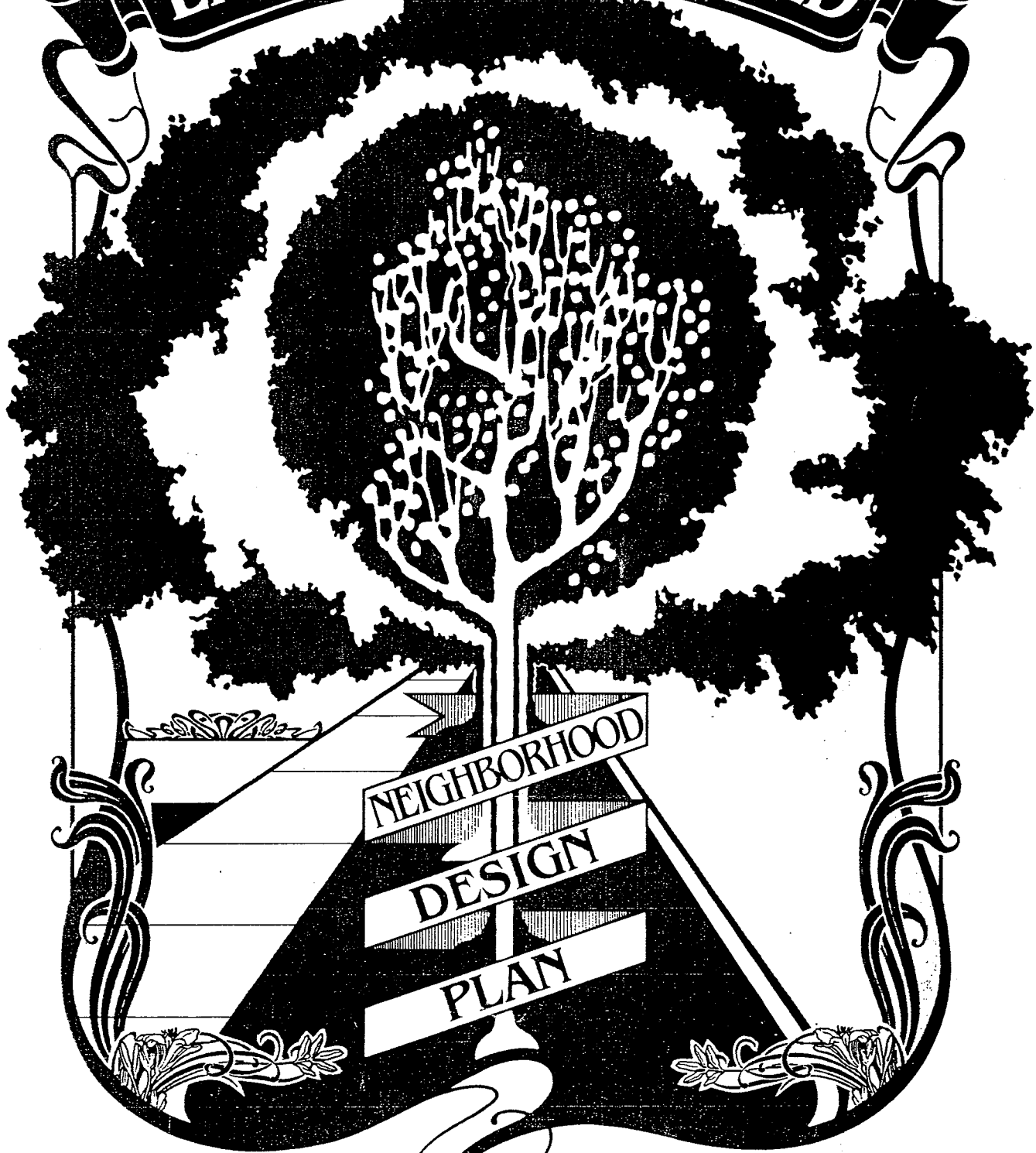


BB

EMERSON GARFIELD



JULY 27, 1986

TABLE OF CONTENTS

I	Introduction & Summary
	<i>Chapter One: Neighborhood Policy Concepts</i>
1	Land Use Policy
21	Circulation Policy
28	Community Facility Policy
	<i>Chapter Two: Neighborhood Design Concepts</i>
34	Enhancing the Neighborhood's Image
41	Historic Heritage & Character
44	Mixed Use Objectives
48	Residential-Commercial Interface: Bonus Density
49	Site Specific Design Recommendations
	<i>Chapter Three: Neighborhood Plan Implementation</i>
55	Overview
56	Map of Zones
58	Special Zoning Mechanisms
60	Neighborhood Improvement Programming and Phase II of Design Plan



ACKNOWLEDGMENTS

City Council

Vicki McNeill, Mayor
Rob Higgins, *Mayor Pro Tem*
Sheri Barnard
Robert Dellwo
Richard Gow
Jack Hebner
David Robinson

City Plan Commission

Larry Chalmers, *President*
John McFaddin, *Vice President*
J. Ware Ashurst
Jean L. Beschel, *Past President*
(1980 - 1985)
Michael Brewer
David Eash
David Gallik
Marion Hay
Betty Hennessey
Emmett Nelson

Emerson Garfield

Design Plan Task Force

Richard Stripes, *Chairman*
Mike Czechowski
Rose Fanger
Wilma Jones
Don Kelly
Michael McGarvey
Jean Russell
Phil Snowdon
Robert Woerner
Charles Sims (*resigned*)

Staff

Lyle E. Balderson, *Manager - Planning*
Michael Adolfae, *Manager - Community
and Economic Development*
Janice Rutherford, *Historic
Preservation Officer (resigned)*
Gloria Cloninger
Charles Dotson
Christine Fisher
Hope Fulwiler
Steve Haynes

The Spokane City Council wishes to express its sincere gratitude to those residents and property owners of the Emerson Garfield Neighborhood who cared enough to invest some of themselves in Spokane's future.



Introduction and Summary of Major Recommendations

This is the Design Plan for the Emerson-Garfield neighborhood on the near north side of the City of Spokane. Specifically it is bounded by Division Street, Belt/Monroe Streets, Boone/Indiana Avenues and the north hill slope.

The reason for this Design Plan and accompanying implementation program is to stabilize and revitalize an important neighborhood in the city and provide greater detail to the Generalized Land Use Plan. The Design Plan provides guidelines for public and private investment in the neighborhood that will best serve the public interest. The Plan will be used by public officials as a basis for land use regulations, for programming specific public works and for the review of specific private projects. The Plan will be used by property owners and developers as a frame of reference for private development.

The Emerson-Garfield Neighborhood Design Plan addresses four main areas of public policy interest: Land Use, Circulation, Community Facilities, and Neighborhood Design. The following is a summary of the major policy direction for each area of interest:

LAND USE: *Residential*

The policies strive to retain the existing low density, single-family character of the neighborhood while allowing medium density housing (tri-plex, four-plex), higher density housing (large apartments) and low rise office in selected areas as shown on the Neighborhood Land Use Plan Map. Where multi-family housing or office is permitted, the plan promotes the reuse of existing single-family residential structures to retain the historical character, residential flavor and appearance. Policies identify appropriate property development objectives to be met during construction dealing with: building height, bulk, and lot coverage; landscaping; signing; offstreet parking; property access and street capacity; and environmental sensitivity and solar access.

The transition between housing areas and business is dealt with in the policies by promoting appropriate property development and layering of use intensities. This includes the potential for multi-family housing and low rise office along the transition as a special incentive for high quality development. Neighborhood quality and image is a recognized concern, with long-term self-improvement programs the thrust of the plan. The need to enforce existing City codes and ordinances is also recognized as very important.

Three alternatives are suggested for the site currently occupied by the Garland Hills Mobile Village: redevelop the site as a mobilehome park under current zoning standards; develop a traditional site built single-family subdivision; develop terraced low rise multi-family housing, taking advantage of the hillside and solar access. The special permit for the present mobilehome park expires in 1990 and it is not to be renewed.



LAND USE: Commercial

In the general commercial areas shown on the Neighborhood Land Use Plan Map, policies permit any use to occur so long as specific design and performance objectives are met. These objectives deal with such issues as: building setbacks and required yards; landscaping; signs; parking lots, service areas and access; containment of operational impacts; and screening of unaesthetic views.

Again, the policies address issues of the transition between business and residential areas, allowing for some business expansion so long as the business remains oriented to the arterial street.

The intersection of Monroe, Northwest Boulevard and Indiana is considered to be a key location in the Neighborhood. The Plan's policies encourage the consideration of business activities at this intersection. Furthermore, they promote the development of businesses that are primarily intended to serve the Neighborhood's needs, as opposed to a citywide market.

CIRCULATION: Motor Vehicles

The Design Plan's policies related to arterial streets recognize the recently adopted Arterial Plan update and do not propose any changes. The policies also deal with internal automobile circulation with proposals that address issues not related to the Arterial Plan (e.g. discouraging through traffic on certain residential streets).

The City's Arterial Plan, in its designation of that portion of the Monroe-Lincoln couplet in the Emerson-Garfield Neighborhood, did not address and resolve whether the northbound leg of that couplet should be placed on Lincoln or Post Street. The neighborhood residents expressed a strong preference for the use of Post and not Lincoln. This Design Plan does not make a choice on this issue but does include design considerations that are applicable to either alternative.

For reasons of safety and efficiency, it is proposed to close Indiana west of Monroe to through traffic (that block of Indiana would still be accessible from Madison), making a four-way, rather than five-way, intersection. The policies also recommend redesigning several of the intersections along Northwest Boulevard to improve site angle and safety.

CIRCULATION: *Pedestrian, Bicycle, Transit*

A Neighborhood pedestrian and bicycle network is identified in the Design Plan connecting all the major community features. It also connects to features outside the Neighborhood and relates to the citywide plan for bikeways. The recommendations are particularly sensitive to the elderly and handicapped, especially with regard to making arterial crossings safer and easier.

The Neighborhood is effectively served by north-south transit routes, but not so by east-west routes. The Plan recommends that east-west routes be established using arterial streets through the Neighborhood. Recommendations also propose alternatives to diesel fuel buses be explored and when viable alternatives evolve, they should be pursued.

COMMUNITY FACILITIES: *Parks and School Playgrounds*

Neighborhood park policies suggest that Drumheller Springs Park be retained in its natural state and not developed for active recreation. Emerson Park is identified as a neighborhood children-oriented, active play area. It is proposed that the old school structure remaining on site be used for indoor recreation.

Corbin Park is seen as appropriately balanced between active play areas and passive recreation areas, and that balance should not change. Policies recommend that assertive measures be taken to control traffic speed around Corbin Park. Posting reduced speed signs is not adequate.

School playgrounds offer recreational opportunities to the Neighborhood and joint agreements between the City and School District have existed for years. The Design Plan promotes the continued joint use of school facilities and the removal of any barriers, physical or psychological, which might inhibit optimum use. The policies also identify the need to disseminate information to children and their parents about the availability of school playgrounds and facilities for Neighborhood recreation.

The playgrounds at private schools are also seen as potential recreational resources. The Design Plan recommends that mutual agreements between the City and these schools be pursued for joint use of playgrounds, including community development funding for playground improvements and expansion.

NEIGHBORHOOD DESIGN CONCEPTS

These policies, for the most part, deal with recommendations to improve the Neighborhood's physical image and preserve the historical character of its housing and commercial structures. Considerable attention is given to street trees, their preservation and replacement, as well as decorative and utilitarian street furniture. Key Neighborhood entrances along the northern boundary are identified and are proposed for aesthetic improvement.

Particularly in the North Riverbank portion of the Neighborhood (south of Indiana), most of the prescribed land uses are in designated mixed use districts. This portion of the Design Plan identifies specific objectives for each mixed use district and includes: mixture of uses to be promoted; structural height, bulk and site coverage; landscaping; off-street parking lots; and unique conditions or characteristics to be promoted.

This portion of the Design Plan also provides some site specific improvement recommendations, including policy recommendations and design drawings for the Monroe-Lincoln couplet and design drawings for reconstruction of intersection with Northwest Boulevard.



EMERSON

GARFIELD

NEIGHBORHOOD

POLICY

CONCEPTS

Chapter 1:

- Land Use Policy
- Circulation Policy
- Community Facility Policy

LAND USE POLICY CONCEPTS

Overview

The policy concepts provided here are a refinement of those adopted in the City's Land Use Plan, giving greater detail and more site specific application to the Emerson-Garfield Neighborhood. They are intended to be used in conjunction with the Neighborhood Land Use Plan Map primarily to guide new development and redevelopment. Since Emerson-Garfield is a very stable, already built-up Neighborhood, little new development is expected. Redevelopment, ranging from simple remodeling to whole structural replacement, is expected to be the more common activity and is the emphasis of these policies.

The Emerson-Garfield Neighborhood has a fairly simple land use pattern, consisting primarily of low-density housing areas separated by business strips along arterial streets. The relationship between commercial and residential uses and the quality of life in the residential areas is a paramount concern. However, the health and long-term vitality of the Neighborhood's business areas is also a significant issue. Both land use and design policies are important.

The land use policies respond to issues raised by the Neighborhood residents through workshops and Task Force meetings. They bring the character and concept of the Neighborhood into focus.

Public policy and physical improvements can influence growth and development but, in general, not directly modify the quality of life. In one regard the quality of life can be affected; that is through prescribing the direction of new development and rehabilitation in the Neighborhood and identifying appropriate site development and performance objectives for creating the desired physical setting. However, social, cultural and environmental aspects of this concept often cannot be approached directly by design plans.

Residential Land Use

GOAL: Encourage the preservation and maintenance of the neighborhood's existing residential character and flavor while striving to improve the quality of life, richness and diversity of experiences.

RESIDENTIAL LOCATION POLICIES:

1. Retain the low density, single-family residential character of areas so designated on the Neighborhood Land Use Plan Map.
2. Permit the development of medium-density housing (tri-plex, four-plex and similar scale housing) in areas so designated on the Neighborhood Land Use Plan Map and as components of Mixed-Use Districts where designated on the Map.

3. Permit the development of high-density housing in areas so designated on the Neighborhood Land Use Plan Map and as components of Mixed-Use Districts where designated on the Map according to the guidance provided in the North Riverbank Design Plan.
4. Permit low rise professional office development as components of Mixed-Use Districts when designated on the Neighborhood Land Use Plan Map and according to the North Riverbank Design Plan where appropriate.

Discussion

The specific location of various uses and combinations of uses is generally prescribed on the Neighborhood Land Use Plan Map (see page 19). Boundaries between use districts are not absolute lines and may fluctuate slightly when policy guidance indicates it is appropriate. However, the width of a typical residential lot in an area under question should be considered the normal limits of flux. When streets or alleys appear to form the boundary between land use districts, they should be considered firm.

When the Neighborhood Land Use Plan Map indicates a combination of uses would be appropriate in a single district, it is called a Mixed-Use District. There are several identified in the Emerson-Garfield Neighborhood, mostly within and adjacent to the North Riverbank portion of the Neighborhood. The Neighborhood Design Concepts Chapter contains a detailed description of each designated Mixed-Use District and this is essential policy information.

DEVELOPMENT CHARACTER POLICIES:

5. Where low rise professional office, medium and high density residential uses are permitted, promote the reuse of existing single-family residential structures and the retention during remodeling of their historic character, residential flavor and appearance. In cases where a residential structure is not reusable for structural/health/safety reasons, permit new construction replacement. Ensure that the new structure blends with the character and appearance of surrounding structures.
6. Assure that site development meets appropriate public objectives when conversion or new construction occurs. These objectives include:
 - a. The height, bulk and lot coverage of buildings should be compatible with the immediate surroundings. For example, in areas that are predominantly single-family residential structures, new development and redevelopment should generally conform to the Minimum Yard, Maximum Height and Maximum Building Coverage standards of the "R1" zone in the Zoning Ordinance.

RESIDENTIAL ZONES

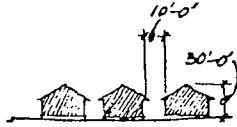
ZONE

PROFILE

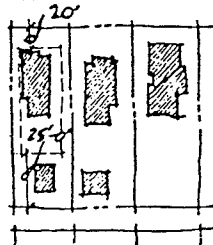
DENSITY

BULK

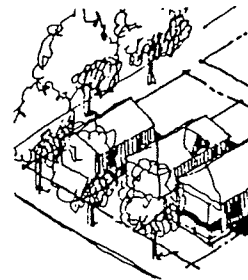
R-1
SINGLE FAMILY



- single family density
- maximum 30' height limit

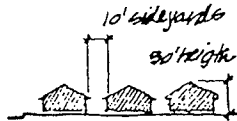


- typical lot size: 7,100 sq. ft.
- maximum site coverage 30%
- typical lot 50' x 142'

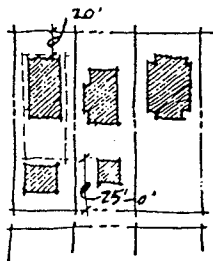


- maximum height 3 stories

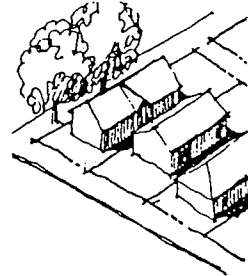
R-2
TWO FAMILY



- single family
- maximum 30' height limit

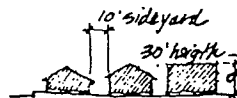


- maximum site coverage 30%

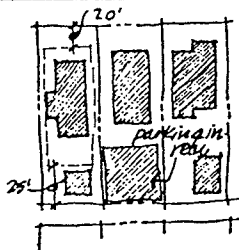


- maximum 30' height

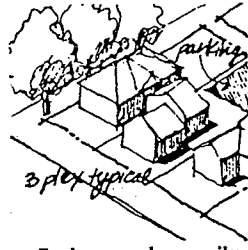
R-3
MULTI-FAMILY



- single family + up to 4-plex
- typically 3-plex in logan

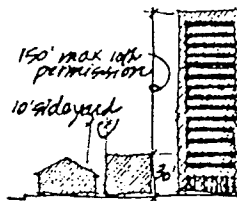


- maximum site coverage 35%
- minimum 2,000 sq. ft. area/unit

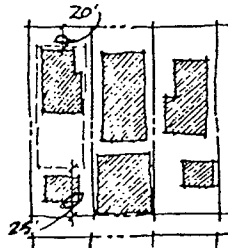


- 3-plex can be easily accommodated. Parking tends to dominate rear yard.

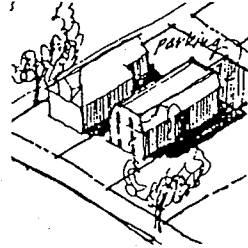
R-4
MULTI-FAMILY



- single family up to multi-family



- maximum site coverage 40%
- minimum 1,000 sq. ft. land area/unit



- 7-plex maximum on typ. lot — with req. parking — not much room for anything else.

