commission Hearing Date (Scheduled) ..... | September 9, 2020 |

2. **Comments Received:** A request for comments was issued to City departments, local agencies, and departments within 400 feet of the proposal, along with pertinent application details on April 24, 2020. By the close of agency comment on May 11, comments were received from Randy Abrahamson, Spokane Tribe Historic Preservation Officer. He indicated no concern for the project.

Following the agency/department comment period, a Notice of Application was issued on June 8, 2020 by mail to all properties and owners within a 400-foot radius of the subject properties, including within 400-feet of any adjacent properties with the same ownership. Notice was also posted on the subject properties, in the closest library branch, and in the Spokesman Review. No comments were received during the 60-day public comment period.

3. **Public Workshop:** A public workshop with the Spokane Plan Commission was held on June 24th, 2020 during which the particulars of the proposal were presented to the Plan Commission for their consideration and discussion. An online public workshop for the general public was held on July 29, 2020. Questions were answered and comments received. No changes were proposed at either workshop.

## VI. APPLICATION REVIEW AND ANALYSIS

1. **Guiding Principles:** SMC 17G.020.010 provides the following guiding principles for the annual comprehensive plan amendment process:
  - A. Keep the comprehensive plan alive and responsive to the community.
  - B. Provide for simultaneous review of proposals to allow for cumulative impact analysis of all applications on a City-wide basis and in conjunction with budget decisions.
  - C. Make map adjustments based on a foundation in policy language, consistently applying those concepts citywide.
  - D. Honor the community's long-term investment in the comprehensive plan, through public participation and neighborhood planning processes, by not making changes lightly.
  - E. Encourage development that will enable our whole community to prosper and reinforce our sense of place and feeling of community, in an ecologically, economically and socially sustainable manner.
  - F. Amendments to the comprehensive plan must result in a net benefit to the general public.



2. **Review Criteria:** SMC 17G.020.030 provides a list of considerations that are to be used, as appropriate, by the applicant in developing an amendment proposal, by planning staff in analyzing a proposal, by the plan commission and by the city council in making a decision on the proposal. Following each consideration is staff analysis relative to the amendment requested.

- A. **Regulatory Changes:** *Amendments to the comprehensive plan must be consistent with any recent state or federal legislative actions, or changes to state or federal regulations, such as changes to the Growth Management Act, or new environmental regulations.*

Staff Analysis: Staff reviewed and processed the proposed amendment under the most current regulations contained in the Growth Management Act, the Washington State Environmental Policy Act (SEPA), and the Spokane Municipal Code. Staff is unaware of any recent federal, state, or legislative actions with which the proposal would be in conflict, and no comments were received to this effect from any applicable agencies receiving notice of the proposal.

The proposal meets this criterion.

- B. **GMA:** *The change must be consistent with the goals and purposes of the State Growth Management Act.*

Staff Analysis: The Growth Management Act (GMA) details 13 goals to guide the development and adoption of the comprehensive plans and development regulations (RCW 36.70A.020, "Planning Goals"), and these goals guided the City's development of its comprehensive plan and development regulations. No comments received or other evidence in the record indicates inconsistency between the proposed plan map amendment and the goals and purposes of the GMA.

The proposal meets this criterion.

- C. **Financing:** *In keeping with the GMA's requirement for plans to be supported by financing commitments, infrastructure implications of approved comprehensive plan amendments must be reflected in the relevant six-year capital improvement plan(s) approved in the same budget cycle.*

Staff Analysis: There will be no immediate impact to the city budget. It is expected that federal or state grant programs will fund these improvements within the next 20 years.

The proposal meets this criterion.

- D. **Funding Shortfall:** *If funding shortfalls suggest the need to scale back on land use objectives and/or service level standards, those decisions must be made with public input as part of this process for amending the comprehensive plan and capital facilities program.*

Staff Analysis: No evidence of a potential funding shortfall as a result of this proposal exists.

- E. **Internal Consistency:**

1. *The requirement for internal consistency pertains to the comprehensive plan as it relates to all of its supporting documents, such as the development regulations, capital facilities program, shoreline master program, downtown plan, critical area regulations, and any neighborhood planning documents adopted after 2001. In addition, amendments should*



*strive to be consistent with the parks plan, and vice versa. For example, changes to the development regulations must be reflected in consistent adjustments to the goals or policies in the comprehensive plan. As appropriate, changes to the map or text of the comprehensive plan must also result in corresponding adjustments to the zoning map and implementation regulations in the Spokane Municipal Code.*

**Staff Analysis:** The proposal is internally consistent with applicable supporting documents of the Comprehensive Plan as follows:

*Capital Facilities Program.* As described in the staff analysis of Criterion C above, no additional infrastructure or capital expenditures by the City are anticipated for this non-project action, and it is not anticipated that the City's integrated Capital Facilities Program would be affected by the proposal until the City seeks grant funding for construction.

*Miscellaneous Comprehensive Plan Goals and Policies.* Staff have compiled a list of Comprehensive Plan Goals and Policies which bear on the proposal in **Exhibit B** of this report. Further discussion of these policies is provided under section K.2 below.

The proposal meets this criterion.

2. *If a proposed amendment is significantly inconsistent with current policy within the comprehensive plan, an amendment proposal must also include wording that would realign the relevant parts of the comprehensive plan and its other supporting documents with the full range of changes implied by the proposal.*

**Staff Analysis:** The proposal is generally consistent with current Comprehensive Plan policies, as described in further detail in the staff analysis of Criterion K.2 below and other criteria in this report. Therefore, no amendment to policy wording is necessary and this criterion does not apply to the subject proposal.

The proposal meets this criterion.

- F. Regional Consistency:** *All changes to the comprehensive plan must be consistent with the countywide planning policies (CWPP), the comprehensive plans of neighboring jurisdictions, applicable capital facilities or special district plans, the regional transportation improvement plan, and official population growth forecasts.*

**Staff Analysis:** The proposed text amendment is consistent with the railroad and roadway elements of the regional transportation plan.

The proposal meets this criterion.

- G. Cumulative Effect:** *All amendments must be considered concurrently in order to evaluate their cumulative effect on the comprehensive plan text and map, development regulations, capital facilities program, neighborhood planning documents, adopted environmental policies and other relevant implementation measures.*

1. **Land Use Impacts:** *In addition, applications should be reviewed for their cumulative land use impacts. Where adverse environmental impacts are identified, mitigation requirements may be imposed as a part of the approval action.*



2. **Grouping:** *Proposals for area-wide rezones and/or site-specific land use plan map amendments may be evaluated by geographic sector and/or land use type in order to facilitate the assessment of their cumulative impacts.*

Staff Analysis: The City is concurrently reviewing this application and eight other applications for Comprehensive Plan amendments, as part of an annual plan amendment cycle. Six applications are for Land Use Plan Map amendments, two are proposed transportation map amendments to the Comprehensive Plan, and one is a proposed text amendment. When considered together, these various applications do not interact, nor do they augment or detract from each other. The cumulative effects of these various applications are minor.

This proposal meets this criterion.

**H. SEPA:** *SEPA<sup>3</sup> Review must be completed on all amendment proposals and is described in Chapter 17E.050.*

1. **Grouping:** *When possible, the SEPA review process should be combined for related land use types or affected geographic sectors in order to better evaluate the proposals' cumulative impacts. This combined review process results in a single threshold determination for those related proposals.*
2. **DS:** *If a determination of significance (DS) is made regarding any proposal, that application will be deferred for further consideration until the next applicable review cycle in order to allow adequate time for generating and processing the required environmental impact statement (EIS).*

Staff Analysis: The application is under review in accordance with the State Environmental Policy Act (SEPA), which requires that the potential for adverse environmental impacts resulting from a proposal be evaluated during the decision-making process. On the basis of the information contained in the environmental checklist, written comments from local and State departments and agencies concerned with land development within the City, and a review of other information available to the Director of Planning Services, a Determination of Non-Significance was issued on August 24, 2020.

The proposal meets this criterion.

- I. **Adequate Public Facilities:** *The amendment must not adversely affect the City's ability to provide the full range of urban public facilities and services (as described in CFU 2.1 and CFU 2.2) citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.*

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<sup>3</sup> State Environmental Protection Act



Staff Analysis: The proposal would not impact the City's ability to provide transportation facilities at the planned level of service. The City does not measure, nor does it have standards for levels of service at railroad crossings.

The proposal meets this criterion.

- J. **UGA:** *Amendments to the urban growth area boundary may only be proposed by the city council or the mayor of Spokane and shall follow the procedures of the countywide planning policies for Spokane County.*

Staff Analysis: The proposal does not include an expansion to the UGA, thus this criteria does not apply.

The proposal meets this criterion.

K. **Demonstration of Need:**

1. **Policy Adjustments:** *Proposed policy adjustments that are intended to be consistent with the comprehensive plan should be designed to provide correction or additional guidance so the community's original visions and values can better be achieved. The need for this type of adjustment might be supported by findings from feedback instruments related to monitoring and evaluating the implementation of the comprehensive plan.*

Staff Analysis: The information provided by the amendment clarifies the safety issues of at-grade railroad crossings and highlights specific locations within the City where additional safety infrastructure may be required.

The proposal meets this criterion.

2. **Map Changes:** *Changes to the land use plan map (and by extension, the zoning map) may only be approved if the proponent has demonstrated that all of the following are true:*

- a. The designation is in conformance with the appropriate location criteria identified in the comprehensive plan (e.g. compatibility with neighboring land uses, proximity to arterials, etc.);

Staff Analysis: This proposal does not include a map amendment, thus this criterion does not apply.

- b. The map amendment or site is suitable for the proposed designation.

Staff Analysis: This proposal does not include a map amendment, thus this criterion does not apply.

- c. The map amendment implements applicable comprehensive plan policies and subarea plans better than the current map designation.

Staff Analysis: This proposal does not include a map amendment, thus this criterion does not apply.



- 3. Rezones, Land Use Plan Amendment:** *Corresponding rezones will be adopted concurrently with land use plan map amendments as a legislative action of the city council. If policy language changes have map implications, changes to the land use plan map and zoning map will be made accordingly for all affected sites upon adoption of the new policy language. This is done to ensure that the comprehensive plan remains internally consistent and to preserve consistency between the comprehensive plan and supporting development regulations.*

Staff Analysis: This proposal does not include a rezone, thus this criterion does not apply.

The proposal meets this criterion.

## **VII. CONCLUSION**

The proposal has been processed and considered according to the requirements of the Spokane Municipal Code. According to the information provided above and the whole of the administrative record, the proposal appears to comply with the considerations for a comprehensive plan amendment as provided in SMC 17G.020.030.

Following the close of public testimony and deliberations regarding conclusions with respect to the review criteria and decision criteria detailed in SMC Chapter 17G.020, Plan Commission will need to make a recommendation to City Council for approval or denial of the requested amendment to Chapter 4 of the City's Comprehensive Plan.

## **VIII. STAFF RECOMMENDATION**

Staff recommends that the Plan Commission and City Council approve this proposal.

## **IX. LIST OF EXHIBITS**

- |                                        |                                           |
|----------------------------------------|-------------------------------------------|
| A. Proposed Text Amendments            | D. SEPA Checklist                         |
| B. List of Relevant Comp Plan Policies | E. SEPA Determination of Non-Significance |
| C. Application Materials               | F. Agency Comments                        |





***Proposed New Text – Chapter 4, Transportation***

*The following text is proposed to be added to Chapter 4, Transportation, of the Comprehensive Plan. All of this text is new, and would begin at the end of page 4-71, immediately following the subsection on bridge projects.*

**Railroad Crossing Projects**

There are many at-grade rail crossings within the city. Most of these already have warning devices and gates installed to provide increased protection for vehicles, cyclists, and pedestrians on the road. However, there are a few arterial crossing locations that could use further improvements, as funding becomes available. These locations are already equipped with warning lights and bells. However, due to increased traffic on the roadway or on the rail line, the locations listed in table TR-9 would benefit from additional safety measures.

| TABLE TR 9 – RAILROAD CROSSING PROJECT LIST           |                                                                                                                           |
|-------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------|
| Project Name                                          | Needed Improvements                                                                                                       |
| Havana Street crossing of UP<br>(n/o Sprague Avenue)  | Widen crossing for sidewalk, install gates, update preemption equipment and track circuit for the adjacent traffic signal |
| Freya Street crossing of UP<br>(n/o Sprague Avenue)   | Install gates, update preemption equipment and track circuit for the adjacent traffic signal                              |
| Mission Street crossing of BNSF<br>(e/o Perry Street) | Install gates, update preemption equipment and track circuit for the adjacent traffic signal                              |

*Notes: UP = Union Pacific Railroad. BNSF = Burlington Northern and Santa Fe Railroad.*





The following policies of the Comprehensive Plan relate to application Z20-045COMP. The full text of the Comprehensive Plan can be found at [www.shapingspokane.org](http://www.shapingspokane.org).

## **Chapter 4—Transportation**

### **TR 2 Transportation Supporting Land Use**

Maintain an interconnected system of facilities that allows travel on multiple routes by multiple modes, balancing access, mobility and place-making functions with consideration and alignment with the existing and planned land use context of each corridor and major street segment.

#### *Key Actions:*

- a. Establish and maintain Street Design Standards and Guidelines reflecting best practices to implement designs that effectively support multi-modal transportation while supporting local context and existing and planned land uses.
- b. Develop transportation decisions, strategies and investments in coordination with land use goals that support the Land Use Plan and Center and Corridor strategy.
- c. Require a transportation plan (which includes connectivity and circulation) as part of any subdivision, Planned Unit Development (PUD), institutional master plan, or other major land use decision – Conduct transportation plans when needed for larger developments or other land uses of appropriate size.

### **TR 5 Active Transportation**

Identify high-priority active transportation projects to carry on completion/ upgrades to the active transportation network.

#### *Key Actions*

- a. Ensure that the pedestrian and bicycle networks provide direct connections between major activity centers and transit stops and stations.
- b. The planning, design and construction of transportation projects should maintain or improve the accessibility and quality of existing and planned pedestrian and bicycle facilities.
- c. Implement a network of low vehicle volume, bike-friendly routes throughout the city.
- d. Support the development of a bike-share program within the city core.
- e. Seek grant funding for projects and programs such as Safe Routes to School, Transportation Alternatives, and other active transportation initiatives.
- f. Utilize the Bicycle Plan and the Pedestrian Plan to guide the location and type of bicycle and pedestrian facilities developed in Spokane to:



- i. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages to transit stops and stations.
  - ii. Provide safe, attractive, convenient and quality pedestrian and bicycle linkages between major activity areas where features that act as barriers prevent safe and convenient access.
  - iii. Provide safe, attractive, convenient and quality pedestrian and bicycle facilities and an aesthetically pleasing environment on bridges.
  - iv. Enhance the pedestrian and bicycle environment along routes to schools to provide a safe walking and riding environment for children. Means of accomplishing this include:
    - encouraging school routes not to cross arterials;
    - having user-activated signals at arterial intersections;
    - implementing safety patrols with traffic-control signs at busy intersections;
    - working with schools to promote walking groups; and
    - strengthening and enforcing pedestrian right-of-way laws.
  - v. Enhance the pedestrian, bicycle and transit environment along routes to desirable destinations for seniors.
  - vi. Enhance the pedestrian, bicycle and transit environment along routes in communities with a high percentage of underserved populations.
  - vii. Provide safe bicycle and pedestrian access to city parks from surrounding neighborhoods.
- g. Provide viable facilities for active transportation modes as alternatives to driving.
- i. Ensure gaps in the bicycle network are identified and prioritized to complete and expand the connected bicycle network.
  - ii. Ensure sidewalk gaps are not present and provide for safe pedestrian circulation within the city. Wherever possible, this should be in the form of sidewalks with a pedestrian buffer strip or other separation from the street.
  - iii. Use pedestrian safety strategies on high bicycle and pedestrian traffic corridors.
  - iv. Establish and maintain crosswalks at key locations where active transportation facilities cross collector and arterial roadways.
- h. h. Provide secure parking for bicyclists at key destinations (i.e. Downtown, identified Centers and Corridors, schools and universities, community centers, key transit locations) and ensure future developments include bicycle parking on site that adheres to city-established design and siting standards.
- i. Work with local and regional partners to implement the “Spokane County Wayfinding and Gateway Feature Placement & Design Plan”.



- j. Coordinate with other departments and partner agencies to combine related projects for the purpose of cost-sharing.

#### TR 8 Moving Freight

Identify a freight network that respects needs of businesses as well as neighborhoods. Maintain an appropriate arterial system map that designates a freight network that enhances freight mobility and operational efficiencies, and increases the city's economic health. The needs for delivery and collection of goods at businesses by truck should be incorporated into the freight network, and the national trend of increased deliveries to residences anticipated.

##### *Key Actions:*

- a. Designate truck freight routes through the city that provide appropriate access without compromising neighborhood safety and livability.
- b. Periodically work with commercial freight mapping services to update their truck route information.
- c. Provide an easy to find freight map on the city's website.
- d. Explore establishing delivery time designations/restrictions in specified areas. City of Spokane Comprehensive Plan 4-25
- e. Support intermodal freight transfer facilities (land to air, rail to roadway, interstate trucking to local delivery).

#### TR 9 Promote Economic Opportunity

Focus on providing efficient and affordable multi-modal access to jobs, education, and workforce training to promote economic opportunity in the city's designated growth areas, develop "Great Streets" that enhance commerce and attract jobs.

##### *Key Actions:*

- a. Ensure street designs support business activity-and thus jobs creation-to ensure that travelers feel comfortable to stop and shop.
- b. Coordinate closely with STA and area colleges and universities to provide convenient, cost-efficient transit service for students.
- c. Use new technology when feasible to increase efficiency in all transportation modes, such as:
  - i. Intelligent feedback to users;
  - ii. Dynamic traffic signals;
  - iii. Priority transit routes and signaling; and,
  - iv. Information sharing about capacity.
- d. Coordinate closely with STA to identify opportunities for service improvements in designated land use areas.



- e. Coordinate with Visit Spokane and other relevant groups to support and promote bicycle tourism in the city and region.
- f. Partner with business entities and organizations to educate them and their members on the economic benefits of transit and active transportation oriented development.
- g. Implement the city's bicycle master plan for improved city-wide mobility.

#### TR 10 Transportation System Efficiency & Innovation

Develop and manage the transportation system to function as efficiently as possible while exploring innovative opportunities and technologies.

##### *Key Actions:*

- a. Develop Access Management Strategies for arterials.
- b. Ensure coordinated, efficient and safe movement of all roadway users through proper signal spacing traffic control timing, and other intersection controls such as roundabouts and new traffic control coordinating technology where appropriate.
- c. Implement Intelligent Transportation System (ITS) improvements as identified by the Spokane Regional Transportation Management Center (SRTMC). City of Spokane Comprehensive Plan 4-26
- d. Work with WSDOT to implement TDM, ITS, and transportation system management strategies developed through the Corridor Sketch Initiative (CSI).

#### TR 12 Prioritize & Integrate Investments

Prioritize investments based on the adopted goals and priorities outlined in the comprehensive plan.

##### *Key Actions:*

- a. Maintain and update as needed the metrics tied to the long range transportation prioritization matrix used to help determine transportation system capital investments.
- b. Link transportation investments with investments made under the Integrated Clean Water Plan to manage stormwater and wastewater.
- c. Utilize a least-cost planning approach in prioritizing and integrating the city's investments in infrastructure.

#### TR 13 Infrastructure Design

Maintain and follow design guidelines (including national guidelines such as MUTCD, NACTO, AASHTO) reflecting best practices that provide for a connected infrastructure designed for our climate and potential emergency management needs, and respecting the local context. Local context may guide signage and elements such as traffic calming, street furniture, bicycle parking, and community spaces. Accessibility guidelines and emergency management needs will be maintained. City of Spokane Comprehensive Plan 4-27



*Key Actions:*

- a. Require that Urban Context streets be designed to provide a pleasant environment for walking and other uses of public space, including such elements as shade trees; plantings; well-designed benches, trash receptacles, news racks, and other furniture; pedestrian-scaled lighting fixtures as appropriate; wayfinding signage; integrated transit shelters; public art; and other amenities.
- b. Maintain street design guidelines reflecting best practices to implement designs that effectively manage traffic flow, reduce the need for street expansions, and make roadways safe for all road users, while ensuring designs correspond with local context.
- c. Collaborate with key agencies to plan the locations of arterials, ensuring compatibility with and satisfy the needs of existing and future land uses.

TR 19 Plan Collaboratively

Work with partner agencies to achieve a regional transportation plan that meets the goals and requirements of the Growth Management Act (GMA) but also reflects the visions and values of the City of Spokane.

*Key Actions:*

- a. Coordinate with SRTC and neighboring jurisdictions on transportation planning, projects and policies to ensure efficient, multi-modal transportation of people and goods between communities regionally. City of Spokane Comprehensive Plan 4-30
- b. Coordinate the setting and maintaining of transportation level of service standards with other agencies and private providers of transportation to ensure coordination and consistency when possible.
- c. Coordinate with WSDOT in areas where Highways of Statewide Significance (HSS) intersect/impact the local roadway network.
- d. Use the adopted Countywide Planning Policies (CWPP) as additional guidance for transportation planning.
- e. Protect the operations of Fairchild Air Force Base, Spokane International Airport and Felts Field with compatible land use regulations and ensure planning is coordinated and consistent with the airfields' respective Master Plans.
- f. Share information between transportation entities on a regular basis and during appropriate phases of projects and comprehensive plan updates and amendments.
- g. Coordinate with Spokane Transit Authority to ensure and support an efficient transit system.

TR 21 Safe & Healthy Community Education & Promotion Campaigns

Promote healthy communities by providing a transportation system that protects and improves environmental quality and partner with other agencies to implement innovative and effective measures to improve safety that combine engineering, education, evaluation, and enforcement.



*Key Actions:*

- a. Develop educational campaigns that promote alternatives to driving alone for the purpose of reducing environmental impacts and travel costs.
- b. Develop partnerships with local agencies to implement public safety campaigns aimed at driver, pedestrian, and bicyclist awareness of and respect for each other. Campaigns should focus on maintaining safe speeds, practicing safe behaviors on the road, and calling attention to vulnerability of some road users.
- c. Develop partnerships to educate residents on the economic and health benefits of active transportation. d. Provide education on the transportation needs of the entire community, the benefits of transportation alternatives, and the rights and responsibilities of sharing the road.





# General Application

Rev.20180104

## DESCRIPTION OF PROPOSAL

A Comprehensive Plan text amendment to add a discussion of railway crossing safety into Chapter 4, Transportation, and listing various known crossings potentially needing update and improvement in the City.

Address of Site Proposal (if not yet assigned, obtain address from Public Works before submitting application):

Text Amendment - No Address

## APPLICANT

Name: Inga Note, Senior Traffic Planning Engineer, City of Spokane

Address: \_\_\_\_\_

Phone: 509-625-6331 Email: inote@spokanecity.org

## PROPERTY OWNER

Name: City of Spokane Public Streets Rights-of-Way

Address: 808 W Spokane Falls Blvd

Phone: 509-625-6331 Email: inote@spokanecity.org

## AGENT

Name: NOT APPLICABLE

Address: \_\_\_\_\_

Phone: \_\_\_\_\_ Email: \_\_\_\_\_

Assessor's Parcel Numbers: NOT APPLICABLE

Legal Description of Site: NOT APPLICABLE









# Comprehensive Plan Amendments

## Threshold Review

### **Pre-application:**

*The first step in applying for an amendment to the City's Comprehensive Plan is to submit a threshold review application. Prior to submitting this application, a private applicant is required to schedule a no-fee pre-application conference with staff. In the case of a map amendment, the applicant is also required to make reasonable efforts to schedule a meeting with the appropriate neighborhood council(s) and document any support or concerns expressed by the neighborhood council(s). Applications are accepted through October 31 each year, during business hours. Applicants are strongly encouraged to make an appointment with Planning Department staff prior to submitting an application.*

### **Description of the Proposed Amendment:**

- In the case of a proposed text amendment, please describe the proposed amendment and provide suggested amendment language.
- In the case of a map amendment, please describe using parcel number(s), address, and a description including size, and maps.

***In addition to describing the proposal, please describe how your applications satisfies the threshold review criteria in SMC 17G.020.026, which are restated below. You may need to use a separate piece of paper.***

1. Describe how the proposed amendment is appropriately addressed as a Comprehensive Plan Amendment.  
*This amendment would add language regarding safety improvements for at-grade railroad crossings in the city. By highlighting railway safety in the Comprehensive Plan it is more likely that the City will be able to secure funding for physical improvements in the future.*
2. The proposed amendment does not raise policy or land use issues that are more appropriately addressed by an ongoing work program approved by the City council or by a neighborhood or subarea planning process.  
*There is no work program currently planned for this.*
3. The proposed amendment can be reasonably reviewed within the resources and time frame of the Annual Comprehensive Plan Amendment Work Program.  
*Yes it can. Adequate time and materials are available within the affected departments' work plans for the year to develop this text amendment within the required timeframe.*
4. In the case of a private application for a land use map change, nearby properties may also seem to be candidates for amendment. At the time of docketing or during plan commission review, expansion of the geographic scope of an amendment proposal may be considered, shared characteristics with nearby, similarly situated property may be identified and the expansion is the minimum necessary to include properties with those shared characteristics. Has the applicant had any outreach to surrounding property owners whose property may be so situated?  
*Not applicable.*
5. Describe how the proposed amendment is consistent with current general policies in the comprehensive plan for site-specific amendment proposals. The proposed amendment must be consistent with policy implementation in the Countywide Planning policies, the GMA, or other state or federal law, and the WAC.  
*The Comprehensive Plan does not include any policies that would be affected by this proposal.*

6. The proposed amendment is not the same as or substantially similar to a proposal that was considered in



the previous year's threshold review process, but was not included in the Annual Comprehensive Plan Amendment Work Program, unless additional supporting information has been generated.

*This is a new proposal, not one previously considered.*

7. If this change is directed by state law or a decision of a court or administrative agency, please describe.

*There are no known local, state, or federal laws calling for this change.*

8. Please provide copy of agenda or other documentation of outreach to neighborhood council made prior to application.

*This was discussed briefly at the Council study session on 2/13/2020 and all neighborhoods were notified via email of its general aspects in an email from the Department of Neighborhood and Planning Services on February 17, 2020 (attached).*

Planning & Development Services, 808 West Spokane Falls Boulevard, Spokane, WA 99201-3336  
[my.spokanecity.org](http://my.spokanecity.org) | Phone: 509.625.6300

(Rev Sept 2017)





# Comprehensive Plan or Land Use Code Amendment

Pre-Application

Rev.20180102

## DESCRIPTION OF THE PROPOSED AMENDMENT:

*(Please check the appropriate box(es))*

- |                                                               |                                                      |
|---------------------------------------------------------------|------------------------------------------------------|
| <input checked="" type="checkbox"/> Comprehensive Plan Change | <input type="checkbox"/> Land Use Designation Change |
| <input type="checkbox"/> Regulatory Code Text Change          | <input type="checkbox"/> Area-Wide Rezone            |

**Please respond to these questions on a separate piece of paper. Incomplete answers may jeopardize your application's chances of being reviewed during this amendment cycle.**

### 1. General Questions *(for all proposals)*:

- a. Summarize the general nature of the proposed amendment.
- b. Why do you feel this change is needed?
- c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?
- d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?
- e. For map amendments:
  1. What is the current Land Use designation and zoning for each affected parcel?
  2. What is the requested Land Use designation and zoning for each affected parcel?
  3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc.
- f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?
- g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?
- h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?  
☐ Yes ☐ No
- i. If yes, please answer the following questions:
  1. When was the amendment proposal submitted?
  2. Was it submitted as a consistent amendment or an inconsistent amendment?
  3. What were the Plan Commission recommendation and City Council decision at that time?
  4. Describe any ways that this amendment proposal varies from the previously considered version.



## Comprehensive Plan Amendment – Pre-Application At-Grade Rail Crossing Improvements – 2020

### 1. General Questions:

a. Summarize the general nature of the proposed amendment.

*This amendment would add language regarding safety improvements to at-grade rail crossings in the City.*

b. Why do you feel this change is needed?

*This will make the grade crossing improvement projects more competitive for funding under certain grant programs.*

c. In what way(s) is your proposal similar to or different from the fundamental concepts contained in the comprehensive plan?

*Not applicable.*

d. For text amendments: What goals, policies, regulations or other documents might be changed by your proposal?

*The proposed text not likely to include or modify any goals or policies, though it might list certain crossings in the City requiring update or improvement.*

e. For map amendments:

1. What is the current Land Use designation and zoning for each affected parcel? *Not Applicable*

2. What is the requested Land Use designation and zoning for each affected parcel? *Not Applicable*

3. Describe the land uses surrounding the proposed amendment site(s); e.g. land use type, vacant/occupied, etc. *Not Applicable*

f. Do you know of any existing studies, plans or other documents that specifically relate to or support your proposal?

*No.*

g. Why did you decide to pursue a comprehensive plan amendment rather than address your concern through some other aspect of the Development Services department's work program (e.g. neighborhood planning, public input on new regulations, etc.)?

*Discussion of this topic in the Comprehensive Plan is an important first step towards improving safety at these crossings throughout the City.*

h. Has there been a previous attempt to address this concern through a comprehensive plan amendment?

*No.*

i. If yes, please answer the following questions: *Not Applicable*

1. When was the amendment proposal submitted?

2. Was it submitted as a consistent amendment or an inconsistent amendment?

3. What were the Plan Commission recommendation and City Council decision at that time?

4. Describe any ways that this amendment proposal varies from the previously considered version.





# Z20-045COMP

## (Text Amendment - Chapter 4 - Railway Crossing Safety)

2019/2020 Comprehensive Plan Amendment Proposals

Application **Z20-045COMP**

Drawn: 2/18/2020

THIS IS NOT A LEGAL DOCUMENT

The information shown on this map is compiled from various sources and is subject to constant revision. Information shown on this map should not be used to determine the location of facilities in relationship to property lines, section lines, streets, etc.

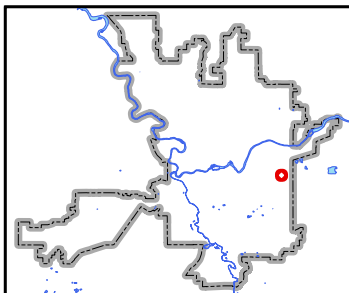
### Legend

- +— Railroad
- Curb Line

The proposed amendment is only to the text of the Comprehensive Plan. The railroad crossing in this figure is provided for informational purposes only. The effect of the text amendment would be citywide, concerning any crossing that may require additional safety improvements.

All photographs were taken in 2018.

### PHOTOGRAPH LOCATION

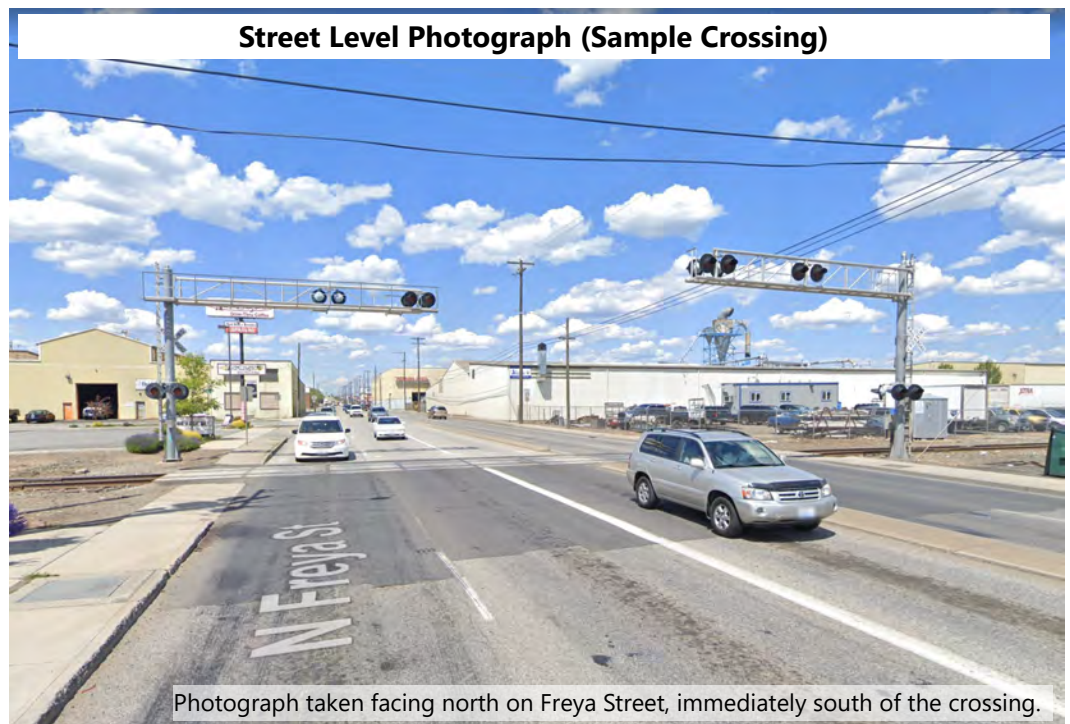


Neighborhood and Planning Services  
Drawn By: Kevin Freibott

### Aerial Overview (Sample Crossing)



### Street Level Photograph (Sample Crossing)



Photograph taken facing north on Freya Street, immediately south of the crossing.



**State Environmental Policy Act (SEPA)  
ENVIRONMENTAL CHECKLIST****File No. Z20-045COMP****PLEASE READ CAREFULLY BEFORE COMPLETING THE CHECKLIST!****Purpose of Checklist:**

The State Environmental Policy Act (SEPA) chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An Environmental Impact Statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

**Instructions for Applicants:**

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

**Use of checklist for nonproject proposals:**

Complete this checklist for nonproject proposals, even though questions may be answered "*does not apply*."

IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (Part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.



**A. BACKGROUND**

1. Name of proposed project: **Railway Crossing Safety Text Amendment, Chapter 4**
2. Applicant: **Inga Note**
3. Address: **808 W Spokane Falls Blvd**  
 City/State/Zip: **Spokane, WA 99201**  
 Phone: **509-625-6331**  
 Agent or Primary Contact: **same**  
 Address: \_\_\_\_\_  
 City/State/Zip: \_\_\_\_\_ Phone: \_\_\_\_\_  
 Location of Project: **This project would affect planned improvements for at-grade railway crossings in the City.**  
 Address: **n/a**  
 Section: \_\_\_\_\_ Quarter: \_\_\_\_\_ Township: \_\_\_\_\_ Range: \_\_\_\_\_  
 Tax Parcel Number(s) **None (affects City Rights-of-Way)**
4. Date checklist prepared: **4/13/2020**
5. Agency requesting checklist: **City of Spokane**
6. Proposed timing or schedule (including phasing, if applicable): **Comprehensive plan amendments are expected to be completed by December 2020.**
7. a. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. **While the proposal would amend the text of Chapter 4 to call for increased safety for at-grade railway crossings, no immediate future construction or reconstruction is planned at this time. Physical modification of crossings would be analyzed for their environmental effects at the time of design and construction.**  
 b. Do you own or have options on land nearby or adjacent to this proposal? If yes, explain. **No**
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. **No specific studies or analyses have been prepared.**
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. **None.**
10. List any government approvals or permits that will be needed for your proposal, if known. **City Council approval of a Comprehensive Plan Amendment.**



11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. **The proposal consists of a text amendment to Chapter 4, Transportation, regarding pedestrian and vehicular safety at at-grade railway crossings in the City of Spokane. This amendment would highlight the need for increase safety improvements like crossing gates at various locations. No immediate or near-term physical changes to any specific crossings are proposed at this time. Future construction or re-construction of crossings in Spokane would be subject to additional SEPA review at the time of design.**

12. Location of the proposal: Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit application related to this checklist. **Any at-grade railway crossings in the City. Three specific locations under consideration for improvements include UPRR crossing #809122U on Freya Street, UPRR crossing #809124H on Havana Street and BNSF crossing #065984U on Mission Avenue.**

13. Does the proposed action lie within the Aquifer Sensitive Area (ASA)? The General Sewer Service Area? The Priority Sewer Service Area? The City of Spokane? (See: Spokane County's ASA Overlay Zone Atlas for boundaries.) **Yes, all crossings are within the ASA, the sewer service area, and the City of Spokane.**

14. The following questions supplement Part A.

a. Critical Aquifer Recharge Area (CARA) / Aquifer Sensitive Area (ASA)

(1) Describe any systems, other than those designed for the disposal of sanitary waste installed for the purpose of discharging fluids below the ground surface (includes systems such as those for the disposal of stormwater or drainage from floor drains). Describe the type of system, the amount of material to be disposed of through the system and the types of material likely to be disposed of (including materials which may enter the system inadvertently through spills or as a result of firefighting activities). **None at this time.**



(2) Will any chemicals (especially organic solvents or petroleum fuels) be stored in aboveground or underground storage tanks? If so, what types and quantities of material will be stored? **N/A, Non-Project Action (text change).**

(3) What protective measures will be taken to insure that leaks or spills of any chemicals stored or used on site will not be allowed to percolate to groundwater. This includes measures to keep chemicals out of disposal systems. **N/A, Non-Project Action (text change).**

(4) Will any chemicals be stored, handled or used on the site in a location where a spill or leak will drain to surface or groundwater or to a stormwater disposal system discharging to surface or groundwater? **N/A, Non-Project Action (text change).**

b. Stormwater

(1) What are the depths on the site to groundwater and to bedrock (if known)? **Varies throughout the City.**

(2) Will stormwater be discharged into the ground? If so, describe any potential impacts. **N/A, Non-Project Action (text change).**

## B. ENVIRONMENTAL ELEMENTS

### 1. Earth

a. General description of the site (check one):

☒ **Flat**   ☐ Rolling   ☐ Hilly   ☐ Steep slopes   ☐ Mountainous

Other:

b. What is the steepest slope on the site (approximate percent slope)? **Generally flat.**

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. **Varies by location.**

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. **N/A, Non-Project Action (text change).**



- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill: **N/A, Non-Project Action (text change).**
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. **No, Non-Project Action (text change).**
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt, or buildings)? **N/A, Non-Project Action (text change).**
- h. Proposed measures to reduce or control erosion or other impacts to the earth, if any: **None.**

## 2. Air

- a. What type of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. **The proposal calls for potential future safety infrastructure like gates and lights. No increased emissions are expected from these features.**
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. **No.**
- c. Proposed measures to reduce or control emissions or other impacts to air, if any: **None, Non-Project Action (text change).**

## 3. Water

- a. SURFACE WATER:
  - (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. **N/A, Non-Project Action (text change). Future construction will be analyzed for effects to surface water at the time of design and development.**
  - (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. **N/A, Non-Project Action (text change).**



- (3) Estimate the amount of fill and dredge material that would be placed in or removed from the surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. **N/A, Non-Project Action (text change).**
- (4) Will the proposal require surface water withdrawals or diversions? If yes, give general description, purpose, and approximate quantities if known. **N/A, Non-Project Action (text change).**
- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. **Varies by location.**
- (6) Does the proposal involve any discharge of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. **No. Non-Project Action (text change).**

b. GROUNDWATER:

- (1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. **No.**
- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. **None, Non-Project Action (text change).**

c. WATER RUNOFF (INCLUDING STORMWATER):

- (1) Describe the source of runoff (including stormwater) and method of collection and disposal if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. **N/A, Non-Project Action (text change).**



(2) Could waste materials enter ground or surface waters? If so, generally describe. **No, Non-Project Action (text change).**

(3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. **N/A, Non-Project Action (text change).**

d. PROPOSED MEASURES to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any. **None. Non-Project Action (text change).**

#### 4. Plants

a. Check the type of vegetation found on the site:

Deciduous tree: ☐ alder ☐ maple ☐ aspen

Other: \_\_\_\_\_

Evergreen tree: ☐ fir ☐ cedar ☐ pine

Other: \_\_\_\_\_

☐ Shrubs ☐ Grass ☐ Pasture ☐ Crop or grain

☐ Orchards, vineyards or other permanent crops

Wet soil plants: ☐ cattail ☐ buttercup ☐ bullrush ☐ skunk cabbage

Other: \_\_\_\_\_

Water plants: ☐ water lily ☒ eelgrass ☐ milfoil

Other: \_\_\_\_\_

Other types of vegetation: **At-grade railway crossings are generally located within city street rights-of-way and railroad easements. Vegetation commonly consists of only scrub brush and other urban weeds.**

b. What kind and amount of vegetation will be removed or altered? **N/A, Non-Project Action (text change).**

c. List threatened and endangered species known to be on or near the site. **None. All locations are paved streets and railway easements.**



d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: **None.**

e. List all noxious weeds and invasive species known to be on or near the site. **N/A, Non-Project Action (text change).**

## 5. Animals

a. Check and List any birds and other animals which have been observed on or near the site or are known to be on or near the site:

Birds: ☐ hawk ☐ heron ☐ eagle ☐ songbirds

Other: \_\_\_\_\_

Mammals: ☐ deer ☐ bear ☐ elk ☐ beaver

Other: \_\_\_\_\_

Fish: ☐ bass ☐ salmon ☐ trout ☐ herring ☐ shellfish

Other: \_\_\_\_\_

Other (not listed in above categories): **Typical urban wildlife may exist on various sites within open areas.**

b. List any threatened or endangered animal species known to be on or near the site.  
**None.**

c. Is the site part of a migration route? If so, explain. **Unknown.**

d. Proposed measures to preserve or enhance wildlife, if any: **None.**

e. List any invasive animal species known to be on or near the site. **None.**

## 6. Energy and natural resources

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. **N/A, Non-Project Action (text change).**



- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. **N/A, Non-Project Action (text change).**
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: **None.**

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe. **Proposed safety improvements like gates and lighting would not emit any hazardous substances or waste.**

- (1) Describe any known or possible contamination at the site from present or past uses. **None.**
- (2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. **N/A, Non-Project Action (text change).**
- (3) Describe any toxic or hazardous chemicals/conditions that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. **N/A, Non-Project Action (text change).**
- (4) Describe special emergency services that might be required. **None.**
- (5) Proposed measures to reduce or control environmental health hazards, if any: **None.**

## b. NOISE:

- (1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? **Common traffic noise from existing roadways.**
- (2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours



noise would come from the site. **Crossing signal bells, in most cases already existing on site.**

(3) Proposed measure to reduce or control noise impacts, if any: **None.**

## 8. Land and shoreline use

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. **All sites consist of existing City Rights of Way or railroad easements serving nearby properties with access.**
- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? **No.**
  - 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: **N/A, Non-Project Action (text change).**
- c. Describe any structures on the site. **None.**
- d. Will any structures be demolished? If so, which? **No.**
- e. What is the current zoning classification of the site? **Varies depending on location. In many cases, there is no current zoning as the site is within City or Railroad rights-of-way.**
- f. What is the current comprehensive plan designation of the site? **Varied depending on location. In many cases there is no land use designated as the site is within City or Railroad rights-of-way.**
- g. If applicable, what is the current shoreline master program designation of the site? **N/A, Non-Project Action (text change).**
- h. Has any part of the site been classified as a critical area by the city or the county? If so, specify. **No.**
- i. Approximately how many people would reside or work in the completed project? **None.**



- j. Approximately how many people would the completed project displace? **None.**
- k. Proposed measures to avoid or reduce displacement impacts, if any: **None.**
- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: **None. Project is consistent.**
- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any: **None.**

## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. **None.**
- b. Approximately how many units, if any, would be eliminated? Indicate whether high-, middle- or low-income housing. **None.**
- c. Proposed measures to reduce or control housing impacts, if any: **None.**

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? **N/A, Non-Project Action (text change).**
- b. What views in the immediate vicinity would be altered or obstructed? **N/A, Non-Project Action (text change).**
- c. Proposed measures to reduce or control aesthetic impacts, if any: **None.**

## 11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? **Future improvements may include signal lights and other similar improvements. The environmental impact of those lights would address in future SEPA analysis at the time of construction.**
- b. Could light or glare from the finished project be a safety hazard or interfere with views? **No.**
- c. What existing off-site sources of light or glare may affect your proposal? **None.**



- d. Proposed measures to reduce or control light and glare impacts, if any: **None.**

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? **Varies throughout City.**
- b. Would the proposed project displace any existing recreational uses? If so, describe. **No.**
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: **None.**

## 13. Historic and cultural preservation

- a. Are there any buildings, structures, or sites, located on or near the sited that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. **N/A, Non-Project Action (text change).**
- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. **N/A, Non-Project Action (text change).**
- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archaeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. **None.**
- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. **None.**

## 14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. **Varies. N/A, Non-Project Action (text change).**
- b. Is site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop **N/A, Non-Project Action (text change).**
- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? **None.**



Evaluation for  
Agency Use Only

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). **The proposal may result in new railway crossing infrastructure installed along certain City rights-of-way.**
- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail or air transportation? If so, generally describe. **The proposed amendment concerns all at-grade railway crossings, naturally located adjacent to rail transportation.**
- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and non-passenger vehicles). What data or transportation models were used to make these estimates? **None.**

*(Note: to assist in review and if known, indicate vehicle trips during PM peak, AM Peak, and Weekday (24 hours).)*

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, general describe. **No.**
- h. Proposed measures to reduce or control transportation impacts, if any: **None.**

## 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. **No.**
- b. Proposed measures to reduce or control direct impacts on public services, if any: **None.**



## 16. Utilities

a. Check utilities currently available at the site:

☒ electricity

☒ natural gas

☒ water

☒ refuse service

☒ telephone

☒ sanitary sewer

☐ septic system

Other: \_\_\_\_\_  
\_\_\_\_\_

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed: **None.**



**C. SIGNATURE**

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* must withdraw any determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/15/2020

Signature:  A1675FE85B1948D...

***Please Print or Type:***

Proponent: City of Spokane  
**Primary Staff Contact: Inga Note, Integrated Capital Management**

Address: 808 W Spokane Falls Blvd

Phone: 509-625-6331

Person completing form (if different from proponent):

Phone: \_\_\_\_\_ Address: \_\_\_\_\_  
 \_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- ☒ A. there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- ☐ B. probable significant adverse environmental impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- ☐ C. there are probable significant adverse environmental impacts and recommends a Determination of Significance.



**D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS*****(Do not use this sheet for project actions)***

Because these questions are very general, it may be helpful to read them in conjunction with the list of elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise? **Future reconstruction of safety improvements may generate temporary construction noise, subject to the City's noise ordinance.**

Proposed measures to avoid or reduce such increases are: **None.**

2. How would the proposal be likely to affect plants, animals, fish or marine life? **As any potentially affected crossings already exist, no effect is expected.**

Proposed measures to protect or conserve plants, animals, fish or marine life are: **None.**

3. How would the proposal be likely to deplete energy or natural resources? **The proposal is not expected to deplete these resources.**

Proposed measures to protect or conserve energy and natural resources are: **None.**

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection, such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains or prime farmlands? **As any potentially affected crossings already exist, no effect is expected.**

Proposed measures to protect such resources or to avoid or reduce impacts are: **None.**

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans? **As any potentially affected crossings already exist, no effect is expected.**

Proposed measures to avoid or reduce shoreline and land use impacts are: **None.**



Evaluation for  
Agency Use Only

6. How would the proposal be likely to increase demands on transportation or public services and utilities? **As the proposal would call for safety improvements at existing crossings, no increased demand for transportation or public services are required. Minor amounts of electrical utility service may be needed for lighting and warning systems.**

Proposed measures to reduce or respond to such demand(s) are: **None.**

7. Identify, if possible, whether the proposal may conflict with local, state or federal laws or requirements for the protection of the environment. **The proposal is not expected to conflict with any local, state, or federal laws.**



## C. SIGNATURE

I, the undersigned, swear under penalty of perjury that the above responses are made truthfully and to the best of my knowledge. I also understand that, should there be any willful misrepresentation or willful lack of full disclosure on my part, the *agency* may withdraw any Determination of Nonsignificance that it might issue in reliance upon this checklist.

Date: 4/15/2020

Signature: \_\_\_\_\_

DocuSigned by:  
*Inga Note*  
A1675FE85B1948D...

***Please Print or Type:***

Proponent: City of Spokane

Primary Staff Contact: Inga Note, Integrated Capital Management

Address: 808 W Spokane Falls Blvd

Phone: 509-625-6331

Person completing form (if different from proponent): \_\_\_\_\_

Phone: \_\_\_\_\_ Address: \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

**FOR STAFF USE ONLY**

Staff member(s) reviewing checklist: Kevin Freibott

Based on this staff review of the environmental checklist and other pertinent information, the staff concludes that:

- A. ☒ there are no probable significant adverse impacts and recommends a Determination of Nonsignificance.
- B. ☐ probable significant adverse impacts do exist for the current proposal and recommends a Mitigated Determination of Nonsignificance with conditions.
- C. ☐ there are probable significant adverse environmental impacts and recommends a Determination of Significance.



## NONPROJECT DETERMINATION OF NONSIGNIFICANCE

**FILE NO(S):** Z20-045COMP

**PROPONENT:** City of Spokane

**DESCRIPTION OF PROPOSAL:** Amendment of Chapter 4, Transportation, of the Comprehensive Plan highlighting the need for enhanced safety features at at-grade railway crossings in the City of Spokane and delineating locations where safety improvements may be necessary. No actual construction is proposed at this time.

**LOCATION OF PROPOSAL, INCLUDING STREET ADDRESS, IF ANY:** The proposal is a city-wide text amendment and would affect various locations throughout the City. Details on the specific amendments to be made to the text will be made available at the website identified below:

<http://my.spokanecity.org/projects/2019-2020-proposed-comprehensive-plan-amendments/>

**LEAD AGENCY:** City of Spokane

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

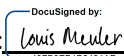
- [ ] There is no comment period for this DNS.
- [ ] This DNS is issued after using the optional DNS process in section 197-11-355 WAC. There is no further comment period on the DNS.
- [ X ] This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for at least 14 days from the date of issuance (below). Comments regarding this DNS must be submitted no later than 5 p.m. on September 7, 2020 if they are intended to alter the DNS.

\*\*\*\*\*

**Responsible Official:** Louis Meuler

**Position/Title:** Interim Director, Planning Services **Phone:** (509) 625-6300

**Address:** 808 W. Spokane Falls Blvd., Spokane, WA 99201

**Date Issued:** August 24, 2020 **Signature:**  \_\_\_\_\_  
43FB9EB1B2494AF...

\*\*\*\*\*

**APPEAL OF THIS DETERMINATION**, after it has become final, may be made to the City of Spokane Hearing Examiner, 808 West Spokane Falls Blvd., Spokane WA 99201. The appeal deadline is Noon on September 14, 2020 (21 days from the date of the signing of this DNS). This appeal must be on forms provided by the Responsible Official, make specific factual objections, and be accompanied by the appeal fee. Contact the Responsible Official for assistance with the specifics of a SEPA appeal.

\*\*\*\*\*





**Spokane Tribe of Indians  
Tribal Historic Preservation Officer**

P.O Box 100 Wellpinit WA 99040

May 5, 2020

**To:** Kevin Freibott, Assistant Planner

**RE: File No. Z20-045COMP**

Mr. Freibott,

Thank you, for contacting the Tribal Historic Preservation Office. We appreciate the opportunity to comment on your undertaking is greatly appreciated, we are hereby in consultation for this project.

After archive research completed of the APE, and a low probability of cultural resources I have no further concern on this project.

**Recommendation:** Inadvertent Discovery plan (IDP) implemented in the plan of action.

This letter is your notification that your project has been cleared, and your project may move forward, as always, if any artifacts or human remains are found upon excavation, this office should be immediately notified and the work in the immediate area **cease**.

Should additional information become available our assessment may be revised.

Again, thank you for this opportunity to comment and consider this a positive action that will assist in protecting our shared heritage.

If questions arise, please contact me at (509) 258 – 4222.

Sincerely,

Randy Abrahamson  
Tribal Historic Preservation Officer





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON COMPREHENSIVE PLAN AMENDMENT Z20-045COMP**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE the Comprehensive Plan Amendment proposal seeking to amend the text of Chapter 4, Transportation, to describe safety for at-grade railroad crossings and to describe certain locations where improvements may be necessary.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Under GMA, comprehensive plans generally may be amended no more frequently than once a year, and all amendment proposals must be considered concurrently in order to evaluate for their cumulative effect.
- C. Amendment Proposal Z20-045COMP (the "Proposal") was submitted in a timely manner for review during the City's 2019/2020 amendment cycle.
- D. The Proposal seeks to amend the text of Chapter 4, Transportation, of the Comprehensive Plan to include discussion of safety for at-grade railroad crossings and to describe certain locations where improvements may be necessary.
- E. Annual amendment proposals are subject to a threshold review process to determine whether the proposals will be included in the City's Annual Comprehensive Plan Amendment Work Program.
- F. On March 2, 2020, the City Council adopted Resolution RES 2020-0014 establishing the 2020 Comprehensive Plan Amendment Work Program, and including the Proposal in the Work Program.
- G. Thereafter, on April 24, 2020, staff requested comments from agencies, departments, and neighborhood councils. Comments were received from the Spokane Tribe indicating no significant concerns.
- H. A Notice of Application was published on June 8, 2020 in the Spokesman Review. The Notice of Application initiated a 60-day public comment period from June 8 to August 7, 2020, during which the City did not receive any comments on the Proposal from the public.
- I. On June 4, 2020, the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program and the Proposal, and was provided with information regarding the dates of Plan Commission workshops and hearings.
- J. On June 10, 2020, the Spokane City Plan Commission held a workshop to study the Proposal.



- K. On June 23, 2020, the Pedestrian Transportation and Traffic Committee of the Community Assembly received a presentation regarding the Proposal.
- L. On June 24, 2020, the Land Use Subcommittee of the Community Assembly received a presentation regarding the 2020 Comprehensive Plan Amendment Work Program, including the Proposal.
- M. On July 21, 2020, the Plan Commission Transportation Subcommittee received a presentation regarding the Proposal.
- N. On July 30, 2020, an online public workshop specific to the bicycle- and transportation-related amendments was provided for the general public to answer questions and receive comments on the proposed amendments.
- O. On August 24, 2020, a State Environmental Policy Act (SEPA) Checklist and Determination of Non-Significance were issued for the Proposal. The deadline to appeal the SEPA determination was September 14, 2020. No comments on the SEPA determination were received.
  - 1. Notice of the SEPA Determination for the Proposal was published in the Official Gazette on September 2 and 9, 2020.
- P. On August 25, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to the Comprehensive Plan.
- Q. On August 26 and September 2, 2020, notice was published in the Spokesman Review providing notice of a SEPA Determination of Non-Significance and notice of the September 9, 2020 Plan Commission Public Hearing.
- R. On August 24, 2020 staff published a report addressing SEPA and providing staff's analysis of the merits of the Proposal, copies of which were circulated as prescribed by SMC 17G.020.060B.8. Staff's analysis of the Proposal recommended approval of the Proposal.
- S. On September 23, 2020, the Plan Commission held a public hearing on the Proposal, including the taking of verbal testimony, and closed the public record. No members of the public testified at the hearing.
- T. As a result of the City's efforts, pursuant to the requirements of SMC 17G.020.070, the public has had extensive opportunities to participate throughout the process and persons desiring to comment were given an opportunity to do so.
- U. Except as otherwise indicated herein, the Plan Commission adopts the findings and analysis set forth in the Staff Report prepared for the Proposal (the "Staff Report").
- V. The Plan Commission finds that the Proposal meets the intent and requirements of the Comprehensive Plan.
- W. The Plan Commission finds that the Proposal meets the decision criteria established by SMC 17G.020.030, as described in the Staff Report.



## CONCLUSIONS:

Based upon the Proposal materials, staff analysis (which is hereby incorporated into these findings, conclusions, and recommendation), SEPA review, agency and public comments received, and public testimony presented regarding Proposal File No. Z20-045COMP, the Plan Commission makes the following conclusions with respect to the review criteria outlined in SMC 17G.020.030:

1. The Proposal was submitted in a timely manner and added to the 2020 Annual Comprehensive Plan Amendment Work Program, and the final review Proposal was submitted as provided in SMC 17G.020.050(D).
2. Interested agencies and the public have had extensive opportunities to participate throughout the process and persons desiring to comment have been given that opportunity to comment.
3. The Proposal is consistent with the goals and purposes of GMA.
4. Any potential infrastructure implications associated with the Proposal will either be mitigated through projects reflected in the City's relevant six-year capital improvement plans or through enforcement of the City's development regulations at time of development.
5. As outlined in above in the Findings of Fact, the Proposal is internally consistent as it pertains to the Comprehensive Plan, as described in SMC 17G.020.030.E.
6. The Proposal is consistent with the Countywide Planning Policies for Spokane County, the comprehensive plans of neighboring jurisdictions, applicable capital facilities plans, the regional transportation plan, and official population growth forecasts.
7. The Proposal has been considered simultaneously with the other proposals included in the 2020 Annual Comprehensive Plan Amendment Work Program in order to evaluate the cumulative effect of all the proposals.
8. SEPA review was completed for the Proposal.
9. The Proposal will not adversely affect the City's ability to provide the full range of urban public facilities and services citywide at the planned level of service, or consume public resources otherwise needed to support comprehensive plan implementation strategies.
10. The Proposal proposes a text amendment that is in conformance with the comprehensive plan.
11. The proposed amendment provides for additional guidance pursuant to the community's original vision.

## RECOMMENDATIONS:

In the matter of Z20-045COMP, a request by City of Spokane staff to amend the text of Chapter 4, Transportation, of the Comprehensive Plan, as based upon the above listed findings and conclusions, by a vote of **9 to 0**, the Spokane Plan Commission recommends City Council **APPROVE** the requested amendment to Chapter 4, Transportation and authorizes the President to prepare and sign on the



Commission's behalf a written decision setting forth the Commission's findings, conclusions, and recommendation on the Proposal.



---

**Todd Beyreuther, President**

Spokane Plan Commission

October 21, 2020



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/9/2020

**Clerk's File #**

ORD C35981

**Renews #****Submitting Dept**

CITY COUNCIL

**Cross Ref #****Contact Name/Phone**

KAREN STRATTON 625-6291

**Project #****Contact E-Mail**

KSTRATTON@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

First Reading Ordinance

**Requisition #****Agenda Item Name**

0320 - ORDINANCE AMENDING GOLF CART ZONES

**Agenda Wording**

An ordinance expanding the number of authorized golf cart zones in Spokane; amending sections 16A.63.010, 16A.63.020, and 16A.63.030 of the Spokane Municipal Code.

**Summary (Background)**

An ordinance authorizing golf cart zones in the Manito, West Central, and Eagle Ridge areas of the city.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Neutral \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

MCCLATCHEY, BRIAN

**Study Session\Other**

PIES Comm., 8/26/19

**Division Director****Council Sponsor**

CM Stratton

**Finance**

HUGHES, MICHELLE

**Distribution List****Legal**

PICCOLO, MIKE

**For the Mayor**

ORMSBY, MICHAEL

**Additional Approvals****Purchasing**



## ORDINANCE NO. C35981

An ordinance expanding the number of authorized golf cart zones in Spokane; amending sections 16A.63.010, 16A.63.020, and 16A.63.030 of the Spokane Municipal Code.

**NOW THEREFORE**, the City of Spokane does ordain:

**Section 1.** That section 16A.63.010 of the Spokane Municipal Code is amended to read as follows:

### **Section 16A.63.010 Definitions**

The following definitions are applicable in this chapter unless the context otherwise requires:

- A. "Alternative Vehicles" means, collectively, golf carts, neighborhood electric vehicles, medium-speed electric vehicles, and utility-type vehicles as defined in this chapter.
- B. "Alternative Vehicle Zone" means all public streets within the boundaries of the "Alternative Vehicle Zone Map", incorporated in this ordinance as Exhibit A, having a speed limit of thirty-five (35) miles per hour or less, and which exhibits signage indicating the area as an Alternative Vehicle Zone.
- C. "Golf cart," means an electric-powered four-wheel vehicle originally designed and manufactured for operation on a golf course for sporting purposes and has a speed attainable in one mile of not more than twenty (20) miles per hour. A golf cart is not a non-highway vehicle or off road vehicle as defined in RCW 46.04.365. A golf cart is not considered a motor vehicle, except for the purpose of chapter 46.61 RCW regarding rules of the road.
- D. "Golf cart ~~((zone))~~zones," means all public streets within the boundaries of the attached ~~((map))~~maps (incorporated into this ordinance as ~~((Exhibit B))~~Exhibits B, C, and D) (designated as the "Golf Cart Zone ~~((Map))~~Maps") having a speed limit of twenty-five (25) miles per hour or less and that ~~((contains))~~have appropriate and uniform signage identifying the area as a golf cart zone. Additionally, operation of golf carts and alternative vehicles on the University District Gateway Bridge is limited to Washington State University-Spokane personnel for maintenance use only and the operation of golf carts and alternative vehicles on the Centennial Trail is limited to Kendall Yards HOA personnel for maintenance use only and Park Department rangers for patrol purposes only.
- E. "Medium-speed electric vehicle" means a self-propelled, electrically powered four-wheeled motor vehicle, equipped with a roll cage or crush-proof body design, whose speed attainable in one mile is more than twenty-five (25) miles per hour but not more than thirty-five (35) miles per hour and otherwise meets or exceeds the federal regulations set forth in 49 C.F.R. Sec. 571.500 and as defined in RCW 46.04.295.



- F. "Neighborhood electric vehicle" means a self-propelled, electrically powered four-wheeled motor vehicle whose speed attainable in one mile is more than twenty miles per hour and not more than twenty-five miles per hour and conforms to federal regulations under Title 49 C.F.R. Part 571.500 and as defined in RCW 46.04.357.
- G. "Operator" means any person who is at least sixteen years of age and completed a driver's education course or has previous experience driving as a licensed driver. "Operator" does not include city personnel or those persons authorized by the chief of police to operate golf carts on city streets.
- H. "Street," means the entire right of way width excluding the sidewalk and between the curb boundary lines and shoulder or swale of public property, when any part thereof is open to the use by the public for purposes of pedestrian, bicycle or vehicular travel including parking.
- I. "Utility-type vehicle" means a vehicle designed for and capable of nonhighway travel only and that travels on four (4) or more tires, has a maximum width of seventy-four inches (74"), has a maximum weight of two thousand pounds (2,000 lbs.), has a wheelbase of one hundred ten inches (110") or less, and satisfies at least one of the following: (i) Has a minimum width of fifty inches (50"); (ii) has a minimum weight of at least nine hundred pounds (900 lbs.); or (iii) has a wheelbase of over sixty-one inches (61").

**Section 2.** That section 16A.63.020 of the Spokane Municipal Code is amended to read as follows:

### **Section 16A.63.020 Restrictions**

Golf carts allowed under this chapter are restricted to those that are electric-powered, and which may not be otherwise modified to allow the same to exceed the speed of twenty (20) miles per hour, and must also be so equipped to maintain a speed of nineteen (19) miles per hour on level ground. Except as otherwise authorized by this chapter, ((No))no vehicles authorized in this chapter shall be operated on the Spokane River Centennial Trail.

**Section 3.** That section 16A.63.030 of the Spokane Municipal Code is amended to read as follows:

### **Section 16A.63.030 Areas of Operation**

Golf carts, neighborhood electric vehicles, medium-speed electric vehicles, and utility-type vehicles as defined in this chapter operated by persons allowed to do so by this chapter may operate with the common flow of traffic on any street within a golf cart zone or alternative vehicle zone having a speed limit of thirty-five (35) miles per hour or less with the exception of an arterial street which is both (1) adjacent to a park or school zone and (2) ((with))has a reduced speed limit of twenty-five (25) miles per hour or less. A golf cart, neighborhood electric vehicle, medium-speed electric vehicle, or utility-type vehicle



may cross over a street within a golf cart zone ((with))that has a speed limit greater than thirty-five (35) miles per hour when safe to do so at street intersections.

**PASSED** by the City Council on \_\_\_\_\_.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

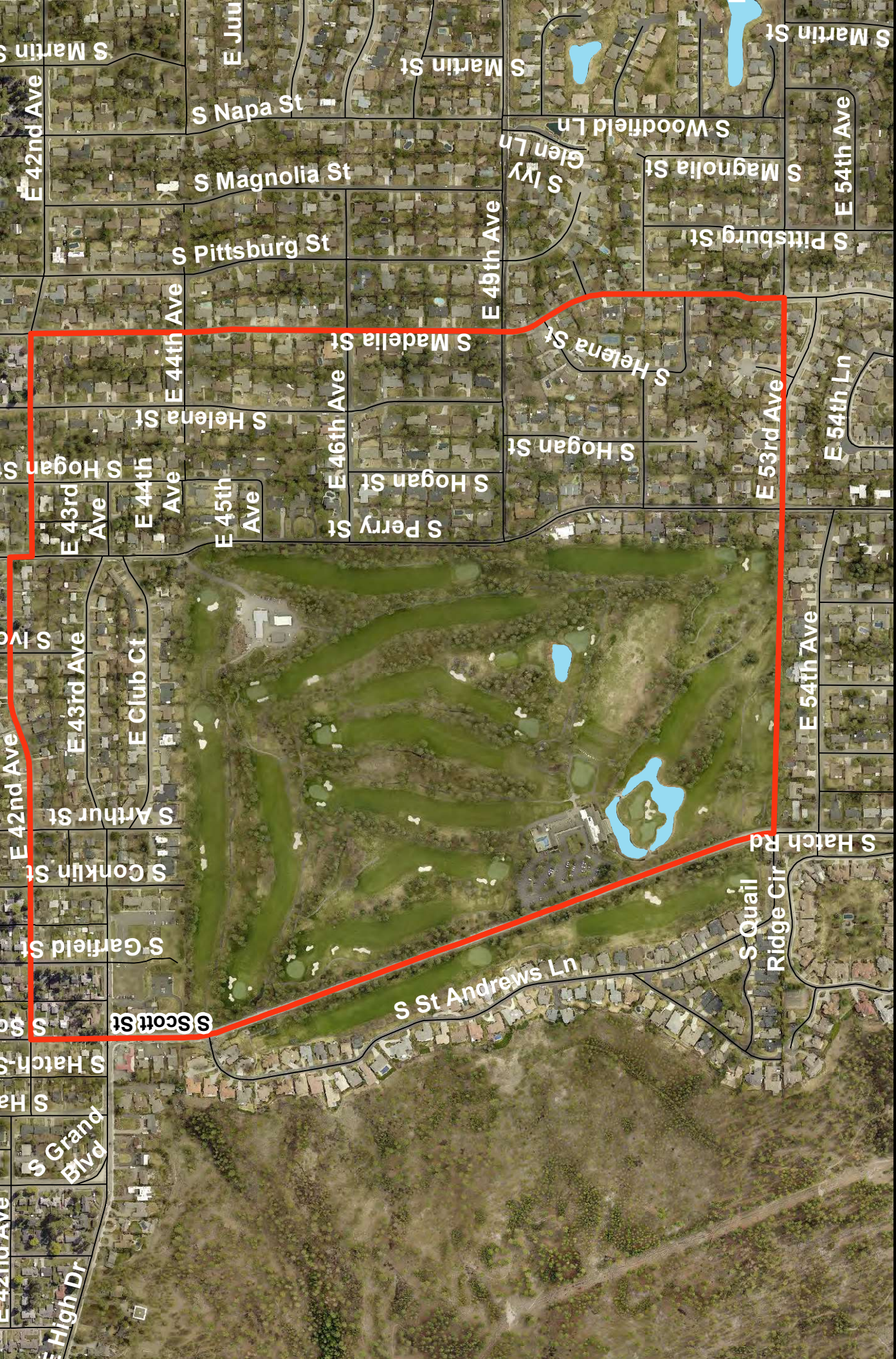
\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

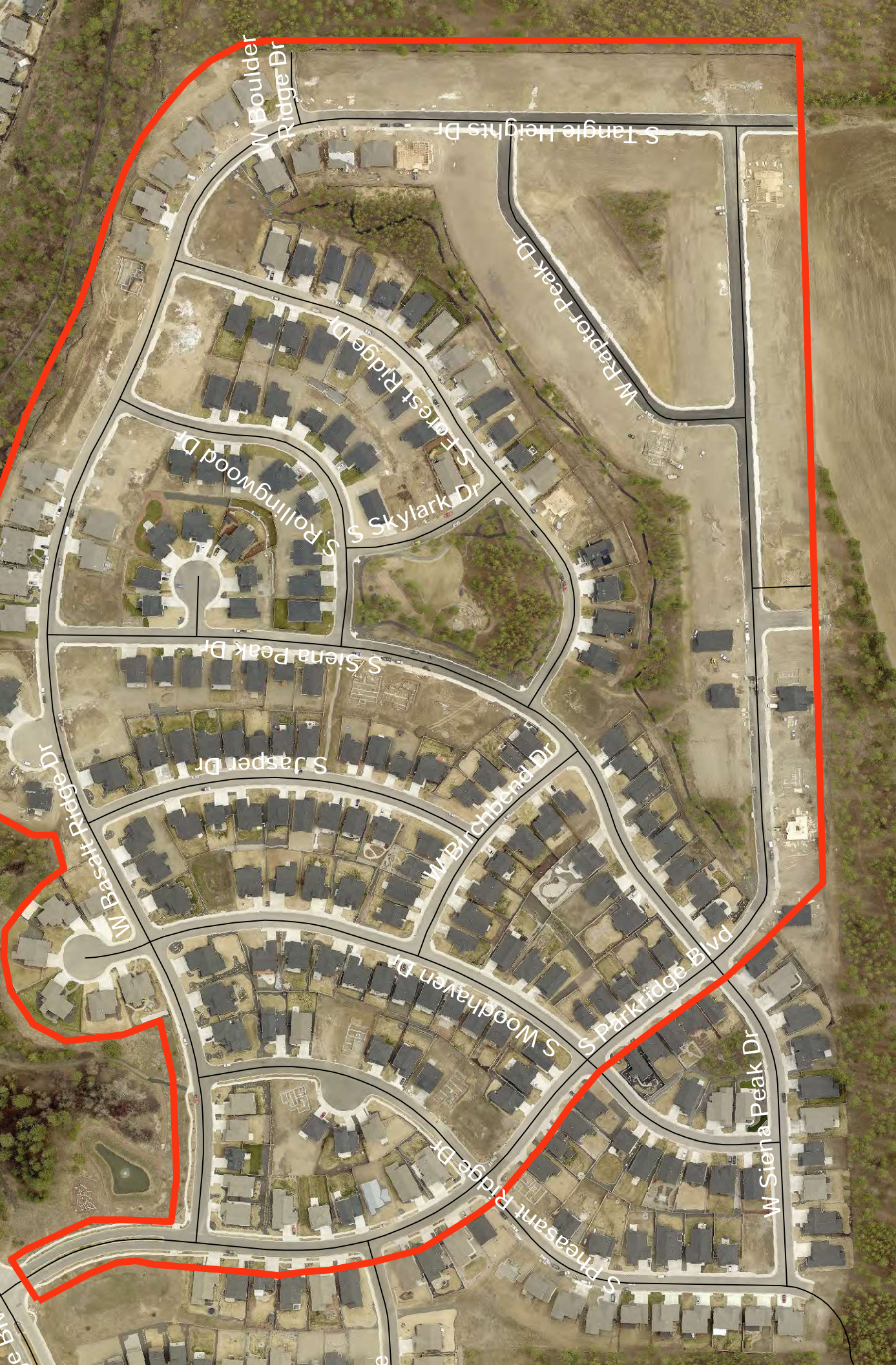
\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date









0

0.125

0.25

0.5 Miles









# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





# Agenda Sheet for City Council Meeting of: 11/23/2020

|                           |                                                                    |
|---------------------------|--------------------------------------------------------------------|
| <b>Date Rec'd</b>         | 11/11/2020                                                         |
| <b>Clerk's File #</b>     | ORD C35982                                                         |
| <b>Renews #</b>           |                                                                    |
| <b>Cross Ref #</b>        |                                                                    |
| <b>Project #</b>          |                                                                    |
| <b>Bid #</b>              |                                                                    |
| <b>Requisition #</b>      |                                                                    |
| <b>Submitting Dept</b>    | CITY COUNCIL                                                       |
| <b>Contact Name/Phone</b> | BRIAN 625-6254<br>MCCLATCHEY                                       |
| <b>Contact E-Mail</b>     | BMCCLATCHEY@SPOKANECITY.ORG                                        |
| <b>Agenda Item Type</b>   | First Reading Ordinance                                            |
| <b>Agenda Item Name</b>   | 0320 - IMPOSING SALES AND USE TAX FOR HOUSING AND RELATED SERVICES |

## **Agenda Wording**

An ordinance imposing a sales and use tax for the construction, acquisition, and rehabilitation of attainable housing and housing-related supportive services; and enacting a new chapter 07.08C of the Spokane Municipal Code.

