



NOTICE OF PUBLIC MEETING
NEIGHBORHOOD PROJECT ADVISORY COMMITTEE
WEST QUADRANT TAX INCREMENT FINANCING DISTRICT

MEETING OF
WEDNESDAY, October 1, 2025
4:00 PM TO 5:00 PM

Please be advised that the next regular meeting of the Neighborhood Project Advisory Committee for the West Quadrant Tax Increment Financing District will be held on **Wednesday, October 1 at 4:00 PM** at **Spokane City Hall, 808 W Spokane Falls Blvd** in **Conference Room 3B** on the third floor. Please note the change in venue to room 3B! A link to the meeting is provided below. The public is encouraged to attend either in person, online, or by phone. Connection instructions are included in the attached agenda and below:

An Online/Telephone Option is Available via Microsoft TEAMS
(software download may be required)

Join on your computer, mobile app or room device

[Click here to join the meeting](#)

Meeting ID: 291 137 164 093 Passcode: bbFZ2x

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Or call in (audio only)

[+1 323-618-1887,,357638756#](#) United States, Los Angeles

Phone Conference ID: 357 638 756#

WQTIF Neighborhood Project Advisory Committee (NPAC) Agenda

October 1, 2025 | 4:00 PM to 5:00 PM | City Hall, 3rd Floor, Conference Room 3B

PLEASE NOTE THE CHANGE IN VENUE TO ROOM 3B

Attendees: Members of the Committee, Open to the Public

Discussion Items *(all times are approximate)*

1. Welcome and Introductions (All) 4:00 pm (5 mins)
2. Approval of August Meeting Minutes (All) 4:05 pm (5 mins)
3. New Request: Broadway Foods/Laundromat Request (Jill Yotz) 4:10 pm (30 mins)
4. New Request: Broadway & Nettleton (Kevin Freibott) 4:40 pm (15 mins)
5. Open Forum 4:55 pm (5 mins)

Open Forum is an opportunity for citizens to discuss items of interest that are not on the current agenda for discussion. Topics should consider the activities/ responsibilities of the NPAC and should not include political campaign topics. Each speaker may be limited to three minutes if several people wish to speak, subject to the Chairperson's determination. If you wish to speak at the open forum, please email Kevin Freibott at kfreibott@spokanecity.org in advance to request a slot.

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AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION: The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. Individuals requesting reasonable accommodations or further information may call, write, or email Human Resources at 509.625.6373, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or ddecorde@spokanecity.org. Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.



West Quadrant TIF Neighborhood Project Advisory Committee

Meeting Minutes

August 6, 2025

Committee Members Present: EJ Iannelli (Chair), Kimberly Lawrence (Vice-Chair), Alan Chatham, Fran Papenleur, Amanda Richardson, Wende Wilber **Absent:** Liz Marlin

Staff Present: Kevin Freibott (Planning & Economic Development)

The meeting began at 4:01 PM.

1. **Welcome and Introductions** – Introductions were made. No action was taken.
2. **Approval of Meeting Minutes** – August minutes were discussed. Minutes were approved as drafted (Unan.).
3. **Traffic Improvements in West Central** – M. Quinn-Hurst presented the City's proposal for intersection improvements at four intersections in West Central, commensurate with the West Central Infrastructure Project requirements. Also presented was the status of a section of "missing" sidewalk on Bridge Avenue. Testimony was taken from members of the public in attendance, primarily centered on the missing sidewalk. Following discussion and deliberation, the NPAC voted to recommend \$129,692 for construction of the four intersection improvements (M: M. Lawrence, S: M. Richardson, 4 aye-0 nay-1 abst.). Following that, a motion was made and passed to recommend \$100,000 for investigation and engineering for the missing sidewalk with the remaining funding to be provided for physical construction of the sidewalk (M: M. Lawrence, S: M. Chatham, 4 aye-0 nay-1 abst.).
4. **NPAC Forms** – M. Freibott presented the current form of NPAC forms and updated the draft nature of the forms pending further Legal Department input. Changes were suggested and made by Mr. Freibott. No action was taken.
5. **Open Forum** – General discussion ensued over neighborhood education/awareness of the TIF but no action was taken.

The meeting was adjourned by the Chair at 5:05 PM.



General Application

Rev.20240311E

Description of Proposal

Address of Site Proposal

(if not yet assigned, obtain address from Development Services Center [509-625-6300] before submitting application):

Applicant

Name:

Address:

Phone:

 Email:

Property Owner

Name:

Address:

Phone:

 Email:

Agent

Name:

Address:

Phone:

 Email:

Legal Description of Site: _____

Size of Property: _____

List Specific Permits Requested in this Application:

Submitted By:

☐ Applicant ☐ Property Owner ☐ Property Purchaser ☐ Agent

In the case of discretionary permits (administrative, hearing examiner, landmarks commission or plan commission), if the applicant is not the property owner, the owner must provide the following acknowledgement:

I, _____, owner of the above-described property, do hereby authorize
_____ to represent me and my interests in all matters regarding this application.

Acknowledgment

[illegible]

On this _____ day of _____, 20_____, before me, the undersigned, a Notary Public in and for the State of Washington, duly commissioned and sworn, personally appeared _____ to me known to be the individual that executed the foregoing instrument and acknowledged the said instrument to be free and his/her free and voluntary act and deed, for the uses and purposes therein mentioned.

Witness my hand and official seal hereto affixed the day and year first above written.

Notary Public in and for the State of Washington, residing at _____



Request for TIF Funding

DRAFT Application

Rev.20250909

In addition to the General Application, requestors for funding from the West Quadrant TIF must fill out the following form and provide it to the Planning & Economic Development Department for review and submittal to the Neighborhood Project Advisory Committee. Supplemental sheets may be attached for longer answers.

PROJECT DETAILS:

Please provide the following information regarding the overall project to which this project relates. If no larger project exists, please provide as much information as possible.