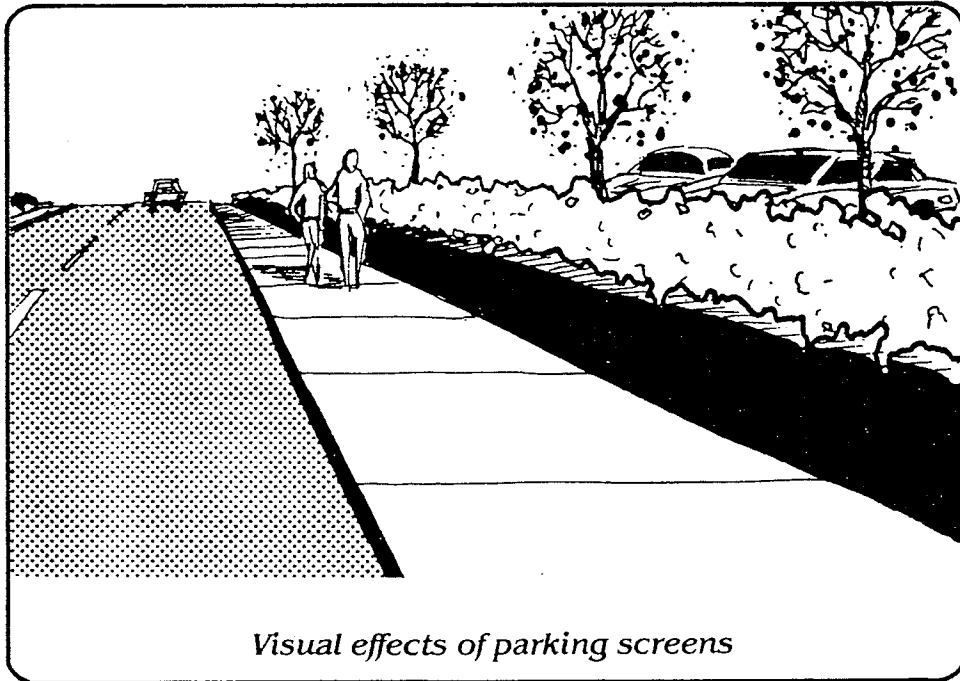
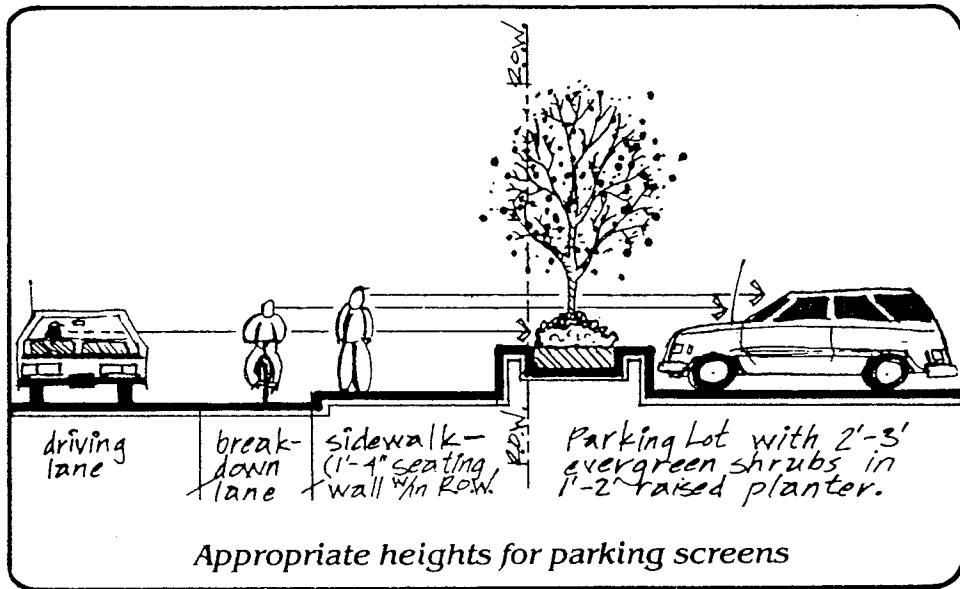
Zoning Compatibility (Policy 6a)

- b. Sites should be landscaped to effectively blend into the surrounding area and to buffer incompatible activities. For example, in areas that are predominantly single-family residential structures, new development and redevelopment should include landscaping as to resemble the yards of a single-family home. When incompatible activities are proposed, such as off-street parking lots adjacent to single-family homes, the landscaping provisions of the "L" zone should be used.

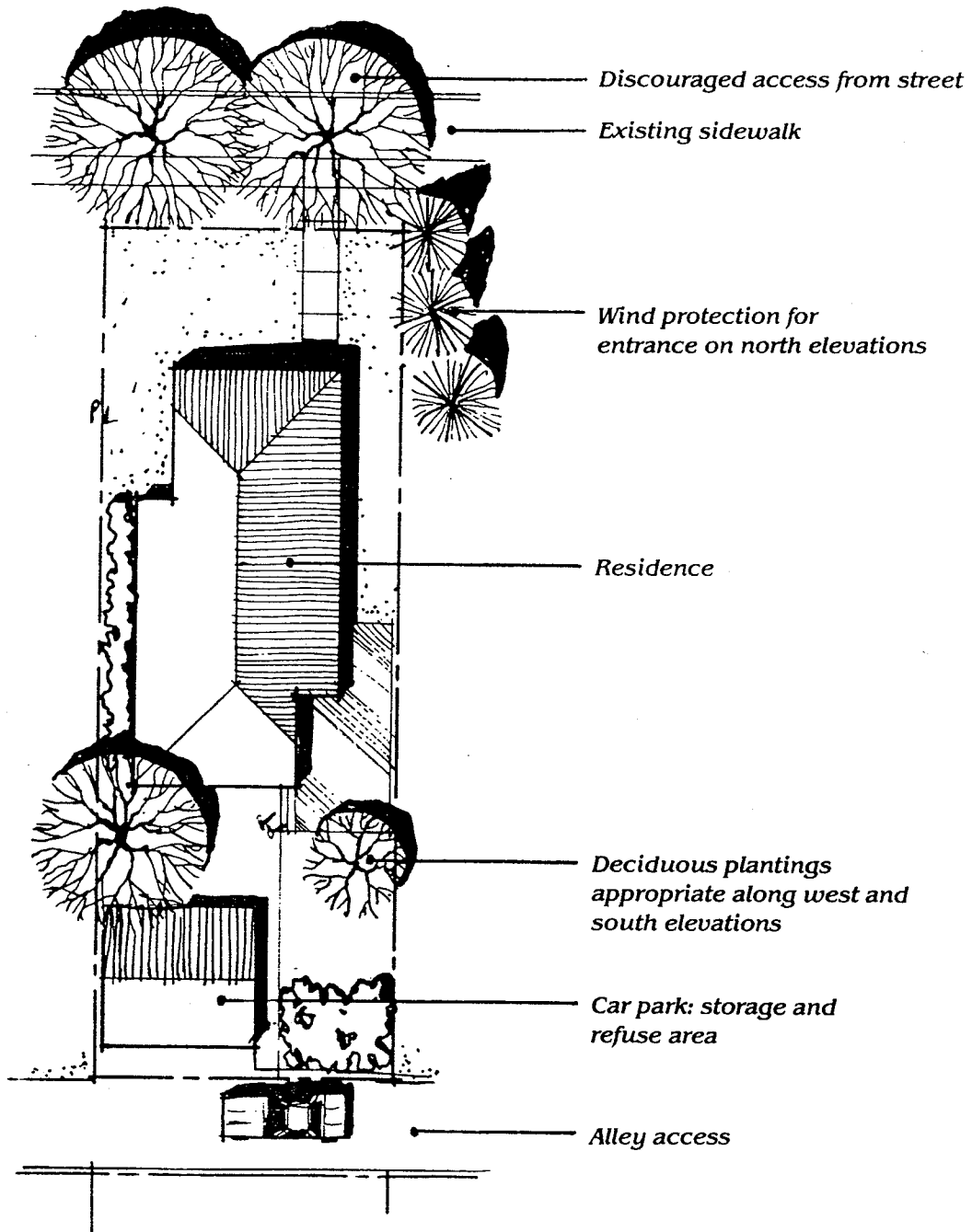
The type and mixture of landscaping materials should also reflect the objective of retaining a single-family residential character. For example, lawn grass would be preferred over bark and rock. The types and placement of trees and shrubs is also important. For example, if the immediate area around a proposed project typically includes street trees, the project should incorporate street trees into its site plan.

It is anticipated that landscaping, once emplaced, will be properly maintained. To assure proper irrigation, nonresidential and multi-family residential uses may be required to install automatic sprinkler systems. It is also essential that shrubs and trees be replaced if they fail to survive.

- c. Multi-family residential and nonresidential uses allowed in residential areas should be permitted such informational and advertising signage as allowed in the Zoning Ordinance and which would be compatible with the surrounding area. For example, in areas that are predominantly single-family residential structures, free standing signs should not be permitted. All display and flood lighting should be permitted as prescribed by the Zoning Ordinance.
- d. For various uses, require off-street parking as indicated in the Zoning Ordinance. For all parking lots, require perimeter landscaping, "208" stormwater drainage swales, and along boundaries with residential uses, require screening (wall and/or dense landscaping) as provided in the "L" zone of the Zoning Ordinance. One of the most significant impacts of off-street parking lots is their unattractive appearance and screening is necessary. It is essential that suitable landscaping materials be used, especially for screening. Deciduous trees and shrubs which lose their leaves in winter provide poor perimeter screening if used alone.
- e. Provide access to properties in residential areas from alleys rather than streets whenever possible. Access and parking are very strongly related. On-street parking is undesirable in residential areas and parking in the front yard is prohibited. Therefore, access and off-street parking should be located at the rear of properties from an alley whenever possible.



Parking Lot Screening (Policy 6d)



Alley Access (Policy 6e)

- f. In evaluating rezoning or special permit proposals for new multi-family residential or office development, assure the capacity of the serving arterial street network is adequate to accommodate the expected vehicular traffic. It is essential that individual development decisions be made responsibly with regard to the capacity of the affected portion of the arterial system. If the traffic to be generated by a proposal exceeds the capacity of the arterial network, it should be denied. To assure an appropriate relationship is maintained, the Traffic Engineer should certify whether or not development proposals will or will not generate traffic in excess of arterial capacity.
 - g. In those portions of the Neighborhood with significant topography (e.g. the slope along the northern boundary and rock outcrops in the southern part of the Neighborhood), minimize disturbance to existing landforms, maintain existing drainage patterns and preserve existing vegetation through sensitive site planning. As much as possible, take advantage of special opportunities afforded by terrain such as earth sheltered housing sites and views.
 - h. Maintain solar access to Neighborhood properties by ensuring that building mass, height and siting is sensitive to adjacent lots. Adopt, citywide, a solar access ordinance which provides adequate solar access standards.
- 7. Encourage the retention of existing housing and encourage the preservation and enhancement of the residential areas' historic character and flavor. Encourage Neighborhood residents to do this voluntarily through education and advisory services. Do not permit the use of Home Rehabilitation funds for projects which would alter or destroy those key characteristic features which impart the essential historic flavor.
 - 8. Establish the boundary/transition between residential areas of the Neighborhood and commercial activities along arterial streets. Through the use of appropriate site development and layering of use intensities, physically define this transition.
 - 9. Permit medium density residential and limited low-rise professional office-uses immediately adjacent to commercial uses to help create the boundary/transition. Accomplish this through the use of density bonuses offered in exchange for the employment of more than minimum buffering mechanisms. Once the boundary/transition has been established, do not allow further encroachment of commercial, office or multi-family housing into the low-density areas.

10. Where residences are severely impacted by incompatible commercial development or other harsh features, such as Jantsch Continuation High School, promote nontraditional, innovative ways of preserving the residential quality of life. This might include the use of traditional housing rearranged and reoriented on the lot or it might include significantly different housing styles and placement on the property.

Discussion

While the Neighborhood Land Use Plan Map generally identifies the location of potential land uses, the character of the Neighborhood is also an essential ingredient. These policies establish the framework for preserving and enhancing the existing character of the Neighborhood by controlling the manner in which new construction, rehabilitation and maintenance take place.

The continued use of sound existing residential structures is of key importance, whether they be for further single-family occupancy or conversion to multi-family living or office uses. Retaining the exterior facade and "look" of a single-family home is essential. The Neighborhood Design Concepts chapter includes a description of those exterior features which give the Neighborhood's housing its character and those maintenance and remodeling practices which could be damaging to those features.

Development of properties, aside from structures themselves, affects the quality of life in the Neighborhood by creating an attractive living environment and allowing uses of differing intensities and operational characteristics to coexist. Since the Neighborhood is predominantly low-density single-family residential, especially north of Indiana, the retention of this appearance should be supported through appropriate structural siting, scale relationships and landscaping.

It is essential along the transitions between residential areas and commercial development that site design objectives respond sensitively to the marked differences in intensity and operational characteristics of single-family housing and business activities. Customarily the burden of buffering between commercial and residential uses has been borne by the homeowner, especially in older development areas. Site design objectives should shift this burden to assure the responsibility of being a good neighbor is balanced between the homeowner and businessman.

Along the boundary between residential and commercial activities, gradations or layering of use intensities and types can be an extremely effective way to buffer established residential areas. These policies permit such development under a bonus density mechanism, which amounts to a negotiated contract between the builder and the public. The builder receives the right to build at increased residential densities

or include other uses (e.g., office). The public receives specifically identified amenities or features which satisfy publicly established objectives. This also allows the flexibility to take advantage of unique opportunities, such as using the massive north wall of the Northwest Boulevard Safeway supermarket for recreational purposes like handball/racquetball courts. The Neighborhood Design Concepts chapter describes the public objectives to be met and the basic mechanism to be employed.

NORTH RIVERBANK POLICIES:

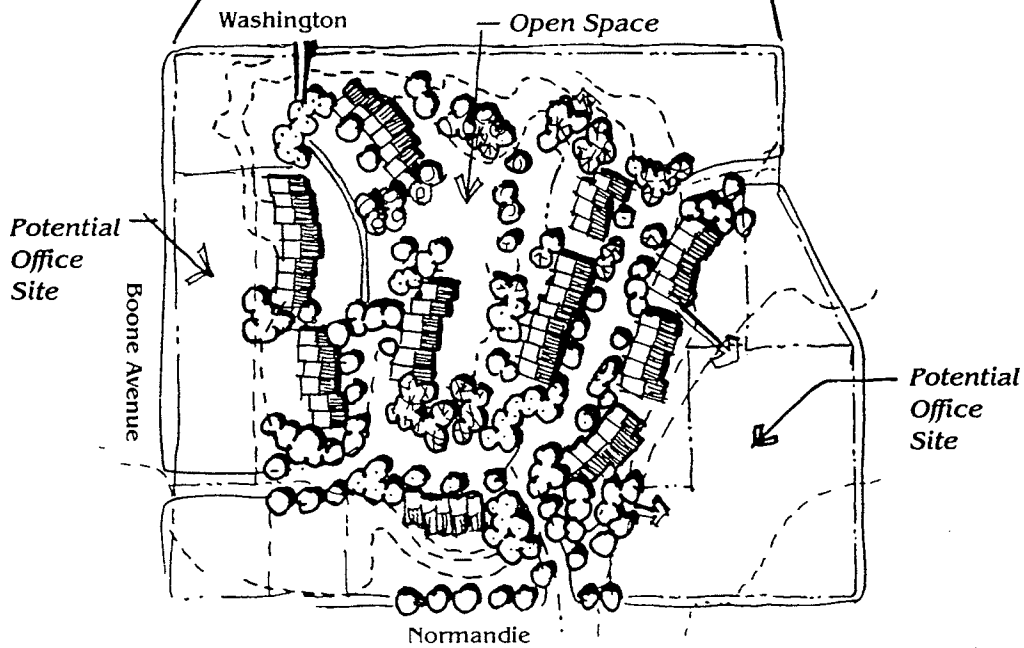
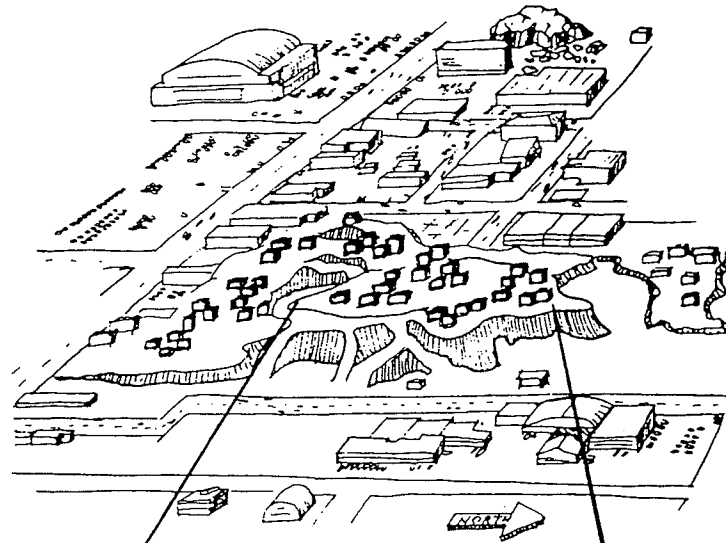
11. Encourage the Neighborhood's more intensive growth and development to occur south of Indiana Avenue, in the North Riverbank Design Area, rather than elsewhere in the Neighborhood.
12. Encourage the proposals of the North Riverbank Design Plan for the rock outcrop and City shops to be pursued with all practical speed to further strengthen the small residential enclave east of North Central High School.

Discussion

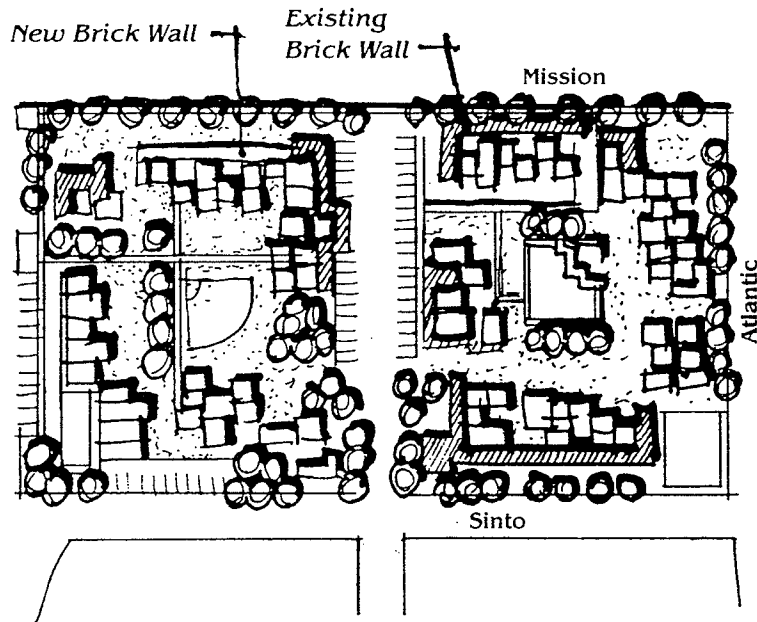
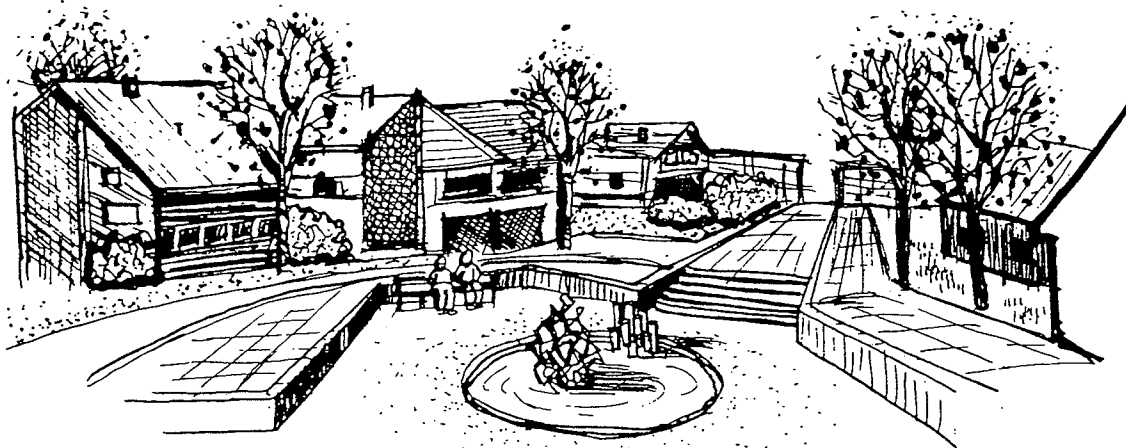
The North Riverbank Design Plan proposes the City should relocate its maintenance and storage operations, making the rock outcrop and shop sites available for development. It further proposes low-rise, high-density residential and limited office development on those sites. The City is currently evaluating the consolidation (and relocation) of its operations, and this policy encourages it to occur as quickly as possible. The small housing area east of North Central High School is currently threatened by higher intensity uses and the development of proposed residential projects would help stabilize this portion of the Neighborhood.