## **Summary (Background)**

The Washington state legislature has authorized, by passing HB 1590, cities and counties to impose an additional 0.1% sales and use tax, provided that the revenues from that tax must be spent on the construction, acquisition, and rehabilitation of affordable housing, and on housing-related supportive services. This ordinance imposes the sales and use tax, describes project funding priorities, sets a sunset date, and provides a framework for application review and project funding recommendations.

Lease? NO Grant related? NO Public Works? NO  
**Fiscal Impact** **Budget Account**

|         |              |   |     |
|---------|--------------|---|-----|
| Revenue | \$ 4,100,000 | # | TBD |
| Select  | \$           | # |     |
| Select  | \$           | # |     |
| Select  | \$           | # |     |

| <b><u>Approvals</u></b>            |                   | <b><u>Council Notifications</u></b> |                                    |
|------------------------------------|-------------------|-------------------------------------|------------------------------------|
| <b><u>Dept Head</u></b>            | MCCLATCHEY, BRIAN | <b><u>Study Session\Other</u></b>   | UD (9/14/20); FA (11/16/20)        |
| <b><u>Division Director</u></b>    |                   | <b><u>Council Sponsor</u></b>       | CMs Wilkerson, Stratton, and Burke |
| <b><u>Finance</u></b>              | WALLACE, TONYA    | <b><u>Distribution List</u></b>     |                                    |
| <b><u>Legal</u></b>                | PICCOLO, MIKE     |                                     |                                    |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL   |                                     |                                    |
| <b><u>Additional Approvals</u></b> |                   |                                     |                                    |
| <b><u>Purchasing</u></b>           |                   |                                     |                                    |
|                                    |                   |                                     |                                    |
|                                    |                   |                                     |                                    |
|                                    |                   |                                     |                                    |



## ORDINANCE NO. C35982

An ordinance imposing a sales and use tax for the construction, acquisition, and rehabilitation of attainable housing and for housing-related supportive services; and enacting a new chapter 07.08C of the Spokane Municipal Code.

**WHEREAS**, Spokane is experiencing historically low rental vacancy rates, rising average rents, and increasing median home prices which are outpacing gains in the median household income in Spokane, increasing the housing cost burden on many households and putting homeownership increasingly out of reach for households with incomes at or below the area median income; and

**WHEREAS**, in addition, we recognize that in Spokane, as in many communities around the country, housing and land use policy has historically discriminated against people of color in many ways, which has had the effect of lower home ownership rates, diminished wealth-building ability, and poorer health and educational outcomes; and

**WHEREAS**, the housing market is complex and in the necessary multi-pronged approach to the crisis in housing affordability in Spokane, one of the most effective methods is to increase the supply of housing, both rental and owner-occupied, at all price points; and

**WHEREAS**, Spokane's crisis of affordable housing is shared state-wide, and in response, the Washington state legislature in the 2020 session enacted House Bill 1590, which allows cities to adopt a 0.1% sales and use tax by ordinance to provide funding for the construction of affordable housing and housing-related supportive services; and

**WHEREAS**, Spokane County had the authority to enact such a sales and use tax until September 20, 2020, until which time the City would not have had the authority to enact such a tax, and Spokane County did not do so; and

**WHEREAS**, the City intends to adopt a 0.1% sales and use tax for fund housing programs and housing-related supportive services, impose a sunset date at which the tax could be discontinued or reauthorized, and establish an advisory committee of residents and stakeholders to review applications and provide recommendations to the City Council on the use of the funds derived from this tax and periodically report on the effectiveness of this chapter; and

**WHEREAS**, creating a locally-controlled source of revenue is vital for the development of innovative ways to build and acquire more housing in Spokane, maintain and rehabilitate existing housing which is affordable to lower-income people, provide the types of services that people who are at risk of homelessness need, increase the number of people in Spokane who are homeowners through a variety of measures such as down payment assistance, the formation of a land trust, cooperative housing, and others; and

**WHEREAS**, all the funding decisions made concerning the funds derived from this chapter shall include broad equity considerations to ensure that people who currently face



housing discrimination and populations that have historically faced discrimination in housing markets or as the result of housing policy, are well-represented in the process of making funding determinations.

**NOW THEREFORE**, the City of Spokane does ordain:

**Section 1.** That there is enacted a new chapter 08.07C of the Spokane Municipal Code to read as follows:

**Chapter 08.07C      Sales and Use Tax for Housing and Housing-Related Supportive Services**

**Section 08.07C.010              Purpose and Intent**

The City Council enacts this chapter with the intent to help strengthen our community by ensuring that everyone has access to housing which is affordable for them, regardless of their income, by providing a locally-controlled source of revenue to accomplish the specific objectives of state law, as identified by the Washington state legislature in HB 1590 (Chapter 222, Laws of 2020).

**Section 08.07C.020 Imposition of Sales and Use Tax; Collection by the State Department of Revenue**

- A. There is imposed a sales and use tax, as the case may be, as authorized by Chapter 222, Laws of 2020, as codified at RCW 82.14.530, upon every taxable event, as defined in chapter 82.14 RCW, occurring within the City of Spokane.
- B. The rate of the tax authorized by this section may not exceed one-tenth of one percent (0.1%) of the selling price in the case of a sales tax, or value of the article used, in the case of a use tax.
- C. The tax authorized by this section is in addition to any other taxes authorized by law and must be collected from persons who are taxable by the state under Chapters 82.08 and 82.12, RCW, upon the occurrence of any taxable event within the city.

**Section 08.07C.030 Uses of Tax Revenues; Funding Priorities**

- A. The City may use the moneys collected by the tax imposed under SMC 08.07C.020 or bonds issued under RCW 82.14.530(5) only for the purposes described in RCW 82.14.530, with a minimum of sixty percent (60%) of the revenue collected under this chapter to be used for the following purposes, consistent with RCW 82.14.530(2), with a specific preference for occupant ownership of housing units, including, without limitation, cooperatively-owned multi-family projects:



1. Constructing affordable housing, which may include new units of affordable housing within an existing structure, and facilities providing housing-related services; or
  2. Constructing mental and behavioral health-related facilities; or
  3. Funding the operations and maintenance costs of new units of affordable housing and facilities where housing-related programs are provided, or newly constructed evaluation and treatment centers.
- B. The remainder of the moneys collected under this chapter must be used for the operation, delivery, or evaluation of mental and behavioral health treatment programs and services or housing-related services, with the overall objective of helping to maintain housing stability.
- C. Recommendations on the use of revenues collected under this chapter shall employ a racial equity framework that promotes equity, works to reduce disparities in housing, and achieve equitable outcomes for marginalized populations and populations that have been subject to historical or present discrimination in housing markets and/or housing policy.
- D. The City may, under the authority of RCW 82.14.530(5), issue general obligation or revenue bonds within the limitations now or hereafter prescribed by state law, and may use, and is authorized to pledge, up to fifty percent (50%) of the revenues collected under the authority of this chapter for repayment of such bonds, in order to finance the provision or construction of affordable housing, facilities where housing-related programs are provided, or evaluation and treatment centers described in RCW 82.14.530(2)(a)(iii).
- E. Revenues collected under this chapter may be used to offset reductions in state or federal funds for the purposes described in RCW 82.14.530(2).
- F. No more than ten percent (10%) of the revenues collected under the authority of this chapter may be used to supplant existing local funds.

#### **Section 08.07C.040      Advisory Committee**

The Council will form a housing action committee (the “Committee”), which shall provide recommendations to the City Council concerning the use of funds derived from the tax imposed by this chapter and chapter 08.07B SMC, and shall provide input on the use of funds derived from any tax increment financing (“TIF”) districts which provide or allow for the use of revenues for affordable housing, as defined in governing law. Appointments to the Committee shall be made to ensure a significant degree of participation by people who have been most negatively impacted by the present housing crisis and historical patterns of discrimination in housing markets and housing policy, including families at risk of homelessness or with a lived experience of homelessness; Black, Indigenous, and



other people of color; immigrants; seniors; people with disabilities; domestic violence survivors, unaccompanied homeless youth or young adults; and veterans.

#### **Section 08.07C.050            Funding Process**

- A. No later than June 1 of each year following the effective date of this section, applications for construction, rehabilitation, or acquisition project funding shall be due for award in the following year. The construction of such projects shall commence within twenty-four (24) months of the award date.
- B. No later than May 1 of each year following the effective date of this section, applications for housing-related services funding shall be due for award beginning October 1 of each year. Such service contracts shall be no longer than twenty-four (24) months in duration.
- C. The Committee, with the assistance of appropriate Administration and Council staff, shall receive and review all applications, and shall make such investigation and information-gathering as it deems appropriate in order to become fully knowledgeable concerning all applications.
- D. When making its recommendations for housing construction funding, the Committee shall give priority to applications which meet the following goals:
  - a. Constructing mixed-use housing;
  - b. Locating housing in designated Centers and Corridors;
  - c. Creating permanently affordable housing, defined as maintaining affordability for households earning up to 60% of AMI for at least 40 years;
  - d. Addressing the racial wealth gap through increasing homeownership for populations that have historically been subject to discrimination in housing markets or housing policy;
  - e. Locating housing near public transit lines, preferably within 1/8 mile of a high-performance transit line;
  - f. Distributing attainable housing throughout the City, in a variety of neighborhoods and in close proximity to services such as parks and open space, schools, and services;
  - g. Using universal design to create a sufficient number of units that are safe and accessible, regardless of age, physical ability or stature; and
  - h. Creating units that use less net energy and require less maintenance in order to reduce long term costs of ownership.
- E. With the assistance of the appropriate Administration and Council staff, the Committee shall make an initial written finding as to whether each application complies with the requirements of this chapter and include that finding with those project applications it recommends for approval, and shall forward such findings



and the complete application file to the City Council or a standing Council committee, as the Council determines, for its review.

F. For each project application referred to a standing Council committee, if the standing Council committee agrees, by majority vote of the members present, with the HAS's recommendation, the committee Chairperson shall forward each such project application, along with the HAS's written findings, to the full City Council for consideration on its regular legislative agenda.

G. Any funding awarded under the authority of this chapter shall be administered by the appropriate department of the City administration, and funding recommendations must take into account the historic performance of the applicant, both in the construction and the maintenance of housing.

#### **Section 08.07C.060 Sunset**

The tax imposed by the City under this chapter shall expire twenty (20) years after the effective date of this Ordinance No. C-\_\_\_\_\_, unless earlier extended by ordinance. Beginning three (3) years before the expiration date, the City's Chief Financial Officer or designee shall provide notice to the City Council and the Mayor of the impending expiration date of the tax, and shall also promptly notify the City Council and Mayor of the repayment status of any bonds issued and secured by the revenues received by virtue of the tax imposed by this chapter.

**Section 2. Severability.** If any word, sentence, provision, clause or section of this ordinance is deemed by a court of competent jurisdiction to be invalid or unenforceable, such severance shall not affect the validity, legality, or constitutionality of the remainder of this ordinance.

**PASSED** by the City Council on \_\_\_\_\_.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney



---

Mayor

---

Date

---

Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods



Services



Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

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CITY ADMINISTRATOR SIGNATURE:

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**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/9/2020

**Clerk's File #**

ORD C35983

**Renews #****Cross Ref #****Submitting Dept**INTEGRATED CAPITAL  
MANAGEMENT**Contact Name/Phone**

INGA NOTE 625-6331

**Project #****Contact E-Mail**

INOTE@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

First Reading Ordinance

**Requisition #****Agenda Item Name**

4250 - ORDINANCE AMENDING SMC 17A.020 STREET DESIGN STANDARDS

**Agenda Wording**

An ordinance relating to Design Standards amending SMC sections 17A.020.010, 17A.020.020, 17A.020.030, 17A.020.130, 17A.020.160, 17A.020.190 and 17A.020.200 of the Spokane Municipal Code.

**Summary (Background)**

Ordinance amending many definitions in SMC 17A.020 to better align with the Design Standards Chapter 3 update. The following definitions are proposed for update: All Weather Surface, Arterial, Bicycle Facilities, Clear View Triangle, Clear Zone, Collector Arterial, Curb Ramp, Minor Arterials, Parkway, Principal Arterial, Shared Use Pathway, and Traveled Way.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Select \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

MILLER, KATHERINE E

**Study Session\Other**

PIES 10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

CP Beggs

**Finance**

ALBIN-MOORE, ANGELA

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## ORDINANCE NO. C35983

AN ORDINANCE relating to Design Standards; amending SMC sections 17A.020.010, 17A.020.020, 17A.020.030, 17A.020.130, 17A.020.160, 17A.020.190 and 17A.020.200 of the Spokane Municipal Code.

WHEREAS, the City of Spokane is updating Chapter 3 of the Design Standards, and as such must ensure consistency with Chapter 17A.020; and

WHEREAS, on July 16, 2020 the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to development regulations; and

WHEREAS, on July 22, 2020 a State Environmental Policy Act (SEPA) checklist was issued with a request for comments on the proposal, many comments were received and incorporated into the document; and

WHEREAS, on August 31, 2020 a Determination of Non-Significance was issued by the responsible official, and no appeal was received prior to the deadline of September 22, 2020; and

WHEREAS, on or about October 14, 2020 following a process involving a number of public workshops and a public hearing, a majority of the City of Spokane Plan Commission voted to recommend approval of an ordinance amending the Engineering Standards in Chapter 17 SMC; -

Now Therefore, the City of Spokane does ordain:

Section 1. That SMC section 17A.020.010 is amended to read as follows:

### **17A.020.010 “A” Definitions**

A. Abandoned Sign Structure.

[See SMC 17C.240.015.](#)

B. Aboveground Storage Tank or AST.

Any one or connected combination of tanks that is used to contain an accumulation of liquid critical materials and the aggregate volume of which (including the volume of piping connected thereto) is more than sixty gallons and the entire exterior surface area of the tank is above the ground and is able to be fully visually inspected. Tanks located in vaults or buildings that are to be visually inspected are considered to be aboveground tanks.

C. Accepted.

A project for which the required plans have been found to be technically adequate.



D. Accessory Dwelling Unit (ADU).

An accessory dwelling unit is a separate additional living unit, including separate kitchen, sleeping, and bathroom facilities, attached or detached from the primary residential unit, on a single-family lot. ADUs are known variously as:

1. "Mother-in-law apartments,"
2. "Accessory apartments," or
3. "Second units."

E. Accessory Structure.

A structure of secondary importance or function on a site. In general, the primary use of the site is not carried on in an accessory structure.

1. Accessory structures may be attached or detached from the primary structure.
2. Examples of accessory structures include:
  - a. Garages,
  - b. Decks,
  - c. Fences,
  - d. Trellises,
  - e. Flagpoles,
  - f. Stairways,
  - g. Heat pumps,
  - h. Awnings, and
  - i. Other structures.
3. See also [SMC 17A.020.160](#) ("Primary Structure").

F. Accessory Use.

A use or activity which is a subordinate part of a primary use and which is clearly incidental to a primary use on a site.

G. Activity.

See Regulated Activity.

H. Administrative Decision.

A permit decision by an officer authorized by the local government. The decision may be for approval, denial, or approval with conditions and is subject to the applicable development standards of the land use codes or development codes.

I. Adult Bookstore or Adult Video Store.

1. A commercial establishment which, as one of its principal business activities, offers for sale or rental for any form of consideration any one or more of the following: books, magazines, periodicals or other printed matter, or photographs, films, motion pictures, video cassettes, compact discs, digital video discs, slides, or other visual representations which are characterized by their emphasis upon the display of "specified anatomical areas," as defined in [SMC 17A.020.190](#), or "specified sexual



activities,” as defined in [SMC 17A.020.190](#). A “principal business activity” exists where the commercial establishment meets any one or more of the following criteria:

- a. At least thirty percent of the establishment’s displayed merchandise consists of said items; or
  - b. At least thirty percent of the retail value (defined as the price charged to customers) of the establishment’s displayed merchandise consists of said items; or
  - c. At least thirty percent of the establishment’s revenues derive from the sale or rental, for any form of consideration, of said items; or
  - d. The establishment maintains at least thirty percent of its floor space for the display, sale, and/or rental of said items (aisles and walkways used to access said items, as well as cashier stations where said items are rented or sold, shall be included in “floor space maintained for the display, sale, and/or rental of said items”); or
  - e. The establishment maintains at least five hundred square feet of its floor space for the display, sale, and/or rental of said items (aisles and walkways used to access said items, as well as cashier stations where said items are rented or sold, shall be included in “floor space maintained for the display, sale, and/or rental of said items”); or
  - f. The establishment regularly offers for sale or rental at least two thousand of said items; or
  - g. The establishment regularly features said items and regularly advertises itself or holds itself out, in any medium, by using “adult,” “XXX,” “sex,” “erotic,” or substantially similar language, as an establishment that caters to adult sexual interests.
2. For purposes of this definition, the term “floor space” means the space inside an establishment that is visible or accessible to patrons, excluding restrooms.

#### J. Adult Business.

An “adult bookstore or adult video store,” an “adult entertainment establishment,” or a “sex paraphernalia store.”

#### K. Adult Entertainment Establishment.

1. An “adult entertainment establishment” is an enclosed building, or any portion thereof, used for presenting performances, activities, or material relating to “specified sexual activities” as defined in [SMC 17A.020.190](#) or “specified anatomical areas” as defined in [SMC 17A.020.190](#) for observation by patrons therein.
2. A motion picture theater is considered an adult entertainment establishment if the preponderance of the films presented is distinguished or characterized by an emphasis on the depicting or describing of “specified sexual activities” or “specified anatomical areas.”



3. A hotel or motel providing overnight accommodations is not considered an adult entertainment establishment merely because it provides adult closed circuit television programming in its rooms for its registered overnight guests.

L. Adult Family Home.

A residential use as defined and licensed by the state of Washington in a dwelling unit.

M. Agency or Agencies.

The adopting jurisdiction(s), depending on the context.

N. Agricultural Activities.

1. Pursuant to WAC 173-26-020(3)(a), agricultural uses and practices including, but not limited to:
  - a. Producing, breeding, or increasing agricultural products;
  - b. Rotating and changing agricultural crops;
  - c. Allowing land used for agricultural activities to lie fallow in which it is plowed and tilled but left unseeded;
  - d. Allowing land used for agricultural activities to lie dormant as a result of adverse agricultural market conditions;
  - e. Allowing land used for agricultural activities to lie dormant because the land is enrolled in a local, state, or federal conservation program, or the land is subject to a conservation easement;
  - f. Conducting agricultural operations; maintaining, repairing, and replacing agricultural equipment;
  - g. Maintaining, repairing, and replacing agricultural facilities, provided that the replacement facility is not closer to the shoreline than the original facility; and
  - h. Maintaining agricultural lands under production or cultivation.
2. The City of Spokane shoreline master program defines agriculture activities as:
  - a. Low-intensity agricultural use is defined as passive grazing and plant cultivation; or
  - b. High-intensity agricultural use includes such activities as feedlots, feed mills, packing plants, agricultural processing plants or warehouse for the purpose of processing, packing, and storage of agricultural products.

O. Agricultural Land.

Areas on which agricultural activities are conducted as of the date of adoption of the updated shoreline master program pursuant to the State shoreline guidelines as evidenced by aerial photography or other documentation. After the effective date of the SMP, land converted to agricultural use is subject to compliance with the requirements herein.



P. AKART.

An acronym for “all known, available, and reasonable methods to control toxicants” as used in the sense of the state Water Pollution Control Act and RCW 90.48.520 thereof. AKART shall represent the most current methodology that can be reasonably required for preventing, controlling, or abating the pollutants associated with a discharge. The concept of AKART applies to both point and nonpoint sources of pollution.

Q. Alkali Wetlands.

Alkali wetlands means wetlands characterized by the occurrence of shallow saline water. In eastern Washington, these wetlands contain surface water with specific conductance that exceeds three thousand micromhos/cm. They have unique plants and animals that are not found anywhere else in eastern Washington such as the alkali bee. Conditions within these wetlands cannot be easily reproduced through compensatory mitigation.

R. All Weather Surface.

A road surface which emergency vehicles and typical passenger vehicles can pass in all types of weather. If unpaved, the top course should be six inches minimum of compacted crushed rock meeting standards for a roadway surface.

~~((R.))~~ S. Alley.

See “Public Way” ([SMC 17A.020.160](#)).

~~((S.))~~ T. Alteration.

A physical change to a structure or site.

1. Alteration does not include normal maintenance and repair or total demolition.
2. Alteration does include the following:
  - a. Changes to the facade of a building.
  - b. Changes to the interior of a building.
  - c. Increases or decreases in floor area of a building; or
  - d. Changes to other structures on the site, or the development of new structures.

~~((T.))~~ U. Alteration of Plat, Short Plat, or Binding Site Plan.

The alteration of a previously recorded plat, short plat, binding site plan, or any portion thereof, that results in a change to conditions of approval or the deletion of existing lots or the change of plat or lot restrictions or dedications that are shown on the recorded plat. An alteration does not include a boundary line adjustment subject to [SMC 17G.080.030](#).

~~((U.))~~ V. Alternative or Post-incarceration Facility.

A group living use where the residents are on probation or parole.



((V. [Deleted]))

W. [Deleted]

X. [Deleted]

Y. [Deleted]

Z. API 653.

The American Petroleum Institute's standards for tank inspection, repair, alteration, and reconstruction.

AA. Appeal.

A request for review of the interpretation of any provision of [Title 17 SMC](#).

AB. Appeal – Standing For.

As provided under RCW 36.70C.060, persons who have standing are limited to the following:

1. The applicant and the owner of property to which the land use decision is directed; and
2. Another person aggrieved or adversely affected by the land use decision, or who would be aggrieved or adversely affected by a reversal or modification of the land use decision. A person is aggrieved or adversely affected within the meaning of this section only when all of the following conditions are present:
  - a. The land use decision has prejudiced or is likely to prejudice that person;
  - b. That person's asserted interests are among those that the local jurisdiction was required to consider when it made the land use decision;
  - c. A judgment in favor of that person would substantially eliminate or redress the prejudice to that person caused or likely to be caused by the land use decision; and
  - d. The petitioner has exhausted his or her administrative remedies to the extent required by law (RCW 36.70C.060).

AC. Applicant.

An application for a permit, certificate, or approval under the land use codes must be made by or on behalf of all owners of the land and improvements. "Owners" are all persons having a real property interest. Owners include:

1. Holder of fee title or a life estate;
2. Holder of purchaser's interest in a sale contract in good standing;
3. Holder of seller's interest in a sale contract in breach or in default;
4. Grantor of deed of trust;
5. Presumptively, a legal owner and a taxpayer of record;
6. Fiduciary representative of an owner;



7. Person having a right of possession or control; or
8. Any one of a number of co-owners, including joint, in common, by entireties, and spouses as to community property.

AD. Application – Complete.

An application that is both counter-complete and determined to be substantially complete as set forth in [SMC 17G.060.090](#).

AE. Aquaculture.

The farming or culture of food fish, shellfish, or other aquatic plants or animals in freshwater or saltwater areas, and may require development such as fish hatcheries, rearing pens and structures, and shellfish rafts, as well as use of natural spawning and rearing areas. Aquaculture does not include the harvest of free-swimming fish or the harvest of shellfish not artificially planted or maintained, including the harvest of wild stock geoducks on DNR-managed lands.

AF. Aquatic Life.

Shall mean all living organisms, whether flora or fauna, in or on water.

AG. Aquifer or Spokane Aquifer.

A subterranean body of flowing water, also known as the Spokane-Rathdrum Aquifer, that runs from Pend Oreille Lake to the Little Spokane River.

AH. Aquifer Sensitive Area (ASA).

That area or overlay zone from which runoff directly recharges the aquifer, including the surface over the aquifer itself and the hillside areas immediately adjacent to the aquifer. The area is shown in the map adopted as part of [SMC 17E.050.260](#).

AI. Aquifer Water Quality Indicators.

Common chemicals used for aquifer water quality screening. These are:

1. Calcium,
2. Magnesium,
3. Sodium,
4. Total hardness,
5. Chloride,
6. Nitrate-nitrogen, and
7. Phosphorus.

AJ. Archaeological Areas and Historical Sites.

Sites containing material evidence of past human life, such as structures and tools and/or cultural sites with past significant historical events. These sites are a nonrenewable resource and provided a critical educational link with the past.

AK. Architectural feature.



Ornamental or decorative feature attached to or protruding from an exterior wall or roof, including cornices, eaves, belt courses, sills, lintels, bay windows, chimneys, and decorative ornaments.

**AL. Architectural Roof Structure.**

Minor tower or turret extending from the cornice or main roof line of a building, typically highlighting a primary corner or building entry. For purposes of the FBC, such features may not be occupied.

1. Area of Shallow Flooding.  
A designated AO or AH Zone on the Flood Insurance Rate Map (FIRM).
2. The base flood depths range from one to three feet.
3. A clearly defined channel does not exist.
4. The path of flooding is unpredictable and indeterminate.
5. Velocity flow may be evident.
6. AO is characterized as sheet flow and AH indicates ponding.

**AM. Area of Shallow Flooding.**

A designated AO or AH Zone on the Flood Insurance Rate Map (FIRM).

1. The base flood depths range from one to three feet.
2. A clearly defined channel does not exist.
3. The path of flooding is unpredictable and indeterminate.
4. Velocity flow may be evident.
5. AO is characterized as sheet flow and AH indicates ponding.

**AN. Area of Special Flood Hazard.**

The land in the floodplain within a community subject to a one percent or greater chance of flooding in any given year. Designation on maps always includes the letters A or V.

**AO. Arterial.**

See:

1. "Principal Arterials" – [SMC 17A.020.160](#),
2. "Minor Arterials" – [SMC 17A.020.130](#), or
3. "Collector Arterial" – [SMC 17A.020.030](#) ~~((or))~~  
~~((4. "Parkway" – SMC 17A.020.160.))~~

**AP. Articulation.**

The emphasis of architectural elements, such as windows, balconies, and entries that create a complementary pattern or rhythm, dividing the buildings into smaller identifiable pieces.

**AQ. Assisted Living Facility.**

A multi-family residential use licensed by the state of Washington as a boarding home pursuant to chapter 18.20 RCW, for people who have either a need for



assistance with activities of daily living (which are defined as eating, toileting, ambulation, transfer [e.g., moving from bed to chair or chair to bath], and bathing) or some form of cognitive impairment but who do not need the skilled critical care provided by nursing homes.

1. An "assisted living facility" contains multiple assisted living units.
2. An assisted living unit is a dwelling unit permitted only in an assisted living facility.

**AR. Attached Housing.**

Two or more dwelling units that are single-family residences on individual lots attached by a common wall at a shared property line. These include:

1. Townhouses,
2. Row houses, and
3. Other similar structures

**AS. Attached Structure.**

Any structure that is attached by a common wall to a dwelling unit.

1. The common wall must be shared for at least fifty percent of the length of the side of the principal dwelling.
2. A breezeway is not considered a common wall.
3. Structures including garages, carports, and house additions attached to the principal dwelling unit with a breezeway are still detached structures for purposes of this chapter and its administration.

**AT. Available Capacity.**

~~((AU.))~~ Capacity for a concurrency facility that currently exists for use without requiring facility construction, expansion, or modification (RCW 76.70A.020).

~~((AV.))~~ AU. Average Grade Level.

Means the average of the natural or existing topography of the portion of the lot, parcel, or tract of real property on that part of the lot to be occupied by the building or structure as measured by averaging the elevations at the center of all exterior walls of the proposed structure.

~~((AW.))~~ AV. Awning

A roof-like cover, often made of fabric or metal, designed and intended for protection from the weather or as a decorative embellishment, and which projects from a wall or roof of a structure over a window, walk, or door.

Section 2. That SMC section 17A.020.020 is amended to read as follows:

**17A.020.020 "B" Definitions**

**A. Backed Sign.**

[See SMC 17C.240.015.](#)



B. Balloon Sign.

[See SMC 17C.240.015.](#)

C. Bank Carving.

The incorporation of masses of alluvium or other weak bank materials into a stream channel because of undermining, usually in high flow stages.

D. Bank Erosion.

The incorporation of masses of alluvium or other weak bank materials into a stream channel.

E. Bankfull Width.

1. For streams, the measurement of the lateral extent of the water surface elevation perpendicular to the channel at bankfull depth. In cases where multiple channels exist, bankfull width is the sum of the individual channel widths along the cross-section.
2. For lakes, ponds, and impoundments, line of mean high water.
3. For periodically inundated areas of associated wetlands, line of periodic inundation, which will be found by examining the edge of inundation to ascertain where the presence and action of waters are so common and usual, and so long continued in all ordinary years, as to mark upon the soil a character distinct from that of the abutting upland.

F. Banner.

[See SMC 17C.240.015.](#)

G. Bas-relief.

Sculptural form in which shapes or figures are carved in a flat surface and project only slightly from the background.

H. Base Flood.

1. The flood having a one percent chance of being equaled or exceeded in any given year, also referred to as the “one hundred year flood.”
2. Designation on maps always includes the letters A or V.

I. Basement.

The portion of a building having its floor sub-grade (below ground level) on all sides.

J. Bedrock.

Means a general term for rock, typically hard, consolidated geologic material that underlies soil or other unconsolidated, superficial material or is exposed at the surface.

K. Bee.



Any stage of development of the common domestic honeybee, *Apis mellifera* species.

L. Beekeeper.

A person owning, possession, or controlling one or more colonies of bees.

M. Best Available Science.

Current scientific information used in the process to designate, protect, or restore critical areas, which is derived from a valid scientific process.

N. Best Management Practices.

The utilization of methods, techniques, or products that have been demonstrated to be the most effective and reliable in minimizing environmental impacts.

O. ~~((Bikeways/Pathways))~~ Bicycle Facilities

Facilities designated for use by ~~((commuters and recreational users on foot or bicycle))~~ bicyclists and sometimes by other non-motorized users. The following types of bikeway facilities are identified and further defined in the ~~((Spokane Regional Pedestrian/Bikeway Plan published by the Spokane Regional Transportation Council))~~ Comprehensive Plan:

1. ~~((Residential bikeway))~~ Bike-Friendly Route.
2. Shared~~((-use))~~ lane.
3. ~~((Paved shoulder))~~ Neighborhood Greenway.
4. Bicycle lane, both striped and physically protected.
5. Shared-use pathway.

P. Binding Site Plan – Final.

A drawing to a scale which:

1. identifies and shows the areas and locations of all streets, roads, improvements, utilities, open spaces, and any other matters provided in [SMC 17G.080.060](#);
2. contains inscriptions or attachments setting forth such appropriate limitations and conditions for the use of the land; and
3. contains provisions making any development be in conformity with the site plan.
4. A binding site plan can only be used on property zoned commercial or industrial.

Q. Binding Site Plan – Preliminary.

A neat and approximate drawing of a proposed binding site plan showing the general layout of streets, alleys, lots, blocks, and other elements required by this chapter. The preliminary binding site plan shall be the basis for the approval or disapproval of the general layout of a binding site plan.

R. Block.



A group of lots, tracts, or parcels within well-defined and fixed boundaries. Blocks shall be recognized as closed polygons, bordered by street right-of-way lines, addition lines, or a combination of the two, unless an alley is desired, in which case a block is comprised of two closed polygons bordered by street and alley right-of-way lines.

S. Block Frontage.

All of the property fronting on one side of a street that is between intersecting or intercepting streets, or that is between a street and a water feature, or end of a dead end street. An intercepting street determines only the boundary of the block frontage on the side of the street which it intercepts.

T. Board.

The board of county commissioners of Spokane County.

U. Boating Facilities.

Boating facilities include uses for boat or launch ramps. Boating facility use generally requires shoreline modification with impacts to the shoreline both waterward and landward of the ordinary high-water marks.

V. Boundary Line Adjustment.

A division made for the purpose of adjusting boundary lines which does not create any additional lot, tract, parcel, site, or division nor create any lot, tract, parcel, site, or division which contains insufficient area and dimension to meet minimum requirements for width and area for a building site.

W. Breakaway Wall.

A wall that is not part of the structural support of the building and is intended through its design and construction to collapse under specific lateral loading forces, without causing damage to the elevated portion of the building or supporting foundation system.

X. Breezeway.

A breezeway is a roofed passageway joining two separate structures.

Y. Building.

1. A "building" is a structure, or part, used or intended for supporting or sheltering any use or occupancy.
2. The term includes "factory-built structure" and "mobile home."
3. "Building" does not include a recreational vehicle.
4. "Building" means a structure that has a roof and is enclosed on at least fifty percent of the area of its sides for purposes of administration of zoning provisions.

Z. Building Base

The plinth or platform upon which a building wall appears to rest, helping



establish pedestrian-scaled elements and aesthetically tying the building to the ground.

**AA. Building Coverage.**

Building coverage is the total amount of ground area covered by a structure or structures.

1. For purposes of calculating building coverage, covered porches, covered decks, pergolas, trellis, or other feature covering a deck, patio or porch are considered structures and included in the building coverage calculations.
2. Building coverage also includes uncovered horizontal structures such as decks, stairways, and entry bridges that are more than forty-two inches above grade.
3. The calculation of building coverage includes the measurements of structures from the exterior wall including protrusions such as bay windows, but does not include the eave overhang.

**AB. Building Envelope.**

The area of a lot that delineates where a building may be placed.

**AC. Building Frontage.**

The length of any side of a building which fronts on a public street, measured in a straight line parallel with the abutting street

**AD. Build-to Line.**

An alignment establishing a certain distance from the property line (street right-of-way line) along which the building is required to be built.

**AE. Bulkhead.**

A solid or open pile wall erected generally parallel to and near the ordinary high-water mark for the purpose of protecting adjacent uplands from water or erosion. Bulkheads are considered a “hard” shoreline stabilization measure.

Section 3. That SMC section 17A.020.030 is amended to read as follows:

**17A.020.030 “C” Definitions**

**A. Candidate Species.**

A species of fish or wildlife, which is being reviewed, for possible classification as threatened or endangered.

**B. Carport.**

A carport is a garage not entirely enclosed on all sides by sight-obscuring walls and/or doors.

**C. Cellular Telecommunications Facility.**



They consist of the equipment and structures involved in receiving telecommunication or radio signals from mobile radio communications sources and transmitting those signals to a central switching computer that connects the mobile unit with the land-based telephone lines.

D. Central Business District.

The general phrase “central business district” refers to the area designated on the comprehensive plan as the “downtown” and includes all of the area encompassed by all of the downtown zoning categories combined.

E. Certificate of Appropriateness.

Written authorization issued by the commission or its designee permitting an alteration or significant change to the controlled features of a landmark or landmark site after its nomination has been approved by the commission.

F. Certificate of Capacity.

A document issued by the planning services department indicating the quantity of capacity for each concurrency facility that has been reserved for a specific development project on a specific property. The document may have conditions and an expiration date associated with it.

G. Certified Erosion and Sediment Control Lead (CESCL).

An individual who is knowledgeable in the principles and practices of erosion and sediment control. The CESCL shall have the skills to assess the:

1. site conditions and construction activities that could impact the quality of stormwater, and
2. effectiveness of erosion and sediment control measures used to control the quality of stormwater discharges.

The CESCL shall have current certification through an approved erosion and sediment control training program that meets the minimum training standards established by the Washington State department of ecology.

H. Change of Use.

For purposes of modification of a preliminary plat, “change of use” shall mean a change in the proposed use of lots (e.g., residential to commercial).

I. Channel Migration Zone (CMZ).

A corridor of variable width that includes the current river plus adjacent area through which the channel has migrated or is likely to migrate within a given timeframe, usually one hundred years.

J. Channelization.

The straightening, relocation, deepening, or lining of stream channels, including construction of continuous revetments or levees for the purpose of preventing gradual, natural meander progression.



K. City.

The City of Spokane, Washington.

L. Clear Street Width.

The width of a street from curb to curb minus the width of on-street parking lanes.

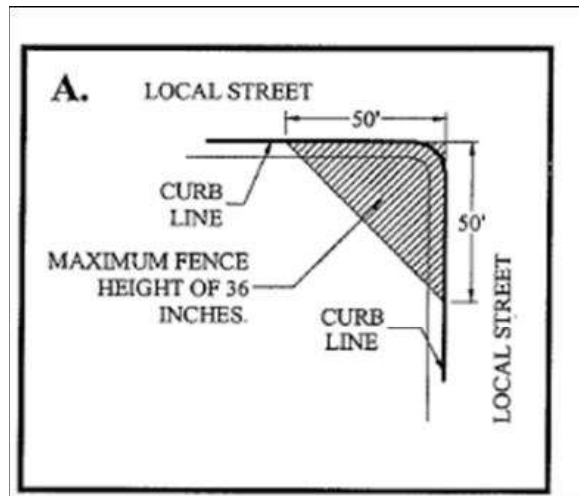
M. Clear Pedestrian Zone.

Area reserved for pedestrian traffic; typically included herein as a portion of overall sidewalk width to be kept clear of obstructions to foot traffic.

N. Clear View Triangle.

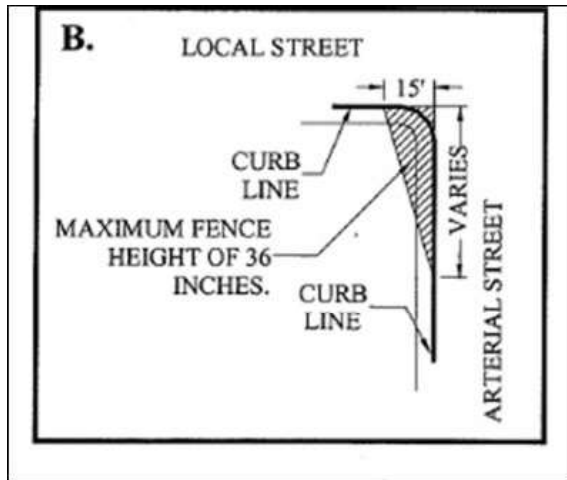
A clear view maintained within a triangular space at the corner of a lot so that it does not obstruct the view of travelers upon the streets.

1. Intersection of two local streets: A right isosceles triangle having sides of fifty feet measured along the curb line of each intersecting residential street((; or)).

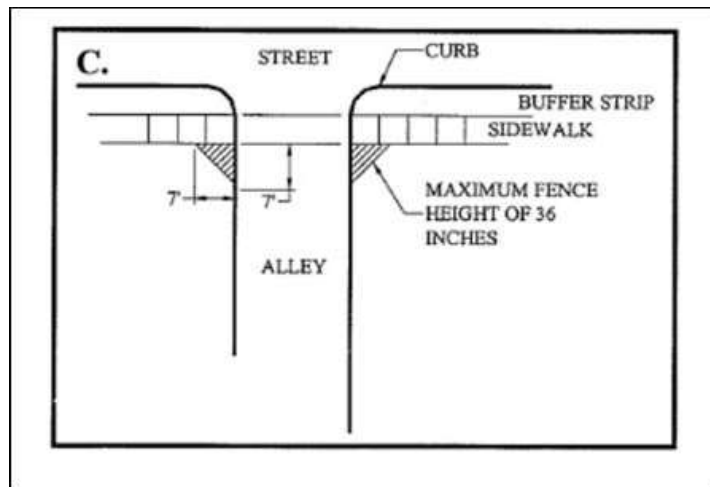


2. Intersection of local and arterial: A right triangle having a fifteen-foot side measured along the curb line of the residential street and a seventy-five foot side along the curb line of the intersecting arterial street, except that when the arterial street has a speed limit of thirty-five miles per hour, the triangle has a side along such arterial of one hundred twenty-two feet, or when the arterial speed limit is 40 mph or greater the dimensions of the triangle shall be determined by Street Department staff using AASHTO's A Policy on Geometric Design as a reference.((; or))





3. Alleys: A right isosceles triangle having sides of seven feet measured along the right-of-way line of an alley and:
  - a. the inside line of the sidewalk; or
  - b. if there is no sidewalk, a line seven feet inside the curb line.



O. Clear Zone.

((An unobstructed, relatively flat area provided beyond the edge of the traveled way for the recovery of errant vehicles.)) The roadside area free of obstacles, starting at the edge of the traveled way.

P. Clearing.

The removal of vegetation or plant cover by manual, chemical, or mechanical means. Clearing includes, but is not limited to, actions such as cutting, felling, thinning, flooding, killing, poisoning, girdling, uprooting, or burning.



Q. Cliffs.

1. A type of habitat in the Washington department of fish and wildlife (WDFW) priority habitat and species system that is considered a priority due to its limited availability, unique species usage, and significance as breeding habitat. Cliffs are greater than twenty-five feet high and below five thousand feet elevation.
2. A "cliff" is a steep slope of earth materials, or near vertical rock exposure. Cliffs are categorized as erosion landforms due to the processes of erosion and weathering that produce them. Structural cliffs may form as the result of fault displacement or the resistance of a cap rock to uniform downcutting. Erosional cliffs form along shorelines or valley walls where the most extensive erosion takes place at the base of the slope.

R. Closed Record Appeal Hearing.

A hearing, conducted by a single hearing body or officer authorized to conduct such hearings, that relies on the existing record created during a quasi-judicial hearing on the application. No new testimony or submission of new evidence and information is allowed.

S. Collector Arterial.

~~((A relatively low speed street serving an individual neighborhood.))~~ Collector arterials (consisting of Major and Minor Collectors) collect and distribute traffic from local streets to principal and minor arterials. They serve both land access and traffic circulation.

~~((1. Collector arterials are typically two-lane roads with on-street parking.))~~

~~((2. Their function is to collect and distribute traffic from local access streets to principal and minor arterials.))~~

T. Co-location.

Is the locating of wireless communications equipment from more than one provider on one structure at one site.

U. Colony.

A hive and its equipment and appurtenances, including one queen, bees, comb, honey, pollen, and brood.

V. Commercial Driveway.

Any driveway access to a public street other than one serving a single-family or duplex residence on a single lot.

W. Commercial Vehicle.

Any vehicle the principal use of which is the transportation of commodities, merchandise, produce, freight, animals, or passengers for hire.

X. Commission – Historic Landmarks.

The City/County historic landmarks commission.



Y. Community Banner.

See [SMC 17C.240.015](#).

Z. Community Meeting.

An informal meeting, workshop, or other public meeting to obtain comments from the public or other agencies on a proposed project permit prior to the submission of an application.

1. A community meeting is between an applicant and owners, residents of property in the immediate vicinity of the site of a proposed project, the public, and any registered neighborhood organization or community council responsible for the geographic area containing the site of the proposal, conducted prior to the submission of an application to the City of Spokane.
2. A community meeting does not constitute an open record hearing.
3. The proceedings at a community meeting may be recorded and a report or recommendation shall be included in the permit application file.

AA. Compensatory Mitigation.

Replacing project-induced wetland losses or impacts, and includes, but is not limited to, the following:

1. Restoration.

The manipulation of the physical, chemical, or biological characteristics of a site with the goal of returning natural or historic functions to a former or degraded wetland. For the purpose of tracking net gains in wetland acres, restoration is divided into re-establishment and rehabilitation.

2. Re-establishment.

The manipulation of the physical, chemical, or biological characteristics of a site with the goal of returning natural or historic functions to a former wetland. Re-establishment results in a gain in wetland acres (and functions). Activities could include removing fill material, plugging ditches, or breaking drain tiles.

3. Rehabilitation.

The manipulation of the physical, chemical, or biological characteristics of a site with the goal of repairing natural or historic functions of a degraded wetland. Rehabilitation results in a gain in wetland function but does not result in a gain in wetland acres. Activities could involve breaching a dike to reconnect wetlands to a floodplain or return tidal influence to a wetland.

4. Creation (Establishment).

The manipulations of the physical, chemical, or biological characteristics present to develop a wetland on an upland or deepwater site where a wetland did not previously exist. Establishment results in a gain in wetland acres. Activities typically involve excavation of upland soils to elevations that will produce a wetland hydroperiod, create hydric soils, and support the growth of hydrophytic plant species.

5. Enhancement.

The manipulation of the physical, chemical, or biological characteristics of a wetland site to heighten, intensify, or improve specific function(s) or to change the growth stage or composition of the vegetation present.



Enhancement is undertaken for specified purposes such as water quality improvement, flood water retention, or wildlife habitat. Enhancement results in a change in some wetland functions and can lead to a decline in other wetland functions, but does not result in a gain in wetland acres. Activities typically consist of planting vegetation, controlling non-native or invasive species, modifying site elevations or the proportion of open water to influence hydroperiods, or some combination of these activities.

6. Protection/Maintenance (Preservation).

Removing a threat to, or preventing the decline of, wetland conditions by an action in or near a wetland. This includes the purchase of land or easements, repairing water control structures or fences or structural protection such as repairing a barrier island. This term also includes activities commonly associated with the term preservation. Preservation does not result in a gain of wetland acres, may result in a gain in functions, and will be used only in exceptional circumstances.

AB. Comprehensive Plan.

The City of Spokane comprehensive plan, a document adopted pursuant to chapter 36.70A RCW providing land use designations, goals and policies regarding land use, housing, capital facilities, housing, transportation, and utilities.

AC. Conceptual Landscape Plan.

A scale drawing showing the same information as a general site plan plus the location, type, size, and width of landscape areas as required by the provisions of chapter [17C.200 SMC](#).

1. The type of landscaping, L1, L2, or L3, is required to be labeled.
2. It is not a requirement to designate the scientific name of plant materials on the conceptual landscape plan.

AD. Concurrency Certificate.

A certificate or letter from a department or agency that is responsible for a determination of the adequacy of facilities to serve a proposed development, pursuant to chapter [17D.010 SMC](#), Concurrency Certification.

AE. Concurrency Facilities.

Facilities for which concurrency is required in accordance with the provisions of this chapter. They are:

1. transportation,
2. public water,
3. fire protection,
4. police protection,
5. parks and recreation,
6. libraries,
7. solid waste disposal and recycling,
8. schools, and



9. public wastewater (sewer and stormwater).

AF. Concurrency Test.

The comparison of an applicant's impact on concurrency facilities to the available capacity for public water, public wastewater (sewer and stormwater), solid waste disposal and recycling, and planned capacity for transportation, fire protection, police protection, schools, parks and recreation, and libraries as required in [SMC 17D.010.020](#).

AG. Conditional Use Permit.

A "conditional use permit" and a "special permit" are the same type of permit application for purposes of administration of this title.

AH. Condominium.

Real property, portions of which are designated for separate ownership and the remainder of which is designated for common ownership solely by the owners of those portions. Real property is not a condominium unless the undivided interests in the common elements are vested in unit owners, and unless a declaration and a survey map and plans have been recorded pursuant to chapter 64.34 RCW.

AI. Confidential Shelter.

Shelters for victims of domestic violence, as defined and regulated in chapter 70.123 RCW and WAC 248-554. Such facilities are characterized by a need for confidentiality.

AJ. Congregate Residence.

A dwelling unit in which rooms or lodging, with or without meals, are provided for nine or more non-transient persons not constituting a single household, excluding single-family residences for which special or reasonable accommodation has been granted.

AK. Conservancy Environments.

Those areas designated as the most environmentally sensitive and requiring the most protection in the current shoreline master program or as hereafter amended.

AL. Container.

Any vessel of sixty gallons or less in capacity used for transporting or storing critical materials.

AM. Context Areas

Established by the Regulating Plan, Context Area designations describe and direct differing functions and features for areas within FBC limits, implementing community goals for the built environment.

AN. Contributing Resource



Contributing resource is any building, object, structure, or site which adds to the historical integrity, architectural quality, or historical significance of the local or federal historic district within which the contributing resource is located.

**AO. Conveyance.**

In the context of chapter [17D.090 SMC](#) or chapter [17D.060 SMC](#), this term means a mechanism for transporting water from one point to another, including pipes, ditches, and channels.

**AP. Conveyance System.**

In the context of chapter [17D.090 SMC](#) or chapter [17D.060 SMC](#), this term means the drainage facilities and features, both natural and constructed, which collect, contain and provide for the flow of surface and stormwater from the highest points on the land down to receiving water. The natural elements of the conveyance system include swales and small drainage courses, streams, rivers, lakes, and wetlands. The constructed elements of the conveyance system include gutters, ditches, pipes, channels, and most flow control and water quality treatment facilities.

**AQ. Copy.**

See [SMC 17C.240.015](#).

**AR. Cottage Housing.**

1. A grouping of individual structures where each structure contains one or two dwelling units.
2. The land underneath the structures may or may not be divided into separate lots.
3. A cottage housing development may contain detached accessory buildings for storing vehicles. It may also include a community building, garden shed, or other facility for use of the residents.
4. The types of units allowed in cottage housing development are detached cottages, attached unit homes and carriage units. For the purposes of [SMC 17C.110.350](#), the definitions of these types are:

- a. Cottage. A detached, single-family residential building.





b. Attached Unit Home. A structure containing two dwelling units designed to look like a single-family home.



c. Carriage Unit. A single-family dwelling unit located above a garage structure.



AR. Council.

The city council of the City of Spokane.

AS. County.

Usually capitalized, means the entity of local government or, usually not capitalized, means the geographic area of the county, not including the territory of incorporated cities and towns.

AT. Covenants, Conditions, and Restrictions (CC&Rs).

A document setting forth the covenants, conditions, and restrictions applicable to a development, recorded with the Spokane County auditor and, typically, enforced by a property owner's association or other legal entity.

AU. Creep.

Slow, downslope movement of the layer of loose rock and soil resting on bedrock due to gravity.

AV. Critical Amount.

The quantity component of the definition of critical material.

AW. Critical Aquifer Recharge Areas (CARA).

Critical aquifer recharge areas (CARA) include locally identified aquifer sensitive areas (ASA) and wellhead protection areas.

AX. Critical Areas.

Any areas of frequent flooding, geologic hazard, fish and wildlife habitat, aquifer sensitive areas, or wetlands as defined under [chapter 17E.010 SMC](#), [chapter](#)



17E.020 SMC, chapter 17E.030 SMC, chapter 17E.040 SMC, and chapter 17E.070.SMC.

AY. Critical Facility.

A facility for which even a slight chance of flooding might be too great. Critical facilities include, but are not limited to:

1. schools;
2. nursing homes;
3. hospitals;
4. police;
5. fire;
6. emergency response installations; and
7. installations which produce, use, or store hazardous materials or hazardous waste.

AZ. Critical Material.

1. A compound or substance, or class thereof, designated by the division director of public works and utilities which, by intentional or accidental release into the aquifer or ASA, could result in the impairment of one or more of the beneficial uses of aquifer water and/or impair aquifer water quality indicator levels. Beneficial uses include, but are not limited to: domestic and industrial water supply,
  - a. domestic and industrial water supply,
  - b. agricultural irrigation,
  - c. stock water, and
  - d. fish propagation.

Used herein, the designation is distinguished from state or other designation.

2. A list of critical materials is contained in the Critical Materials Handbook, including any City modifications thereto.

BA. Critical Material Activity.

A land use or other activity designated by the manager of engineering services as involving or likely to involve critical materials. A list of critical materials activities is contained in the Critical Materials Handbook.

BB. Critical Materials Handbook.

The latest edition of a publication as approved and amended by the division director of public works and utilities from time to time to accomplish the purposes of this chapter.

1. The handbook is based on the original prepared by the Spokane water quality management program ("208") coordination office, with the assistance of its technical advisory committee. It is on file with the director of engineering services and available for public inspection and purchase.



2. The handbook, as approved and modified by the division director of public works and utilities, contains:
  - a. a critical materials list,
  - b. a critical materials activities list, and
  - c. other technical specifications and information.
3. The handbook is incorporated herein by reference. Its provisions are deemed regulations authorized hereunder and a mandatory part of this chapter.

#### BC. Critical Review.

The process of evaluating a land use permit request or other activity to determine whether critical materials or critical materials activities are involved and, if so, to determine what appropriate measures should be required for protection of the aquifer and/or implementation of the Spokane aquifer water quality management plan.

#### BD. Critical Review Action.

1. An action by a municipal official or body upon an application as follows:
  - a. Application for a building permit where plans and specifications are required, except for Group R and M occupancies ([SMC 17G.010.140](#) and [SMC 17G.010.150](#)).
  - b. Application for a shoreline substantial development permit ([SMC 17G.060.070\(B\)\(1\)](#)).
  - c. Application for a certificate of occupancy ([SMC 17G.010.170](#)).
  - d. Application for a variance or a certificate of compliance ([SMC 17G.060.070\(A\)](#) or [SMC 17G.060.070\(B\)\(1\)](#)).
  - e. Application for rezoning ([SMC 17G.060.070\(A\)](#)).
  - f. Application for conditional permit ([SMC 17G.060.070\(A\)](#)).
  - g. Application for a business license ([SMC 8.01.120](#)).
  - h. Application for a permit under the Fire Code ([SMC 17F.080.060](#)).
  - i. Application for a permit or approval requiring environmental review in an environmentally sensitive area ([SMC 17E.050.260](#)).
  - j. Application for connection to the City sewer or water system.
  - k. Application for construction or continuing use of an onsite sewage disposal system ([SMC 13.03.0149](#) and [SMC 13.03.0304](#)).
  - l. Application for sewer service with non-conforming or non-standard sewage ([SMC 13.03.0145](#), [SMC 13.03.0314](#), and [SMC 13.03.0324](#)).
  - m. Application involving a project identified in [SMC 17E.010.120](#).
  - n. Issuance or renewal of franchise; franchisee use of cathodic protection also requires approval or a franchise affecting the City water supply or water system.
  - o. Application for an underground storage tank permit ([SMC 17E.010.210](#)); and
  - p. Application for permit to install or retrofit aboveground storage tank(s) ([SMC 17E.010.060\(A\)](#) and [SMC 17E.010.400\(D\)](#)).



2. Where a particular municipal action is requested involving a land use installation or other activity, and where said action is not specified as a critical review action, the City official or body responsible for approval may, considering the objectives of this chapter, designate such as a critical review action and condition its approval upon compliance with the result thereof.

**BE. Critical Review Applicant.**

A person or entity seeking a critical review action.

**BF. Critical Review Officer – Authority.**

1. The building official or other official designated by the director of public works and utilities.
2. For matters relating to the fire code, the critical review officer is the fire official.
3. The critical review officer carries out and enforces the provisions of this chapter and may issue administrative and interpretive rulings.
4. The critical review officer imposes requirements based upon this chapter, regulations, and the critical materials handbook.
5. The officer may adopt or add to any requirement or grant specific exemptions, where deemed reasonably necessary, considering the purpose of this chapter

**BG. Critical Review Statement.**

A checklist, disclosure form, or part of an application for a critical review action, disclosing the result of critical review. Where not otherwise provided as part of the application process, the critical review officer may provide forms and a time and place to file the statement.

**BH. Cumulative Impacts.**

The combined, incremental effects of human activity on ecological or critical area functions and values. Cumulative impacts result when the effects of an action are added to or interact with other effects in a particular place and within a particular time. It is the combination of these effects, and any resulting environmental degradation, that should be the focus of cumulative impact analysis and changes to policies and permitting decisions.

**BI. Curb Ramp.**

A ramp constructed in the sidewalk to ~~((allow wheelchair access))~~ provide an accessible route from the sidewalk to the street.

**BJ. Cutbank.**

The concave bank of a moving body of water that is maintained as a steep or even overhanging cliff by the actions of water at its base.

Section 4. That SMC section 17A.020.130 is amended to read as follows:



## 17A.020.130 “M” Definitions

### A. Main Assembly Area.

The principal room for persons gathering for religious services.

### B. Maintenance.

Or “repair” means those usual activities required to prevent a decline, lapse, or cessation from a lawfully established condition or to restore the character, scope, size, and design of a serviceable area, structure, or land use to a state comparable to its previously authorized and undamaged condition. This does not include any activities that change the character, scope, or size of the original structure, facility, utility, or improved area beyond the original design.

### C. Manufactured Home.

1. “Manufactured home” is a single-family dwelling unit constructed after June 15, 1976, built in accordance with department of housing and urban development Manufactured Home Construction and Safety Standards Act, which is a national, preemptive building code.
2. “Manufactured home accessory structure” is any attached or detached addition to a manufactured home, such as an awning, basement, carport, garage, porch, or storage structure, which is ordinarily appurtenant.

### D. Manufactured Home Park.

Two or more manufactured homes or mobile homes used as dwelling units on a single parcel or lot.

### E. Marquee Sign.

[See SMC 17C.240.015.](#)

### F. Marsh.

A low, flat wetland area on which the vegetation consists mainly of herbaceous plants such as cattails, bulrushes, tules, sedges, skunk cabbage, or other hydrophytic plants. Shallow water usually stands on a marsh at least during part of the year.

### G. Mean Annual Flow.

The average flow of a river or stream (measured in cubic feet per second) from measurements taken throughout the year. If available, flow data for the previous ten years should be used in determining mean annual flow.

### H. Mining.

The extraction and removal of sand, gravel, minerals, or other naturally occurring material from the earth for economic use.

### I. Minor Arterials

~~((A two- to four-lane facility which collects and distributes traffic from principal~~



~~arterials to collector arterials and local access streets.))~~ A street providing service for trips of moderate length, connecting the principal arterial system to local streets, generally prioritizing mobility over access, and providing intra-community circulation.

J. Mitigation – Mitigate.

An action which avoids a negative adverse impact and is reasonable and capable of being accomplished.

K. Mitigation – Mitigation Sequencing.

The use of any or all of the following actions listed in descending order of preference:

1. Avoiding the impact altogether by not taking a certain action or parts of an action.
2. Minimizing impacts by limiting the degree or magnitude of the action and its implementation, by using appropriate technology, or by taking affirmative steps to avoid or reduce impacts.
3. Rectifying the impact by repairing, rehabilitating, or restoring the affected environment.
4. Reducing or eliminating the impact over time by preservation and maintenance operations during the life of the action.
5. Compensating for the impact by replacing, enhancing, or providing substitute resources or environments; or
6. Monitoring the impact and the compensation project and taking appropriate corrective measures.

Mitigation may include a combination of the above measures.

L. Mobile Home.

A factory-built dwelling built prior to June 15, 1976, to standards other than the housing and urban development code, and acceptable under applicable state codes in effect at the time of construction or introduction of the home into the state. Mobile homes have not been built since introduction of the housing and urban development Manufactured Home Construction and Safety Standards Act.

M. Mobile Home Park.

Any real property which is rented or held out for rent to others for the placement of two or more mobile homes, manufactured homes, or park models for the primary purpose of production of income, except where such real property is rented or held out for rent for seasonal recreational purpose only and is not intended for year-round occupancy.

N. Modification to a Preliminary Plat, Short Plat, or Binding Site Plan.

A change, prior to recording, of an approved preliminary plat, preliminary short plat, or binding site plan that includes, but is not limited to, the addition of new lots or tracts, or a change of the boundaries or dimensions of lots or tracts.



O. Modular Home.

A single-family dwelling unit (which may be in the form of a factory-built or manufactured housing permit as well as a standard building permit) constructed in a factory in accordance with International Building Code and bearing the appropriate gold insignia indicating such compliance. The term includes “pre-fabricated,” “panelized,” and “factory-built” units.

P. Modulation.

A measured and proportioned inflection in a building’s face. Articulation, modulation, and their interval create a sense of scale important to residential buildings.

Q. Monitoring.

Periodic evaluation of a wetlands restoration, creation, or enhancement site or habitat management plan area to determine changes at the site, such as vegetation growth, hydrologic changes, soil development, and use of the site by birds and animals.

R. Monument.

A physical survey monument as shown in the City's standard plans.

S. Monument Sign.

[See SMC 17C.240.015.](#)

T. Multi-family Residential Building.

A common wall dwelling or apartment house that consists of three or more dwelling units.

U. Multiple Containment.

A means of spill or leak control involving a containment structure having one or more layers of material between the primary container and the environment.

1. Containment layers must be resistant to the material stored.
2. The volume within the containment system must be at least as large as the primary container.
3. Containment layers may be separated by an interstitial space.

V. Municipal Separate Storm Sewer System (MS4).

A conveyance, or system of conveyances (including roads with drainage systems, municipal streets, catch basins, curbs, gutters, ditches, manmade channels, or storm drains):

1. owned or operated by a state, city, town, borough, county, parish, district, association, or other public body (created by or pursuant to state law) having jurisdiction over disposal of wastes, stormwater, or other wastes, including special districts under state law such as sewer district, flood control district, or drainage district, designated and approved management



- agency under section 208 of the Clean Water Act that discharges to water of the United States;
- 2. designed or used for collecting or conveying stormwater;
- 3. which is not a combined sewer; and
- 4. which is not part of a publicly owned treatment works (POTW) as defined at 40 CFR (Code of Federal Regulation) 122.2.

W. MUTCD.

The U.S. department of transportation Manual on Uniform Traffic Control Devices.

Section 5. That SMC section 17A.020.160 is amended to read as follows:

**17A.020.160 “P” Definitions**

A. Painted Wall Highlights.

[See SMC 17C.240.015.](#)

B. Painted Wall Sign.

[See SMC 17C.240.015.](#)

C. Parcel.

See “Lot” ([SMC 17A.020.120](#)).

D. Parkway.

~~((1. A street serving as a principal, minor, or collector arterial, typically with recreational or scenic opportunities.))~~

~~((2. Parkways will often have landscaped medians.))~~

A thoroughfare designated as a collector or arterial, with a median reflecting the park-like character implied in the name - [SMC 17D.050A.040.U](#).

E. Party of Record.

Any person who has appeared at a hearing of the hearing examiner by presenting testimony or making written comment.

F. Paved Area.

- 1. An uncovered, hard-surfaced area or an area covered with a perforated hard surface (such as “Grasscrete”) that is able to withstand vehicular traffic or other heavy-impact uses.
- 2. Graveled areas are not paved areas.

G. Pedestrian Buffer Strips (PBS).

A hard-surfaced or planted area(s) between travel or parking lanes and sidewalks, also called planting strips. PBS improves safety by separating



vehicles and pedestrians and provide space for drainage, street trees and snow storage.

H. Pedestrian Path.

A continuous, unobstructed, reasonably direct route between an on-site parking lot and a Primary Building Entry designed and suitable for pedestrian use. Minimum requirements for Pedestrian Paths are listed in [Section 17C.123.040](#) of the FBC.

I. Pedestrian-Scaled Fixtures (lighting).

Pole-mounted light fixtures placed and designed to illuminate foot-traffic areas including exterior lots, pathways or sidewalks. For purposes of the HFBC, Pedestrian-Scaled Fixtures are defined by height as measured from ground to bottom of shade or bulb.

J. Pedestrian-Scaled Signs.

[See SMC 17C.240.015.](#)

K. Pedestrian Street.

1. A street designated on the official zoning map as a pedestrian street where development standards are required to promote a pedestrian friendly street. Pedestrian streets offer a pleasant and safe walking environment. Design features include minimal interruptions of the sidewalk by driveways, publicly usable site furnishing such as benches, tables, and bike racks, and visually interesting buildings close to the sidewalk.

L. Performance Guarantee.

A “financial guarantee” providing for and securing to the City the actual construction and installation of the required improvements.

M. Performance/Warranty Retainer.

A “financial guarantee” both providing for and securing to the City the actual construction and installation of such improvements, and securing to the City the successful operation of the improvements for two years after the City’s final inspection and acceptance of the improvements.

N. Permanent Erosion and Sediment Control Measures.

A combination of plants, mulch, sod, matting, erosion control blankets, and permanent structures that will provide long-term soil stabilization.

O. Permanent Sign.((1))

[See SMC 17C.240.015.](#)

P. Permanent Stabilization.

See Permanent Erosion and Sediment Control Measures.



Q. Permeable Sediment.

Sediment permitting the flow of water.

R. Person.

Any natural person, whether acting individually or in a representative capacity, partnership, joint venture, corporation, or other legal entity.

S. Pier.

Any platform structure, fill, or anchored device in or floating upon water bodies to provide moorage for watercraft engaged in commerce, including, but not limited to, wharves, mono-buoys, quays, ferry terminals, and fish weighing station.

T. Planned Capacity.

For all capital facilities, except transportation, capacity for a concurrency facility that does not exist, but for which the necessary facility construction, expansion, or modification project is contained in the current adopted City of Spokane comprehensive plan, capital improvement program and scheduled to be completed within six years. (RCW 36.70A.020).

U. Planned Capacity for Transportation Facilities.

Capacity for transportation facilities, including roads and transit, that does not exist, but where transportation improvements or strategies to accommodate the impacts of development are made concurrent with the development.

1. These strategies may include:

- a. increased public transportation service,
- b. ride sharing programs,
- c. demand management, and
- d. other transportation systems management strategies.

2. For transportation facilities, "concurrent with the development" shall mean that improvements or strategies are in place at the time of development, or that a financial commitment is in place to complete the improvements or strategies within six years (RCW 36.70A.070(6)(b)).