Project Name: Broadway Foods & Broadway Laundromat

Project Address or General Location: 2229 & 2228 W Broadway Ave. Spokane WA 99201

Construction Dates (or Estimate): Starts (mm/yy) 01/2025 Ends (mm/yy) 06/2025

Total Cost of Project* (Approx.): \$ Click or tap here to enter text. Total Requested from TIF: \$ 174,714

Other Public or Private Funding for this Project (if any) and Approximate Amounts: funded through private debt & equity

**Please attach itemized cost estimates for improvements to be funded by the TIF, if available.*

PROPOSAL DESCRIPTION:

Please describe the overall project: Infrastructure improvements were required in order to develop two corners of the Nettleton-Broadway intersection and provide critical commercial/retail services to the neighborhood. Developer-paid, The developer paid for City-owned infrastructure at the cross-section of Nettleton and Broadway, including: new sidewalks, curbs, driveways and ramps; alley pavement, upgraded utility connections, electric pole relocations, and city landscaping.

Features you hope to fund with TIF funding (check all that apply):

- | | | |
|--|---|---|
| <input type="checkbox"/> Affordable Housing | <input checked="" type="checkbox"/> Streets & Curbs | <input checked="" type="checkbox"/> Sidewalks |
| <input type="checkbox"/> Crosswalks | <input checked="" type="checkbox"/> Signage & Signals | <input checked="" type="checkbox"/> Landscaping |
| <input checked="" type="checkbox"/> Street Trees | <input type="checkbox"/> Lighting/Streetlights | <input checked="" type="checkbox"/> Utility Connections |
| <input checked="" type="checkbox"/> Utility Lines | <input checked="" type="checkbox"/> Planning/Design | <input type="checkbox"/> Maintenance/Operations |
| <input checked="" type="checkbox"/> Other: <u>Nettleton St. alley asphalt behind both the grocery store and laundromat</u> | | |

Partner Agencies or Organizations Affiliated with this Project: Click or tap here to enter text.

Related City Projects or Programs (if any): West Central Infrastructure Project: a crosswalk at Nettleton and Broadway has been identified as a priority for the neighborhood and the NPAC. It is not included in the costs of this application, but we are also requesting this project be moved up in priority. The new businesses will increase foot traffic, particularly of families, and we are concerned about visibility for pedestrians at this intersection. Please prioritize a crosswalk WITH FLASHING LIGHTS/SIGNS at this intersection.

COMMUNITY ENGAGEMENT:

Neighborhood Councils You Have Contacted Regarding This Proposal:

☐ Riverside

☒ West Central

☐ Emerson/Garfield

☐ Other: Click or tap here to enter text.

Presentations You Have Given to Neighborhood Councils Regarding This Proposal (*provide future dates if presentations are scheduled but have not yet occurred*):

Riverside (Dates): Click or tap here to enter text. West Central (Dates): 3/13/24, 10/8/25

Emerson/Garfield (Dates): Click or tap here to enter text.

Other Engagement You Have Undertaken on this Project: We presented the project on 3/13/2024 to the West Central Neighborhood Council and received unilateral support and enthusiasm. We will present our request for TIF funds, specifically, at the next council meeting: 10/8/25.

SUPPLEMENTARY QUESTIONS:

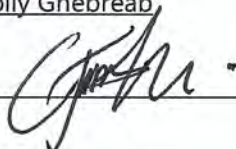
Please attach a sheet or sheets answering the following general questions to the best of your knowledge and understanding. These questions, while general in nature, will help the Neighborhood Project Advisory Committee understand your project's value, needs, and contribution to the community and your project's relationship to the TIF's intended purpose.

1. Describe the problem, challenge, and/or opportunity your project is hoping to address.
2. Describe why TIF funds are essential/needed for this project.
3. Describe the end users and/or customers of your project. Who will be impacted by your project (positively as well as negatively) and how.
4. How might your project affect the health and safety of the neighborhood and the wider community?
5. Describe the short- and long-term economic benefits of your project, including any benefits to the immediate neighborhood as well as the larger community surrounding your project. Please submit documentation if any exists.
6. Describe how your project might benefit the surrounding neighborhood in other ways. Topics to consider are historic preservation, arts and creative outlets, job creation, affordable housing, infill (development in vacant lots, dilapidated properties), recreational access or amenities, and transportation safety or connectivity.
7. How does your project relate to the City's Comprehensive Plan? Identify any concepts in the Comprehensive Plan that relate to your project, including any goals or policies related to your project. (see www.shapingspokane.org)
8. Please attach any relevant site plans or drawings for your proposed project, preferably showing which improvements are to be TIF-funded.

SUBMITTED BY:

Name (PRINT): Jolly Ghebreab

Signature: _____



Date: 09/26/2025

☐ Applicant

☒ Property Owner

☐ Property Purchaser

☐ Agent

DRAFT

Request for TIF Funding
Broadway Foods Offsite Infrastructure
October 1, 2025

SUPPLEMENTARY QUESTIONS

1. Describe the problem, challenge, and/or opportunity your project is hoping to address.

Slightly west of downtown and the only neighborhood to be surrounded by the Spokane River, West Central was once a thriving middle-class suburb with a deep sense of place. Home to notable landmarks such as the old Natatorium Park and the County Courthouse, residents were served by many small shops within the neighborhood and were connected to the rest of the city via trolley car. However, the area has seen economic decline, property neglect, and social challenges since the 1970's. Over the years, many properties fell into disrepair and crime, poverty, and infrastructure issues became more pronounced.

Earlier efforts to convert historic properties into small multifamily homes now provide much needed affordable housing to many of Spokane's poorer residents. In recent history, single family zoning regulations prevented additional small businesses from establishing themselves. As a result, the neighborhood lacks amenities within walking distance. Development in the southern portion of the neighborhood (Kendall Yards) has led to improvements, but also raises concerns about gentrification, which now threatens the diversity and fabric of the neighborhood.

Despite the disrepair and recent grim history, the community's strength lies in its diversity (both socioeconomic and cultural) and numerous community assets. These include multiple faith-based communities, a community center, a healthcare facility, a performance space, a large garden, several commercial storefronts, and the resilient spirit its residents. The people who call West Central home are the people who keep our city thriving and diverse: service providers, retail workers, nonprofit organizers, teachers, artists, pastors, entrepreneurs, retirees, disabled, veterans, families, homeowners, and renters. The homes are a mixture of historic and modern; rehabbed and dilapidated.