MONROE STREET HOUSING POLICIES:

14. Encourage a mixture of residential and commercial uses to occur together within buildings along Monroe Street, typically with business activities on the ground floor and apartments/condominiums above. Assure adequate site development standards are maintained, especially relative to off-street parking.



North Riverbank Rock Outcrop Site
(Policy 12)



Residential Enclave within the North Riverbank Rock Outcrop Site (*Policy 12*)

Discussion

Many of the fine, older brick buildings along Monroe Street have historically maintained a relationship between business and residential activities. In many parts of the country, it is a very common and desirable living environment. By promoting this manner of development, this Design Plan fosters greater diversity and richness in the Neighborhood and provides a mechanism for greater economic benefit from development of the Monroe corridor.

NEIGHBORHOOD IMAGE POLICIES:

15. Promote a variety of Neighborhood self-improvement programs such as organized Neighborhood cleanup/fixup programs, the recognition of superior site development and maintenance (e.g., awarding plaques or certificates, annual cash prizes, free publicity for commercial establishments), recognition of historic assets, and similar activities.
16. Aggressively enforce all existing ordinances and regulations, especially when they relate to the appearance and quality of the Neighborhood. Strictly enforce all conditions and agreements of development permits, both initially and over the long term.

Discussion

Since most of the Neighborhood is already developed, appearance is largely a matter of property maintenance by individual owners and occupants. Except when development permits impose specific maintenance levels as a condition of issuance, most site care related issues deal with personal and collective Neighborhood pride. Substantial organizational programs that elevate and reinforce pride in property appearance can have a major effect on the quality of life and even promote high-quality reinvestment in the Neighborhood. Cooperation by residents is also an essential part of code enforcement by alerting City officials to conditions requiring attention.

EAST BLUFF POLICIES:

17. Do not renew or extend the current Special Permit for the Garland Hills Mobile Village mobile home park. Consider the following alternate development types for its site:
 - a. Redevelop the site as a manufactured home park under the provisions of Section 245 of the Zoning Ordinance.
 - b. Develop terraced low-rise multi-family housing, taking advantage of the hillside and solar access of the site. Assure the overall height of development does not obstruct the views of residents above. Also assure structures developed on the flat portion of the site do not detract the solar access of structures terraced into the hillside.

- c. Develop, through a traditional subdivision, site built single-family or duplex housing, remaining primarily on the flat portion of the site below the hill.

Discussion

The Garland Hills Mobile Village was developed in 1974 as "temporary" housing during Expo '74. The proponent's long-term intent, expressed at that time, was eventually to convert the mobile home park site to multi-family apartments; however, only the portion fronting on Post Street was ever built. The remainder of the site is now a substandard mobile home park, and at the expiration of its Special Permit in 1990, it should be redeveloped according to this policy.

If development alternative "a" or "c" is pursued, the Zoning and Subdivision Ordinances possess adequate site standards, and the existing infrastructure capacity is also adequate. However, if alternative "b" is pursued, Cora Avenue will have to be improved to collector arterial standards, including signalized intersections at Post and Division Streets. Further, the site redevelopment should provide an adequate (20' or more) landscaped setback from Cora Avenue's north frontage. Any remaining portion of the steep slope should be stabilized with appropriate vegetation, mechanical means or some combination. During the interim the conditions of approval imposed by the Zoning Board on the initial Special Permit should be enforced, especially those dealing with perimeter landscaping and slope stabilization. If the property owner is unwilling to comply, the current Special Permit should be revoked.

Commercial Land Use

GOAL: *Promote economically healthy, long-term businesses to establish and remain in the Emerson-Garfield neighborhood and to maintain themselves in an attractive desirable manner. Commercial use policies:*

1. Permit any use to occur through appropriate rezoning in areas designated General Commercial on the Neighborhood Land Use Plan Map but only if specified design and performance objectives can be met. These objectives include:
 - a. Require front, side and rear yards as prescribed in the "L" zone of the Zoning Ordinance, except that whenever a proposal is adjacent, with or without an intervening alley, to a residential use, the front yard shall be at least 20 feet, the side yards at least 10 feet and the rear yard at least 15 feet. Not more than 50% of the site may be covered by structure (except in the North Riverbank area, where 75% of the site may be covered) and allow the building heights only to three stories. However, permit the equivalent building volume to be rearranged to any height up to 75 feet.



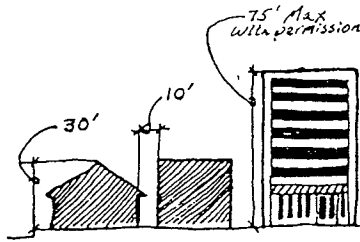
ZONE

**B-2
COMMUNITY
BUSINESS**

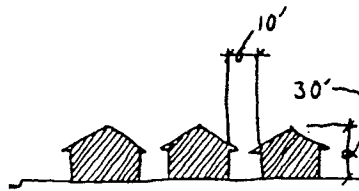
**RO-1
Residential
Office**

BUSINESS ZONES

PROFILE

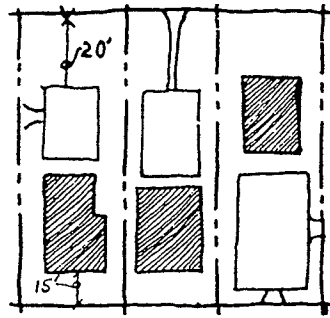


- Use "L" Zone standards.
- Allow building to 75' under certain conditions.

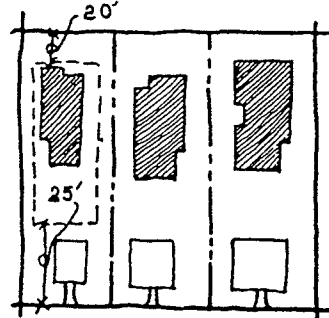


- Office should resemble residential structures.
- Site standards same as residential zone.

DENSITY

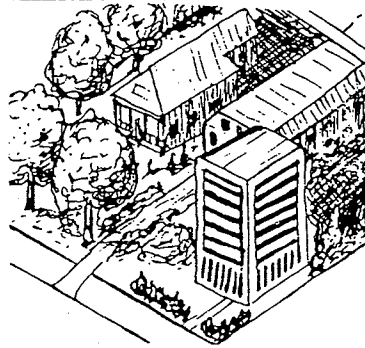


- Allow site coverage to 50%.
- Require larger yards adjacent to residence.

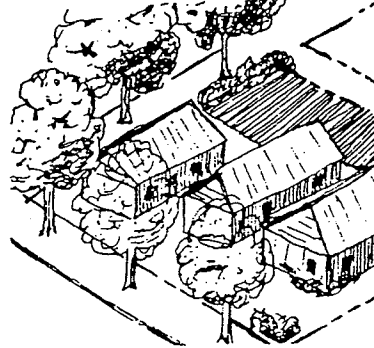


- Typical lot 50' x 142'.
- Off-street parking behind building in rear.
- Access from alley.

BULK



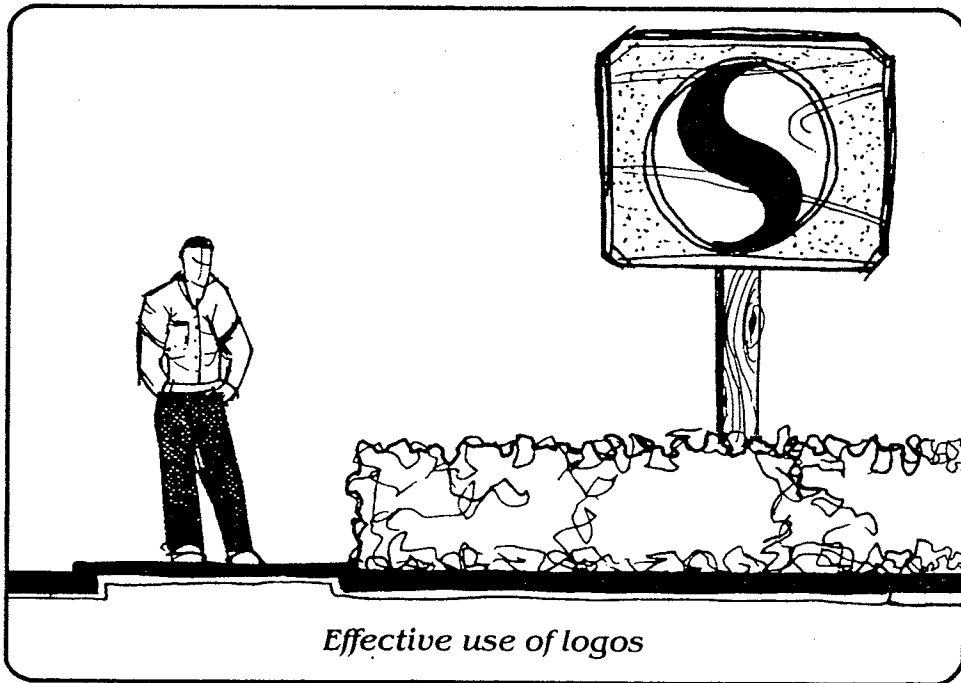
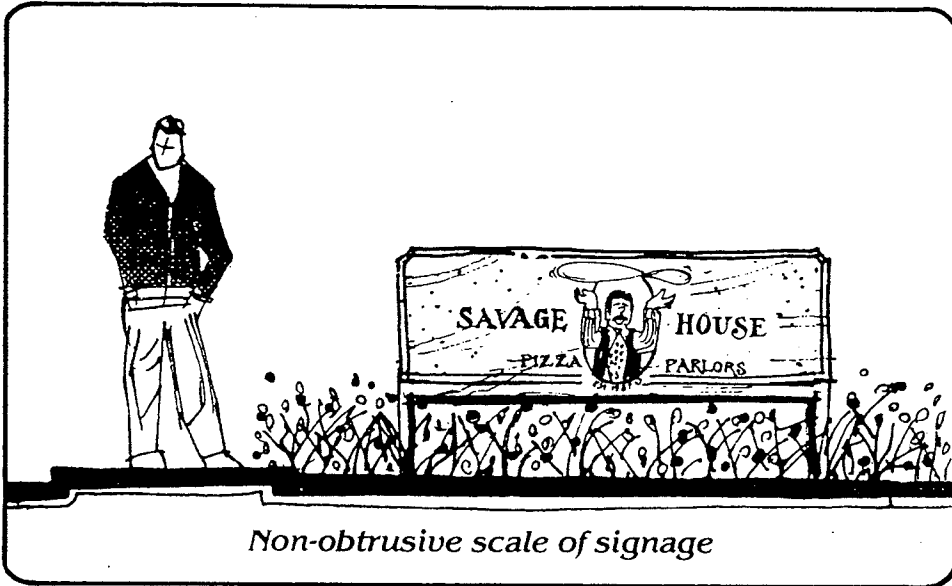
- Reduce maximum site coverage for taller structures.
- Landscape parking.



- Landscape character — residential.
- Plantings to buffer parking.

Zoning Standards for Commercial Use (Policy 1a)

- b. Landscape all required yards and portions of lots between building and required yards except that portion of the rear yard and lot used for necessary parking. Necessary "208" storm water drainage areas are considered part of the landscaping.
- c. Allow signs outside the North Riverbank area as provided by the "L" zone of the Zoning Ordinance, except that signs may not extend higher than 30 feet nor above building's roof ridge line, and that billboards are prohibited.
- d. Require parking lots to be improved as described in Residential Policy #6.
- e. Require service access to the rear of buildings, using alleys whenever possible. Design parking lots to minimize direct access to arterial streets. Consider, instead of individual business accesses, consolidated access for a block, access to a business from side streets which intersect the arterial or from alleys within a block of businesses. Preference for off-street parking location will vary business to business; however, the key is eliminating or reducing direct access to arterials (see also Commercial Policy #3). Assure that parking lot designs provide ready access to all buildings for emergency vehicles.
- f. In evaluating rezoning and special permit proposals for new retail or service commercial development, assure the capacity of the serving arterial street network is adequate to accommodate the expected vehicular traffic. It is essential that individual development decisions be made responsibly with regard to the capacity of the affected portion of the arterial system. If the traffic to be generated by a proposal exceeds the capacity of the arterial network, it should be denied. To assure an appropriate relationship is maintained, the Traffic Engineer should certify whether or not development proposals will or will not generate traffic in excess of arterial capacity.
- g. Enclose and contain within a structure all operations, processing and storage (including storage of waste materials) functions, or screen them from view of adjacent properties. Assure that proper fire safety and aquifer protection measures are taken within structures.
- h. Enclose or screen from view of adjacent properties all off-street loading areas and refuse storage areas.



Streetscape Signage (Policy 1c)

- i. Contain on-site (or provide attenuation to levels compatible with applicable federal, state or local standards) all noise, dust, odor, light, vibration, gases, wastewater, including stormwater, and any other impact of operations.

Discussion

Various activities and land uses produce impacts of different types and magnitudes. Traditionally, uses with similar impacts are grouped together so as not to offend one another or at least to confine the impacts to specific areas. However, it is now technically possible to mitigate to acceptable levels, almost any impact from any activity. Therefore, even the most objectionable activity would be acceptable in any location if wholly contained within a building which also completely confines the noise, light, fumes and odors, and the building and site are attractively constructed and appointed. It would be considered a good neighbor.

It is intended that rigid standards not be set, but development proponents be given the flexibility to meet these objectives in whatever way is socially acceptable. This system thrives on innovation. The resulting environment is far richer and more active than typical strip business districts.

The cost of meeting the necessary design and performance objectives will, from a practical standpoint, limit the range of activities locating in a neighborhood setting. However, in this situation the market place becomes the primary driving force rather than local government regulation. The guiding principle becomes one of being a responsible neighbor, respecting and being sensitive to those around you, no matter what activity you are engaged in or where you are located.



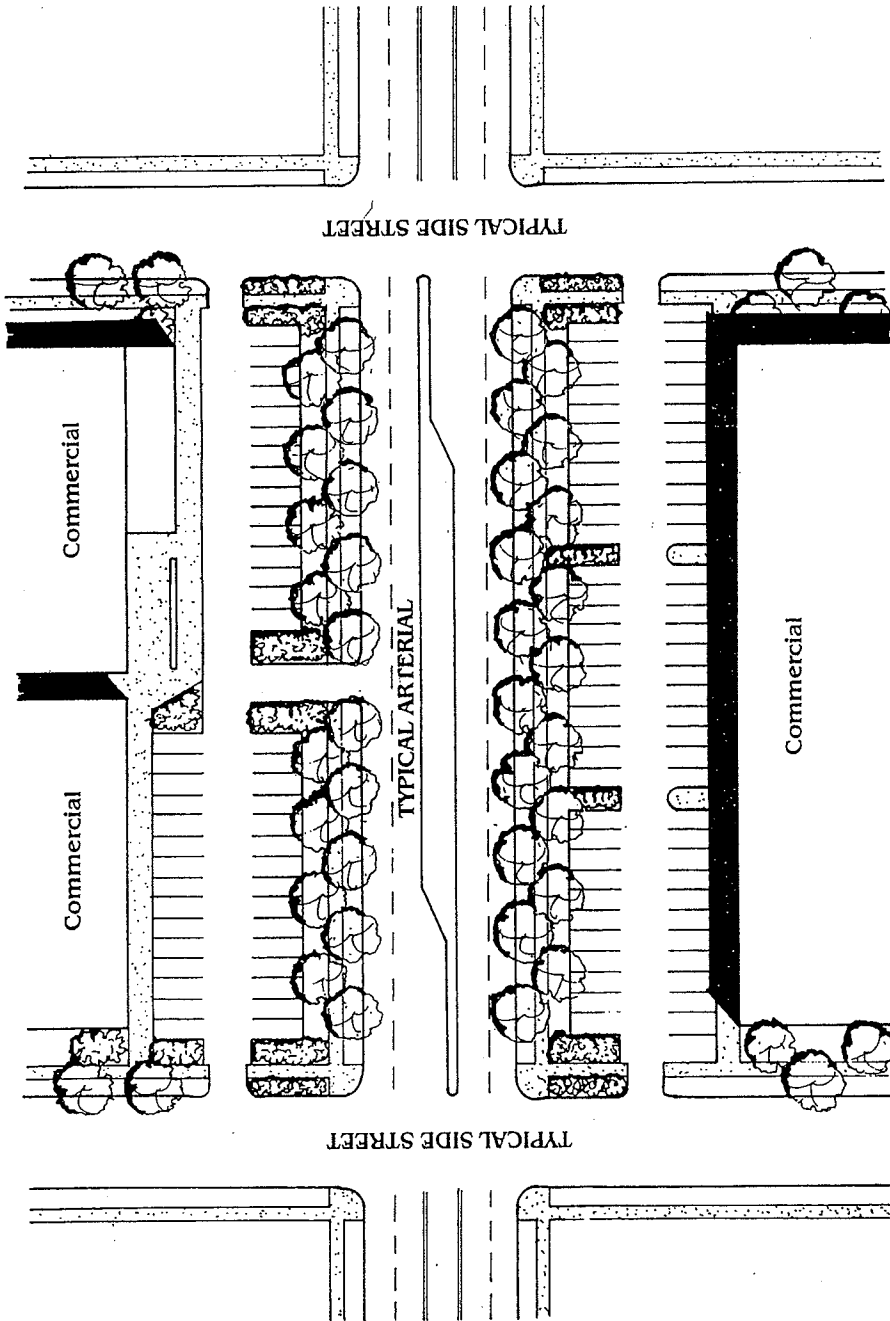
BUSINESS LOCATION POLICIES:

2. Establish the limits of business development generally as shown on the Neighborhood Land Use Plan Map for areas designated General Commercial, Commercial Mixed Use and Design Corridor Mixed Use.
3. Allow limited expansion of arterial business sites but only if their orientation remains toward the arterial. Promote access to business sites from side streets perpendicular to the arterial but only within the front one-third of the site's depth from the arterial.
4. Encourage the concentration of business activities at the intersection of Northwest Boulevard and Monroe, promoting neighborhood scale, retail and business services.
5. Retain the feeling of openness along Northwest Boulevard and improve its physical attractiveness. Consider making public improvements along the arterial right-of-way, especially the small triangles of land and traffic islands.

Policy 3

- Allow expansion if orientation is toward arterial
- Promote access from side streets

CITY OF SPokane PLANNING COMMISSION



Discussion

The general commercial areas in the Neighborhood are, for the most part, traditional commercial strips. They evolved early in the Neighborhood's history providing retail sales and business services oriented to passing traffic.

Strip commercial development is among the most unattractive forms of business activity. It typically develops as small individual sites with little or no off-street parking, buildings constructed to the property line with a proliferation of unsightly protruding signs. There is often no uniformity of architectural style; sites have no landscaping or other amenities; and the scale of buildings varies widely. Those businesses that do have off-street parking require access to the arterial which significantly reduces the ability of the arterials to carry traffic (automobiles entering and exiting the arterial from multiple curb cuts substantially slow traffic).