V. Planned Unit Development (PUD).

1. A planned unit development is a project permit for an overlay zone, approved by the hearing examiner, which does not fully comply with all of the development standards of the base zone in which it is located, but is approved based on superior or innovative design.
2. The City may permit a variety of types, design, and arrangement of structures and enable the coordination of project characteristics with features of a particular site in a manner consistent with the public health, safety, and welfare.



W. Plans.

Planning documents, which are developed by the various departments of the City, pertaining to the orderly development of public facilities.

X. Planting Zone.

Area for street trees, ground cover or other plantings; typically included herein as a portion of overall sidewalk width reserved for locating permanent trees and tree grates.

Y. Plat – Final.

A map or representation of a subdivision, showing thereon the division of a tract or parcel of land into lots, blocks, streets, alleys, or other divisions and dedications and containing all elements and requirements set forth in this chapter and chapter 58.17 RCW.

Z. Plat – Preliminary.

1. A neat and approximate drawing of a proposed subdivision showing the general layout of streets, alleys, lots, blocks, and other elements of a subdivision required by this chapter and chapter 58.17 RCW.
2. The preliminary plat shall be the basis for the approval or disapproval of the general layout of a subdivision.

AA. Plaza.

Areas generally open to the public on a controlled basis and used for passive recreational activities and relaxation.

Plazas are paved areas typically provided with amenities, such as seating, drinking, and ornamental fountains, art, trees, and landscaping, for use by pedestrians.

AB. Plinth.

The base or platform upon which a building wall or column appears to rest, helping establish pedestrian-scaled elements and aesthetically tying the building to the ground.

AC. Pollutant.

Any substance which is prohibited or limited by applicable laws or regulations, which is released or discharged in conjunction with development. Any substance that causes or contributes to violation of air, land, or water quality standards, released or discharged.

AD. Pollution.

Contamination, or other alteration of the physical, chemical, or biological properties of air, land, water or wetlands, or such discharge of any liquid, gaseous, solid, radioactive, or other substance into air, land, water, or wetlands as will or is likely to cause a nuisance or render such air, land, water, or wetlands harmful, detrimental, or injurious to the public health, safety, or welfare,



or to domestic, commercial, industrial, agricultural, recreational, or other legitimate beneficial uses, or to livestock, wildlife, fish, native vegetation, or other aquatic life.

**AE. Potential Geologically Hazardous Areas.**

Areas designated on maps maintained in the City's planning services department. They are classified "potential" because they have not been confirmed by field investigation nor do they necessarily include the full extent of all geologically hazardous areas within the City. The maps are intended to alert property owners, purchasers, developers, etc., to the possible existence of significant geological hazards, which may warrant further geotechnical study.

**AF. Practicable Alternative.**

An alternative that is available and capable of being carried out after taking into consideration cost, existing technology, and logistics in light of overall project purposes and having less impact to critical areas. It may involve using an alternative site in the general region that is available to the applicant and may feasibly be used to accomplish the project.

**AG. Predevelopment Meetings.**

Meetings between City or agency staff and an applicant or their representatives prior to formal submission of a detailed application. They are intended to provide an overview of the regulatory requirements, application process, and procedural submission requirements.

**AH. Principal Buildings.**

Where multiple buildings occupy a single lot, those buildings that are associated with the prevailing use of that site.

**AI. Primary Building Entry.**

Access or entrance of first rank, importance or value, visually associated with the prevailing ground-floor use of a building.

**AJ. Primary Building Walls.**

Any exterior building wall that faces a street and contains a public entrance to the occupant's premises or tenant space. If an individual tenant space does not have a street facing wall, or does not have a street facing wall containing a public entrance, then the primary building wall for that individual tenant space is any wall containing a public entrance that faces a parking area on the site. (See Figure 1, [SMC 17C.240.130](#), Primary Building Walls)

**AK. Primary Container.**

The container that is in direct contact with the material of concern during the course of normal transport, use, or storage.



AL. Primary Drainage Basin.

The basin of the stream or tributary within which a project is proposed, not including basins of major tributaries. For the purpose of this regulation the primary drainage basin of:

1. Latah Creek is not a part of the primary drainage basin of the Spokane River,
2. Marshall Creek is not a part of the primary drainage basin of Latah Creek.

AM. Primary Structure.

1. A structure or combination of structures of chief importance or function on a site. In general, the primary use of the site is carried out in a primary structure.
2. The difference between a primary and accessory structure is determined by comparing the size, placement, similarity of design, use of common building materials, and the orientation of the structures on a site.

AN. Primary Use.

1. An activity or combination of activities of chief importance on the site. One of the main purposes for which the land or structures are intended, designed or ordinarily used.
2. A site may have more than one primary use.

AO. Principal Arterials.

~~((A four to six lane street serving as a primary facility for access between the central business district, major employment districts, and major shopping centers.))~~ A street serving major activity centers, providing a high degree of mobility and serving the longest trip demands within the urban area.

AP. Priority Habitats.

Habitat areas determined by WDFW to have unique or significant value to many species and that meet one or more of the following criteria:

1. High wildlife density.
2. High species diversity.
3. Important wildlife breeding habitat.
4. Important wildlife seasonal ranges.
5. Important movement corridors.
6. Limited availability.
7. High vulnerability to habitat alteration.

AQ. Priority Species.

A wildlife species requiring protective measures for their perpetuation due to their population status, their sensitivity to habitat alteration, and/or their recreational importance.



AR. Private Street.

Roadway which is not controlled or maintained by a public authority, and which serve two or more properties.

AS. Project Permit or Project Permit Application.

Any land use or environmental permit or license required for a project action, including, but not limited to, building permits, short plats, subdivisions, binding site plans, planned unit developments, conditional uses, shoreline substantial development permits, site plan review, permits, or approvals required by the critical area ordinance, and site specific rezones authorized by a comprehensive plan or subarea plan, but excluding the adoption or amendment of a comprehensive plan, subarea plan, or development regulations, except as otherwise specifically identified under RCW 36.70B.140.

AT. Projecting Sign.

[See SMC 17C.240.015.](#)

AU. Protected Species.

A general classification of animals by WDFW that includes all those species not classified as listed, game, fur-bearing, or non-protected. This also includes all birds not classified as game or non-protected.

AV. Proximity.

That two or more properties are either adjacent or separated by a street or alley.

AW. Public Access.

The public's right to get to and use the City's public waters, the water/land interface and associated shoreline area. It includes physical access that is either lateral (areas paralleling the shore) or perpendicular (an easement or public corridor to the shore), and/or visual access facilitated by means such as scenic streets and overlooks, viewing towers, and other public sites or facilities.

AX. Public Facilities.

Any City-owned, operated, or contracted public facility or service in whole, or in part, whether existing or planned, including, but not limited to:

1. parks,
2. recreation facilities,
3. playgrounds,
4. streets,
5. transportation facilities,
6. open spaces,
7. fire facilities,
8. storm water drainage ponds, and
9. all such appurtenances and improvements.



AY. Public Property.

Any City-owned real property, air space, or other interest in real estate, including streets, alleys, or other public rights-of-way, owned by or controlled by this municipality or any other governmental unit.

AZ. Public Way.

1. A dedicated "public way" is a tract of land:
  - a. conveyed or reserved by deed,
  - b. dedicated by plat, or
  - c. acquired by decree of court,
  - d. which has been accepted and dedicated by action of the city council to the public right-of-way and for secondary use as an easement for public utilities.
2. An "alley" is a public way, usually not exceeding sixteen feet in width, designed or intended to provide secondary access to abutting properties.

Section 6. That SMC section 17A.020.190 is amended to read as follows:

**17A.020.190 "S" Definitions**

A. Salmonid.

Belonging to the family of Salmonidae, including the salmons, trouts, chars, and whitefishes.

B. Sandwich Board Sign.

[See SMC 17C.240.015.](#)

C. Scrub-shrub Wetland.

An area of vegetated wetland with at least thirty percent of its surface area covered by woody vegetation less than twenty feet in height at the uppermost strata.

D. Secondary Building Walls.

Exterior building walls that are not classified as primary building walls.

E. Secondary Containment.

A means of spill or leak containment involving a second barrier or tank constructed outside the primary container and capable of holding the contents of the primary container.

F. Sediment.

Mineral or organic matter deposited as a result of erosion.

G. Sedimentation.

The settling and accumulation of particles such as soil, sand, and gravel, suspended in water or in the air.



H. SEPA Rules.

Chapter 197-11 WAC adopted by the department of ecology.

I. Service Area.

A geographic area defined by the City, which encompasses public facilities that are part of a plan.

J. Serviceable.

Means presently useable.

K. Setback.

The minimum distance required between a specified object, such as a building and another point. Setbacks are usually measured from lot lines to a specified object. In addition, the following setbacks indicate where each setback is measured from:

1. "Front setback" means a setback that is measured from a front lot line.
2. "Rear setback" means a setback that is measured from a rear lot line.
3. "Side setback" means a setback that is measured from a side lot line.
4. "Street setback" means a setback that is measured from a street lot line.

L. Sex Paraphernalia Store.

A commercial establishment that regularly features sexual devices and regularly advertises or holds itself out, in any medium, as an establishment that caters to adult sexual interests. This definition shall not be construed to include:

1. Any pharmacy, drug store, medical clinic, any establishment primarily dedicated to providing medical or healthcare products or services; or
2. Any establishment located within an enclosed regional shopping mall.

M. Sexual Device.

Any three dimensional object designed for stimulation of the male or female human genitals, anus, buttocks, female breast, or for sadomasochistic use or abuse of oneself or others and shall include devices commonly known as dildos, vibrators, penis pumps, cock rings, anal beads, butt plugs, nipple clamps, and physical representations of the human genital organs. Nothing in this definition shall be construed to include devices primarily intended for protection against sexually transmitted diseases or for preventing pregnancy.

N. Shall.

Unless the context indicates otherwise, the term "shall" means:

1. In reference to the obligations imposed by this title upon owners or occupants of premises or their agents, a mandatory obligation to act, or when used with a negative term to refrain from acting, in compliance with this code at the risk of denial of approval or civil or criminal liability upon failure so to act, the term being synonymous with "must";



2. With respect to the functions of officers and agents of the City, a direction and authorization to act in the exercise of sound discretion; or
3. The future tense of the verb “to be.”

O. Shallow Groundwater.

Naturally occurring water within an unconfined (water table) aquifer, partially confined aquifer or perched groundwater aquifer, and which is present at depth of fifteen feet or less below the ground surface, at any time, under natural conditions.

P. Shared Use Pathway.

A non-motorized transportation pathway shared by pedestrians, scooters and bicyclists. May be located next to a street or in a separate right-of-way.

~~((P.))~~Q. Shorelands.

Or “shoreline areas” or “shoreline jurisdiction” means all “shorelines of the state” and “shorelands” as defined in RCW 90.58.030. Those lands extending landward for two hundred feet in all directions as measured on a horizontal plane from the ordinary high-water mark; floodways and contiguous floodplain areas landward two hundred feet from such floodways; and all wetlands and river deltas associated with the streams, lakes, and tidal waters which are subject to the provisions of the entire shoreline master program; the same to be designated as to location by the department of ecology.

~~((Q.))~~R. Shoreline and Ecosystems Enhancement Plan and Program.

See SMC 17E.020.090, Habitat Management Plans.

~~((R.))~~S. Shoreline Buffer.

1. A designated area adjacent to the ordinary high-water mark and running landward to a width as specified by this regulation intended for the protection or enhancement of the ecological function of the shoreline area.
2. The buffer will consist primarily of natural vegetation or planted vegetation which maintains or enhances the ecological functions of the shoreline area.
3. The term “buffer area” has the same meaning as “buffer.”

~~((S.))~~T. Shoreline Enhancement.

Any alteration of the shoreline that improves the ecological function of the shoreline area or any aesthetic improvement that does not degrade the shoreline ecological function of the shoreline.

~~((T.))~~U. Shoreline Environment Designations.

The categories of shorelines established by local shoreline master programs in order to provide a uniform basis for applying policies and use regulations within distinctively different shoreline areas. The basic recommended system classifies



shorelines into four distinct environments (natural, conservancy, rural, and urban). See WAC 173-16-040(4).

~~((U-))~~V. Shoreline Habitat and Natural Systems Enhancement Projects.

1. Shoreline habitat and natural systems enhancement projects include those activities proposed and conducted specifically for the purpose of establishing, restoring, or enhancing habitat for propriety species in shorelines.
2. Provided that the primary purpose of such actions is clearly restoration of the natural character and ecological functions of the shoreline, projects may include shoreline modification actions such as:
3. Modification of vegetation,
4. Removal of nonnative or invasive plants,
5. Shoreline stabilization, dredging, and filling.

~~((V-))~~W. Shoreline Jurisdiction.

See "Shorelands."

~~((W-))~~X. Shoreline Letter of Exemption.

Authorization from the City which establishes that an activity is exempt from shoreline substantial development permit requirements under [SMC 17E.060.300](#) and WAC 173-14-040, but subject to regulations of the Act and the entire shoreline master program.

~~((X-))~~Y. Shoreline Master Program.

1. The comprehensive use plan for a described area, and the use regulations together with maps, diagrams, charts, or other descriptive material and text, a statement of desired goals, and standards developed in accordance with the policies enunciated in RCW 90.58.020.
2. For the City of Spokane, the shoreline master program includes the:
3. Shoreline Goals and Policies (Comprehensive Plan Chapter 14),
4. Shoreline Regulations ([chapter 17E.060 SMC](#)),
5. City of Spokane Shoreline Restoration Plan (stand-alone document), and
6. Shoreline Inventory and Analysis (Comprehensive Plan Volume III).

~~((Y-))~~Z. Shoreline Mixed Use.

Combination of water-oriented and non-water oriented uses within the same structure or development area.

~~((Z-))~~AA. Shoreline Modifications.

Those actions that modify the physical configuration or qualities of the shoreline area, usually through the construction of a physical element such as a dike, breakwater, pier, weir, dredged basin, fill, bulkhead, or other shoreline structure. They can include other actions, such as clearing, grading, or application of chemicals.



~~((AA-))~~AB. Shoreline Protection.

1. Structural and nonstructural methods to control flooding or address erosion impacts to property and dwellings or other structures caused by natural processes, such as current, flood, wind, or wave action.
2. The terms “Shoreline protection measure” and this term have the same meaning.
3. Substantial enlargement of an existing shoreline protection improvement is regarded as new shoreline protection measure.

~~((AB-))~~AC. Shoreline Recreational Development.

Recreational development includes commercial and public facilities designed and used to provide recreational opportunities to the public. Water-dependent, water-related and water-enjoyment recreational uses include river or stream swimming areas, boat launch ramps, fishing areas, boat or other watercraft rentals, and view platforms

~~((AG-))~~AD. Shoreline Restoration.

1. The re-establishment or upgrading of impaired ecological shoreline processes or functions. This may be accomplished through measures including, but not limited to, re-vegetation, removal of intrusive shoreline structures and removal or treatment of toxic materials.
2. Restoration does not imply a requirement for returning the shoreline area to aboriginal or pre-European settlement conditions.

~~((AD-))~~AE. Shoreline Stabilization.

Structural or non-structural modifications to the existing shoreline intended to reduce or prevent erosion of uplands or beaches. They are generally located parallel to the shoreline at or near the ordinary high-water mark. Other construction classified as shore defense works include groins, jetties, and breakwaters, which are intended to influence wave action, currents, and/or the natural transport of sediments along the shoreline.

~~((AE-))~~AF. Shoreline Structure.

A permanent or temporary edifice or building, or any piece of work artificially built or composed of parts joined together in some definite manner, whether installed on, above, or below the surface of the ground or water, except for vessels.

~~((AF-))~~AG. Shorelines Hearings Board (SHB).

The shorelines hearings board is a quasi-judicial body with powers of de novo review authorized by chapter 90.58 RCW to adjudicate or determine the following matters:

1. Appeals from any person aggrieved by the granting, denying, or rescinding of a permit issued or penalties incurred pursuant to chapter 90.58 RCW.
2. Appeals of department rules, regulations, or guidelines; and



3. Appeals from department decisions to approve, reject, or modify a proposed master program or program amendment of local governments which are not planning under RCW 36.70A.040.

~~((AG-))~~AH. Short Plat – Final.

The final drawing of the short subdivision and dedication, prepared for filing for record with the Spokane county auditor and containing all elements and requirements set forth in this chapter and chapter 58.17 RCW.

~~((AH-))~~AI. Short Plat – Preliminary.

1. A neat and approximate drawing of a proposed short subdivision showing the general layout of streets, alleys, lots, blocks, and other elements of a short subdivision required by this title and chapter 58.17 RCW.
2. The preliminary short plat shall be the basis for the approval or disapproval of the general layout of a short subdivision.

~~((AI-))~~AJ. Short Subdivision.

A division or redivision of land into nine or fewer lots, tracts, parcels, or sites for the purpose of sale, lease, or transfer of ownership. (RCW 58.17.020(6)).

~~((AJ-))~~AK. Sign.

[See SMC 17C.240.015.](#)

~~((AK-))~~AL. Sign – Animated Sign.

[See SMC 17C.240.015.](#)

~~((AL-))~~AM. Sign – Electronic Message Center Sign.

[See SMC 17C.240.015.](#)

~~((AM-))~~AN. Sign Face.

[See SMC 17C.240.015.](#)

~~((AN-))~~AO. Sign – Flashing Sign.

[See SMC 17C.240.015.](#)

~~((AO-))~~AP. Sign Maintenance.

[See SMC 17C.240.015.](#)

~~((AP-))~~AQ. Sign – Off-premises.

[See SMC 17C.240.015.](#)

~~((AQ-))~~AR. Sign Repair.

[See SMC 17C.240.015.](#)

~~((AR-))~~AS. Sign Structure.

[See SMC 17C.240.015.](#)



~~((AS-))~~AT. Significant Vegetation Removal.

The removal or alteration of trees, shrubs, and/or ground cover by clearing, grading, cutting, burning, chemical means, or other activity that causes significant ecological impacts to functions provided by such vegetation.

1. The removal of invasive or noxious weeds does not constitute significant vegetation removal.
2. Tree pruning, not including tree topping, where it does not affect ecological functions, does not constitute significant vegetation removal.

~~((AT-))~~AU. Single-family Residential Building.

A dwelling containing only one dwelling unit.

~~((AU-))~~AV. Single-room Occupancy Housing (SRO).

A structure that provides living units that have separate sleeping areas and some combination of shared bath or toilet facilities.

1. The structure may or may not have separate or shared cooking facilities for the residents.
2. SRO includes structures commonly called residential hotels and rooming houses.

~~((AV-))~~AW. Site.

Any parcel of land recognized by the Spokane County assessor's office for taxing purposes. A parcel may contain multiple lots.

~~((AW-))~~AX. Site – Archaeological.

1. A place where a significant event or pattern of events occurred. It may be the:
  - a. Location of prehistoric or historic occupation or activities that may be marked by physical remains; or
  - b. Symbolic focus of a significant event or pattern of events that may not have been actively occupied.
2. A site may be the location of a ruined or now non-extant building or structure if the location itself possesses historic, cultural, or archaeological significance.

~~((AX-))~~AY. Site, Parent.

The initial aggregated area containing a development, and from which individual lots may be divided, as used in the context of SMC 17C.110.360 Pocket Residential Development, and SMC 17G.080.065, Alternative Residential Subdivisions.

~~((AY-))~~AZ. Slump.

The intermittent movement (slip) of a mass of earth or rock along a curved plane.

~~((AZ-))~~BA. SMC.

The Spokane Municipal Code, as amended.



~~((BA-))~~BB. Soil.

The naturally occurring layers of mineral and organic matter deposits overlaying bedrock. It is the outer most layer of the Earth.

~~((BB-))~~BC. Sound Contours.

A geographic interpolation of aviation noise contours as established by the 2010 Fairchild AFB Joint Land Use Study and placed on the official zoning map. When a property falls within more than one noise zone, the more restrictive noise zone requirements shall apply for the entire property.

~~((BC-))~~BD. Sound Transmission Class (STC).

A single-number rating for describing sound transmission loss of a wall, partition, window or door.

~~((BD-))~~BE. Special Drainage District (SDD).

An area associated with shallow groundwater, intermittent standing water, or steep slopes where infiltration of water and dispersion of water into the soils may be difficult or delayed, creating drainage or potential drainage problems. SDDs are designated in [SMC 17D.060.130](#).

~~((BE-))~~BF. Special Event Sign. See [SMC 17C.240.015](#).

~~((BF-))~~BG. Species of Concern.

Species native to Washington State listed as state endangered, state threatened, state sensitive, or state candidate, as well as species listed or proposed for listing by the U.S. Fish and Wildlife Service or the National Marine Fisheries Service.

~~((BG-))~~BH. Specified Anatomical Areas.

They are human:

1. Genitals, pubic region, buttock, and female breast below a point immediately above the top of the areola, when such areas are less than completely and opaquely covered;
2. Male genitals in a discernibly turgid state, even if completely and opaquely covered.

~~((BH-))~~BI. Specified Sexual Activities.

Any of the following:

1. Human genitals in a state of sexual stimulation or arousal;
2. Acts of human masturbation, sexual intercourse, or sodomy; and
3. Fondling or other erotic touching of human genitals, pubic region, buttock, or female breast.

~~((BI-))~~BJ. Spokane Regional Stormwater Manual (SRSW).

A technical document establishing standards for stormwater design and



management to protect water quality, natural drainage systems, and down-gradient properties as urban development occurs.

~~((BJ.))~~BK. Spokane Register of Historic Places.

The register maintained by the historic preservation office, which includes historic landmarks and districts in the City and County.

~~((BK.))~~BL. Sports Field.

An open area or stadium in which scheduled sports events occur on a regular basis. Sports events include both competitive and noncompetitive events such as track and field activities, soccer, baseball, or football games.

~~((BL.))~~BM. Stabilization.

The process of establishing an enduring soil cover of vegetation or mulch or other ground cover and may be in combination with installation of temporary or permanent structures.

~~((BM.))~~BN. Standard Plans.

Refers to the City of Spokane's standard plans.

~~((BN.))~~BO. Standard References

Standard engineering and design references identified in [SMC 17D.060.030](#).

~~((BO.))~~BP. State Candidate Species.

Fish and wildlife species that WDFW will review for possible listing as state endangered, threatened, or sensitive.

~~((BP.))~~BQ. State Endangered Species.

Any wildlife species native to the State of Washington that is seriously threatened with extinction throughout all or a significant portion of its range within the state.

~~((BQ.))~~BR. State Register.

The register maintained pursuant to chapter 195, Laws of 1977, 1st ex. sess., section 6 (chapter 27.34 RCW).

~~((BR.))~~BS. State Sensitive Species.

Any wildlife species native to the State of Washington that is vulnerable or declining and is likely to become endangered or threatened throughout a significant portion of its range within the state without cooperative management or removal of threats.

~~((BS.))~~BT. State Threatened Species.

Any wildlife species native to the State of Washington that is likely to become an endangered species within the foreseeable future throughout a significant portion of its range within the state without cooperative management or removal of threats.



~~((BT.))~~BU. Stealth Facilities.

Any cellular telecommunications facility that is designed to blend into the surrounding environment. Examples of stealth facilities include:

1. Architecturally screened roof-mounted antennas;
2. Building-mounted antennas painted to match the existing structure;
3. Antennas integrated into architectural elements; and
4. Antenna structures designed to look like light poles, trees, clock towers, bell steeples, or flag poles.

~~((BU.))~~BV. Stewardship.

Acting as supervisor or manager of the City and County's historic properties.

~~((BV.))~~BW. Stormwater.

1. Any runoff flow occurring during or following any form of natural precipitation, and resulting from such precipitation, including snowmelt.
2. "Stormwater" further includes any locally accumulating ground or surface waters, even if not directly associated with natural precipitation events, where such waters contribute or have a potential to contribute to runoff onto the public right-of-way, public storm or sanitary sewers, or flooding or erosion on public or private property.

~~((BW.))~~BX. Stormwater Management Program (SWMP).

A set of actions and activities designed to reduce the discharge of pollutants from the regulated MS4 to the maximum extent practicable and to protect water quality, and comprising the components listed in S5 or S6 of the Eastern Washington Phase II Municipal Permit (WAR04-6505) and any additional actions necessary to meet the requirements of applicable TMDLs.

~~((BX.))~~BY. Story.

That portion of a building included between the upper surface of any floor and the upper surface of the floor next above, except:

1. The topmost story is that portion of a building included between the upper surface of the topmost floor and the ceiling or roof above;
2. That portion of a building between the eaves and the ridge, when over twenty feet in height, is considered a story;
3. That portion of a building below the eaves which exceeds fourteen feet in height is considered a story, each fourteen feet of height (or major part of fourteen feet) being an additional story; and
4. A basement or unused under-floor space is a story if the finished floor level directly above is either more than:
  - a. Six feet above grade for more than half of the total perimeter, or
  - b. Twelve feet above grade at any point.

~~((BY.))~~BZ. Stream.

A naturally occurring body of periodic or continuously flowing water where the:



1. Mean annual flow is greater than twenty cubic feet per second; and
2. Water is contained with a channel (WAC 173-22-030(8)).

~~((BZ-))~~CA. Street.

See "Public Way" ([SMC 17A.020.160](#)).

~~((GA-))~~CB. Street Classifications.

1. Arterial and local access streets are classified in section 4.5 of the comprehensive plan as follows:
  - a. Principal arterial.
  - b. Minor arterial.
  - c. Collector arterial.
  - d. Local access street.
  - e. Parkway.
2. Definitions of all of the above classifications are included herein. Private streets are not classified but are defined under [SMC 17A.020.160, "P" Definitions](#).

~~((CB-))~~CC. Street Frontage.

The lot line abutting a street.

~~((GG-))~~CD. Strobe Light.

A lamp capable of producing an extremely short, brilliant burst of light.

~~((GD-))~~CE. Structural Alteration.

[See SMC 17C.240.015](#).

~~((GE-))~~CF. Structure.

Any object constructed in or on the ground, including a gas or liquid storage tank that is principally above ground.

1. Structure includes:
  - a. Buildings,
  - b. Decks,
  - c. Fences,
  - d. Towers,
  - e. Flag poles,
  - f. Signs, and
  - g. Other similar objects.
2. Structure does not include paved areas or vegetative landscaping materials.

~~((GF-))~~CG. Structure – Historic.

A work made up of interdependent and interrelated parts in a definite pattern of organization. Generally constructed by man, it is often an engineering project.



~~((GG-))~~CH. Subdivision.

A division or redivision of land into ten or more lots, tracts, or parcels for the purpose of sale, lease, or transfer of ownership (RCW 58.17.020).

~~((CH-))~~CI. Subject Property.

The site where an activity requiring a permit or approval under this code will occur.

~~((CI-))~~CJ. Sublevel Construction Controls.

Design and construction requirements provided in [SMC 17F.100.090](#).

~~((CJ-))~~CK. Submerged Aquatic Beds.

Wildlife habitat area made up of those areas permanently under water, including the submerged beds of rivers and lakes and their aquatic plant life.

~~((CK-))~~CL. Substantial Damage – Floodplain.

Damage of any origin sustained by a structure whereby the cost of restoring the structure to its pre-existing condition would equal or exceed fifty percent of the assessed value of the structure before the damage occurred.

~~((CL-))~~CM. Substantial Development.

For the shoreline master program, shall mean any development of which the total cost or fair market value exceeds the dollar amount set forth in RCW 90.58 and WAC 173-26 for any improvement of property in the shorelines of the state.

~~((CM-))~~CN. Substantial Improvement – Floodplain.

1. Any repair, reconstruction, or improvement of a structure, the cost of which equals or exceeds fifty percent of the assessed value of the structure either:
  - a. Before the improvement or repair is started, or
  - b. If the structure has been damaged and is being restored, before the damage occurred.
2. For the purposes of this definition, “substantial improvement” is considered to occur when the first alteration of any wall, ceiling, floor, or other structural part of the building commences, whether or not that alteration affects the external dimensions of the structure.
3. The term does not, however, include either any:
  - a. Project for improvement of a structure to correct existing violations of state or local health, sanitary, or safety code specifications which have been identified by the local code enforcement official and which are the minimum necessary to assure safe living conditions; or
  - b. Alteration of a structure listed on the National Register of Historic or State Inventory of Historic Places.



~~((CN-))~~CO. Suffix.

Describes the roadway type and is located after the root roadway name (i.e., street, avenue, court, lane, way, etc.). The appropriate suffix shall be used in accordance with [SMC 17D.050A.040\(U\)](#).

Section 7. That SMC section 17A.020.200 is amended to read as follows:

**17A.020.200 “T” Definitions**

A. Temporary Erosion and Sediment Control Measures.

Erosion and sediment control devices used to provide temporary stabilization of a site, usually during construction or ground disturbing activities, before permanent devices are installed.

B. Temporary Sign.

A sign placed on a structure or the ground for a specifically limited period of time as provided in [SMC 17C.240.240\(G\)](#).

C. Temporary Structure.

A structure approved for location on a lot by the department for a period not to exceed six months with the intent to remove such structure after the time period expires.

D. Tenant Space.

Portion of a structure occupied by a single commercial lease holder with its own public entrance from the exterior of the building or through a shared lobby, atrium, mall, or hallway and separated from other tenant spaces by walls.

E. Through Pedestrian Zone.

The portion of a sidewalk that is intended for pedestrian travel and is entirely free of permanent and temporary objects.

F. Tideland.

Land on the shore of marine water bodies between the line of ordinary high tide and the line of extreme low tide.

G. Total Maximum Daily Load (TMDL).

A calculation of the maximum amount of a pollutant that a water body can receive and still meet water quality standards, and an allocation of that amount to the pollutant's sources. A TMDL is the sum of the allowable loads of a single pollutant from all contributing point and non point sources. The calculation shall include a margin of safety to ensure that the water body can be used for the purposes the state has designated. The calculation shall also account for seasonable variation in water quality. Water quality standards are set by states, territories, and tribes. They identify the uses for each water body, for example, drinking water supply, contact recreation (swimming), and aquatic life support



(fishing), and the scientific criteria to support that use. The Clean Water Act, section 303, establishes the water quality standards and TMDL programs.

H. [Deleted].

I. [Deleted].

J. [Deleted].

K. Tracking.

The deposition of sediment onto paved surfaces from the wheels of vehicles.

L. Tract.

A piece of land created and designated as part of a land division that is not a lot, lot of record or a public right-of-way. Tracts are created and designated for a specific purpose. Land uses within a tract are restricted to those uses consistent with the stated purpose as described on the plat, in maintenance agreements, or through conditions, covenants and restrictions (CC&Rs).

M. Traveled Way.

The area of street which is intended to carry vehicular traffic, ~~((including))~~ excluding any shoulders.

N. Type I Application.

An application for a project permit that is subject to an administrative approval and is not categorically exempt from environmental review under chapter 43.21C RCW (SEPA) and the City of Spokane Environmental Ordinance [chapter 17E.050 SMC](#), and does not require a public hearing. Type I applications are identified in [Table 17G.060-1](#) in [chapter 17G.060 SMC](#). These applications may include, but are not limited to, building permits and grading permits.

O. Type II Application.

An application for a project permit that is subject to an administrative decision of a department director, that may or may not be categorically exempt from chapter 43.21C RCW (SEPA), and does not require a public hearing. The Type II applications are identified in [Table 17G.060-1](#) in [chapter 17G.060 SMC](#). These applications may include, but are not limited to, short plats, binding site plans, shoreline substantial development permits, and some conditional use permits; provided, the planning director may require conditional use permits which are otherwise characterized as Type II applications under this title to be submitted and processed as Type III applications when the director issues written findings that the Type III process is in the public interest.

P. Type III Application.

An application for a project permit that is subject to a quasi-judicial decision of the hearing examiner that may or may not be categorically exempt from chapter



43.21C RCW (SEPA) and the City of Spokane Environmental Ordinance [chapter 17E.050 SMC](#) and requires a public hearing. Type III applications are identified in [Table 17G.060-1](#) in [chapter 17G.060 SMC](#). These applications may include, but are not limited to, rezones, conditional use permits, preliminary long plats, or shoreline conditional use permits.

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON THE STREET ENGINEERING DESIGN STANDARDS CHAPTER 3 AND SMC UPDATES**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE proposed updates to Chapter 3 of the City of Spokane Street Engineering Design Standards and accompanying revisions to Spokane Municipal Code (SMC) 17A.020, 17C.200 and 17H.010.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs provide for implementation of the transportation section of the Comprehensive Plan.
- C. Proposed updates to Chapter 3 Design Standards –Streets, Alleys, Bikeways & Sidewalks include updates to street geometrics, stormwater facilities, bicycle facilities, access management, clear zones, design vehicles, and street lighting.
- D. Proposed revisions to SMC 17A.020 Definitions address roadway surfaces, arterial street types, bicycle facilities, clear zones, curb ramps, parkways, and shared-use pathways.
- E. Proposed revisions to SMC 17C.200 Street Tree Requirements, 12.01 and 12.02 tree grates and overhanging vegetation.
- F. Proposed revisions to SMC 17H.010 Engineering Standards address transit as a street use, Transportation Impact Fees, right-of-way width, local street widths, on-street parking, snow storage, medians, traffic calming, shared-use pathways, pedestrian buffer strips, curb ramps, street lighting, horizontal clearances, and the bicycle network.
- G. Pursuant to SMC 04.12.010 the Plan Commission shall hold public hearings and make recommendations to the City Council regarding development regulations implementing the Comprehensive Plan.
- H. On November 5, 2019, July 21, 2020, and August 11, 2020, the Plan Commission Transportation Subcommittee received a presentation on proposed updates to Chapter 3 of the Design Standards and corresponding SMC updates.
- I. On November 18, 2019, a group of public stakeholders in the disability and active transportation communities were gathered for an All Ages and Abilities Focus Discussion, and received a presentation on proposed updates to Chapter 3 Design Standards and related revisions to SMCs.
- J. On November 22, 2019, a group of public stakeholders in the development community received a presentation on the proposed updates to Chapter 3 Design Standards and the related revisions to SMCs.



- K. On June 24, 2020, the Design Review Board of the City of Spokane received a presentation regarding the proposed updates to the street design standards as addressed in Chapter 3 Design Standards, and corresponding revisions to related SMCs.
- L. On July 16, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to development regulations.
- M. On July 22, 2020, a State Environmental Policy Act (SEPA) checklist was issued with a request for comments from agencies, departments, and neighborhood councils. The deadline to comment was August 10, 2020. The City received several comments on the documents and made revisions.
- N. The City Plan Commission held workshops on July 22<sup>nd</sup> and August 12<sup>th</sup>, 2020, to obtain public comments on Chapter 3 of the Design Standards and the SMC updates.
- O. On March 23<sup>rd</sup>, 2020 and August 24<sup>th</sup>, 2020, the Public Infrastructure, Environment, and Sustainability Committee of the City Council received presentations on Chapter 3 of the Design Standards and related SMC updates.
- P. On August 31, 2020, a Determination of Non-Significance (DNS) was issued for the Proposal. The deadline to appeal the determination was September 22, 2020. No comments on the SEPA determination were received.
- Q. The City Plan Commission held a public hearing beginning on September 23<sup>rd</sup>, 2020 and continuing on October 14<sup>th</sup>, 2020 to consider Chapter 3 of the Design Standards and related SMC updates.
- R. The City Council must receive a recommendation from the City Plan Commission to certify that update to Chapter 3 of the Engineering Design Standards and accompanying revisions to SMCs are in conformance with the City's Comprehensive Plan in effect on the day of certification.

#### **CONCLUSIONS:**

- A. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been prepared in full consideration of the City's Comprehensive Plan.
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been reviewed by the City Plan Commission and found to be in conformance with the goals and policies of the City's 2001 Comprehensive Plan.

#### **RECOMMENDATIONS:**

- A. The Spokane City Plan Commission is certifying that the update to Chapter 3 of the Engineering Design Standards and accompanying Spokane Municipal Code revisions



are in conformance with the City of Spokane's Comprehensive Plan as required by RCW 36.70A and are recommended for adoption by the Spokane City Council.

- B. By a vote of 8 to 0, the Plan Commission recommends the approval of these amended documents by the City Council.

  
Todd Beyreuther (Nov 3, 2020 12:40 PST)

Nov 3, 2020

---

**Todd Beyreuther, President**  
Spokane Plan Commission  
October \_\_, 2020








# Plan Commission Engineering Design Standards

Final Audit Report

2020-11-03

|                 |                                               |
|-----------------|-----------------------------------------------|
| Created:        | 2020-11-03                                    |
| By:             | Jackie Churchill (jchurchill@spokanecity.org) |
| Status:         | Signed                                        |
| Transaction ID: | CBJCHBCAABAA0A-sozYn37EwtYYE0cyKINZRAQ-YN7v_  |

## "Plan Commission Engineering Design Standards" History

-  Document created by Jackie Churchill (jchurchill@spokanecity.org)  
2020-11-03 - 6:37:05 PM GMT- IP address: 73.83.158.109
-  Document emailed to Todd Beyreuther (tbeyreuther@spokanecity.org) for signature  
2020-11-03 - 6:38:52 PM GMT
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-  Document e-signed by Todd Beyreuther (tbeyreuther@spokanecity.org)  
Signature Date: 2020-11-03 - 8:40:04 PM GMT - Time Source: server- IP address: 107.77.205.172
-  Agreement completed.  
2020-11-03 - 8:40:04 PM GMT





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/9/2020

**Clerk's File #**

ORD C35984

**Renews #****Cross Ref #****Submitting Dept**INTEGRATED CAPITAL  
MANAGEMENT**Contact Name/Phone**

INGA NOTE 625-6331

**Project #****Contact E-Mail**

INOTE@SPOKANECITY.ORG

**Bid #****Agenda Item Type**

First Reading Ordinance

**Requisition #****Agenda Item Name**4250 - ORDINANCE AMENDING SMC 17C.200 AND 12.02.0202 STREET DESIGN  
STANDARD**Agenda Wording**

An ordinance relating to street development standards for the Unified Development Code; amending section 17C.200.050, 12.01.010, and 12.02.0202 of the Spokane Municipal Code.

**Summary (Background)**

Amending these sections to include discussion of tree grates, pruning and overhead clearances.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Select \$

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Council Notifications****Dept Head**

MILLER, KATHERINE E

**Study Session\Other**

10/26/20

**Division Director**

SIMMONS, SCOTT M.

**Council Sponsor**

CP Beggs

**Finance**

ALBIN-MOORE, ANGELA

**Distribution List****Legal**

PICCOLO, MIKE

eraea@spokanecity.org

**For the Mayor**

ORMSBY, MICHAEL

inote@spokanecity.org

**Additional Approvals**

mdavis@spokanecity.org

**Purchasing**

kemiller@spokanecity.org

icmaccounting@spokanecity.org

jrichman@spokanecity.org



## ORDINANCE NO. C35984

AN ORDINANCE relating to street development standards for the Unified Development Code; amending sections 17C.200.050, 12.01.010, and 12.02.0202 of the Spokane Municipal Code.

WHEREAS, the City of Spokane is updating Chapter 3 of the Design Standards, and as such must ensure consistency with Chapter 17C.200 and Title 12; and

WHEREAS, on July 16, 2020 the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to development regulations; and

WHEREAS, on July 22, 2020 a State Environmental Policy Act (SEPA) checklist was issued with a request for comments on the proposal, many comments were received and incorporated into the document; and

WHEREAS, on August 31, 2020 a Determination of Non-Significance was issued by the responsible official, and no appeal was received prior to the deadline of September 22, 2020; and

WHEREAS, on or about October 14, 2020 following a process involving a number of public workshops and a public hearing, a majority of the City of Spokane Plan Commission voted to recommend approval of an ordinance amending the Engineering Standards in Chapter 17 SMC; -

Now Therefore, the City of Spokane does ordain:

Section 1. That SMC section 17C.200.050 is amended to read as follows:

### **17C.200.050 Street Tree Requirements**

#### **A. Purpose.**

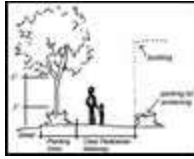
To provide consistent street frontage character within the street right-of-way. The street tree standards also maintain and add to Spokane's tree canopy and enhance the overall appearance of commercial and neighborhood development. Trees are an integral aspect of the Spokane landscape and add to the livability of Spokane. They provide aesthetic and economic value to property owners and the community at large.

#### **B. Street Tree Implementation.**

1. Street trees are required along all city streets in downtown, commercial, center and corridor, industrial, residential and FBC zones.



2. Street trees shall be planted between the curb and the walking path of the sidewalk.



3. Street trees, tree grates and other landscaping shall be maintained and irrigated by the adjacent property owner. If the adjacent property owner fails to maintain the adjacent street trees and other landscaping, the City may perform the required tree and other landscaping work at the abutting property owner's expense as referenced in [SMC 12.02.0210\(B\)\(2\)](#).
4. If a street has a uniform planting of street trees or a distinctive species within the right-of-way, then new street trees should be of a similar form, character and planting pattern.
5. For a full list of approved trees in the city of Spokane, see the urban forestry program's approved street tree list. Species selection should be guided by individual site conditions including hydrology, soil, solar orientation, and physical constraints

#### C. Planting Zones.

1. Provide continuous planting strips or individual planting areas per Table 17C.200.050-1, Tree Planting Dimensional Standards.

| <b>TABLE 17C.200.050-1</b><br><b>Tree Planting Dimensional Standards [1]</b> |                                                                                             |                                                                              |
|------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------|------------------------------------------------------------------------------|
| <b>ZONE</b>                                                                  | <b>CONTINUOUS<br/>PLANTING STRIP</b><br>(minimum width as<br>measured from back of<br>curb) | <b>INDIVIDUAL<br/>PLANTING AREA</b><br>(width as measured from back of curb) |
| Downtown                                                                     | Individual Planting Areas<br>(tree vaults) required [1]                                     | 4 ft. minimum<br>6 ft. maximum [2]                                           |
| CC                                                                           | 5 ft.                                                                                       | 4 ft. minimum<br>6 ft. maximum [2]                                           |
| FBC                                                                          | Individual Planting Areas<br>(tree vaults ) required [1]                                    | 5 ft [2]                                                                     |
| Commercial                                                                   | 5 ft.                                                                                       | 4 ft. minimum<br>6 ft. maximum [2]                                           |
| Industrial                                                                   | 6 ft.                                                                                       | Continuous Planting Strip required [3]                                       |
| RA, RSF, RTF                                                                 | 6 ft.                                                                                       | Continuous Planting Strip required [3]                                       |
| RMF, RHD                                                                     | 6 ft.                                                                                       | Continuous Planting Strip required [3]                                       |



|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                    |                |                                       |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------|---------------------------------------|
| School/Church Loading Zone                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         | Not Applicable | 4 ft. minimum<br>6 ft. maximum [2, 4] |
| <p>Notes:</p> <p>[1] Individual Planting Areas (tree vaults) are the standard for the Downtown and FBC Zones. Proposals for Continuous Planting Strips may be evaluated on a case by case basis.</p> <p>[2] Un-compacted soils are necessary for street trees. Individual planting areas (or tree vaults) must be of a size to accommodate a minimum of 100 cubic feet of un-compacted soils per tree at a maximum depth of three feet. Refer to the Engineering Design Standards for examples of potential options in individual planting areas.</p> <p>[3] Continuous Planting Strips are the standard for Industrial and Residential Zones. However, individual planting areas meeting the CC standard may be proposed and evaluated on a case by case basis in Industrial, RMF and RHD Zones.</p> <p>[4] In all zones, within a school/church loading zone, street tree location may vary from the standard as long as street trees are located within the right-of-way.</p> <p>[5] In all zones, when a continuous planting strip will double as a stormwater swale, the minimum width shall be 6.5 feet.</p> |                |                                       |

## 2. Continuous Planting Strips.

- a. Continuous planting strips may be planted with living ground cover or low plantings that are maintained at a height less than three (3) feet from ground level.
- b. When auto traffic is immediately adjacent to the curb, new street trees must be planted at least three (3) feet from the edge of the automobile travel way.



## 3. Individual Planting Areas.

- a. When an individual planting area is not symmetrical, the longer dimension shall run along the curb.
- b. Tree grates or plantings are acceptable. However, when there is on-street parking, a tree grate or a paved walk eighteen (18) inches wide behind the curb are encouraged to help avoid conflicts with car doors and foot traffic. The minimum clear pedestrian walking path as required for the zone shall be maintained.

### Tree Grates





### **Street Trees with plantings up to 3 ft.**



- c. Where tree grates are used, they shall be ADA accessible and have a similar size and material as tree grates found in adjacent developments. Where tree grates are used, tree guards are encouraged for tree protection.

### **Tree Grate with Tree Guard**



- d. Un-compacted soils are necessary for street trees. A minimum of one hundred (100) cubic feet per tree at a maximum depth of three feet is required. See Engineering Design Standards for examples of potential options in individual planting areas and for retrofitting sidewalks.



### **D. Size Requirements for New Street Trees.**

1. Street trees shall meet the most recent ANSI standards for a two-inch caliper tree at the time of planting
2. Larger shade trees with spreading canopies or branches are desirable where possible. Species of street trees within the public rights-of-way shall



be approved by the City urban forester and reviewed by the director of engineering services.

3. If overhead power lines are present, street trees shall be limited to a mature height of twenty-five (25) feet to avoid conflict with utility lines and maintenance crews.

#### E. Spacing Requirements for Street Tree Spacing.

The objective, when planting and maintaining street trees, is to create and maintain in a healthy condition a continuous tree canopy over the sidewalk.

1. Continuous planting strips.

Average spacing shall be twenty-five (25) feet for small and columnar trees and thirty (30) feet for canopy trees. The planning director may allow increased spacing for exceptionally large trees or upon the recommendation of the urban forester.

2. Individual planting areas.

Average spacing for all tree sizes and types shall be twenty-five (25) feet. Trees planted adjacent to parallel parking stalls with meters may be spaced twenty (20) feet apart.

3. Street tree plantings shall consider the location of existing utilities, lighting, driveways, business entrances and existing and proposed signs. See the Engineering Design Standards for required dimensions.

#### F. Clear View Zone.

Landscaped areas between the curb and sidewalk, as well as landscaped areas within the clear view triangle as defined in [SMC 17A.020.030](#) shall be maintained or plant material chosen to maintain a vertical clear view zone between three and eight feet from ground level.



Section 2. That SMC section 12.01.010 amended to read as follows:

#### **12.01.010 Sidewalk Maintenance – Owner’s Responsibility**



- A. Every owner and occupant of premises shall keep the sidewalk area including tree grates adjacent to any portion of the real property (including corners) in good and safe condition and repair at all times.
- B. Where the director of engineering services determines there has been a failure to comply with this section, the director may send written notice to the premises advising of the violation. If the violation is not corrected, the director may proceed to remove or repair the condition, at the owner's and/or occupant's sole expense and liability.

Section 3. That SMC section 12.02.0202 is amended to read as follows:

**12.02.0202 Overhanging Vegetation to be Removed**

Owners of property within the City must prune (~~remove or destroy~~) all trees, plants, shrubs, or vegetation, or parts thereof, which overhang any sidewalk (8 feet of required clearance) or street (14 feet of required clearance) or which are growing thereon in such a manner as to obstruct or impair the free and full use of the sidewalk or street by the public. If pruning cannot remedy the obstruction, removal will be required. Such condition is declared a nuisance and subject to the process and provisions under [SMC 12.02.0210](#).

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON THE STREET ENGINEERING DESIGN STANDARDS CHAPTER 3 AND SMC UPDATES**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE proposed updates to Chapter 3 of the City of Spokane Street Engineering Design Standards and accompanying revisions to Spokane Municipal Code (SMC) 17A.020, 17C.200 and 17H.010.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs provide for implementation of the transportation section of the Comprehensive Plan.
- C. Proposed updates to Chapter 3 Design Standards –Streets, Alleys, Bikeways & Sidewalks include updates to street geometrics, stormwater facilities, bicycle facilities, access management, clear zones, design vehicles, and street lighting.
- D. Proposed revisions to SMC 17A.020 Definitions address roadway surfaces, arterial street types, bicycle facilities, clear zones, curb ramps, parkways, and shared-use pathways.
- E. Proposed revisions to SMC 17C.200 Street Tree Requirements, 12.01 and 12.02 tree grates and overhanging vegetation.
- F. Proposed revisions to SMC 17H.010 Engineering Standards address transit as a street use, Transportation Impact Fees, right-of-way width, local street widths, on-street parking, snow storage, medians, traffic calming, shared-use pathways, pedestrian buffer strips, curb ramps, street lighting, horizontal clearances, and the bicycle network.
- G. Pursuant to SMC 04.12.010 the Plan Commission shall hold public hearings and make recommendations to the City Council regarding development regulations implementing the Comprehensive Plan.
- H. On November 5, 2019, July 21, 2020, and August 11, 2020, the Plan Commission Transportation Subcommittee received a presentation on proposed updates to Chapter 3 of the Design Standards and corresponding SMC updates.
- I. On November 18, 2019, a group of public stakeholders in the disability and active transportation communities were gathered for an All Ages and Abilities Focus Discussion, and received a presentation on proposed updates to Chapter 3 Design Standards and related revisions to SMCs.
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- Q. The City Plan Commission held a public hearing beginning on September 23<sup>rd</sup>, 2020 and continuing on October 14<sup>th</sup>, 2020 to consider Chapter 3 of the Design Standards and related SMC updates.
- R. The City Council must receive a recommendation from the City Plan Commission to certify that update to Chapter 3 of the Engineering Design Standards and accompanying revisions to SMCs are in conformance with the City's Comprehensive Plan in effect on the day of certification.

#### **CONCLUSIONS:**

- A. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been prepared in full consideration of the City's Comprehensive Plan.
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been reviewed by the City Plan Commission and found to be in conformance with the goals and policies of the City's 2001 Comprehensive Plan.

#### **RECOMMENDATIONS:**

- A. The Spokane City Plan Commission is certifying that the update to Chapter 3 of the Engineering Design Standards and accompanying Spokane Municipal Code revisions



are in conformance with the City of Spokane's Comprehensive Plan as required by RCW 36.70A and are recommended for adoption by the Spokane City Council.

- B. By a vote of 8 to 0, the Plan Commission recommends the approval of these amended documents by the City Council.

  
Todd Beyreuther (Nov 3, 2020 12:40 PST)

Nov 3, 2020

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**Todd Beyreuther, President**  
Spokane Plan Commission  
October \_\_, 2020








# Plan Commission Engineering Design Standards

Final Audit Report

2020-11-03

|                 |                                               |
|-----------------|-----------------------------------------------|
| Created:        | 2020-11-03                                    |
| By:             | Jackie Churchill (jchurchill@spokanecity.org) |
| Status:         | Signed                                        |
| Transaction ID: | CBJCHBCAABAA0A-sozYn37EwtYYE0cyKINZRAQ-YN7v_  |

## "Plan Commission Engineering Design Standards" History

-  Document created by Jackie Churchill (jchurchill@spokanecity.org)  
2020-11-03 - 6:37:05 PM GMT- IP address: 73.83.158.109
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# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
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Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

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CITY ADMINISTRATOR SIGNATURE:

---





**Agenda Sheet for City Council Meeting of:**  
11/23/2020

|                       |            |
|-----------------------|------------|
| <b>Date Rec'd</b>     | 11/9/2020  |
| <b>Clerk's File #</b> | ORD C35985 |
| <b>Renews #</b>       |            |

|                           |                                                               |                      |  |
|---------------------------|---------------------------------------------------------------|----------------------|--|
| <b>Submitting Dept</b>    | INTEGRATED CAPITAL                                            | <b>Cross Ref #</b>   |  |
| <b>Contact Name/Phone</b> | IGNA NOTE 625-6331                                            | <b>Project #</b>     |  |
| <b>Contact E-Mail</b>     | INOTE@SPOKANECITY.ORG                                         | <b>Bid #</b>         |  |
| <b>Agenda Item Type</b>   | First Reading Ordinance                                       | <b>Requisition #</b> |  |
| <b>Agenda Item Name</b>   | 4250 - ORDINANCE AMENDING SMC 17H.010 STREET DESIGN STANDARDS |                      |  |

**Agenda Wording**

An ordinance relating to Engineering Standards, amending sections 17H.010.030, 17H.010.050, 17H.010.060, 17H.010.070, 17H.010.120, 17H.010.140, 17H.010.160, 17H.010.180, 17H.010.190, 17H.010.200, 17H.010.210, 17H.010.220, 17H.010.230, 17H.010.240,

**Summary (Background)**

Amendments include updates to traffic impact fee references, right-of-way widths for arterials and local street, widths of local streets for residential and other zoning, emergency vehicle staging areas, traffic calming, sidewalks, pedestrian buffer strips, curb ramps, crosswalks, driveways, street lighting, vertical clearances, horizontal clearances, and bicycle network.

|                      |                   |                       |
|----------------------|-------------------|-----------------------|
| Lease? NO            | Grant related? NO | Public Works? NO      |
| <b>Fiscal Impact</b> |                   | <b>Budget Account</b> |

|        |    |   |
|--------|----|---|
| Select | \$ | # |
| Select | \$ | # |
| Select | \$ | # |
| Select | \$ | # |

|                             |                     |                               |          |
|-----------------------------|---------------------|-------------------------------|----------|
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## City of Spokane Design Standards

Nov 1, 2020

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## STREETS, ALLEYS, BIKEWAYS, AND SIDEWALKS

### 3.0 Preface

The City of Spokane's adopted infrastructure standards require the design of integrated systems. For streets, this means not only that the full network of streets will function as a system, but that the other systems the streets intersect (transit, bike, emergency response, stormwater) will be seamlessly integrated.

Each section of the standards begins with a brief definition of the topic followed by the design standard.

The following key points guide this document.

- Street design is an iterative process, that entails flexibility and trade-offs. Within the built environment, particularly, physical space might limit what or how specific elements fit together to deliver a desired contextual experience. Decisions may be pushed by regulation, physical constraints, or public opinion, but ultimately should be guided by planned strategies and/or engineering standards.
- Balance is important. To maximize one component often means less achievement of another (prioritizing vehicle speeds often degrades conditions for people on foot and bike). The proper balance will vary from place to place in the city.
- Successful design will reflect community priorities, as defined through public outreach activities at planning, scoping, and design levels. Often, achieving a design that balances scope delivery, while accomplishing community goals will require compromise.
- The ultimate focus of street design should be how the street feels for users (drivers, pedestrians, shoppers, transit riders, residents, schoolchildren, etc.) on the ground level, and the desires of the city and community for how they want the space to function.

Transportation facility design will generally be based as either: new infrastructure built to facilitate development activities, or modification of pre-existing infrastructure. Development of new infrastructure will be held to the requirements here-in to deliver the most appropriate conditions to provide travel through the various urban conditions. Often the existing built environment does not adhere to today's standards, which have been updated over the years to reflect best practices. Thus, preservation or reconstruction work will often require more balancing of priority, and will necessarily vary from the standards due to limitations of space or inadequate meshing of facilities. New Development, Preservation, and Reconstruction work are defined in section 3.1 Definitions to provide guidance for the conditions wherein variance from the standards may be acceptable.



## 3.1 Definitions

**AASHTO** See Section 1.2

**ADA** See Section 1.2

**ADAAG** See Section 1.2

**Alley** A public or private way designed or intended to provide access to abutting properties. Alleys are generally not intended for through vehicle movements, but are useful to providing access to businesses and residences, and in some locations bicycle and pedestrian improvements.

**Arterial** See *Principal Arterial*, *Minor Arterial*, or *Collector Arterial*.

**Bicycle Facilities** Facilities designated for use by commuter and recreational bicyclists. The following types of bicycle facilities are identified in the Master Bicycle Plan:

- Neighborhood Greenway
- Shared Lane
- Bike Lane
- Bike-Friendly Route
- Shared Use Path
- Soft Surface Path (unpaved)

**Buffer Strips** Hard surfaced or landscaped areas between travel or parking lanes and sidewalks, also called Planting Strips.

**Center Crown** A roadway cross-section with the highest point of the *traveled way* located at the center of the road.

**City Engineer** Individual overseeing all operations and functions regulated by this title, subject to the authority of the Mayor. See SMC 13.01.0202

**Clear View Triangle** The corner area at an intersection or driveway which must be free of obstructions to provide adequate sight distance. See SMC 17A.020.030N for dimensions.

**Clear Zone** The roadside area free of obstacles, starting at the edge of the traveled way.

**Collector Arterial** A street providing access and circulation in lower-density residential and commercial/industrial areas. *Collector arterials (consisting of FHWA classifications Urban Major Collector and Urban Minor Collector)* collect and distribute traffic from *local access streets* to *principal* and *minor arterials*. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Cross Slope** A slope that is perpendicular to the direction of travel.

**Crown (Roadway Crown)** The term used to define the highest point of the *traveled way* on a roadway cross-section. The City recognizes three types of roadway sections to facilitate drainage: *center crown*, *quarter crown* and *curb crown*, which are defined herein.

**Curb Crown** A roadway cross-section with the highest point of the *traveled way* located at one curb.



**Curb Ramp** A ramp constructed in the sidewalk to provide an accessible route from the sidewalk to the street.

**Entrance Gate Queuing Area** A length of street on the public side of an entrance gate that allows vehicles to exit the connecting street prior to the gate.

**Driveway** A cement concrete driveway structure as shown in the Standard Plans.

**Fire Lane** A road or other passageway developed to allow the passage of emergency vehicles. A fire lane is not necessarily intended for general vehicular traffic usage. Refer to SMC 17F.080 Appendix D for dimensions and requirements.

**Integral Curb and Gutter** Concrete curb and gutter which is formed and placed as one unit.

**Local Access Street** A street that provides access from individual properties to *collector arterials* and *minor arterials* in residential, commercial and light industrial areas. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Median** A painted or raised traffic island used to channel, separate and otherwise control vehicular traffic.

**Minor Arterial** A street providing service for trips of moderate length, connecting the principal arterial system and providing intra-community circulation. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Monument** A physical survey monument as shown in the City's Standard Plans.

**MUTCD** See Section 1.2 and SMC 17A.020.130.

**NACTO** Refers to the National Association of City Transportation Officials.

**Neighborhood Greenway** A low-volume street that is designed to prioritize pedestrian and bicycle travel. Most often greenways will be implemented on local access streets, and elements of the greenway may disrupt through-travel by automobile as a means of regulating vehicle volume. Greenways are best implemented near and parallel to an arterial street which boasts access to goods and services, thus also providing ready access to users of the greenway. Another crucial element of a greenway is signage that identifies the route as a greenway and provides wayfinding.

**New Development** Development or redevelopment of land adjacent to (and often inclusive of a portion of) the Right of Way, or development of land with the intent of dedicating Right of Way infrastructure. Most private development falls under this category, and occasionally the City of Spokane will develop new streets through vacant or underdeveloped land.

**Path** Facility designed for use by bicyclists and pedestrians, usually separated from vehicle traffic by a median or landscaped area.

**Place-making** An element of streetscaping that involves the use of unique design features with the ability to set a street segment apart, helping to create an environment for economic vitality and innovation. Application of place-making design elements should be used in connection with planned land uses and in coordination with stakeholders.

**Preservation** A roadway maintenance project conducted by the City of Spokane to refresh the driving surface of the street and thus prolong the pavement service life. These projects are



generally confined to the pavement area between curbs. Example treatments may include grind and overlay, chipseal, micro-seal, slurry seal, crack seal, etc.

**Principal Arterial** A street serving major activity centers and providing a high degree of mobility. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Private Streets** Roadways which are not controlled or maintained by a public authority, and which serve two or more properties.

**PROWAG** Refers to the Public Right-of-Way Accessibility Guidelines.

**Quarter Crown** A roadway cross-section with the highest point of the *traveled way* located at a distance from one curb of one-fourth the roadway width (as measured from face of curb to face of curb).

**Reconstruction** A roadway corridor project that typically replaces the full depth of asphalt pavement, updates curb ramps, and may include utility updates as appropriate. Sidewalk repair, replacement, capacity improvements, signal and lighting upgrades and transit stop improvements may also be included in a reconstruction project. These projects are administered by the City of Spokane, and the scope of each project is determined in accordance with city plans. As this type of work is done within the built environment, space constraints may impede the full realization of the design standards. Prioritization of standards is generally addressed within this document, but each individual project will need to be scoped with future use conditions in mind.

**Shared-Use Pathway** A non-motorized transportation pathway shared by pedestrians, scooters and bicyclists. May be located next to a street or in a separate right-of-way. Examples include the Children of the Sun Trail, Ben Burr Trail, Fish Lake Trail and Centennial Trail.

**Street Classifications** In conformance with FHWA guidance, arterial and local access streets are classified in the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements section as follows:

- Principal Arterial
- Minor Arterial
- Collector Arterial (Major Collector or Minor Collector)
- Local Access Street

Definitions of all of the above classifications are included herein. Private streets are not classified.

**Street Character** Character consists of refined street definition based upon a street's function within the transportation network (or classification) and its context (land use zoning).

**Street Realm** A part of the right-of-way designed for a particular user group or use (pedestrian, flexible zone, vehicle, median). See Figure 2.

**Streetscape or Streetscaping** The combinations of living and non-living items that provide opportunities for place-making. Generally everything beyond the asphalt makes up the streetscape, although the median may include streetscaping elements.

**Structural Sidewalks** Structural sidewalks shall be defined as all elevated slabs, grates, and panels located within a sidewalk or driveway not supported on grade. Typical examples of elevated structural sidewalks are concrete slabs, steel grates, and steel plates for utility vault lids, service elevator covers, utility covers, and building basements.



**Traveled Way** The area of roadway which is intended to carry vehicular traffic, not including any shoulders. See SMC 17A.02.200.

## 3.2 Street Character

Street design is governed by two primary factors: zoning context and classification. Zoning context refers to the environment (land use zoning) in which a street is found. For example, sidewalks must be wider on downtown streets to accommodate higher pedestrian volumes and place-making elements. Street classification speaks to its function within the network, an arterial street with planned bicycle facilities will be built with the facilities the full length of the street regardless of how the facilities might change due to zoning the street passes through. Street Character is defined by the combination of zoning and classification. A principal arterial should have a different character through a CC zone than through a Residential zone.

### 3.2-1 Street Zoning Application

Spokane's comprehensive plan refers to urban infrastructure contexts for the city. This refers to the land use zoning through which a street traverses and to which the street facilities provide access. Land Use Zoning is defined in Title 17C "Land Use Standards" of the Spokane Municipal Code. Zoning is applied and defined for each land parcel in the city. Streets themselves are not assigned specific zoning, but should take on the context of zoning they front.

Zoning can, for the purposes of selecting street design characteristics, be lumped into four categories: Centers and Corridors, Downtown/Commercial, Residential, and Industrial. While zoning might change multiple times along a given block, some street characteristics will necessarily remain constant. Design criteria should be selected for the most generous zoning on a given block, and should be applied block by block. In some instances a street may traverse a different zoning for only one or two blocks, and best judgement should be applied as to whether to shift the street character in such instances. Emphases should be given to place-making opportunities when considering these shifts in street character.

Some consideration should be given to the planned versus the existing land use. The Zoning code allows for a variety of uses within several of the zoning contexts. For instance, the zoning for Centers and Corridors, CC1 allows for commercial, office, or residential development. When developing the street serving a planned development, or when rehabilitating a street within the built environment, it is worth considering what land use is to be expected for the life-span of the roadway, or about 20 years.

Motor vehicle volume (Average Daily Traffic – ADT) on a given street should be a strong determinant when considering how the facilities of the street fit together to provide appropriate levels of safety and provision to all users of the street. The street classification is largely determined by existing and planned traffic volume as well as the percentage of freight traffic on the street, and combined with the street type derived from the Land Use Zoning, provides the basis for design expectations for a given street.

Spokane exhibits four street classifications:

- Principal Arterial – Spokane's largest streets that provide regional connections and serve the highest volumes of traffic.
- Minor Arterials – Similar in design to Principal Arterials, Minor Arterials typically have fewer lanes and connect Collectors to Principal Arterials.



- Collector (Major and Minor) – Streets that circulate through neighborhood hubs and connect to minor and principal arterials. Collector streets are further defined as Major and Minor Collectors depending on traffic volume, but for the purposes of design, these will be treated under the same criterion.
- Local Access – Low-volume and low-speed urban streets providing access to homes and businesses.

In combination, the zoning contexts and street classifications result in sixteen overall street characterizations for Spokane. Street character, identified at the start of a project is the basis for this design standard, and sets the starting point for decision-making balance through the design process.

Street design for a given street should change with the context. For example, Garland Avenue's zoning changes several times from Alberta to Division, as depicted in Figure 1. Cross sectional design elements for the CC1-NC zone will be selected differently than for the RSF zone. Consult the zoning maps when beginning a street improvement project to understand context changes along a corridor that may warrant design adjustments from one stretch of roadway to the next.

Figure 1 – Zoning map (full map available at <https://my.spokanecity.org/opendata/gis/> )



Industrial route streets serve the areas where industrial zoning is assigned. Freight routes, as planned for traversing the city, may also be considered Industrial despite other zoning such streets traverse. Due to the high percentage of larger commercial trucks, vehicle lanes are typically wider (11 to 12 feet) to provide sufficient space, which is most important approaching intersections where truck lane changes and turn movements require wider geometric layouts than passenger vehicles. These streets require special attention to factors such as pedestrian crossings, pedestrian visibility, and bicycle facility design to ensure corridors may balance industrial needs and multi-modal functions, particularly where industrial land uses are co-existent with pedestrian-generating facilities.

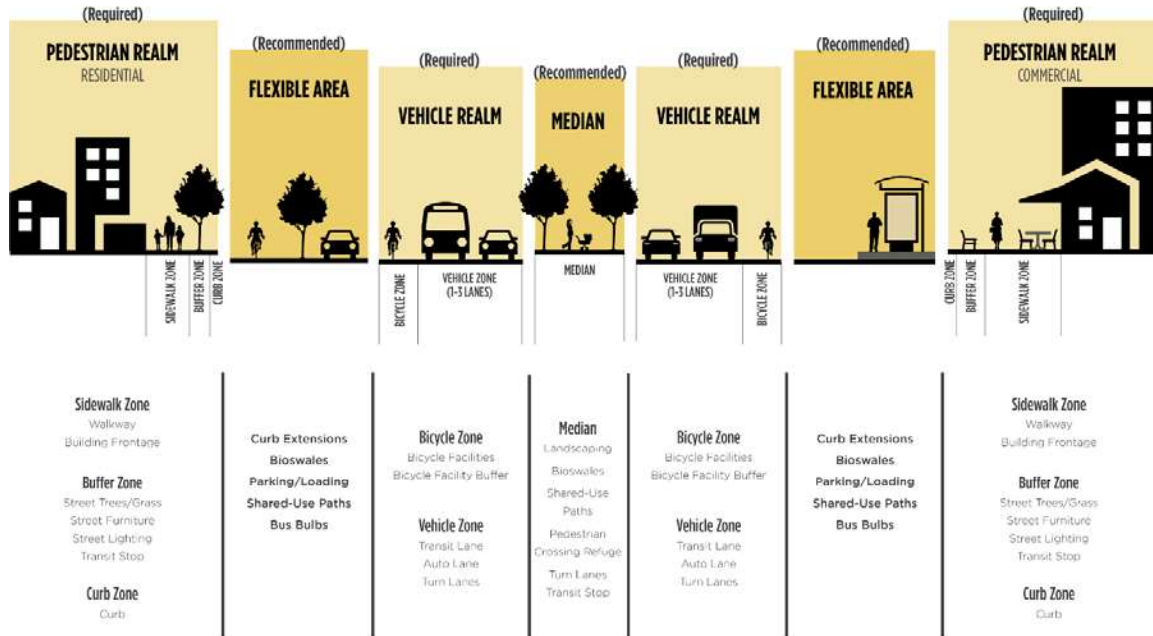
### 3.2-2 Street Realms and Zones

The cross section of a street includes some elements that are standard to all streets and others that are recommended for certain street character. Within the overarching street areas (Pedestrian Realm, Flexible Area, Vehicle Realm, Median) various elements can be arranged to provide a high-quality street depending on the needs of a given area. By thinking of streets in zones, designers ensure multimodal outcomes by considering all needs in relation to land use zoning context. All Spokane streets must have sidewalks, for example, which fall under a



“required” zone, whereas additional elements such as curb extensions or medians can only be built if enough room exists after placing the required elements.

Figure 2 – Street Realms and Zones



The **Pedestrian Realm** includes the area from the property line or building front to the curb and is made up of three primary zones: the sidewalk zone, the buffer zone, and the curb zone, as defined below.