There is a notable divide between the amenities and identities between Kendall Yards and West Central residents. This divide highlights the following three challenges in the neighborhood:

Challenge 1: Equitable access to healthy nutrition

The retail and commercial offerings in Kendall Yards have developed to serve the wealthier citizens in the community. Many poorer residents still rely on neighborhood retailers who are easy to walk to for inexpensive supplies. Nutritious food options are limited. In addition

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to the two convenience stores, retail options are: breweries/bars, an ice cream shop, and a bakery. There is no true grocery store.

Solution: The owners of the two primary stores that serve citizens in West Central have heard the call for a healthier product at a site that feels safe for families and individuals to access (i.e. NOT a 24/7 convenience store vibe).

Challenge 2: Laundry and Sanitation

West Central is comprised of both homeowners and renters who live with aging infrastructure and old buildings in various states of disrepair. Many small, converted homes do not have laundry facilities for tenants.

Moreover, many people who are housing insecure in West Central do not have an easy way to keep clean. The ability to wash one's clothes has important outcomes to health, employment, and psychological well-being.

Solution: These local business owners have built a laundromat across the street from the new grocery store. The closest laundromat is in the north part of West Central on Ash and Nora on a busy, car-oriented street. The new laundromat at Broadway & Nettleton is in the heart of the neighborhood, next to a food bank, and directly in front of a bus stop that connects the neighborhood to downtown. Parking is also available onsite. The establishment is extremely accessible to the diverse needs of the community.

Challenge 3: Uniting Kendall Yards and West Central under the shared identity of “neighbors”.

There is a psychological divide between the two neighborhoods that is exacerbated by the challenges of physical connectivity. The expensive, new construction homes of Kendall Yards are a stark contrast to the historic and oft dilapidated homes in the rest of West Central that encompass a greater socioeconomic range. The infrastructure reflects this divide: there are only two intersections that connect the neighborhoods. Broadway Foods and the Laundromat are located on one of them.

In numerous ways, Kendall Yards has been an enormous benefit to the WC area of the city. However, it has also been a factor in speeding up gentrification and property speculation in the WC neighborhood.

Jolly Mart and Bongs are two well-established convenience stores in the center of West Central. The owners work behind the counter regularly and have built trusted relationships

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with the area's residents and are well-situated to understand the day-to-day struggles of the community. In response to deep listening, they are building a laundromat and grocery store at the Nettleton-Broadway intersection where the old Broadway Foods used to exist. This intersection is a unique opportunity for commercial/retail that serves BOTH neighborhoods. It offers an opportunity to bridge the physical and psychological divide by providing common amenities and a space for community gathering.

2. Describe why TIF funds are essential/needed for this project.

The two businesses can only be built if the required offsite infrastructure improvements are installed for both corners of the intersection. The project developers are small business owners with a long history of serving West Central and a track record of listening to the neighborhood. They are constantly seeking ways to leverage their investments for additional benefit to the community. The improvements are costly, and every dollar saved on infrastructure results in improved amenities for the neighborhood. Furthermore, using TIF funds in this project is an opportunity to showcase the TIF's goals of catalyzing economic development in an area that is the lynchpin between Kendall Yards and greater West Central.

3. Describe the end users and/or customers of your project. Who will be impacted by your project (positively as well as negatively) and how.

Broadway Foods will serve residents in both neighborhoods (Kendall Yards and West Central) with necessary grocery items, produce, and hot foods, as well as provide a seating area where residents can linger and mingle. The other grocery offerings are Bongs and Jolly Mart (West Central), which are in the style of "convenience stores", or My Fresh Basket (Kendall Yards). The current convenience stores are not set up for a robust produce line or ready-made fresh food, and the owners have noted that the style of the stores and merchandise selection serve as a deterrent to families. My Fresh Basket, a full-service grocery store with a deli, is an excellent amenity for residents in Kendall Yards and downtown, but it is too expensive for most residents in West Central. Broadway Foods seeks to provide a "middle option"- affordable, accessible, and healthy for all residents and their families.

Assuming that Kendall Yards homes are equipped with laundry amenities, the Laundromat will likely serve more WC residents and those who need larger machines to wash bedding,

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sleeping bags, etc. The laundromat shares an alley with a local food bank and could be a helpful resource for the people they serve.

The laundromat was recently a vacant lot, while the lot across the street was home to the old Broadway Foods building which was vacant and boarded up. Vacant and dilapidated properties are known to attract nuisance activity; the reactivated spaces will encourage community traffic and discourage vandalism or other clandestine behavior. As these corners attract business, increased foot and car traffic may impact direct neighbors, but the owners have already worked to build relationships with those residing around the new development, and there is generally positive feedback.

4. How might your project affect the health and safety of the neighborhood and the wider community?

This project has positive, direct impacts to the health and safety of the neighborhood:

- **Time and Money Savings:** New, accessible services reduce household time and cost to travel outside of the neighborhood, providing concrete monetary and time savings, especially for low-income households.
- **Nutrition and Food Justice:** The grocery store will provide quality, fresh food items in an area that lacks this amenity. Moreover, it serves residents without requiring access to a car to drive to the store.
- **Services and Sanitation:** The laundromat will provide a local amenity to residents, particularly those who are low-income or housing insecure, so that they can be clean. Easy access to sanitation amenities has especially positive impacts on marginalized and/or poor communities' health and employment outcomes.
- **Jobs and Youth Development:** New businesses in the neighborhood also mean new job opportunities for residents, including youth, that results in immediate wage recirculation locally. West Central and Kendall Yards already have programs that invest in youth development (River City Youth, Spark Central). Local entry-level job opportunities for young people teach skills like customer service, money handling, and responsibility which improve employability.
- **Revitalize Blight:** These blocks were recently vacant and dull; the new businesses will be frequented by neighbors and workers, providing active, positive engagement with and regular maintenance to the area.