Commercial strips are linear features; therefore, the interface between business and residences is much larger than clustered commercial districts. Developing without adequate standards, extremely harsh transitions were created. Homes that experience these harsh conditions are undesirable living situations. Promoting an attitude and concept of businesses being good neighbors of residential areas and improving the overall attractiveness of commercial areas is the thrust of these policies.

MAPLE/ASH TRANSITION POLICIES:

6. Allow medium-density residential uses to occur on the west frontage of Ash (south of Montgomery) and east frontage of Maple (south of Mansfield) if appropriate site design and performance objectives can be met (see Residential Policy 6). Allow such development to extend one-half block away from the arterials but only if orientation to the arterial is retained, and access is allowed only on one-third of the site closest to the arterial.

Discussion

The block between Maple and Ash south of Albertson's is designated as a mixed-use district promoting a combination of medium-density residential, and low-rise office uses. To facilitate an easy transition to the low-density residential areas to the east and west, a layering of land use intensities is suggested.

CIRCULATION POLICY CONCEPTS

Overview

Because people generally live in one place but work, shop and recreate in another, a good bit of moving around takes place. The circulation system which results occurs at a variety of scales, e.g., city and neighborhood.

The patterns are determined by the location of major activity centers and residential areas, as well as the location and magnitude of physical transportation improvements, i.e., arterial streets and bus routes.

Circulation in the Emerson-Garfield Neighborhood responds to primarily two factors: trip origins and destinations, and mode of travel. Trip origins and destinations largely determine whether traffic is passing through the neighborhood or is moving within. Mode of travel differentiates between the needs of automobile traffic, bus service, pedestrians, and bicyclists. The policy concepts in this section were developed to respond to the differences created by these two factors.

Vehicular Through Traffic

These policy concepts relate to the city's Arterial Streets Plan and its relationship to the Emerson-Garfield Neighborhood. This Plan identifies present arterials, the streets which are proposed to become arterials, and the traffic loads that these arterials are designed to carry. There are three arterial designations which are relevant to the Emerson-Garfield Neighborhood. They are summarized as follows:

Principal Arterial - designed to expedite movement of through traffic to major traffic generators (such as the Central Business District, major industrial complexes, universities, military installations) and between major sectors of the city. They collect and distribute traffic from freeways and expressways to lesser arterials or directly to traffic destinations. They generally include four to six travel lanes with restricted parking, and they are designed to effectively carry up to 30,000 vehicles each day.

Minor Arterial - designed to collect and distribute traffic from higher type arterials to lesser streets or directly to traffic destinations and to serve secondary traffic generators (such as shopping centers, high schools, apartment areas, hospitals). They move traffic from neighborhood to neighborhood within major sectors (community) of the city. They generally include two to four travel lanes (with or without parking restrictions), and they are designed to effectively carry up to 10,000 vehicles each day.

Collector Arterial - designed to collect and distribute traffic from higher type arterials to local access streets or directly to traffic destinations. They provide for movement within a neighborhood serving neighborhood traffic generators (such as supermarkets, elementary schools, churches, clinics). They generally include two travel lanes with parking, and they are designed to effectively carry up to 2,500 vehicles each day.

VEHICULAR THROUGH TRAFFIC POLICIES:

1. The following streets are designated Principal Arterials as indicated in the Arterial Plan:
 - a. Division Street through the Neighborhood.
 - b) Washington Street from Buckeye Avenue through the south boundary of the neighborhood;
 - c) Monroe and (Lincoln or Post) Streets as a couplet from Buckeye Avenue through the south boundary of the neighborhood, then Monroe Street alone from Buckeye Avenue through the north boundary of the neighborhood;
 - d) Northwest Boulevard from Monroe Street through the west boundary of the neighborhood; and
 - e) Mission Avenue from Maple/Ash through the east boundary of the neighborhood.

2. The following streets are designated Minor Arterials as indicated in the Arterial Plan:
 - a) Boone Avenue (thence Atlantic Street and Sharp Avenue) through the neighborhood;
 - b) Indiana Avenue from Monroe Street through the east boundary of the neighborhood;
 - c) Buckeye Avenue from Maple/Ash Streets through the east boundary of the neighborhood; and
 - d) Mission Avenue from Maple/Ash Avenue through the west boundary of the neighborhood.

3. The following streets are designated Collector Arterials as indicated in the Arterial Plan:
 - a) Belt from Northwest Boulevard to Boone Avenue;
 - b) Indiana Avenue from Belt Street, thence Adams Street to Northwest Boulevard;
 - c) Howard Street from Buckeye Avenue through the south boundary of the neighborhood; and
 - d) Post Street from Buckeye Avenue through the north boundary of the neighborhood.

Discussion

All policies reflect those designations in the Arterial Plan, and due to its recent adoption, no changes are recommended. The development of a couplet using Monroe and either Lincoln or Post is uncertain at this time. The City Council's adoption of The Arterial Plan anticipates a limited couplet system initially that would revert to a single arterial (Monroe) south of the Emerson-Garfield neighborhood. This initial phase will not effect the neighborhood. However a second phase, if it is found necessary, would extend the couplet development north to Buckeye.

Whether Post or Lincoln becomes the northbound leg, each leg of the couplet should be limited to three travel lanes. The balance of the street right-of-way should be used to incorporate such streetscape improvements and design features as are described in the Neighborhood Design concepts chapter.

INTERNAL NEIGHBORHOOD CIRCULATION POLICIES

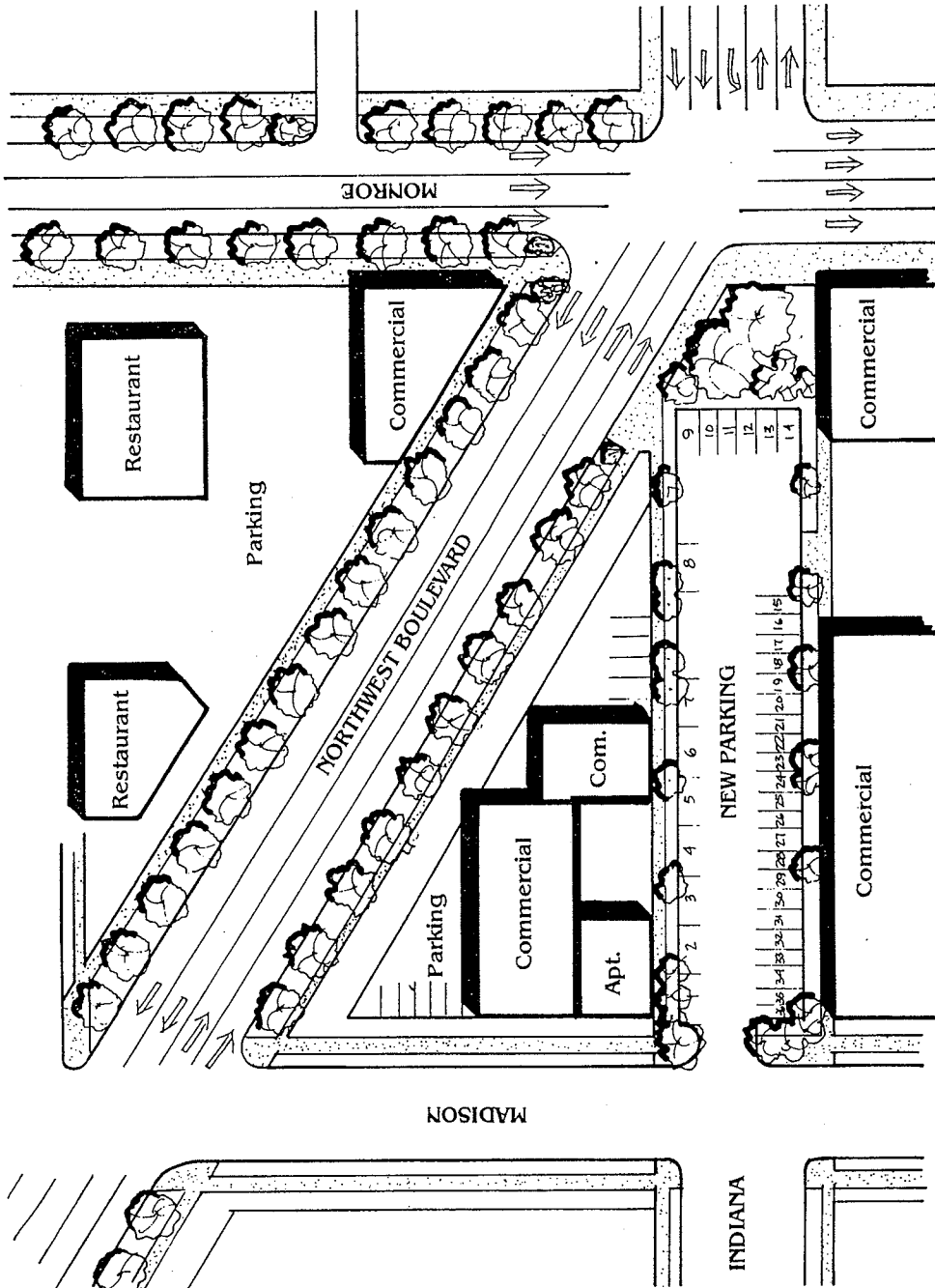
4. Discourage through traffic on Montgomery Avenue between Monroe Street and Northwest Boulevard. To accomplish this, consider the following:
 - a. Modify the signal light at Monroe and Montgomery to flash amber for Monroe and red for Montgomery until activated by pedestrian or auto to provide a green signal for Montgomery and red for Monroe.
 - b. Construct a physical restriction on Montgomery near Northwest Boulevard to discourage through traffic while still allowing minimum essential access.
5. Discourage through traffic on Grace and Cleveland Avenues between Northwest Boulevard and Maple/Ash Streets. Determine how this can best be accomplished by consulting with area residents (e.g., prohibit left turns from eastbound Northwest Boulevard.).
6. Close Indiana Avenue between Monroe and Madison Streets, thereby creating a four-way rather than five-way intersection. Retain access to properties from Madison and convert as much of the Indiana right-of-way between Monroe and Madison to automobile parking as possible. Control left turns from Monroe to Northwest Boulevard and Indiana.
7. Redesign the intersections along Northwest Boulevard to reduce their number, improve their spacing and improve visibility for entering or crossing the arterial. Section three of the Neighborhood Design Concepts chapter includes specific improvement recommendations.

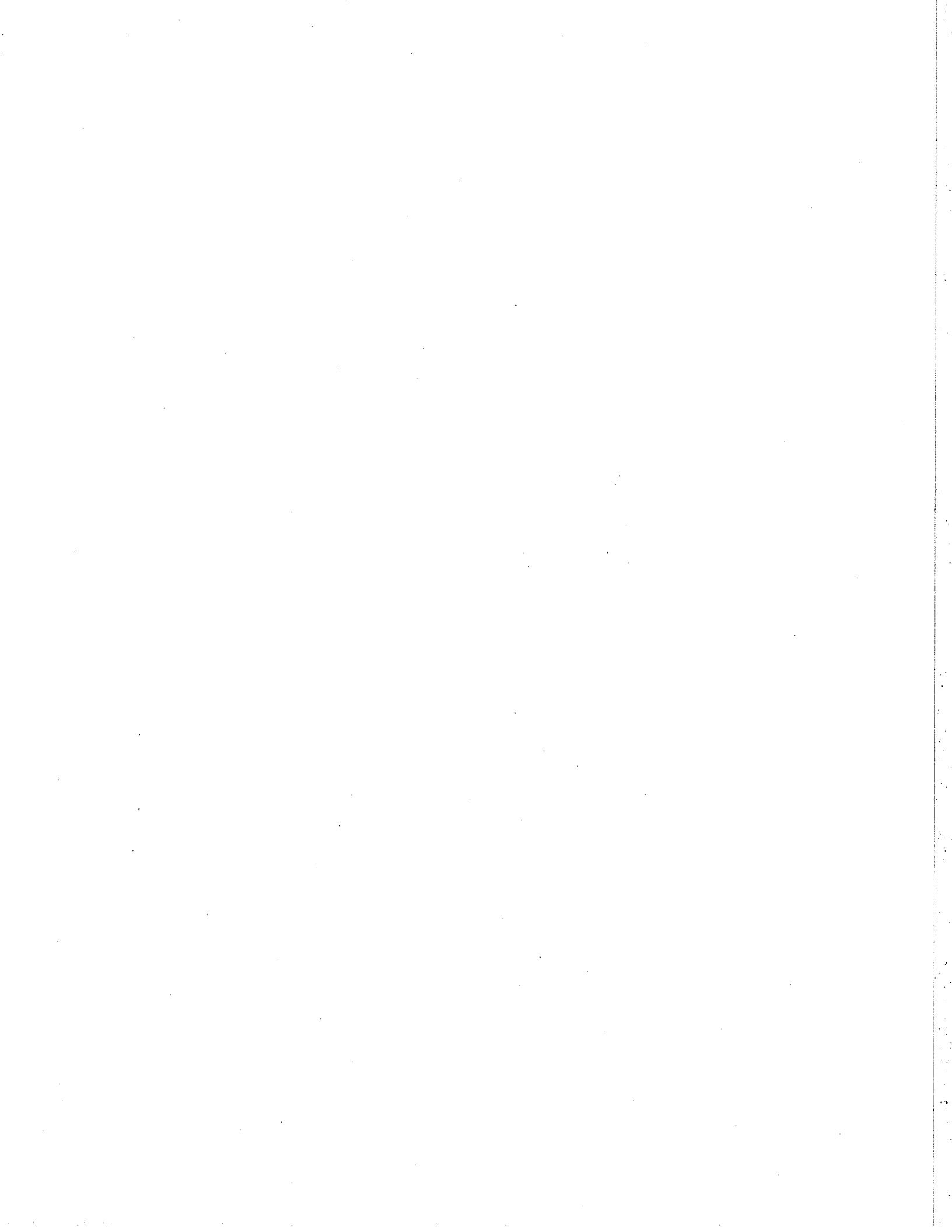


Policy 6

- Close Indiana Avenue between Monroe and Madison Streets
- Retain access to properties from Madison
- Convert to automobile parking
- Control left turns from Monroe to Northwest Boulevard and Indiana

CITY OF SPokane PLANNING COMMISSION





Discussion

Adequate internal circulation is essential to the connection of neighborhood features and residential areas. Typically, neighborhood collector arterials make these connections; however, Principal and Minor arterials perform the function as well. The policies largely relate to the City Arterial Streets Plan insofar as evaluating its adequacy in providing internal circulation.

Equally important as assuring adequate internal circulation is promoting and/or discouraging particular travel patterns (e.g., discouraging inappropriate travel on residential access streets). Protecting residential areas from traffic that should be carried by arterial streets is a major ingredient in maintaining the Neighborhood's quality of life.

BICYCLE AND PEDESTRIAN CIRCULATION POLICIES:

8. Support the implementation of the City Bikeways Plan insofar as it applies to the Emerson-Garfield neighborhood.
9. Develop bicycle and pedestrian routes that connect the neighborhood's major features (e.g., Corbin Park, Emerson Park, Drumheller Springs, Audubon and Garfield Elementary Schools, Safeway and Albertson's, Monroe/Indiana intersection, North Central High School and the Rock Outcrop).
10. Provide connections from this internal route system to major features outside the neighborhood and to other parts of the city.
11. Consider in the development of these routes the provision of adequate sidewalks, on or off street space for bicycles, improved arterial crossings, improved appearance and aesthetic character, and adequate directional and informational signage.
12. In designing route improvements, pay special attention to the needs of the elderly and/or the handicapped, with particular regard for safe arterial crossings and curb cuts (e.g., timing of signal lights to allow elderly and handicapped to cross during a normal light cycle).
13. Reduce the number of automobile travel lanes on Howard Street between Indiana and Buckeye from four to two, utilizing the excess right-of-way for non-motorized traffic and streetscape beautification.

Discussion

The Bikeways Plan proposed a comprehensive network of bikeways in Spokane which corresponded to and connected with those proposed by county government just outside the city limits. With regard to how it addresses bicycle traffic passing through the Emerson-Garfield Neighborhood, the

network is adequate. However, the Bikeways Plan does not accommodate pedestrian travel, nor does it adequately deal with non-motorized internal neighborhood circulation. The neighborhood pedestrian and bicycle circulation system as illustrated on the map, page 27 is designed to complement rather than compete with the Bikeways Plan. Specific improvement projects, where suitable, should be joint ventures between the city and the neighborhood, with both parties' interests being addressed.

TRANSIT SERVICE POLICIES

14. Continue to provide north-south transit routes through the Emerson-Garfield Neighborhood separated by no more than four blocks and use arterial streets wherever possible.
15. Provide east-west transit routes through the neighborhood on arterial streets.

Discussion

The north-south transit routes already provide adequate service for most of the Neighborhood as the above-mentioned policy indicates, but east-west routes are significantly deficient. Major improvements in service through increased frequency of service and new east-west crosstown routes are proposed.

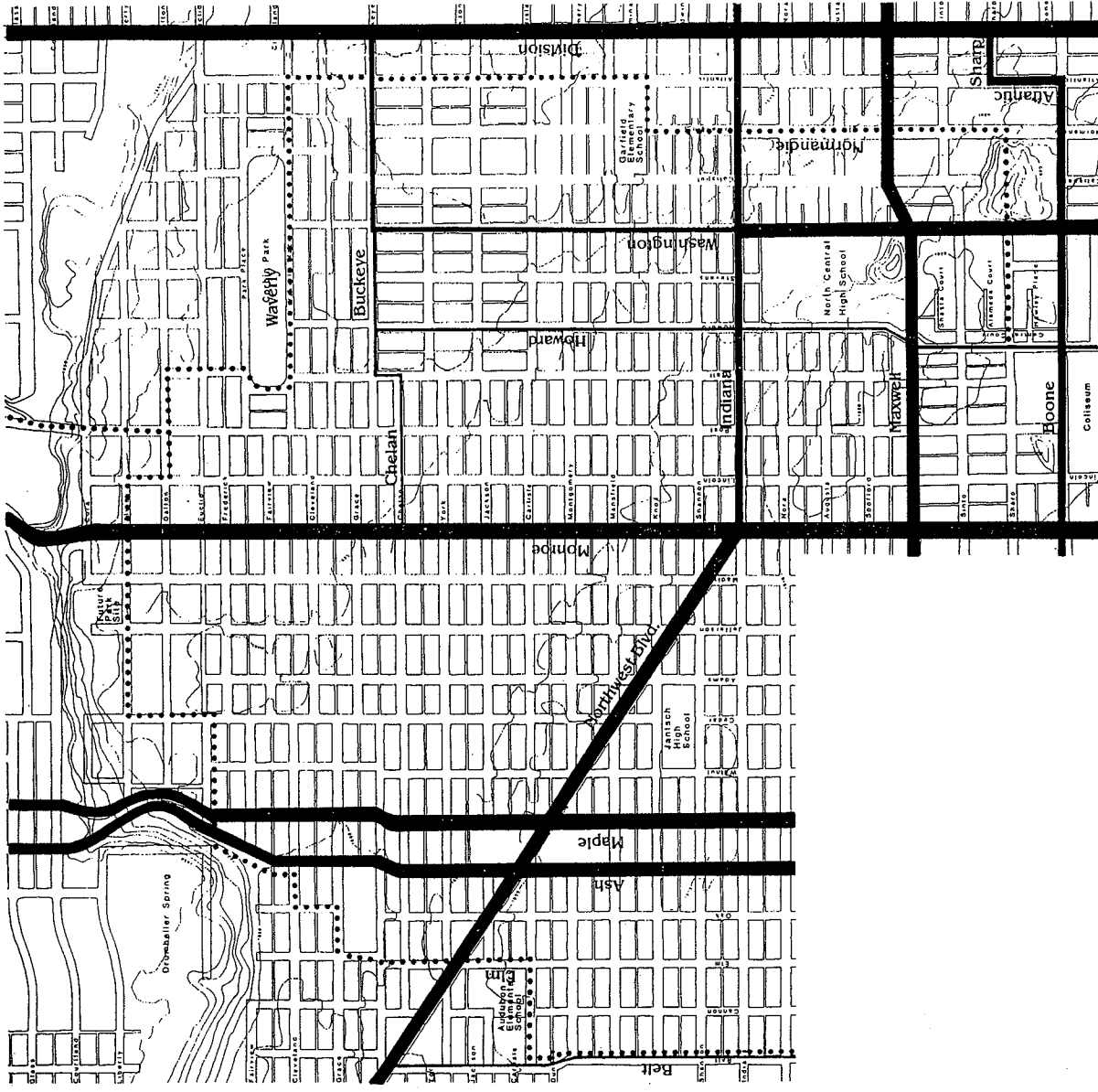
While it is realized that diesel powered buses are preferable to gasoline driven vehicles, there is still concern over suspended particulate pollution (unburned hydrocarbons). Since the Spokane metropolitan area is identified by the federal Environmental Protection Agency as a non-attainment (of federal standards) area for suspended particulates, the exploration of alternative propulsion technologies is encouraged.