- **Sidewalk Zone.** The sidewalk zone is the area dedicated to pedestrian travel between the buffer zone and the property line. A minimum of 5 to 8 feet of concrete surfacing must be built as defined in the Land Use Zoning. ADA standards also dictate minimum dimensions to be kept clear of obstacles and protruding objects and provide a direct connection along pedestrian access routes. Vending tables, sidewalk cafes, or other activities that protrude into the through-walking space must conform to SMC Section 17C for minimum through-way requirements for the applicable Land Use Zone. In addition to the pedestrian walkway, the sidewalk zone also includes the building frontage wherein could be located vending tables, sidewalk cafes and various street furnishings.
- **Buffer Zone.** The buffer zone is located between the curb and sidewalk zone. This area can be paved or planted, depending on the street character. It may include street trees, parking meters, planters, rain gardens, bioretention swales (overlapping into flexible area), bus shelters, utility poles and boxes, lamp posts, traffic signs and signals, bike racks, news racks and stands, waste receptacles, street furniture and drinking fountains. In addition to the curb zone, the buffer zone provides a buffer for pedestrians from the adjacent roadway and can accommodate snow storage in the winter. Vegetation in this area will generally be maintained by the adjacent property owner, except in the case that such serves a stormwater management purpose. In that case, the city will often maintain vegetation.
- **Curb Zone.** The curb zone is a continuation of the sidewalk elevation plane, typically lies between the traveled way and the buffer zone, and typically consists of 6-inch-wide



elements; although wider elements like bicycle parking or riding facilities are sometimes included. The curb zone will commonly be incorporated into the flexible area for curb extensions or raised cycle tracks, for example. It provides space to open a car door, for vehicle overhangs and for pedestrians to wait for taxis or buses. For those with visual impairments, the curb indicates the border between the sidewalk and the roadway. The curb zone should be free of all objects, furniture, sign posts etc.; particularly adjacent to on-street parking.

**Flexible Area (optional).** This space between the vehicle realm – where vehicles and bicyclists move – and the curb zone can be programmed for car parking, bike parking, landscaping, stormwater management (general overlap with buffer zone), pavement-level protected bike lanes, shared-use paths, bus bulbs, or curb extensions. Shy space, a distance commonly required on the right side of a vehicle to allow for driver deviation near curbs is also part of this area. Not all streets have enough space for both required and optional elements.

**Vehicle Realm.** This area has two zones:

- **Bicycle Zone.** Consult the Master Bicycle Plan and Section 3.5 to determine the type of facility and design desired. Depending on the street character, this zone may include shared lane markings, a lane, a buffer between the lane and vehicles, or other components. In some cases the bicycle facilities will be placed in the Flexible Area, such as in the case of a multi-use path or parking-protected bike lanes.
- **Vehicle Zone.** Auto or transit vehicle lanes are included in this zone, including the outer travel lane, inner travel lane(s), and optional Two-Way Left Turn Lane (TWLTL).

**Median.** Medians calm traffic, provide refuge for pedestrians crossing the street (especially along wider streets), and present opportunities for landscaping, streetscaping, stormwater management and transit stops. Medians can be used midblock in tandem with turn lanes at intersections. Similar to the Flexible Area, not all streets need medians, and when medians are considered, access to utility access or controls, left turns, alley access, etc. should be maintained where appropriate. Based upon available right-of-way and community input, a menu of options can exist in a median. Pedestrian refuge medians should be installed in accordance with SMC 17H.010.210 and SMC 17H.010.215.

**Dimensions in Table 1.** Flexibility in street design may be maintained by referencing a range of possible dimensions rather than prescribing exact requirements. A design, may thus be crafted based upon the unique elements of each street. Street design, particularly within the built environment, requires a range of possible elements and dimensions in order to deliver desired outcomes. Table 1 lays out the target dimensions for street zone elements by street classification and zoning contexts.

Wider sidewalks, buffer zones, swales and medians are allowed without a deviation. Shared-use path width may be decreased to 10' or increased without a deviation. Deviations beyond these standards must be approved by the City Engineer per SMC 17H.010.020.



(required)

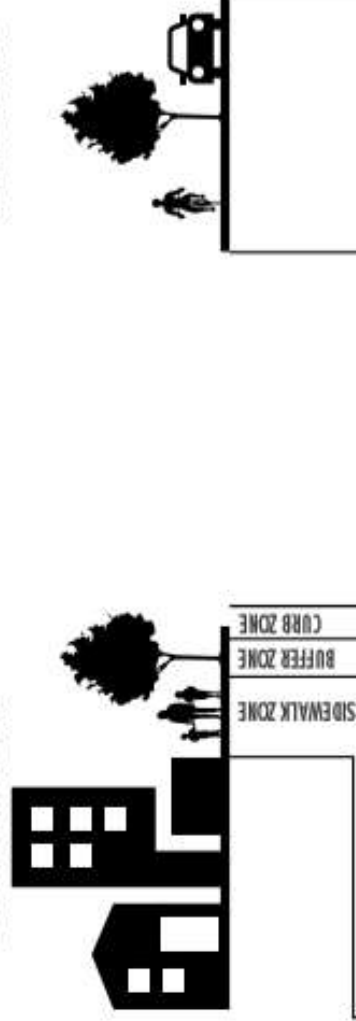
## PEDESTRIAN REALM

(recommended)

## FLEXIBLE AREA

(required)

## VEHICLE REALM



| Sidewalk Zone <sup>b</sup>                                      | Buffer Zone <sup>cd</sup> | Curb Zone | Opt. Shared Use Path <sup>de</sup> | Stormwater Management <sup>h</sup> | Curb Extensions/<br>Bus Bulbs <sup>i</sup> | Parking | Bicycle Lane <sup>a</sup> | Bicycle Buffer <sup>f</sup> | Vehicle Zone Outer Lane <sup>g</sup> | Vehicle Zone Inner Lane <sup>g</sup> | Vehicle Lane |
|-----------------------------------------------------------------|---------------------------|-----------|------------------------------------|------------------------------------|--------------------------------------------|---------|---------------------------|-----------------------------|--------------------------------------|--------------------------------------|--------------|
| Downtown DTC, DTG, DTU, DTS; Form Based Code CA1, CA2, CA3, CA4 |                           |           |                                    |                                    |                                            |         |                           |                             |                                      |                                      |              |
| 7                                                               | 5                         | 0.5       | 12                                 | Varies                             | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | Varies                             | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | Varies                             | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | Varies                             | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   | NA           |
| Commercial O, OR, NR, NML, CB, GC                               |                           |           |                                    |                                    |                                            |         |                           |                             |                                      |                                      |              |
| 7                                                               | 5                         | 0.5       | 12                                 | 10                                 | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | 10                                 | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | 10                                 | 7                                          | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 7                                                               | 5                         | 0.5       | 12                                 | 6.5                                | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   | NA           |
| Residential RA, RSF, RSF-C, RTF, RME, RHD                       |                           |           |                                    |                                    |                                            |         |                           |                             |                                      |                                      |              |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | 8       | 6                         | 1.5-5'                      | 11                                   | 11                                   | 11           |
| 5                                                               | 6                         | 0.5       | 12                                 | 6.5                                | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   | NA           |
| Industrial IJ, HI, PI                                           |                           |           |                                    |                                    |                                            |         |                           |                             |                                      |                                      |              |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | NA      | 6                         | 3                           | 12                                   | 12                                   | 12           |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | 8       | 6                         | 3                           | 12                                   | 12                                   | 12           |
| 5                                                               | 6                         | 0.5       | 12                                 | 10                                 | NA                                         | 8       | 6                         | 1.5-5'                      | 12                                   | 12                                   | 12           |
| 5                                                               | 6                         | 0.5       | 12                                 | 6.5                                | NA                                         | 7       | 6                         | NA                          | 11                                   | NA                                   | NA           |

For adjustments are allowed to fit the street context. See discussion in section 3.2-2 for further detail.

at sidewalk requirements. In locations where existing sidewalks exceed the dimension in Table 1, the sidewalk width street improvement.

continuous buffer requires a 5-foot minimum width for commercial zones. For residential and industrial zones, the narrower buffer may be used in select zones if tree vaults are implemented.

or concrete. When stormwater disposal is a governing concern, consideration should be given to use pervious

H. Consult the Spokane Regional Stormwater Manual and/or Eastern Washington Low Impact Development Guidance Manual for facilities. The stormwater catchment area must meet the required volume generated by the planned impervious area. In Down & Corridor zoning roadside swales are less common and alternative stormwater facilities in accordance with the above identifying stormwater piped to another location.

I. On transit corridors, use bus bulbs if space allows to ease boarding, reduce sidewalk congestion, and allow buses to easily be done only if there is a second lane for vehicles to continue around stopped buses.

J. "High Traffic" and "Medium Traffic" lane routes on the Master Bicycle Plan should include buffers. Separation buffer between



### 3.2-3 Place-Making Elements

According to the Project for Public Spaces, place-making facilitates creative patterns of use, paying particular attention to the physical, cultural, and social identities that define a place and support its ongoing evolution. Key to a successful place-making effort, is an associated community-based participation which helps identify a location's assets, inspiration, and potential to contribute to people's health, happiness, and well-being. This public participation also builds coalitions that will help care for the physical components of place-making, as well as assist in programming events held in such places.

As regards streetscaping, place-making involves the use of "unique design features that have the ability to set a street or segment of a street apart, helping to create an environment for economic vitality and innovation. Application of place-making design elements should be used in connection with planned land uses and in coordination with stakeholders." (Spokane Comprehensive Plan). This can occur through a number of planning efforts, including sub-area planning, neighborhood planning, and staff-level or board-level design review.

Capital Street projects have a unique opportunity to enhance place-making within the right-of-way. Examples of place-making treatments are provided below.

- Use of historic sidewalk patterns and stamping street names into the concrete.
- Preserving historic brick patterns in the gutter.
- Use of neighborhood specific tree grates and manhole covers.
- Re-use of historic granite curbing.
- Decorative lighting fixtures per the districts and standard types outlined in this document.
- Installation of benches, historic plaques, artwork, planter boxes, etc.
- Establishment of scenic overlooks.
- Trees and other plantings in the buffer strip or center median.
- Installation of street furnishing such as benches, bike racks, custom trash receptacles and media corrals.
- Bulb-outs at intersections or crosswalks
- Architectural features such as balconies, marquees, or arcades that may project out into the right-of-way (subject to appropriate clearances)
- Parklets and/or streateries

Other than potential landscape or hardscape improvements in a median, place-making treatments would generally be restricted to Pedestrian Realms, Alleyways, and Flexible Areas. Any place-making treatments in the Vehicle Realm (e.g. custom lighting or artwork on Skyways) must meet the other provisions of this document.

## 3.3 Right of Way

Follow the guidelines of SMC 17H.010.050 to determine minimum dimensions required for right of way for new development. Preservation and reconstruction work will often seek a balance of uses due to limited available space. Such balance should be determined based on land use context and right of way available.

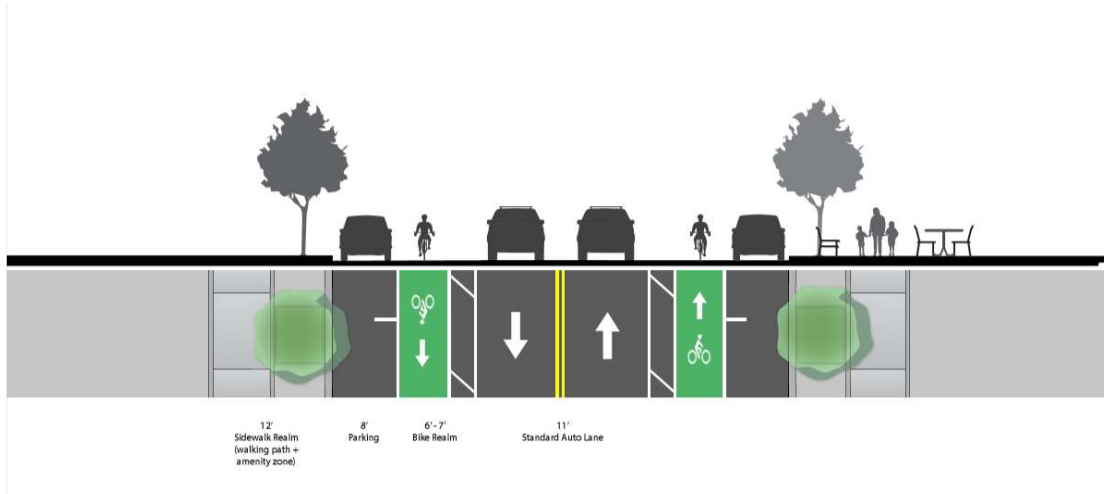
Narrower right of way widths may be allowed in new development only at the discretion of the City Engineer. Variance requests will be evaluated based on topography, traffic circulation,



emergency vehicle access, zoning, utilities, existing development and on-street parking requirements.

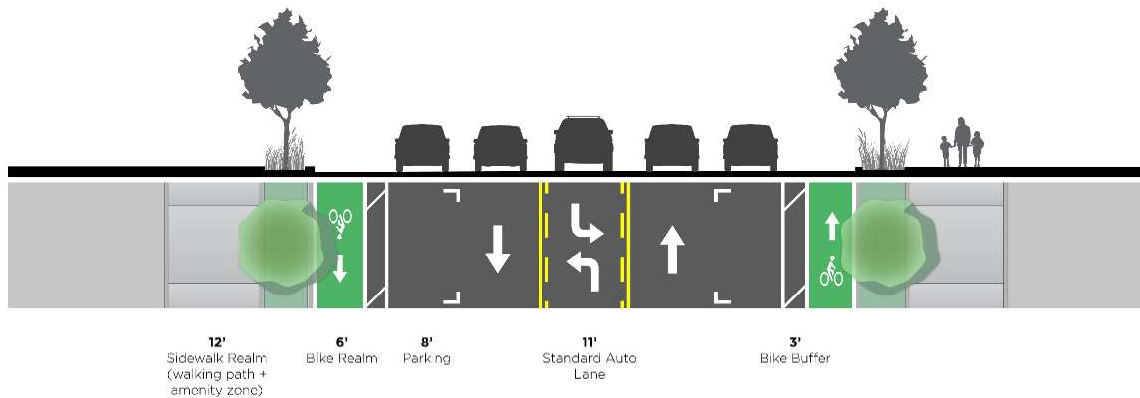
Application of Table 1 to a new and existing right-of-way is illustrated below. In some cases, the designer will be laying out a new street (Figure 3).

**Figure 3 – Sample layout of an Urban Collector**



In retrofit situations, Minor Arterials built to the city's earlier standards can have space reallocated based upon current pedestrian, bicycle, stormwater, transit, and/or other plans (Figure 4).

**Figure 4 –Reallocation of space on example Urban Collector/Minor Arterials**



In alignment with city goals (from the Transportation Chapter of the Comprehensive plan) TR A: Promote a Sense of Place and TR B: Provide Transportation Choices to achieve a balanced, multimodal transportation approach (emphasis on walking, biking, transit) street space must be reallocated if possible to users aside from drivers. FHWA has published guidelines for when a road can be downsized to three lanes (two through lanes and a center turn lane). Roads with 10,000 ADT or less are considered great candidates for a road diet. Roads with 10,000-15,000 ADT are good candidates in many instances, but agencies should conduct intersection analyses and consider signal retiming with implementation. Roads with 15,000 -20,000 ADT may be good candidates but agencies should first conduct a corridor analysis. Excess vehicle lanes can be allocated to parking, landscaping, stormwater facilities, bicycle facilities, or widened sidewalks.



When undertaking a repaving or reconstruction project on multi-lane streets with ADT of 20,000 or less, designers must undertake a traffic analysis and consider reconfiguring traffic.

## 3.4 Pedestrian Realm Facilities

### 3.4-1 Sidewalks

Sidewalks are the basic element of walkability, and can be augmented with planted buffer strips, center medians, and street furniture. The sidewalk zone includes both the area in front of a building where cafes or vendors might operate as well as the area for walking through. Ensure that for land uses where cafes and other active frontage uses are expected, appropriate unobstructed sidewalk width for walking is maintained per SMC 10.28.

Sidewalks shall be located as required by SMC 17H.010.180. Width and profile grade design criteria for sidewalks are outlined in Table 1 and Table 8. Sidewalks shall be designed in accordance with the Standard Plans and City of Spokane GSPs, and may use pervious concrete where feasible (SMC 17C.110.410, 17C.120.230, 17C.130.230).

Shared-use pathways may be substituted for sidewalks. This will typically occur in locations designated as shared-use paths on the Bicycle Plan, but other locations may be identified through the development permitting process or through a capital project design process.

Where existing, elevated structural sidewalks (vaulted over building basement spaces) are intended to be kept, they shall be designed in accordance with the applicable portions of the latest edition of the Uniform Building Code. The minimum concentrated load, L, to be used in the design shall be 10,000 pounds applied over a contact area of 100 square inches. The minimum single axle load shall be 20,000 pounds. The design tire load shall be 600 pounds per inch of tire width. The construction of new buildings with open space under the sidewalk shall not be allowed, nor shall private utilities for said buildings be placed under the sidewalk.

When development occurs on sites with existing sidewalks; broken, heaved, or delaminated sidewalk adjacent to the project shall be repaired or replaced as part of the project. Locations of sidewalk repair or replacement shall be included on plans submitted to Developer Services for review.

Reconstruction projects, where funding sources allow, should also consider sidewalk condition and completeness. Existing sidewalk width may fall short of the current standard. Consideration for widening will be a decision during the scoping phase while funding is gathered. Preservation projects is not required to adjust sidewalk width or condition of sidewalk parallel to the roadway, but grind and overlays are required to attend to ADA compliance updates at street crossings, in accordance with federal regulations.

Pedestrian detours must be planned and implemented whenever work reduces the through-walking path below acceptable ADA standards. Temporary sidewalk, when necessary, may displace vehicle parking or travel lanes, as appropriate, in order to provide a walking path detour for high-use sidewalks.

### 3.4-2 Buffer Zone

Buffer strips (separated sidewalk) can add greenery to a street, provide snow storage space, and provide horizontal separation for pedestrians from vehicle traffic. Buffer Strips can be hard surfaced or planted depending on the land-use zoning. The requirements for buffer strips are



included in SMC 17H.010.190, which requires buffer strips on both sides of all streets; SMC 17C.200.050, which guides dimensional requirements for incorporating street trees; and Table 1 which compiles the dimensional requirements from each land use zoning as defined in SMC 17C.

Reconstruction work should include pedestrian buffer strips where space allows. However, space limitations may determine availability of this option. Roadway narrowing may be considered when conditions allow, to create the necessary space for pedestrian buffers. Refer to the Pedestrian Master Plan, and prioritize buffers particularly for projects within pedestrian priority areas. Even creating this condition on one side is preferable to neither side. When creating a buffer on one side, take into account the continuity of pedestrian travel and likely destinations like schools, markets or community facilities. Street maintenance activities (non-capital) are not required to consider linear elements beyond the curblines unless attending to ADA or utility items.

### **3.4-3 Curbs and Gutters**

Integral cement concrete curb and gutter shall be constructed per the City standard plan on roadways with profile grades below 1.0 percent. Special drainage issues may allow the use of alternative curb profiles depending upon road profile and setting, upon approval of the City Engineer. When repairing or replacing existing sections of curb, the type of curb constructed may match the adjacent curb.

The curb radius at alley entrances is addressed in the City's Standard Plans.

Consider curb extensions (bulb-outs or bumpouts) at intersection corners whenever on-street parking is present along the block. Curb extensions shorten the crosswalk width, assure parking setbacks from intersections and crosswalks, and delineate (or "book-end") parking lanes. The extension from the curbline should generally be 1 foot less than the parking lane width, but in some instances additional "shy distance" from the adjacent travel lane may be considered. Bumpout design must consider whether a bike lane is planned in the future. Curb extensions may also be used midblock to provide traffic calming or to protect a midblock crosswalk. Bumpouts should generally be implemented as part of a series, as singular instances of bumpouts on a corridor could result in a hazard. Use appropriate design and accommodated vehicles and refer to effective turning radii when designing curb extensions. Curb bumpouts should be delineated with flexible candles on the curb line near the travel paths to aid in winter visibility for drivers and snow plowing.



Figure 5 – Curb extension works to narrow a road adjacent to a school



Source: Googlemaps

### 3.4-4 Curb Ramps

Curb ramps can improve access for many, especially wheelchair users, people wheeling strollers, people with mobility challenges and older adults. How curb ramps are installed affect accessibility, particularly for people experiencing vision loss. Visual impairment can be very limiting for individuals, and physical clues built into street infrastructure are quite helpful. Curb ramps shall be designed in accordance with the recommendations of PROWAG, NACTO, the WSDOT Standard Specifications, and the City of Spokane Standard Plans and General Special Provisions. Curb ramps shall be located in accordance with the City of Spokane Standard Plans, SMC 17H.010.200, and SMC 17H.010.210E. Reconstruction and grind and overlay type preservation projects shall include ADA compliance updates as required by federal regulations.

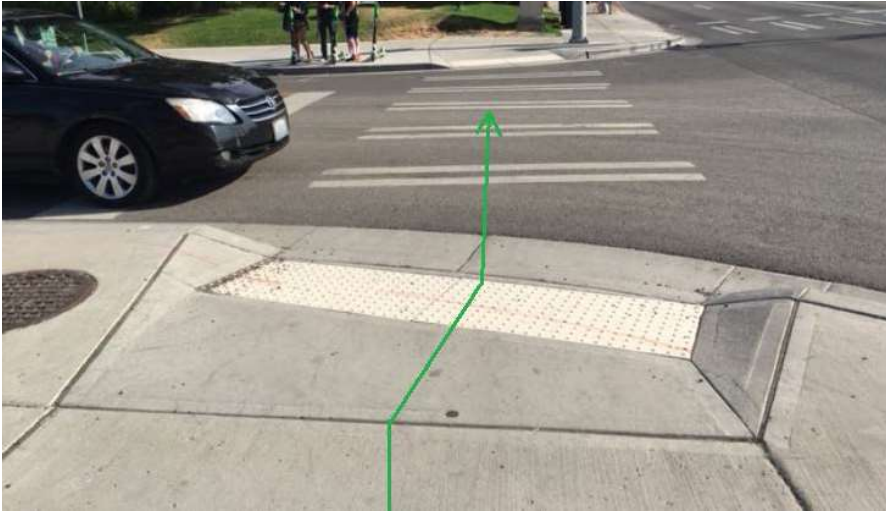
In all new construction and reconstruction projects placement of two ADA compliant curb ramps per corner is required. The ramp layout should maintain the pedestrian line of travel when feasible. Ramps should be aligned such that the running slope (and edge curb if used) is parallel to the crosswalk markings and direction of pedestrian travel. Grade breaks at the top and bottom of the ramp should be perpendicular to the direction of travel. The low-point for stormwater collection should not be in front of the ramp.

Figure 6 – Ramp running slope aligned with direction of pedestrian travel and ramp on opposite corner





**Figure 7 – Ramp running slope misaligned with crosswalk does not provide information to sight impaired individuals**



For retrofit or preservation work the priority is to use two curb ramps per corner. However, the use of single curb ramps per corner may be appropriate when relocation of utilities would be required to accommodate dual ramps, topographic constraints, right-of-way constraints or intersections with small curb radii. When using a single curb ramp per corner, it is helpful to avoid deviating from the pedestrian line of travel. Alignment cues such as use of perpendicular angles should be utilized. Curb ramps are generally built with flared sides, but at times will be built with pedestrian curbs flanking the ramp. Pedestrian curbs used in this manner should be parallel to the crosswalk.

**Figure 8 – Dual ramps with curbs instead of flares**





### 3.4-5 Street and Pedestrian Lighting

#### General

This section provides general information on street lighting with the City of Spokane. Additional detail, such as the need, type and location, and request process for new lighting is determined by the “Street Lighting Guidelines”, a document available from the Street Department.

Street lighting will generally be provided by the serving utility company. In these locations the maintenance and capital costs are included in the utility company rate. However, on bridges, traffic signals, downtown, certain business districts, and other locations the City may provide lighting equipment and maintenance in addition to the energy costs.

#### Arterial Street Lighting

Arterial lighting is typically 200 watt LED equivalent with one luminaire per intersection. Continuous roadway lighting on arterials is considered on a case by case basis. Lighting levels may be increased on arterials if the City Engineer determines higher levels are appropriate. Generally, low-volume neighborhood collector arterials will have lighting similar to residential streets while high-volume minor and principal arterials may have continuous high-level lighting service.

Arterial lighting will typically be installed on wood poles. The City Engineer may elect to install metal poles on certain streets. Adjacent property owners have the option of upgrading to metal poles through direct negotiation with the serving utility company.

If the arterial lighting service provided by the City does not fit the desired needs of the adjacent property owner, developer, or neighborhood association, they may install a private lighting system. The City will not participate in the costs of any such system. The presence of such a private system will not preclude the City from providing street lighting in conformance with the “Street Lighting Guidelines” if requested. All private lighting systems will require appropriate permits and encroachment agreements.

Preservation projects will not be required to update street lighting. Reconstruction projects should consider updating lighting as defined here-in.

#### Decorative Street Lighting

Decorative street lighting is limited to specific areas of the city and are considered an appropriate kind of place-making element. These areas are defined below. For new installations the maintenance cost may be funded by a business district or similar organization. This section is not applicable to lighting installed and maintained by the Parks Department.

The city has adopted three specific luminaire styles that must be used for all new city-maintained installations or updates. The styles are referred to as Modern Acorn, Transitional Series and Traditional Series. Project designers should refer to Standard Plan J-200 for the specific type to use in the CBD and North Bank/Spokane Arena Districts, and refer to the Street Department for guidance on specific types not listed on that plan.

**Central Business District.** A large area generally defined as the area south of the Spokane River, west of Division, north of I-90 and east of Maple Street. Some areas in the CBD provide decorative pedestrian lighting and street lighting, others are pedestrian only. Much of this area still has the Expo '74 lights that are being removed and replaced



with street improvement projects. The infrastructure supporting this lighting (conduits, wiring, electrical cabinets) also need to be updated when the newer decorative fixture are installed.

**University District (south).** Parts of the south University District including the Sherman Plaza, the south bridge landing, on Riverside from Sherman to Sheridan, on Sheridan from Riverside to Sprague. Overlaps with the East Sprague Business District lighting.

**East Sprague Business District.** The area along Sprague Avenue east of Division to Altamont Street.

**North Bank/Spokane Arena.** There is some decorative lighting in the vicinity of the Spokane Arena and north edge of Riverfront Park.

**Monroe-Lincoln South.** This business district has pedestrian lighting on the arterial street from approximately 10<sup>th</sup> Avenue to 15<sup>th</sup> Avenue.

**North Monroe.** Monroe Street from the river north to Alice Avenue. There is a gap between Mallon and Indiana.

The following districts have special fixtures that are maintained by other entities.

**University District (north).** The area east of Division, south of the river, and north of the railroad. This lighting is maintained by WSU.

**Kendall Yards.** The Kendall Yards development has decorative pedestrian lighting throughout the development. This lighting is maintained by Kendall Yards.

**Gonzaga District.** Parts of the Gonzaga campus including the frontage along Hamilton Street. This lighting is maintained by Gonzaga.

Many of the decorative lighting areas have legacy fixtures that are maintained by the City but no longer used for new installations.

**West Broadway.** Broadway Avenue from approximately Elm Street to Walnut Street within the West Central neighborhood.

**Browne's Addition.** The intersection of Pacific Avenue and Canon Street in the Browne's Addition neighborhood.

**Perry District.** Along Perry Street from 8<sup>th</sup> Avenue to 12<sup>th</sup> Avenue.

**Sunset Boulevard.** Along Sunset Boulevard from 5<sup>th</sup> Avenue to Hemlock Street, generally associated with the Inland Empire Way underpass.

**Hillyard District.** The Hillyard Business District has decorative lighting along Market Street.

**Cliff Drive.** On the Cliff Drive bridge over Grove Road.

### Local Access Street Lighting

Local Access Street lighting consists of a 100 watt LED equivalent lighting fixture on a wood pole at each intersection. Midblock lights may be installed on long blocks of 600 feet or more. However, lights will not be placed less than 200 feet apart.



- The Streets Department maintains a first-come, first-serve priority listing for new lights to be installed as funding comes available.
- Street lights will not be provided at dead ends or at the end of cul-de-sacs. However a midblock street light may be approved for cul-de-sac streets at least 600 feet long.
- The person or group requesting lighting may upgrade the basic wood pole to a metal pole through private negotiations with the electrical service company.
- If the basic street lighting service provided by the city does not fit the desired needs of the adjacent property owner, developer, or neighborhood association, they may install a private lighting system after obtaining the appropriate permits and encroachment agreements. The city will not normally participate in the cost of any such system.

### **3.4-6 Roadside Planting**

Any roadside planting shall conform to the City's clearances/clear zone standards as discussed in Section 3.12 and SMC 17A.020.030N, and SMC 17C.200.050. A permit in accordance with SMC12.02.960 is required for the planting, removal, or pruning of any street tree. Guidelines for proper tree installation can be obtained from the Urban Forestry program of the Parks and Recreation Division. Locations of all existing and proposed street trees shall be shown on plans submitted for review.

The standards within this chapter provide a target set of dimensions for basic tree growth space. Following these standards will support the growth of street trees in an urban environment, and but will not likely support a thriving canopy that can be experienced in more park-like settings. Within the confines here-in defined, tree growth and health will, in time, be stunted, requiring replacement at a younger age. In order to develop a more mature canopy, additional space (beyond these standards) for root growth would be necessary. In further consideration of larger growth expectations, the planter width should appropriately provide for larger trees. The following recommendations set the stage for the standard street tree, thus if larger growth is desired, additional considerations should be discussed during the tree permitting process.

#### **Existing Street Trees**

When development occurs on sites with existing street trees, the following items must be addressed as part of the project:

- All dead or diseased trees must be removed and replaced.
- Trees that are missing shall be replaced.
- Broken or missing irrigation systems shall be repaired or replaced as needed when incorporating new plantings.
- Broken or missing tree grates shall be repaired or replaced.
- All concrete tree grates shall be replaced with metal grates meeting ADA requirements.
- When structural sidewalk is removed and backfilled, concrete planter vaults shall be removed and replaced with an appropriate containment facility providing at least 100 cubic feet of soil.
- Gaps between the tree grate and the soil surface exceeding 6 inches shall be filled in with pea gravel.



- Tree grates that are not flush with the surrounding sidewalk shall be raised or lowered as necessary to prevent a tripping hazard.
- If existing trees have roots that have heaved pavement or sidewalk, work with Urban Forestry to determine an appropriate course of action.

### **New Street Trees**

Tree selection shall be coordinated through Urban Forestry. Approval shall be obtained from the City Engineer and the Urban Forester prior to planting tree(s) in the City right of way. A Street Tree Permit (SMC 12.02.960) is also required before planting tree(s) in the City right of way.

In an effort to assist in the selection of an appropriate tree, the City has published a document entitled "Spokane Urban Forestry Approved Street Tree List" which is included in Appendix F. Not all of the trees appearing on this list are acceptable for every situation. Requests to plant trees not included on the list will be considered on a case-by-case basis. Urban Forestry can provide the most current list.

When locating street trees, the following specific criteria shall apply. In the case that these criteria would prohibit planting of street trees, the Urban Forester and City Engineer may consider alternatives:

- a) Street tree installations shall meet all City of Spokane visibility requirements as defined by clear view triangle (SMC 17A.020.030) for intersections and driveway approaches and be placed to provide minimum stopping sight distance for stop signs and visibility for warning and other regulatory signs.
- b) Street trees shall be located so as to not interfere with street signs, visibility of regulatory and warning signs, lighting poles, STA stops or pads and to accommodate ADA pedestrian requirements. Also tree locations should consider the tree canopy reach, the impact that may have on fire aerial operations and visibility of warning and regulatory signs.
- c) Minimum separation distances from the centerline of a tree to other structures or improvements in the planting strip shall be as follows:
  - 1) 10 feet to edge of single-family residential driveway, 15 feet to edge of commercial or multi-family driveway (10 feet may be allowed in some cases);
  - 2) 20 feet to street light luminaire (15 feet may be allowed where lighting pattern is not affected);
  - 3) 10 feet to hydrants and utility poles. Lower limbs must be pruned for full visibility of the hydrant. No new utility pole location shall be established closer than 10 feet to an existing tree;
  - 4) As required to provide an adequate clear sight triangle as defined below and shown in the Appendix;
  - 5) 15 feet to underground duct or pipe;
  - 6) 5 feet from curb cut for drainage;
  - 7) 20 feet from drywell, unless the species permits a closer placement due to crown diameter;



- 8) and shall conform with the Arboricultural Manual: Specifications and Standards of Practice.
- d) Trees that are suitable for wet areas shall be selected for planting within bioretention or biofiltration areas. Trees that are planted within bioretention or biofiltration areas shall not interfere with, obstruct, or retard the flow of water in the stormwater facility.
- e) Spacing of street trees will be determined by the permitting department. Clustering of trees may be allowed under specific circumstances. Contact Urban Forestry Department for more information.
- f) If trees are to be planted in an area with no planting strip, the following criteria shall apply:
- 1) A permanent, hard walking surface at least four feet wide shall be provided between the tree well or planting area and any structure or obstruction.
  - 2) Sidewalk cuts in concrete for tree planting shall be at least 48-inch x 96-inch as shown on the Standard Plans to allow air and water to the root area. Regardless of the sidewalk cut size, the soil volume below the sidewalk should facilitate a minimum of 100 cubic feet for each tree.
  - 3) In cases where the existing walk cannot meet the four foot width requirement after tree planting, additional sidewalk width must be added within street right of way or an easement or the tree position must be modified.
- g) Irrigation systems shall be required for all areas where street trees are planted. In most cases, irrigation is to be provided by adjacent land owners.
- h) Any proposed deviation from these conditions shall require submittal of a written request/ explanation to the Department of Engineering Services or Development Services Center and shall be subject to review and approval by the City Engineer and/or the Director of Parks and Recreation.

### **3.4-7 Transit Stops**

Transit riders must walk along and often cross the street to access and exit their bus stop. Transit-supportive design provides safe and convenient walking routes considering every passenger's trip from start to finish. Transit stops play an important role as part of the streetscape; with the integration of quality bus shelters, wayfinding maps, real-time information systems, and other key features, bus stops have the potential to enhance the public realm.

#### **Stop Placement**

Stop placement must be determined through discussion with STA. Locate bus stops in safe and secure locations where they meet both passenger and operational needs. Each intersection and potential bus stop exhibits unique characteristics that should be considered. Near and far side stops at signals both have pros and cons. Locating stops on the far side reduces conflicts between right-turning vehicles and buses, but can also result in traffic queues through the intersection. Far side stops also allow buses to clear the intersection and efficiently continue operations. Near side stops place the riders closer to the crosswalk.

In-lane vs. pullout stops have similar pros and cons. In-lane bus stops speed up the operation for transit riders since the bus doesn't need to maneuver out of the lane and then wait for traffic to come back in. They also require less curb space than pullouts which can work better in areas



where on-street parking is a priority. In-lane stops work best when the stop time can be minimized through the use of off-board fare payment and curb height that matches the bus floor level. Pullout stops prioritize through traffic movement including through-moving transit, and may be desirable when the bus dwell time is consistently expected to be long (such as at a high school with large groups getting off at one time) or on higher speed roadways such as US 2 in the West Plains.

Coordinate all stop placements with STA such that operations are directly considered.

### **Pedestrian crossing facilities near bus stops**

Locate safe, convenient, and ADA-accessible crossing facilities at or near all bus stops matched to street type. Bus stops on the far-side of intersections require pedestrians to cross behind the vehicle. On the far-side, provide a 90-foot no parking zone with the bus stop located at the far end of the zone.

Where it is impractical to locate bus stops on the far side, near side bus stops should be located at least 30 feet from the intersection crosswalk to ensure pedestrian visibility and space to load/unload bicycles. Provide a 100-foot no parking zone with the bus stop located at least 30 feet from the crosswalk. No parking zones will need to be longer for bus pullout conditions. Refer to route bus size and Transit Authority plans for routes along the roadway when selecting the proper facility type and size.

### **Bike facilities near bus stops**

Bus stops adjacent to bike lanes create conflict zones. There are several design options that can be used to provide safer interaction between these two transportation modes. Figure 9 shows bike lanes separated from bus stop activity using an island bus stop design. This design channelizes the bike lane between the island and the curb.

**Figure 9 – Island bus stop separates bike lane from bus traffic**



Figure 10 shows a shared bike lane and bus stop where the bike lane rises up to the bus platform level and shares the space used for the bus boarding zone. While the example photo shows a temporary installation would typically use a concrete bumpout.



Figure 10 – Shared bike lane and bus stop using temporary platform



### Bus Stop Amenities

Bus stop amenities encompass the infrastructure present where passengers wait for transit vehicles. They include physical infrastructure such as seating, shelters, and lighting, and informational infrastructure like transit maps or real time information boards. Bus stops with higher levels of activity typically have more intensive infrastructure. Shelters will be located outside of the required boarding and alighting area. Coordinate with STA to ensure shelter location, seating, schedule information, and properly located bus stop signs do not interfere with pedestrian zones and accessibility.

- **Paved and Accessible Boarding and Alighting Areas.** Provide a paved and unobstructed boarding and alighting area that is a minimum 8' x 8', providing space for ramp deployment while ensuring ADA accessibility. A sidewalk can serve part of this purpose, but may require additional space to meet STA design standards<sup>1</sup>. Higher-use transit stops may warrant additional paved frontage for queueing passengers.
- **Supply Secure Bicycle Parking Where Demand Warrants.** Secure bike parking at bus stops encourage people to ride bikes to transit, expanding the reach of transit for many users. Provide leased bike lockers, on-demand eLockers, and basic bike racks where appropriate. Locate basic bicycle parking such as staple racks at all HPT stops and bicycle lockers at all park-and-ride locations. Other optional parking facilities include bike corrals or covered parking areas.

Preservation work is performed between curb lines, and need not address transit facility updates. Reconstruction work should coordinate closely with the needs of current and future transit facilities and incorporate these as appropriate.

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<sup>1</sup> <https://www.spokanetransit.com/projects-plans/bus-stop-design-standards>



## 3.5 Flexible Area

This space between the Pedestrian Realm and the Vehicle Realm can be programmed for car parking, bike parking, landscaping, stormwater management, pavement-level protected bike lanes, shared-use paths, bus bulbs or curb extensions.

### 3.5-1 On-Street Parking

Parking lanes allow drivers and bicyclists to park their vehicles in the public right-of-way, providing convenient access to businesses and homes, and offering loading zones for freight vehicles. Carefully managed, on-street parking can offer traffic calming, economic development, and access benefits. On-street parking lane widths shall be in accordance with SMC 17H.010.120, the City's Comprehensive Plan and/or as directed by the City Engineer. Requests for a reduced street cross-section will be evaluated on a case-by-case basis and a waiver of the on-street parking requirement granted at the discretion of the City Engineer.

Parking and utility access locations should not share the same space. When conducting preservation work that refreshes the paved surface, there is opportunity to re-balance the uses of space. The scoping of such projects should consider the need for parking or access points, which offset one another. Consolidation of access driveways can provide additional parking space. This must be done in coordination with adjacent property owners, and in accordance with access management standards.

Some older streets in Spokane function as “yield streets”. These are bi-directional streets with a through-way narrower than two cars in width, meaning drivers must yield to each other to pass. Yield street operation work best on residential streets when parking utilization is 40-60%, creating a “checkered” parking scheme, which allows drivers to pull over in empty parking spaces or driveways. Yield street operation works best on residential local access streets with two-way traffic that measure 24-26 feet wide with parking on both sides, or 16-20 feet wide with parking on one side.

**Figure 11 – Example of a Yield Street: Baldwin Ave between N Hamilton St and N Perry St (25-feet wide)**





## Parking Lane Width

Parking on arterial streets must be accommodated by 8-foot-wide parking lanes. See Table 1 for parking dimensions. Parking width on residential streets may be narrower, but the street must meet minimum width requirements defined in SMC 17H.010.060.

## Bicycle Lanes Adjacent to Parking

When bicycle lanes are included in the Master Bicycle Plan, consult Table 1 for the desired bicycle lane width to be used in tandem with parking lanes. Ideally, provide a buffer between the bike lane and travel lane, allowing cyclists to ride outside the parked car “door zone”. Where parking has a high usage and turnover, consider using parking-protected bike lanes with a door zone buffer to reduce conflicts between bikes and cars.

## Angle Parking

Angle parking may increase parking supply if sufficient uninterrupted curb length is available, and is useful in mixed-use areas and retail and commercial districts. Angle parking tends to create a traffic calming effect by inducing caution for motorists driving adjacent to the parking zone. Refer to the city’s standard plan G-60 for dimensions.

Utilize back-in angle parking, which requires the driver to back into the space; particularly when placed adjacent to bicycle lanes. This allows drivers to load vehicles from the sidewalk, improves driver-bicyclist visibility as the driver departs the space, and increases safety for the driver as the person can pull out into traffic rather than blindly backing up into traffic.

## Other Parking Lane Uses

New uses of the parking lanes such as bike corrals and parklets increase the use of the public space for active living, placemaking and recreation.

### Bike Corrals

Bike corrals expand the amount of bicycle parking on a street without taking valuable space away from the sidewalk. Bike Corrals typically replace one parking space at the request of a local business or property owner and accommodates 12-24 bikes. Corrals can be installed at corners to “daylight” an intersection since bicycle parking has minimal effect on the visibility of pedestrians to moving vehicle traffic. Bike corrals have been shown to have a positive impact on nearby business.<sup>2</sup> Corral location must consider:

- Safety for users
  - Set corral back from travel lanes in a parking lane
  - Use corrals on streets with low speed limits and low parking turnover
- Rack placement
  - Perpendicular to curb/roadway for wider parking lanes
  - Angled racks better for narrower lanes
- Land uses
  - Commercial and retail uses have more demand for corrals

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<sup>2</sup> Meisel, Drew. Bike Corrals: Local Business Impacts, Benefits, and Attitudes." Portland State University. [http://bikeportland.org/wp-content/uploads/2010/05/PDX\\_Bike\\_Corral\\_Study.pdf](http://bikeportland.org/wp-content/uploads/2010/05/PDX_Bike_Corral_Study.pdf)



- Design
  - Demarcate corral with bollards, rubber curbs, and striping. Planters and reflective bollards may also be used.

Before installing a bike corral, require a maintenance agreement between the city and a local business owner or community organization who will maintain the corral and clear it from snow, dirt, or debris.

#### Parklets

Parklets repurpose street right-of-way, often motor vehicle parking spaces, into publicly accessible spaces for all to use. Parklets provide additional public space for people to sit, enjoy meals, meet others, and use for art and plantings. Parklets help communities reimagine the role of the public street. Parklets should be installed on low speed streets.

Before installing a parklet, require a maintenance agreement between the city and a local business owner or community organization who will maintain the space and clear it from snow, dirt or debris.

Requirements for parklet design, planning, and maintenance can be found in the SMC 10.55 Parklets and Streeteries.

### **3.5-2 Stormwater Management**

#### **Low-Impact Development Stormwater Treatments**

Stormwater facilities are addressed in SMC 17D.060. Conventional stormwater management infrastructure is engineered to convey the largest volume of water from a site as quickly as possible, collecting surface runoff in subsurface structures.<sup>3</sup> Sustainable stormwater management, by contrast, views rainwater as an amenity, using it to improve urban ecology, microclimates, air quality, and the aesthetic quality of the public realm.

Low impact development design utilizes landscaping, engineering, and urban design tools to mimic natural watershed capabilities.

**Figure 12 – Lincoln Street stormwater management**



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<sup>3</sup> "Chapter 3, Fundamentals of Stormwater Management," *New Hampshire Stormwater Manual* (Concord: New Hampshire Department of Environmental Services, 2006).



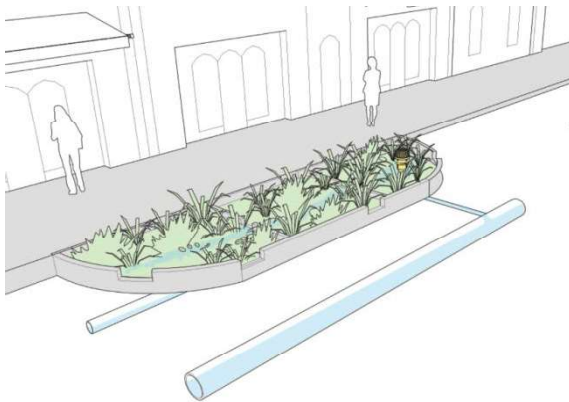
Stormwater facilities that fit the urban landscape, particularly in retrofit situations, are described below. Consult the Spokane Regional Stormwater Manual and Eastern Washington Low Impact Development Guidance Manual for detailed standards and placement guidance. Some tools for Low Impact Development are listed below.

#### Bioretention Facilities

Bioretention facilities are shallow landscaped depressions that receive stormwater from small contributing areas. They can be integrated into the site as a landscaped amenity because they are small-scale and dispersed. Bioretention facilities can be placed flexibly within medians, curb extensions, or public space. Maintenance of bioretention facilities involves vegetation management, soil replacement, and sediment and debris removal. In some cases it may be preferable to pipe stormwater to a nearby site where a single large bioretention facility can be constructed. This option must be enacted in accordance with the stormwater development guidelines. City reconstruction projects may have more flexibility to operate in this manner due to the extents and connectedness of the right of way.

When bioretention facilities are added to collectors or arterials, the designer should consult with STA to determine if current or future bus stops may be needed within the project limits. Adding a bus stop later on will reduce the area available for stormwater treatment.

**Figure 13 – Bioretention facility**

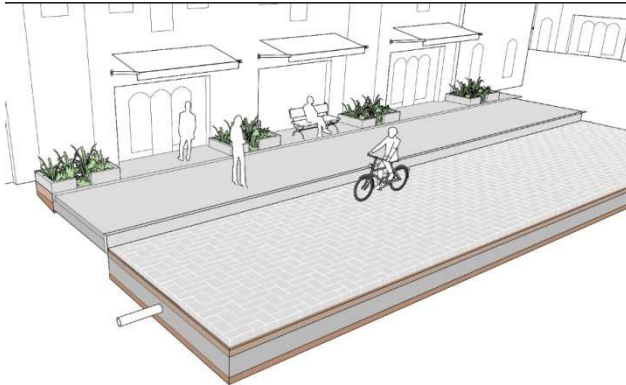


#### Permeable pavement

Permeable pavements are being tested in the city for sidewalks, transit stops, pathways, parking lanes and travel lane surfacing. Permeable pavements generally do not work well on travel lanes of roads with high volumes and extreme loads, or where hazardous materials, dirt, or anything that could clog the pavement are loaded and unloaded. Permeable pavements may work well in parking lots, sidewalks, residential streets, medians, driveways, and fire lanes. Maintenance of permeable pavement involves street sweeping, leaf pick up, and may include pressure washing and vacuuming.



Figure 14 – Permeable pavement



### 3.5-3 Shared-Use Pathways

Shared-Use Pathways can be used adjacent to roadways under certain conditions. They work best in locations where limited vehicle volumes can cross the pathway. Common placements would be a pathway between the road and a ridge, river, railroad, freeway, or other manmade or natural feature that restricts vehicular cross traffic. Examples of this in Spokane include the Centennial Trail along Pettet Drive and Upriver Drive, the Ben Burr connection on 3<sup>rd</sup> Avenue, the South Gorge Trail in Peaceful Valley, and the pathway along Government Way. Low-volume street or driveway interactions may be accommodated with design features such as signage, pavement markings and adequate sight distance.

Figure 15 – Shared-Use Pathway along Pettet Drive



Shared-Use Pathways shall be employed where designated in the City's Comprehensive Plan and in the Master Bicycle Plan, and shall be designed in accordance with SMC 17H.010.260. When constructed within the road right-of-way, these will typically be constructed behind the curb and accommodate both bicycles and pedestrians. Additional width to provide at least 2' separation from the curb is desirable.



In locations with a high volume of pedestrians (downtown, college campus) or significant through bicycle traffic, it may be desirable to physically separate the pedestrians and cyclists using striping and pavement markings.

Figure 16 – Shared-Use Pathway with Separate Bicycle and Pedestrian Lanes



## 3.6 Vehicle Realm Access Management and Connectivity

### 3.6-1 Access Management and Driveway Design

Driveway locations shall be designed to provide for safe operations and minimal disruption of traffic flow. In general, the higher the street classification, the fewer the number of access points that are allowed. In areas of high-density housing, shared driveways are encouraged. Multiple unshared driveways with minimal separation between them are discouraged. Minimize driveway width and place them to reduce conflict points.

Access management enables better property access by allowing people to get off the main road and circulate through local streets. On higher speed streets, frequent access points become a safety hazard for all users. Reduce the number of driveways per property to reduce conflict points across all modes, as appropriate and when opportunity arises (see Figure 15).

Access management (i.e. consolidation or reduction of the number of driveway access points along a corridor) may be conducted during street reconstruction projects. However, driveway installations and renovations are generally completed as part of new development and must adhere to the following:

- **Encourage Alley Development to Reduce Driveways on Streets with higher Bike/Ped Activity.** Alleys provide alternative access to adjoining properties. Require utilization of these alleys instead of driveways to reduce the number of access points on main streets. Develop new alleys where possible to provide this alternative access.
- **Design Driveways to Favor Pedestrians and Bicyclists.** Driveways should not be designed as small intersections, but as minor curb cuts. Whenever possible, sidewalks across driveways should maintain their grade rather than sloping down to the street. The



material used to delineate the sidewalk should continue through the driveway. See Figure 13, Figure 14, Standard Plans F-103, F-104, and F104B for examples.

- **During Street Projects, Assess Closure of Driveways.** When street projects are undertaken, evaluate the potential for consolidating driveways along the street to reduce the number of access points. Where streets do not meet the established driveway spacing standard, require new development and consider opportunities during reconstruction projects to address this.
- **High Volume Commercial Driveways.** These driveways should be considered in areas where high volume deliveries are required, where the receiving business may be likely to have a designated loading dock. Commercial driveways may also be considered in a dense commercial center, where multiple businesses could share commercial delivery space without restricting parking availability for customers. It is critical that this type of driveway design does not over-ride the facilities for the most vulnerable users, such as pedestrians. If visibility is a challenge for commercial vehicles entering or exiting, warning systems may be installed to warn drivers and pedestrians alike of an approaching vehicle.
- **Infill Access.** On case-by-case basis, single family residential zones can be developed using a variance to develop interior lots that share a driveway with primary lots. This is meant to facilitate development of lots that could not otherwise be developed in accordance with the standards. This applies only to parcels that are between 0.2 and 1.5 acres in size (8,700 to 63,430 ft<sup>2</sup>), with an approved Design Variance. Utility, emergency fire access, stormwater considerations, and other considerations must also be met.

Figure 17 - Brick sidewalk pattern is continued over the driveway to establish pedestrian dominance.

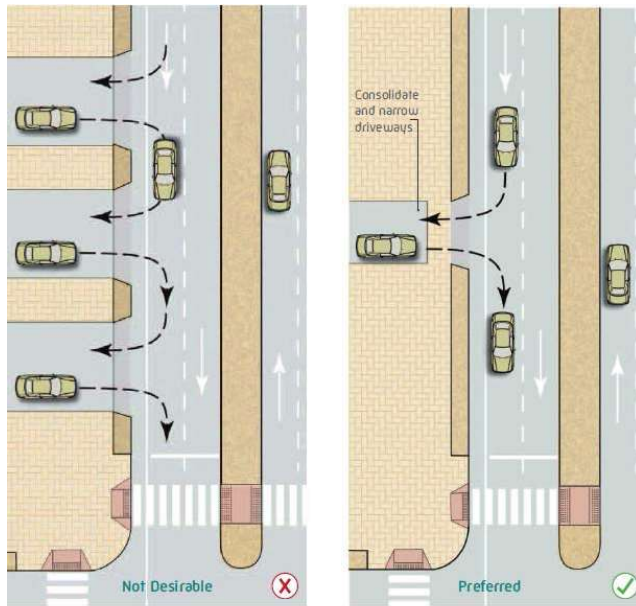


Figure 18 - Continuous Sidewalk Design Establishes Pedestrian Space over Driveway





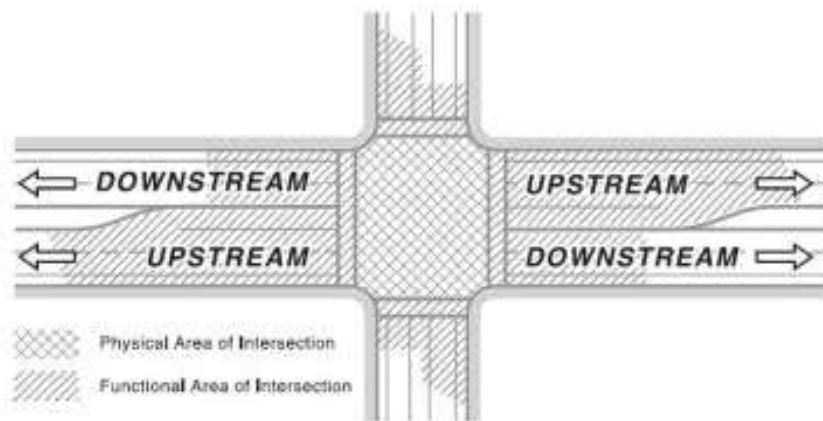
Figure 19 – Consolidated driveways increase safety for drivers and pedestrians



### Access Management Standards

- Principal and Minor Arterial driveway spacing: minimum 125 feet
- Collector driveway spacing: minimum 90 feet
- Local Residential driveway spacing: one per parcel for new development
- Driveways shall be located outside the Functional Intersection Area at signals (area beyond physical intersection that includes decision and maneuvering distance), or in the alternative, may be restricted to right-in, right-out.

Figure 20 – Functional Intersection Area



- One driveway per commercial parcel with one additional access point per fronting street allowed if the property frontage is over 1/4 mile in length and the site generates more than 100 PM peak hour departing trips.
- Commercial driveway approaches should be at least 75 feet from the point of curvature of a public road curb return on arterial streets and at least 30 feet for local access streets.



- For commercial driveways handling high volumes, a deceleration lane may be provided approaching the driveway, as justified by a traffic study or operational analysis. The driveway design must still maintain a tight turning radius to foster low speeds.
- Residential driveway approaches should not be closer than 15 feet from the point of curvature of a public road curb return on arterial streets and 10 feet for local access streets.
- Limit the Width of Driveways. Driveway width should be no more than 40% of the frontage (SMC 17H.010.220).
- Restrict Driveways near Bus Stops and Intersections. Do not place driveways within 100 feet of major intersections and 50 feet of other junctions, including bus stops, crosswalks, and small intersections.
- Shared driveways is a strategy to consolidate the number of access points along a block to reduce the number of potential conflict points between motorists and pedestrians. Driveways can be consolidated in instances where a single parcel has multiple access points, or where neighboring parcels may share parking resources. Driveway consolidation typically occurs during redevelopment as parcels and land use along a corridor change. Guidance for shared driveways for Single Family Residential Zoning development projects is found in the Infill Access and Utilities Standard.
- See additional access standards for Downtown Zones in SMC 17C.124.280 and Residential Zones in SMC 17C.110.535.

### **Standards for State Highways**

Specific access standards apply for state highways within the City limits, which are classified as managed access facilities. Managed access is based on the premise that access rights of a property owner are subordinate to the public's right and interest in a safe and efficient highway system.

In accordance with Chapter 47.50 RCW, the City adopts by reference, the provisions of Chapter 468-52 WAC, together with all future amendments, in order to regulate and control vehicular access and connection points of ingress to and egress from, the State Highway System within the incorporated areas of the City of Spokane.

### **3.6-2 Street Connectivity**

Connectivity refers to the density and directness of connections in path or road networks. Well-connected street networks have short links, frequent intersections, and minimal dead-ends or cul-de-sacs. High connectivity creates a more accessible and resilient transportation network, providing direct routes between destinations, multiple route options, and ultimately more capacity.

In designing streets, subdivisions, and retrofitting streets:

- The layout of new streets should consider future extensions of public roads and utilities into adjacent undeveloped parcels.
- Create blocks no longer than 660 feet in length. In urban settings (dense housing, centers and corridors, downtown, or commercial), strive to create short blocks that foster circulation.
- If topography, surrounding development patterns or other constraints make it impossible to meet the 660' block length, the City Engineer may approve a longer length if the total



perimeter of the block is less than 2000 feet. In these situations, pedestrian connections should still be provided at 660 feet or less.

- While rare; when opportunities arise (in the built environment) retrofit areas of the city with existing blocks longer than 660 feet in length with, at minimum, walking and bicycling connections. See Figure 21 for an example.
- When retrofitting areas of the city to create greater connectivity; utilities, emergency access, and maintenance access should be reviewed.

**Figure 21 – Baymount Court connects through to Eagle Ridge Blvd for pedestrians and bicyclists.**



### **3.6-3 Alleys**

Alleys shall be constructed in accordance with SMC 17H.010.130 and the Standard Plans. All alleys shall have a minimum paved width of 12 feet with a 4-foot buffer strip on each side. The buffer strips may be paved, grassed, or graveled. The buffer strip may be used for utilities, but must be kept free of all vertical obstructions. Fences may not be placed in the buffer strip.

Preservation and reconstruction work will generally re-pave alley entrances to assure level matching of paving to the alley surfacing. When applicable, entrance design should coordinate with alley activation surfacing designs. Alley paving projects must comply with ADA standards where intersecting with sidewalks.

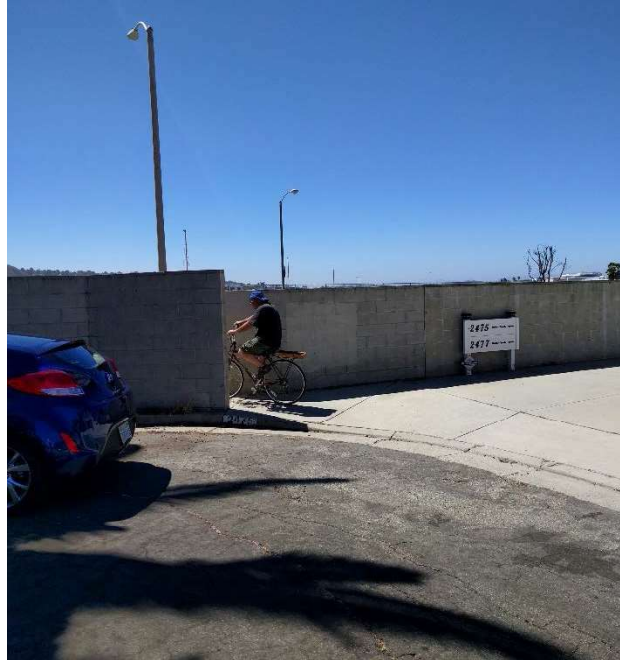
### **3.6-4 Turnarounds**

Cul-de-sacs limit connectivity, lengthen emergency response time, and create a physical barrier between residents and trip generators. SMC 17H.010.080 restricts the construction of new cul-de-sacs unless specific conditions are met. Standard Plans W-114 and W-115 show design details of turnarounds.



In new developments, require a “stub-out” future roadway connection at the end of a street that will connect to future development. Connect existing turnarounds to any pedestrian and bicycle trails in the vicinity to close a gap in the walking and bicycling network.

**Figure 22 - Example of bicycle and pedestrian connection from a dead-end street, providing additional connectivity.**



If cul-de-sacs are provided, use the following types:

- **Standard Cul-de-sac:** The standard cul-de-sac is preferred for construction on local access dead end streets. The radius point of the bulb is on the street centerline. Install a stub-out at the end of the turnaround.
- **Offset Cul-de-sac:** An offset cul-de-sac has a radius point offset from the centerline, with one curb being tangent to the bulb curb. Like the standard cul-de-sac, it is intended for use on local access dead end streets.
- **Temporary Cul-de-sac:** A temporary cul-de-sac is similar to the standard cul-de-sac but allows for planned street continuation. Curbing is not installed in the temporary cul-de-sac, and the roadway dimensions resume at the terminus in preparation of further street construction (the terminus is suitably blocked to eliminate immediate access). When the street is extended, new curbs are constructed along the roadway tangent, extending from the end points of the original curbs and the excess asphalt is removed.
- **Hammerhead:** The hammerhead termination may be used on local access dead ends, but is primarily intended for use in dead end residential alleys. Construction of a hammerhead termination on local access streets is allowed only on approval of the City Engineer.

The following specific design criteria shall apply to the design of cul-de-sacs:

1. Cul-de-sac islands may be an option for any permanent cul-de-sac. The island area shall be finished in a manner approved by the City Engineer.



2. Minimum curb radius for the bulb shall be 50 feet plus the radius of a center island, if used.
3. Minimum right of way radius for the bulb section shall be 56 feet plus the radius of a center island, if used. If the sidewalk is to be located on an easement, the minimum right of way radius is 51 feet.
4. Unless otherwise approved by the City Engineer, cul-de-sacs shall be designed to "drain out" to the adjacent street to avoid flooding if the storm drainage system fails.
5. Cul-de-sac profiles shall be established to provide minimum 2% grades at all places along the gutter lines.
6. Provide a 14-foot wide connection (10-foot path plus 2-foot buffers) for pedestrians and bicyclists along fences separating two yards

### **3.6-5 Entrance Gates and Queuing Area**

Proposed entrance gates may be allowed and designed in accordance with SMC 17H.010.100 and shall not interfere with emergency vehicle access. An adequate fire lane must be provided. If a center island is used, a minimum 14-foot wide lane between the face of curb and center island shall be provided. The center island shall not extend past the end of the gate when it is fully opened. In a case where there is no center island, the minimum road width is 20 feet. No parking on either side of the street will be allowed within 48 feet of the gate on both sides of the gate. The no parking zone shall be clearly signed on both sides of the gate. When fully opened, the gate shall not block access to structures or fire hydrants.

Gated streets require a queuing area to allow vehicles to exit the connecting street prior to the gate. The queuing area must be at least 48 feet long (measured from the intersecting curb line) to accommodate fire vehicles. Queuing areas longer than 150 feet will require a public turnaround designed to City Standards.

## **3.7 Vehicle Realm Geometrics**

### **3.7-1 Bike Facilities**

Bicycle facilities shall be employed where designated in the City's Comprehensive Plan and in the Master Bicycle Plan, and shall be designed in accordance with SMC 17H.010.260. Implementation of planned bicycle routes should be prioritized whenever reconstruction or preservation work is conducted, and new development should consider implementation of bicycle facilities to appropriately tie into the planned or existing network.

Side slopes adjacent to bikeways shall meet the requirements of Table 3. Minimum widths for bicycle facilities are shown in Table 1. Bicycle facility dimensions include the gutter pan.

Consult the Bicycle Master Plan for design details on each bike facility type, and consider factors such as ADT, speed limit, and number of lanes when designing the bicycle facilities in accordance with the contextual guidance from FHWA shown in Figure 22 below.

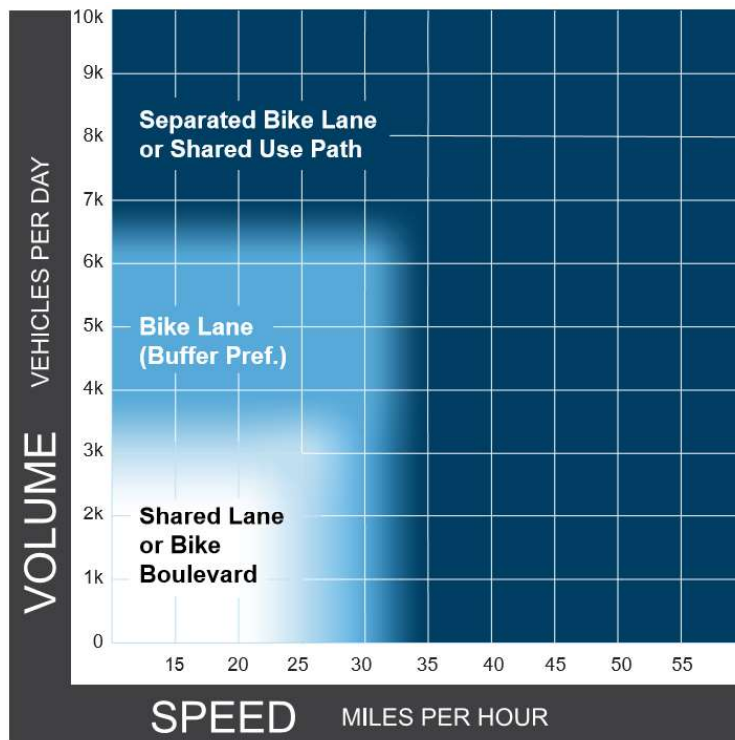
Stress analysis research shows intersections are the toughest part to navigate, especially for people interested but concerned about cycling for transportation. Consult the MUTCD, NACTO Urban Bikeway Design Guide, AASHTO Guide for the Development of Bicycle Facilities, and FHWA Bikeway Selection Guide for corridor and intersection treatments.



**Buffered bike lanes** combine a single-direction bike lane with a buffer to provide a comfortable facility for users. The overall dimension should not be less than 6 feet without a buffer, or less than 7 feet including a buffer. This wider dimension accounts for curb-side obstructions or parked vehicle door dangers. Design should use a parallel line buffer design rather than cross-hatching to minimize the maintenance expense, although short lengths of cross-hatching may be used near conflict zones (intersections or driveways) to better communicate the purpose of the parallel lines as bike lane markings. Vertical elements may be introduced into the bike lane buffer. Planters may be used in downtown and other lower speed areas if they follow the guidelines in the Horizontal Clear Zone section. Reflective plastic bollards may be appropriate elsewhere.

Two-way bike lanes (on the same side of the road) are not addressed in Figure 1. If used they should be a minimum width of 8', although 10' is preferred, with a 2' minimum buffer.

Figure 23 – FHWA Bikeway Guide



**Notes**

- 1 Chart assumes operating speeds are similar to posted speeds. If they differ, use operating speed rather than posted speed.
- 2 Advisory bike lanes may be an option where traffic volume is <3K ADT.

**Neighborhood Greenways** (aka Bike Boulevard) are residential bikeways that prioritize bicycle and pedestrian travel over vehicle through-put. Several tools may be employed to create a greenway. Generally a greenway will be sited on a residential street paralleling a nearby arterial street. Thus connections to destinations along the arterial are readily accessed, though the stress experienced by the walker or biker are much lower. Prioritizing pedestrian and bicycle traffic is achieved by providing appropriate facilities for these modes of travel and by calming or reducing vehicle traffic flows. Greenways are commonly attributed with slow speed, minimum stop signs, and protected crossings of arterial streets.



Some greenway tools include signing and intersection treatments. Signage should be used to highlight the designated greenway, and should also provide distance-based wayfinding to community destinations for bicycle and walking traffic. Intersection treatments are particularly important to the success of a greenway. Intersections with arterial streets need to provide safe and functional crossing methods for bicycle and pedestrian traffic. Intersection treatments might also be used to dissuade vehicle traffic from the greenway. This can be done through limiting turn movements onto the corridor from more busy streets or even by diverting traffic off of the corridor at lower volume intersections. It is important that these treatments are used only on designated greenway corridors, as the impact to neighborhood traffic patterns can be significant.

**Shared-Use Pathways** are typically off-street facilities designed for all non-motorized users. A minimum width of 12 feet is commonly used, although wider sections may be desirable to accommodate high volumes or utility access. Guidelines for shared-use pathways next to roadways are discussed in Section 3.5-4.

**Green paint** should be used only in high conflict areas. Examples of high conflict areas include marking a bike lane through an intersection where there are heavy conflicting right turn movements, marking a contra-flow bike lane through an intersection, or marking the entrance to a right-turn only lane where vehicles must cross the bike lane. Green paint can also be used to connect corridors that are otherwise unclear, when introducing bicycle facilities newly to a corridor, to aid in wayfinding or in places where vehicles are found to encroach on the bicycle facility.

**Bicycle detours** must be planned and implemented whenever work interrupts a bicycle lane. Temporary shared-use lanes may be used, if traffic volumes are acceptable. When traffic volumes are high, bicycle detours should guide cyclists on routes and temporary facilities with relatively similar safety conditions as the route being detoured from.

### 3.7-2 Profile Grades

The maximum profile grade for all streets, alleys, and pathways is 8%. A variance may be granted by the City Engineer considering topography, safety, maintainability, function, and emergency vehicle access. The minimum profile grade for all streets, alleys, and pathways is 0.8%. Cul-de-sac profiles shall be established per section 3.7-3. The profile grade at all residential intersections, along minor roadways at arterials, and for all roadways at controlled intersections shall be no greater than 3% at any point within 100 feet of the near end of the curb radius on minor roadways.

Preservation work need not correct profile grade issues, except as possible to eliminate minor inconsistencies. Reconstruction projects should address needed profile improvements.