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- **Accessibility and Safety.** West Central has struggled with walkability and traffic safety, especially along arterials. Providing amenities within walking distance with enhanced sidewalks, better crosswalks, bike parking, and bus stop amenities around the site will enhance overall connectivity and safety.
- **Community Identity:** Positive health outcomes are linked to a strong sense of community. Gathering spaces like laundromats and local shops often become informal social hubs, strengthening neighborhood ties that support safety and collaboration.

5. Describe the short- and long-term economic benefits of your project, including any benefits to the immediate neighborhood as well as the larger community surrounding your project. Please submit documentation if any exists.

In the short term, the project generates jobs, boosts local spending, and reduces household costs. Cashiers, stock clerks, and laundry attendants provide new employment opportunities for neighborhood residents. Staff are likely to live nearby, meaning much of their income stays within the neighborhood economy. Residents save time and money by accessing groceries and laundry within walking distance, lowering transportation expenses and freeing up income for other uses.

In the long term, the reactivation of this corner creates a resilient neighborhood economy with ripple effects that strengthen the larger community—through better employment, improved property values, healthier residents, and more equitable access to essential services. Moreover, investment often begets more investment. Demonstrating that a small business cluster thrives can draw interest from financial institutions, housing developers, and additional businesses.

For the immediate neighborhood this looks like a stable employment base, increased property values, and neighborhood vitality as access to amenities like groceries and laundry facilities makes the area more desirable, boosting real estate values and local tax revenues.

These businesses can also be an “anchor” for other small businesses. Many case studies in neighborhood revitalization show that, once foot traffic increases, other enterprises (cafés, barbershops, childcare providers) often open nearby, creating a “neighborhood hub.” Finally, the immediate neighborhood may see a stronger local spending loop as

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residents spend more within walking distance rather than taking money out of the community by shopping further away.

The economic benefits to the larger community include:

- An economic multiplier effect where wages, profits, and local supply purchases circulate beyond the immediate block, supporting regional farmers, distributors, and service providers.
- Reduced transportation burden as lower car usage means fewer traffic-related costs to the city (wear on roads, parking demand) and potentially lower emissions.
- Food access and equity improvements: Grocery stores in underserved areas help address food deserts, improving long-term health outcomes that reduce public healthcare costs.

6. Describe how your project might benefit the surrounding neighborhood in other ways. Topics to consider are historic preservation, arts and creative outlets, job creation, affordable housing, infill (development in vacant lots, dilapidated properties), recreational access or amenities, and transportation safety or connectivity.

A neighborhood grocery and laundromat aren't just services—they're social infrastructure. They preserve culture, nurture creativity, provide dignity in everyday routines, and knit together housing, recreation, and mobility in ways that elevate quality of life across all socioeconomic groups. For West Central, these amenities can knit together arts, housing, transportation, and recreation in a way that builds pride and stability without displacing long-term residents. There are profound opportunities to partner with current neighborhood assets- other businesses, youth organizations, and community groups- to amplify their impact. In this way, these businesses become both practical amenities and symbolic investments in the neighborhood's future.

- **“Third-Space”:** A small grocery or laundromat often doubles as an informal “third space” where community art can be displayed—murals, rotating exhibits, youth art projects, or cultural storytelling. Laundromats, in particular, can provide waiting time—an opportunity for creative programming like book swaps, art workshops, or music nights. Public art around the building (murals,

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sculptures, mosaics) can beautify the streetscape and deter vandalism, and the project owners are already in discussion about mural opportunities for the buildings.

- **Work-Life Balance:** Beyond income, local jobs provide stability and reduce the need for long commutes, strengthening work–life balance. Many West Central residents rely on public transit or walking; a neighborhood-based job cuts down commuting barriers. Moreover, local hiring fosters pride and makes the grocery and laundromat feel like “our place” rather than an outside business imposed on the neighborhood.
- **Supports Affordable Housing:** These businesses also support affordable housing. Neighborhood amenities like groceries and laundromats make existing affordable housing more viable and attractive by reducing “amenity deserts.” West Central is one of Spokane’s lower-income neighborhoods, with significant numbers of renters. Proximity to daily necessities means households without cars save money and stress, making housing affordability stretch further.
- **Resilient Community:** Filling a vacant or blighted property with a community-serving use reduces crime and increases safety. West Central still has vacant lots and dilapidated properties. Repurposing them with community-serving businesses strengthens the neighborhood’s fabric. Infill reduces the “gaps” along pedestrian corridors, making streets feel safer and more inviting to walk, and it sets a precedent for small-scale, incremental growth rather than large, gentrifying projects.
- **Recreational amenities:** By anchoring foot traffic, the development can support nearby parks, trails, and community centers, which are numerous in Kendall Yards and West Central.
- **Transportation:** By anchoring everyday needs locally and within walking distance, the project strengthens the argument for improved transit service and multimodal infrastructure throughout the neighborhood, benefiting the whole neighborhood.

7. How does your project relate to the City’s Comprehensive Plan? Identify any concepts in the Comprehensive Plan that relate to your project, including any goals or policies related to your project. (see www.shapingspokane.org)

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A small neighborhood grocery and laundromat in West Central strongly align with Spokane's Comprehensive Plan goals — especially policies in Chapter 11 for complete neighborhoods, neighborhood-serving commercial uses, improved transportation access, and equity/anti-displacement — and it can leverage neighborhood planning processes and local infrastructure programs. The Plan promotes multimodal access (walking, biking, transit), and both the laundry and grocery are located along Bus 21. This increases mobility equity and reduces car dependence, matching City priorities.

This project furthers the vision and priorities of Chapter 11 for Neighborhoods in Comprehensive Plan including:

- N 2.2 Neighborhood Centers - Develop neighborhoods that enable citizens to live, work, shop, socialize, and receive other essential services within their neighborhood.
- N 2.3 Special Needs Ensure that neighborhood-based services are available for special needs and located in proximity to public transit routes in order to be accessible to local residents.
- N 2.4 Neighborhood Improvement Encourage revitalization and improvement programs to conserve and upgrade existing properties and buildings.
- N 4.5 Multimodal Transportation Promote a variety of transportation options to reduce automobile dependency and neighborhood traffic. N 4.6 Pedestrian and Bicycle Connections Establish a continuous pedestrian and bicycle network within and between all neighborhoods.