TENNERSON GARFIELD NEIGHBORHOOD

Neighborhood Circulation Plan

	Major/Primary Arterial
	Secondary/Minor Arterial
	Collector Arterial
	Pedestrian/Bicycle Route

CITY OF SPokane PLANS COMMISSION



COMMUNITY FACILITIES POLICY CONCEPTS

Overview

The most obvious community facilities which are also major features within a neighborhood are public schools and parks (private schools and their playgrounds are included here as well). These facilities are often the focal points of neighborhood activities, and they help to define the character of the surrounding area. Community centers are community facilities, and they, too, act as a focus for social interaction among neighborhood residents.

PARKS AND OPEN SPACES POLICIES

1. Delete from the city-wide Park and Open Spaces Plan the proposal to expand the Havermale (now Jantsch) playfield for neighborhood park purposes.

Discussion

When the Park and Open Spaces Plan was adopted, the site of Jantsch Continuation High School was still Havermale Junior High, and the parking lot was a small playfield. It was appropriate at that time to recommend expansion of the playfield for improved neighborhood recreational use. This is no longer the case. Adequate park facilities are provided for this area with the enlarged playfield at Audubon Elementary, the planned expansion of A. M. Cannon Park and Skeet-so-Mish Mini-Park to the south.

2. Retain Drumheller Springs Park as a natural area, protecting its natural vegetation and wildlife. Where exotic vegetation has invaded the Park, it should be cleaned out and replaced with a natural species.
3. Utilize the old Emerson School multi-purpose structure for an indoor recreational facility. Consider the possibility of a live-in caretaker for the purposes of maintaining and watching over the property.

4. Retain the existing balance of active recreational facilities and passive recreational opportunities at Corbin Park.
5. Control the speed of traffic on streets around Corbin Park. Explore methods that would provide positive speed control, not simply posting a reduced speed limit.

Discussion

Neighborhood parks which serve the Emerson-Garfield area each have distinctive characteristics, and the policies seek to preserve and enhance this character--not change it. Drumheller Springs Park, just beyond the neighborhood's northwest boundary, is a natural area with a rich plant and wildlife population which should be preserved. Emerson Park is designed primarily as an active recreational facility for children, and adding the old school's multi-purpose structure would enhance this function (the structure is now being used as a storage warehouse). Corbin Park contains a mixture of active recreational facilities for children and young adults and passive recreational opportunities for families and senior citizens. Retaining the existing balance and improving user safety is desirable.

6. Encourage the provision of public open space and major viewpoints on the rock outcrop at Washington and Boone as proposed in the North Riverbank Design Plan. Assure that the open space is easily accessible to the general public and that people using it feel comfortable (that is, not made to feel like intruders).

Discussion

This is a proposal for a new park and open space facility, not previously a part of the Park and Open Spaces Plan. The open space and viewpoints on the rock outcrop are described in the North Riverbank Design Plan as an integrated part of a recommended residential and office development for that site.

SCHOOL PLAYGROUND POLICIES

1. Continue to promote the joint use of public school playgrounds as neighborhood parks and community playfields. Where appropriate, provide additional play equipment to enhance park and playground functions. Reduce barriers to the use of school playgrounds, such as fences and closed gates.
2. Explore the potential for enhancing the use of school multi-purpose rooms and gymnasiums for organized recreation and open neighborhood free play.
3. Provide information to neighborhood parents and children regarding the availability of school playgrounds and facilities for organized recreational activities and free play opportunities.

Discussion

School playgrounds provide excellent opportunities for play, whether organized or informal, and the City of Spokane and School District 81 have long had agreements for joint use of facilities. Therefore, school playgrounds are considered to be serving the same function as neighborhood parks. Oftentimes, however, there exists a psychological barrier against using school facilities other than as part of school-related activities. These perceived barriers are often reinforced by physical elements such as fences with closed gates around playfields. The use of these public facilities for general neighborhood recreation can be significantly enhanced by breaking down the psychological barriers and neutralizing the effect of physical barriers. There also may be potential for more intensive use of schools' indoor and outdoor facilities. It is recognized, of course, that using school facilities requires some formal supervision. While this is normally a function of the City Parks and Recreation Department, some appropriate means of funding this supervisory requirement will have to be found.

4. Explore the potential for developing a small neighborhood park in association with St. Anthony's private school at Montgomery and Adams, including a formal, mutually beneficial operating agreement.
5. Explore the possibility of joint use and improvement of the Spokane Junior Academy playground for neighborhood park use. If appropriate, provide additional play equipment to enhance its playground function.

Discussion

A public-private partnership is suggested for a new, small neighborhood park/school playground at St. Anthony's and for the joint use of Spokane Junior Academy's playground. Similar agreements with other private schools exist, with public funds being used to acquire property and construct facilities. The completed park/playground is used cooperatively by the school and the surrounding neighborhood, with a formal agreement of mutual benefit executed between the School and the City.

COMMUNITY CENTER POLICIES

1. Expand the Corbin Community Center parking lot to Lincoln Street and strengthen the functional and visual relationship between the parking lot and the Center's main entrance. Concurrently, restrict on-street parking around Corbin Community Center.
2. Promote more effective information sharing in the Emerson-Garfield neighborhood regarding programs and activities at the West Central Community Center.

Discussion

The Corbin Community Center is operated by a private, nonprofit organization in cooperation with the City, with programs largely tailored to the elderly. There are two small parking lots to the rear of the Center but they are not well-oriented to the front of the building on Lincoln Street. Intense on-street parking congests the streets, especially west of the center (this is also due in part to employee parking from nearby businesses). Expanding the south parking lot to Lincoln will strengthen its relationship to the Center's main entrance, making it more likely to be used. Removing on-street parking will improve area aesthetics and the function of the adjoining streets.

Better information distribution and publicity is necessary for programs of both community centers but especially for the West Central Community Center, because it is outside the Emerson-Garfield neighborhood. There may also be a psychological hesitation to participate in activities at the West Central Community Center activities, because it is associated with another neighborhood organization. The Center staff should strive to overcome this perception.

UTILITY POLICIES

1. Require connection to city utilities (sewer and water) systems for all new development in the Emerson-Garfield neighborhood.
2. Support the speedy completion of all storm sewer-sanitary sewer separation projects in the neighborhood.





Discussion

The city water system serves all the developed portions of the neighborhood without significant deficiencies. It has the potential capacity to easily serve those undeveloped areas when projects are proposed.

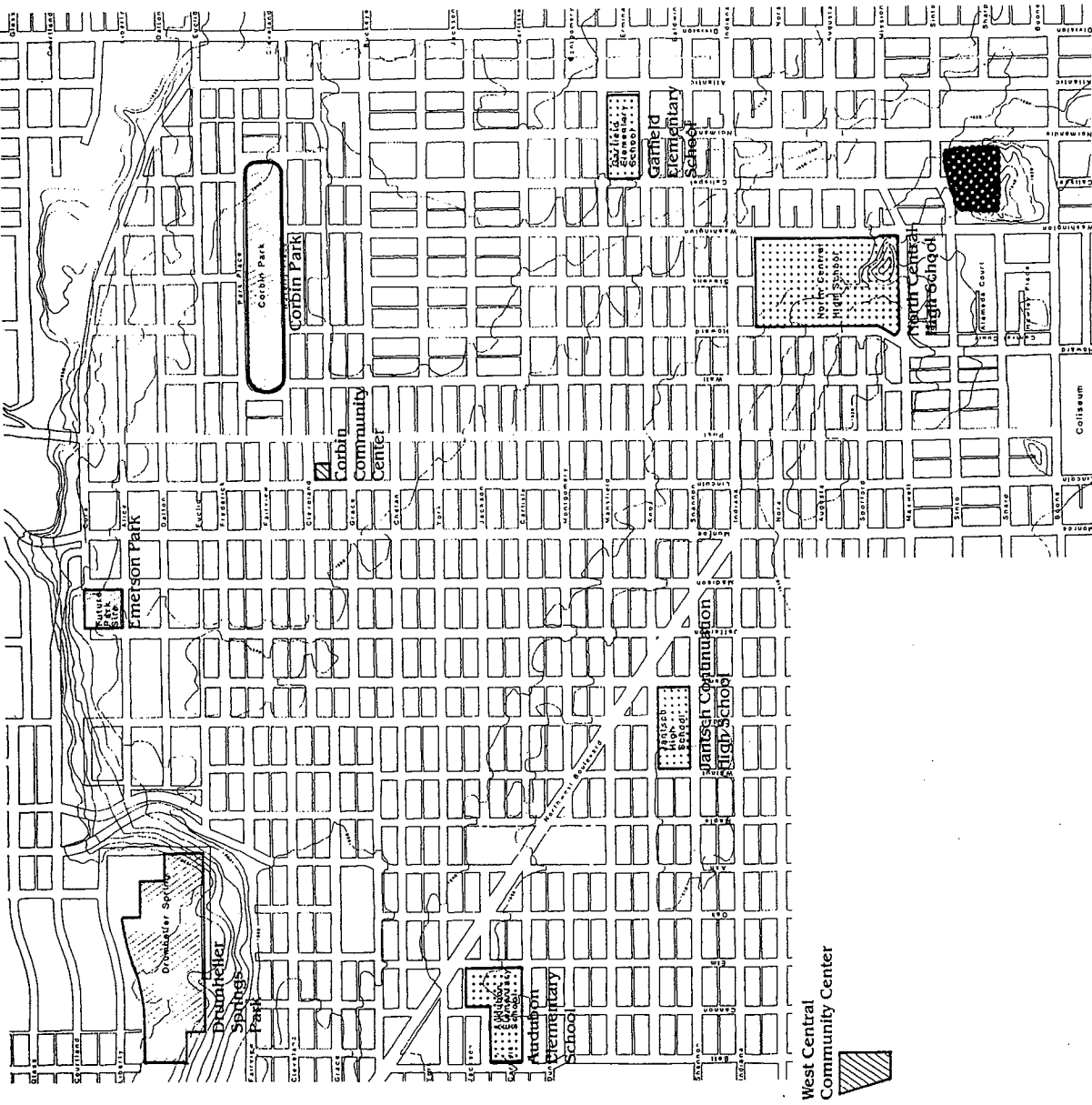
The sewer system, while available and serving all the developed portions of the neighborhood, does have significant system deficiencies. Because it is a combined storm water-sanitary waste water collection system, heavy rainfall often causes it to overflow, passing untreated sewage into the river. Since it also passes storm water along with sanitary waste water to the sewage treatment plant, the plant is treating unnecessarily large volumes of water, misusing much of its treatment capacity. The city is undertaking a major program to separate the storm water from the waste stream and several large projects are planned for the Emerson-Garfield neighborhood.

EMERSON GARFIELD NEIGHBORHOOD

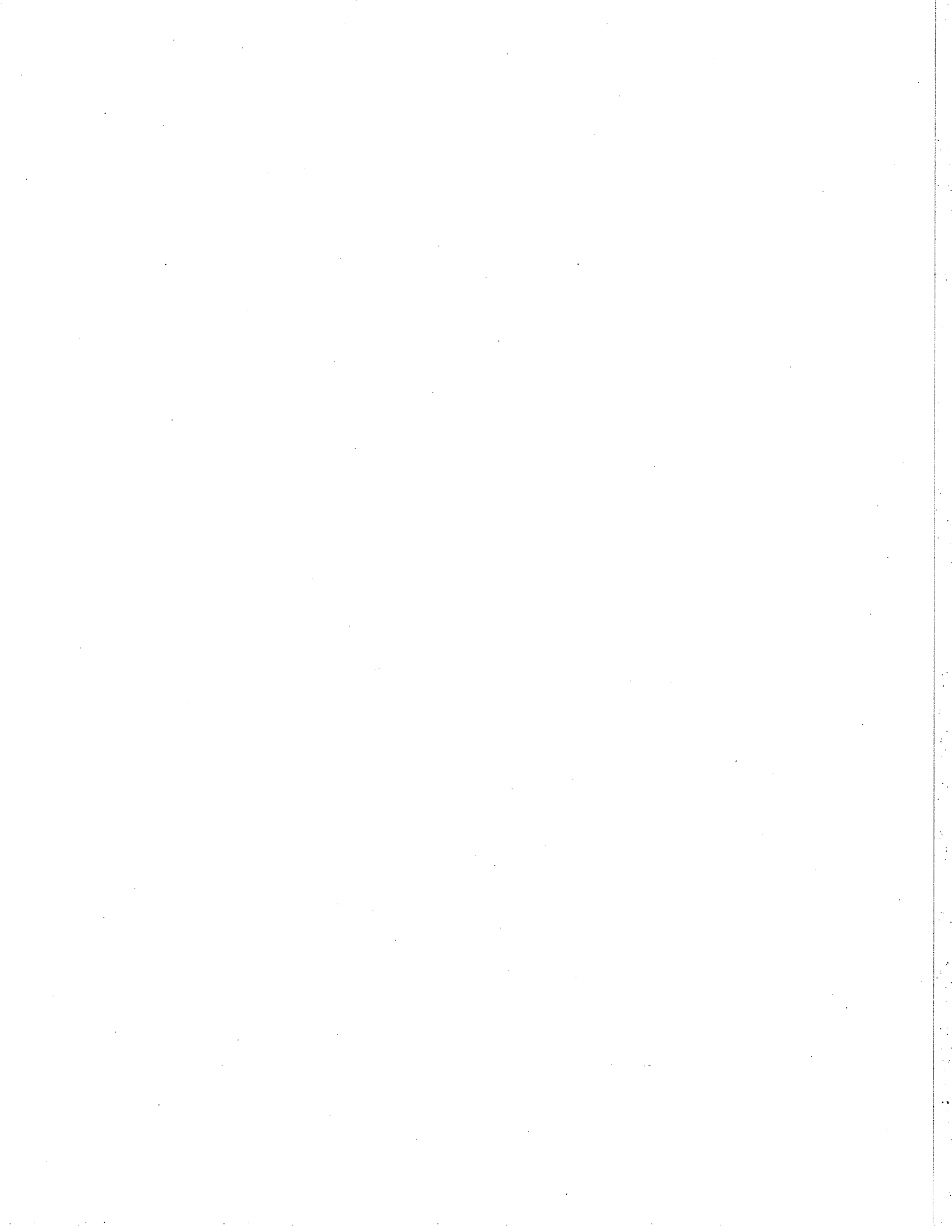
Neighborhood Facilities Plan

-  Neighborhood Parks
-  Public Schools
-  Community Centers
-  Other Open Space

CITY OF YONKERS PLANNING COMMISSION



West Central Community Center 



EMERSON

GARFIELD

NEIGHBORHOOD

DESIGN

CONCEPTS

Chapter 2:

- Enhancing the Neighborhoods Image
- Historic Heritage & Character
- Mixed Use Objectives
- Residential-Commercial Interface: Bonus Density
- Site Specific Design Recommendation



ENHANCING THE NEIGHBORHOOD'S IMAGE

A good neighborhood contributes to a sense of well being. Satisfaction with life and personal happiness are, in part, the result of how good people feel about their relationship to their surroundings. The feeling of pride associated with being a member of a family, an organization or a neighborhood contributes substantially to this satisfaction by providing a sense of belonging and contribution.

A design plan can affect physical surroundings--the image of a neighborhood and the home. Pride can be enhanced. A perception of beauty is dependent on more than the absence of nuisances or more than a topical application of pretty things. Beauty in the neighborhood is the result of compatible development, pleasing spaces, unity, variety, and richness. A sense of belonging in such a neighborhood raises a person's self-esteem and happiness.

We perceive our neighborhood through all our senses, but especially through sight. By establishing a strong, positive visual impression, a neighborhood image becomes special and long lasting.

STREET TREE POLICIES

1. Plant new trees along the neighborhood's streets where they do not currently exist, and replace those suffering from age and disease.

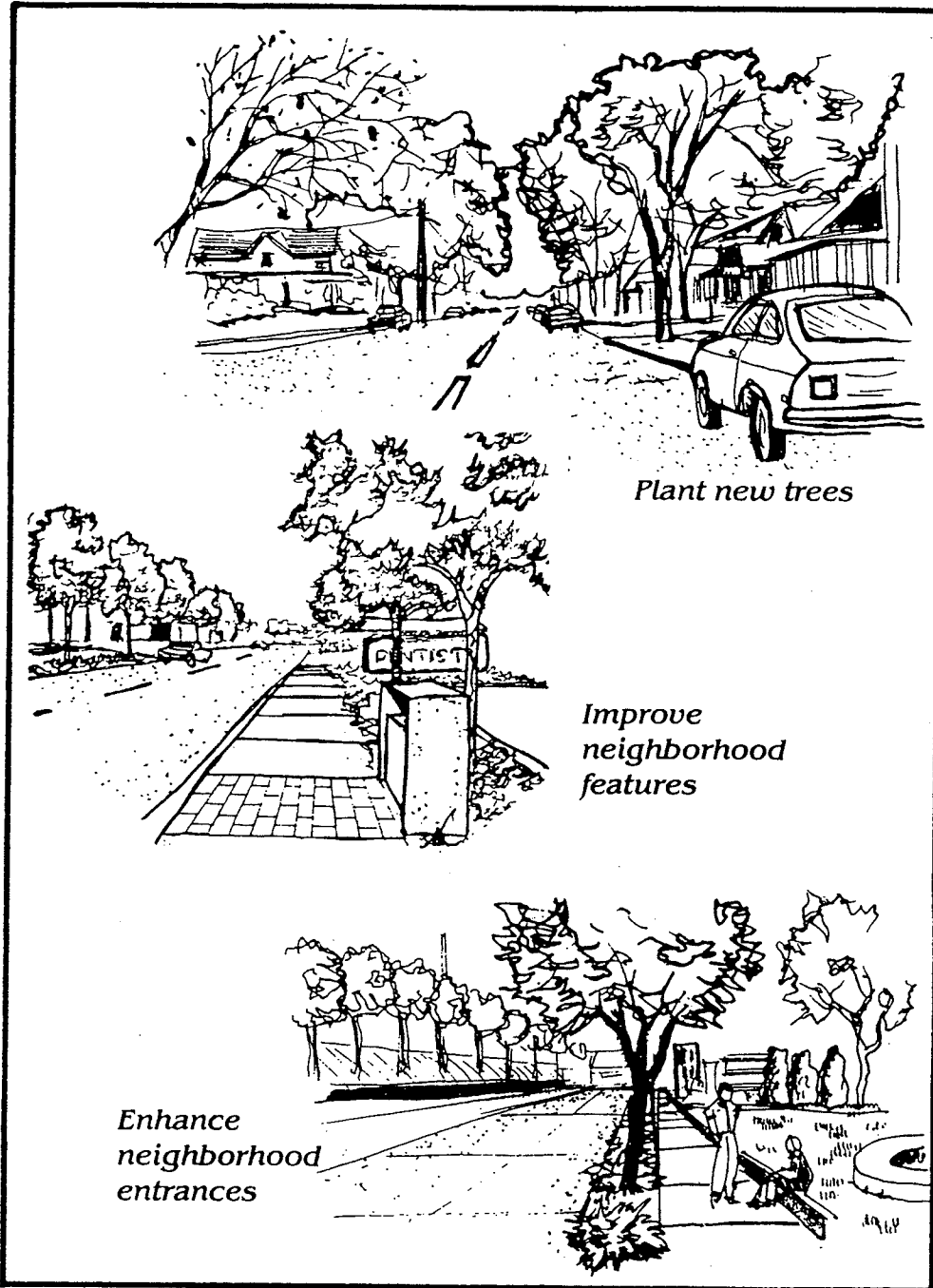
Discussion

Trees make this city. Neighborhoods with abundant trees along its streets and in its yards are considered to be among the most desirable places to live. Emerson-Garfield has a rich urban forest in many areas; however, many of the trees are approaching the end of their life span and will require replacement. Trees contribute to the beauty of the streetscape whether they grow within the public right-of-way or are planted on private property adjacent to the street.