### 3.7-3 Horizontal Curves

Horizontal curves are to be determined in accordance with normal civil engineering procedures, considering design speeds, sight distances, roadway crown, building proximity, and vertical grades. For arterial streets with speeds of 30 mph or higher, A 100-foot horizontal curve radius

Figure 24 – Neighborhood Greenway Sign





shall be considered the minimum unless otherwise authorized by the City Engineer. The maximum superelevation on horizontal curves shall be 2%. The minimum horizontal curve radii shall be determined per AASHTO Design for Low Speed Urban Streets, based on design speed, which shall be the posted speed limit, and considering the roadway crown. Pavement widening on horizontal curves to accommodate large vehicles shall be considered per AASHTO Chapter III - Elements of Design, Table III-23.

Preservation work need not correct horizontal curvature issues, except as possible to eliminate minor inconsistencies when the roadway is not bounded by curbing. Reconstruction projects should address needed horizontal curvature improvements within a reasonable effort and cost.

### **3.7-4 Vertical Curves**

Refer to Table 2 for sag and crest vertical curve design criteria. Vertical curves must provide adequate stopping sight distance as defined in the 2011 AASHTO “A Policy on Geometric Design of Highways and Streets”.

Preservation work need not correct vertical curvature issues. Reconstruction projects should address needed vertical curvature improvements, as possible while matching adjacent buildings and driveway grades.

### **3.7-5 Roadway Side Slopes**

Roadway side slopes shall meet the requirements of Table 3; special sloping may be required to meet minimum sight distances.

Preservation work need not correct side slope issues. Reconstruction projects should address needed improvements, particularly where safety has proven to be compromised due to obstructions to sight distance.

### **3.7-6 Design Speed**

Street design sets the context for driver response. Historic design practices have used 85<sup>th</sup> percentile observed speeds or have established design speed higher than the posted speed. In particular, design speed is used during design of horizontal curves. Because design speed is one of the factors in determining street context, it should be established as the posted or target speed. This practice will avoid “speed creep”, which can occur when streets are built to operate at higher speeds than posted and the next design period resets with a speed study revealing the 85<sup>th</sup> percentile has increased. Streets designed for the target operating speed have proven to have greater user compliance, and are thus safer for all users.

**Table 4 – Target speeds by street type**

|                                                        | <b>RESIDENTIAL, INDUSTRIAL, CB AND GC</b> |                |           |       | <b>CC, DOWNTOWN, FORM BASED CODE</b> |                |           |       |
|--------------------------------------------------------|-------------------------------------------|----------------|-----------|-------|--------------------------------------|----------------|-----------|-------|
| Street Type                                            | Principal Arterial                        | Minor Arterial | Collector | Local | Principal Arterial                   | Minor Arterial | Collector | Local |
| Design Speed =<br>Posted Speed =<br>Target Speed (mph) | 30-35                                     | 30-35          | 30        | 25    | 20-30                                | 20-30          | 20-30     | 20-25 |



### 3.7-7 Vertical Clearances

The clearance above any street surface shall be as provided in SMC 17H.010.240 and SMC 12.02.0462.

Preservation projects must coordinate with Urban Forestry to ensure the tree canopy is in compliance. Reconstruction projects must similarly ensure the tree canopy is in compliance, and should consider opportunities to improve upon other hazards or obstructions.

### 3.7-8 Horizontal Clear Zones

This section is intended to replace the former City of Spokane clear zone policy ADMIN 0370-08-04. Clear zones are unobstructed, traversable areas that extend beyond the curb-to-curb dimensions of the traveled street. Clear zones allow for loss of control and other erratic driving behavior. Commonly found fixed objects in the right-of-way include: trees with a diameter of 4 inches or more (measured at 6" above ground surface), wooden poles or posts greater than 16 square inches in cross-section (without breakaway features), bridge piers, retaining walls, landscaping walls, some types of fences, signal poles, signal/lighting/ITS cabinets, culvert ends, utility poles and luminaire poles.

Generally, clear zones can be reduced in urban areas since wide unobstructed sidewalk and/or shoulders lining the roadway encourage higher-speed driver behavior. The presence of street trees and other roadside features tend to decrease overall speeds, increasing safety for all users and more comfort for people walking and biking. The City of Spokane Comprehensive Plan promotes a sense of place, encourages the installation of street trees in the planting/pedestrian buffer strips, and encourages other urban amenities along and adjacent to roadways such as planters, bollards, benches, light fixtures, kiosks, clocks and transit shelters.

The City of Spokane is granted jurisdiction over clear zones along City streets and managed access State highways within the City per RCW 47.24.020(2). Along managed access State highways this authority applies only beyond the curb, or if no curbs, beyond the portion of the roadway used for highway purposes. Between the curbs (median areas) the Washington State Department of Transportation (WSDOT) has jurisdiction over clear zone. WSDOT has full authority over clear zones inside and outside curbs along State limited access facilities within the City.

**Table 5 – Minimum Clear Zone (distance from edge of traveled way)**

|                                                                     | Posted Speed<br>20-35mph                   |                                    | Posted Speed<br>40 or above                |                                    |
|---------------------------------------------------------------------|--------------------------------------------|------------------------------------|--------------------------------------------|------------------------------------|
|                                                                     | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> |
| State Highways                                                      | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 |
| New street construction                                             | n/a                                        | 4                                  | n/a                                        | 10                                 |
| Street reconstruction including width<br>or profile adjustments     | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| Street reconstruction not including<br>width or profile adjustments | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| New installations not related to<br>street construction             | n/a                                        | 4                                  | n/a                                        | 10 <sup>1</sup>                    |



<sup>1</sup> If 10 feet clear distance cannot be provided within the available right-of-way, the design engineer may evaluate and justify placement as near the outer edge of the right-of-way as practical.

<sup>2</sup> On a curbed street all fixed objects shall be at least 1.5 behind curb regardless of the location of the travelled way. This is to ensure clearance for parked vehicle doors, snow removal, sign overhang, etc.

<sup>3</sup> Fixed objects / trees with less than 1.5 feet clearance should be considered for removal or relocation. If clearance is between 1.0 and 1.5 feet existing fixed objects including trees may remain unless damage indicates a history of vehicle collision, the object or tree conflicts with the condition or operation of a street, alley or sidewalk, or removal/relocation is required due to other public safety, convenience or aesthetic considerations.

When indicated by Table 5, rigid objects within the clear zone should be removed or not installed, relocated to a position outside the minimum clear zone, remodeled to make traversable, breakaway, or shielded.

- A larger clear zone on the outside of horizontal curves is desirable. On streets with on-street parking, bike lanes, or on streets without curb the clear zone is measured from the edge of traveled way.
- Signals, cabinets, illumination poles, parking meters and ITS equipment are exempt from the policy, although desired placement is at least 1.5 feet from the face of curb.
- Traffic control signs, fire hydrants and residential mailboxes may be placed in the clear zone if on a breakaway fixture or a frangible design.
- Planter boxes, benches, bike racks, transit shelters, bollards, utility standpipe vents, clocks, trash cans, fencing for sidewalk cafes, kiosks, security barriers, mail drop boxes, tree guard and other street furniture typically used in the downtown and centers and corridors are exempt from the policy, although desired placement is at least 1.5 feet from the face of curb.
- Any planter boxes placed in the street as traffic calming or delineation devices should be of a frangible design or pinned in place. Height including sight blocking vegetation shall not exceed 36 inches.
- Within medians the clear zone should be 1.5 feet along straight sections, and 3 feet near intersections where the median is near the alignment of turning movements.
- The width of on-street parking and bike lanes can be included in the measurement of clear zone distance.
- In areas where sidewalk does not exist, the future location of sidewalk shall be evaluated. Existing buildings or other property improvements may make it prohibitive to provide separated sidewalk with planting or pedestrian buffer strips in the future. If it is determined that future sidewalk will necessitate installation adjacent to curb, the distance behind curb shall be increased to allow installation of the proper width sidewalk without obstructions.
- Attainment of these clear zone values does not relieve the Design Engineer of the responsibility to evaluate sight distances in accordance with applicable design standards.
- A three foot clearance to roadside objects should be provided near turning radii at intersections and driveways to prevent a truck overhang from striking an object.



### 3.7-9 Roadway Drainage

Stormwater collected within the roadway must be effectively routed to drainage facilities, such that flow accumulations and pooling are minimized, or otherwise efficiently dissipated. Minimum roadway profile grades are shown in Table 6. Standard Plan W-101 provides a chart for selecting a roadway crown section based on roadway width and curb height differential. Refer to the City's Standard Plans for cross-section and staking data. For vertical curves, the designer's attention is called to the limiting K-value factors shown in the Table 2 footnotes.

Generally, no more than three lanes should be sloped in any one direction. On wide streets, a quarter-crown or center-crown cross-section is recommended, or the designer may consider stormwater collection at the median.

Refer to Section 3.4-5 herein for stormwater disposal methods and design requirements. New development and re-development treatment requirements are addressed in the stormwater design guidelines.

### 3.7-10 Through Traffic Lanes

Refer to Table 1 for traffic lane design width guidelines.

Reconstruction and preservation work shall incorporate markings for all users of the street as determined within this standard for planned pedestrian, bicycle, and vehicular facilities.

### 3.7-11 Exclusive Turn Lanes

Left and right dedicated turn lanes widen the intersection, often require adding another signal phase, and may lengthen the overall delay for users. Dedicated turn lanes should be used only when specifically determined by an engineering analysis to solve congestion issues. The engineering analysis should consider the impact not only on the target intersection, but also the surrounding street network. Refer to appropriate MUTCD guidelines for design and application of dedicated turn lanes.

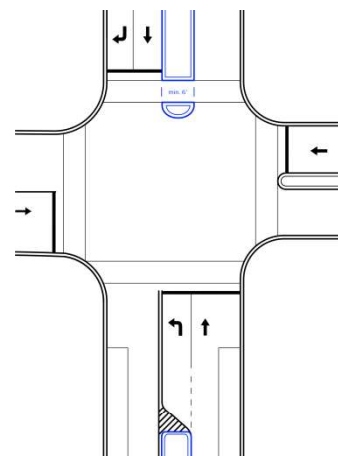
In connected networks, left turns can be restricted at periodic intersections to avoid having long exposed pedestrian crossings at every intersection.

Preservation work need not incorporate roadway reconfiguration projects, unless planned as a follow-up to reconstruction work that conducts such changes, and thus would otherwise leave pavement patching.

### 3.7-12 Tapers

The standard taper length for narrowing or offsetting of a lane shall be based on the design speed, per the U.S. Department of Transportation Manual on Uniform Traffic Control Devices (MUTCD).

Figure 25 – Pedestrian refuge at left turn lane pocket





### 3.7-13 Survey Monuments

At a minimum, monumentation shall be provided in the following locations:

- a) At center of each cul-de-sac
- b) At point of curvature on all horizontal curves
- c) At point of tangency on all horizontal curves
- d) On the roadway centerline at the end of every plat.

Monument pins with cases shall be installed at these locations in accordance with the City's Standard Plans.

These specifications apply to all preservation and reconstruction work.

## 3.8 Median Realm

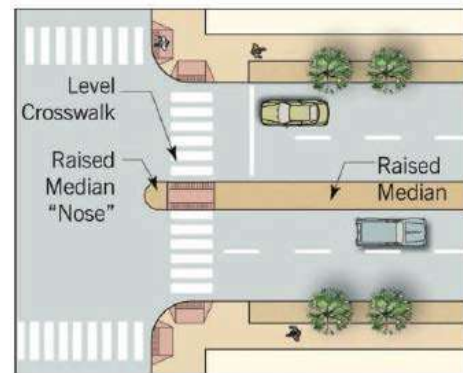
Build medians in accordance with Table 1 on new streets. In retrofit situations, vehicle lanes could be narrowed to add pedestrian refuge islands or medians at unsignalized marked crosswalks on principal or minor arterials in dense zoning<sup>4</sup>. Pedestrian refuge islands should be considered for wider street crossings. A minimum of 6 feet is required for a pedestrian refuge median (8 feet is optimal). However, in retrofit situations a narrow pass-through may be more desirable than no island at all. A narrow median pass-through can provide a place for crosswalk warning signage and also work to reduce vehicular speeds by visually narrowing the roadway. When crosswalks go through a median, protect the crosswalk users with a raised median nose. The end of the median must be marked with a vertical marker for snow plow delineation.

Some transit routes may find it beneficial to place bus stops in the median. This type of setup requires left-side boarding doors on the bus and crosswalks to reach the median. The City Line route, opening in 2021, has designed several median stops.

Speeds can be reduced at neighborhood entry points by installing a short median. This treatment provides a cue to drivers that they are leaving an arterial street and entering a local street. See Figure 28.

Medians, where constructed, shall not exceed 600 feet in length without a break that allows emergency vehicles to cross through the median and continue in the same direction (S-Turn movement). See SMC 17H.010.140 requirements on emergency vehicles access and staging areas on local streets. The break in the median does not need to allow for U-turn movements. Consider the space required for turning movements when installing in tandem with bulbouts.

**Figure 26 - Protecting crossings with a median tip provides safety from turning traffic**



<sup>4</sup> Per crosswalk ordinance <https://static.spokanecity.org/documents/projects/crosswalkordinance/adopted-crosswalk-ord-c35141.pdf>



**Figure 27 – Neighborhood entry median.**



Medians may be combined with on-street parking, bulb-outs or chicanes provided that fire staging areas are provided periodically. These designs must be closely coordinated with the fire department to ensure adequate access to hydrants and structures. Staging areas must not be used for snow storage and must be clearly marked to restrict parking. Hydrants should be located at the staging areas which improves fire access and helps to enforce the parking restriction. Hydrants could also be located in the median, allowing better access and limiting the possibility of blockage by parked cars. Prior to approving hydrants in the median, the method for snowplowing this area and keeping the hydrant clear must be discussed with Streets. Median landscaping should consider the height of adjacent buildings and the need for aerial equipment. Neighborhoods developed with this pattern should also provide a grid network to allow for alternative routes during emergency events.

**Figure 28 – Summit Parkway with medians, bulb-outs and fire staging areas.**



Preservation work need not adjust nor replace medians. Reconstruction projects should consider the space used by the median, and the utility of that space to be maintained as median or other uses. Pavement and median condition should be considered as possible replacement items during scoping of capital work.



### 3.9 Neighborhood Traffic Calming

Traffic calming increases safety through vertical and horizontal traffic slowing measures, and by reducing traffic in residential neighborhood areas. Install traffic calming strategically to protect vulnerable users, reduce speeds in areas exhibiting safety concerns, and as part of the city's Neighborhood Traffic Calming Program. Tools include:

- Horizontal measures – Chicanes, intersection and midblock curb extensions, traffic circles
- Vertical measures – Raised crosswalks, tabletop intersections, installation of sidewalks.
- Traffic reduction – Diverters, medians with walking and bicycling cut-throughs

A formal neighborhood traffic calming program is presently administered by the City through Neighborhood Services. Included in the program is a "Traffic Calming Toolbox", outlining the basic options for solving concerns within any given neighborhood. This toolbox, although not exhaustive, is a reference for optional traffic calming elements within capital or development projects. The NACTO Urban Street Design Guide is also a good reference for traffic calming design. When considering traffic reduction measures, consideration should be given to where traffic will reroute to.

Implementation of traffic calming is required only for approved applications. New developments may include traffic calming measures as appropriate, per SMC 17H.010.160. Preservation and reconstruction projects will install traffic calming elements as programmed.

### 3.10 Pavement Design

#### 3.10-1 Asphalt Binder Selection

All Hot Mix Asphalt binder and aggregates used in the traveled way shall conform with WSDOT specifications, and meet the requirements for durability and performance.

These specifications apply to all rehabilitation maintenance and capital work.

#### 3.10-2 Pavement Section Thickness

The minimum asphalt thickness shall be in accordance with Standard Plan W-101A. As noted in W-101A, the City Engineer may require a pavement design for local access (residential or commercial) streets. This will be evaluated on a case-by-case basis. All arterials require a pavement design, which shall be approved by the City Engineer. A rational pavement design for either arterials or residential streets must contain the following:

1. Traffic Loading – an estimate of the number and types of loadings that roadway will carry for the design life. This estimate of loading must be established by a procedure accepted by the City Engineer and be expressed in 18-Kip Equivalent Single Axle Loads (ESAL's).
2. Subgrade Support—a representative value for the stiffness of the native material on which the road will be built. This value will be established by a procedure accepted by the City Engineer and be expressed as resilient modulus (MR). When determining MR, soil sampling is to include:
  - a) Obtaining a sufficient number of soil samples which adequately represents the subgrade MR, and where significant changes in MR occur;



- b) Constructing a soil log to a minimum of five foot depth below proposed subgrade and classify the soil per USC; and
  - c) Recording the location of where the samples were obtained, normally by station and offset. This record shall be provided to Engineering Services.
3. Analysis- a procedure for establishing the surfacing depth requirements for a given traffic loading and subgrade resilient modulus. The City Engineer must approve this procedure. The following procedure is pre-approved: Guide for Design of Pavement Structures (26), 1994 the American Association of State Highway and Transportation Officials (AASHTO).

The pavement design life is 20 years for new construction and 15 years for pavement overlays. The structural pavement calculations, soil sample locations, lab results, design criteria and recommendations are to be included in a report prepared by the sponsor's engineer. All design factors used are to be listed in the report, including traffic loads projected to occur over the life of the pavement. The report is to be stamped by an engineer, licensed in the State of Washington.

These specifications apply to all preservation and reconstruction work.

### **3.10-3 Pavement Patching**

The City of Spokane adopted the Spokane Regional Pavement Cut Policy in 2005. The adoption resolution is included in Appendix F. This pavement cut policy is updated on a regular basis through coordination with Avista and other local agencies in the Spokane area. All pavement cuts for utility work and patches shall be designed and constructed in accordance with the latest version of this policy.

These specifications apply to all preservation and reconstruction work.

## **3.11 Intersections**

Intersections represent the most complex pieces of the network. They are the place at which multiple modes meet and need to pass safely through. Keeping intersections compact increases eye contact between users, and making them legible or intuitive means each user knows where he or she belongs. Follow these principles of intersection design:

- Make intersections as compact as possible
- Identify utility maintenance access in design considerations
- Analyze intersections as part of a network, not in isolation
- Design intersections as shared spaces
- Integrate space and time; for example adjust signalization timing to improve flow on a corridor

The maximum centerline distance between intersections shall be 660 feet. The minimum recommended centerline distance is 150 feet, or 300 feet for signalized intersections. In general, intersections should be at right angles. The minimum acute intersecting angle for streets shall be 70-degrees. For stop sign-controlled streets the 70-degree (tangent) portion shall extend along the controlled street a minimum of 30 feet from the end of the curb radius. For all cases, the effects of sight distance shall be considered.

Preservation projects may implement adjustments to striping patterns, but will not be expected to adjust curb placement except as necessary for ADA compliance measures. Intersection design



principles should be reconsidered for reconstruction projects. This is particularly important if there are high incidents of collision, but may also be important if the use patterns have evolved since the original construction; i.e. a new industrial area has developed.

### 3.11-1 Design Vehicle

Streets should be designed to serve the most vulnerable user. Designing streets for the largest possible vehicle results in streets with oversized intersections and large turning radii. The result is higher operating speeds for the most frequent vehicles on the street – passenger cars. Use both design vehicles and accommodated vehicles for intersection design. Each intersection is unique, and designing for the largest most frequent vehicle (comprising 10% or more of Average Daily Traffic) allows for better –controlled turning speeds on streets and at intersections. Follow these guidelines for selecting design and accommodated vehicles:

- **Establish a *design vehicle*.** The selected design vehicle should be the largest vehicle that accounts for at least 10% of a street's average daily traffic. Selection of the design vehicle should consider the make-up and expectation for traffic flowing through a given intersection. The design vehicle will dictate the minimum turn radius.
- **Establish an *accommodated vehicle for infrequent users*.** The accommodated vehicle is the largest expected vehicle. Use curb and turning radii that allows the accommodated vehicle to use the full street for turns, including parking lanes, bikeways, and adjacent lanes. Consider medians and curb lines as barriers. Restrict parking near intersections and employ recessed stop lines if needed.

**Figure 29 – Infrequent accommodated vehicle can encroach into opposing lane**



The use of design and accommodated vehicles during design allows more flexibility to adjust designs in favor of pedestrian or bicycle traffic (the most vulnerable users). The following points illustrate options to consider space requirements with this greater latitude.

- Consider the use of tools such as **staggered (offset) stop lines** (where opposing queue storage is adequate) to accommodate vehicles before electing to widen intersection curb alignments.



Figure 30 – Recessed stop bar used where bus must turn right frequently



- The largest frequent user (candidate design vehicle) of most local streets is a 30-foot delivery truck (SU-30). SU-30 vehicles have similar width and wheelbase to a school bus.
- If designing a segment of a designated emergency response route, use appropriate fire apparatus as the accommodated vehicle. In some instances, truck selection might be determined by the fire trucks expected to use the route based on proximity to nearest fire stations.

Table 7 summarizes likely design and accommodated vehicles by context and street type.

Table 7 - Minimum Design Vehicle Standards

|                                                                                                                                                                                  | RESIDENTIAL, INDUSTRIAL <sup>1</sup> ,<br>CB AND GC |       | CC, DOWNTOWN, FORM BASED<br>CODE |              |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------|-------|----------------------------------|--------------|
| Street Type                                                                                                                                                                      | Arterials <sup>2</sup>                              | Local | Arterials <sup>2</sup>           | Local        |
| Design Vehicle<br>(10% or more of ADT)                                                                                                                                           | WB-40                                               | SU-30 | SU-30 & STA 40'<br>bus           | SU-30        |
| Control Vehicle<br>(Infrequent Largest User)                                                                                                                                     | WB-62                                               | WB-62 | Ladder truck                     | Ladder truck |
| <sup>1</sup> Urban streets zoned for industrial uses may require larger design and control vehicles.                                                                             |                                                     |       |                                  |              |
| <sup>2</sup> Intersections of arterials with a local street should use the local street design vehicle unless nearby land uses dictate the need to accommodate a larger vehicle. |                                                     |       |                                  |              |



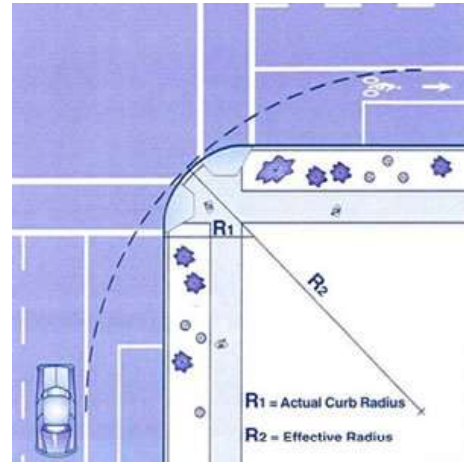
### 3.11-2 Curb Radius

Curb radii influence driver behavior—positively and negatively—affecting turning speeds and the safety of all users. Minimize curb radius based upon the design and accommodated vehicle. Calculate both the actual radius – the radius of the curb itself- and the effective radius, or the wheel track of vehicles. For example, at intersections with on street parking and no curb extensions, the effective radius is much higher than the actual radius. In all cases, consider the widths of the approach and receiving lanes, as crowding may cause poor driver response.

Retrofit existing curbs with curb extensions to reduce actual and effective turning radius. Consider curb extensions whenever on-street parking is present. However, consideration for stormwater flow-lines must be incorporated into design and retrofits.

Curb radius determines turning speed. Use corner radius to keep turning speeds low while allowing the design vehicle to turn.

Figure 31 – Actual vs. Effective Radius



Source: saferoutesinfo.org

Table 8 – Intersection Curb radius and speed

|                                                                                   | RESIDENTIAL, INDUSTRIAL, CB AND GC | CC, DOWNTOWN, FORM BASED CODE |
|-----------------------------------------------------------------------------------|------------------------------------|-------------------------------|
| Actual Radius                                                                     | 20 feet minimum                    | 10 feet minimum               |
| Effective Radius                                                                  | 25 feet minimum                    | 20 feet minimum               |
| Turning Speed <sup>1</sup>                                                        | 10-15 mph                          | 10 mph                        |
| <sup>1</sup> For right turn movements. Left turns will typically be 5 mph faster. |                                    |                               |

### 3.11-3 Bus Bulbs at Intersections

For bus bulbs at intersections, a bulb for a single bus measures 30' long, allowing both doors to open on the bulb, and measures 6-8' wide. On heavy ridership routes where more than one articulated bus platforms several times per day, the bulb measures up to 140' in length. The return angle will be 45 degrees. If the route requires buses to turn right after stopping at a bulb, ensure actual and effective radius meets appropriate bus turning templates.

### 3.11-4 Clear Sight Triangle

For design purposes the clear horizontal sight distance triangle at intersections shall be as described in AASHTO "A Policy on Geometric Design of Highways and Streets", Chapter 9, section on Sight Distance.

For vegetation enforcement purposes, use the clear view triangle shown in SMC 17A.020.030.



### **3.11-5 Roundabouts**

Roundabouts will be reviewed in every case and shall be designed in accordance with WSDOT's design standards. Roundabouts are intended for arterials and collectors. Roundabouts can ease congestion and improve safety at skewed or five-leg intersections.

Typically, roundabouts are larger scale facilities, as they are intended for use along arterials and collectors as previously noted. They facilitate traffic flow without the need for signalization. Roundabouts generally reduce the number of conflict points for vehicles in the intersection and reduce the severity of collisions between vehicles. Design is critical to facilitate safe travel for bicyclists or pedestrians to limit conflicts at the legs of the intersection, as well as to provide needed information for pedestrian alignment and crossing. While vehicle safety is generally improved, improper design can degrade safety for bicycle and pedestrian travel.

Compact urban roundabouts may also be used at city intersections. They have a smaller footprint with and use a completely mountable center island. In many cases existing curb or sidewalk can be left in place.

Preservation work will generally be applied to roundabout pavement surfaces, but implementation of these facilities would qualify as reconstruction.

## **3.12 Signing and Pavement Markings**

### **3.12-1 Traffic Control Signs**

All existing and proposed official traffic control signs required by MUTCD as part of street design shall be shown on the plans, and shall be subject to review and approval by the City Engineer. The plans shall include all existing and proposed signs, show the full width of the street, include any signs on the opposite side of the street, and show existing conditions beyond the proposed development. Prior to construction, shop drawings for all new street signs shall be submitted to Street Maintenance - Signs and Markers for approval.

Preservation and reconstruction work should update signage as appropriate.

Warning and regulatory signs provide motorists with critical information and need to be visible in order to be effective. Provide minimum sight distances according to Table 3-1 in the 2011 AASHTO "A Policy on Geometric Design of Highways and Streets".

### **3.12-2 Pavement Markings**

Design plans for pavement markings shall be approved by the City Engineer prior to construction. Plans shall include all existing and proposed striping, show the full width of the street, and show existing conditions beyond the proposed development. Any existing markings that are to be removed shall be clearly designated.

Preservation and reconstruction work shall incorporate markings for all users of the street as determined within this standard for planned pedestrian, bicycle, and vehicular facilities.

Plastic is the preferred material for pavement markings on Principal and Minor Arterials. Stop lines, crosswalk lines, wide lines (gore stripe), dotted wide lines, dotted bicycle lines, dotted extension lines, arrows, words and symbols shall be preformed thermoplastic. Other lines may be paint with thermoplastic dots according to the City of Spokane Standard Plans.



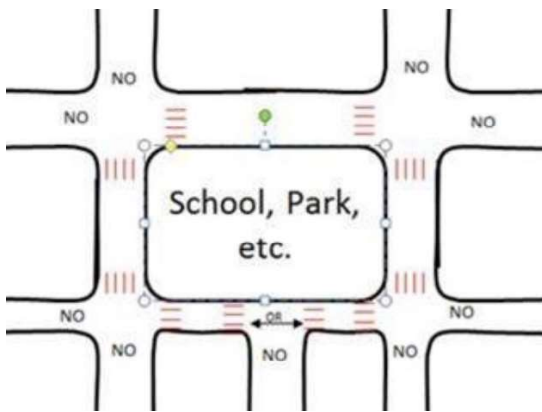
### 3.12-3 Crosswalks

Facilitate safe pedestrian crossings along centers and corridors, and near pedestrian generators. The crosswalk standards are outlined in SMC 17H.010.210 and SMC 17H.010.215. In general these sections of code require the following:

- **Placement.** Provide marked crosswalks along centers and corridors and near schools, parks, hospitals, churches, trail crossings, and other significant pedestrian generating facilities.
- **Design.** In the Downtown, Commercial, Centers and Corridors, and Form Based Code zones, a minimum 6-foot pedestrian refuge at unsignalized crosswalk locations is encouraged where the total crossing is 3 or more automotive lanes.
- **Striping.** Refer to City of Spokane Standard Plans.
- **Stop bar.** Refer to City of Spokane Standard Plans.
- **RRFBs/PHBs.** Install pedestrian-activated tools such as Rectangular Rapid-Flash Pedestrian Beacons and Pedestrian Hybrid Beacons in locations that serve pedestrian generators as ascribed by engineering analysis and approved by the City Engineer. The MUTCD and FHWA-SA-18-018 shall be used as a reference for determining the appropriate crosswalk treatment.

The following exhibit is intended to provide clarification on crosswalk placement based on SMC 17H.010.210.

Figure 32 – Crosswalk placement near schools and parks



## 3.13 Traffic Signals and Intelligent Transportation Systems

### 3.13-1 Traffic Signal Design

Street traffic signals shall be designed with direct coordination and review by the City Street Department. Preservation and reconstruction work should consider traffic signal updates and replacements as appropriate.

- In downtown, use signal progression to promote smooth progression of vehicular traffic at or below the posted speed in an effort to reduce congestion. Work to reduce signal delay on heavily used bike routes.



- Use of Pedestrian Recall is addressed in SMC 16A.84.040.
- In urban areas with heavy pedestrian traffic, consider the use of Leading Pedestrian Intervals (LPI). LPIs add a few seconds of time for pedestrians to establish themselves in the crosswalk before the vehicle signal turns green, enforcing that turning traffic yield to pedestrians. If LPI is used without Accessible Pedestrian Signals the walk interval may need to be increased to aid sight impaired pedestrians who listen for the parallel traffic movement to know when to walk. LPI is addressed in SMC 16A.84.
- Signalized intersections should be re-timed approximately every five years to reduce both air pollution and delay.
- At rehabilitated or new signals, retrofit with Accessible Pedestrian Signals. Prioritize APS installations near concentrations of vulnerable populations, such as near senior centers or hospitals. Intersection APS retrofits are addressed in SMC 16A.84.060.
- Signal interconnection of traffic signals to the Central City Signal Server via fiber optic or copper Ethernet for progressing traffic through an area. New signal and pedestrian hybrid beacon installations should include interconnect infrastructure.

### **3.13-2 Intelligent Transportation Systems**

The City of Spokane uses several types of Intelligent Transportation Systems (ITS) throughout the City to help monitor and manage traffic flow.

- PTZ cameras provide live video feeds to the regional traffic management center and are used by city staff to monitor traffic conditions, adjust signal timing, and perform studies. Additional fixed cameras provide telemetry at several intersections throughout the City.
- Permanent count stations are located throughout the City. These provide count information throughout the year.
- Over 95% of the City's traffic signals communicate with a central server via Ethernet over copper or fiber. Remote access is also available to all City owned PTZ, fixed cameras and dynamic message signs.
- Dynamic Message Signs have been installed in key arterial locations within the city to display messages related to traffic control and safety.
- Flashing school beacons have been installed at most of the schools in the city limits to provide real-time information to drivers on the times the 20 mph speed limit is in effect.
- Speed feedback signs have been installed through the traffic calming program. Some models can provide count and speed data.
- Bike and pedestrian count stations are installed on select regional trails within the city and provide time of day, weekday vs. weekend and season count data for use in planning.
- Remote Weather Information System (RWIS) units provide information on air temperature, humidity, dew point and road surface temperature. One is currently installed on the south hill.
- Bluetooth/WiFi readers are used to monitor corridor travel times on Maple/Ash, Division, Freya/Greene/Market, and US 2 in cooperation with the Spokane Regional Traffic Management Center.

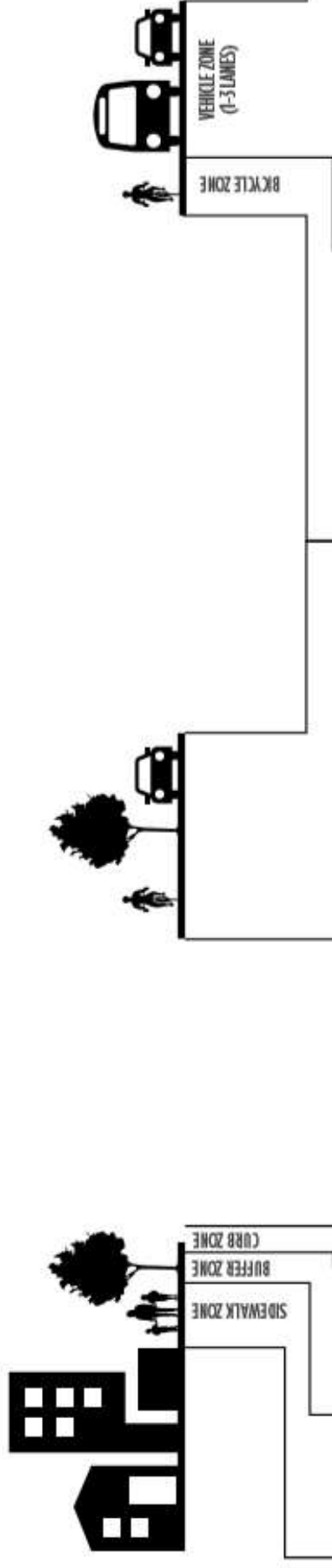
## **3.14 Reference Tables**



## PEDESTRIAN REALM

**FLEXIBLE AREA**

## VEHICLE REALM



|           | Sidewalk Zone <sup>a</sup>                                 | Buffer Zone <sup>c,d</sup> | Curb Zone | Opt. Shared Use Path <sup>e</sup> | Stormwater Management <sup>h</sup> | Curb Extensions/<br>Bus Bulbs <sup>i</sup> | Parking | Bicycle Lane <sup>g</sup> | Bicycle Buffer <sup>j</sup> | Vehicle Zone Outer Lane <sup>k</sup> | Vehicle Zone Inner Lane <sup>k</sup> | Vehicle Left Turn <sup>l</sup> |
|-----------|------------------------------------------------------------|----------------------------|-----------|-----------------------------------|------------------------------------|--------------------------------------------|---------|---------------------------|-----------------------------|--------------------------------------|--------------------------------------|--------------------------------|
|           | Downtown DTC, DTG, DTS; Form Based Code CA1, CA2, CA3, CA4 |                            |           |                                   |                                    |                                            |         |                           |                             |                                      |                                      |                                |
| Local     | 7                                                          | 5                          | 0.5       | 12                                | Varies                             | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Collector | 7                                                          | 5                          | 0.5       | 12                                | Varies                             | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Arterial  | 7                                                          | 5                          | 0.5       | 12                                | Varies                             | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Freeway   | 7                                                          | 5                          | 0.5       | 12                                | Varies                             | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   |                                |
|           | Commercial O, OR, NR, NMU, CB, GC                          |                            |           |                                   |                                    |                                            |         |                           |                             |                                      |                                      |                                |
| Local     | 7                                                          | 5                          | 0.5       | 12                                | 10                                 | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Collector | 7                                                          | 5                          | 0.5       | 12                                | 10                                 | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Arterial  | 7                                                          | 5                          | 0.5       | 12                                | 10                                 | 7                                          | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Freeway   | 7                                                          | 5                          | 0.5       | 12                                | 6.5                                | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   |                                |
|           | Residential RA, RSF, RSF-C, RTF, RME, RHD                  |                            |           |                                   |                                    |                                            |         |                           |                             |                                      |                                      |                                |
| Local     | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Collector | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Arterial  | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | 8       | 6                         | 1.5-6'                      | 11                                   | 11                                   |                                |
| Freeway   | 5                                                          | 6                          | 0.5       | 12                                | 6.5                                | NA                                         | 7       | 6                         | NA                          | 10                                   | NA                                   |                                |
|           | Industrial LI, HI, PI                                      |                            |           |                                   |                                    |                                            |         |                           |                             |                                      |                                      |                                |
| Local     | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | NA      | 6                         | 3                           | 12                                   | 12                                   |                                |
| Collector | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | 8       | 6                         | 3                           | 12                                   | 12                                   |                                |
| Arterial  | 5                                                          | 6                          | 0.5       | 12                                | 10                                 | NA                                         | 8       | 6                         | 1.5-6'                      | 12                                   | 12                                   |                                |
| Freeway   | 5                                                          | 6                          | 0.5       | 12                                | 6.5                                | NA                                         | 7       | 6                         | NA                          | 11                                   | NA                                   |                                |

and minor adjustments are allowed to fit the street context. See discussion in section 3.2.2 for further detail.

residential sidewalk requirements. In locations where existing sidewalks exceed the dimension in Table 1, the sidewalk width is not a factor in the determination of street improvement.

and continuous buffer requires a 5-foot minimum width for commercial zones. For residential and industrial zones, the buffer may be reduced to 3 feet. In select cases, a narrower buffer may be used in select zones if tree vaults are implemented.

planted or concrete. When stormwater disposal is a governing concern, consideration should be given to use pervious

options which are chosen based on what makes most sense according to city plans, environmental responsibilities, and so on will fit within the project. Only in very rare cases will more than one fit - for instance, a parking lane plus bio-retention.

H. Consult the Spokane Regional Stormwater Manual and/or Eastern Washington Low Impact Development Guidance Manual for design standards for stormwater facilities. The stormwater catchment area must meet the required volume generated by the planned impervious area. In Downtown Spokane, the stormwater catchment area must also include the area within the City's urban growth boundary. Stormwater facilities shall be designed to meet the requirements of the City of Spokane's Stormwater Management Ordinance.

1. On Transit corridors, use bus bays to ease boarding, reduce sidewalk congestion, and allow buses to easily re-enter traffic.

J. "High Traffic" and "Medium Traffic" lane routes on the Master Bicycle Plan should include buffers. Separation buffer between boulevards should be implemented via parallel lane edge stripes with a periodic cross-hatch. 3' is the minimum buffer unless a raised curb is used. Wider buffers are allowable but should be well marked with hatching or bollards.

K. When constraints are prohibitive, consider 10-foot lane width as the minimum.



**Table 2 – Vertical Curve Design Parameters**

|                                                                                                                                                                                                                                                                                                                                                                                         | ARTERIALS<br>(all types) | LOCAL  | ALLEY  | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|--------|--------|------------------------------------|
| Minimum Design Speed <sup>1</sup>                                                                                                                                                                                                                                                                                                                                                       | 30 mph                   | 25 mph | 20 mph | 20 mph                             |
| Vertical Curves <sup>2</sup> are required if the Algebraic Grade Difference, A, is:                                                                                                                                                                                                                                                                                                     | A>1%                     | A>2%   | A>2%   | A>2%                               |
| Minimum Length is 3 times the Design Speed                                                                                                                                                                                                                                                                                                                                              |                          |        |        |                                    |
| <sup>1</sup> Design speed is posted speed. In practice speeds may be less or more than shown depending on other design factors not accounted for herein. The design engineer shall justify the use of values other than those listed above.<br><sup>2</sup> Curves must meet stopping sight distance per AASHTO 2011. "K" of 167 is used to find the maximum curve length for drainage. |                          |        |        |                                    |

**Table 3 – Side slopes**

|                                                                                                       | ARTERIALS    | LOCALS       | ALLEYS       | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|-------------------------------------------------------------------------------------------------------|--------------|--------------|--------------|------------------------------------|
| Grade break at back of walk<br>up<br>down                                                             | 4:1<br>4:1   | 1.5:1<br>2:1 |              |                                    |
| Grade break at back of walk<br>up<br>down                                                             | 1.5:1<br>2:1 | 1.5:1<br>2:1 |              |                                    |
| Grade break at edge of pavement<br>up<br>down                                                         |              |              | 1.5:1<br>2:1 | 1.5:1<br>2:1                       |
| Grade break at edge of traveled way, including any shoulders<br>up<br>down                            |              |              | 1.5:1<br>2:1 | 1.5:1<br>2:1                       |
| <b>Notes:</b><br>Use WSDOT standards when curbs do not exist.<br>Grades shown are horizontal:vertical |              |              |              |                                    |



**Table 4 – Target Speeds by Street Type**

|                                                        | RESIDENTIAL, INDUSTRIAL, CB<br>AND GC |                |           |       | CC, DOWNTOWN, FORM BASED CODE |                |           |       |
|--------------------------------------------------------|---------------------------------------|----------------|-----------|-------|-------------------------------|----------------|-----------|-------|
| Street Type                                            | Principal Arterial                    | Minor Arterial | Collector | Local | Principal Arterial            | Minor Arterial | Collector | Local |
| Design Speed =<br>Posted Speed =<br>Target Speed (mph) | 30-35                                 | 30-35          | 30        | 25    | 20-30                         | 20-30          | 20-30     | 20-25 |

**Table 5 – Minimum Clear Zone (distance from edge of traveled way)**

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | Posted Speed<br>20-35mph                   |                                    | Posted Speed<br>40 or above                |                                    |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------|------------------------------------|--------------------------------------------|------------------------------------|
|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> |
| State Highways                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 |
| New street construction                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | n/a                                        | 4                                  | n/a                                        | 10                                 |
| Street reconstruction including width<br>or profile adjustments                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| Street reconstruction not including<br>width or profile adjustments                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| New installations not related to<br>street construction                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | n/a                                        | 4                                  | n/a                                        | 10 <sup>1</sup>                    |
| <sup>1</sup> If 10 feet clear distance cannot be provided within the available right-of-way, the design engineer may evaluate and justify placement as near the outer edge of the right-of-way as practical.<br><sup>2</sup> On a curbed street all fixed objects shall be at least 1.5 behind curb regardless of the location of the travelled way. This is to ensure clearance for parked vehicle doors, snow removal, sign overhang, etc.<br><sup>3</sup> Fixed objects / trees with less than 1.5 feet clearance should be considered for removal or relocation. If clearance is between 1.0 and 1.5 feet existing fixed objects including trees may remain unless damage indicates a history of vehicle collision, the object or tree conflicts with the condition or operation of a street, alley or sidewalk, or removal/relocation is required due to other public safety, convenience or aesthetic considerations. |                                            |                                    |                                            |                                    |



**Table 6 – Street Profile Grades**

|                                                                                                                                                                                                                                                                                                                                                                                                                                           | ARTERIALS | LOCALS            | ALLEYS | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|-------------------|--------|------------------------------------|
| Minimum Profile Grade                                                                                                                                                                                                                                                                                                                                                                                                                     | 0.8%      | 0.8% <sup>1</sup> | 0.8%   | 0.8%                               |
| Maximum Profile Grade                                                                                                                                                                                                                                                                                                                                                                                                                     | 8.0%      | 8.0%              | 8.0%   | 8.0%                               |
| Grade at Intersections <sup>2</sup>                                                                                                                                                                                                                                                                                                                                                                                                       | n/a       | n/a               | n/a    | n/a                                |
| <sup>1</sup> Cul-de-sac profiles shall be established to provide minimum one percent grades at all places along the gutter lines.<br><sup>2</sup> Unless otherwise approved by the Engineer, the profile grade at all residential intersections, along the minor roadway at arterials, and for all roadways at controlled intersection shall be no greater than three percent at any point within 100 feet of the near end of the radius. |           |                   |        |                                    |

**Table 7 – Minimum Design Vehicle Standards**

|                                                                                                                                                                                                                                                                                          | RESIDENTIAL, INDUSTRIAL <sup>1</sup> ,<br>CB AND GC |       | CC, DOWNTOWN, FORM BASED<br>CODE |              |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------|-------|----------------------------------|--------------|
| Street Type                                                                                                                                                                                                                                                                              | Arterials <sup>2</sup>                              | Local | Arterials <sup>2</sup>           | Local        |
| Design Vehicle<br>(10% or more of ADT)                                                                                                                                                                                                                                                   | WB-40                                               | SU-30 | SU-30 & STA 40'<br>bus           | SU-30        |
| Control Vehicle<br>(Infrequent Largest User)                                                                                                                                                                                                                                             | WB-62                                               | WB-62 | Ladder truck                     | Ladder truck |
| <sup>1</sup> Urban streets zoned for industrial uses may require larger design and control vehicles.<br><sup>2</sup> Intersections of arterials with a local street should use the local street design vehicle unless nearby land uses dictate the need to accommodate a larger vehicle. |                                                     |       |                                  |              |

**Table 8 – Curb radius standard**

|                                                                                   | RESIDENTIAL, INDUSTRIAL, CB AND GC | CC, DOWNTOWN, FORM BASED CODE |
|-----------------------------------------------------------------------------------|------------------------------------|-------------------------------|
| Actual Radius                                                                     | 20 feet minimum                    | 10 feet minimum               |
| Effective Radius                                                                  | 25 feet minimum                    | 20 feet minimum               |
| Turning Speed <sup>1</sup>                                                        | 10-15 mph                          | 10 mph                        |
| <sup>1</sup> For right turn movements. Left turns will typically be 5 mph faster. |                                    |                               |



**Table 9 – Profile grade of sidewalks and buffer strips**

|                                                                      | All Zoning                   |            |
|----------------------------------------------------------------------|------------------------------|------------|
| Street Type                                                          | Arterials                    | Local      |
| Sidewalk Cross Slope                                                 | 1.5% to 2%                   | 1.5% to 2% |
| Sidewalk Profile Grade<br>Contiguous with curb<br>Isolated from curb | Same grade as street profile |            |
|                                                                      | 5% max                       | 5% max     |





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

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CITY ADMINISTRATOR SIGNATURE:

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## ORDINANCE NO. C35985

AN ORDINANCE relating to Design Standards, amending SMC sections 17H.010.030, 17H.010.050, 17H.010.060, 17H.010.070, 17H.010.120, 17H.010.140, 17H.010.160, 17H.010.180, 17H.010.190, 17H.010.200, 17H.010.210, 17H.010.220, 17H.010.230, 17H.010.240, 17H.010.250 and 17H.010.260 of the Spokane Municipal Code.

WHEREAS, the City of Spokane is updating Chapter 3 of the Design Standards, and as such must ensure consistency with Chapter 17H.010; and

WHEREAS, on July 16, 2020 the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to development regulations; and

WHEREAS, on July 22, 2020 a State Environmental Policy Act (SEPA) checklist was issued with a request for comments on the proposal, many comments were received and incorporated into the document; and

WHEREAS, on August 31, 2020 a Determination of Non-Significance was issued by the responsible official, and no appeal was received prior to the deadline of September 22, 2020; and

WHEREAS, on or about October 14, 2020 following a process involving a number of public workshops and a public hearing, a majority of the City of Spokane Plan Commission voted to recommend approval of an ordinance amending the Engineering Standards in Chapter 17 SMC; -

Now Therefore, the City of Spokane does ordain:

Section 1. That SMC section 17H.010.030 is amended to read as follows:

### **17H.010.030 Street Layout Design**

- A. Street design is governed by the comprehensive plan and city design standards.
- B. Streets shall be designed in light of topography and existing and planned street patterns. It is encouraged that low impact development principles be considered, evaluated and utilized where practical as described in the Eastern Washington Low Impact Development Guidance Manual.
- C. Adequate access shall be provided to all parcels of land. The street system shall facilitate all forms of transportation including pedestrians, bicycles, vehicles, transit and emergency services.



- D. When property is divided into large parcels, streets shall be laid out so as to allow the addition of future streets in a consistent pattern in the event of redivision.
- E. Street names should be logical, consistent and understandable to satisfy the needs of emergency and delivery vehicles. Street names must be approved by the City and comply with the requirements of chapter 17D.050A SMC, Roadway Naming.
- F. The layout of new streets shall provide for the continuation of existing streets in adjoining subdivisions. If a public street or right-of-way terminates at a plat boundary, provisions shall be made for the extension of the public street to the adjacent property or to another public street in a manner consistent with public mobility and utility infrastructure needs.
- G. Street layout shall provide for future extension of streets into areas which are presently not subdivided.
- H. Traffic generators within the project should be considered and the street system designed appropriately. Individual projects may require a traffic study subject to chapter [17D.080 SMC](#), (~~Voluntary Impact Fees, chapter 17D.010 SMC~~) Transportation Impact Fees, chapter [SMC 17D.075](#), Concurrency Certification, or chapter [17E.050 SMC](#), SEPA.
- I. The minimum centerline distance between intersections shall be one hundred fifty feet.
- J. Bordering arterial routes should be considered and design continuity provided.
- K. When any parcels in a subdivision adjoin an existing or proposed arterial street, the hearing examiner may require access by way of frontage streets and may restrict access to the arterial.
- L. Subdivisions comprised of more than thirty lots shall include two access points acceptable to the city fire department and the director of engineering services.
- M. A grid pattern featuring more street intersections and shorter block lengths should be implemented wherever possible.
- N. Block lengths should not exceed six hundred sixty feet.
- O. A block width should allow for two tiers of lots between parallel streets and double frontage lots should be avoided.



P. Permanent dead-end or cul-de-sac streets may be allowed when the property is isolated by topography or the configuration of existing platted lots and streets. Dead-ends and cul-de-sacs will be reviewed in every case for connectivity.

Section 2. That SMC section 17H.010.050 is amended to read as follows:

**17H.010.050 Right-of-Way**

- A. Public right-of-way widths shall be in accordance with the city's comprehensive plan, the city's engineering design standards or as directed by the director of engineering services. Minimum right-of-way widths are as shown in Tables 17H.010-1 and 17H.010-2(~~(, Right-of-way and Street Widths)~~). The right-of-way width varies based on the required street elements including number of lanes, on-street parking, bike lanes, medians, turn lanes, roadside swales, pedestrian buffer strips, transit needs and street trees.
- B. Where infill development occurs on partially constructed blocks, the proposed right-of-way width shall at least match the existing right-of-way width for the rest of the block and adjoining blocks.
- C. Private streets shall be located on a tract; street easements are not permitted. The width of the tract for private streets shall meet the minimum right-of-way widths designated in Tables 17H.010-1 and 17H.010-2.
- D. Public rights-of-way or private tracts shall contain all street elements including paving, curbing, gutters and pedestrian buffer strips or swales in accordance with the city's design standards.
- E. Sidewalks may be located on easements on private property.
- F. Narrower right-of-way widths may be allowed at the discretion of the director of engineering services. Variance requests will be evaluated based on topography, traffic circulation, emergency vehicle access, zoning, existing development and on-street parking requirements.

| ((Table 17H.010-1<br>Right of way and Street Widths |                                         |                        |                      |
|-----------------------------------------------------|-----------------------------------------|------------------------|----------------------|
| -                                                   | Minimum Right of way Width <sup>1</sup> |                        | Minimum Street Width |
| -                                                   | Sidewalks in ROW                        | Sidewalks on Easements | Curb to Curb         |



| ARTERIAL                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                     |        |                     |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------|--------|---------------------|
| Principal                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 6 lane — 110 ft.<br>4 lane — 90 ft. | NA     | Varies <sup>2</sup> |
| Minor                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | 4 lane — 102 ft.<br>2 lane — 75 ft. | NA     | Varies <sup>2</sup> |
| Collector                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 65 ft.                              | NA     | 40 ft.              |
| LOCAL ACCESS                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |                                     |        |                     |
| Commercial                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | 65 ft.                              | 55 ft. | 40 ft.              |
| Residential Standard                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | 60 ft.                              | 50 ft. | 36 ft.              |
| Residential Low Density <sup>3</sup>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | 56 ft.                              | 46 ft. | 32 ft.              |
| Residential Restricted Parking <sup>3,4</sup>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | 51 ft.                              | 41 ft. | 27 ft.              |
| Hillside Development <sup>4,5</sup>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | 40 ft.                              | 35 ft. | 27 ft.              |
| Cul-de-sac (radius)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | 56 ft.                              | 51 ft. | 50 ft.              |
| Alley <sup>6</sup>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         | 20 ft.                              | 20 ft. | 12 ft.              |
| <p>Notes:</p> <p><sup>1</sup>Additional right-of-way may be required if roadside swales are used to control storm drainage.</p> <p><sup>2</sup>Curb-to-curb width varies depending on street features including number of lanes, on-street parking, bike lane, median and turn lanes.</p> <p><sup>3</sup>Narrow streets are appropriate only in low density (four to ten units per acre) residential neighborhoods. Adequate emergency vehicle access and staging areas must be provided as discussed in <a href="#">SMC 17H.010.140</a>.</p> <p><sup>4</sup>Parking is allowed on one side of the street only. Refer to <a href="#">SMC 17H.010.120</a> for on-street parking requirements.</p> <p><sup>5</sup>Refer to <a href="#">SMC 17H.010.110</a> for more information.</p> <p><sup>6</sup>Alleys do not require sidewalk or curb. The widths shown apply to right-of-way and pavement width.))</p> |                                     |        |                     |

| <p align="center"><u>Table 17H.010-1</u><br/><u>Arterial Right-of-way Widths</u></p> |                           |                     |
|--------------------------------------------------------------------------------------|---------------------------|---------------------|
| -                                                                                    | <u>Right-of-way Width</u> | <u>Street Width</u> |



| -                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | <u>Minimum<sup>1</sup></u> | <u>Typical</u>        | <u>Curb to Curb</u>       |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------|-----------------------|---------------------------|
| <u>ARTERIAL (all types)</u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             |                            |                       |                           |
| <u>2 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>60 ft</u>               | <u>60 ft – 80 ft</u>  | <u>Varies<sup>3</sup></u> |
| <u>3 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>65 ft</u>               | <u>65 ft – 80 ft</u>  | <u>Varies<sup>3</sup></u> |
| <u>4 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>75 ft</u>               | <u>75 ft – 100 ft</u> | <u>Varies<sup>3</sup></u> |
| <u>5 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>90 ft</u>               | <u>80 ft – 100 ft</u> | <u>Varies<sup>3</sup></u> |
| <u>6 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>100 ft</u>              | <u>90 ft - 110 ft</u> | <u>Varies<sup>3</sup></u> |
| <u>7 lanes<sup>2</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | <u>100 ft</u>              | <u>90 ft – 125 ft</u> | <u>Varies<sup>3</sup></u> |
| <u>Notes:</u><br><sup>1</sup> <u>Additional right-of-way may be required if roadside swales are used to control storm drainage, for bike lanes if designated on the plan, or for wider sidewalks depending on the zoning.</u><br><sup>2</sup> <u>Lanes can be through lanes, turn pockets, or continuous TWLTL.</u><br><sup>3</sup> <u>Curb-to-curb width varies depending on street features including number of lanes, on-street parking, bike lane, median and turn lanes. See Design Standards for more detail.</u> |                            |                       |                           |

| <u>Table 17H.010-2</u><br><u>Local Access Right-of-way and Street Widths</u> |                                               |                               |                             |
|------------------------------------------------------------------------------|-----------------------------------------------|-------------------------------|-----------------------------|
|                                                                              | <u>Minimum Right-of-way Width<sup>1</sup></u> |                               | <u>Minimum Street Width</u> |
|                                                                              | <u>Sidewalks in ROW</u>                       | <u>Sidewalks on Easements</u> | <u>Curb to Curb</u>         |
| <u>LOCAL ACCESS</u>                                                          |                                               |                               |                             |
| <u>Commercial/Industrial</u>                                                 | <u>60 ft.</u>                                 | <u>50 ft.</u>                 | <u>36 ft.</u>               |
| <u>Residential High Density<sup>2</sup></u>                                  | <u>60 ft.</u>                                 | <u>50 ft.</u>                 | <u>36 ft.</u>               |
| <u>Residential Standard<sup>3</sup></u>                                      | <u>56 ft.</u>                                 | <u>46 ft.</u>                 | <u>32 ft.</u>               |
| <u>Residential One-side Parking<sup>4</sup></u>                              | <u>51 ft.</u>                                 | <u>41 ft.</u>                 | <u>27 ft.</u>               |
| <u>Hillside Development<sup>4.5</sup></u>                                    | <u>40 ft.</u>                                 | <u>35 ft.</u>                 | <u>27 ft.</u>               |



|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              |               |               |               |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------|---------------|---------------|
| <u>Cul-de-sac<br/>(radius)</u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | <u>56 ft.</u> | <u>51 ft.</u> | <u>50 ft.</u> |
| <u>Alley<sup>6</sup></u>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | <u>20 ft.</u> | <u>20 ft.</u> | <u>12 ft.</u> |
| <p><u>Notes:</u></p> <p><sup>1</sup><u>Additional right-of-way may be required if roadside swales are used to control storm drainage.</u></p> <p><sup>2</sup><u>Appropriate in areas where parking on both sides of the street is expected on a regular basis, such as apartment complexes. Refer to <a href="#">SMC 17H.010.070</a> for more information.</u></p> <p><sup>3</sup><u>Appropriate in areas where homes have street-facing garages and driveways for parking. On-street parking is used by visitors and extra vehicles. Refer to <a href="#">SMC 17H.010.070</a> for more information.</u></p> <p><sup>4</sup><u>Parking is allowed on one side of the street only. Refer to <a href="#">SMC 17H.010.120</a> for on-street parking requirements.</u></p> <p><sup>5</sup><u>Refer to <a href="#">SMC 17H.010.110</a> for more information.</u></p> <p><sup>6</sup><u>Alleys do not require sidewalk or curb. The widths shown apply to right-of-way and pavement width.</u></p> |               |               |               |

Section 3. That SMC section 17H.010.060 is amended to read as follows:

#### **17H.010.060 Street Width - General**

- A. Minimum curb-to-curb street widths are shown in Tables 17H.010-1 and 17H.010-2. Street width varies based on the required street elements including number of lanes, on-street parking, bike lane, median, and turn lanes.
- B. Generally, street design shall allow for a twenty-feet clear width for emergency vehicle access. New streets with less than a twenty-feet clear width shall provide emergency vehicle staging areas as described in [SMC 17H.010.140](#).
- C. ~~((Spacing between collector arterials shall be no more than one-half mile.))~~  
The clear width may be reduced to fourteen feet on each side of a median for distances of fifty linear feet or less. This may be used for purposes of traffic calming, crosswalks or neighborhood entry medians.
- D. Where infill development occurs on partially constructed blocks, the proposed street width may match the existing street width for the rest of that block.

Section 4. That SMC section 17H.010.070 is amended to read as follows:

#### **17H.010.070 Street Width – ~~((Low Density)) Residential ((Zones))~~ Uses**



A. The street width may be reduced to twenty-seven feet on local access streets in low density (four to ten units per acre) residential zones if parking is omitted on one side of the street. Refer to [SMC 17H.010.120](#) for on-street parking requirements.

B. The local access residential street((s)) standard shall be ((in low density residential areas may be narrowed to)) thirty-two feet with parking on both sides. This is intended for use in areas with street-facing garages and driveways, where on-street parking is primarily used by visitors and extra vehicles. ((if the following conditions are met:))

- ~~1. Each block is connected on both ends and does not exceed six hundred sixty feet in length.~~
- ~~2. The narrower street does not extend more than one thousand three hundred twenty feet without intersecting a street with twenty feet clear width.~~
- ~~3. Adequate emergency vehicle access and staging areas are provided. Refer to SMC 17H.010.140 for emergency access and staging requirements.~~
- ~~4. The profile grade for the street does not exceed eight percent.))~~

C. The residential high-density standard shall be thirty-six feet with parking on both sides. This is intended for use in areas where parking on both sides of the street is expected on a regular basis, such as near apartment complexes.

~~((G.))~~ D. Additional parking restrictions may be required near intersections on ~~((narrowed))~~ thirty-two foot streets. The turning movements of service and emergency vehicles must be evaluated to ensure that on-street parking does not interfere with access.

~~((D. Streets that are designed to connect to an adjacent site or that will serve lots on an adjacent site may not be narrowed.))~~

Section 5. That SMC section 17H.010.120 is amended to read as follows:

#### **17H.010.120 On-Street Parking**

A. Streets located in the central business district and in centers and corridors ~~((require))~~ should provide on-street parking.

B. Principal, minor and collector arterials outside of the central business district, centers and corridors will be reviewed on a case-by-case basis to determine on-street parking needs.

C. On-street parking lanes ~~((shall))~~ should be eight feet wide. In low density residential areas meeting the criteria in [SMC 17H.010.070](#), parking lanes may be narrowed to seven feet to allow for a narrower street section.



D. ~~((Generally, all))~~ In locations with densities greater than ten units per acre new local access streets shall provide on-street parking on both sides of the street. Parking may be omitted from one side of a residential street in the following situations:

1. Hillside developments as described in [SMC 17H.010.110](#) where lots are developed on only one side of the street.
2. Neighborhoods where garage access is provided from alleys and driveway access to the street is restricted.
3. The side of a street adjacent to side yards, rear yards, or common areas such as stormwater facilities. Parking may not be omitted adjacent to parks or other recreational facilities.
4. Locations with low density (four to ten units per acre).

E. Where parking has been omitted, "No Parking" signs shall be installed at the developer's expense.

F. Street width may vary within a subdivision to provide one or two-sided parking appropriate to the adjacent properties.

Section 6. That SMC section 17H.010.140 is amended to read as follows:

#### **17H.010.140 Emergency Vehicle Access and Staging Areas**

- A. Where the street design does not allow for a twenty-foot clear width, emergency vehicle staging areas shall be designated.
- B. Staging areas shall be at least fifty feet long and twenty feet wide. Staging areas shall not be obstructed in any manner, including the parking of vehicles, or snow storage. The minimum width shall be maintained at all times.
- C. Staging areas shall be spaced so that the maximum distance from a staging area to the property line of any lot is one hundred fifty feet.
- D. Staging areas require a significant visual cue acceptable to the department of engineering services and the city fire department; signing alone is not adequate.
- E. Paired driveways and street intersections that meet the minimum dimensions may be designated as staging areas. When used, paired driveways require a deed restriction on the affected lots.
- F. Mid-block bump-outs may be allowed in areas where garage access is provided off of alleys on approval of the director of engineering. Other physical alternatives will be considered on a case by case basis and allowed at the discretion of the director of engineering services and the



city fire department.

- G. Staging areas shall not create a street maintenance or parking enforcement problem and must be approved by the director of engineering services, director of streets and the city fire department.
- H. ~~((Placement of f))~~ Fire hydrants ~~((shall consider the location of))~~ should be located within the staging areas. Fire hydrants may also be located within the median.

Section 7. That SMC section 17H.010.160 is amended to read as follows:

#### **17H.010.160 Traffic Calming**

- A. Allowable traffic calming features include traffic circles, chicanes, curb extensions, medians, entry-way treatments, landscaping, turn or access restrictions and other traffic calming features set forth in ~~((the Traffic Calming Policy for Residential Streets))~~ Chapter 3 of the Design Standards.
- B. ~~((Installation of traffic calming features on existing streets requires a public meeting and a petition representing at least fifty percent plus one of the households in the petition area. This process is outlined in the Traffic Calming Policy for Residential Streets.))~~ The City's Neighborhood Traffic Calming program provides opportunities for installing traffic calming devices on existing streets.
- C. Installation of traffic calming features ~~((on new streets))~~ through development actions will be evaluated on a case by case basis and approved by the ~~((director of engineering services and the director of streets))~~ City Engineer.
- D. All proposed traffic calming features will be evaluated based on posted speed, traffic volumes, pedestrian generators within the project area, roadway geometry, residential density and collision history as applicable.
- E. Traffic calming features shall not create a street maintenance, safety or parking enforcement problem.

Section 8. That SMC section 17H.010.180 is amended to read as follows:

#### **17H.010.180 Sidewalks**

- A. Sidewalks shall be located on both sides of the street for all public and private streets.



- B. Sidewalk shall be constructed around the bulb of cul-de-sacs so that every lot is served by a sidewalk.
- C. In steep, hillside areas, where development occurs only on one side of the street, sidewalk may be omitted from one side in accordance with SMC [17H.010.110](#). However, it must be demonstrated that the segment to be omitted is not a critical link in the sidewalk system.
- D. All sidewalks shall be designed and constructed in accordance with the City's design standards, standard plans and specifications.
- E. ~~((All sidewalks shall provide connectivity to the regional pedestrian network as shown on Map TR 1 of the City's Comprehensive Plan when the project is adjacent to a portion of that network.))~~ Shared-use pathways may be substituted for sidewalks.

Section 9. That SMC section 17H.010.190 is amended to read as follows:

#### **17H.010.190 Pedestrian Buffer Strips**

- A. Pedestrian buffer strips are required on both sides of all streets between the sidewalk and the curb. The width and type of pedestrian buffer strip for each street shall comply with the requirements of ~~((the comprehensive plan and))~~ the city's design standards.
- B. Planted strips are required on residential local access streets. A ~~((minimum three-foot wide))~~ concrete pedestrian buffer strip may be allowed in place of the planted strip for certain land uses such as churches and schools that require passenger loading and unloading, or at bus stops. These will be evaluated on a case-by-case basis and allowed at the discretion of the director of engineering services.
- C. In situations where a separation between the sidewalk and the street is constrained by topography, narrow right-of-way or existing development, a variance from this standard may be granted by the director of engineering services.
- D. In cases where sidewalk has been omitted on one side of the street, the pedestrian buffer strip may also be omitted on that side.
- E. Pedestrian buffer strips may be omitted around the bulb of cul-de-sacs.

Section 10. That SMC section 17H.010.200 is amended to read as follows:

#### **17H.010.200 Curb Ramps**



- A. At all intersections where new curbs, sidewalks or both are to be constructed, curb ramps are to be placed and constructed as shown on the standard plans. Where a ramp is built on one corner of an intersection, a ramp shall also be provided at a corresponding location on the opposite corner of the intersection.
- B. Not less than two curb ramps per lineal block shall be constructed on or near the crosswalks at intersections or other convenient locations approved by the director of engineering services. Two curb ramps are required on each corner unless utilities, topography, right-of-way or other existing conditions make two ramps infeasible.
- C. Installation of curb ramps shall also be required on existing sidewalks whenever curbing is replaced.
- D. Proposed curb ramps at locations other than intersections must be approved by the director of streets prior to construction.

Section 11. That SMC section 17H.010.210 is amended to read as follows:

#### **17H.010.210 Crosswalks**

- A. Generally, marked crosswalks are installed in centers and corridors (CC, DTC, DTG, DTS, DTU zones), adjacent to schools, parks, hospitals, churches, trail crossings and other significant pedestrian-generating facilities, at signalized intersections and at locations identified in the Pedestrian Master Plan.
- B. On arterial streets at locations identified in section A, marked crosswalks shall be installed at every intersection, on all legs accessible to pedestrians, when the street is reconstructed, resurfaced or when such crosswalks can be funded from grant or programmatic sources. Mid-block marked crosswalks may be installed on arterial streets where significant pedestrian traffic generators exist or where pedestrian conditions warrant. Exceptions to this section are allowed when engineering studies determine that a crosswalk proposed for marking does not meet nationally-recognized safety standards.
- C. Installation of marked crosswalks at locations other than those identified in subsection A requires an engineering study and the approval of the director of streets. Neighborhood councils shall be consulted and their input taken prior to installation or removal of a crosswalk.



- D. An advanced stop line shall be located in advance of each crosswalk at an arterial intersection and any mid-block crosswalk in locations defined in Section A, per the Manual on Uniform Traffic Control Devices (MUTCD).
- E. Americans with Disabilities Act (ADA) compliant curb ramps shall be installed at all newly marked crosswalks. The preferred curb ramp design shall be directional (perpendicular or parallel), as defined by American Association of State Highway and Transportation Officials (AASHTO) guidelines, where right-of-way and topography allow. Guidance per Federal Highway Administration Best Practices Design Guide shall inform curb ramp design.
- F. In the event a bus stop is planned, relocated or modified as part of the establishment of a new crosswalk or improvement thereto, the new bus stop shall meet ADA accessibility standards adopted by reference in 49 CFR 37. Any new bus stop shall not straddle or compromise a crosswalk.
- G. In centers and corridors (CC, DTC, DTG, DTS, DTU zones) on new, resurfaced, rehabilitated or reconstructed arterial intersections with three or more lanes and no traffic signal, marked crosswalks with a mid-point pedestrian refuge shall be constructed, unless in conflict with an adopted sub-area or neighborhood plan or contrary to the findings of an engineering study. Travel lane widths may be narrowed and/or the number of travel lanes may be reduced and/or additional, existing right-of-way may be utilized to accommodate pedestrian refuges. Pedestrian refuges shall be vegetated or treed or otherwise contain elements to establish a sense of place. Landscaping shall be designed and maintained to provide appropriate visibility between pedestrians and approaching motorists from both directions.
- H. Raised crosswalks may be installed in lieu of pedestrian refuges. Detectable warnings shall be included at the curb line on all raised crosswalks.
- I. The design of marked crosswalks on arterial streets shall meet guidance in the Federal Highway Administration Best Practices Design Guide, NACTO or other nationally recognized guidelines.
- J. Crosswalk markings and signs shall be maintained.
- K. Marked crosswalks shall only be removed on the recommendation of the ~~((Planning Director))~~ City Engineer, after consultation with the neighborhood council and with City Council approval, which shall be authorized by resolution.