The project supports additional policies in Chapter 4 (Transportation) for neighborhood accessibility, Chapter 7 (Economic Development) as it relates to small business as well as Chapter 8 (Urban Design), and Chapter 10 (Social Health). Applicable policies include:

- TR 7 Neighborhood Access Require developments to have open, accessible, internal multi-modal transportation connections to adjacent properties and streets on all sides.
- DP 2.12 Infill Development - Encourage infill construction and area redevelopment that complement and reinforce positive commercial and residential character.
- SH 4.1 Universal Accessibility - Ensure that neighborhood facilities and programs are universally accessible.

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- SH 8.3 Access to Fresh Produce - Develop incentive programs to encourage convenience stores – especially those located in areas with limited access to full-service grocery stores, to carry fresh produce.
- ED 3.5 Locally-Owned Businesses - Support opportunities to expand and increase the number of locally-owned businesses in Spokane.
- ED 3.6 Small Businesses - Recognize the significant contributions of small businesses to the city's economy and seek to enhance small business opportunities.
- ED 6.1 Infrastructure Projects - Promote infrastructure projects that enhance the city's quality of life and business climate.

8. Please attach any relevant site plans or drawings for your proposed project, preferably showing which improvements are to be TIF-funded.

Attachments:

- Site plans and landscaping with markups for Broadway Foods and Laundromat
- Offsites Costs (TIF-applicable)
- Invoice supporting documentation
- Letters of support from neighbors

Offsites

Work Performed	Broadway Foods	Laundromat	Total	Notes
Remove existing sidewalk & install new concrete w/prep and compa	\$21,700	\$23,000	\$44,700	
Remove existing curb & install new concrete	\$11,950	\$11,500	\$23,450	
Export all extra dirt (50% city)	\$9,000	\$0	\$9,000	
New concrete driveway	\$5,500	\$4,500	\$10,000	
5 Tie in new 4" domestic water	\$9,000	\$0	\$9,000	This is all City-owned
Install water services		\$12,000	\$12,000	This is all City-owned
Prep & Install asphalt in alley	\$12,000	\$12,000	\$24,000	Need CO for Laundromat - Jolly is working with contractor to get this
Replace and relocate Avista poles from center of sidewalks		\$21,794	\$21,794	
City Tree Permit	\$375	\$375	\$750	
Street Trees (arborist)	\$3,750	\$3,750	\$7,500	need invoice - Jolly is getting from landscaper
Landscaping - Labor & Materials	\$4,100	\$3,400	\$7,500	
Asphalt Improvements over connections	\$1,300	\$3,200	\$4,500	Need amounts for this
Parking Signage	\$520		\$520	
Total	\$79,195	\$95,519	\$174,714	

Invoice

Avista Pole relocation	x	
Broadway Foods and Laundry Offsite Trees permit	x	
Broadway Foods & Laundromat site costs	x	
Broadway Signage invoice	x	
Street trees & landscaping - Broadway Foods	x	Confirming these are only offsite costs
Street Trees & landscaping - Laundromat	x	Confirming these are only offsite costs

CIVIL ENGINEERING PLANS
FOR
BROADWAY GROCERY
2229 W BROADWAY AVE SPOKANE, WA 99201

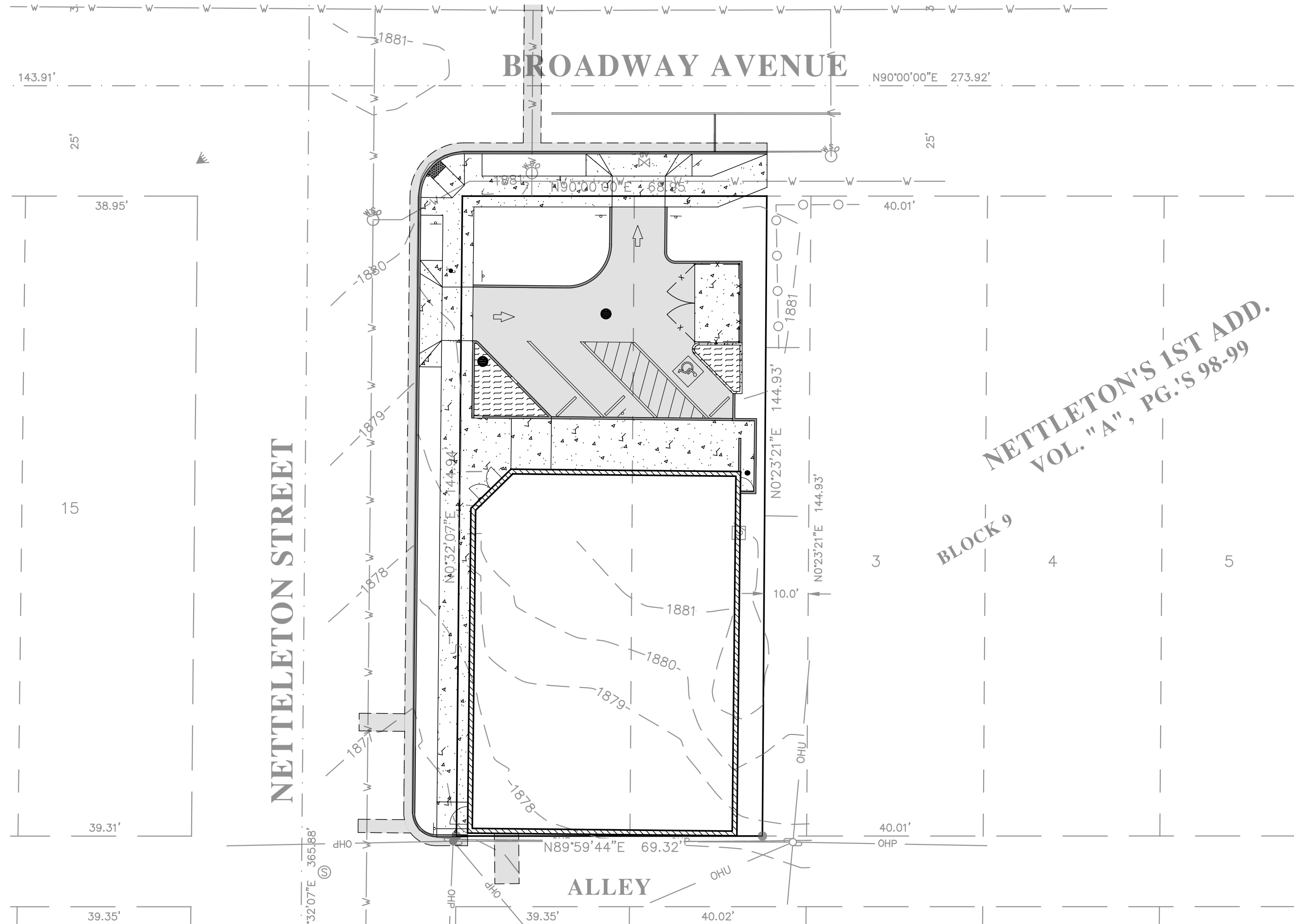
ABBREVIATIONS:

±	PLUS OR MINUS
AC	ASPHALT CONCRETE
AFN	AUDITORS FILE NUMBER
APPROX	APPROXIMATELY
ASPH	ASPHALT
BC	BEGIN CURVE
BKSW	BACK OF SIDEWALK
BLD	BUILDING
BOTW	BOTTOM OF WALL (EL)
BVC	BEGIN VERTICAL CURVE
CB	CATCH BASIN
CL	CENTER LINE
CONC	CONCRETE
CTR	CENTER
CULV	CULVERT
CW	CONCRETE WALK
DI	DUCTILE IRON PIPE
DWY	DRIVEWAY
EC	END OF CURVE
EG	EXISTING GRADE
EL	ELEVATION
EOC	EDGE OF CONCRETE
EOG	EDGE OF GRAVEL
EP	EDGE OF PAVEMENT
ESC	EROSION & SEDIMENTATION CONTROL
EVC	END OF VERTICAL CURVE
EX	EXISTING
FF	FINISHED FLOOR
FG	FINISHED GRADE
FH	FIRE HYDRANT
FL	FLOW LINE
FOC	FACE OF CURB
FS	FINISHED SURFACE
GB	GRADE BREAK
GRVL	GRAVEL
GV	GATE VALVE
GW	GROUND WATER
HP	HIGH POINT
HMA	HOT MIX ASPHALT
IE	INVERT ELEVATION
INT	INTERSECTION
IRR	IRRIGATION
LF	LINEAL FEET
LOC	LOCATION

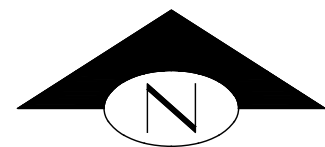
LS	LANDSCAPING
LT	LEFT (OFFSET)
MON	MONUMENT
MUTCD	MANUAL UNIFORM TRAFFIC CONTROL DEVICES
NO.	NUMBER
NTS	NOT TO SCALE
OHW	ORDINARY HIGH WATER
PED	PEDESTRIAN
PC	POINT OF CURVATURE
PCC	POINT OF REVERSE CURVATURE
PL	PROPERTY LINE
PROP	PROPOSED
PT	POINT OF TANGENCY
PVC	POLYVINYL CHLORIDE
PVMT	PAVEMENT
R	RADIUS
ROW	RIGHT OF WAY
RP	RADIUS POINT (CURVE/ CIRCLE)
RT	RIGHT (OFFSET)
PT	POINT OF TANGENCY
RWCL	RIGHT OF WAY CENTER LINE
SD	STORM DRAIN
SDMH	STORM DRAIN MANHOLE
SDR	STANDARD DIMENSION RATIO
SE	SOUTH EAST
SF	FILTER FENCE
SS	SANITARY SEWER
SSCO	SANITARY SEWER CLEANOUT
SSMH	SANITARY SEWER MANHOLE
SSS	SANITARY SIDE SEWER
STA	STATION (ALIGNMENT REFERENCE)
STD	STANDARD
TOC	TOP (BACK) OF CURB
TOE	TOE OF GRADE BREAK
TOP	TOP OF GRADE BREAK
TOPW	TOP OF WALL (EL)
TP	TELEPHONE POLE
TYP	TYPICAL
UNO	UNLESS NOTED OTHERWISE
UT	UNDERGROUND TELEPHONE
VC	VERTICAL CURVE
W/	WITH
WM	WATER MAIN
WSDOT	WASHINGTON STATE DEPARTMENT OF TRANSPORTATION

PROPOSED FEATURES LEGEND:

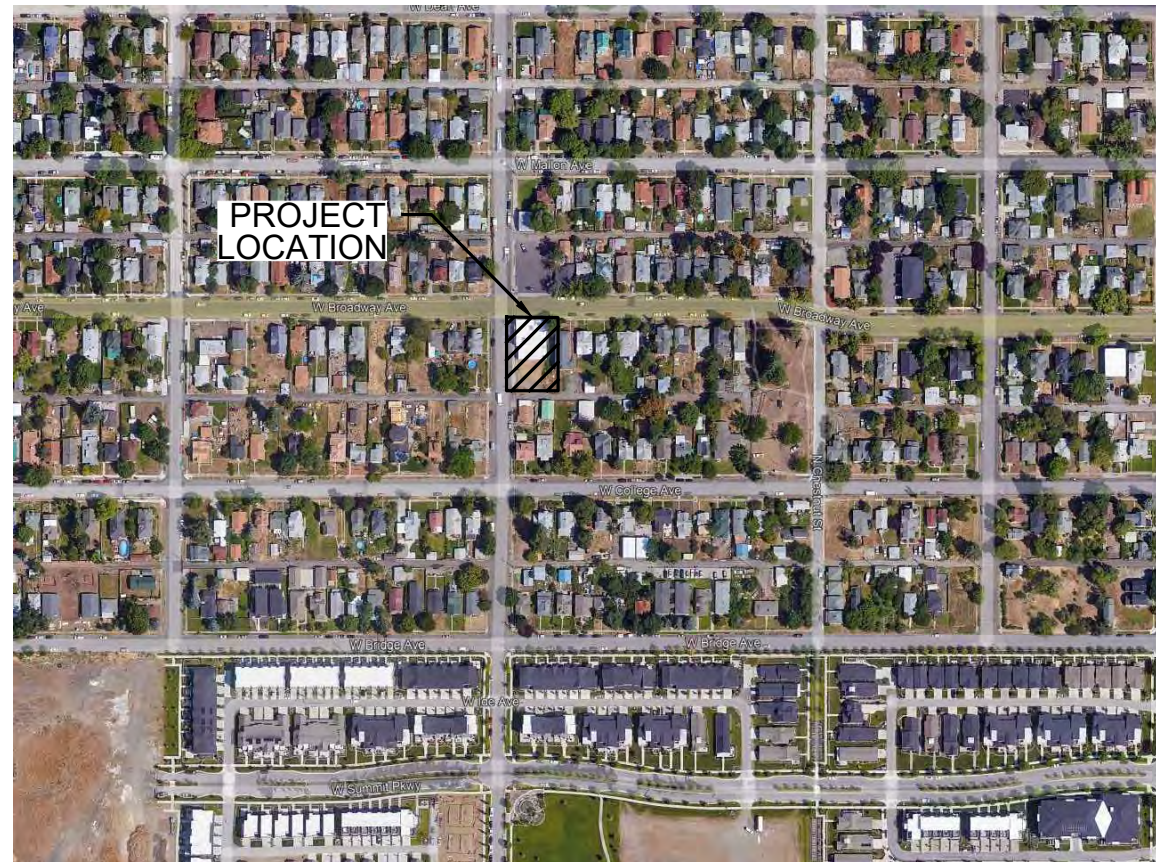
	CONCRETE		SEWER MANHOLE (SSMH)
	ASPHALT		SEWER CLEANOUT (SSCO)
	LANDSCAPE		STORM MANHOLE (SDMH)
	GRAVEL		STORM CATCH BASIN (CB)
	SWALE BOTTOM AREA		STORM CLEANOUT (SDCO)
	SPOT ELEVATION		CULVERT END
	PERFORATED PIPE		OUTFALL ROCK PAD
	STORMWATER PIPE		CONCRETE CURB
	SANITARY SEWER		CONCRETE CURB INLET
	DOMESTIC WATER LINE		
	FIRE WATER LINE		
	TRENCH / SAWCUT LINE		
	PUBLIC ROW		
	ROW CENTERLINE		
	KEY NOTE REFERENCE		
	FLOW DIRECTION ARROW		
	FIRE HYDRANT		
	GATE VALVE (GV)		
	CAP		
	REDUCER (RED)		
	THRUST BLOCKING		
	WATER SERVICE METER		
	POST INDICATOR VALVE (PIV)		
	FIRE DEPARTMENT CONNECTION (FDC)		



SITE MAP
SCALE: 1" = 20'



SHEET LIST TABLE	
SHEET NUMBER	SHEET TITLE
C01	COVER SHEET
C02	NOTES
C03	EXISTING CONDITIONS AND DEMOLITION PLAN
C04	TEMPORARY EROSION & SEDIMENT CONTROL PLAN
C05	SITE PLAN
C06	GRADING PLAN
C07	STORMWATER PLAN
C08	UTILITY PLAN
C09	DETAILS
C10	DETAILS
C11	DETAILS
C12	SIGNAGE & STRIPING PLAN



VICINITY MAP

NTS

DESIGN TEAM:

APPLICANT:
BOONE EXPRESS, LLC
2040 W BOONE AVE
SPOKANE, WA 99201
PHONE: (571) 228-7816
ATTN: JOLLY GHEBREAB

ARCHITECT:
COPELAND ARCHITECTS
121 W PACIFIC AVE, SUITE 300
SPOKANE, WA 99201
PHONE: (509) 838-3600
FAX: (206) 523-1012
ATTN: ERIK FUENTES, PE
SCOTT STALLMAN, PE

ENGINEER:
DAVIDO CONSULTING GROUP, INC.
601 W MAIN ST, SUITE 617
SPOKANE, WA 99201
PHONE: (509) 806-3600
FAX: (206) 523-1012
ATTN: ERIK FUENTES, PE
SCOTT STALLMAN, PE

LOCAL PURVEYORS:

SEWER/STORMWATER:
CITY OF SPOKANE
909 E. SPRAGUE AVENUE
SPOKANE, WA 99202
PHONE: (509) 625-6323
ATTN: MIKE NILSSON

WATER DEPARTMENT:
CITY OF SPOKANE
909 E. SPRAGUE AVENUE
SPOKANE, WA 99202
PHONE: (509) 625-7844
ATTN: DUANE STUDER

FIRE DEPARTMENT:
SPOKANE FIRE DEPARTMENT (STATION 1)
44 W. RIVERSIDE AVENUE
SPOKANE, WA 99201
PHONE: (509) 625-7056
ATTN: DAVE KOKOT

NATURAL GAS & POWER:
AVISTA UTILITIES
1411 E. MISSION AVENUE
SPOKANE, WA 99220
NATURAL GAS ATTN: CHRISTIAN WRIGHT
POWER ATTN: NICK HARMON
PHONE: (509) 495-4889

SURVEY INFORMATION:

THE SURVEY AND MAPPING INFORMATION, INCLUDING BUT NOT LIMITED TO EXISTING SURFACE FEATURES, PROPERTY LINES, RIGHT-OF-WAY, CENTERLINE, EASEMENTS, AND RECORD INFORMATION, SHOWN ON THESE IMPROVEMENT PLANS WERE PROVIDED BY THE SURVEYOR BELOW. A COPY, SIGNED AND SEALED BY THE PROFESSIONAL LAND SURVEYOR IS ON FILE WITH THE DESIGN ENGINEER.