An example of an effective and desirable street tree planting arrangement involves a double layer of trees, one in the parking strip along the street curb, and another just inside the sidewalk on private property. The pedestrian environment along the sidewalk is especially attractive. The double layer of trees between the street and home provides greater buffering of traffic impacts than a single row along the parking strip, and it even provides a psychological barrier between the sidewalk and home, resulting in a greater sense of privacy. In those areas where parking strips do not exist, trees should be planted just inside the sidewalk, perhaps in an offset double layer.

In addition to native pine, several varieties of broadleaf deciduous trees exist throughout the neighborhood. Norway Maple is the most common variety. Black Locust, Silver Maple, Birch and London Plane are also





Plant new trees

Improve neighborhood features

Enhance neighborhood entrances

Design Concepts (Policy 1)

well represented, and around Corbin Park, Elm is abundant. Replacement trees and fill-in trees, as well as new tree plantings, should be selected from varieties which are compatible with the rest of the neighborhood. Specific areas can benefit from a coordinated tree planting to reinforce a design concept.

Street trees are often associated with severe sidewalk damage; in fact, some neighborhood funds have been spent in repair of sidewalks broken and heaved by tree roots. Often these problems occur because consideration has not been given to the size of the planting area, the growth patterns of the tree, and planting and maintenance techniques. In general, shallow root varieties should be avoided, and deep root varieties should be promoted. Tree wells can be used to physically confine surface root growth, and trees should be provided with deep irrigation rather than surface watered. The Spokane Parks and Recreation Department has produced a guide for street tree selection. It is entitled "Street Trees for Spokane--A Guide To Selecting Trees for Street-Side Planting Areas," and it is available to the public upon request.

STREET FURNITURE POLICIES

1. Use street furniture along arterials and key neighborhood linkages to improve the street-side environment and help to express specific themes.

Discussion

Street furniture includes both functional and decorative items, such as litter and waste receptacles, newspaper stands, sign standards, benches, water fountains and planters. Often considered luxuries or unnecessary amenities, if they are properly placed along prescribed routes, they greatly enhance the street side environment.

Street furniture should be attractively designed and constructed and should reflect the character of the immediate surroundings. For example, modernistic designs are inappropriate in neighborhoods with rich historical character. Street furniture designs can also be used to create and support specific themes in mixed-use districts or along particular linkages between key neighborhood features.

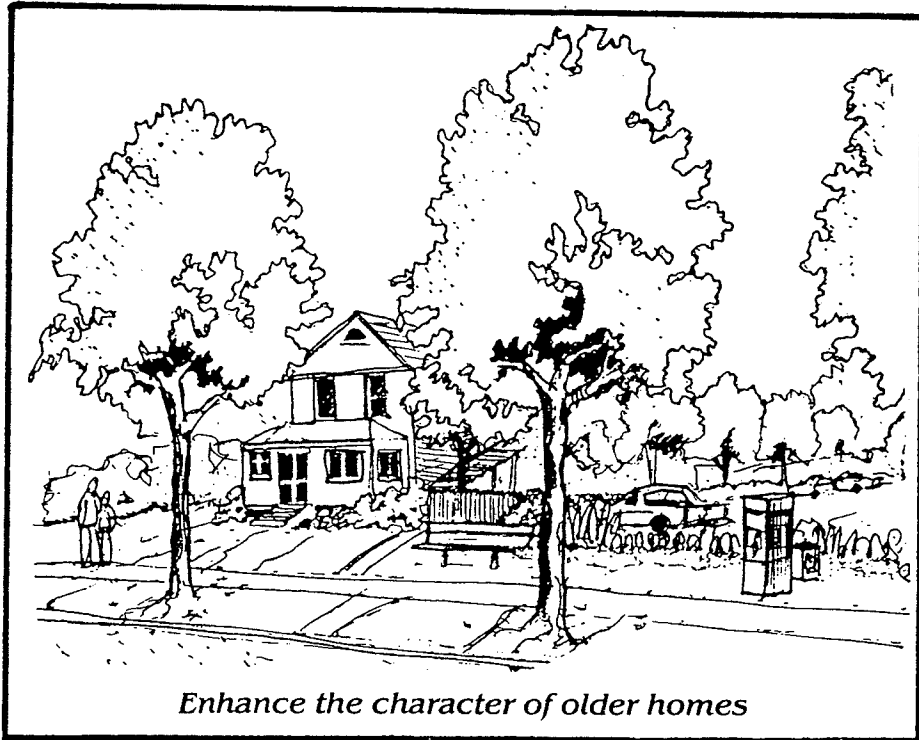
NEIGHBORHOOD ENTRANCE POLICIES

1. Develop and enhance selected neighborhood entrances.

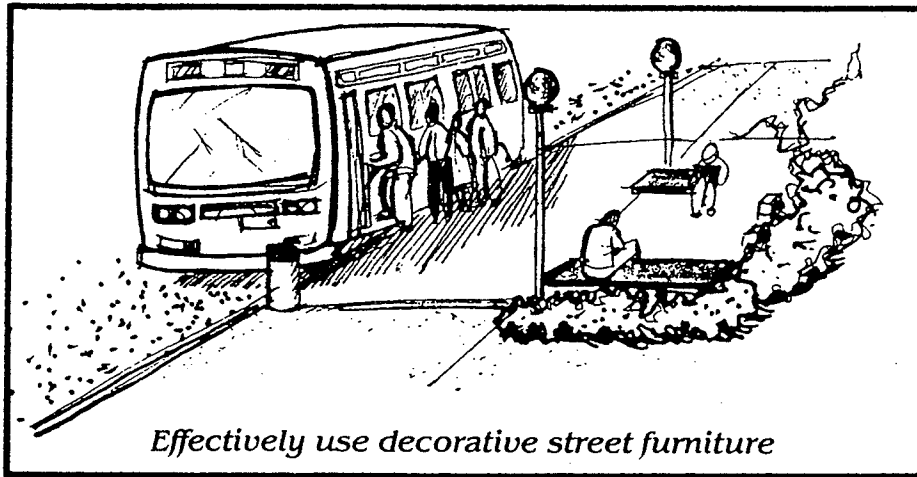
Discussion

Entering the Emerson-Garfield neighborhood from elsewhere in the city is not particularly noteworthy. There is little in the way of man-made or natural features marking the neighborhood's boundary except along the





Enhance the character of older homes



Effectively use decorative street furniture

Design Concepts (Policy 1)

north, where several opportunities exist to improve the visual image of the neighborhood. There is excess right-of-way along arterials as they break down the north hill slope, and simple beautification through landscaping would be a desirable improvement. Such opportunities exist at Maple/Ash, Monroe and Post. Some form of neighborhood identification, such as a sign or symbol, could be included but it is not essential. The intent is to create a positive image of the neighborhood.

NEIGHBORHOOD IMAGE POLICIES

1. Project a positive neighborhood image and identity through subtle pleasant features and improvements.

Discussion

The image created for the neighborhood should be pleasant and discreet. Improvements along arterials, in commercial areas and throughout the residential portions of the Emerson-Garfield neighborhood should contribute to a sense of unity and order, a richness of character, the neighborhood's excellence and a caring quality of life.

Informational and directional signs are important along routes linking the neighborhood's key features. Signage should reflect the nature and character of the features they connect, as well as respond to the immediate surroundings. Imagination should be employed, and small metal rectangles on stark metal standards should not be considered acceptable. Advertising signs for businesses are important elements, and they should also contribute to a pleasing neighborhood image.

Ornamental street lighting can be a major part of a neighborhood's image. Variations in style can be used to highlight specific routes through or within the neighborhood, as well as help establish themes for mixed-use districts. It is important that special street lighting be used only where it is necessary to convey a message. Used universally throughout the neighborhood, such techniques lose their significance and impact.

HISTORIC CHARACTER POLICIES

1. Identify and recognize the significant private properties and buildings in the neighborhood, acknowledging their contribution to the neighborhood's overall character and flavor.

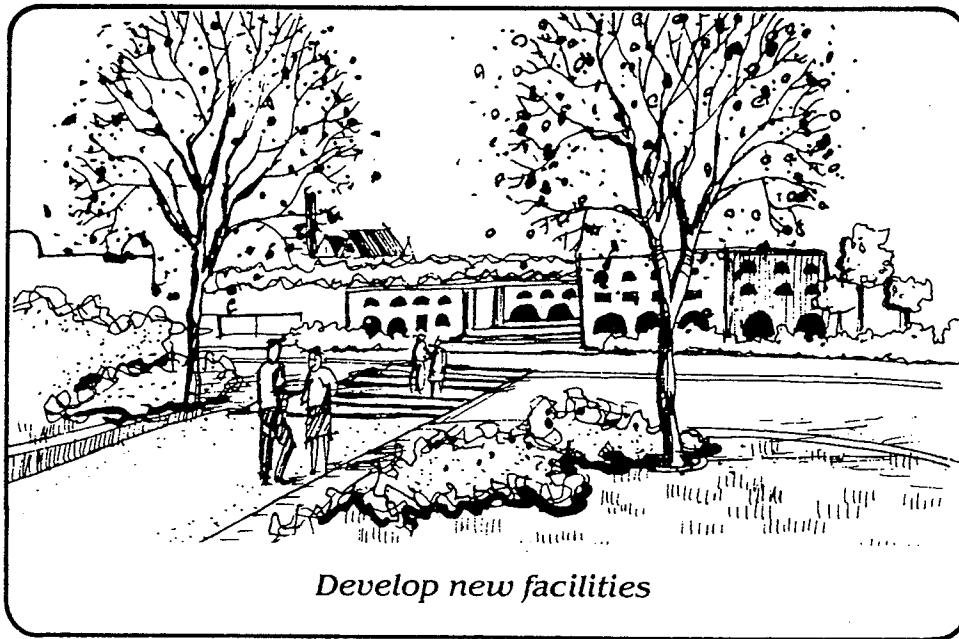
Discussion

Several parts of the neighborhood have a rich historic background that is still reflected in the external appearance of the structures. To preserve this character for future generations, we must identify those structures and areas of significance and formally recognize them. For properties or districts that qualify designation on the city, state or national register of historic landmarks should be sought. Neighborhood





Restore neighborhood businesses



Develop new facilities

Design Concepts (Policy 1)

recognition, through presentation of plaques or certificates or through special citation in neighborhood newsletters/newspapers, is also an important consideration.

Similar recognition efforts can be used to reward property owners and developers who produce high quality, sensitive projects or who diligently pursue excellence in property maintenance and design. Annual neighborhood awards in several categories (e.g., best new project, outstanding maintenance, best landscaping for residential and commercial activities) could be considered by the Steering Committee.

PROPERTY MAINTENANCE POLICIES

1. Encourage property owners to restore and maintain residences and businesses effectively.

Discussion

Aside from the obvious benefit of extending the economic life of homes and businesses, rehabilitation and maintenance contribute to an attractive neighborhood. A by-product of some residents' self-sufficient lifestyles is the accumulation of used materials in residential yards. Such accumulation occurs at the rear of businesses as well. These materials have value to their owners as building materials or recyclables, but outside storage, particularly in front yards and on public rights-of-way, is a blight on neighborhood appearance. More active enforcement of health and nuisance codes is required where problems are severe enough to affect the safety or welfare of the public. Less severe situations, which are primarily eyesores, can be addressed by the neighborhood through education, self-help programs, organized neighborhood cleanup days, and peer pressure.

A loan fund for property maintenance could be created and managed through a neighborhood corporation. The fund could be "seeded" by Community Development funds, neighborhood resident/business contributions or both. Small low interest (6%-8%) loans could be made to neighborhood property owners for small improvement projects and property maintenance activities.



HISTORIC HERITAGE AND CHARACTER

The Emerson-Garfield neighborhood covers a large area made up of widely diverse sections wherein the buildings house widely different uses. The architecture in the neighborhood reflects this diversity; it derives from nearly every period of the City's development; it includes nearly every style built in Spokane since the 1890's; and it houses the gamut of uses from residential and institutional to commercial and industrial.

Developing general guidelines for Emerson-Garfield was, therefore, problematical. However, identifying the parts of the neighborhood which exhibit cohesive historical character and then dealing with those areas will provide direction to the most visually cohesive portions in the district.

IDENTIFICATION OF PERIOD STYLES AND THEIR INCIDENCE IN THE NEIGHBORHOOD

Corbin Park is a distinctively cohesive historical district within the neighborhood. It is listed on the Washington State Register of Historic Places. Around the stadium-shaped park are a collection of remarkably well-preserved Classic Box and Bungalow style homes, virtually all built within the first two decades of the twentieth century. This district should receive special attention with regard to architectural guidelines. Care should be taken to preserve the historic character of these homes and to avoid intrusive new construction.

Portions of the Emerson-Garfield neighborhood still exhibit large segments of visually cohesive bungalow development. Particularly in the area east of Washington Street, from Corbin Park south to Indiana and Nora, there are block and two-block-long portions of bungalows, built between 1905 and 1925. Here setbacks are uniform, bulk and scale are similar, and most homes have porches. Roof types vary, but most are gabled or hipped. Windows are usually vertically oriented, double-hung sashes. Materials include wood, brick, stone, and stucco, all native to the area.

There are pockets of similar residential development (1905-1925) throughout the neighborhood. Those cohesive stretches of similar residences should adhere to the guidelines suggested below.

Scattered throughout these neighborhoods are many houses dating from 1900 or slightly earlier. Some of these exhibit Queen Anne styling and many are a simpler version, sometimes referred to as the Princess Anne style. They are usually two-story houses, most have porches, some are embellished with balconies and turrets, their materials are often variegated, and their roof forms might be multi-shaped. Their windows too are vertically oriented double-hung sashes, and many have stylistically shaped leaded glass lights. Again, materials tend to be wood, brick, stone, and stucco, often mingled in the same structure to produce varied texture. Several Classic Box homes remain. They are two-story square buildings with hipped roofs and classic details. They are almost always symmetrical.

Notable throughout the neighborhood are a number of early single brick commercial structures set by themselves in the midst of predominantly



residential areas. McGinnis Paper on North Atlantic and the Nygreen Block at Knox and Howard are examples of this phenomenon. Dating from the first two decades of this century, these buildings provide typical examples of the simple commercial architecture of that period: rectangular brick construction, restrained but decorative corbelling at the cornice line, and symmetrical fenestration of double-hung sash windows. On the eastern edge of the neighborhood, midway between its north and south boundaries, are several blocks of 1920s or 1930s brick houses. These are distinctive enclaves of well-maintained homes of the period, quite typical of Spokane residential areas. The homes are usually one or one and a half stories high, gable-roofed, asymmetrical more often than not. The later ones used touches of Norman Farmhouse or English Cottage design (steeply pitched and curving rakeboards, arched openings, and, very often, curved walkways). Uniform lot size and setbacks add to the visual cohesiveness of these streets.

A few highly stylized institutional buildings which have architectural merit as single structures dot the neighborhood. The North Monroe Library, St. Andrew's Church, St. Anthony's Church, Knox Presbyterian Church, Second Church of Christ Scientist, and Holy Trinity Greek Orthodox Church are examples. Every effort should be made to preserve these landmarks in their original form and to avoid intrusive new construction which would detract from their visual contribution to the neighborhood.

DEVELOPMENT GUIDELINES TO PRESERVE HISTORIC CHARACTER

General

In areas where cohesive historic character remains:

- Height: New buildings should be within 10% of the average building height in the particular district.
- Bulk: New buildings should not have a perimeter greater than 10% over the average building perimeter in the district. Width/length ratios of new buildings should be consistent with existing buildings.
- Spacing: Setbacks and lot sizes should be consistent with existing proportions.
- Materials: New construction should be encouraged to use wood, stone, brick or stucco. Synthetic materials tend to be intrusive in most historic neighborhoods.
- Ornamental Details: Variety of detail, so long as it is compatible with surrounding existing structures, should be encouraged.
- Porches: Porches of a scale consistent with those on existing neighboring houses should be provided.

Areas of Predominantly Bungalow Style Houses

Windows and

Window Rhythm: Along streets of bungalows, the windows of new construction should be vertical in orientation and should be placed in a rhythm, void to solid, consistent with fenestration on neighboring houses.

Roofs: Most bungalow rooflines are either gabled or hipped; new construction along streets of bungalows should conform and should exhibit similar pitch.

Areas of Predominantly Queen Anne and Classic Box Style Homes

Windows: Windows in new construction along these streets should be vertical in orientation and should repeat the predominant one-over-one double-hung sash configuration.

Window Rhythm: Fenestration is a very important design element in these turn-of-the-century styles. Care should be taken in new construction to repeat the void to solid ratio found in existing homes.

Areas of Predominantly English Cottage Style Brick Homes

Materials: Consistency of material is very important on such streets; brick should be encouraged in new construction.

Roofs: Roof pitch along streets of 1920s-1930s brick homes are very often steeper than found in bungalows; new construction should conform.

Miscellaneous

Any additions or alterations to single buildings of note should conform to and reflect the building's style, design elements, material, scale, and fenestration.

The City/County Historic Preservation Officer is available for consultation and technical assistance as plans for additions and alterations are developed. The office has a variety of written materials which provide guidelines for new construction in historic areas.



MIXED USE OBJECTIVES

For each of the Mixed Use Districts shown on the Neighborhood Land Use Plan map (page 19), the following objectives should be used to evaluate proposed development. These are supplemental to the guidelines for each component use found in the land use policies. Most of the Mixed Use Districts are within the North Riverbank portion of the neighborhood, and the North Riverbank Design Plan provides substantial guidance for activities within these districts. These objectives are in addition to the policies of the North Riverbank Design Plan, providing further detail for decision making. These objectives are not intended to conflict with those of the North Riverbank Design Plan.