- L. The City administration should adopt policies and guidelines to implement the provisions of this section. Such policies and guidelines shall not conflict with the provisions of this section.

Section 12. That SMC section 17H.010.220 Code is amended to read as follows:

#### **17H.010.220 Driveways**

- A. No driveway shall be located so as to create a hazard to pedestrians or motorists, or invite or compel illegal or unsafe traffic movements. The edge of the driveway at the curb shall not extend past the end of radius of the curb of an adjoining street, nor into a crosswalk.
- B. Every driveway must provide access to an off-street parking area located on private property. Every vehicle entering the driveway must be able to park, stand, or load entirely off the street right-of-way, sidewalk or pathway.
- C. Garage and carport entrances must be set back at least twenty feet from the back of sidewalk.
- D. No parking is allowed in an alley. Garages and carports may be built to the rear property line unless parking in front of the entrance is proposed, then the structure must be a minimum of eighteen feet from the edge of the alley tract, easement or right-of-way.
- E. Unless otherwise approved by the director of engineering services, the entire nominal driveway width shall be confined within lines perpendicular to the curb line and passing through the property corners. Shared driveways will be evaluated on a case by case basis.
- F. No driveway shall be constructed in such a manner as to be a hazard to any existing drainage inlet, street lighting standard, utility pole, traffic regulating device, fire hydrant, or other public facility. The cost of relocating any such public facility, when necessary to do so, shall be borne by the applicant. Relocation of any public facility shall be performed in coordination with the agency holding authority for the structure.
- G. The total nominal width of all driveways on a street for any one ownership shall not exceed forty percent of the frontage.
- H. Circular drives may be approved by the City Engineer for traffic safety purposes on residential lots with at least fifty feet of frontage on a Principal or Minor Arterial. Circular drives must be consistent with current zoning regulations((-)), although the City Engineer may approve exceptions to



these requirements. If a public alley provides paved access, a circular drive is not allowed.

- I. Any driveway which has become abandoned, unused, or unnecessary for any reason, shall be closed and the owner shall replace any such driveway with curb and sidewalk matching adjacent improvements or constructed in accordance with the standard plans and specifications.
- J. Wherever, in a single ownership, the total width of existing driveways on a street is over forty percent of the frontage of the ownership on that street, or any driveways are wider than twenty feet, such existing driveways shall be made to conform to the provisions of this section upon the alteration or repair of any one or more of the driveways. The director of engineering services or the director of streets may require such changes in any or all the driveways of that ownership as he/she may deem necessary for the better movement of traffic or to provide better protection to pedestrians.
- K. An approach permit issued by the department of engineering services is required for the construction or modification of any driveway onto a public right-of-way. Plans and an operation analysis may be required as part of the review.
- L. In new developments, an approach permit is not required when driveway locations are shown on the approved street plans and the driveway drops are constructed in conjunction with the streets. However, if a driveway is to be relocated or modified, a new approach permit must be obtained prior to construction.

Section 13. That SMC section 17H.010.230 Code is amended to read as follows:

### **17H.010.230 Street Lighting**

- A. ~~((For arterial streets,))~~ Lighting plans shall be provided to the ~~((department of engineering services))~~ Streets Department for review and acceptance prior to construction. See the City of Spokane Design Standards section on street lighting.
- B. At a minimum a street light shall be provided at every arterial intersection.
- ~~((B.))~~ C. ~~((Where street lighting is implemented on local access streets, a plan must be submitted and accepted by the director of engineering services. The lighting proposal will be reviewed for lighting type, spacing, and location.))~~ Street lights on new local access streets shall be operated and maintained by a homeowners' association if one is established.



Section 14. That SMC section 17H.010.240 Code is amended to read as follows:

#### **17H.010.240 Vertical Clearances**

- A. The clearance above any street surface shall be a minimum of sixteen and one-half feet to overhead obstacles. This height shall be maintained across the full width of the street, extending to two feet behind the face of curb.
- B. Vertical clearances for street signs above sidewalks and other pedestrian areas shall be as shown in the standard plans.
- C. Vertical clearance requirements for skywalks and private/commercial signs shall be as provided in [SMC 12.02.0462](#) and ((chapter 11.17 SMC)) [SMC 17C.240](#).

Section 15. That SMC section 17H.010.250 is amended to read as follows:

#### **17H.010.250 Horizontal Clearances**

- A. The clear horizontal ((sight distance)) view triangle at intersections shall be as provided in ((SMC 11.19.590)) [SMC 17A.020.030](#).
- B. For situations not addressed by SMC 17A.020.030, horizontal sight distance shall be as described in AASHTO "A Policy on Geometric Design of Highways and Streets", Chapter 9, section on Sight Distance.
- ((B-))C. The minimum clear zone distances are as provided in the City's Design Standards ((clear zone policy (ADMIN 0370-08-04))). The values presented in the table are minimum allowable clear zone distances. Design engineers should evaluate and provide larger clear zone distances wherever practical.

Section 16. That SMC section 17H.010.260 is amended to read as follows:

#### **17H.010.260 Bicycle Network**

- A. Bicycle facilities shall be employed where designated in the City's comprehensive plan(( and in the ~~Spokane Regional Pedestrian/Bikeway Plan~~)). Bicycle facilities include shared-use pathways, bicycle lanes including striped and protected lanes, ((paved shoulders, shared-use lanes, and residential bikeways)) shared lanes, neighborhood greenways and bike-friendly routes. See [SMC 17A.020.020](#).



B. All new bicycle facilities shall be designed in accordance with ~~((Section))~~  
Chapters ((4020)) 1515 and 1520 of the WSDOT Design Manual and the  
City's design standards.

C. ~~((Where required by the Spokane Regional Pedestrian/ Bikeway Plan,~~  
~~signing shall be provided by the project sponsor for designated bicycle~~  
~~routes.))~~ Bicycle lanes may include raised lanes, curb-separated or  
buffers.

D. The usable width for bicycle facilities is normally from face of curb to lane  
stripe, but adjustments may need to be made for drainage structures,  
parking, or other obstructions to maintain this space.

PASSED BY THE CITY COUNCIL ON \_\_\_\_\_

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/9/2020

**Clerk's File #**

ORD C35986

**Renews #****Submitting Dept**

INTEGRATED CAPITAL

**Contact Name/Phone**

IGNA NOTE 625-6331

**Contact E-Mail**

INOTE@SPOKANECITY.ORG

**Agenda Item Type**

First Reading Ordinance

**Agenda Item Name**

4250 - ORDINANCE REVISING THE DESIGN STANDARDS IN CHAPTER 3

**Cross Ref #****Project #****Bid #****Requisition #****Agenda Wording**

An ordinance revising the Design Standards Chapter 3 - Streets, Alleys, and Bikeways for the City of Spokane.

**Summary (Background)**

The City Design Standards guide and govern the development, redevelopment, and reconstruction of facilities built in the right-of-way. This transportation chapter update includes the current state of practice across the nation, with focus and reference sections that bring the design of pedestrian and bicycle facilities up to standards for better serving all ages and abilities. These standards also promote continuity and networking of the City's streets and sidewalks, as well as the

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact****Budget Account**

Select

\$

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Select

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Select

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Select

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#

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**Study Session\Other**

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# Briefing Paper

## Public Infrastructure Environment & Sustainability

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |                                                                                                                                           |  |  |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------|--|--|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | Public Works Division / Integrated Capital Management                                                                                     |  |  |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | Street Design Standards and SMC's update                                                                                                  |  |  |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 10/26/2020                                                                                                                                |  |  |
| <b>Author (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                            | <a href="mailto:inote@spokanecity.org">inote@spokanecity.org</a> , <a href="mailto:kemiller@spokanecity.org">kemiller@spokanecity.org</a> |  |  |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | CM Mumm                                                                                                                                   |  |  |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     |                                                                                                                                           |  |  |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | PIES, Urban Experience                                                                                                                    |  |  |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | <input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative             |  |  |
| <b>Alignment:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             |                                                                                                                                           |  |  |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Improving Streets                                                                                                                         |  |  |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | Adoption in 2020                                                                                                                          |  |  |
| <b>Outcome:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |                                                                                                                                           |  |  |
| <p><u>Background/History:</u> <i>The Street Design Standards update has been a work in progress for the past few years. These standards are used during engineering of streets, sidewalks, pathways and stormwater facilities within the public right of way. They are used by City Staff as well as the Development Community. Proposed changes to select sections of the Spokane Municipal Code are also available for review.</i></p> <p><i>Public outreach in late 2019 included meeting with developers, an All Ages and Abilities group, and the Plan Commission Transportation Subcommittee. Staff briefed the PIES committee on 3/23/2020 and 8/24/2020. Public outreach since March 2020 included meeting with or soliciting feedback from internal staff groups, the Bicycle Advisory Board, Design Review Board, the PCTS, and SEPA notices. Plan Commission workshops were held on 7/22/2020 and 8/12/2020. The Plan Commission Hearing started on 9/23/2020 and continued on 10/14/2020.</i></p> |                                                                                                                                           |  |  |
| <p><u>Executive Summary:</u></p> <ul style="list-style-type: none"> <li>• A draft of the Street Standards and the proposed SMC revisions are available on the project webpage <a href="https://my.spokanecity.org/projects/street-design-standards-update/">https://my.spokanecity.org/projects/street-design-standards-update/</a></li> <li>• 9/23/2020 and 10/14/2020: Plan Commission Hearing</li> <li>• Nov-Dec 2020: City Council workshops and hearings.</li> <li>• Changes since the last PIES presentation: expanded use of 32' street width for low density residential, revised curb ramp language, and sidewalk widths</li> </ul>                                                                                                                                                                                                                                                                                                                                                                  |                                                                                                                                           |  |  |
| <p><u>Budget Impact:</u></p> <p>Approved in current year budget? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If new, specify funding source:</p> <p>Other budget impacts: (revenue generating, match requirements, etc.)</p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   |                                                                                                                                           |  |  |
| <p><u>Operations Impact:</u></p> <p>Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>Specify changes required:</p> <p>Known challenges/barriers:</p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                            |                                                                                                                                           |  |  |



ORDINANCE NO.C35986

AN ORDINANCE revising the Design Standards Chapter 3 – Streets, Alleys, and Bikeways for the City of Spokane.

WHEREAS, the City of Spokane adopted the Design Standards in 2007 along with revisions in 2009 and 2010; and

WHEREAS, the City completed an update of the Comprehensive Plan Transportation Chapter in 2017 which recommended revisions to these standards; and

WHEREAS, City staff from Planning, Developer Services, Engineering, Streets and Integrated Capital Management have worked together to develop this document; and

WHEREAS, the Clear Zone standards adopted in Policy No. 0370-08-04 have been revised and incorporated into the updated Design Standards Chapter 3; and

WHEREAS, on August 31, 2020 the responsible official issued a determination of non-significance (DNS) under SEPA (Chapter 43.21C RCW) relating to the proposed changes and notice of said DNS was published with the Plan Commission hearing notice in the Spokesman Review on September 9, 2020 and September 16, 2020; and

WHEREAS, the Plan Commission held workshops on the Design Standards update on July 22, 2020 and August 12, 2020, and a public hearing starting on September 9, 2020 and continuing on October 14, 2020; and

WHEREAS, based on written and verbal testimony that is part of the record and is summarized in the City Plan Commission Recommendation, Findings and Conclusions signed on November 3, 2020, the Plan Commission recommended that the City Council adopt the proposed Design Standards; -

Now Therefore,

BE IT RESOLVED by the city council for the City of Spokane that it hereby adopts the attached Design Standards for Streets, Alleys, Bikeways and Sidewalks and the standards shall become effective immediately upon passage of this resolution; and

BE IT RESOLVED by the city council for the City of Spokane that the Clear Zone Policy No. 0370-08-04 included in Appendix F of the Design Standards is hereby rescinded.



PASSED BY THE CITY COUNCIL ON \_\_\_\_\_

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date



## City of Spokane Design Standards

Nov 1, 2020

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**Design Standards**  
City of Spokane

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## STREETS, ALLEYS, BIKEWAYS, AND SIDEWALKS

### 3.0 Preface

The City of Spokane's adopted infrastructure standards require the design of integrated systems. For streets, this means not only that the full network of streets will function as a system, but that the other systems the streets intersect (transit, bike, emergency response, stormwater) will be seamlessly integrated.

Each section of the standards begins with a brief definition of the topic followed by the design standard.

The following key points guide this document.

- Street design is an iterative process, that entails flexibility and trade-offs. Within the built environment, particularly, physical space might limit what or how specific elements fit together to deliver a desired contextual experience. Decisions may be pushed by regulation, physical constraints, or public opinion, but ultimately should be guided by planned strategies and/or engineering standards.
- Balance is important. To maximize one component often means less achievement of another (prioritizing vehicle speeds often degrades conditions for people on foot and bike). The proper balance will vary from place to place in the city.
- Successful design will reflect community priorities, as defined through public outreach activities at planning, scoping, and design levels. Often, achieving a design that balances scope delivery, while accomplishing community goals will require compromise.
- The ultimate focus of street design should be how the street feels for users (drivers, pedestrians, shoppers, transit riders, residents, schoolchildren, etc.) on the ground level, and the desires of the city and community for how they want the space to function.

Transportation facility design will generally be based as either: new infrastructure built to facilitate development activities, or modification of pre-existing infrastructure. Development of new infrastructure will be held to the requirements here-in to deliver the most appropriate conditions to provide travel through the various urban conditions. Often the existing built environment does not adhere to today's standards, which have been updated over the years to reflect best practices. Thus, preservation or reconstruction work will often require more balancing of priority, and will necessarily vary from the standards due to limitations of space or inadequate meshing of facilities. New Development, Preservation, and Reconstruction work are defined in section 3.1 Definitions to provide guidance for the conditions wherein variance from the standards may be acceptable.



## 3.1 Definitions

**AASHTO** See Section 1.2

**ADA** See Section 1.2

**ADAAG** See Section 1.2

**Alley** A public or private way designed or intended to provide access to abutting properties. Alleys are generally not intended for through vehicle movements, but are useful to providing access to businesses and residences, and in some locations bicycle and pedestrian improvements.

**Arterial** See *Principal Arterial*, *Minor Arterial*, or *Collector Arterial*.

**Bicycle Facilities** Facilities designated for use by commuter and recreational bicyclists. The following types of bicycle facilities are identified in the Master Bicycle Plan:

- Neighborhood Greenway
- Shared Lane
- Bike Lane
- Bike-Friendly Route
- Shared Use Path
- Soft Surface Path (unpaved)

**Buffer Strips** Hard surfaced or landscaped areas between travel or parking lanes and sidewalks, also called Planting Strips.

**Center Crown** A roadway cross-section with the highest point of the *traveled way* located at the center of the road.

**City Engineer** Individual overseeing all operations and functions regulated by this title, subject to the authority of the Mayor. See SMC 13.01.0202

**Clear View Triangle** The corner area at an intersection or driveway which must be free of obstructions to provide adequate sight distance. See SMC 17A.020.030N for dimensions.

**Clear Zone** The roadside area free of obstacles, starting at the edge of the traveled way.

**Collector Arterial** A street providing access and circulation in lower-density residential and commercial/industrial areas. *Collector arterials (consisting of FHWA classifications Urban Major Collector and Urban Minor Collector)* collect and distribute traffic from *local access streets* to *principal* and *minor arterials*. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Cross Slope** A slope that is perpendicular to the direction of travel.

**Crown (Roadway Crown)** The term used to define the highest point of the *traveled way* on a roadway cross-section. The City recognizes three types of roadway sections to facilitate drainage: *center crown*, *quarter crown* and *curb crown*, which are defined herein.

**Curb Crown** A roadway cross-section with the highest point of the *traveled way* located at one curb.



**Curb Ramp** A ramp constructed in the sidewalk to provide an accessible route from the sidewalk to the street.

**Entrance Gate Queuing Area** A length of street on the public side of an entrance gate that allows vehicles to exit the connecting street prior to the gate.

**Driveway** A cement concrete driveway structure as shown in the Standard Plans.

**Fire Lane** A road or other passageway developed to allow the passage of emergency vehicles. A fire lane is not necessarily intended for general vehicular traffic usage. Refer to SMC 17F.080 Appendix D for dimensions and requirements.

**Integral Curb and Gutter** Concrete curb and gutter which is formed and placed as one unit.

**Local Access Street** A street that provides access from individual properties to *collector arterials* and *minor arterials* in residential, commercial and light industrial areas. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Median** A painted or raised traffic island used to channel, separate and otherwise control vehicular traffic.

**Minor Arterial** A street providing service for trips of moderate length, connecting the principal arterial system and providing intra-community circulation. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Monument** A physical survey monument as shown in the City's Standard Plans.

**MUTCD** See Section 1.2 and SMC 17A.020.130.

**NACTO** Refers to the National Association of City Transportation Officials.

**Neighborhood Greenway** A low-volume street that is designed to prioritize pedestrian and bicycle travel. Most often greenways will be implemented on local access streets, and elements of the greenway may disrupt through-travel by automobile as a means of regulating vehicle volume. Greenways are best implemented near and parallel to an arterial street which boasts access to goods and services, thus also providing ready access to users of the greenway. Another crucial element of a greenway is signage that identifies the route as a greenway and provides wayfinding.

**New Development** Development or redevelopment of land adjacent to (and often inclusive of a portion of) the Right of Way, or development of land with the intent of dedicating Right of Way infrastructure. Most private development falls under this category, and occasionally the City of Spokane will develop new streets through vacant or underdeveloped land.

**Path** Facility designed for use by bicyclists and pedestrians, usually separated from vehicle traffic by a median or landscaped area.

**Place-making** An element of streetscaping that involves the use of unique design features with the ability to set a street segment apart, helping to create an environment for economic vitality and innovation. Application of place-making design elements should be used in connection with planned land uses and in coordination with stakeholders.

**Preservation** A roadway maintenance project conducted by the City of Spokane to refresh the driving surface of the street and thus prolong the pavement service life. These projects are



generally confined to the pavement area between curbs. Example treatments may include grind and overlay, chipseal, micro-seal, slurry seal, crack seal, etc.

**Principal Arterial** A street serving major activity centers and providing a high degree of mobility. Refer to the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements for additional discussion.

**Private Streets** Roadways which are not controlled or maintained by a public authority, and which serve two or more properties.

**PROWAG** Refers to the Public Right-of-Way Accessibility Guidelines.

**Quarter Crown** A roadway cross-section with the highest point of the *traveled way* located at a distance from one curb of one-fourth the roadway width (as measured from face of curb to face of curb).

**Reconstruction** A roadway corridor project that typically replaces the full depth of asphalt pavement, updates curb ramps, and may include utility updates as appropriate. Sidewalk repair, replacement, capacity improvements, signal and lighting upgrades and transit stop improvements may also be included in a reconstruction project. These projects are administered by the City of Spokane, and the scope of each project is determined in accordance with city plans. As this type of work is done within the built environment, space constraints may impede the full realization of the design standards. Prioritization of standards is generally addressed within this document, but each individual project will need to be scoped with future use conditions in mind.

**Shared-Use Pathway** A non-motorized transportation pathway shared by pedestrians, scooters and bicyclists. May be located next to a street or in a separate right-of-way. Examples include the Children of the Sun Trail, Ben Burr Trail, Fish Lake Trail and Centennial Trail.

**Street Classifications** In conformance with FHWA guidance, arterial and local access streets are classified in the Auto Network portion of the City's Comprehensive Plan chapter 4, section 4.4 Modal Elements section as follows:

- Principal Arterial
- Minor Arterial
- Collector Arterial (Major Collector or Minor Collector)
- Local Access Street

Definitions of all of the above classifications are included herein. Private streets are not classified.

**Street Character** Character consists of refined street definition based upon a street's function within the transportation network (or classification) and its context (land use zoning).

**Street Realm** A part of the right-of-way designed for a particular user group or use (pedestrian, flexible zone, vehicle, median). See Figure 2.

**Streetscape or Streetscaping** The combinations of living and non-living items that provide opportunities for place-making. Generally everything beyond the asphalt makes up the streetscape, although the median may include streetscaping elements.

**Structural Sidewalks** Structural sidewalks shall be defined as all elevated slabs, grates, and panels located within a sidewalk or driveway not supported on grade. Typical examples of elevated structural sidewalks are concrete slabs, steel grates, and steel plates for utility vault lids, service elevator covers, utility covers, and building basements.



**Traveled Way** The area of roadway which is intended to carry vehicular traffic, not including any shoulders. See SMC 17A.02.200.

## 3.2 Street Character

Street design is governed by two primary factors: zoning context and classification. Zoning context refers to the environment (land use zoning) in which a street is found. For example, sidewalks must be wider on downtown streets to accommodate higher pedestrian volumes and place-making elements. Street classification speaks to its function within the network, an arterial street with planned bicycle facilities will be built with the facilities the full length of the street regardless of how the facilities might change due to zoning the street passes through. Street Character is defined by the combination of zoning and classification. A principal arterial should have a different character through a CC zone than through a Residential zone.

### 3.2-1 Street Zoning Application

Spokane's comprehensive plan refers to urban infrastructure contexts for the city. This refers to the land use zoning through which a street traverses and to which the street facilities provide access. Land Use Zoning is defined in Title 17C "Land Use Standards" of the Spokane Municipal Code. Zoning is applied and defined for each land parcel in the city. Streets themselves are not assigned specific zoning, but should take on the context of zoning they front.

Zoning can, for the purposes of selecting street design characteristics, be lumped into four categories: Centers and Corridors, Downtown/Commercial, Residential, and Industrial. While zoning might change multiple times along a given block, some street characteristics will necessarily remain constant. Design criteria should be selected for the most generous zoning on a given block, and should be applied block by block. In some instances a street may traverse a different zoning for only one or two blocks, and best judgement should be applied as to whether to shift the street character in such instances. Emphases should be given to place-making opportunities when considering these shifts in street character.

Some consideration should be given to the planned versus the existing land use. The Zoning code allows for a variety of uses within several of the zoning contexts. For instance, the zoning for Centers and Corridors, CC1 allows for commercial, office, or residential development. When developing the street serving a planned development, or when rehabilitating a street within the built environment, it is worth considering what land use is to be expected for the life-span of the roadway, or about 20 years.

Motor vehicle volume (Average Daily Traffic – ADT) on a given street should be a strong determinant when considering how the facilities of the street fit together to provide appropriate levels of safety and provision to all users of the street. The street classification is largely determined by existing and planned traffic volume as well as the percentage of freight traffic on the street, and combined with the street type derived from the Land Use Zoning, provides the basis for design expectations for a given street.

Spokane exhibits four street classifications:

- Principal Arterial – Spokane's largest streets that provide regional connections and serve the highest volumes of traffic.
- Minor Arterials – Similar in design to Principal Arterials, Minor Arterials typically have fewer lanes and connect Collectors to Principal Arterials.

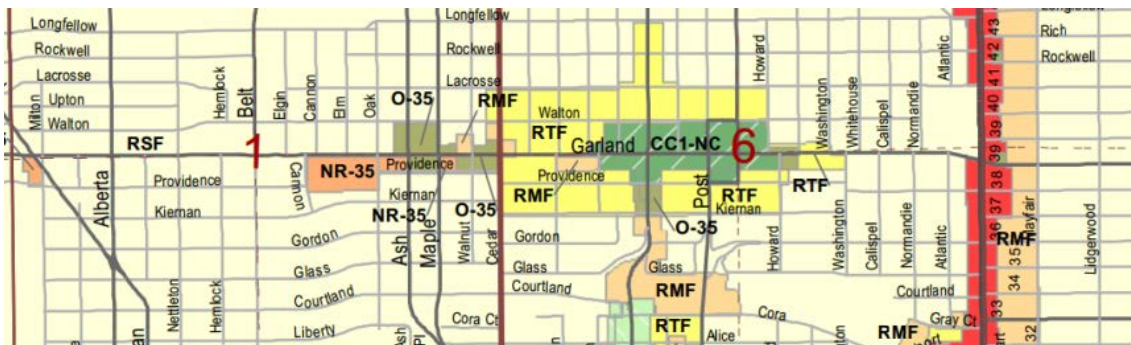


- Collector (Major and Minor) – Streets that circulate through neighborhood hubs and connect to minor and principal arterials. Collector streets are further defined as Major and Minor Collectors depending on traffic volume, but for the purposes of design, these will be treated under the same criterion.
- Local Access – Low-volume and low-speed urban streets providing access to homes and businesses.

In combination, the zoning contexts and street classifications result in sixteen overall street characterizations for Spokane. Street character, identified at the start of a project is the basis for this design standard, and sets the starting point for decision-making balance through the design process.

Street design for a given street should change with the context. For example, Garland Avenue's zoning changes several times from Alberta to Division, as depicted in Figure 1. Cross sectional design elements for the CC1-NC zone will be selected differently than for the RSF zone. Consult the zoning maps when beginning a street improvement project to understand context changes along a corridor that may warrant design adjustments from one stretch of roadway to the next.

Figure 1 – Zoning map (full map available at <https://my.spokanecity.org/opendata/gis/>)



Industrial route streets serve the areas where industrial zoning is assigned. Freight routes, as planned for traversing the city, may also be considered Industrial despite other zoning such streets traverse. Due to the high percentage of larger commercial trucks, vehicle lanes are typically wider (11 to 12 feet) to provide sufficient space, which is most important approaching intersections where truck lane changes and turn movements require wider geometric layouts than passenger vehicles. These streets require special attention to factors such as pedestrian crossings, pedestrian visibility, and bicycle facility design to ensure corridors may balance industrial needs and multi-modal functions, particularly where industrial land uses are co-existent with pedestrian-generating facilities.

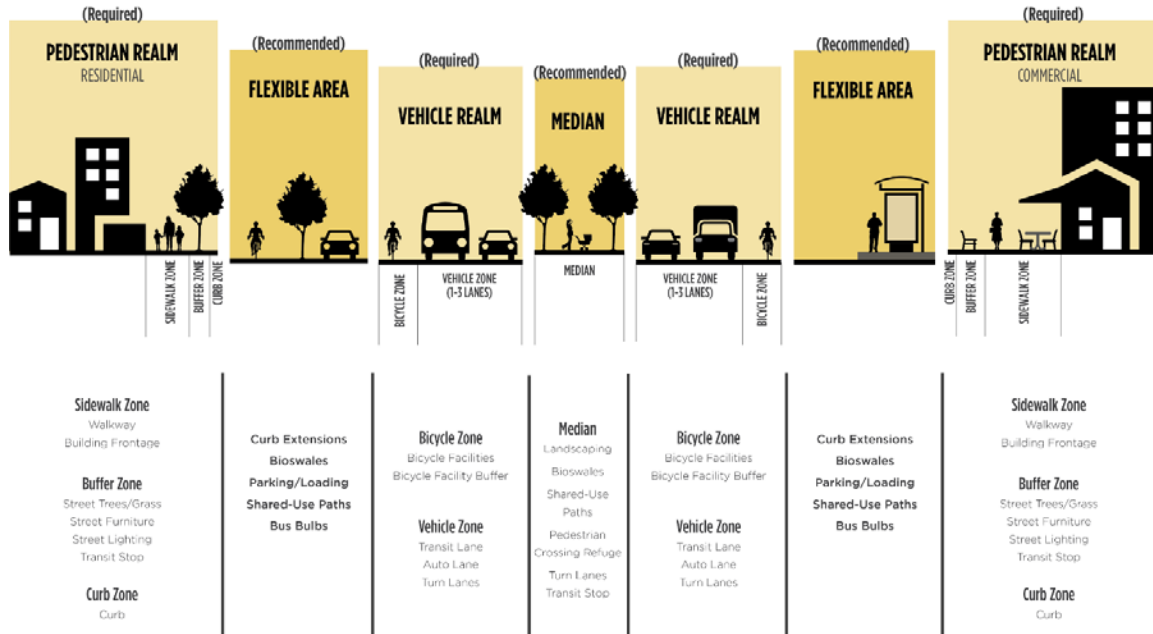
### 3.2-2 Street Realms and Zones

The cross section of a street includes some elements that are standard to all streets and others that are recommended for certain street character. Within the overarching street areas (Pedestrian Realm, Flexible Area, Vehicle Realm, Median) various elements can be arranged to provide a high-quality street depending on the needs of a given area. By thinking of streets in zones, designers ensure multimodal outcomes by considering all needs in relation to land use zoning context. All Spokane streets must have sidewalks, for example, which fall under a



“required” zone, whereas additional elements such as curb extensions or medians can only be built if enough room exists after placing the required elements.

Figure 2 – Street Realms and Zones



The **Pedestrian Realm** includes the area from the property line or building front to the curb and is made up of three primary zones: the sidewalk zone, the buffer zone, and the curb zone, as defined below.

- **Sidewalk Zone.** The sidewalk zone is the area dedicated to pedestrian travel between the buffer zone and the property line. A minimum of 5 to 8 feet of concrete surfacing must be built as defined in the Land Use Zoning. ADA standards also dictate minimum dimensions to be kept clear of obstacles and protruding objects and provide a direct connection along pedestrian access routes. Vending tables, sidewalk cafes, or other activities that protrude into the through-walking space must conform to SMC Section 17C for minimum through-way requirements for the applicable Land Use Zone. In addition to the pedestrian walkway, the sidewalk zone also includes the building frontage wherein could be located vending tables, sidewalk cafes and various street furnishings.
- **Buffer Zone.** The buffer zone is located between the curb and sidewalk zone. This area can be paved or planted, depending on the street character. It may include street trees, parking meters, planters, rain gardens, bioretention swales (overlapping into flexible area), bus shelters, utility poles and boxes, lamp posts, traffic signs and signals, bike racks, news racks and stands, waste receptacles, street furniture and drinking fountains. In addition to the curb zone, the buffer zone provides a buffer for pedestrians from the adjacent roadway and can accommodate snow storage in the winter. Vegetation in this area will generally be maintained by the adjacent property owner, except in the case that such serves a stormwater management purpose. In that case, the city will often maintain vegetation.
- **Curb Zone.** The curb zone is a continuation of the sidewalk elevation plane, typically lies between the traveled way and the buffer zone, and typically consists of 6-inch-wide



elements; although wider elements like bicycle parking or riding facilities are sometimes included. The curb zone will commonly be incorporated into the flexible area for curb extensions or raised cycle tracks, for example. It provides space to open a car door, for vehicle overhangs and for pedestrians to wait for taxis or buses. For those with visual impairments, the curb indicates the border between the sidewalk and the roadway. The curb zone should be free of all objects, furniture, sign posts etc.; particularly adjacent to on-street parking.

**Flexible Area (optional).** This space between the vehicle realm – where vehicles and bicyclists move – and the curb zone can be programmed for car parking, bike parking, landscaping, stormwater management (general overlap with buffer zone), pavement-level protected bike lanes, shared-use paths, bus bulbs, or curb extensions. Shy space, a distance commonly required on the right side of a vehicle to allow for driver deviation near curbs is also part of this area. Not all streets have enough space for both required and optional elements.

**Vehicle Realm.** This area has two zones:

- **Bicycle Zone.** Consult the Master Bicycle Plan and Section 3.5 to determine the type of facility and design desired. Depending on the street character, this zone may include shared lane markings, a lane, a buffer between the lane and vehicles, or other components. In some cases the bicycle facilities will be placed in the Flexible Area, such as in the case of a multi-use path or parking-protected bike lanes.
- **Vehicle Zone.** Auto or transit vehicle lanes are included in this zone, including the outer travel lane, inner travel lane(s), and optional Two-Way Left Turn Lane (TWLTL).





**Median.** Medians calm traffic, provide refuge for pedestrians crossing the street (especially along wider streets), and present opportunities for landscaping, streetscaping, stormwater management and transit stops. Medians can be used midblock in tandem with turn lanes at intersections. Similar to the Flexible Area, not all streets need medians, and when medians are considered, access to utility access or controls, left turns, alley access, etc. should be maintained where appropriate. Based upon available right-of-way and community input, a menu of options can exist in a median. Pedestrian refuge medians should be installed in accordance with SMC 17H.010.210 and SMC 17H.010.215.

**Dimensions in Table 1.** Flexibility in street design may be maintained by referencing a range of possible dimensions rather than prescribing exact requirements. A design, may thus be crafted based upon the unique elements of each street. Street design, particularly within the built environment, requires a range of possible elements and dimensions in order to deliver desired outcomes. Table 1 lays out the target dimensions for street zone elements by street classification and zoning contexts.

Wider sidewalks, buffer zones, swales and medians are allowed without a deviation. Shared-use path width may be decreased to 10' or increased without a deviation. Deviations beyond these standards must be approved by the City Engineer per SMC 17H.010.020.



Table 1 Street Dimensions

| Table 1 Street Dimensions   |                                                                                    |                            |           |                                                                                     |                                    |                                         |         |                                                                                     |                                      |                                        |                                      |                                                                                     |                     |
|-----------------------------|------------------------------------------------------------------------------------|----------------------------|-----------|-------------------------------------------------------------------------------------|------------------------------------|-----------------------------------------|---------|-------------------------------------------------------------------------------------|--------------------------------------|----------------------------------------|--------------------------------------|-------------------------------------------------------------------------------------|---------------------|
|                             | (Required)<br>PEDESTRIAN REALM                                                     |                            |           | (Recommended)<br>FLEXIBLE AREA <sup>e</sup>                                         |                                    |                                         |         | (Required)<br>VEHICLE REALM                                                         |                                      |                                        |                                      | (Recommended)<br>MEDIAN                                                             |                     |
|                             |  |                            |           |  |                                    |                                         |         |  |                                      |                                        |                                      |  |                     |
|                             | Sidewalk Zone <sup>b</sup>                                                         | Buffer Zone <sup>c,d</sup> | Curb Zone | Opt. Shared Use Path <sup>f,g</sup>                                                 | Stormwater Management <sup>h</sup> | Curb Extensions/ Bus Bulbs <sup>i</sup> | Parking | Bicycle Lane <sup>a</sup>                                                           | Bicycle Buffer <sup>j</sup>          | Vehicle Zone Outer Lane <sup>k,l</sup> | Vehicle Zone Inner Lane <sup>k</sup> | Vehicle Zone Left Turn or TWLTL                                                     | Median <sup>m</sup> |
|                             | Downtown DTC, DTG, DTU, DTS; Form Based Code CA1, CA2, CA3, CA4                    |                            |           |                                                                                     |                                    |                                         |         |                                                                                     | Center & Corridor CC1, CC2, CC3, CC4 |                                        |                                      |                                                                                     |                     |
| Urban Principal Arterial    | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | Varies                             | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 11                                                                                  | 6-20                |
| Urban Minor Arterial        | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | Varies                             | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 11                                                                                  | 6-20                |
| Urban Major/Minor Collector | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | Varies                             | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 10                                                                                  | 6-20                |
| Urban Local Access          | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | Varies                             | NA                                      | 7       | 6                                                                                   | NA                                   | 10                                     | NA                                   | NA                                                                                  | 6-20                |
|                             | Commercial O, OR, NR, NMU, CB, GC                                                  |                            |           |                                                                                     |                                    |                                         |         |                                                                                     |                                      |                                        |                                      |                                                                                     |                     |
| Urban Principal Arterial    | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | 10                                 | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 11                                                                                  | 6-20                |
| Urban Minor Arterial        | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | 10                                 | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 11                                                                                  | 6-20                |
| Urban Major/Minor Collector | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | 10                                 | 7                                       | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 10                                                                                  | 6-20                |
| Urban Local Access          | 7                                                                                  | 5                          | 0.5       | 12                                                                                  | 6.5                                | NA                                      | 7       | 6                                                                                   | NA                                   | 10                                     | NA                                   | NA                                                                                  | 6-20                |
|                             | Residential RA, RSF, RSF-C, RTF, RMF, RHD                                          |                            |           |                                                                                     |                                    |                                         |         |                                                                                     |                                      |                                        |                                      |                                                                                     |                     |
| Urban Principal Arterial    | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 10                                                                                  | 6-20                |
| Urban Minor Arterial        | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 10                                                                                  | 6-20                |
| Urban Major/Minor Collector | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | 8       | 6                                                                                   | 1.5-6'                               | 11                                     | 11                                   | 10                                                                                  | 6-20                |
| Urban Local Access          | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 6.5                                | NA                                      | 7       | 6                                                                                   | NA                                   | 10                                     | NA                                   | NA                                                                                  | 6-20                |
|                             | Industrial LI, HI, PI                                                              |                            |           |                                                                                     |                                    |                                         |         |                                                                                     |                                      |                                        |                                      |                                                                                     |                     |
| Urban Principal Arterial    | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | NA      | 6                                                                                   | 3                                    | 12                                     | 12                                   | 12                                                                                  | 6-20                |
| Urban Minor Arterial        | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | 8       | 6                                                                                   | 3                                    | 12                                     | 12                                   | 12                                                                                  | 6-20                |
| Urban Major/Minor Collector | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 10                                 | NA                                      | 8       | 6                                                                                   | 1.5-6'                               | 12                                     | 12                                   | 12                                                                                  | 6-20                |
| Urban Local Access          | 5                                                                                  | 6                          | 0.5       | 12                                                                                  | 6.5                                | NA                                      | 7       | 6                                                                                   | NA                                   | 11                                     | NA                                   | NA                                                                                  | 6-20                |

A. Table 1 dimensions are target values and minor adjustments are allowed to fit the street context. See discussion in section 3.2-2 for further detail.

B. See SMC 17H.010 for exceptions to residential sidewalk requirements. In locations where existing sidewalks exceed the dimension in Table 1, the sidewalk width should be maintained with redevelopment or street improvement.

C. Per SMC 17C.200.050-1, a tree-planted continuous buffer requires a 5-foot minimum width for commercial zones. For residential and industrial zones, the minimum increases to 6 feet. Alternatively, a narrower buffer may be used in select zones if tree vaults are implemented.

D. Buffers in commercial areas may be planted or concrete. When stormwater disposal is a governing concern, consideration should be given to use pervious surfaces.

E. The flexible area includes a menu of options which are chosen based on what makes most sense according to city plans, environmental responsibilities, and context. In some cases, none of these will fit within the project. Only in very rare cases will more than one fit - for instance, a parking lane plus bio-retention swale.

F. In places designated for shared-use paths, the path can take the place of the sidewalk zone.

G. Consult section 3.5 of this document for guidance on facility type and selection. Possible facilities include bike lanes, buffered bike lanes and parking protected bikes lanes. Physical or grade-separation may be preferred depending on conditions. Bicycle facilities may operate in the Flexible Area or the Vehicle Realm. Bicycle boulevards and shared roadways are possibilities on local access street.

H. Consult the Spokane Regional Stormwater Manual and/or Eastern Washington Low Impact Development Guidance Manual for desired locations for stormwater facilities. The stormwater catchment area must meet the required volume generated by the planned impervious area. In Downtown, Form Based Code or Center & Corridor zoning roadside swales are less common and alternative stormwater facilities in accordance with the above identified manuals may be considered or stormwater piped to another location.

I. On transit corridors, use bus bulbs if space allows to ease boarding, reduce sidewalk congestion, and allow buses to easily re-enter traffic. This should typically be done only if there is a second lane for vehicles to continue around stopped buses.

J. "High Traffic" and "Medium Traffic" lane routes on the Master Bicycle Plan should include buffers. Separation buffer between bike lane and vehicle lane should be implemented via parallel lane edge stripes with a periodic cross-hatch. 3' is the minimum buffer unless a raised curb is used, in which case 1.5' is the minimum. Wider buffers are allowable but should be well marked with hatching or bollards.

K. When constraints are prohibitive, consider 10-foot lane width as the minimum.

L. Travel lane includes the width of the gutter pan, if integral curb and gutter is used.

M. Medians less than 6 feet wide are considered traffic channelization. A pedestrian refuge is a raised median with a minimum width of 6 feet. Wider medians may be implemented in the context of boulevards.



### 3.2-3 Place-Making Elements

According to the Project for Public Spaces, place-making facilitates creative patterns of use, paying particular attention to the physical, cultural, and social identities that define a place and support its ongoing evolution. Key to a successful place-making effort, is an associated community-based participation which helps identify a location's assets, inspiration, and potential to contribute to people's health, happiness, and well-being. This public participation also builds coalitions that will help care for the physical components of place-making, as well as assist in programming events held in such places.

As regards streetscaping, place-making involves the use of "unique design features that have the ability to set a street or segment of a street apart, helping to create an environment for economic vitality and innovation. Application of place-making design elements should be used in connection with planned land uses and in coordination with stakeholders." (Spokane Comprehensive Plan). This can occur through a number of planning efforts, including sub-area planning, neighborhood planning, and staff-level or board-level design review.

Capital Street projects have a unique opportunity to enhance place-making within the right-of-way. Examples of place-making treatments are provided below.

- Use of historic sidewalk patterns and stamping street names into the concrete.
- Preserving historic brick patterns in the gutter.
- Use of neighborhood specific tree grates and manhole covers.
- Re-use of historic granite curbing.
- Decorative lighting fixtures per the districts and standard types outlined in this document.
- Installation of benches, historic plaques, artwork, planter boxes, etc.
- Establishment of scenic overlooks.
- Trees and other plantings in the buffer strip or center median.
- Installation of street furnishing such as benches, bike racks, custom trash receptacles and media corrals.
- Bulb-outs at intersections or crosswalks
- Architectural features such as balconies, marquees, or arcades that may project out into the right-of-way (subject to appropriate clearances)
- Parklets and/or streateries

Other than potential landscape or hardscape improvements in a median, place-making treatments would generally be restricted to Pedestrian Realms, Alleyways, and Flexible Areas. Any place-making treatments in the Vehicle Realm (e.g. custom lighting or artwork on Skyways) must meet the other provisions of this document.

## 3.3 Right of Way

Follow the guidelines of SMC 17H.010.050 to determine minimum dimensions required for right of way for new development. Preservation and reconstruction work will often seek a balance of uses due to limited available space. Such balance should be determined based on land use context and right of way available.

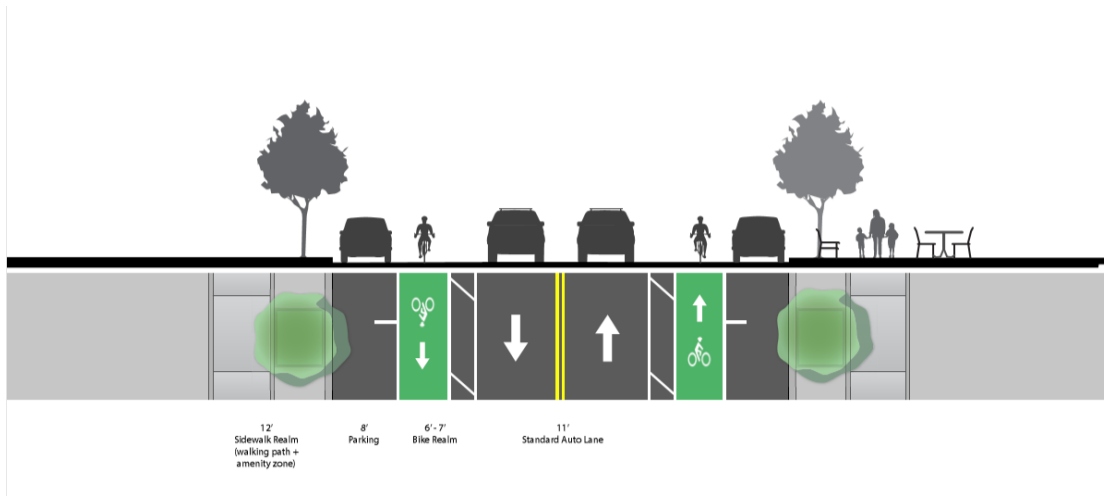
Narrower right of way widths may be allowed in new development only at the discretion of the City Engineer. Variance requests will be evaluated based on topography, traffic circulation,



emergency vehicle access, zoning, utilities, existing development and on-street parking requirements.

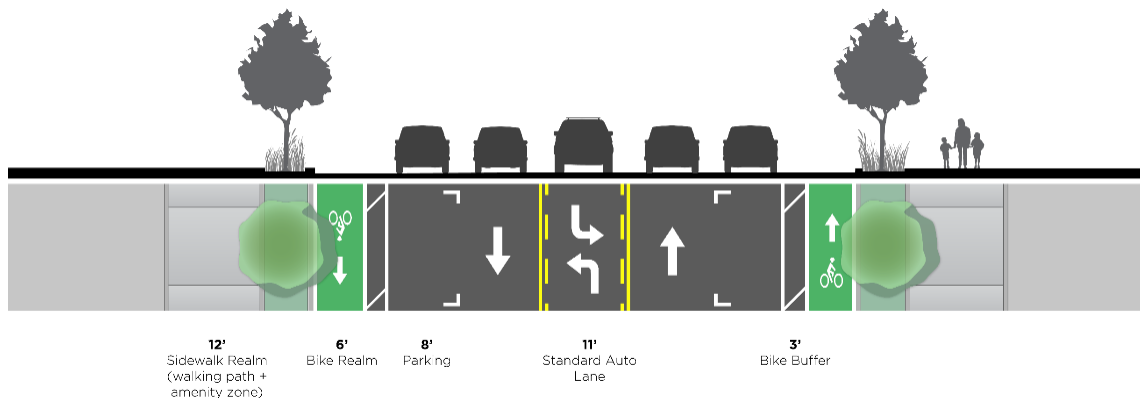
Application of Table 1 to a new and existing right-of-way is illustrated below. In some cases, the designer will be laying out a new street (Figure 3).

Figure 3 – Sample layout of an Urban Collector



In retrofit situations, Minor Arterials built to the city's earlier standards can have space reallocated based upon current pedestrian, bicycle, stormwater, transit, and/or other plans (Figure 4).

Figure 4 –Reallocation of space on example Urban Collector/Minor Arterials



In alignment with city goals (from the Transportation Chapter of the Comprehensive plan) TR A: Promote a Sense of Place and TR B: Provide Transportation Choices to achieve a balanced, multimodal transportation approach (emphasis on walking, biking, transit) street space must be reallocated if possible to users aside from drivers. FHWA has published guidelines for when a road can be downsized to three lanes (two through lanes and a center turn lane). Roads with 10,000 ADT or less are considered great candidates for a road diet. Roads with 10,000-15,000 ADT are good candidates in many instances, but agencies should conduct intersection analyses and consider signal retiming with implementation. Roads with 15,000 -20,000 ADT may be good candidates but agencies should first conduct a corridor analysis. Excess vehicle lanes can be allocated to parking, landscaping, stormwater facilities, bicycle facilities, or widened sidewalks.



When undertaking a repaving or reconstruction project on multi-lane streets with ADT of 20,000 or less, designers must undertake a traffic analysis and consider reconfiguring traffic.

## 3.4 Pedestrian Realm Facilities

### 3.4-1 Sidewalks

Sidewalks are the basic element of walkability, and can be augmented with planted buffer strips, center medians, and street furniture. The sidewalk zone includes both the area in front of a building where cafes or vendors might operate as well as the area for walking through. Ensure that for land uses where cafes and other active frontage uses are expected, appropriate unobstructed sidewalk width for walking is maintained per SMC 10.28.

Sidewalks shall be located as required by SMC 17H.010.180. Width and profile grade design criteria for sidewalks are outlined in Table 1 and Table 8. Sidewalks shall be designed in accordance with the Standard Plans and City of Spokane GSPs, and may use pervious concrete where feasible (SMC 17C.110.410, 17C.120.230, 17C.130.230).

Shared-use pathways may be substituted for sidewalks. This will typically occur in locations designated as shared-use paths on the Bicycle Plan, but other locations may be identified through the development permitting process or through a capital project design process.

Where existing, elevated structural sidewalks (vaulted over building basement spaces) are intended to be kept, they shall be designed in accordance with the applicable portions of the latest edition of the Uniform Building Code. The minimum concentrated load, L, to be used in the design shall be 10,000 pounds applied over a contact area of 100 square inches. The minimum single axle load shall be 20,000 pounds. The design tire load shall be 600 pounds per inch of tire width. The construction of new buildings with open space under the sidewalk shall not be allowed, nor shall private utilities for said buildings be placed under the sidewalk.

When development occurs on sites with existing sidewalks; broken, heaved, or delaminated sidewalk adjacent to the project shall be repaired or replaced as part of the project. Locations of sidewalk repair or replacement shall be included on plans submitted to Developer Services for review.

Reconstruction projects, where funding sources allow, should also consider sidewalk condition and completeness. Existing sidewalk width may fall short of the current standard. Consideration for widening will be a decision during the scoping phase while funding is gathered. Preservation projects is not required to adjust sidewalk width or condition of sidewalk parallel to the roadway, but grind and overlays are required to attend to ADA compliance updates at street crossings, in accordance with federal regulations.

Pedestrian detours must be planned and implemented whenever work reduces the through-walking path below acceptable ADA standards. Temporary sidewalk, when necessary, may displace vehicle parking or travel lanes, as appropriate, in order to provide a walking path detour for high-use sidewalks.

### 3.4-2 Buffer Zone

Buffer strips (separated sidewalk) can add greenery to a street, provide snow storage space, and provide horizontal separation for pedestrians from vehicle traffic. Buffer Strips can be hard surfaced or planted depending on the land-use zoning. The requirements for buffer strips are



included in SMC 17H.010.190, which requires buffer strips on both sides of all streets; SMC 17C.200.050, which guides dimensional requirements for incorporating street trees; and Table 1 which compiles the dimensional requirements from each land use zoning as defined in SMC 17C.

Reconstruction work should include pedestrian buffer strips where space allows. However, space limitations may determine availability of this option. Roadway narrowing may be considered when conditions allow, to create the necessary space for pedestrian buffers. Refer to the Pedestrian Master Plan, and prioritize buffers particularly for projects within pedestrian priority areas. Even creating this condition on one side is preferable to neither side. When creating a buffer on one side, take into account the continuity of pedestrian travel and likely destinations like schools, markets or community facilities. Street maintenance activities (non-capital) are not required to consider linear elements beyond the curblines unless attending to ADA or utility items.

### **3.4-3 Curbs and Gutters**

Integral cement concrete curb and gutter shall be constructed per the City standard plan on roadways with profile grades below 1.0 percent. Special drainage issues may allow the use of alternative curb profiles depending upon road profile and setting, upon approval of the City Engineer. When repairing or replacing existing sections of curb, the type of curb constructed may match the adjacent curb.

The curb radius at alley entrances is addressed in the City's Standard Plans.

Consider curb extensions (bulb-outs or bumpouts) at intersection corners whenever on-street parking is present along the block. Curb extensions shorten the crosswalk width, assure parking setbacks from intersections and crosswalks, and delineate (or "book-end") parking lanes. The extension from the curbline should generally be 1 foot less than the parking lane width, but in some instances additional "shy distance" from the adjacent travel lane may be considered. Bumpout design must consider whether a bike lane is planned in the future. Curb extensions may also be used midblock to provide traffic calming or to protect a midblock crosswalk. Bumpouts should generally be implemented as part of a series, as singular instances of bumpouts on a corridor could result in a hazard. Use appropriate design and accommodated vehicles and refer to effective turning radii when designing curb extensions. Curb bumpouts should be delineated with flexible candles on the curb line near the travel paths to aid in winter visibility for drivers and snow plowing.



Figure 5 – Curb extension works to narrow a road adjacent to a school



Source: Googlemaps

### 3.4-4 Curb Ramps

Curb ramps can improve access for many, especially wheelchair users, people wheeling strollers, people with mobility challenges and older adults. How curb ramps are installed affect accessibility, particularly for people experiencing vision loss. Visual impairment can be very limiting for individuals, and physical clues built into street infrastructure are quite helpful. Curb ramps shall be designed in accordance with the recommendations of PROWAG, NACTO, the WSDOT Standard Specifications, and the City of Spokane Standard Plans and General Special Provisions. Curb ramps shall be located in accordance with the City of Spokane Standard Plans, SMC 17H.010.200, and SMC 17H.010.210E. Reconstruction and grind and overlay type preservation projects shall include ADA compliance updates as required by federal regulations.

In all new construction and reconstruction projects placement of two ADA compliant curb ramps per corner is required. The ramp layout should maintain the pedestrian line of travel when feasible. Ramps should be aligned such that the running slope (and edge curb if used) is parallel to the crosswalk markings and direction of pedestrian travel. Grade breaks at the top and bottom of the ramp should be perpendicular to the direction of travel. The low-point for stormwater collection should not be in front of the ramp.

Figure 6 – Ramp running slope aligned with direction of pedestrian travel and ramp on opposite corner





**Figure 7 – Ramp running slope misaligned with crosswalk does not provide information to sight impaired individuals**



For retrofit or preservation work the priority is to use two curb ramps per corner. However, the use of single curb ramps per corner may be appropriate when relocation of utilities would be required to accommodate dual ramps, topographic constraints, right-of-way constraints or intersections with small curb radii. When using a single curb ramp per corner, it is helpful to avoid deviating from the pedestrian line of travel. Alignment cues such as use of perpendicular angles should be utilized. Curb ramps are generally built with flared sides, but at times will be built with pedestrian curbs flanking the ramp. Pedestrian curbs used in this manner should be parallel to the crosswalk.

**Figure 8 – Dual ramps with curbs instead of flares**





### 3.4-5 Street and Pedestrian Lighting

#### General

This section provides general information on street lighting with the City of Spokane. Additional detail, such as the need, type and location, and request process for new lighting is determined by the “Street Lighting Guidelines”, a document available from the Street Department.

Street lighting will generally be provided by the serving utility company. In these locations the maintenance and capital costs are included in the utility company rate. However, on bridges, traffic signals, downtown, certain business districts, and other locations the City may provide lighting equipment and maintenance in addition to the energy costs.

#### Arterial Street Lighting

Arterial lighting is typically 200 watt LED equivalent with one luminaire per intersection. Continuous roadway lighting on arterials is considered on a case by case basis. Lighting levels may be increased on arterials if the City Engineer determines higher levels are appropriate. Generally, low-volume neighborhood collector arterials will have lighting similar to residential streets while high-volume minor and principal arterials may have continuous high-level lighting service.

Arterial lighting will typically be installed on wood poles. The City Engineer may elect to install metal poles on certain streets. Adjacent property owners have the option of upgrading to metal poles through direct negotiation with the serving utility company.

If the arterial lighting service provided by the City does not fit the desired needs of the adjacent property owner, developer, or neighborhood association, they may install a private lighting system. The City will not participate in the costs of any such system. The presence of such a private system will not preclude the City from providing street lighting in conformance with the “Street Lighting Guidelines” if requested. All private lighting systems will require appropriate permits and encroachment agreements.

Preservation projects will not be required to update street lighting. Reconstruction projects should consider updating lighting as defined here-in.

#### Decorative Street Lighting

Decorative street lighting is limited to specific areas of the city and are considered an appropriate kind of place-making element. These areas are defined below. For new installations the maintenance cost may be funded by a business district or similar organization. This section is not applicable to lighting installed and maintained by the Parks Department.

The city has adopted three specific luminaire styles that must be used for all new city-maintained installations or updates. The styles are referred to as Modern Acorn, Transitional Series and Traditional Series. Project designers should refer to Standard Plan J-200 for the specific type to use in the CBD and North Bank/Spokane Arena Districts, and refer to the Street Department for guidance on specific types not listed on that plan.

**Central Business District.** A large area generally defined as the area south of the Spokane River, west of Division, north of I-90 and east of Maple Street. Some areas in the CBD provide decorative pedestrian lighting and street lighting, others are pedestrian only. Much of this area still has the Expo '74 lights that are being removed and replaced



with street improvement projects. The infrastructure supporting this lighting (conduits, wiring, electrical cabinets) also need to be updated when the newer decorative fixture are installed.

**University District (south).** Parts of the south University District including the Sherman Plaza, the south bridge landing, on Riverside from Sherman to Sheridan, on Sheridan from Riverside to Sprague. Overlaps with the East Sprague Business District lighting.

**East Sprague Business District.** The area along Sprague Avenue east of Division to Altamont Street.

**North Bank/Spokane Arena.** There is some decorative lighting in the vicinity of the Spokane Arena and north edge of Riverfront Park.

**Monroe-Lincoln South.** This business district has pedestrian lighting on the arterial street from approximately 10<sup>th</sup> Avenue to 15<sup>th</sup> Avenue.

**North Monroe.** Monroe Street from the river north to Alice Avenue. There is a gap between Mallon and Indiana.

The following districts have special fixtures that are maintained by other entities.

**University District (north).** The area east of Division, south of the river, and north of the railroad. This lighting is maintained by WSU.

**Kendall Yards.** The Kendall Yards development has decorative pedestrian lighting throughout the development. This lighting is maintained by Kendall Yards.

**Gonzaga District.** Parts of the Gonzaga campus including the frontage along Hamilton Street. This lighting is maintained by Gonzaga.

Many of the decorative lighting areas have legacy fixtures that are maintained by the City but no longer used for new installations.

**West Broadway.** Broadway Avenue from approximately Elm Street to Walnut Street within the West Central neighborhood.

**Browne's Addition.** The intersection of Pacific Avenue and Canon Street in the Browne's Addition neighborhood.

**Perry District.** Along Perry Street from 8<sup>th</sup> Avenue to 12<sup>th</sup> Avenue.

**Sunset Boulevard.** Along Sunset Boulevard from 5<sup>th</sup> Avenue to Hemlock Street, generally associated with the Inland Empire Way underpass.

**Hillyard District.** The Hillyard Business District has decorative lighting along Market Street.

**Cliff Drive.** On the Cliff Drive bridge over Grove Road.

### Local Access Street Lighting

Local Access Street lighting consists of a 100 watt LED equivalent lighting fixture on a wood pole at each intersection. Midblock lights may be installed on long blocks of 600 feet or more. However, lights will not be placed less than 200 feet apart.



- The Streets Department maintains a first-come, first-serve priority listing for new lights to be installed as funding comes available.
- Street lights will not be provided at dead ends or at the end of cul-de-sacs. However a midblock street light may be approved for cul-de-sac streets at least 600 feet long.
- The person or group requesting lighting may upgrade the basic wood pole to a metal pole through private negotiations with the electrical service company.
- If the basic street lighting service provided by the city does not fit the desired needs of the adjacent property owner, developer, or neighborhood association, they may install a private lighting system after obtaining the appropriate permits and encroachment agreements. The city will not normally participate in the cost of any such system.

### **3.4-6 Roadside Planting**

Any roadside planting shall conform to the City's clearances/clear zone standards as discussed in Section 3.12 and SMC 17A.020.030N, and SMC 17C.200.050. A permit in accordance with SMC12.02.960 is required for the planting, removal, or pruning of any street tree. Guidelines for proper tree installation can be obtained from the Urban Forestry program of the Parks and Recreation Division. Locations of all existing and proposed street trees shall be shown on plans submitted for review.

The standards within this chapter provide a target set of dimensions for basic tree growth space. Following these standards will support the growth of street trees in an urban environment, and but will not likely support a thriving canopy that can be experienced in more park-like settings. Within the confines here-in defined, tree growth and health will, in time, be stunted, requiring replacement at a younger age. In order to develop a more mature canopy, additional space (beyond these standards) for root growth would be necessary. In further consideration of larger growth expectations, the planter width should appropriately provide for larger trees. The following recommendations set the stage for the standard street tree, thus if larger growth is desired, additional considerations should be discussed during the tree permitting process.

#### **Existing Street Trees**

When development occurs on sites with existing street trees, the following items must be addressed as part of the project:

- All dead or diseased trees must be removed and replaced.
- Trees that are missing shall be replaced.
- Broken or missing irrigation systems shall be repaired or replaced as needed when incorporating new plantings.
- Broken or missing tree grates shall be repaired or replaced.
- All concrete tree grates shall be replaced with metal grates meeting ADA requirements.
- When structural sidewalk is removed and backfilled, concrete planter vaults shall be removed and replaced with an appropriate containment facility providing at least 100 cubic feet of soil.
- Gaps between the tree grate and the soil surface exceeding 6 inches shall be filled in with pea gravel.



- Tree grates that are not flush with the surrounding sidewalk shall be raised or lowered as necessary to prevent a tripping hazard.
- If existing trees have roots that have heaved pavement or sidewalk, work with Urban Forestry to determine an appropriate course of action.

### **New Street Trees**

Tree selection shall be coordinated through Urban Forestry. Approval shall be obtained from the City Engineer and the Urban Forester prior to planting tree(s) in the City right of way. A Street Tree Permit (SMC 12.02.960) is also required before planting tree(s) in the City right of way.

In an effort to assist in the selection of an appropriate tree, the City has published a document entitled "Spokane Urban Forestry Approved Street Tree List" which is included in Appendix F. Not all of the trees appearing on this list are acceptable for every situation. Requests to plant trees not included on the list will be considered on a case-by-case basis. Urban Forestry can provide the most current list.

When locating street trees, the following specific criteria shall apply. In the case that these criteria would prohibit planting of street trees, the Urban Forester and City Engineer may consider alternatives:

a) Street tree installations shall meet all City of Spokane visibility requirements as defined by clear view triangle (SMC 17A.020.030) for intersections and driveway approaches and be placed to provide minimum stopping sight distance for stop signs and visibility for warning and other regulatory signs.

b) Street trees shall be located so as to not interfere with street signs, visibility of regulatory and warning signs, lighting poles, STA stops or pads and to accommodate ADA pedestrian requirements. Also tree locations should consider the tree canopy reach, the impact that may have on fire aerial operations and visibility of warning and regulatory signs.

c) Minimum separation distances from the centerline of a tree to other structures or improvements in the planting strip shall be as follows:

- 1) 10 feet to edge of single-family residential driveway, 15 feet to edge of commercial or multi-family driveway (10 feet may be allowed in some cases);
- 2) 20 feet to street light luminaire (15 feet may be allowed where lighting pattern is not affected);
- 3) 10 feet to hydrants and utility poles. Lower limbs must be pruned for full visibility of the hydrant. No new utility pole location shall be established closer than 10 feet to an existing tree;
- 4) As required to provide an adequate clear sight triangle as defined below and shown in the Appendix;
- 5) 15 feet to underground duct or pipe;
- 6) 5 feet from curb cut for drainage;
- 7) 20 feet from drywell, unless the species permits a closer placement due to crown diameter;



- 8) and shall conform with the Arboricultural Manual: Specifications and Standards of Practice.
- d) Trees that are suitable for wet areas shall be selected for planting within bioretention or biofiltration areas. Trees that are planted within bioretention or biofiltration areas shall not interfere with, obstruct, or retard the flow of water in the stormwater facility.
- e) Spacing of street trees will be determined by the permitting department. Clustering of trees may be allowed under specific circumstances. Contact Urban Forestry Department for more information.
- f) If trees are to be planted in an area with no planting strip, the following criteria shall apply:
- 1) A permanent, hard walking surface at least four feet wide shall be provided between the tree well or planting area and any structure or obstruction.
  - 2) Sidewalk cuts in concrete for tree planting shall be at least 48-inch x 96-inch as shown on the Standard Plans to allow air and water to the root area. Regardless of the sidewalk cut size, the soil volume below the sidewalk should facilitate a minimum of 100 cubic feet for each tree.
  - 3) In cases where the existing walk cannot meet the four foot width requirement after tree planting, additional sidewalk width must be added within street right of way or an easement or the tree position must be modified.
- g) Irrigation systems shall be required for all areas where street trees are planted. In most cases, irrigation is to be provided by adjacent land owners.
- h) Any proposed deviation from these conditions shall require submittal of a written request/ explanation to the Department of Engineering Services or Development Services Center and shall be subject to review and approval by the City Engineer and/or the Director of Parks and Recreation.

### **3.4-7 Transit Stops**

Transit riders must walk along and often cross the street to access and exit their bus stop. Transit-supportive design provides safe and convenient walking routes considering every passenger's trip from start to finish. Transit stops play an important role as part of the streetscape; with the integration of quality bus shelters, wayfinding maps, real-time information systems, and other key features, bus stops have the potential to enhance the public realm.

#### **Stop Placement**

Stop placement must be determined through discussion with STA. Locate bus stops in safe and secure locations where they meet both passenger and operational needs. Each intersection and potential bus stop exhibits unique characteristics that should be considered. Near and far side stops at signals both have pros and cons. Locating stops on the far side reduces conflicts between right-turning vehicles and buses, but can also result in traffic queues through the intersection. Far side stops also allow buses to clear the intersection and efficiently continue operations. Near side stops place the riders closer to the crosswalk.

In-lane vs. pullout stops have similar pros and cons. In-lane bus stops speed up the operation for transit riders since the bus doesn't need to maneuver out of the lane and then wait for traffic to come back in. They also require less curb space than pullouts which can work better in areas



where on-street parking is a priority. In-lane stops work best when the stop time can be minimized through the use of off-board fare payment and curb height that matches the bus floor level. Pullout stops prioritize through traffic movement including through-moving transit, and may be desirable when the bus dwell time is consistently expected to be long (such as at a high school with large groups getting off at one time) or on higher speed roadways such as US 2 in the West Plains.

Coordinate all stop placements with STA such that operations are directly considered.

### **Pedestrian crossing facilities near bus stops**

Locate safe, convenient, and ADA-accessible crossing facilities at or near all bus stops matched to street type. Bus stops on the far-side of intersections require pedestrians to cross behind the vehicle. On the far-side, provide a 90-foot no parking zone with the bus stop located at the far end of the zone.

Where it is impractical to locate bus stops on the far side, near side bus stops should be located at least 30 feet from the intersection crosswalk to ensure pedestrian visibility and space to load/unload bicycles. Provide a 100-foot no parking zone with the bus stop located at least 30 feet from the crosswalk. No parking zones will need to be longer for bus pullout conditions. Refer to route bus size and Transit Authority plans for routes along the roadway when selecting the proper facility type and size.

### **Bike facilities near bus stops**

Bus stops adjacent to bike lanes create conflict zones. There are several design options that can be used to provide safer interaction between these two transportation modes. Figure 9 shows bike lanes separated from bus stop activity using an island bus stop design. This design channelizes the bike lane between the island and the curb.

Figure 9 – Island bus stop separates bike lane from bus traffic



Figure 10 shows a shared bike lane and bus stop where the bike lane rises up to the bus platform level and shares the space used for the bus boarding zone. While the example photo shows a temporary installation would typically use a concrete bumpout.



Figure 10 – Shared bike lane and bus stop using temporary platform



### Bus Stop Amenities

Bus stop amenities encompass the infrastructure present where passengers wait for transit vehicles. They include physical infrastructure such as seating, shelters, and lighting, and informational infrastructure like transit maps or real time information boards. Bus stops with higher levels of activity typically have more intensive infrastructure. Shelters will be located outside of the required boarding and alighting area. Coordinate with STA to ensure shelter location, seating, schedule information, and properly located bus stop signs do not interfere with pedestrian zones and accessibility.

- **Paved and Accessible Boarding and Alighting Areas.** Provide a paved and unobstructed boarding and alighting area that is a minimum 8' x 8', providing space for ramp deployment while ensuring ADA accessibility. A sidewalk can serve part of this purpose, but may require additional space to meet STA design standards<sup>1</sup>. Higher-use transit stops may warrant additional paved frontage for queueing passengers.
- **Supply Secure Bicycle Parking Where Demand Warrants.** Secure bike parking at bus stops encourage people to ride bikes to transit, expanding the reach of transit for many users. Provide leased bike lockers, on-demand eLockers, and basic bike racks where appropriate. Locate basic bicycle parking such as staple racks at all HPT stops and bicycle lockers at all park-and-ride locations. Other optional parking facilities include bike corrals or covered parking areas.

Preservation work is performed between curb lines, and need not address transit facility updates. Reconstruction work should coordinate closely with the needs of current and future transit facilities and incorporate these as appropriate.

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<sup>1</sup> <https://www.spokanetransit.com/projects-plans/bus-stop-design-standards>



## 3.5 Flexible Area

This space between the Pedestrian Realm and the Vehicle Realm can be programmed for car parking, bike parking, landscaping, stormwater management, pavement-level protected bike lanes, shared-use paths, bus bulbs or curb extensions.