NORTH RIVERBANK COMMERCIAL MIXED USE

1. Encourage a mix of offices, retail and service commercial, commercial recreation, and multi-family residential uses.
2. Require Boone Avenue orientation for retail and service commercial uses.
3. For the portion of the Commercial Mixed Use District in the Emerson-Garfield Neighborhood, utilize the building height, bulk and site coverage standards specified for the High Density Residential/Low Rise Office District immediately to the north.
4. Landscape all portions of sites not required for structures, parking or loading.
5. Not including public streets and alleys, provide at least 25% of the land area in this district as open space. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
6. Since this portion of the Commercial Mixed Use District adjoins the rock outcrop and City shop sites, pursue development of a similar character and theme as proposed in the North Riverbank Design Plan for these sites.

NORTH RIVERBANK DESIGN CORRIDOR MIXED USE

1. Encourage a mix of multi-family residential, office, retail and service business and public/cultural uses.
2. Require Boone Avenue orientation for retail and service commercial uses.
3. Allow building heights to reach nine stories provided that no more than 75% of the site area is covered. South of Maxwell Avenue, the equivalent building volume may be arranged to any height up to 150 feet.



4. Landscape all portions of sites not required for structures, parking or loading.
5. Not including public streets and alleys, provide at least 25% of the land area in this district as open space. Encourage, however, the rearrangement of building volume to create taller structures and more open space, up to 45% where feasible. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
6. Since this district is a key design feature of the North Riverbank Design Plan, pursue development in character as described in that plan, especially Chapter 13.

NORTH RIVERBANK HIGH DENSITY RESIDENTIAL/LOW RISE OFFICE

1. Encourage a mix of multi-family residential uses and low rise professional offices.
2. Allow building heights to reach three stories provided that no more than 75% of the site area is covered. South of Maxwell/Mission Avenue, the equivalent building volume may be arranged to any height up to 150 feet.
3. Landscape all portions of sites not required for structures, parking or loading.
4. Not including public streets and alleys, provide at least 25% of the land area in this district as open space. Encourage, however, the rearrangement of building volume to create taller structures and more open space, up to 45% where feasible. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
5. Since a significant portion of this mixed-use district involves site specific recommendations in the North Riverbank Design Plan (Rock Outcrop Site: Chapter 15 and City Shop Site: Chapter 16), pursue development consistent with that Plan.

MEDIUM DENSITY RESIDENTIAL/LOW RISE OFFICE/INSTITUTIONAL

1. Encourage a mix of small scale apartment uses (tri-plex and four-plex), low rise professional office and public or private nonprofit institutional uses.
2. Require Indiana Avenue orientation for office and institutional uses.
3. Allow building heights only to two stories provided that no more than 35% of the site area is covered. The equivalent building volume may be arranged to any height up to 60 feet.

4. Landscape all portions of sites not required for structures, parking or loading.
5. Not including public streets and alleys, provide at least 65% of the land area in this district as open space. Encourage, however, the rearrangement of building volume to create taller structures and more open space, up to 80% where feasible. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
6. To the maximum extent possible, utilize through redevelopment existing single-family residential structures for multi-family housing, offices and institutional uses. Retain, during remodeling and site development, the flavor and sense of a single-family residential neighborhood.

MEDIUM DENSITY RESIDENTIAL, LOW RISE OFFICE, GENERAL COMMERCIAL

1. Encourage a mix of small scale apartment uses (tri-plex and four-plex), low rise professional office, retail business and service commercial uses.
2. Require Indiana or Washington orientation for retail business, service commercial and professional office uses.
3. Allow building heights only to three stories, provided that no more than 50% of the site area is covered. The equivalent building volume may be arranged to any height up to 75 feet.
4. Landscape all portions of sites not required for structures, parking or loading.
5. Not including public streets and alleys, provide at least 50% of the land area in this district as open space. Encourage, however, the rearrangement of building volume to create taller structures and more open space, up to 75% where feasible. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
6. To the maximum extent possible through redevelopment, utilize existing single-family residential structures for multi-family housing, offices and businesses. Retain, especially in those areas not fronting on arterial, the flavor and sense of a single-family residential neighborhood.

MEDIUM DENSITY RESIDENTIAL, LOW RISE OFFICE

1. Encourage a mix of small scale apartment uses (tri-plex and four-plex) and low rise professional offices.
2. Require Maple or Ash orientation for the professional office uses.



3. Allow building heights only to two stories, provided that no more than 40% of the site area is covered. The equivalent building volume may be arranged to any height up to 75 feet.
4. Landscape all portions of sites not required for structures, parking or loading.
5. Not including public streets and alleys, provide at least 60% of the land area in this district as open space. Encourage, however, the rearrangement of building volume to create taller structures and more open space, up to 75% where feasible. Parking lots may be considered as open space if appropriate perimeter and interior landscaping is included (see site development guidelines for parking lots).
6. To the maximum extent possible through redevelopment, utilize existing single family residential structures for multi family and office use. Retain, especially in those areas not fronting on the arterial, the flavor and sense of a single family residential neighborhood.



THE RESIDENTIAL — COMMERCIAL INTERFACE

Residential policy 9, page 7, allows small apartments (tri-plex, four-plex) and small scale professional office uses to occur in the transition area where arterial strip commercial development interfaces with the single-family residential neighborhood. This is to be allowed as a bonus (something beyond single family or duplex housing) in exchange for specific site design features which help accomplish desired public objectives. This interface is the only portion of the neighborhood where density bonus mechanisms may be applied. This section describes where bonus provisions may be applied, how much of a bonus may be granted, and what public objectives must be met in exchange for the bonus.

BONUS PROVISIONS AND OBJECTIVES

1. Lots that might be considered for the application of bonus provisions must be contiguous with commercial development along Division Street, Monroe Street or Northwest Boulevard (that is, the site must share a common boundary, with or without an intervening alley, with property being used for business). It must also be located in an area identified as "Low Density Residential" on the Neighborhood Land Use Plan Map and share a common boundary, with or without an intervening alley, with property being used for single-family or duplex housing.
2. Allow multi-family housing up to a density of one dwelling unit for each 2,000 square feet of lot area. Utilize existing single-family residential structures whenever possible. If new structures must be built, assure they are designed and constructed to be compatible with surrounding single-family homes.
3. Allow professional and business offices in existing single-family residential structures whenever possible. If new structures must be built, limit them to 1,200 square feet on the ground floor and no more than two stories high. Assure new structures are designed and constructed to be compatible with surrounding single-family homes.
4. Assure that adequate off-street parking is provided (utilize standards from the Zoning Ordinance) and that it is located in the rear yard. Provide a landscape screening around all parking lots regardless of size.
5. Provide intensive landscaping, multiple layers deep with at least one layer eight feet or more in height, on the boundaries of a site that is shared with single-family or duplex housing. Avoid the use of solid masonry fence for screening.
6. When office uses are allowed, require signage to be mounted flush against the building, and either not lighted or indirectly lighted.
7. Assure that access to properties utilizing these bonus provisions is oriented toward the arterial commercial development rather than the low density residential area.

SITE SPECIFIC DESIGN RECOMMENDATIONS

MONROE/LINCOLN COUPLET DESIGN RECOMMENDATIONS

If it is determined that an arterial couplet utilizing Monroe and either Lincoln or Post is necessary and such a system is eventually built, the impact on the Emerson-Garfield Neighborhood can be substantially reduced by pursuing some specifically tailored design considerations. Some of these could be used to improve the conditions along Monroe even if the couplet project is not built. The design concepts described below are also illustrated in the accompanying drawings:

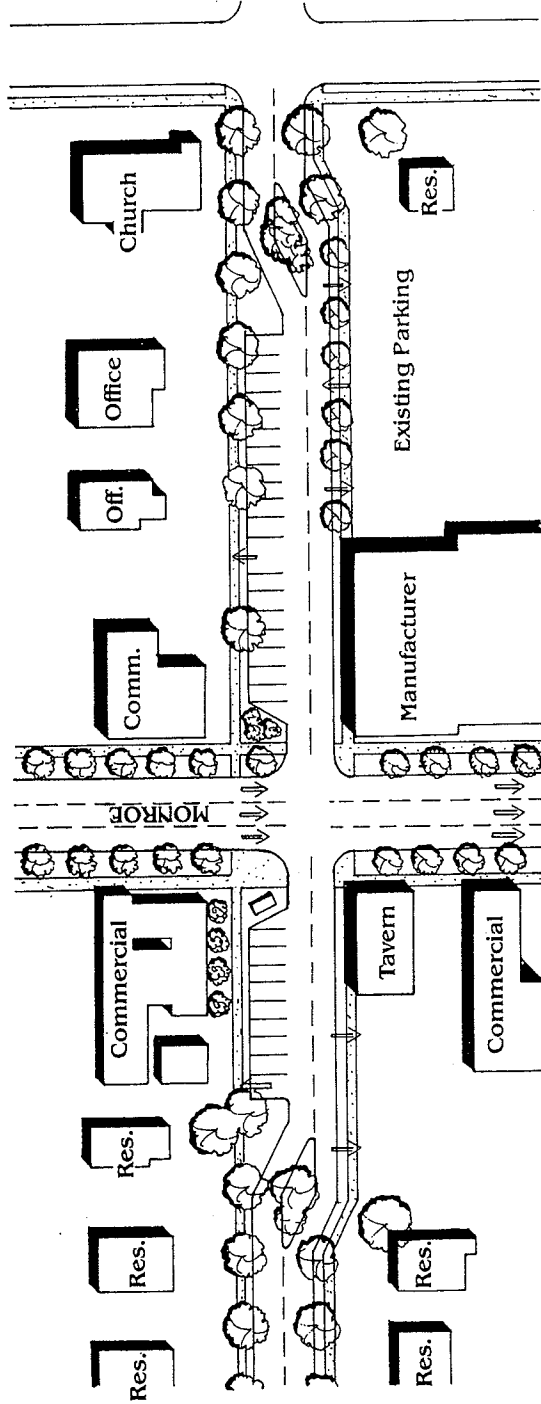
1. Provide three southbound travel lanes on Monroe north of Indiana and four south of Indiana. Similarly, provide three northbound lanes north of Indiana and four south of Indiana.
2. Remove curbside on-street parking along both sides of Monroe and whichever street becomes the northbound leg.
3. Widen the sidewalks extensively along both sides of Monroe, using space gained by reducing the number of travel lanes.
4. Close selective (approximately every other) cross streets along Monroe at about mid block or at the limit of business development. Utilize this space for off-street parking lots for Monroe Street businesses.
5. Provide pull-out bay transit stops along Monroe and the northbound couplet leg, including patron benches and/or shelters and route/schedule information.
6. Provide specific streetscape amenities that will create a strong pedestrian orientation along appropriate portions of the Monroe Street business development. Improve specific segments with appropriate theme supportive features, such as furniture, decorative lighting, signage and building facade. Include abundant street trees.
7. Where appropriate to support a theme or help focus attention, rebuild sidewalks with alternate/nontraditional surface texture.



Policy 4

- Close selective cross streets along Monroe
- Utilize this space for off-street parking

CITY OF SPARKLE PLAIN COMMISSION



NORTHWEST BOULEVARD DESIGN RECOMMENDATIONS

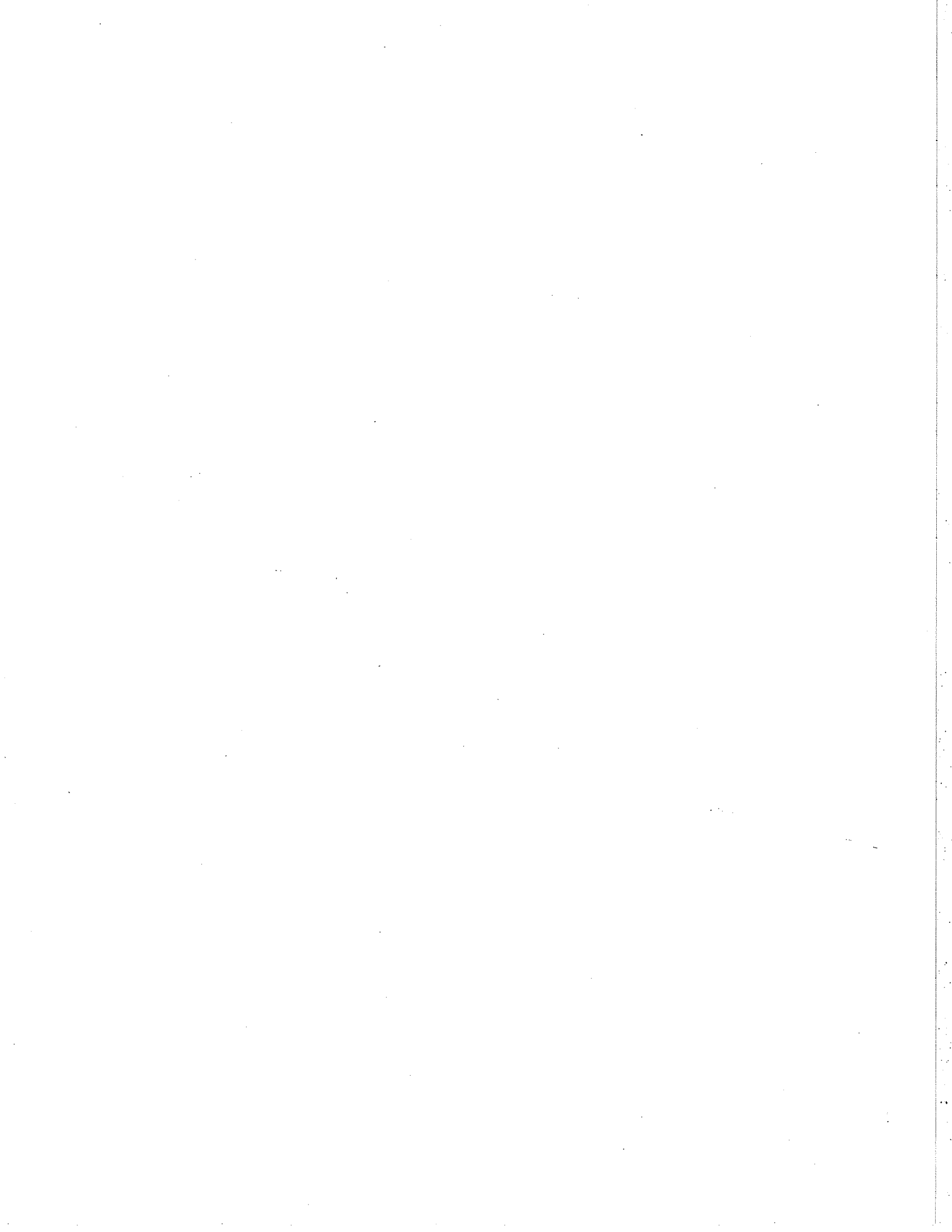
Northwest Boulevard is rather unique in the city in the way it presents an open, spacious appearance, especially when compared to streets like Monroe and Division. There are several reasons for this however, the wide street right-of-way and the manner in which Northwest Boulevard overlays the older traditional street grid is most significant.

The wide right-of-way has resulted in ample travel lanes and wide 12-15 foot parking strips in most areas. Improving the appearance of these parking strips through plantings of street trees, shrubs and lawn, and through installation of street furniture will enhance the openness and desirability of Northwest Boulevard as a travel route and as a place for neighborhood shopping.

Northwest Boulevard was built upon an existing north-south grid street pattern, producing a series of awkward angular intersections. They are duplicative and hazardous due to the difficult sight angles produced. In some places, the intersections can be eliminated altogether or consolidated with other nearby intersections, and in nearly all cases the angle can be improved to create a standard "T" intersection.

The design drawings presented here are generic examples of intersection improvements contemplated for all Northwest Boulevard. Each of the existing intersections should be evaluated and specific application/adjustment of these designs made. These design drawings also show the manner of streetscape anticipated along the wide parking strips.