### 3.5-1 On-Street Parking

Parking lanes allow drivers and bicyclists to park their vehicles in the public right-of-way, providing convenient access to businesses and homes, and offering loading zones for freight vehicles. Carefully managed, on-street parking can offer traffic calming, economic development, and access benefits. On-street parking lane widths shall be in accordance with SMC 17H.010.120, the City's Comprehensive Plan and/or as directed by the City Engineer. Requests for a reduced street cross-section will be evaluated on a case-by-case basis and a waiver of the on-street parking requirement granted at the discretion of the City Engineer.

Parking and utility access locations should not share the same space. When conducting preservation work that refreshes the paved surface, there is opportunity to re-balance the uses of space. The scoping of such projects should consider the need for parking or access points, which offset one another. Consolidation of access driveways can provide additional parking space. This must be done in coordination with adjacent property owners, and in accordance with access management standards.

Some older streets in Spokane function as “yield streets”. These are bi-directional streets with a through-way narrower than two cars in width, meaning drivers must yield to each other to pass. Yield street operation work best on residential streets when parking utilization is 40-60%, creating a “checkered” parking scheme, which allows drivers to pull over in empty parking spaces or driveways. Yield street operation works best on residential local access streets with two-way traffic that measure 24-26 feet wide with parking on both sides, or 16-20 feet wide with parking on one side.

Figure 11 – Example of a Yield Street: Baldwin Ave between N Hamilton St and N Perry St (25-feet wide)





## Parking Lane Width

Parking on arterial streets must be accommodated by 8-foot-wide parking lanes. See Table 1 for parking dimensions. Parking width on residential streets may be narrower, but the street must meet minimum width requirements defined in SMC 17H.010.060.

## Bicycle Lanes Adjacent to Parking

When bicycle lanes are included in the Master Bicycle Plan, consult Table 1 for the desired bicycle lane width to be used in tandem with parking lanes. Ideally, provide a buffer between the bike lane and travel lane, allowing cyclists to ride outside the parked car “door zone”. Where parking has a high usage and turnover, consider using parking-protected bike lanes with a door zone buffer to reduce conflicts between bikes and cars.

## Angle Parking

Angle parking may increase parking supply if sufficient uninterrupted curb length is available, and is useful in mixed-use areas and retail and commercial districts. Angle parking tends to create a traffic calming effect by inducing caution for motorists driving adjacent to the parking zone. Refer to the city’s standard plan G-60 for dimensions.

Utilize back-in angle parking, which requires the driver to back into the space; particularly when placed adjacent to bicycle lanes. This allows drivers to load vehicles from the sidewalk, improves driver-bicyclist visibility as the driver departs the space, and increases safety for the driver as the person can pull out into traffic rather than blindly backing up into traffic.

## Other Parking Lane Uses

New uses of the parking lanes such as bike corrals and parklets increase the use of the public space for active living, placemaking and recreation.

### Bike Corrals

Bike corrals expand the amount of bicycle parking on a street without taking valuable space away from the sidewalk. Bike Corrals typically replace one parking space at the request of a local business or property owner and accommodates 12-24 bikes. Corrals can be installed at corners to “daylight” an intersection since bicycle parking has minimal effect on the visibility of pedestrians to moving vehicle traffic. Bike corrals have been shown to have a positive impact on nearby business.<sup>2</sup> Corral location must consider:

- Safety for users
  - Set corral back from travel lanes in a parking lane
  - Use corrals on streets with low speed limits and low parking turnover
- Rack placement
  - Perpendicular to curb/roadway for wider parking lanes
  - Angled racks better for narrower lanes
- Land uses
  - Commercial and retail uses have more demand for corrals

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<sup>2</sup> Meisel, Drew. Bike Corrals: Local Business Impacts, Benefits, and Attitudes.” Portland State University. [http://bikeportland.org/wp-content/uploads/2010/05/PDX\\_Bike\\_Corral\\_Study.pdf](http://bikeportland.org/wp-content/uploads/2010/05/PDX_Bike_Corral_Study.pdf)



- Design
  - Demarcate corral with bollards, rubber curbs, and striping. Planters and reflective bollards may also be used.

Before installing a bike corral, require a maintenance agreement between the city and a local business owner or community organization who will maintain the corral and clear it from snow, dirt, or debris.

#### Parklets

Parklets repurpose street right-of-way, often motor vehicle parking spaces, into publicly accessible spaces for all to use. Parklets provide additional public space for people to sit, enjoy meals, meet others, and use for art and plantings. Parklets help communities reimagine the role of the public street. Parklets should be installed on low speed streets.

Before installing a parklet, require a maintenance agreement between the city and a local business owner or community organization who will maintain the space and clear it from snow, dirt or debris.

Requirements for parklet design, planning, and maintenance can be found in the SMC 10.55 Parklets and Streeteries.

### **3.5-2 Stormwater Management**

#### **Low-Impact Development Stormwater Treatments**

Stormwater facilities are addressed in SMC 17D.060. Conventional stormwater management infrastructure is engineered to convey the largest volume of water from a site as quickly as possible, collecting surface runoff in subsurface structures.<sup>3</sup> Sustainable stormwater management, by contrast, views rainwater as an amenity, using it to improve urban ecology, microclimates, air quality, and the aesthetic quality of the public realm.

Low impact development design utilizes landscaping, engineering, and urban design tools to mimic natural watershed capabilities.

Figure 12 – Lincoln Street stormwater management



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<sup>3</sup> "Chapter 3, Fundamentals of Stormwater Management," *New Hampshire Stormwater Manual* (Concord: New Hampshire Department of Environmental Services, 2006).



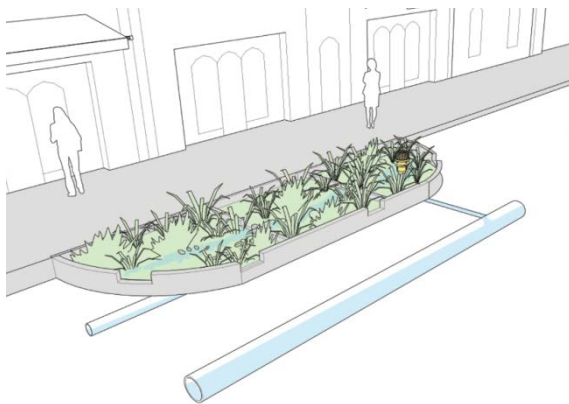
Stormwater facilities that fit the urban landscape, particularly in retrofit situations, are described below. Consult the Spokane Regional Stormwater Manual and Eastern Washington Low Impact Development Guidance Manual for detailed standards and placement guidance. Some tools for Low Impact Development are listed below.

### Bioretention Facilities

Bioretention facilities are shallow landscaped depressions that receive stormwater from small contributing areas. They can be integrated into the site as a landscaped amenity because they are small-scale and dispersed. Bioretention facilities can be placed flexibly within medians, curb extensions, or public space. Maintenance of bioretention facilities involves vegetation management, soil replacement, and sediment and debris removal. In some cases it may be preferable to pipe stormwater to a nearby site where a single large bioretention facility can be constructed. This option must be enacted in accordance with the stormwater development guidelines. City reconstruction projects may have more flexibility to operate in this manner due to the extents and connectedness of the right of way.

When bioretention facilities are added to collectors or arterials, the designer should consult with STA to determine if current or future bus stops may be needed within the project limits. Adding a bus stop later on will reduce the area available for stormwater treatment.

Figure 13 – Bioretention facility

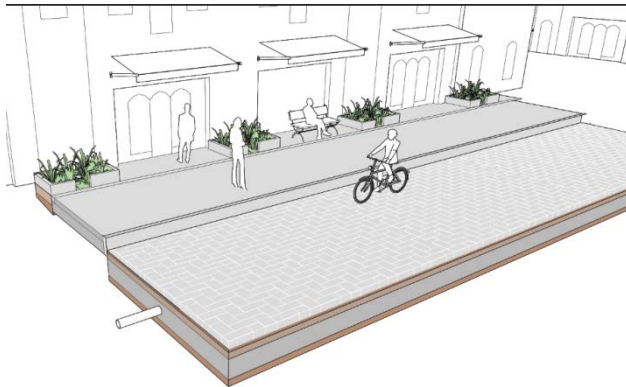


### Permeable pavement

Permeable pavements are being tested in the city for sidewalks, transit stops, pathways, parking lanes and travel lane surfacing. Permeable pavements generally do not work well on travel lanes of roads with high volumes and extreme loads, or where hazardous materials, dirt, or anything that could clog the pavement are loaded and unloaded. Permeable pavements may work well in parking lots, sidewalks, residential streets, medians, driveways, and fire lanes. Maintenance of permeable pavement involves street sweeping, leaf pick up, and may include pressure washing and vacuuming.



Figure 14 – Permeable pavement



### 3.5-3 Shared-Use Pathways

Shared-Use Pathways can be used adjacent to roadways under certain conditions. They work best in locations where limited vehicle volumes can cross the pathway. Common placements would be a pathway between the road and a ridge, river, railroad, freeway, or other manmade or natural feature that restricts vehicular cross traffic. Examples of this in Spokane include the Centennial Trail along Pettet Drive and Upriver Drive, the Ben Burr connection on 3<sup>rd</sup> Avenue, the South Gorge Trail in Peaceful Valley, and the pathway along Government Way. Low-volume street or driveway interactions may be accommodated with design features such as signage, pavement markings and adequate sight distance.

Figure 15 – Shared-Use Pathway along Pettet Drive



Shared-Use Pathways shall be employed where designated in the City's Comprehensive Plan and in the Master Bicycle Plan, and shall be designed in accordance with SMC 17H.010.260. When constructed within the road right-of-way, these will typically be constructed behind the curb and accommodate both bicycles and pedestrians. Additional width to provide at least 2' separation from the curb is desirable.



In locations with a high volume of pedestrians (downtown, college campus) or significant through bicycle traffic, it may be desirable to physically separate the pedestrians and cyclists using striping and pavement markings.

Figure 16 – Shared-Use Pathway with Separate Bicycle and Pedestrian Lanes



## 3.6 Vehicle Realm Access Management and Connectivity

### 3.6-1 Access Management and Driveway Design

Driveway locations shall be designed to provide for safe operations and minimal disruption of traffic flow. In general, the higher the street classification, the fewer the number of access points that are allowed. In areas of high-density housing, shared driveways are encouraged. Multiple unshared driveways with minimal separation between them are discouraged. Minimize driveway width and place them to reduce conflict points.

Access management enables better property access by allowing people to get off the main road and circulate through local streets. On higher speed streets, frequent access points become a safety hazard for all users. Reduce the number of driveways per property to reduce conflict points across all modes, as appropriate and when opportunity arises (see Figure 15).

Access management (i.e. consolidation or reduction of the number of driveway access points along a corridor) may be conducted during street reconstruction projects. However, driveway installations and renovations are generally completed as part of new development and must adhere to the following:

- **Encourage Alley Development to Reduce Driveways on Streets with higher Bike/Ped Activity.** Alleys provide alternative access to adjoining properties. Require utilization of these alleys instead of driveways to reduce the number of access points on main streets. Develop new alleys where possible to provide this alternative access.
- **Design Driveways to Favor Pedestrians and Bicyclists.** Driveways should not be designed as small intersections, but as minor curb cuts. Whenever possible, sidewalks across driveways should maintain their grade rather than sloping down to the street. The



material used to delineate the sidewalk should continue through the driveway. See Figure 13, Figure 14, Standard Plans F-103, F-104, and F104B for examples.

- **During Street Projects, Assess Closure of Driveways.** When street projects are undertaken, evaluate the potential for consolidating driveways along the street to reduce the number of access points. Where streets do not meet the established driveway spacing standard, require new development and consider opportunities during reconstruction projects to address this.
- **High Volume Commercial Driveways.** These driveways should be considered in areas where high volume deliveries are required, where the receiving business may be likely to have a designated loading dock. Commercial driveways may also be considered in a dense commercial center, where multiple businesses could share commercial delivery space without restricting parking availability for customers. It is critical that this type of driveway design does not over-ride the facilities for the most vulnerable users, such as pedestrians. If visibility is a challenge for commercial vehicles entering or exiting, warning systems may be installed to warn drivers and pedestrians alike of an approaching vehicle.
- **Infill Access.** On case-by-case basis, single family residential zones can be developed using a variance to develop interior lots that share a driveway with primary lots. This is meant to facilitate development of lots that could not otherwise be developed in accordance with the standards. This applies only to parcels that are between 0.2 and 1.5 acres in size (8,700 to 63,430 ft<sup>2</sup>), with an approved Design Variance. Utility, emergency fire access, stormwater considerations, and other considerations must also be met.

Figure 17 - Brick sidewalk pattern is continued over the driveway to establish pedestrian dominance.

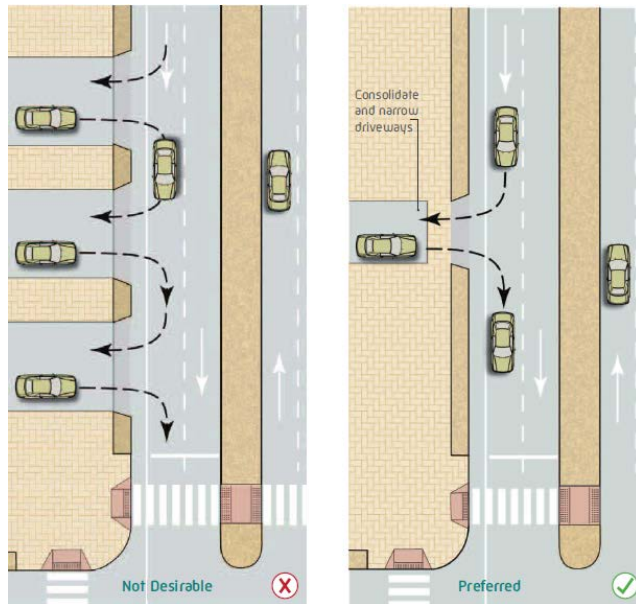


Figure 18 - Continuous Sidewalk Design Establishes Pedestrian Space over Driveway





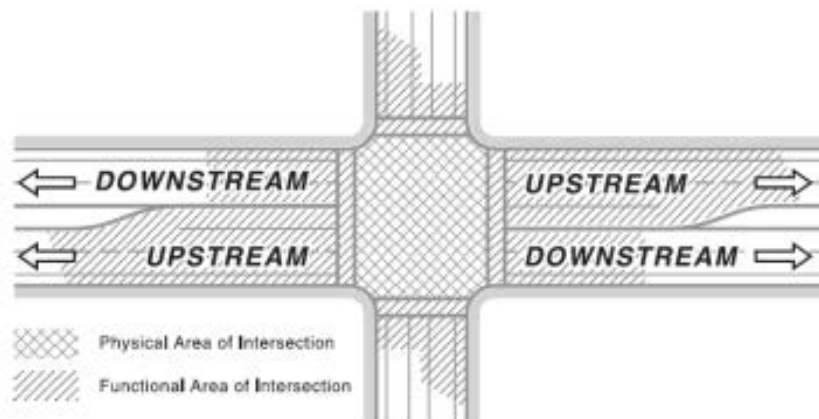
Figure 19 – Consolidated driveways increase safety for drivers and pedestrians



### Access Management Standards

- Principal and Minor Arterial driveway spacing: minimum 125 feet
- Collector driveway spacing: minimum 90 feet
- Local Residential driveway spacing: one per parcel for new development
- Driveways shall be located outside the Functional Intersection Area at signals (area beyond physical intersection that includes decision and maneuvering distance), or in the alternative, may be restricted to right-in, right-out.

Figure 20 – Functional Intersection Area



- One driveway per commercial parcel with one additional access point per fronting street allowed if the property frontage is over ¼ mile in length and the site generates more than 100 PM peak hour departing trips.
- Commercial driveway approaches should be at least 75 feet from the point of curvature of a public road curb return on arterial streets and at least 30 feet for local access streets.



- For commercial driveways handling high volumes, a deceleration lane may be provided approaching the driveway, as justified by a traffic study or operational analysis. The driveway design must still maintain a tight turning radius to foster low speeds.
- Residential driveway approaches should not be closer than 15 feet from the point of curvature of a public road curb return on arterial streets and 10 feet for local access streets.
- Limit the Width of Driveways. Driveway width should be no more than 40% of the frontage (SMC 17H.010.220).
- Restrict Driveways near Bus Stops and Intersections. Do not place driveways within 100 feet of major intersections and 50 feet of other junctions, including bus stops, crosswalks, and small intersections.
- Shared driveways is a strategy to consolidate the number of access points along a block to reduce the number of potential conflict points between motorists and pedestrians. Driveways can be consolidated in instances where a single parcel has multiple access points, or where neighboring parcels may share parking resources. Driveway consolidation typically occurs during redevelopment as parcels and land use along a corridor change. Guidance for shared driveways for Single Family Residential Zoning development projects is found in the Infill Access and Utilities Standard.
- See additional access standards for Downtown Zones in SMC 17C.124.280 and Residential Zones in SMC 17C.110.535.

### **Standards for State Highways**

Specific access standards apply for state highways within the City limits, which are classified as managed access facilities. Managed access is based on the premise that access rights of a property owner are subordinate to the public's right and interest in a safe and efficient highway system.

In accordance with Chapter 47.50 RCW, the City adopts by reference, the provisions of Chapter 468-52 WAC, together with all future amendments, in order to regulate and control vehicular access and connection points of ingress to and egress from, the State Highway System within the incorporated areas of the City of Spokane.

### **3.6-2 Street Connectivity**

Connectivity refers to the density and directness of connections in path or road networks. Well-connected street networks have short links, frequent intersections, and minimal dead-ends or cul-de-sacs. High connectivity creates a more accessible and resilient transportation network, providing direct routes between destinations, multiple route options, and ultimately more capacity.

In designing streets, subdivisions, and retrofitting streets:

- The layout of new streets should consider future extensions of public roads and utilities into adjacent undeveloped parcels.
- Create blocks no longer than 660 feet in length. In urban settings (dense housing, centers and corridors, downtown, or commercial), strive to create short blocks that foster circulation.
- If topography, surrounding development patterns or other constraints make it impossible to meet the 660' block length, the City Engineer may approve a longer length if the total



- perimeter of the block is less than 2000 feet. In these situations, pedestrian connections should still be provided at 660 feet or less.
- While rare; when opportunities arise (in the built environment) retrofit areas of the city with existing blocks longer than 660 feet in length with, at minimum, walking and bicycling connections. See Figure 21 for an example.
  - When retrofitting areas of the city to create greater connectivity; utilities, emergency access, and maintenance access should be reviewed.

Figure 21 – Baymount Court connects through to Eagle Ridge Blvd for pedestrians and bicyclists.



### 3.6-3 Alleys

Alleys shall be constructed in accordance with SMC 17H.010.130 and the Standard Plans. All alleys shall have a minimum paved width of 12 feet with a 4-foot buffer strip on each side. The buffer strips may be paved, grassed, or graveled. The buffer strip may be used for utilities, but must be kept free of all vertical obstructions. Fences may not be placed in the buffer strip.

Preservation and reconstruction work will generally re-pave alley entrances to assure level matching of paving to the alley surfacing. When applicable, entrance design should coordinate with alley activation surfacing designs. Alley paving projects must comply with ADA standards where intersecting with sidewalks.

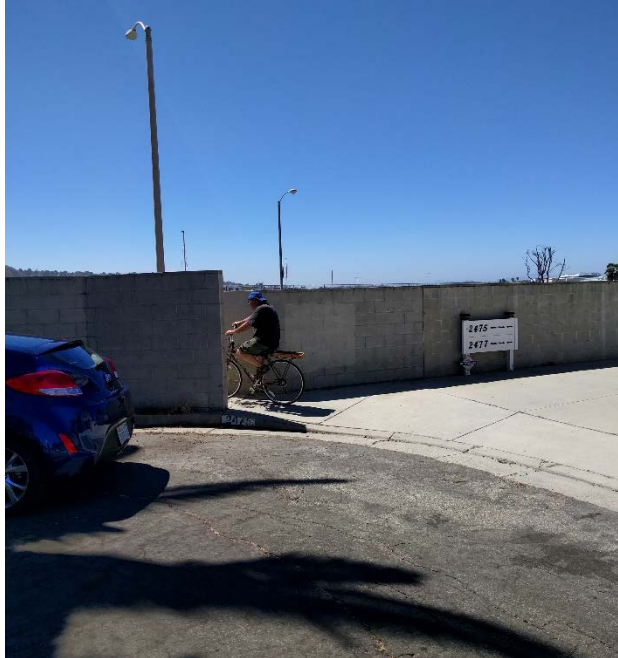
### 3.6-4 Turnarounds

Cul-de-sacs limit connectivity, lengthen emergency response time, and create a physical barrier between residents and trip generators. SMC 17H.010.080 restricts the construction of new cul-de-sacs unless specific conditions are met. Standard Plans W-114 and W-115 show design details of turnarounds.



In new developments, require a “stub-out” future roadway connection at the end of a street that will connect to future development. Connect existing turnarounds to any pedestrian and bicycle trails in the vicinity to close a gap in the walking and bicycling network.

Figure 22 - Example of bicycle and pedestrian connection from a dead-end street, providing additional connectivity.



If cul-de-sacs are provided, use the following types:

- **Standard Cul-de-sac:** The standard cul-de-sac is preferred for construction on local access dead end streets. The radius point of the bulb is on the street centerline. Install a stub-out at the end of the turnaround.
- **Offset Cul-de-sac:** An offset cul-de-sac has a radius point offset from the centerline, with one curb being tangent to the bulb curb. Like the standard cul-de-sac, it is intended for use on local access dead end streets.
- **Temporary Cul-de-sac:** A temporary cul-de-sac is similar to the standard cul-de-sac but allows for planned street continuation. Curbing is not installed in the temporary cul-de-sac, and the roadway dimensions resume at the terminus in preparation of further street construction (the terminus is suitably blocked to eliminate immediate access). When the street is extended, new curbs are constructed along the roadway tangent, extending from the end points of the original curbs and the excess asphalt is removed.
- **Hammerhead:** The hammerhead termination may be used on local access dead ends, but is primarily intended for use in dead end residential alleys. Construction of a hammerhead termination on local access streets is allowed only on approval of the City Engineer.

The following specific design criteria shall apply to the design of cul-de-sacs:

1. Cul-de-sac islands may be an option for any permanent cul-de-sac. The island area shall be finished in a manner approved by the City Engineer.



2. Minimum curb radius for the bulb shall be 50 feet plus the radius of a center island, if used.
3. Minimum right of way radius for the bulb section shall be 56 feet plus the radius of a center island, if used. If the sidewalk is to be located on an easement, the minimum right of way radius is 51 feet.
4. Unless otherwise approved by the City Engineer, cul-de-sacs shall be designed to "drain out" to the adjacent street to avoid flooding if the storm drainage system fails.
5. Cul-de-sac profiles shall be established to provide minimum 2% grades at all places along the gutter lines.
6. Provide a 14-foot wide connection (10-foot path plus 2-foot buffers) for pedestrians and bicyclists along fences separating two yards

### **3.6-5 Entrance Gates and Queuing Area**

Proposed entrance gates may be allowed and designed in accordance with SMC 17H.010.100 and shall not interfere with emergency vehicle access. An adequate fire lane must be provided. If a center island is used, a minimum 14-foot wide lane between the face of curb and center island shall be provided. The center island shall not extend past the end of the gate when it is fully opened. In a case where there is no center island, the minimum road width is 20 feet. No parking on either side of the street will be allowed within 48 feet of the gate on both sides of the gate. The no parking zone shall be clearly signed on both sides of the gate. When fully opened, the gate shall not block access to structures or fire hydrants.

Gated streets require a queuing area to allow vehicles to exit the connecting street prior to the gate. The queuing area must be at least 48 feet long (measured from the intersecting curb line) to accommodate fire vehicles. Queuing areas longer than 150 feet will require a public turnaround designed to City Standards.

## **3.7 Vehicle Realm Geometrics**

### **3.7-1 Bike Facilities**

Bicycle facilities shall be employed where designated in the City's Comprehensive Plan and in the Master Bicycle Plan, and shall be designed in accordance with SMC 17H.010.260. Implementation of planned bicycle routes should be prioritized whenever reconstruction or preservation work is conducted, and new development should consider implementation of bicycle facilities to appropriately tie into the planned or existing network.

Side slopes adjacent to bikeways shall meet the requirements of Table 3. Minimum widths for bicycle facilities are shown in Table 1. Bicycle facility dimensions include the gutter pan.

Consult the Bicycle Master Plan for design details on each bike facility type, and consider factors such as ADT, speed limit, and number of lanes when designing the bicycle facilities in accordance with the contextual guidance from FHWA shown in Figure 22 below.

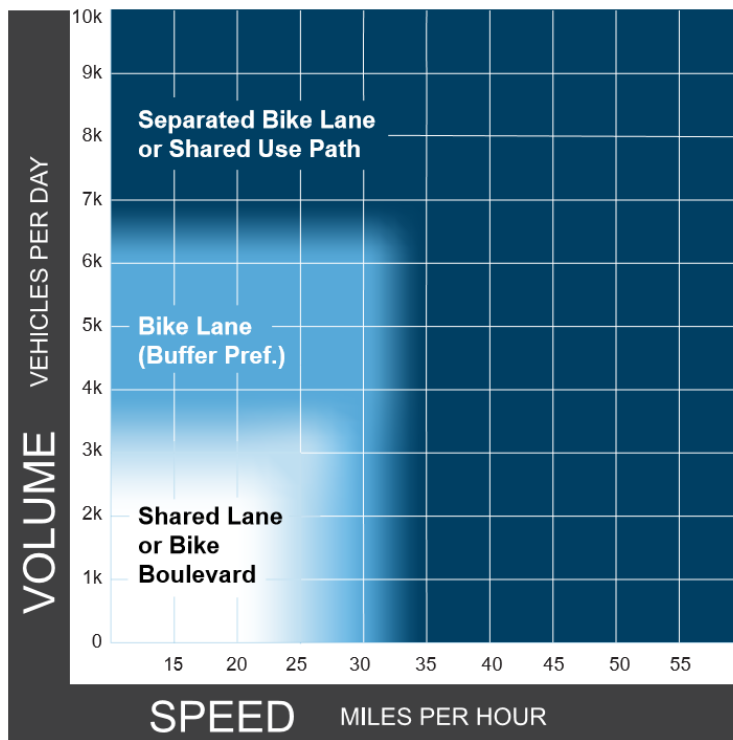
Stress analysis research shows intersections are the toughest part to navigate, especially for people interested but concerned about cycling for transportation. Consult the MUTCD, NACTO Urban Bikeway Design Guide, AASHTO Guide for the Development of Bicycle Facilities, and FHWA Bikeway Selection Guide for corridor and intersection treatments.



**Buffered bike lanes** combine a single-direction bike lane with a buffer to provide a comfortable facility for users. The overall dimension should not be less than 6 feet without a buffer, or less than 7 feet including a buffer. This wider dimension accounts for curb-side obstructions or parked vehicle door dangers. Design should use a parallel line buffer design rather than cross-hatching to minimize the maintenance expense, although short lengths of cross-hatching may be used near conflict zones (intersections or driveways) to better communicate the purpose of the parallel lines as bike lane markings. Vertical elements may be introduced into the bike lane buffer. Planters may be used in downtown and other lower speed areas if they follow the guidelines in the Horizontal Clear Zone section. Reflective plastic bollards may be appropriate elsewhere.

Two-way bike lanes (on the same side of the road) are not addressed in Figure 1. If used they should be a minimum width of 8', although 10' is preferred, with a 2' minimum buffer.

Figure 23 – FHWA Bikeway Guide



**Notes**

- 1 Chart assumes operating speeds are similar to posted speeds. If they differ, use operating speed rather than posted speed.
- 2 Advisory bike lanes may be an option where traffic volume is <3K ADT.

**Neighborhood Greenways** (aka Bike Boulevard) are residential bikeways that prioritize bicycle and pedestrian travel over vehicle through-put. Several tools may be employed to create a greenway. Generally a greenway will be sited on a residential street paralleling a nearby arterial street. Thus connections to destinations along the arterial are readily accessed, though the stress experienced by the walker or biker are much lower. Prioritizing pedestrian and bicycle traffic is achieved by providing appropriate facilities for these modes of travel and by calming or reducing vehicle traffic flows. Greenways are commonly attributed with slow speed, minimum stop signs, and protected crossings of arterial streets.



Some greenway tools include signing and intersection treatments. Signage should be used to highlight the designated greenway, and should also provide distance-based wayfinding to community destinations for bicycle and walking traffic. Intersection treatments are particularly important to the success of a greenway. Intersections with arterial streets need to provide safe and functional crossing methods for bicycle and pedestrian traffic. Intersection treatments might also be used to dissuade vehicle traffic from the greenway. This can be done through limiting turn movements onto the corridor from more busy streets or even by diverting traffic off of the corridor at lower volume intersections. It is important that these treatments are used only on designated greenway corridors, as the impact to neighborhood traffic patterns can be significant.

**Shared-Use Pathways** are typically off-street facilities designed for all non-motorized users. A minimum width of 12 feet is commonly used, although wider sections may be desirable to accommodate high volumes or utility access. Guidelines for shared-use pathways next to roadways are discussed in Section 3.5-4.

**Green paint** should be used only in high conflict areas. Examples of high conflict areas include marking a bike lane through an intersection where there are heavy conflicting right turn movements, marking a contra-flow bike lane through an intersection, or marking the entrance to a right-turn only lane where vehicles must cross the bike lane. Green paint can also be used to connect corridors that are otherwise unclear, when introducing bicycle facilities newly to a corridor, to aid in wayfinding or in places where vehicles are found to encroach on the bicycle facility.

**Bicycle detours** must be planned and implemented whenever work interrupts a bicycle lane. Temporary shared-use lanes may be used, if traffic volumes are acceptable. When traffic volumes are high, bicycle detours should guide cyclists on routes and temporary facilities with relatively similar safety conditions as the route being detoured from.

Figure 24 – Neighborhood Greenway Sign



### 3.7-2 Profile Grades

The maximum profile grade for all streets, alleys, and pathways is 8%. A variance may be granted by the City Engineer considering topography, safety, maintainability, function, and emergency vehicle access. The minimum profile grade for all streets, alleys, and pathways is 0.8%. Cul-de-sac profiles shall be established per section 3.7-3. The profile grade at all residential intersections, along minor roadways at arterials, and for all roadways at controlled intersections shall be no greater than 3% at any point within 100 feet of the near end of the curb radius on minor roadways.

Preservation work need not correct profile grade issues, except as possible to eliminate minor inconsistencies. Reconstruction projects should address needed profile improvements.

### 3.7-3 Horizontal Curves

Horizontal curves are to be determined in accordance with normal civil engineering procedures, considering design speeds, sight distances, roadway crown, building proximity, and vertical grades. For arterial streets with speeds of 30 mph or higher, A 100-foot horizontal curve radius



shall be considered the minimum unless otherwise authorized by the City Engineer. The maximum superelevation on horizontal curves shall be 2%. The minimum horizontal curve radii shall be determined per AASHTO Design for Low Speed Urban Streets, based on design speed, which shall be the posted speed limit, and considering the roadway crown. Pavement widening on horizontal curves to accommodate large vehicles shall be considered per AASHTO Chapter III - Elements of Design, Table III-23.

Preservation work need not correct horizontal curvature issues, except as possible to eliminate minor inconsistencies when the roadway is not bounded by curbing. Reconstruction projects should address needed horizontal curvature improvements within a reasonable effort and cost.

### **3.7-4 Vertical Curves**

Refer to Table 2 for sag and crest vertical curve design criteria. Vertical curves must provide adequate stopping sight distance as defined in the 2011 AASHTO “A Policy on Geometric Design of Highways and Streets”.

Preservation work need not correct vertical curvature issues. Reconstruction projects should address needed vertical curvature improvements, as possible while matching adjacent buildings and driveway grades.

### **3.7-5 Roadway Side Slopes**

Roadway side slopes shall meet the requirements of Table 3; special sloping may be required to meet minimum sight distances.

Preservation work need not correct side slope issues. Reconstruction projects should address needed improvements, particularly where safety has proven to be compromised due to obstructions to sight distance.

### **3.7-6 Design Speed**

Street design sets the context for driver response. Historic design practices have used 85<sup>th</sup> percentile observed speeds or have established design speed higher than the posted speed. In particular, design speed is used during design of horizontal curves. Because design speed is one of the factors in determining street context, it should be established as the posted or target speed. This practice will avoid “speed creep”, which can occur when streets are built to operate at higher speeds than posted and the next design period resets with a speed study revealing the 85<sup>th</sup> percentile has increased. Streets designed for the target operating speed have proven to have greater user compliance, and are thus safer for all users.

Table 4 – Target speeds by street type

|                                                        | RESIDENTIAL, INDUSTRIAL, CB AND GC |                |           |       | CC, DOWNTOWN, FORM BASED CODE |                |           |       |
|--------------------------------------------------------|------------------------------------|----------------|-----------|-------|-------------------------------|----------------|-----------|-------|
| Street Type                                            | Principal Arterial                 | Minor Arterial | Collector | Local | Principal Arterial            | Minor Arterial | Collector | Local |
| Design Speed =<br>Posted Speed =<br>Target Speed (mph) | 30-35                              | 30-35          | 30        | 25    | 20-30                         | 20-30          | 20-30     | 20-25 |



### 3.7-7 Vertical Clearances

The clearance above any street surface shall be as provided in SMC 17H.010.240 and SMC 12.02.0462.

Preservation projects must coordinate with Urban Forestry to ensure the tree canopy is in compliance. Reconstruction projects must similarly ensure the tree canopy is in compliance, and should consider opportunities to improve upon other hazards or obstructions.

### 3.7-8 Horizontal Clear Zones

This section is intended to replace the former City of Spokane clear zone policy ADMIN 0370-08-04. Clear zones are unobstructed, traversable areas that extend beyond the curb-to-curb dimensions of the traveled street. Clear zones allow for loss of control and other erratic driving behavior. Commonly found fixed objects in the right-of-way include: trees with a diameter of 4 inches or more (measured at 6" above ground surface), wooden poles or posts greater than 16 square inches in cross-section (without breakaway features), bridge piers, retaining walls, landscaping walls, some types of fences, signal poles, signal/lighting/ITS cabinets, culvert ends, utility poles and luminaire poles.

Generally, clear zones can be reduced in urban areas since wide unobstructed sidewalk and/or shoulders lining the roadway encourage higher-speed driver behavior. The presence of street trees and other roadside features tend to decrease overall speeds, increasing safety for all users and more comfort for people walking and biking. The City of Spokane Comprehensive Plan promotes a sense of place, encourages the installation of street trees in the planting/pedestrian buffer strips, and encourages other urban amenities along and adjacent to roadways such as planters, bollards, benches, light fixtures, kiosks, clocks and transit shelters.

The City of Spokane is granted jurisdiction over clear zones along City streets and managed access State highways within the City per RCW 47.24.020(2). Along managed access State highways this authority applies only beyond the curb, or if no curbs, beyond the portion of the roadway used for highway purposes. Between the curbs (median areas) the Washington State Department of Transportation (WSDOT) has jurisdiction over clear zone. WSDOT has full authority over clear zones inside and outside curbs along State limited access facilities within the City.

Table 5 – Minimum Clear Zone (distance from edge of traveled way)

|                                                                     | Posted Speed<br>20-35mph                   |                                    | Posted Speed<br>40 or above                |                                    |
|---------------------------------------------------------------------|--------------------------------------------|------------------------------------|--------------------------------------------|------------------------------------|
|                                                                     | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> |
| State Highways                                                      | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 |
| New street construction                                             | n/a                                        | 4                                  | n/a                                        | 10                                 |
| Street reconstruction including width<br>or profile adjustments     | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| Street reconstruction not including<br>width or profile adjustments | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| New installations not related to<br>street construction             | n/a                                        | 4                                  | n/a                                        | 10 <sup>1</sup>                    |



<sup>1</sup> If 10 feet clear distance cannot be provided within the available right-of-way, the design engineer may evaluate and justify placement as near the outer edge of the right-of-way as practical.

<sup>2</sup> On a curbed street all fixed objects shall be at least 1.5 feet behind curb regardless of the location of the travelled way. This is to ensure clearance for parked vehicle doors, snow removal, sign overhang, etc.

<sup>3</sup> Fixed objects / trees with less than 1.5 feet clearance should be considered for removal or relocation. If clearance is between 1.0 and 1.5 feet existing fixed objects including trees may remain unless damage indicates a history of vehicle collision, the object or tree conflicts with the condition or operation of a street, alley or sidewalk, or removal/relocation is required due to other public safety, convenience or aesthetic considerations.

When indicated by Table 5, rigid objects within the clear zone should be removed or not installed, relocated to a position outside the minimum clear zone, remodeled to make traversable, breakaway, or shielded.

- A larger clear zone on the outside of horizontal curves is desirable. On streets with on-street parking, bike lanes, or on streets without curb the clear zone is measured from the edge of traveled way.
- Signals, cabinets, illumination poles, parking meters and ITS equipment are exempt from the policy, although desired placement is at least 1.5 feet from the face of curb.
- Traffic control signs, fire hydrants and residential mailboxes may be placed in the clear zone if on a breakaway fixture or a frangible design.
- Planter boxes, benches, bike racks, transit shelters, bollards, utility standpipe vents, clocks, trash cans, fencing for sidewalk cafes, kiosks, security barriers, mail drop boxes, tree guard and other street furniture typically used in the downtown and centers and corridors are exempt from the policy, although desired placement is at least 1.5 feet from the face of curb.
- Any planter boxes placed in the street as traffic calming or delineation devices should be of a frangible design or pinned in place. Height including sight blocking vegetation shall not exceed 36 inches.
- Within medians the clear zone should be 1.5 feet along straight sections, and 3 feet near intersections where the median is near the alignment of turning movements.
- The width of on-street parking and bike lanes can be included in the measurement of clear zone distance.
- In areas where sidewalk does not exist, the future location of sidewalk shall be evaluated. Existing buildings or other property improvements may make it prohibitive to provide separated sidewalk with planting or pedestrian buffer strips in the future. If it is determined that future sidewalk will necessitate installation adjacent to curb, the distance behind curb shall be increased to allow installation of the proper width sidewalk without obstructions.
- Attainment of these clear zone values does not relieve the Design Engineer of the responsibility to evaluate sight distances in accordance with applicable design standards.
- A three foot clearance to roadside objects should be provided near turning radii at intersections and driveways to prevent a truck overhang from striking an object.



### 3.7-9 Roadway Drainage

Stormwater collected within the roadway must be effectively routed to drainage facilities, such that flow accumulations and pooling are minimized, or otherwise efficiently dissipated. Minimum roadway profile grades are shown in Table 6. Standard Plan W-101 provides a chart for selecting a roadway crown section based on roadway width and curb height differential. Refer to the City's Standard Plans for cross-section and staking data. For vertical curves, the designer's attention is called to the limiting K-value factors shown in the Table 2 footnotes.

Generally, no more than three lanes should be sloped in any one direction. On wide streets, a quarter-crown or center-crown cross-section is recommended, or the designer may consider stormwater collection at the median.

Refer to Section 3.4-5 herein for stormwater disposal methods and design requirements. New development and re-development treatment requirements are addressed in the stormwater design guidelines.

### 3.7-10 Through Traffic Lanes

Refer to Table 1 for traffic lane design width guidelines.

Reconstruction and preservation work shall incorporate markings for all users of the street as determined within this standard for planned pedestrian, bicycle, and vehicular facilities.

### 3.7-11 Exclusive Turn Lanes

Left and right dedicated turn lanes widen the intersection, often require adding another signal phase, and may lengthen the overall delay for users. Dedicated turn lanes should be used only when specifically determined by an engineering analysis to solve congestion issues. The engineering analysis should consider the impact not only on the target intersection, but also the surrounding street network. Refer to appropriate MUTCD guidelines for design and application of dedicated turn lanes.

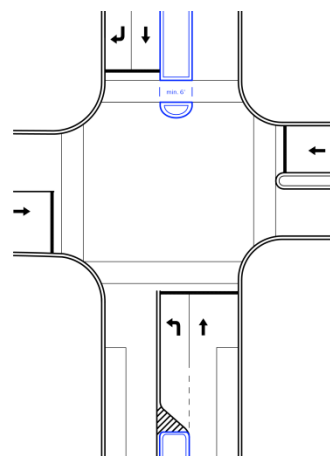
In connected networks, left turns can be restricted at periodic intersections to avoid having long exposed pedestrian crossings at every intersection.

Preservation work need not incorporate roadway reconfiguration projects, unless planned as a follow-up to reconstruction work that conducts such changes, and thus would otherwise leave pavement patching.

### 3.7-12 Tapers

The standard taper length for narrowing or offsetting of a lane shall be based on the design speed, per the U.S. Department of Transportation Manual on Uniform Traffic Control Devices (MUTCD).

Figure 25 – Pedestrian refuge at left turn lane pocket





### 3.7-13 Survey Monuments

At a minimum, monumentation shall be provided in the following locations:

- a) At center of each cul-de-sac
- b) At point of curvature on all horizontal curves
- c) At point of tangency on all horizontal curves
- d) On the roadway centerline at the end of every plat.

Monument pins with cases shall be installed at these locations in accordance with the City's Standard Plans.

These specifications apply to all preservation and reconstruction work.

## 3.8 Median Realm

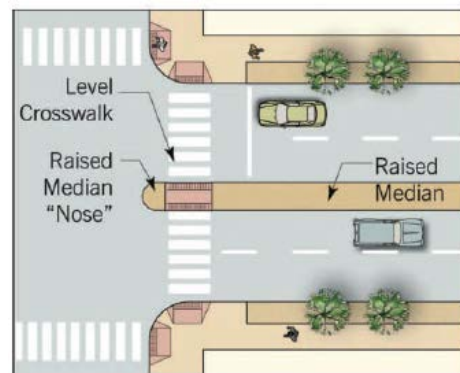
Build medians in accordance with Table 1 on new streets. In retrofit situations, vehicle lanes could be narrowed to add pedestrian refuge islands or medians at unsignalized marked crosswalks on principal or minor arterials in dense zoning<sup>4</sup>. Pedestrian refuge islands should be considered for wider street crossings. A minimum of 6 feet is required for a pedestrian refuge median (8 feet is optimal). However, in retrofit situations a narrow pass-through may be more desirable than no island at all. A narrow median pass-through can provide a place for crosswalk warning signage and also work to reduce vehicular speeds by visually narrowing the roadway. When crosswalks go through a median, protect the crosswalk users with a raised median nose. The end of the median must be marked with a vertical marker for snow plow delineation.

Some transit routes may find it beneficial to place bus stops in the median. This type of setup requires left-side boarding doors on the bus and crosswalks to reach the median. The City Line route, opening in 2021, has designed several median stops.

Speeds can be reduced at neighborhood entry points by installing a short median. This treatment provides a cue to drivers that they are leaving an arterial street and entering a local street. See Figure 28.

Medians, where constructed, shall not exceed 600 feet in length without a break that allows emergency vehicles to cross through the median and continue in the same direction (S-Turn movement). See SMC 17H.010.140 requirements on emergency vehicles access and staging areas on local streets. The break in the median does not need to allow for U-turn movements. Consider the space required for turning movements when installing in tandem with bulbouts.

Figure 26 - Protecting crossings with a median tip provides safety from turning traffic



<sup>4</sup> Per crosswalk ordinance <https://static.spokanecity.org/documents/projects/crosswalkordinance/adopted-crosswalk-ord-c35141.pdf>



Figure 27 – Neighborhood entry median.



Medians may be combined with on-street parking, bulb-outs or chicanes provided that fire staging areas are provided periodically. These designs must be closely coordinated with the fire department to ensure adequate access to hydrants and structures. Staging areas must not be used for snow storage and must be clearly marked to restrict parking. Hydrants should be located at the staging areas which improves fire access and helps to enforce the parking restriction. Hydrants could also be located in the median, allowing better access and limiting the possibility of blockage by parked cars. Prior to approving hydrants in the median, the method for snowplowing this area and keeping the hydrant clear must be discussed with Streets. Median landscaping should consider the height of adjacent buildings and the need for aerial equipment. Neighborhoods developed with this pattern should also provide a grid network to allow for alternative routes during emergency events.

Figure 28 – Summit Parkway with medians, bulb-outs and fire staging areas.



Preservation work need not adjust nor replace medians. Reconstruction projects should consider the space used by the median, and the utility of that space to be maintained as median or other uses. Pavement and median condition should be considered as possible replacement items during scoping of capital work.



### 3.9 Neighborhood Traffic Calming

Traffic calming increases safety through vertical and horizontal traffic slowing measures, and by reducing traffic in residential neighborhood areas. Install traffic calming strategically to protect vulnerable users, reduce speeds in areas exhibiting safety concerns, and as part of the city's Neighborhood Traffic Calming Program. Tools include:

- Horizontal measures – Chicanes, intersection and midblock curb extensions, traffic circles
- Vertical measures – Raised crosswalks, tabletop intersections, installation of sidewalks.
- Traffic reduction – Diverters, medians with walking and bicycling cut-throughs

A formal neighborhood traffic calming program is presently administered by the City through Neighborhood Services. Included in the program is a "Traffic Calming Toolbox", outlining the basic options for solving concerns within any given neighborhood. This toolbox, although not exhaustive, is a reference for optional traffic calming elements within capital or development projects. The NACTO Urban Street Design Guide is also a good reference for traffic calming design. When considering traffic reduction measures, consideration should be given to where traffic will reroute to.

Implementation of traffic calming is required only for approved applications. New developments may include traffic calming measures as appropriate, per SMC 17H.010.160. Preservation and reconstruction projects will install traffic calming elements as programmed.

### 3.10 Pavement Design

#### 3.10-1 Asphalt Binder Selection

All Hot Mix Asphalt binder and aggregates used in the traveled way shall conform with WSDOT specifications, and meet the requirements for durability and performance.

These specifications apply to all rehabilitation maintenance and capital work.

#### 3.10-2 Pavement Section Thickness

The minimum asphalt thickness shall be in accordance with Standard Plan W-101A. As noted in W-101A, the City Engineer may require a pavement design for local access (residential or commercial) streets. This will be evaluated on a case-by-case basis. All arterials require a pavement design, which shall be approved by the City Engineer. A rational pavement design for either arterials or residential streets must contain the following:

1. Traffic Loading – an estimate of the number and types of loadings that roadway will carry for the design life. This estimate of loading must be established by a procedure accepted by the City Engineer and be expressed in 18-Kip Equivalent Single Axle Loads (ESAL's).
2. Subgrade Support—a representative value for the stiffness of the native material on which the road will be built. This value will be established by a procedure accepted by the City Engineer and be expressed as resilient modulus (MR). When determining MR, soil sampling is to include:
  - a) Obtaining a sufficient number of soil samples which adequately represents the subgrade MR, and where significant changes in MR occur;



- b) Constructing a soil log to a minimum of five foot depth below proposed subgrade and classify the soil per USC; and
  - c) Recording the location of where the samples were obtained, normally by station and offset. This record shall be provided to Engineering Services.
3. Analysis- a procedure for establishing the surfacing depth requirements for a given traffic loading and subgrade resilient modulus. The City Engineer must approve this procedure. The following procedure is pre-approved: Guide for Design of Pavement Structures (26), 1994 the American Association of State Highway and Transportation Officials (AASHTO).
- The pavement design life is 20 years for new construction and 15 years for pavement overlays. The structural pavement calculations, soil sample locations, lab results, design criteria and recommendations are to be included in a report prepared by the sponsor's engineer. All design factors used are to be listed in the report, including traffic loads projected to occur over the life of the pavement. The report is to be stamped by an engineer, licensed in the State of Washington.
- These specifications apply to all preservation and reconstruction work.

### **3.10-3 Pavement Patching**

The City of Spokane adopted the Spokane Regional Pavement Cut Policy in 2005. The adoption resolution is included in Appendix F. This pavement cut policy is updated on a regular basis through coordination with Avista and other local agencies in the Spokane area. All pavement cuts for utility work and patches shall be designed and constructed in accordance with the latest version of this policy.

These specifications apply to all preservation and reconstruction work.

## **3.11 Intersections**

Intersections represent the most complex pieces of the network. They are the place at which multiple modes meet and need to pass safely through. Keeping intersections compact increases eye contact between users, and making them legible or intuitive means each user knows where he or she belongs. Follow these principles of intersection design:

- Make intersections as compact as possible
- Identify utility maintenance access in design considerations
- Analyze intersections as part of a network, not in isolation
- Design intersections as shared spaces
- Integrate space and time; for example adjust signalization timing to improve flow on a corridor

The maximum centerline distance between intersections shall be 660 feet. The minimum recommended centerline distance is 150 feet, or 300 feet for signalized intersections. In general, intersections should be at right angles. The minimum acute intersecting angle for streets shall be 70-degrees. For stop sign-controlled streets the 70-degree (tangent) portion shall extend along the controlled street a minimum of 30 feet from the end of the curb radius. For all cases, the effects of sight distance shall be considered.

Preservation projects may implement adjustments to striping patterns, but will not be expected to adjust curb placement except as necessary for ADA compliance measures. Intersection design



principles should be reconsidered for reconstruction projects. This is particularly important if there are high incidents of collision, but may also be important if the use patterns have evolved since the original construction; i.e. a new industrial area has developed.

### 3.11-1 Design Vehicle

Streets should be designed to serve the most vulnerable user. Designing streets for the largest possible vehicle results in streets with oversized intersections and large turning radii. The result is higher operating speeds for the most frequent vehicles on the street – passenger cars. Use both design vehicles and accommodated vehicles for intersection design. Each intersection is unique, and designing for the largest most frequent vehicle (comprising 10% or more of Average Daily Traffic) allows for better –controlled turning speeds on streets and at intersections. Follow these guidelines for selecting design and accommodated vehicles:

- **Establish a *design vehicle*.** The selected design vehicle should be the largest vehicle that accounts for at least 10% of a street's average daily traffic. Selection of the design vehicle should consider the make-up and expectation for traffic flowing through a given intersection. The design vehicle will dictate the minimum turn radius.
- **Establish an *accommodated vehicle for infrequent users*.** The accommodated vehicle is the largest expected vehicle. Use curb and turning radii that allows the accommodated vehicle to use the full street for turns, including parking lanes, bikeways, and adjacent lanes. Consider medians and curb lines as barriers. Restrict parking near intersections and employ recessed stop lines if needed.

Figure 29 – Infrequent accommodated vehicle can encroach into opposing lane



The use of design and accommodated vehicles during design allows more flexibility to adjust designs in favor of pedestrian or bicycle traffic (the most vulnerable users). The following points illustrate options to consider space requirements with this greater latitude.

- Consider the use of tools such as **staggered (offset) stop lines** (where opposing queue storage is adequate) to accommodate vehicles before electing to widen intersection curb alignments.



Figure 30 – Recessed stop bar used where bus must turn right frequently



- The largest frequent user (candidate design vehicle) of most local streets is a 30-foot delivery truck (SU-30). SU-30 vehicles have similar width and wheelbase to a school bus.
- If designing a segment of a designated emergency response route, use appropriate fire apparatus as the accommodated vehicle. In some instances, truck selection might be determined by the fire trucks expected to use the route based on proximity to nearest fire stations.

Table 7 summarizes likely design and accommodated vehicles by context and street type.

Table 7 - Minimum Design Vehicle Standards

|                                                                                                                                                                                  | RESIDENTIAL, INDUSTRIAL <sup>1</sup> ,<br>CB AND GC |       | CC, DOWNTOWN, FORM BASED<br>CODE |              |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------|-------|----------------------------------|--------------|
| Street Type                                                                                                                                                                      | Arterials <sup>2</sup>                              | Local | Arterials <sup>2</sup>           | Local        |
| Design Vehicle<br>(10% or more of ADT)                                                                                                                                           | WB-40                                               | SU-30 | SU-30 & STA 40'<br>bus           | SU-30        |
| Control Vehicle<br>(Infrequent Largest User)                                                                                                                                     | WB-62                                               | WB-62 | Ladder truck                     | Ladder truck |
| <sup>1</sup> Urban streets zoned for industrial uses may require larger design and control vehicles.                                                                             |                                                     |       |                                  |              |
| <sup>2</sup> Intersections of arterials with a local street should use the local street design vehicle unless nearby land uses dictate the need to accommodate a larger vehicle. |                                                     |       |                                  |              |



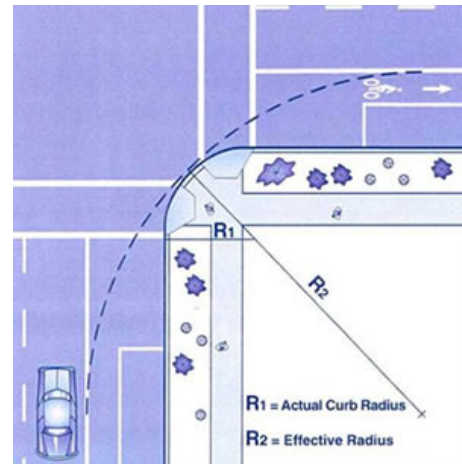
### 3.11-2 Curb Radius

Curb radii influence driver behavior—positively and negatively—affecting turning speeds and the safety of all users. Minimize curb radius based upon the design and accommodated vehicle. Calculate both the actual radius – the radius of the curb itself- and the effective radius, or the wheel track of vehicles. For example, at intersections with on street parking and no curb extensions, the effective radius is much higher than the actual radius. In all cases, consider the widths of the approach and receiving lanes, as crowding may cause poor driver response.

Retrofit existing curbs with curb extensions to reduce actual and effective turning radius. Consider curb extensions whenever on-street parking is present. However, consideration for stormwater flow-lines must be incorporated into design and retrofits.

Curb radius determines turning speed. Use corner radius to keep turning speeds low while allowing the design vehicle to turn.

Figure 31 – Actual vs. Effective Radius



Source: saferoutesinfo.org

Table 8 – Intersection Curb radius and speed

|                                                                                   | RESIDENTIAL, INDUSTRIAL, CB AND GC | CC, DOWNTOWN, FORM BASED CODE |
|-----------------------------------------------------------------------------------|------------------------------------|-------------------------------|
| Actual Radius                                                                     | 20 feet minimum                    | 10 feet minimum               |
| Effective Radius                                                                  | 25 feet minimum                    | 20 feet minimum               |
| Turning Speed <sup>1</sup>                                                        | 10-15 mph                          | 10 mph                        |
| <sup>1</sup> For right turn movements. Left turns will typically be 5 mph faster. |                                    |                               |

### 3.11-3 Bus Bulbs at Intersections

For bus bulbs at intersections, a bulb for a single bus measures 30' long, allowing both doors to open on the bulb, and measures 6-8' wide. On heavy ridership routes where more than one articulated bus platforms several times per day, the bulb measures up to 140' in length. The return angle will be 45 degrees. If the route requires buses to turn right after stopping at a bulb, ensure actual and effective radius meets appropriate bus turning templates.

### 3.11-4 Clear Sight Triangle

For design purposes the clear horizontal sight distance triangle at intersections shall be as described in AASHTO "A Policy on Geometric Design of Highways and Streets", Chapter 9, section on Sight Distance.

For vegetation enforcement purposes, use the clear view triangle shown in SMC 17A.020.030.



### **3.11-5 Roundabouts**

Roundabouts will be reviewed in every case and shall be designed in accordance with WSDOT's design standards. Roundabouts are intended for arterials and collectors. Roundabouts can ease congestion and improve safety at skewed or five-leg intersections.

Typically, roundabouts are larger scale facilities, as they are intended for use along arterials and collectors as previously noted. They facilitate traffic flow without the need for signalization. Roundabouts generally reduce the number of conflict points for vehicles in the intersection and reduce the severity of collisions between vehicles. Design is critical to facilitate safe travel for bicyclists or pedestrians to limit conflicts at the legs of the intersection, as well as to provide needed information for pedestrian alignment and crossing. While vehicle safety is generally improved, improper design can degrade safety for bicycle and pedestrian travel.

Compact urban roundabouts may also be used at city intersections. They have a smaller footprint with and use a completely mountable center island. In many cases existing curb or sidewalk can be left in place.

Preservation work will generally be applied to roundabout pavement surfaces, but implementation of these facilities would qualify as reconstruction.

## **3.12 Signing and Pavement Markings**

### **3.12-1 Traffic Control Signs**

All existing and proposed official traffic control signs required by MUTCD as part of street design shall be shown on the plans, and shall be subject to review and approval by the City Engineer. The plans shall include all existing and proposed signs, show the full width of the street, include any signs on the opposite side of the street, and show existing conditions beyond the proposed development. Prior to construction, shop drawings for all new street signs shall be submitted to Street Maintenance - Signs and Markers for approval.

Preservation and reconstruction work should update signage as appropriate.

Warning and regulatory signs provide motorists with critical information and need to be visible in order to be effective. Provide minimum sight distances according to Table 3-1 in the 2011 AASHTO "A Policy on Geometric Design of Highways and Streets".

### **3.12-2 Pavement Markings**

Design plans for pavement markings shall be approved by the City Engineer prior to construction. Plans shall include all existing and proposed striping, show the full width of the street, and show existing conditions beyond the proposed development. Any existing markings that are to be removed shall be clearly designated.

Preservation and reconstruction work shall incorporate markings for all users of the street as determined within this standard for planned pedestrian, bicycle, and vehicular facilities.

Plastic is the preferred material for pavement markings on Principal and Minor Arterials. Stop lines, crosswalk lines, wide lines (gore stripe), dotted wide lines, dotted bicycle lines, dotted extension lines, arrows, words and symbols shall be preformed thermoplastic. Other lines may be paint with thermoplastic dots according to the City of Spokane Standard Plans.



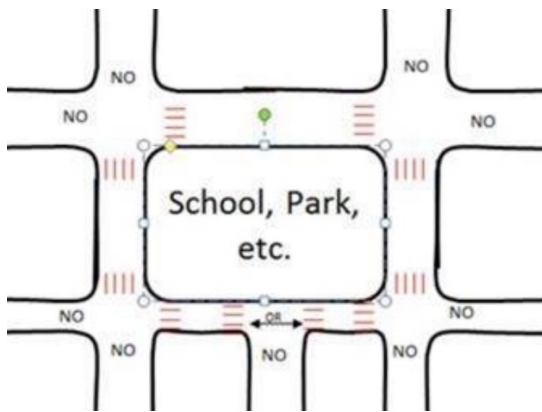
### 3.12-3 Crosswalks

Facilitate safe pedestrian crossings along centers and corridors, and near pedestrian generators. The crosswalk standards are outlined in SMC 17H.010.210 and SMC 17H.010.215. In general these sections of code require the following:

- **Placement.** Provide marked crosswalks along centers and corridors and near schools, parks, hospitals, churches, trail crossings, and other significant pedestrian generating facilities.
- **Design.** In the Downtown, Commercial, Centers and Corridors, and Form Based Code zones, a minimum 6-foot pedestrian refuge at unsignalized crosswalk locations is encouraged where the total crossing is 3 or more automotive lanes.
- **Striping.** Refer to City of Spokane Standard Plans.
- **Stop bar.** Refer to City of Spokane Standard Plans.
- **RRFBs/PHBs.** Install pedestrian-activated tools such as Rectangular Rapid-Flash Pedestrian Beacons and Pedestrian Hybrid Beacons in locations that serve pedestrian generators as ascribed by engineering analysis and approved by the City Engineer. The MUTCD and FHWA-SA-18-018 shall be used as a reference for determining the appropriate crosswalk treatment.

The following exhibit is intended to provide clarification on crosswalk placement based on SMC 17H.010.210.

Figure 32 – Crosswalk placement near schools and parks



## 3.13 Traffic Signals and Intelligent Transportation Systems

### 3.13-1 Traffic Signal Design

Street traffic signals shall be designed with direct coordination and review by the City Street Department. Preservation and reconstruction work should consider traffic signal updates and replacements as appropriate.

- In downtown, use signal progression to promote smooth progression of vehicular traffic at or below the posted speed in an effort to reduce congestion. Work to reduce signal delay on heavily used bike routes.



- Use of Pedestrian Recall is addressed in SMC 16A.84.040.
- In urban areas with heavy pedestrian traffic, consider the use of Leading Pedestrian Intervals (LPI). LPIs add a few seconds of time for pedestrians to establish themselves in the crosswalk before the vehicle signal turns green, enforcing that turning traffic yield to pedestrians. If LPI is used without Accessible Pedestrian Signals the walk interval may need to be increased to aid sight impaired pedestrians who listen for the parallel traffic movement to know when to walk. LPI is addressed in SMC 16A.84.
- Signalized intersections should be re-timed approximately every five years to reduce both air pollution and delay.
- At rehabilitated or new signals, retrofit with Accessible Pedestrian Signals. Prioritize APS installations near concentrations of vulnerable populations, such as near senior centers or hospitals. Intersection APS retrofits are addressed in SMC 16A.84.060.
- Signal interconnection of traffic signals to the Central City Signal Server via fiber optic or copper Ethernet for progressing traffic through an area. New signal and pedestrian hybrid beacon installations should include interconnect infrastructure.

### **3.13-2 Intelligent Transportation Systems**





The City of Spokane uses several types of Intelligent Transportation Systems (ITS) throughout the City to help monitor and manage traffic flow.

- PTZ cameras provide live video feeds to the regional traffic management center and are used by city staff to monitor traffic conditions, adjust signal timing, and perform studies. Additional fixed cameras provide telemetry at several intersections throughout the City.
- Permanent count stations are located throughout the City. These provide count information throughout the year.
- Over 95% of the City's traffic signals communicate with a central server via Ethernet over copper or fiber. Remote access is also available to all City owned PTZ, fixed cameras and dynamic message signs.
- Dynamic Message Signs have been installed in key arterial locations within the city to display messages related to traffic control and safety.
- Flashing school beacons have been installed at most of the schools in the city limits to provide real-time information to drivers on the times the 20 mph speed limit is in effect.
- Speed feedback signs have been installed through the traffic calming program. Some models can provide count and speed data.
- Bike and pedestrian count stations are installed on select regional trails within the city and provide time of day, weekday vs. weekend and season count data for use in planning.
- Remote Weather Information System (RWIS) units provide information on air temperature, humidity, dew point and road surface temperature. One is currently installed on the south hill.
- Bluetooth/WiFi readers are used to monitor corridor travel times on Maple/Ash, Division, Freya/Greene/Market, and US 2 in cooperation with the Spokane Regional Traffic Management Center.

## **3.14 Reference Tables**



Table 1 Street Dimensions

| Table 1 Street Dimensions                                                          |                            |           |                                     |                                                                                     |                                         |         |                           |                                                                                     |                                        |                                      |                                 |                                                                                     |      |
|------------------------------------------------------------------------------------|----------------------------|-----------|-------------------------------------|-------------------------------------------------------------------------------------|-----------------------------------------|---------|---------------------------|-------------------------------------------------------------------------------------|----------------------------------------|--------------------------------------|---------------------------------|-------------------------------------------------------------------------------------|------|
| (Required)<br>PEDESTRIAN REALM                                                     |                            |           |                                     | (Recommended)<br>FLEXIBLE AREA <sup>e</sup>                                         |                                         |         |                           | (Required)<br>VEHICLE REALM                                                         |                                        |                                      |                                 | (Recommended)<br>MEDIAN                                                             |      |
|  |                            |           |                                     |  |                                         |         |                           |  |                                        |                                      |                                 |  |      |
| Sidewalk Zone <sup>b</sup>                                                         | Buffer Zone <sup>c,d</sup> | Curb Zone | Opt. Shared Use Path <sup>f,g</sup> | Stormwater Management <sup>h</sup>                                                  | Curb Extensions/ Bus Bulbs <sup>i</sup> | Parking | Bicycle Lane <sup>g</sup> | Bicycle Buffer <sup>j</sup>                                                         | Vehicle Zone Outer Lane <sup>k,l</sup> | Vehicle Zone Inner Lane <sup>k</sup> | Vehicle Zone Left Turn or TWLTL | Median <sup>m</sup>                                                                 |      |
| Downtown DTC, DTG, DTU, DTS; Form Based Code CA1, CA2, CA3, CA4                    |                            |           |                                     |                                                                                     |                                         |         |                           |                                                                                     |                                        |                                      |                                 |                                                                                     |      |
| Urban Principal Arterial                                                           | 7                          | 5         | 0.5                                 | 12                                                                                  | Varies                                  | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 11                                                                                  | 6-20 |
| Urban Minor Arterial                                                               | 7                          | 5         | 0.5                                 | 12                                                                                  | Varies                                  | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 11                                                                                  | 6-20 |
| Urban Major/Minor Collector                                                        | 7                          | 5         | 0.5                                 | 12                                                                                  | Varies                                  | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 10                                                                                  | 6-20 |
| Urban Local Access                                                                 | 7                          | 5         | 0.5                                 | 12                                                                                  | Varies                                  | NA      | 7                         | 6                                                                                   | NA                                     | 10                                   | NA                              | NA                                                                                  | 6-20 |
| Commercial O, OR, NR, NMU, CB, GC                                                  |                            |           |                                     |                                                                                     |                                         |         |                           |                                                                                     |                                        |                                      |                                 |                                                                                     |      |
| Urban Principal Arterial                                                           | 7                          | 5         | 0.5                                 | 12                                                                                  | 10                                      | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 11                                                                                  | 6-20 |
| Urban Minor Arterial                                                               | 7                          | 5         | 0.5                                 | 12                                                                                  | 10                                      | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 11                                                                                  | 6-20 |
| Urban Major/Minor Collector                                                        | 7                          | 5         | 0.5                                 | 12                                                                                  | 10                                      | 7       | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 10                                                                                  | 6-20 |
| Urban Local Access                                                                 | 7                          | 5         | 0.5                                 | 12                                                                                  | 6.5                                     | NA      | 7                         | 6                                                                                   | NA                                     | 10                                   | NA                              | NA                                                                                  | 6-20 |
| Residential RA, RSF, RSF-C, RTF, RMF, RHD                                          |                            |           |                                     |                                                                                     |                                         |         |                           |                                                                                     |                                        |                                      |                                 |                                                                                     |      |
| Urban Principal Arterial                                                           | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 10                                                                                  | 6-20 |
| Urban Minor Arterial                                                               | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 10                                                                                  | 6-20 |
| Urban Major/Minor Collector                                                        | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | 8                         | 6                                                                                   | 1.5-6'                                 | 11                                   | 11                              | 10                                                                                  | 6-20 |
| Urban Local Access                                                                 | 5                          | 6         | 0.5                                 | 12                                                                                  | 6.5                                     | NA      | 7                         | 6                                                                                   | NA                                     | 10                                   | NA                              | NA                                                                                  | 6-20 |
| Industrial LI, HI, PI                                                              |                            |           |                                     |                                                                                     |                                         |         |                           |                                                                                     |                                        |                                      |                                 |                                                                                     |      |
| Urban Principal Arterial                                                           | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | NA                        | 6                                                                                   | 3                                      | 12                                   | 12                              | 12                                                                                  | 6-20 |
| Urban Minor Arterial                                                               | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | 8                         | 6                                                                                   | 3                                      | 12                                   | 12                              | 12                                                                                  | 6-20 |
| Urban Major/Minor Collector                                                        | 5                          | 6         | 0.5                                 | 12                                                                                  | 10                                      | NA      | 8                         | 6                                                                                   | 1.5-6'                                 | 12                                   | 12                              | 12                                                                                  | 6-20 |
| Urban Local Access                                                                 | 5                          | 6         | 0.5                                 | 12                                                                                  | 6.5                                     | NA      | 7                         | 6                                                                                   | NA                                     | 11                                   | NA                              | NA                                                                                  | 6-20 |

A. Table 1 dimensions are target values and minor adjustments are allowed to fit the street context. See discussion in section 3.2-2 for further detail.

B. See SMC 17H.010 for exceptions to residential sidewalk requirements. In locations where existing sidewalks exceed the dimension in Table 1, the sidewalk width should be maintained with redevelopment or street improvement.

C. Per SMC 17C.200.050-1, a tree-planted continuous buffer requires a 5-foot minimum width for commercial zones. For residential and industrial zones, the minimum increases to 6 feet. Alternatively, a narrower buffer may be used in select zones if tree vaults are implemented.

D. Buffers in commercial areas may be planted or concrete. When stormwater disposal is a governing concern, consideration should be given to use pervious surfaces.

E. The flexible area includes a menu of options which are chosen based on what makes most sense according to city plans, environmental responsibilities, and context. In some cases, none of these will fit within the project. Only in very rare cases will more than one fit - for instance, a parking lane plus bio-retention swale.

F. In places designated for shared-use paths, the path can take the place of the sidewalk zone.

G. Consult section 3.5 of this document for guidance on facility type and selection. Possible facilities include bike lanes, buffered bike lanes and parking protected bikes lanes. Physical or grade-separation may be preferred depending on conditions. Bicycle facilities may operate in the Flexible Area or the Vehicle Realm. Bicycle boulevards and shared roadways are possibilities on local access street.

H. Consult the Spokane Regional Stormwater Manual and/or Eastern Washington Low Impact Development Guidance Manual for desired locations for stormwater facilities. The stormwater catchment area must meet the required volume generated by the planned impervious area. In Downtown, Form Based Code or Center & Corridor zoning roadside swales are less common and alternative stormwater facilities in accordance with the above identified manuals may be considered or stormwater piped to another location.

I. On transit corridors, use bus bulbs if space allows to ease boarding, reduce sidewalk congestion, and allow buses to easily re-enter traffic. This should typically be done only if there is a second lane for vehicles to continue around stopped buses.

J. "High Traffic" and "Medium Traffic" lane routes on the Master Bicycle Plan should include buffers. Separation buffer between bike lane and vehicle lane should be implemented via parallel lane edge stripes with a periodic cross-hatch. 3' is the minimum buffer unless a raised curb is used, in which case 1.5' is the minimum. Wider buffers are allowable but should be well marked with hatching or bollards.

K. When constraints are prohibitive, consider 10-foot lane width as the minimum.

L. Travel lane includes the width of the gutter pan, if integral curb and gutter is used.

M. Medians less than 6 feet wide are considered traffic channelization. A pedestrian refuge is a raised median with a minimum width of 6 feet. Wider medians may be implemented in the context of boulevards.



**Table 2 – Vertical Curve Design Parameters**

|                                                                                                                                                                                                                                                                                                                                                                                         | ARTERIALS<br>(all types) | LOCAL  | ALLEY  | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|--------|--------|------------------------------------|
| Minimum Design Speed <sup>1</sup>                                                                                                                                                                                                                                                                                                                                                       | 30 mph                   | 25 mph | 20 mph | 20 mph                             |
| Vertical Curves <sup>2</sup> are required if the Algebraic Grade Difference, A, is:                                                                                                                                                                                                                                                                                                     | A>1%                     | A>2%   | A>2%   | A>2%                               |
| Minimum Length is 3 times the Design Speed                                                                                                                                                                                                                                                                                                                                              |                          |        |        |                                    |
| <sup>1</sup> Design speed is posted speed. In practice speeds may be less or more than shown depending on other design factors not accounted for herein. The design engineer shall justify the use of values other than those listed above.<br><sup>2</sup> Curves must meet stopping sight distance per AASHTO 2011. "K" of 167 is used to find the maximum curve length for drainage. |                          |        |        |                                    |

**Table 3 – Side slopes**

|                                                                                                | ARTERIALS    | LOCALS       | ALLEYS       | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|------------------------------------------------------------------------------------------------|--------------|--------------|--------------|------------------------------------|
| Grade break at back of walk<br>up<br>down                                                      | 4:1<br>4:1   | 1.5:1<br>2:1 |              |                                    |
| Grade break at back of walk<br>up<br>down                                                      | 1.5:1<br>2:1 | 1.5:1<br>2:1 |              |                                    |
| Grade break at edge of pavement<br>up<br>down                                                  |              |              | 1.5:1<br>2:1 | 1.5:1<br>2:1                       |
| Grade break at edge of traveled<br>way, including any shoulders<br>up<br>down                  |              |              | 1.5:1<br>2:1 | 1.5:1<br>2:1                       |
| Notes:<br>Use WSDOT standards when curbs do not exist.<br>Grades shown are horizontal:vertical |              |              |              |                                    |



**Table 4 – Target Speeds by Street Type**

|                                                        | RESIDENTIAL, INDUSTRIAL, CB<br>AND GC |                   |           |       | CC, DOWNTOWN, FORM BASED CODE |                   |           |       |
|--------------------------------------------------------|---------------------------------------|-------------------|-----------|-------|-------------------------------|-------------------|-----------|-------|
| Street Type                                            | Principal<br>Arterial                 | Minor<br>Arterial | Collector | Local | Principal<br>Arterial         | Minor<br>Arterial | Collector | Local |
| Design Speed =<br>Posted Speed =<br>Target Speed (mph) | 30-35                                 | 30-35             | 30        | 25    | 20-30                         | 20-30             | 20-30     | 20-25 |

**Table 5 – Minimum Clear Zone (distance from edge of traveled way)**

|                                                                     | Posted Speed<br>20-35mph                   |                                    | Posted Speed<br>40 or above                |                                    |
|---------------------------------------------------------------------|--------------------------------------------|------------------------------------|--------------------------------------------|------------------------------------|
|                                                                     | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> | Existing Fixed<br>Objects <sup>(2,3)</sup> | New Fixed<br>Object <sup>(2)</sup> |
| State Highways                                                      | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 | WSDOT <sup>1</sup>                         | WSDOT <sup>1</sup>                 |
| New street construction                                             | n/a                                        | 4                                  | n/a                                        | 10                                 |
| Street reconstruction including width<br>or profile adjustments     | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| Street reconstruction not including<br>width or profile adjustments | 1.5                                        | 4                                  | 6                                          | 10 <sup>1</sup>                    |
| New installations not related to<br>street construction             | n/a                                        | 4                                  | n/a                                        | 10 <sup>1</sup>                    |

<sup>1</sup> If 10 feet clear distance cannot be provided within the available right-of-way, the design engineer may evaluate and justify placement as near the outer edge of the right-of-way as practical.

<sup>2</sup> On a curbed street all fixed objects shall be at least 1.5 behind curb regardless of the location of the travelled way. This is to ensure clearance for parked vehicle doors, snow removal, sign overhang, etc.

<sup>3</sup> Fixed objects / trees with less than 1.5 feet clearance should be considered for removal or relocation. If clearance is between 1.0 and 1.5 feet existing fixed objects including trees may remain unless damage indicates a history of vehicle collision, the object or tree conflicts with the condition or operation of a street, alley or sidewalk, or removal/relocation is required due to other public safety, convenience or aesthetic considerations.



**Table 6 – Street Profile Grades**

|                                                                                                                                                                                                                                                                                                                                                                                                                                           | ARTERIALS | LOCALS            | ALLEYS | BICYCLE /<br>PEDESTRIAN<br>PATHWAY |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|-------------------|--------|------------------------------------|
| Minimum Profile Grade                                                                                                                                                                                                                                                                                                                                                                                                                     | 0.8%      | 0.8% <sup>1</sup> | 0.8%   | 0.8%                               |
| Maximum Profile Grade                                                                                                                                                                                                                                                                                                                                                                                                                     | 8.0%      | 8.0%              | 8.0%   | 8.0%                               |
| Grade at Intersections <sup>2</sup>                                                                                                                                                                                                                                                                                                                                                                                                       | n/a       | n/a               | n/a    | n/a                                |
| <sup>1</sup> Cul-de-sac profiles shall be established to provide minimum one percent grades at all places along the gutter lines.<br><sup>2</sup> Unless otherwise approved by the Engineer, the profile grade at all residential intersections, along the minor roadway at arterials, and for all roadways at controlled intersection shall be no greater than three percent at any point within 100 feet of the near end of the radius. |           |                   |        |                                    |

**Table 7 – Minimum Design Vehicle Standards**

|                                                                                                                                                                                                                                                                                          | RESIDENTIAL, INDUSTRIAL <sup>1</sup> ,<br>CB AND GC |       | CC, DOWNTOWN, FORM BASED<br>CODE |              |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------|-------|----------------------------------|--------------|
| Street Type                                                                                                                                                                                                                                                                              | Arterials <sup>2</sup>                              | Local | Arterials <sup>2</sup>           | Local        |
| Design Vehicle<br>(10% or more of ADT)                                                                                                                                                                                                                                                   | WB-40                                               | SU-30 | SU-30 & STA 40'<br>bus           | SU-30        |
| Control Vehicle<br>(Infrequent Largest User)                                                                                                                                                                                                                                             | WB-62                                               | WB-62 | Ladder truck                     | Ladder truck |
| <sup>1</sup> Urban streets zoned for industrial uses may require larger design and control vehicles.<br><sup>2</sup> Intersections of arterials with a local street should use the local street design vehicle unless nearby land uses dictate the need to accommodate a larger vehicle. |                                                     |       |                                  |              |

**Table 8 – Curb radius standard**

|                                                                                   | RESIDENTIAL, INDUSTRIAL, CB AND GC | CC, DOWNTOWN, FORM BASED CODE |
|-----------------------------------------------------------------------------------|------------------------------------|-------------------------------|
| Actual Radius                                                                     | 20 feet minimum                    | 10 feet minimum               |
| Effective Radius                                                                  | 25 feet minimum                    | 20 feet minimum               |
| Turning Speed <sup>1</sup>                                                        | 10-15 mph                          | 10 mph                        |
| <sup>1</sup> For right turn movements. Left turns will typically be 5 mph faster. |                                    |                               |



**Table 9 – Profile grade of sidewalks and buffer strips**

|                                                                      | All Zoning                   |            |
|----------------------------------------------------------------------|------------------------------|------------|
| Street Type                                                          | Arterials                    | Local      |
| Sidewalk Cross Slope                                                 | 1.5% to 2%                   | 1.5% to 2% |
| Sidewalk Profile Grade<br>Contiguous with curb<br>Isolated from curb | Same grade as street profile |            |
|                                                                      | 5% max                       | 5% max     |





## **PLAN COMMISSION FINDINGS OF FACT, CONCLUSIONS, AND RECOMMENDATIONS ON THE STREET ENGINEERING DESIGN STANDARDS CHAPTER 3 AND SMC UPDATES**

**A Recommendation of the Spokane Plan Commission to the City Council to APPROVE proposed updates to Chapter 3 of the City of Spokane Street Engineering Design Standards and accompanying revisions to Spokane Municipal Code (SMC) 17A.020, 17C.200 and 17H.010.**

### **FINDINGS OF FACT:**

- A. The City of Spokane adopted a Comprehensive Plan in May of 2001 that complies with the requirements of the Growth Management Act (GMA).
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs provide for implementation of the transportation section of the Comprehensive Plan.
- C. Proposed updates to Chapter 3 Design Standards –Streets, Alleys, Bikeways & Sidewalks include updates to street geometrics, stormwater facilities, bicycle facilities, access management, clear zones, design vehicles, and street lighting.
- D. Proposed revisions to SMC 17A.020 Definitions address roadway surfaces, arterial street types, bicycle facilities, clear zones, curb ramps, parkways, and shared-use pathways.
- E. Proposed revisions to SMC 17C.200 Street Tree Requirements, 12.01 and 12.02 tree grates and overhanging vegetation.
- F. Proposed revisions to SMC 17H.010 Engineering Standards address transit as a street use, Transportation Impact Fees, right-of-way width, local street widths, on-street parking, snow storage, medians, traffic calming, shared-use pathways, pedestrian buffer strips, curb ramps, street lighting, horizontal clearances, and the bicycle network.
- G. Pursuant to SMC 04.12.010 the Plan Commission shall hold public hearings and make recommendations to the City Council regarding development regulations implementing the Comprehensive Plan.
- H. On November 5, 2019, July 21, 2020, and August 11, 2020, the Plan Commission Transportation Subcommittee received a presentation on proposed updates to Chapter 3 of the Design Standards and corresponding SMC updates.
- I. On November 18, 2019, a group of public stakeholders in the disability and active transportation communities were gathered for an All Ages and Abilities Focus Discussion, and received a presentation on proposed updates to Chapter 3 Design Standards and related revisions to SMCs.
- J. On November 22, 2019, a group of public stakeholders in the development community received a presentation on the proposed updates to Chapter 3 Design Standards and the related revisions to SMCs.



- K. On June 24, 2020, the Design Review Board of the City of Spokane received a presentation regarding the proposed updates to the street design standards as addressed in Chapter 3 Design Standards, and corresponding revisions to related SMCs.
- L. On July 16, 2020, the Washington State Department of Commerce and appropriate state agencies were given the required 60-day notice of intent to adopt before adoption of any proposed changes to development regulations.
- M. On July 22, 2020, a State Environmental Policy Act (SEPA) checklist was issued with a request for comments from agencies, departments, and neighborhood councils. The deadline to comment was August 10, 2020. The City received several comments on the documents and made revisions.
- N. The City Plan Commission held workshops on July 22<sup>nd</sup> and August 12<sup>th</sup>, 2020, to obtain public comments on Chapter 3 of the Design Standards and the SMC updates.
- O. On March 23<sup>rd</sup>, 2020 and August 24<sup>th</sup>, 2020, the Public Infrastructure, Environment, and Sustainability Committee of the City Council received presentations on Chapter 3 of the Design Standards and related SMC updates.
- P. On August 31, 2020, a Determination of Non-Significance (DNS) was issued for the Proposal. The deadline to appeal the determination was September 22, 2020. No comments on the SEPA determination were received.
- Q. The City Plan Commission held a public hearing beginning on September 23<sup>rd</sup>, 2020 and continuing on October 14<sup>th</sup>, 2020 to consider Chapter 3 of the Design Standards and related SMC updates.
- R. The City Council must receive a recommendation from the City Plan Commission to certify that update to Chapter 3 of the Engineering Design Standards and accompanying revisions to SMCs are in conformance with the City's Comprehensive Plan in effect on the day of certification.

#### **CONCLUSIONS:**

- A. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been prepared in full consideration of the City's Comprehensive Plan.
- B. Chapter 3 of the Engineering Design Standards and the accompanying SMCs have been reviewed by the City Plan Commission and found to be in conformance with the goals and policies of the City's 2001 Comprehensive Plan.

#### **RECOMMENDATIONS:**

- A. The Spokane City Plan Commission is certifying that the update to Chapter 3 of the Engineering Design Standards and accompanying Spokane Municipal Code revisions



are in conformance with the City of Spokane's Comprehensive Plan as required by RCW 36.70A and are recommended for adoption by the Spokane City Council.

- B. By a vote of 8 to 0, the Plan Commission recommends the approval of these amended documents by the City Council.

  
Todd Beyreuther (Nov 3, 2020 12:40 PST)

Nov 3, 2020

---

**Todd Beyreuther, President**  
Spokane Plan Commission  
October \_\_, 2020








# Plan Commission Engineering Design Standards

Final Audit Report

2020-11-03

|                 |                                               |
|-----------------|-----------------------------------------------|
| Created:        | 2020-11-03                                    |
| By:             | Jackie Churchill (jchurchill@spokanecity.org) |
| Status:         | Signed                                        |
| Transaction ID: | CBJCHBCAABAA0A-sozYn37EwtYYE0cyKINZRAQ-YN7v_  |

## "Plan Commission Engineering Design Standards" History

-  Document created by Jackie Churchill (jchurchill@spokanecity.org)  
2020-11-03 - 6:37:05 PM GMT- IP address: 73.83.158.109
-  Document emailed to Todd Beyreuther (tbeyreuther@spokanecity.org) for signature  
2020-11-03 - 6:38:52 PM GMT
-  Email viewed by Todd Beyreuther (tbeyreuther@spokanecity.org)  
2020-11-03 - 8:39:02 PM GMT- IP address: 107.77.205.172
-  Document e-signed by Todd Beyreuther (tbeyreuther@spokanecity.org)  
Signature Date: 2020-11-03 - 8:40:04 PM GMT - Time Source: server- IP address: 107.77.205.172
-  Agreement completed.  
2020-11-03 - 8:40:04 PM GMT





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---



**Agenda Sheet for City Council Meeting of:**

11/23/2020

**Date Rec'd**

11/12/2020

**Clerk's File #**

ORD C35987

**Renews #****Cross Ref #****Project #****Bid #****Requisition #****Submitting Dept**

CITY COUNCIL

**Contact Name/Phone**

GIACOBBE BYRD 6715

**Contact E-Mail**

GBYRD@SPOKANECITY.ORG

**Agenda Item Type**

First Reading Ordinance

**Agenda Item Name**

0320 PARK RULES AND REGULATIONS TITLE 12 AMENDMENTS

**Agenda Wording**

Park Rules and Regulations Title 12 Amendments

**Summary (Background)**

This ordinance makes amendments to Title 12 of the Spokane Municipal Code. See the attached briefing paper for more information.

Lease? NO

Grant related? NO

Public Works? NO

**Fiscal Impact**

Select \$

**Budget Account**

#

Select \$

#

Select \$

#

Select \$

#

**Approvals****Dept Head**

ALLERS, HANNAHLEE

**Division Director****Finance**

HUGHES, MICHELLE

**Legal**

PICCOLO, MIKE

**For the Mayor**

ORMSBY, MICHAEL

**Council Notifications****Study Session\Other**

9/14 Urban Experience

**Council Sponsor**

CM Kinnear

**Distribution List**

mmuramatsu@spokanecity.org

gjones@spokanecity.org

**Additional Approvals****Purchasing**



## Briefing Paper (Finance & Administration)

|                                                                                                                                  |                                                                                                                               |
|----------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                | City Council / Parks                                                                                                          |
| <b>Subject:</b>                                                                                                                  | Park Rules and Regulations Title 12 Amendments                                                                                |
| <b>Date:</b>                                                                                                                     | 11/09/2020                                                                                                                    |
| <b>Contact (email &amp; phone):</b>                                                                                              | Giacobbe Byrd; <a href="mailto:gbyrd@spokanecity.org">gbyrd@spokanecity.org</a> ; (509) 625-6715                              |
| <b>City Council Sponsor:</b>                                                                                                     | Lori Kinnear                                                                                                                  |
| <b>Executive Sponsor:</b>                                                                                                        | Garrett Jones                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                    | Urban Experience; Public Safety and Community Health                                                                          |
| <b>Type of Agenda item:</b>                                                                                                      | <input type="checkbox"/> Consent <input checked="" type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan) | Strategic Plan                                                                                                                |
| <b>Strategic Initiative:</b>                                                                                                     | Safe and Healthy                                                                                                              |
| <b>Deadline:</b>                                                                                                                 | 11/30/2020                                                                                                                    |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                              | Create safer City Parks by updating park rules and regulations                                                                |

### Background/History:

Three basic issues have caused the Parks Department to review and update the Park Rules:

1. Current park rules are silent with respect to events and other activities that commonly occur in city parks. Some activities are prohibited by law while others, including many events, require a permit. Regularly reported activity suggests that certain laws and rules may not be clear.
2. The Parks Department has a process for events that require park reservations and for those requiring special event permits, yet current park rules provide no mention of those processes.
3. City services are sometimes required to address the cleanup and other impacts of special events. Current park rules lack a framework for cost recovery for cleanup and make no reference to the cost recovery provisions of Chapter 10.39 SMC.

### Executive Summary:

To address the aforementioned gaps, the ordinance makes important changes to the Park Rules, as summarized below.

1. Clarifies that the prohibition against the use and distribution of drug paraphernalia applies not only to parks, but also to their adjacent sidewalks and in parked vehicles next to a park. This new rule is consistent with current law and creates no new crime, but it specifies the scope of the restriction as related to public parks, closing an important gap on an illegal activity that has had a staggeringly negative impact on neighborhoods with nearby parks.
2. Adds the non-criminal penalty of park exclusion for violators who distribute drug paraphernalia in a park.
3. Adds a reference to special events that require a permit. Includes cross-references to the Parks Department procedures for special events and to the special event ordinance, Chapter



10.39 SMC, and also cross-references the food vendor permitting process under the Municipal Code (Chapter 10.51 SMC).

4. Acknowledges that events not requiring a special event permit may still require a park reservation. Clarifies that “there is no cost to submit reservation application for events that do not include the use of a shelter, but the application requires the submission of a clean-up plan and may be subject to cost recovery for the actual costs of clean-up by park employees.”
5. Prohibits driving any motor vehicle in Riverfront Park without express permission from the director of the parks department. Lists certain sorts of motorized vehicles that are exempt.
6. Clarifies that, pursuant to SMC 16A.61.577, the City has the authority to impound cars parked in turf areas or parked overnight on other park property.

**Budget Impact:**

Approved in current year budget? ☐ Yes ☐ No ☒ N/A

Annual/Reoccurring expenditure? ☐ Yes ☐ No ☒ N/A

If new, specify funding source:

Other budget impacts: (revenue generating, match requirements, etc.)

**Operations Impact:**

Consistent with current operations/policy? ☐ Yes ☒ No ☐ N/A

Requires change in current operations/policy? ☒ Yes ☐ No ☐ N/A

Specify changes required:

Known challenges/barriers:



## ORDINANCE NO. C35987

An ordinance relating to Parks; amending Section 12.06A.040 of the Spokane Municipal Code concerning park rules and regulations.

**NOW, THEREFORE,** the City of Spokane does ordain:

**Section 1.** That Section 12.06A.040 SMC is amended to read as follows:

### Section 12.06A.040 Rules and Regulations

The Park Board has established rules governing behavior on park property, and such rules may be enforced consistent with this ordinance.

Except when done in places designated and in the manner prescribed by rule, regulation or special permission of the park board or department:

#### A. Park Grounds and Maintenance

1. No person may cut, trim, tag or in any way tamper with the trees or landscaping, or dig, stake, pierce or penetrate the ground of any park.

#### B. Vehicles and Watercraft

1. No person may ride or drive any motor vehicle in Riverfront Park without express permission from the director of the parks department or his or her designee. Nothing in this section shall be construed as prohibiting the use of electric scooters, electric bicycles or other personal electric mobility devices in Riverfront Park.

- ~~((4-))~~ 2. No person may drive or ride any vehicle or animal on the grass or in any areas of the park other than designated drives, ways, boulevards or paths. Nothing in this section shall be construed as prohibiting a person from riding a mountain bike on established paths and trails in natural/conservation/undeveloped areas.

- ~~((2-))~~ 3. No person may park outside designated parking areas. Cars parked in turf areas, parked overnight or left for multiple days in parking lots may be deemed unauthorized pursuant to SMC 16A.61.577 and impounded by a registered tow truck operator at the direction of a law enforcement officer or other public official with jurisdiction.

- ~~((3-))~~ 4. No person may operate or drive any vehicle, including bicycles, skateboards and roller skates, in a manner which is likely to endanger persons and/or property.

- ~~((4-))~~ 5. No person may intentionally enter, swim, dive or float, with or without a boat, raft, craft or other flotation device, in or upon any pond in a park or the Spokane



River at any point between the west line of the Division Street Bridge and the west line of the Monroe Street Bridge.

C. Speed

1. No person may ride or drive a vehicle at a speed in excess of five miles per hour in Riverfront Park.
2. At all parks other than Riverfront Park, no person may ride or drive a vehicle at a speed in excess of fifteen miles per hour unless otherwise posted.

D. Games and Athletics

1. No person may engage in, conduct, or hold any trials or competitions for speed, endurance, or hill climbing involving any vehicle, boat, aircraft, or animal in any park, except by permission of the director of the parks department or his or her designee.
2. No person may play or practice any game that involves the running or the throwing or hitting of a ball or other projectile such as golf, archery, hockey, tennis or baseball, when and where such activity is likely to be dangerous.
3. No person may operate remote controlled vehicles, unmanned air systems or other hobby craft in a manner that is dangerous to persons or property.
4. Swimming pools, wading pools, golf courses, softball diamonds and basketball courts may be used only during hours designated by the director of the parks department or his or her designee.

E. Animals

1. No person may allow any animal to run at large in any park or enter any pond, pool, fountain or stream thereof except within a designated off-leash area. A violation of this section is a class 4 civil infraction.
2. All persons bringing pets to a park must provide for the disposal of animal waste from their pets. Failure to do so is a class 4 infraction.
3. No person may tease, annoy, disturb, attack, catch, injure, or kill, throw stones or any object at, or strike with any stick or weapon, any animal, bird, fowl or other wildlife in any park.
4. Fishing shall be allowed in rivers and creeks adjacent to parks, but shall not be allowed in the ponds of any park.



5. No person may feed any wildlife in any park. A violation of this section is a class 4 civil infraction.

#### F. Drugs and Alcohol

1. Except as specifically authorized by the director of the parks department or his or her designee, no person shall open the package containing liquor or consume liquor in a public park. A violation of this section is a class 3 civil infraction.
2. As provided in RCW 69.50.445, it is unlawful to open a package containing marijuana, useable marijuana, marijuana-infused products, or marijuana concentrates, or consume marijuana, useable marijuana, marijuana-infused products, or marijuana concentrates, in any park. A violation of this section is a class 3 civil infraction.
3. It is unlawful for any person in or adjacent to a city park, including in parked vehicles or on sidewalks on both sides of the adjacent street, to use drug paraphernalia in violation of RCW 69.50.412 and SMC 10.15A.020. A violation of this section is a misdemeanor.
4. No person shall sell or give, or permit to be sold or given, to any person any drug paraphernalia in any form, including hypodermic syringes, needles and other objects used, intended for use or primarily designed for use in injecting unlawful drugs or controlled substances into the human body, in or adjacent to any park. A violation of this section is a class 1 civil infraction and violators may be subject to exclusion from one or more city parks for one year.

#### G. Weapons and Projectiles

1. Any person who possesses a dangerous weapon as defined in RCW 9.41.250 is guilty of a gross misdemeanor, except when lawfully carrying firearms consistent with state law.
2. No person may shoot, fire, throw or explode any fireworks, explosive, bow and arrow, slingshot or other weapon, toy or real, which discharges a pellet or other object with harmful force.

#### H. Food

1. Except as provided in SMC 10.51.040(A), no person may sell food inside or adjacent to a park without first obtaining the following:
  - a. Written authorization from the director of the parks department, or his or her designee, to vend at a particular location or locations, as required by SMC 10.51.070 and SMC 17C.390.030; and



- b. A valid a mobile food vendor's permit as required by SMC 10.51.010.

#### I. Events

1. Special events held in a city park require a park reservation and must also be authorized by a special event permit issued by the director of the parks department under the procedures and requirements for special events as provided in Chapter 10.39 SMC.
2. Regardless of whether an event requires a special event permit, park reservations are required to reserve park space and to serve or distribute food for groups of over fifteen people. There is no cost to submit reservation application for events that do not include the use of a shelter, but the application requires the submission of a clean-up plan and may be subject to cost recovery for the actual costs of clean-up by park employees.

#### ~~((H.))~~ J Other Uses of Park Property and Facilities

1. No person may use or occupy park property to sleep, store property or for any other purpose when done in a manner that obstructs or prevents others from its use and enjoyment. A violation of this provision is a misdemeanor.
2. No person may build a fire in a park during official burn bans or where fire restrictions are otherwise imposed. All fires must be contained to designated fireplaces and park-supplied barbecue pits.
3. Where the park board has provided for the collection of fees, rents or charges for the use of park facilities, including municipal golf courses, no person may enter upon or use such park facilities without paying such required fees, rents or charges.
4. No person may be in a City park during the hours of closure without the express permission of the director of the parks department or his or her designee. All City parks shall be closed from ten p.m. to six a.m., except Riverfront Park, which shall be closed from midnight to six a.m. throughout the year.
5. No person may sell or barter any goods or services without prior permission of the director of the parks department or his or her designee.

- ~~((I.))~~ K. No person may violate such rules and regulations as may from time to time be promulgated by the park board or the director of parks and recreation pursuant to and in supplementation of the City Charter and this code.



**Section 2.** That Section 16A.61.577 SMC is amended to read as follows:

Section 16A.61.577 Impoundment of Unauthorized Vehicles on Public Property

A. Definitions

1. "Impound" means to take and hold a vehicle in legal custody. There are two types of impounds—public and private.
2. "Public impound" means that the vehicle has been impounded at the direction of a law enforcement officer or by a public official having jurisdiction over the public property upon which the vehicle was located.
3. "Public Property" means any street, road, public highway or other publicly owned property.
4. "Unauthorized vehicle", for purposes of this section, means a vehicle that is subject to impoundment after being left unattended in one of the following circumstances:
  - a. Constituting an accident or a traffic hazard as defined in RCW 46.55.113 . . . . Immediately
  - b. On a highway and tagged as described in RCW 46.55.085 . . . . 24 hours
  - c. In a publicly owned or controlled parking facility, properly posted under RCW 46.55.070 . . . . Immediately
  - d. Outside the designated parking areas of a public park, or parked overnight or left for multiple days in the parking lot of a public park.
  - ~~((d-))~~ e. In violation of any of the restrictions subject to vehicle impoundment under Chapter 16A.61 SMC.

- B. If a vehicle is in violation of the time restrictions of RCW 46.55.010(14) as set forth in subsections (4)(a) through (4)(c) above, or is in violation of any of the restrictions subject to vehicle impoundment set forth in section (4)(d) above, it may be impounded by a registered tow truck operator at the direction of a law enforcement officer or other public official with jurisdiction if the vehicle is on public property.
- C. In addition to law enforcement officers, the Director of Developer Services and/or Parking Enforcement, or his or her designee, is a public official with jurisdiction



over the public property and with authority to authorize impoundment of unauthorized vehicles on public property.

- D. The impoundment of unauthorized vehicles on public property under this section shall incorporate all procedures related to vehicle impoundment as set forth in Chapter 46.55 RCW. Chapter 46.55 RCW, as now enacted or hereinafter amended, is hereby adopted by reference as if fully set forth herein.

PASSED by the City Council on \_\_\_\_\_.

\_\_\_\_\_  
Council President

Attest:

Approved as to form:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Assistant City Attorney

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Date

\_\_\_\_\_  
Effective Date





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---





**Agenda Sheet for City Council Meeting of:**  
11/02/2020

|                              |               |
|------------------------------|---------------|
| <b><u>Date Rec'd</u></b>     | 10/13/2020    |
| <b><u>Clerk's File #</u></b> | OPR 2020-0781 |
| <b><u>Renews #</u></b>       |               |
| <b><u>Cross Ref #</u></b>    |               |
| <b><u>Project #</u></b>      |               |
| <b><u>Bid #</u></b>          |               |
| <b><u>Requisition #</u></b>  | RE19678       |

|                                  |                          |
|----------------------------------|--------------------------|
| <b><u>Submitting Dept</u></b>    | FLEET OPERATIONS         |
| <b><u>Contact Name/Phone</u></b> | ERIC OLSEN 835-4505      |
| <b><u>Contact E-Mail</u></b>     | EOLSEN@SPOKANEPOLICE.ORG |
| <b><u>Agenda Item Type</u></b>   | Purchase w/o Contract    |
| <b><u>Agenda Item Name</u></b>   | 5100-PURCHASE OF K8S     |

**Agenda Wording**

Fleet Services would like to purchase 2 Police K8s from Columbia Ford in Longview, WA, using WA State Contract #05916. Total purchase amount is \$97,254.31, including tax.

**Summary (Background)**

The 2 Police K8s will replace units that have reached the end of their economic life. We recommend approval for the purchase of 2 Police K8s for the Police Department. Funding for this is included in the Police Department budget.

|                                    |                 |                                     |                                   |              |
|------------------------------------|-----------------|-------------------------------------|-----------------------------------|--------------|
| <b><u>Fiscal Impact</u></b>        | Grant related?  | NO                                  | <b><u>Budget Account</u></b>      |              |
|                                    | Public Works?   | NO                                  |                                   |              |
| Expense                            | \$              | 97,254.31                           | # 590279115940005640499999        |              |
| Select                             | \$              |                                     | #                                 |              |
| Select                             | \$              |                                     | #                                 |              |
| Select                             | \$              |                                     | #                                 |              |
| <b><u>Approvals</u></b>            |                 | <b><u>Council Notifications</u></b> |                                   |              |
| <b><u>Dept Head</u></b>            | PAINE, DAVID    |                                     | <b><u>Study Session\Other</u></b> | 10/19/2020   |
| <b><u>Division Director</u></b>    | WALLACE, TONYA  |                                     | <b><u>Council Sponsor</u></b>     | Breean Beggs |
| <b><u>Finance</u></b>              | ORLOB, KIMBERLY |                                     | <b><u>Distribution List</u></b>   |              |
| <b><u>Legal</u></b>                | ODLE, MARI      |                                     | mmartinez                         |              |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL |                                     |                                   |              |
| <b><u>Additional Approvals</u></b> |                 |                                     |                                   |              |
| <b><u>Purchasing</u></b>           | PRINCE, THEA    |                                     |                                   |              |
| <b><u>POLICE</u></b>               | MEIDL, CRAIG    |                                     |                                   |              |
|                                    |                 |                                     |                                   |              |
|                                    |                 |                                     |                                   |              |



# Briefing Paper

## Finance and Administration Committee

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                         |                                                                                                                               |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                       | Finance, Fleet Services                                                                                                       |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                         | Purchase of Police K8s                                                                                                        |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                            | October 19, 2020                                                                                                              |
| <b>Author (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                      | Micaela Martinez mmartinez@spokanecity.org 625-7823                                                                           |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                            | Breean Beggs                                                                                                                  |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                               | Tonya Wallace                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                           | Finance and Administration Committee                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                             | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                        | Strategic Plan                                                                                                                |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                            | Innovative Infrastructure: Maintaining our fleet of support equipment                                                         |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                        |                                                                                                                               |
| <b>Outcome:</b> (deliverables, delivery duties, milestones to meet)                                                                                                                                                                                                                                                                                                                                                                                                     |                                                                                                                               |
| <b>Background/History:</b><br>Fleet Services would like to purchase 2 Police K8s from Columbia Ford in Longview, WA, using WA State Contract #05916. Total purchase amount is \$97,254.31, including tax.                                                                                                                                                                                                                                                               |                                                                                                                               |
| <b>Executive Summary:</b><br><br><u>Impact</u> <ul style="list-style-type: none"> <li>The 2 Police K8s will replace units that have reached the end of their economic life.</li> </ul> <u>Action</u> <ul style="list-style-type: none"> <li>We recommend approval for the purchase of 2 Police K8s for the Police Department.</li> </ul> <u>Funding</u> <ul style="list-style-type: none"> <li>Funding for this is included in the Police Department budget.</li> </ul> |                                                                                                                               |
| <b>Budget Impact:</b><br>Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>If new, specify funding source:<br>Other budget impacts: (revenue generating, match requirements, etc.)                                                                                                                         |                                                                                                                               |
| <b>Operations Impact:</b><br>Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>Specify changes required:<br>Known challenges/barriers:                                                                                                                                             |                                                                                                                               |



## Martinez, Micaela

**From:** NOREPLY@des.wa.gov  
**Sent:** Tuesday, October 6, 2020 1:09 PM  
**To:** Martinez, Micaela  
**Cc:** Steve.Hatfield@des.wa.gov  
**Subject:** Vehicle Quote - 2020-10-74 - SPOKANE, CITY OF - 23210

[CAUTION - EXTERNAL EMAIL - Verify Sender]

**Vehicle Quote Number: 2020-10-74**      [Create Purchase Request](#)      [View organization purchase requests](#)

This is a **quote** only. You must create a purchase request to order this vehicle(s)

### Contract & Dealer Information

|                              |                                       |
|------------------------------|---------------------------------------|
| Contract #: 05916            | Dealer Contact: Marie Tellinghiusen   |
| Dealer: Columbia Ford (W403) | Dealer Phone: (360) 423-4321 Ext: 187 |
| 700 7th Avenue               | Dealer Email: orders@colford.com      |
| Longview WA 98632            |                                       |

### Organization Information

Organization: SPOKANE, CITY OF - 23210  
Email: mmartinez@spokanecity.org  
Quote Notes:  
Vehicle Location: SPOKANE CITY

### Color Options & Qty

Agate Black (UM) - 2

Tax Exempt: N

### Vehicle Options

| Order Code    | Option Description                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | Qty | Unit Price  | Ext. Price  |
|---------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----|-------------|-------------|
| 2021-0521-001 | 2021 Ford Police Interceptor AWD Pursuit-Rated Utility/SUV (K8A/500A)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          | 2   | \$32,879.00 | \$65,758.00 |
| 2021-0521-002 | INFORMATION ONLY: Columbia Ford offers a \$300 prompt payment discount if payment is remitted within 20 days of vehicle delivery.                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | 2   | \$0.00      | \$0.00      |
| 2021-0521-003 | INFORMATION ONLY: Columbia Ford CARS Cancellation Fees: NO fee to cancel order if vehicle has not been scheduled for production and is able to be cancelled at factory. \$500 cancellation fee if vehicle has been serialized and is locked in for production by manufacturer. \$750 cancellation fee if vehicle has been delivered to customer and must be picked up by dealer and re-stocked into inventory. Absolutely NO cancellation if customer has licensed/registered vehicle. Upfits/Equipment ordered for vans, trucks, chassis cabs and police/fire vehicles will have a 10-30% re-stocking fee; custom bodies cannot be cancelled. | 2   | \$0.00      | \$0.00      |
| 2021-0521-004 | INFORMATION ONLY: (#010-099 Ford Factory Options) (#100-149) Ford Factory VSO Vehicle Special Order options, i.e. red/red LEDs) (#200-299 Dealer-Installed Options, including Setina Upfits) (#350-439 Day Wireless Upfits -- Click on Display Upfits at bottom of option list)                                                                                                                                                                                                                                                                                                                                                                | 2   | \$0.00      | \$0.00      |
| 2021-0521-005 | INFORMATION ONLY (2020MY): [EXTERIOR Dimensions: 198.8in Overall Length, 119.1in Wheelbase, 69.4in Height, 78.9in Width (mirrors folded), 89.3in Width (mirrors extended), Ground Clearances (7.4in w/ 3.3L HEV, 7.2in w/ 3.0L EcoBoost, 7.6in w/ 3.3L Direct-injection V6)] [INTERIOR Dimensions: Front/Rear: Head (40.7in/40.4in), Shoulder (61.8in/61.3in), Hip (59.3in/59.1in), Leg (40.9in/40.7in),                                                                                                                                                                                                                                       | 2   | \$0.00      | \$0.00      |



|                                                                                                             |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |   |            |            |
|-------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---|------------|------------|
| Cargo Opening Height 31.9in, Cargo Opening Width 47.6in, Cargo Area Width 47.9in, Cargo Area Length 46.2in] |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |   |            |            |
| 2021-0521-010                                                                                               | 2021 Ford Police Interceptor AWD Pursuit-Rated Utility/SUV, 3.3L V6 Direct-Injection (285 HP @ 6500 RPM, 260 lb.-ft. Torque @ 3000 RPM) (136-MPH Top Speed), 10-Speed Automatic Police-Calibrated Transmission (Column Shifter), 255/60R 18 All-Season BSW Tires, HD Steel Wheels, HD 80-Amp 730CCA Battery, HD 250 Amp Alternator, 21.4 Gallon Fuel Tank, 3.73 Axle Ratio, 6465# GVWR, 1670# Payload, 5000# Towing Capacity, 7.6in Ground Clearance (K8A/500A) THIS IS THE BASE VEHICLE -- Please review standard specs to view complete description.                                                                                                                                                                                                                                                                                                         | 2 | \$0.00     | \$0.00     |
| 2021-0521-012                                                                                               | Alternative Hybrid (HEV) Engine System [318 HP (combined system HP), 285 HP (gas engine) @ 6500 RPM, 260 lb.-ft. Torque @ 4000 RPM) (6840# GVWR, 1670 # Payload, 5000# Towing Capacity, 7.4in Ground Clearance) [Includes 3.3L V6 Direct-Injection Hybrid Engine System, Lithium-Ion Battery Pack (does not intrude into the cargo area), police calibrated high-performance regenerative braking system, DC/DC converter 220-Amp (in lieu of alternator), H7 AGM Battery - 800 CCA / 80-Amp, 19-Gallon Fuel Tank, 8-Year/100,000-Mile Hybrid Unique Component Warranty] (Not compatible with 3.0L V6 EcoBoost option) (99W/44B)                                                                                                                                                                                                                               | 2 | \$3,518.00 | \$7,036.00 |
| 2021-0521-013                                                                                               | Ready for the Road Package. [Includes contents of Front Headlamp Lighting Solution #66A, Tail Lamp Lighting Solution #66B, Rear Lighting Solution #66C; Pre-wiring for grille LED lights, siren and speaker #60A, 100 Watt Siren/Speaker #18X, Rear console Plate #85R, Hidden Door-Lock Plunger/Rear Door Handles/Rear Windows Inoperable #52P; PLUS: Grille Linear LED Lights (Red/Blue) and harness; Whelen Cencom Light Controller Head with dimmable backlight; Whelen Cencom Relay Center/Siren/Amp with Traffic Advisor mounted behind 2nd row seat; Light controller/relay cencom wiring (wiring harness) with additional input/output pigtailed; high current pigtail; Whelan Specific WECAN cable (console to cargo area) connects Cencom to Control Head] (Not available with Ultimate Wiring Package #67U or Interior Upgrade Package #65U ) (67H) | 2 | \$3,582.00 | \$7,164.00 |
| 2021-0521-021                                                                                               | Side Marker LED, Sideview Mirrors (Driver side - Red / Passenger side - Blue) (Located on backside of exterior mirror housing) (LED lights only. Wiring and controller are not included.) (Must also order Pre-wiring for grille lamp, siren and speaker #60A) (63B)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                           | 2 | \$289.00   | \$578.00   |
| 2021-0521-030                                                                                               | Noise Suppression Bonds (Ground Straps)(60R)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | 2 | \$100.00   | \$200.00   |
| 2021-0521-031                                                                                               | Switchable Red/White Lighting in Cargo Area (deletes 3rd row map light) (17T)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 2 | \$50.00    | \$100.00   |
| 2021-0521-033                                                                                               | Dark Car Feature (courtesy lamp disable when any door is opened) (Not available with Daytime Running Lights #942) (43D)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                        | 2 | \$25.00    | \$50.00    |
| 2021-0521-034                                                                                               | Police Engine Idle Feature (when activated, allows the key to be removed from ignition while vehicle remains idling, which allows driver to leave the engine running and prevents vehicle from unauthorized use when driver is outside of the vehicle) (47A)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | 2 | \$259.00   | \$518.00   |
| 2021-0521-036                                                                                               | BLIS Blind Spot Monitoring with Cross-Traffic Alert (Includes manual fold-away heated mirrors) (55B/54Z)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | 2 | \$543.00   | \$1,086.00 |
| 2021-0521-041                                                                                               | Rearview Camera, Alternative (video will be displayed in 4in center stack instead of in rearview mirror)(D87R)(No Charge)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                      | 2 | \$0.00     | \$0.00     |
| 2021-0521-043                                                                                               | Reverse Sensing System (76R)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | 2 | \$275.00   | \$550.00   |
| 2021-0521-045                                                                                               | Police Perimeter Alert - detects motion in an approximately 270-degree radius on sides and back of vehicle (If movement is determined to be a threat, chime will sound at Level 1; Doors will lock and windows will automatically go up at Level 2; Includes visual display in center stack with tracking) (68B)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                               | 2 | \$673.00   | \$1,346.00 |
| 2021-0521-047                                                                                               | Global Lock/Unlock Feature (Door panel switches will lock/unlock all doors and rear liftgate. Eliminates overhead console liftgate unlock switch and 45-second timer. Also eliminates the blue liftgate release button if ordered with Remote Keyless Entry System #55F) (18D)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 | 2 | \$0.00     | \$0.00     |
| 2021-0521-049                                                                                               | Fleet Keyed Alike (Call dealer for available key codes) (Allowed to also order Remote Keyless Entry #55F) (KEY)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | 2 | \$50.00    | \$100.00   |
| 2021-0521-054                                                                                               | Spot Lamps, LED Bulbs, Dual (Whelen) (51V)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                     | 2 | \$663.00   | \$1,326.00 |



|                                                                                                                                                                               |   |          |            |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---|----------|------------|
| 2021-0521-061 Underbody Deflector Plate (engine and transmission shield) (76D)                                                                                                | 2 | \$334.00 | \$668.00   |
| 2021-0521-063 2nd Row Cloth Seat in lieu of Vinyl (Charcoal) (Included with Interior Upgrade Pkg #65U) (88F)                                                                  | 2 | \$60.00  | \$120.00   |
| 2021-0521-099 INFORMATION ONLY: Delayed Warranty Start, customer submits request at www.fordwsd.com                                                                           | 2 | \$0.00   | \$0.00     |
| 2021-0521-200 INFORMATION ONLY: 200-299 Dealer-Installed Options                                                                                                              | 2 | \$0.00   | \$0.00     |
| 2021-0521-251 Setina - PB5 HD Fender Wraps (PIT Bars) (Must also order a Push Bumper) (DLR) (SET100)                                                                          | 2 | \$417.00 | \$834.00   |
| 2021-0521-254 Setina - PB400 Push Bumper, Steel, includes Mar Pad (DLR) (SET110)                                                                                              | 2 | \$519.00 | \$1,038.00 |
| 2021-0521-279 Setina - 12VS Rear Partition, Polycarbonate panel (DLR) (SET400)                                                                                                | 2 | \$581.00 | \$1,162.00 |
| 2021-0521-389 Interior Dome Light (Cargo): White LED dome light installed and wired to central controller switch. Light will be installed in cargo area. (DWS-DOME-2) (DW389) | 2 | \$42.00  | \$84.00    |

#### Quote Totals

|                         |             |
|-------------------------|-------------|
| <b>Total Vehicles:</b>  | 2           |
| <b>Sub Total:</b>       | \$89,718.00 |
| <b>8.4 % Sales Tax:</b> | \$7,536.31  |
| <b>Quote Total:</b>     | \$97,254.31 |



## Expenditure Control Form



1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

**Today's Date:** 10/14/2020    **Type of expenditure:**    Goods ☒    Services ☐

**Department:** Police

**Approving Supervisor:** Kevin Schmitt

**Amount of Proposed Expenditure:** 191,631.69

**Funding Source:** SPD SIP Funds

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

This is to supplement 4 SPD Patrol vehicles that were included in SPD's capital plan for 2020. Grant funds were received to pay for 4 vehicles so this request is for 4 additional vehicles under the SIP. Total vehicle purchased using SIP funds remains unchanged from approved capital plan.

**What are the impacts if expenses are deferred?**

To avoid supplanting, these vehicles must be ordered in 2020.

**What alternative resources have been considered?**

N/A

**Description of the goods or service and any additional information?**

SPD was awarded funding through WASPC to purchase 2 patrol vehicles for use by the Behavioral Health Unit. SPD also received JAG funding for 2 patrol vehicles for a total of 4 new vehicles. Four vehicles previously bought with SIP funds were re-allocated to grant funding to meet grant deadlines and now we are re-buying 4 vehicles with SIP funds.

**Person Submitting Form/Contact:** Kevin Schmitt x4087

**FINANCE SIGNATURE:**

*Tonya Wallace*

CBC812B631244E9...

**CITY ADMINISTRATOR SIGNATURE:**

*Scott Simmons*

E2AAB6F5A12B489...





**Agenda Sheet for City Council Meeting of:**  
11/02/2020

|                              |               |
|------------------------------|---------------|
| <b><u>Date Rec'd</u></b>     | 10/20/2020    |
| <b><u>Clerk's File #</u></b> | OPR 2020-0782 |
| <b><u>Renews #</u></b>       |               |
| <b><u>Cross Ref #</u></b>    |               |
| <b><u>Project #</u></b>      |               |
| <b><u>Bid #</u></b>          |               |
| <b><u>Requisition #</u></b>  | RE19677       |

|                                  |                             |
|----------------------------------|-----------------------------|
| <b><u>Submitting Dept</u></b>    | FLEET OPERATIONS            |
| <b><u>Contact Name/Phone</u></b> | ERIC OLSEN 835-4505         |
| <b><u>Contact E-Mail</u></b>     | EOLSEN@SPOKANEPOLICE.ORG    |
| <b><u>Agenda Item Type</u></b>   | Purchase w/o Contract       |
| <b><u>Agenda Item Name</u></b>   | 5100-PURCHASE OF TWO TAHOES |

**Agenda Wording**

Fleet Services would like to purchase 2 Police Tahoes from Bud Clary Chevrolet in Longview, WA, using WA State Contract #05916. Total purchase amount is \$94,377.38, including tax.

**Summary (Background)**

The 2 Police Tahoes will replace units that have reached the end of their economic life. We recommend approval for the purchase of 2 Police Tahoes for the Police Department. Funding for this is included in the Police Department budget.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |

|         |              |                            |
|---------|--------------|----------------------------|
| Expense | \$ 94,377.38 | # 590279115940005640499999 |
| Select  | \$           | #                          |
| Select  | \$           | #                          |
| Select  | \$           | #                          |

**Approvals**

|                                 |                 |
|---------------------------------|-----------------|
| <b><u>Dept Head</u></b>         | PAINE, DAVID    |
| <b><u>Division Director</u></b> | WALLACE, TONYA  |
| <b><u>Finance</u></b>           | ORLOB, KIMBERLY |
| <b><u>Legal</u></b>             | ODLE, MARI      |
| <b><u>For the Mayor</u></b>     | ORMSBY, MICHAEL |

**Council Notifications**

|                                   |              |
|-----------------------------------|--------------|
| <b><u>Study Session\Other</u></b> | 10/19/20     |
| <b><u>Council Sponsor</u></b>     | Breean Beggs |

**Distribution List**

mmartinez

**Additional Approvals**

|                          |                  |
|--------------------------|------------------|
| <b><u>Purchasing</u></b> | PRINCE, THEA     |
| <b><u>POLICE</u></b>     | LUNDGREN, JUSTIN |



# Briefing Paper

## Finance and Administration Committee

|                                                                                                                                                                                                                                                                                                                                                                                                                                                                               |                                                                                                                               |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| <b>Division &amp; Department:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                             | Finance, Fleet Services                                                                                                       |
| <b>Subject:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                               | Purchase of Police Tahoes                                                                                                     |
| <b>Date:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | October 19, 2020                                                                                                              |
| <b>Author (email &amp; phone):</b>                                                                                                                                                                                                                                                                                                                                                                                                                                            | Micaela Martinez mmartinez@spokanecity.org 625-7823                                                                           |
| <b>City Council Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Breean Beggs                                                                                                                  |
| <b>Executive Sponsor:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                     | Tonya Wallace                                                                                                                 |
| <b>Committee(s) Impacted:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                 | Finance and Administration Committee                                                                                          |
| <b>Type of Agenda item:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                   | <input checked="" type="checkbox"/> Consent <input type="checkbox"/> Discussion <input type="checkbox"/> Strategic Initiative |
| <b>Alignment:</b> (link agenda item to guiding document – i.e., Master Plan, Budget, Comp Plan, Policy, Charter, Strategic Plan)                                                                                                                                                                                                                                                                                                                                              | Strategic Plan                                                                                                                |
| <b>Strategic Initiative:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                  | Innovative Infrastructure: Maintaining our fleet of support equipment                                                         |
| <b>Deadline:</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                              |                                                                                                                               |
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| <b>Executive Summary:</b><br><br><u>Impact</u> <ul style="list-style-type: none"> <li>The 2 Police Tahoes will replace units that have reached the end of their economic life.</li> </ul> <u>Action</u> <ul style="list-style-type: none"> <li>We recommend approval for the purchase of 2 Police Tahoes for the Police Department.</li> </ul> <u>Funding</u> <ul style="list-style-type: none"> <li>Funding for this is included in the Police Department budget.</li> </ul> |                                                                                                                               |
| <b>Budget Impact:</b><br>Approved in current year budget? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Annual/Reoccurring expenditure? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>If new, specify funding source:<br>Other budget impacts: (revenue generating, match requirements, etc.)                                                                                                                               |                                                                                                                               |
| <b>Operations Impact:</b><br>Consistent with current operations/policy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No<br>Requires change in current operations/policy? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No<br>Specify changes required:<br>Known challenges/barriers:                                                                                                                                                   |                                                                                                                               |



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**Department:** Police

**Approving Supervisor:** Kevin Schmitt

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**Person Submitting Form/Contact:** Kevin Schmitt x4087

**FINANCE SIGNATURE:**

*Tonya Wallace*

CBC812B631244E9...

**CITY ADMINISTRATOR SIGNATURE:**

*Scott Simmons*

E2AAB6F5A12B489...



## Martinez, Micaela

**From:** NOREPLY@des.wa.gov  
**Sent:** Tuesday, October 6, 2020 1:04 PM  
**To:** Martinez, Micaela  
**Cc:** Steve.Hatfield@des.wa.gov  
**Subject:** Vehicle Quote - 2020-10-73 - SPOKANE, CITY OF - 23210

[CAUTION - EXTERNAL EMAIL - Verify Sender]

**Vehicle Quote Number: 2020-10-73**      [Create Purchase Request](#)      [View organization purchase requests](#)

This is a **quote** only. You must create a purchase request to order this vehicle(s)

### Contract & Dealer Information

|                                    |                              |
|------------------------------------|------------------------------|
| Contract #: 05916                  | Dealer Contact: Becky Davis  |
| Dealer: Bud Clary Chevrolet (W262) | Dealer Phone: (360) 423-1700 |

### Organization Information

Organization: SPOKANE, CITY OF - 23210  
Email: mmartinez@spokanecity.org  
Quote Notes: Tahoe w/out Console  
Vehicle Location: SPOKANE CITY

### Color Options & Qty

SATIN STEEL METALLIC G9K - 1

Tax Exempt: N

### Vehicle Options

| Order Code    | Option Description                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | Qty | Unit Price  | Ext. Price  |
|---------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----|-------------|-------------|
| 2021-0501-001 | 2021 CHEVROLET TAHOE POLICE PURSUIT VEHICLE-(CC10706)2WD 9C1:Identifier for Police Package Vehicle includes, (K47) heavy-duty air filter, (KX4) 250 amp high output alternator, (K6K) 760 cold-cranking amps auxiliary battery, electrical power & vehicle signals for customer connection located at the center front floor. Auxiliary battery circuit for customer connection located in the rear cargo area, (Z56) heavy-duty, police-rated suspension, (XCS) 275/55R20SL all-season tires, (RAV) P275/55R20 all-season spare tire, Police brakes, (RC1) front skid plate, (PXT) 20 steel wheels, Certified speedometer, SEO (5J3) Surveillance Mode interior lighting calibration, SEO (UT7) blunt cut cargo area and blunt cut console area ground wires,(V53) delete luggage rack side rails, (ATD) third row seat delete, (NP0) active single-speed transfer case (4WD only)  | 1   | \$37,988.00 | \$37,988.00 |
| 2021-0501-002 | 2021 CHEVROLET TAHOE POLICE PURSUIT VEHICLE-(CK10706)4WD 9C1:Identifier for Police Package Vehicle includes, (K47) heavy-duty air filter, (KX4) 250 amp high output alternator, (K6K) 760 cold-cranking amps auxiliary battery, electrical power & vehicle signals for customer connection located at the center front floor. Auxiliary battery circuit for customer connection located in the rear cargo area, (Z56) heavy-duty, police-rated suspension, (XCS) 275/55R20SL all-season tires, (RAV) P275/55R20 all-season spare tire, Police brakes, (RC1) front skid plate, (PXT) 20" steel wheels, Certified speedometer, SEO (5J3) Surveillance Mode interior lighting calibration, SEO (UT7) blunt cut cargo area and blunt cut console area ground wires,(V53) delete luggage rack side rails, (ATD) third row seat delete, (NP0) active single-speed transfer case (4WD only) | 1   | \$4,045.00  | \$4,045.00  |
| 2021-0501-005 | (AMF)Remote Keyless Entry Package includes 4 additional transmitters, NOTE: programming of remotes is at customer's expense. Programming remotes is not a warranty expense (Requires (9C1) Police Vehicle.)programming included in price through dealer                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | 1   | \$200.00    | \$200.00    |



|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |   |           |           |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---|-----------|-----------|
| 2021-0501-009 (AU7)Key, common, fleet (Includes SEO (6E2) complete vehicle fleet common key or SEO (6E8) complete vehicle fleet common key and (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                 | 1 | \$75.00   | \$75.00   |
| 2021-0501-018 (R9YFleet Free Maintenance Credit. This option code provides a credit in lieu of the free oil changes, tire rotations and inspections for one maintenance service during 1st year of ownership. The invoice will detail the applicable credit. The customer will be responsible for all oil change, tire rotations and inspections costs for this vehicle. (Requires one of the following Fleet or Government order types: FBC, FBN, FCA, FCN, FEF, FLS, FNR, FRC or FGO. Not available with FDR order types.) *CREDIT* | 1 | (\$30.00) | (\$30.00) |
| 2021-0501-020 (5Y1)Front center seat (20% seat) delete power driver and passenger bucket seats in base cloth trim. Derived from RPO (AZ3) 40/20/40 split-bench seat with the 20% section removed, which also removes the auxiliary power outlet, USB port and input jack for audio system. Does not include a floor console. All exposed floor area will remain untrimmed. (Requires (BG9) Black rubberized vinyl floor covering. Not available with (A50) front bucket seats or (B30) color-keyed carpeting floor covering.)         | 1 | \$0.00    | \$0.00    |
| 2021-0501-021 (6C7)Lighting, red and white front auxiliary dome Red and white auxiliary dome lamp is located on headliner between front row seats (red is LED, white is incandescent). The auxiliary lamp is wired independently from standard dome lamp (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                             | 1 | \$170.00  | \$170.00  |
| 2021-0501-024 (6J3)Wiring, grille lamps and siren speakers (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                           | 1 | \$99.00   | \$99.00   |
| 2021-0501-025 (6J4)Wiring, horn and siren circuit (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                                    | 1 | \$55.00   | \$55.00   |
| 2021-0501-026 (6J7)Flasher system, headlamp and taillamp, DRL compatible with control wire (Requires (9C1) Police Vehicle. Includes SEO (5J9) taillamp flasher calibration, Red/White and SEO (5LO) taillamp flasher calibration, Red/Red.)                                                                                                                                                                                                                                                                                           | 1 | \$50.00   | \$50.00   |
| 2021-0501-035 (UN9)Radio Suppression Package, with ground straps (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                     | 1 | \$95.00   | \$95.00   |
| 2021-0501-050 All weather mats, front seat floors only (DLR)*** Mats do not are not secured down, note for when using the vinyl flooring                                                                                                                                                                                                                                                                                                                                                                                              | 1 | \$115.00  | \$115.00  |
| 2021-0501-059 Mud guards, front and rear(DLR)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         | 1 | \$185.00  | \$185.00  |
| 2021-0501-110 (KSPEAK)100 watt siren speakerNOTE: MUST ORDER SEO 6J3RECOMMENDATION: order SEO 6J4 wiring to enable horn tap functionality                                                                                                                                                                                                                                                                                                                                                                                             | 1 | \$205.00  | \$205.00  |

#### Quote Totals

|                         |             |
|-------------------------|-------------|
| <b>Total Vehicles:</b>  | 1           |
| <b>Sub Total:</b>       | \$43,252.00 |
| <b>8.4 % Sales Tax:</b> | \$3,633.17  |
| <b>Quote Total:</b>     | \$46,885.17 |



## Martinez, Micaela

**From:** NOREPLY@des.wa.gov  
**Sent:** Thursday, October 8, 2020 8:57 AM  
**To:** Martinez, Micaela  
**Cc:** Steve.Hatfield@des.wa.gov  
**Subject:** Vehicle Quote - 2020-10-92 - SPOKANE, CITY OF - 23210

[CAUTION - EXTERNAL EMAIL - Verify Sender]

**Vehicle Quote Number: 2020-10-92**      [Create Purchase Request](#)      [View organization purchase requests](#)

This is a **quote** only. You must create a purchase request to order this vehicle(s)

### Contract & Dealer Information

|                                    |                              |
|------------------------------------|------------------------------|
| Contract #: 05916                  | Dealer Contact: Becky Davis  |
| Dealer: Bud Clary Chevrolet (W262) | Dealer Phone: (360) 423-1700 |

### Organization Information

Organization: SPOKANE, CITY OF - 23210  
Email: mmartinez@spokanecity.org  
Quote Notes: Tahoe with Console  
Vehicle Location: SPOKANE CITY

### Color Options & Qty

BLACK GBA - 1

Tax Exempt: N

### Vehicle Options

| Order Code    | Option Description                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                   | Qty | Unit Price  | Ext. Price  |
|---------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----|-------------|-------------|
| 2021-0501-001 | 2021 CHEVROLET TAHOE POLICE PURSUIT VEHICLE-(CC10706)2WD 9C1:Identifier for Police Package Vehicle includes, (K47) heavy-duty air filter, (KX4) 250 amp high output alternator, (K6K) 760 cold-cranking amps auxiliary battery, electrical power & vehicle signals for customer connection located at the center front floor. Auxiliary battery circuit for customer connection located in the rear cargo area, (Z56) heavy-duty, police-rated suspension, (XCS) 275/55R20SL all-season tires, (RAV) P275/55R20 all-season spare tire, Police brakes, (RC1) front skid plate, (PXT) 20 steel wheels, Certified speedometer, SEO (5J3) Surveillance Mode interior lighting calibration, SEO (UT7) blunt cut cargo area and blunt cut console area ground wires,(V53) delete luggage rack side rails, (ATD) third row seat delete, (NP0) active single-speed transfer case (4WD only)  | 1   | \$37,988.00 | \$37,988.00 |
| 2021-0501-002 | 2021 CHEVROLET TAHOE POLICE PURSUIT VEHICLE-(CK10706)4WD 9C1:Identifier for Police Package Vehicle includes, (K47) heavy-duty air filter, (KX4) 250 amp high output alternator, (K6K) 760 cold-cranking amps auxiliary battery, electrical power & vehicle signals for customer connection located at the center front floor. Auxiliary battery circuit for customer connection located in the rear cargo area, (Z56) heavy-duty, police-rated suspension, (XCS) 275/55R20SL all-season tires, (RAV) P275/55R20 all-season spare tire, Police brakes, (RC1) front skid plate, (PXT) 20" steel wheels, Certified speedometer, SEO (5J3) Surveillance Mode interior lighting calibration, SEO (UT7) blunt cut cargo area and blunt cut console area ground wires,(V53) delete luggage rack side rails, (ATD) third row seat delete, (NP0) active single-speed transfer case (4WD only) | 1   | \$4,045.00  | \$4,045.00  |
| 2021-0501-004 | (A50)Seats, front bucket (When ordered with (9C1) Police Vehicle, includes (PQA) 1FL Safety Package. Includes (D07) floor console.)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 1   | \$745.00    | \$745.00    |



|                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |   |           |           |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---|-----------|-----------|
| 2021-0501-005 (AMF)Remote Keyless Entry Package includes 4 additional transmitters, NOTE: programming of remotes is at customer's expense. Programming remotes is not a warranty expense (Requires (9C1) Police Vehicle.)programming included in price through dealer                                                                                                                                                                                                                                                                 | 1 | \$200.00  | \$200.00  |
| 2021-0501-009 (AU7)Key, common, fleet (Includes SEO (6E2) complete vehicle fleet common key or SEO (6E8) complete vehicle fleet common key and (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                 | 1 | \$75.00   | \$75.00   |
| 2021-0501-014 (USR)USB data ports, 2, one type-A and one type-C, located within center console (Included and only available with (A50) front bucket seats.)                                                                                                                                                                                                                                                                                                                                                                           | 1 | \$0.00    | \$0.00    |
| 2021-0501-018 (R9YFleet Free Maintenance Credit. This option code provides a credit in lieu of the free oil changes, tire rotations and inspections for one maintenance service during 1st year of ownership. The invoice will detail the applicable credit. The customer will be responsible for all oil change, tire rotations and inspections costs for this vehicle. (Requires one of the following Fleet or Government order types: FBC, FBN, FCA, FCN, FEF, FLS, FNR, FRC or FGO. Not available with FDR order types.) *CREDIT* | 1 | (\$30.00) | (\$30.00) |
| 2021-0501-021 (6C7)Lighting, red and white front auxiliary dome Red and white auxiliary dome lamp is located on headliner between front row seats (red is LED, white is incandescent). The auxiliary lamp is wired independently from standard dome lamp (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                             | 1 | \$170.00  | \$170.00  |
| 2021-0501-024 (6J3)Wiring, grille lamps and siren speakers (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                           | 1 | \$99.00   | \$99.00   |
| 2021-0501-025 (6J4)Wiring, horn and siren circuit (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                                    | 1 | \$55.00   | \$55.00   |
| 2021-0501-026 (6J7)Flasher system, headlamp and taillamp, DRL compatible with control wire (Requires (9C1) Police Vehicle. Includes SEO (5J9) taillamp flasher calibration, Red/White and SEO (5LO) taillamp flasher calibration, Red/Red.)                                                                                                                                                                                                                                                                                           | 1 | \$50.00   | \$50.00   |
| 2021-0501-035 (UN9)Radio Suppression Package, with ground straps (Requires (9C1) Police Vehicle.)                                                                                                                                                                                                                                                                                                                                                                                                                                     | 1 | \$95.00   | \$95.00   |
| 2021-0501-050 All weather mats, front seat floors only (DLR)*** Mats do not are not secured down, note for when using the vinyl flooring                                                                                                                                                                                                                                                                                                                                                                                              | 1 | \$115.00  | \$115.00  |
| 2021-0501-110 (KSPEAK)100 watt siren speakerNOTE: MUST ORDER SEO 6J3RECOMMENDATION: order SEO 6J4 wiring to enable horn tap functionality                                                                                                                                                                                                                                                                                                                                                                                             | 1 | \$205.00  | \$205.00  |

#### Quote Totals

|                         |             |
|-------------------------|-------------|
| <b>Total Vehicles:</b>  | 1           |
| <b>Sub Total:</b>       | \$43,812.00 |
| <b>8.4 % Sales Tax:</b> | \$3,680.21  |
| <b>Quote Total:</b>     | \$47,492.21 |





**Agenda Sheet for City Council Meeting of:**  
10/26/2020

|                       |               |
|-----------------------|---------------|
| <b>Date Rec'd</b>     | 10/14/2020    |
| <b>Clerk's File #</b> | FIN 2020-0001 |
| <b>Renews #</b>       |               |
| <b>Cross Ref #</b>    |               |
| <b>Project #</b>      |               |
| <b>Bid #</b>          |               |
| <b>Requisition #</b>  |               |

|                           |                          |
|---------------------------|--------------------------|
| <b>Submitting Dept</b>    | FINANCE & ADMIN          |
| <b>Contact Name/Phone</b> | PAUL INGIOSI 625-6061    |
| <b>Contact E-Mail</b>     | PINGIOSI@SPOKANECITY.ORG |
| <b>Agenda Item Type</b>   | Hearings                 |
| <b>Agenda Item Name</b>   | 0410 - BUDGET HEARINGS   |

**Agenda Wording**

Hearings for review of the 2021 Proposed Budget beginning Monday, November 2, 2020 and continuing thereafter at the regular City Council meetings through December 7, 2020.

**Summary (Background)**

As part of the annual budget process, the City Council will hold public hearings on the 2021 Proposed Budget for the City of Spokane. Public testimony is welcome on all sections of the budget at each hearing. The first hearing will be held on November 2, 2020 and are currently scheduled to continue each Monday through December 7, 2020. The City Council may continue the hearing up to the 25th day prior to the beginning of the next fiscal year.

|                             |                   |                              |
|-----------------------------|-------------------|------------------------------|
| <b><u>Fiscal Impact</u></b> | Grant related? NO | <b><u>Budget Account</u></b> |
|                             | Public Works? NO  |                              |
| Select \$                   |                   | #                            |
| Select \$                   |                   | #                            |
| Select \$                   |                   | #                            |
| Select \$                   |                   | #                            |

|                                    |                 |                                     |                              |
|------------------------------------|-----------------|-------------------------------------|------------------------------|
| <b><u>Approvals</u></b>            |                 | <b><u>Council Notifications</u></b> |                              |
| <b><u>Dept Head</u></b>            | INGIOSI, PAUL   | <b><u>Study Session\Other</u></b>   | Finance Committee - 10/19/20 |
| <b><u>Division Director</u></b>    | WALLACE, TONYA  | <b><u>Council Sponsor</u></b>       | Council President Beggs      |
| <b><u>Finance</u></b>              | WALLACE, TONYA  | <b><u>Distribution List</u></b>     |                              |
| <b><u>Legal</u></b>                | PICCOLO, MIKE   |                                     |                              |
| <b><u>For the Mayor</u></b>        | ORMSBY, MICHAEL |                                     |                              |
| <b><u>Additional Approvals</u></b> |                 |                                     |                              |
| <b><u>Purchasing</u></b>           |                 |                                     |                              |
|                                    |                 |                                     |                              |
|                                    |                 |                                     |                              |
|                                    |                 |                                     |                              |





# Expenditure Control Form

1. All requests being made must be accompanied by this form.
2. Route **ALL** requests to the Finance Department for signature.
3. If request is greater than \$100,000 it requires signatures by Finance and the City Administrator. Finance Dept. will route to City Administrator.

Today's Date:

Type of expenditure:

Goods

☐

Services

☐

Department:

Approving Supervisor:

Amount of Proposed Expenditure:

Funding Source:

**Please verify correct funding sources. Please indicate breakdown if more than one funding source.**

**Why is this expenditure necessary now?**

**What are the impacts if expenses are deferred?**

**What alternative resources have been considered?**

**Description of the goods or service and any additional information?**

Person Submitting Form/Contact:

FINANCE SIGNATURE:

---

CITY ADMINISTRATOR SIGNATURE:

---