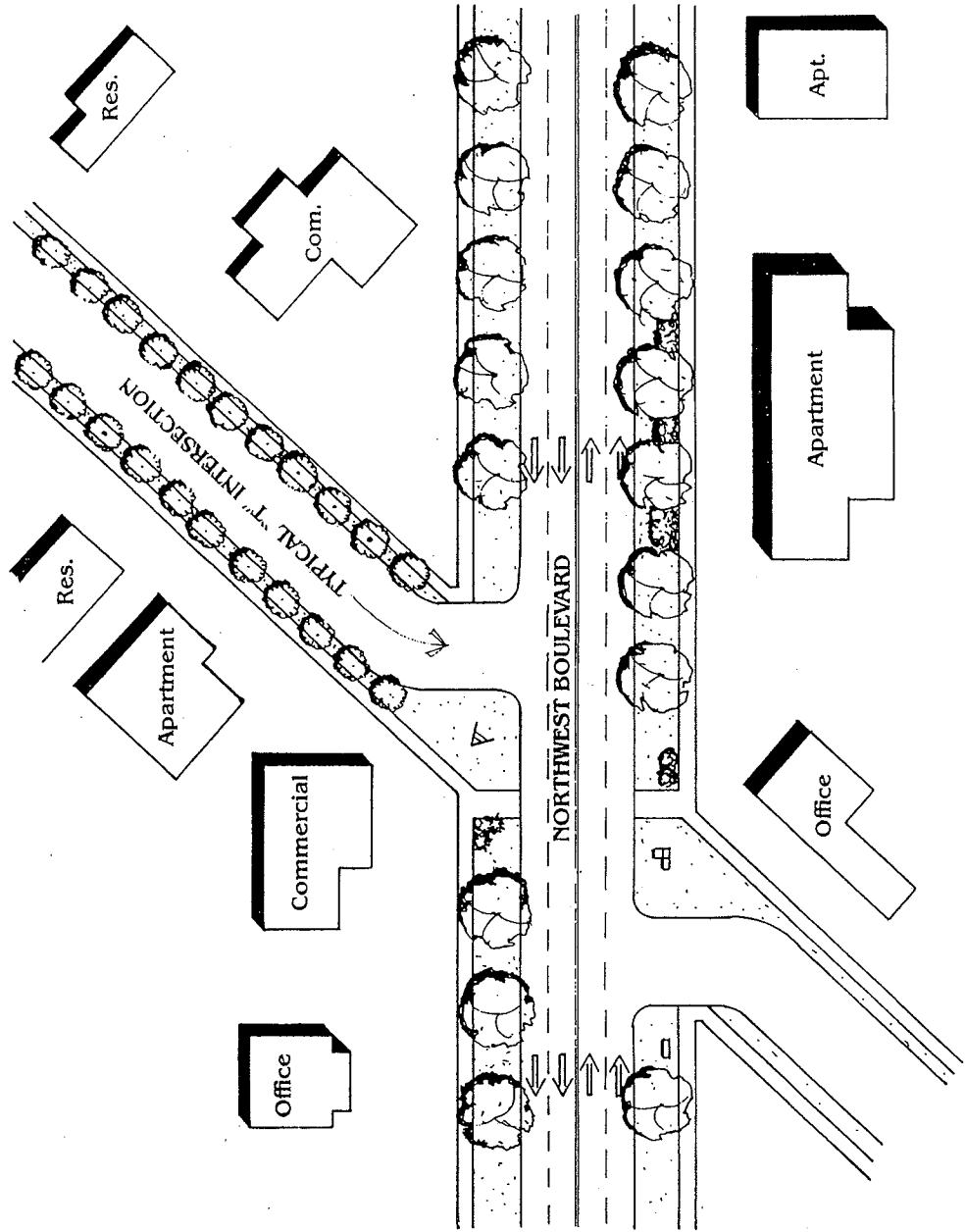




Intersection Improvements

- Realign side streets to develop standard "T" intersections

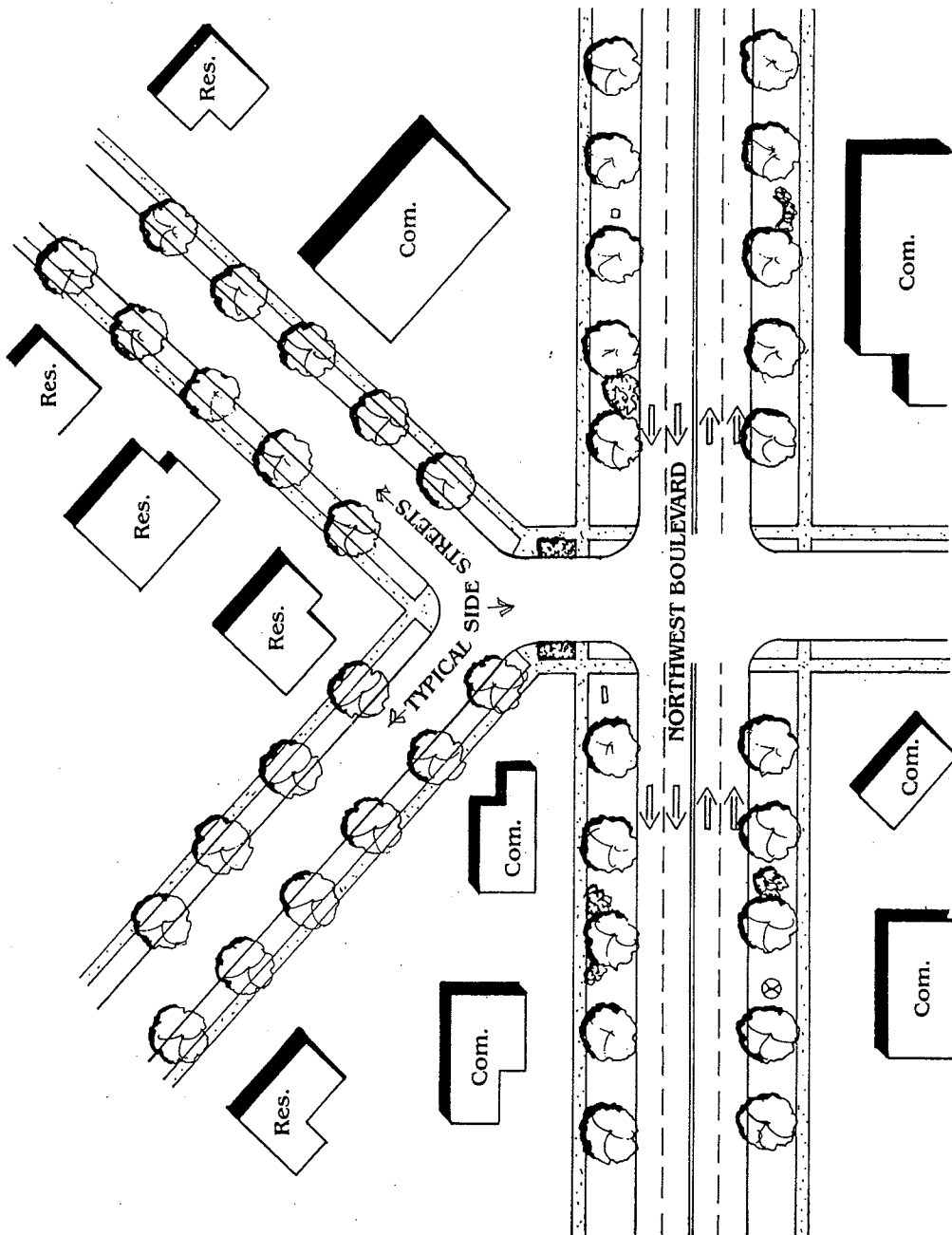
CITY OF FORTUNE PUEBLO COMMISSION



Intersection Improvements

- Realign awkward intersections along Northwest Boulevard to create standard "T" intersections

CITY OF FORTMIT PLAN COMMISSION



EMERSON

GARFIELD

NEIGHBORHOOD

PLAN

IMPLEMENTATION

Chapter 3:

- Overview
- Map of Zones
- Special Zoning Mechanisms
- Neighborhood Improvement Programming and Phase II of Design Plan



OVERVIEW

Implementing a neighborhood design plan requires a number of different activities. Money must be spent. Public funds are required to improve street-scapes, realign intersections, plant trees and do the other public projects described in the Plan's policies. These are coordinated through a neighborhood scale capital improvement program which is described later in this chapter.

Private capital is also required to build and remodel structures, make site improvements and conduct business. The effect of this spending is incredibly significant since far more private capital will be invested in the neighborhood than public funds. The bulk of this Plan's policy language is oriented to guiding private investment and development.

Much of what is described in this Plan as desirable is attitudinal and accomplishment of these objectives will require a long term modification in social and behavior conditions. Reaching a point whereby most residents of the neighborhood recognize and fulfill their responsibility to be a good neighbor is going to take strong leadership. The neighborhood steering committee or its successor organization will have to lead and serve to change individual behavior toward the objectives of this Plan.

Most of the policy guidance relating to private development can be implemented through traditional zoning techniques. Zoning categories appropriate to coincide with the land use designated on the Neighborhood Land Use Plan have been applied and are shown on the Map of Zones. You will notice that most of the basic zoning categories also have an "L", Limited Zone, suffix. The Limited Zone suffix contains most of the site improvement standards necessary to assure the quality of development prescribed in the Plan's policies.

General commercial areas along the arterials are different and cannot be implemented simply by traditional zoning. Although these areas are zoned B2-L "Community Business Zone-Limited", the Plan's policies provide for dramatically increased development opportunities. They also prescribe corresponding development and operational responsibilities necessary to maintain the desired quality of life.

MAP OF ZONES

The map on page 3-4 shows the distribution of zoning districts adopted to implement the land use policies of this Design Plan. As introduced earlier, most basic zoning categories include the "L", Limited Zone, suffix. This means the site development standards prescribed in Section 11.19.290 of the Zoning Code are to be considered part of the standards of the underlying zone. These standards address screening activities, on-site containment of operational impacts, landscaping and buffering, lighting, signage, off-street parking and loading areas, maximum height and bulk and minimum yards. These, along with the basic standards of the underlying zone, correspond to the site development objectives of the land use policies.

RESIDENTIAL ZONES

Four residential zones are found in the Emerson-Garfield neighborhood representing a range of urban densities and housing types. The R1, One Family Residence zone covers by far the largest portion of the neighborhood and is intended primarily for the use and protection of one-family residences. Normally the standards of Limited Zone suffix are not necessary in R1 zones.

The R2, Two-Family Residence Zone, is intended primarily to accommodate two-family (duplex) residences or a single lot in selected low-density areas. It is expected that housing in these zones will be a mixture of one- and two-family structures with new duplexes being built on bypassed vacant lots or being created by conversion from older single-family homes. Although the Zoning Code does allow conversion of some older homes (pre-1929) to triplexes, the Design Plan's policies regarding adequate site development will tend to limit this occurrence.

The R3, Multi-Family Residence Zone, is intended to accommodate small multi-family structures (triplexes and fourplexes) on a single lot. It also allows larger multi-family structures with more than four dwelling units on larger lots so long as the lot contains at least 2,000 square feet for each unit.

Only in the North Riverbank portion of the neighborhood can the R4 Multi-Family Residence Zone be found. Here the D, Design Area, suffix is applied. This designation prescribes specific site development and performance standards oriented to the policies of the North Riverbank Design Plan. The R4 Zone is intended primarily to accommodate large apartment buildings with 1,000 square feet of site area for each dwelling unit.

COMMERCIAL ZONES

Other than the mixed use zones, which are predominantly commercial (and will be discussed later), the only commercial zone in the neighborhood is B2, Community Business Zone. Although the B2 Zone is not entirely

appropriate to implement the policies of the Design Plan, it is relatively close. None of the existing zones would better implement the general commercial policies and the creation of a new zone for that purpose would not be prudent. The project compliance review and zoning agreements/plans-in-lieu mechanisms (to be described later) allow the flexibility to utilize the guidance of the Plan's policies, even though it may vary from the requirements of the B2 Zone. In specific cases, it may be appropriate to rezone portions of the neighborhood's general commercial areas to C, Commercial, or M, Manufacturing Zones in order to fully implement the policies of this plan. Such rezones should be considered only when specific development or redevelopment proposals are proposed.

MIXED USE ZONES

All the mixed use areas are located within the North Riverbank area except the "Medium Density Residential/Low Rise Office/Institutional" and "Medium Density Residential/Low Rise Office/General Commercial, and "Medium Density Residential/Low Rise Office" districts. These are delineated on the Map of Zones by the designation RO-1L, Limited Residential Office, Category One. Once again, this zone is not really appropriate to implement Design Plan policies for the Medium Density Residential/Low Rise Office/General Commercial districts. The Plan's policies anticipate some B1 and B2 uses occurring along some arterial frontages which are not usually associated with the RO-1 zone. Full implementation of these land use categories will be dependent upon the project compliance and rezoning mechanisms.

The RO-1D zone is used to implement the High Density Residential/Low Rise Office districts described in the North Riverbank Design Plan. In addition to the uses and development standards associated with Category One of the Residential Office zone, certain B1, Local Business, uses are permitted. Further, the height of structures is limited to three stories.

The Design Corridor Mixed Use District of the North Riverbank Design Plan is implemented by the B2-1D Community Business Design Zone, Category One. In addition to the normal provisions of the B2-1 zone, the Design designation brings special considerations for: height and bulk, lot coverage, visual and pedestrian penetration, and design incentives.

The B2-D, Community Business Design Zone-Category Two, is used to implement the Commercial Mixed Use areas in the North Riverbank Design Plan. In addition to the normal provisions of the B2 Zone, the Design designation brings special considerations for: height and bulk, lot coverage, and design incentives.

SPECIAL ZONING MECHANISMS

PROJECT COMPLIANCE REVIEW

The provisions of the Zoning Code (permitted uses and site standards) are oriented to regulating development on a city-wide scale. While they are generally adequate for activities within the Emerson-Garfield Neighborhood, they do not take into account the unique subtle directions of the Design Plan's policies. Section 11.19.244 of the Zoning Code, "Project Compliance with Design Plans" provides a mechanism whereby the unique guidance of each neighborhood's design plan can be utilized during the development permit process.

In essence each development project in a design area, whether new construction or remodeling, must be reviewed by the City Planning Director to determine if it complies with the adopted design plan. The necessary development authorizations (building permits, certificates of occupancy, rezones, special permits, shoreline permits, variances, subdivisions, planned unit developments, etc.) cannot be issued until the Planning Director has certified in writing that the project substantially complies with the spirit, intent and objectives of the design plan. This procedure will be especially useful in the general commercial areas, along the commercial-residential transition where density/use bonuses may be permitted, and in mixed use areas.

The Planning Director may seek advice on a project's compliance from the Urban Design Committee due to the complexity of the proposal or the potential magnitude of its impacts. The determination issued by the Planning Director may be appealed to the City Plan Commission.

ZONING AGREEMENTS AND PLANS-IN-LIEU

Section 11.19.290 of the city Zoning Code provides a special, flexible mechanism promoting innovative approaches to development in design plan neighborhoods and throughout the city. Essentially, this mechanism allows the Zoning Board to accept unique development plans and a written agreement in lieu of strict compliance with the normal standards of the Zoning Code.

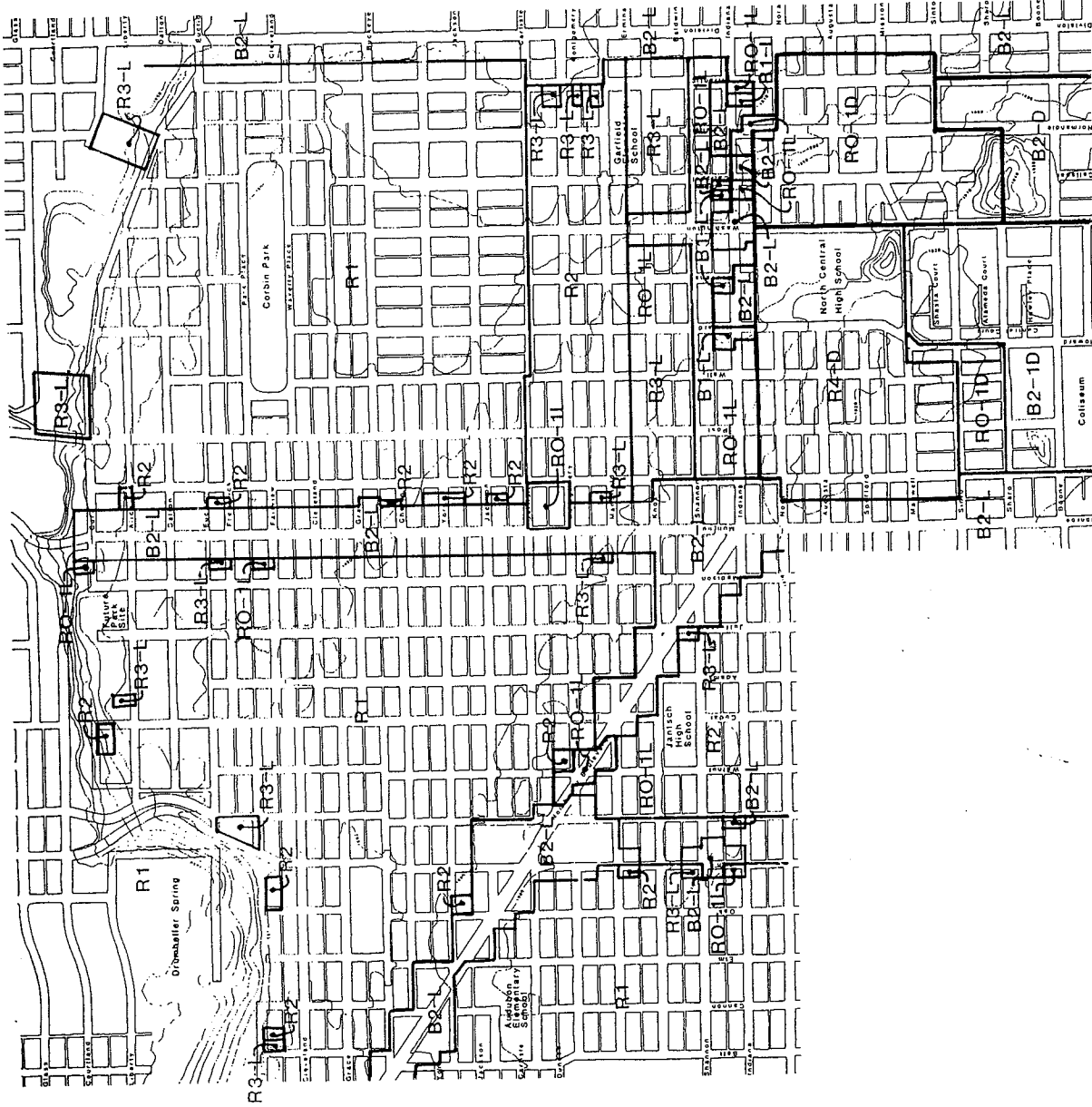
This mechanism may be applied as a supplementary technique in association with rezones, special permits, planned unit developments, subdivisions and other zoning/land use permit processes. When used, this mechanism is to provide a more desirable development without being unduly detrimental to the surrounding properties.

When used in design areas, the plans-in-lieu zoning agreements are required to further the objectives of appropriate design plans, and this would be determined through Project Compliance Review as described earlier. In addition, the Hearing Examiner must request review of proposed plans by the Urban Design Committee and consider this advisory report during the decision making process.

Neighborhood Map of Zones

- R1 - One Family Residence
- R2 - Two Family Residence
- R3-1 - Multi-family Residence
- R4-D - Multi-family Residence
- RO-1L - Residence - Office
- RO-1D - Residence - Office
- B1-L - Local Business
- B2-L - Community Business
- B2-1D - Community Business (Design Corridor Mixed Use)
- B2-D - Community Business (Commercial Mixed Use)

NOTE: Each zone is designated as being a Design (D) or Limited (L) zone. CITY OF SPANISH FLAT COMMISSION



The plans-in-lieu zoning agreement mechanism includes the potential for granting a density bonus up to 30% above the density of the underlying zone. However, the density bonus allowance does not apply in design areas where the design plan expressly prohibits bonuses. In the Emerson-Garfield Design Area, density bonuses are not allowed except as provided along the residential-commercial interface described in Chapter Two.

NEIGHBORHOOD IMPROVEMENT PROGRAM

WHAT IS IT?

Scheduling public funded physical improvement projects within a neighborhood, identifying the approximate cost of those projects and determining the appropriate source for funds is known as a neighborhood improvement program (NIP). They are commonly identified as short-term (3 to 6 years into the future) and long-term (extending 20 to 50 years into the future). Short-term NIPs usually have very accurate cost estimates, committed funding sources and perhaps even project design work completed, while long-term programs are more nebulous.

WHERE DOES IT COME FROM?

The public physical improvement projects which make up the Emerson-Garfield Neighborhood Improvement Program will be determined directly from the Design Plan's policies. The NIP is a vital mechanism to implement the Design Plan, with the potential for producing significant and immediate results.

Subsequent to the final adoption by City Council of the Phase I (policy phase) Design Plan, the neighborhood task force begins Phase II, the design phase. This will begin with an evaluation of the Plan's policies and creation of a public projects list. The value of each project in terms of furthering the Plan's intent and urgency of completion is discussed, and priorities are established. The approximate cost of each project is determined through preliminary design activities, normally with the assistance of a qualified design professional retained as consultant.

With cost estimates in hand, the list of project priorities is once again examined and reordered to recognize funding limitations and to assure the maximum benefit is gained for available expenditures. Also at this time appropriate funding sources for each project, especially short-term, are identified.

HOW WILL EMERSON-GARFIELD'S NIP BE FUNDED?

A variety of sources are potentially available to fund capital improvement projects in the Emerson-Garfield Neighborhood. Some are reasonably assured while others should be considered limited in their availability. Beginning in Community Development Block Grant year 1988 (July 1988), the Emerson-Garfield Neighborhood is scheduled to receive \$1.8 million for what is known as concentrated construction. The receipt of these funds will be spread over a period of four years, with annual allotments of between \$300,000 and \$900,000. This is the key short-term funding source for NIPs.

By the conclusion of the Design Plan's Phase II, it is anticipated that most, if not nearly all, of the Neighborhood's annual allocation of funds through the Steering Committee will be integrated into the NIP. The Neighborhood's three-year plan should pursue the objectives of the Design Plan and only those funds necessary to provide social service functions would not be integrated into the NIP.

Other potential sources of funding include the City's general fund (through the city-wide capital improvement program), general obligation bonds or excess levy issues (both must be voter approved), councilmanic bonds or special grants. None of these should be considered probable. If used for projects within the Emerson-Garfield Neighborhood, it would probably be as part of a city-wide improvement program, and any relationship to the Emerson-Garfield NIP would be purely coincidental.

OTHER PHASE II ACTIVITIES

Toward the latter part of the Design Plan's Phase II activity, the design consultant retained by the Task Force will begin detailed project designs for those projects to be undertaken first. Detailed cost estimates will also be produced at that time. By the conclusion of Phase II, final approved designs and specifications for the first order of projects should be complete. When concentrated construction funds became available, those projects should be ready to bid and construction contracts can be let, beginning Phase III.