BENTHIN & ASSOCIATES
1004 N ATLANTIC ST, SPOKANE, WA 99201
PHONE: (509) 325-4529

STANDARDS:

THE IMPROVEMENTS SHOWN ON THE PLANS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE STANDARD DRAWINGS AND SPECIFICATIONS LISTED BELOW.

- CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION)
- WSDOT STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION)

BASIS OF BEARINGS:

BASIS OF BEARINGS FOR THIS SURVEY IS THE MONUMENT LINE ALONG BROADWAY AVENUE PER FOUND MONUMENTS, PER ROS BK. 90, PG. 32.

LEGAL DESCRIPTION:

PER STATUTORY WARRANTY DEED AFN. 6860148

PARCEL A
LOT 1 AND LOT 2, EXCEPT THE EAST 10 FEET THEREOF, BLOCK 9, NETTLETON'S FIRST ADDITION, ACCORDING TO PLAT RECORDED IN VOLUME "A" OF PLATS, PAGE 98, IN THE CITY OF SPOKANE, SPOKANE COUNTY, WASHINGTON.

PARCEL B
LOTS 29 AND 30, BLOCK 8, NETTLETON'S FIRST ADDITION, AS PER PLAT RECORDED IN VOLUME "A" OF PLATS, PAGE 98, RECORDS OF SPOKANE COUNTY, SITUATE IN THE CITY OF SPOKANE, COUNTY OF SPOKANE, STATE OF WASHINGTON.

REVISION	BY	DATE	NO.	1	2	3	4	5	6	7	8
CITY PERMIT COMMENTS #1	SS	08/25/2023	1								
CITY PERMIT COMMENTS #2	EF	09/22/2023	2								
CITY PERMIT COMMENTS #3	EF	11/07/2023	3								
LEED ACCREDITED PROFESSIONAL & THE RELATED ARCHITECT & THE LANDSCAPE ARCHITECT ARE OWNED BY THE U.S. GREEN BUILDING COUNCIL & ARE AWARDED TO INDIVIDUALS UNDER LICENSE BY THE GREEN BUILDING CERTIFICATION INSTITUTE.											
601 W Main Ave Suite 617 Spokane, WA 99201 P: 509.600.3600 F: 206.523.1012 www.dcgengr.com											
DCG civil structural											
CALL 811 2 BUSINESS DAYS BEFORE YOU DIG UNDERGROUND UTILITY LOCATIONS ARE APPROX.											
11/07/2023											
BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, & ALL OTHER EX FEATURES & CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN & OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.											
OWNER: BOONE EXPRESS, LLC 2040 W BOONE AVE SPOKANE, WA 99201											
PROJECT: BROADWAY GROCERY 2229 W BROADWAY AVE, SPOKANE, WA 99201											
PROJ. MANAGER: EF											
DESIGNED BY: SS											
DRAWN BY: SS											
CHECKED BY: EF											
SCALE: AS SHOWN											
DATE: 11/07/2023	REV: 3	SHEET: 12									
SHEET NUMBER											
C01											

GENERAL NOTES:

- ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION METHODS SHALL CONFORM TO WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARD PLANS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION (LATEST EDITION), AND THE CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION).
- CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO HOLD HARMLESS, INDEMNIFY AND DEFEND THE OWNER, THE DESIGN PROFESSIONAL AND THEIR CONSULTANTS, AND THE CITY AGENCY, AND EACH OF THEIR OFFICERS, EMPLOYEES, AND AGENTS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL.
- THE LOCATIONS OF UNDERGROUND OBSTRUCTIONS SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND SHOULD NOT BE TAKEN AS FINAL OR ALL INCLUSIVE. THE CONTRACTOR IS CAUTIONED THAT THE PLANS MAY NOT INCLUDE ALL EXISTING UTILITIES AND THAT THE OWNER, THE CITY AGENCY, AND DESIGN PROFESSIONAL ASSUME NO RESPONSIBILITY OF OBSTRUCTIONS WHICH MAY BE ENCOUNTERED.
- THE CONTRACTOR SHALL RECOGNIZE THAT UNDERGROUND FACILITIES NOT SHOWN AS CIVIL IMPROVEMENTS (POSE, TELEPHONE, TV, IRRIGATION, ETC.) SHALL BE COORDINATED AND CONSTRUCTED PRIOR TO PLACEMENT OF BASE ROCK AND PAVING.
- CONTRACTOR IS RESPONSIBLE FOR PRESERVATION AND/OR PERPETUATION OF ALL EXISTING SURVEY MONUMENTS (IRON PIPES ON LOT LINES AND CORNER, CENTERLINE WELL DISKS, ETC.), WHICH CONTROL SUBDIVISIONS, TRACTS, BOUNDARIES, STREETS, HIGHWAYS, OR OTHER RIGHT-OF-WAY, EASEMENTS, OR PROVIDE SURVEY CONTROL WHICH WILL BE DISTURBED OR REMOVED DUE TO CONTRACTOR'S WORK. CONTRACTOR SHALL PROVIDE A MINIMUM OF 10 (TEN) WORKING DAYS NOTICE TO THE ENGINEER/SURVEYOR PRIOR TO DISTURBANCE OR REMOVAL OF EXISTING MONUMENTS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE OR DETERIORATION OCCURRING TO EXISTING PUBLIC IMPROVEMENTS AS A DIRECT RESULT OF CONSTRUCTION ACTIVITY RELATED TO CONSTRUCTION OF THE COMMON IMPROVEMENTS (GRADING, ROAD CONSTRUCTION, UTILITY INSTALLATION, ETC.). REQUIRED REPAIR MAY REQUIRE PATCHING, SEALING OR OVERLAYING AFFECTED AREAS AS APPROPRIATE TO RETURN THE ROADS TO AS GOOD A CONDITION AS THEY WERE PRIOR TO CONSTRUCTION. IF THE CONTRACTOR DOES NOT ACT PRUDENTLY IN A TIMELY MANNER, THE CITY MAY, AT ITS DISCRETION PERFORM THE CORRECTION AND CHARGE THE CONTRACTOR FOR ALL COSTS AND OVERHEAD INCURRED.
- CONSTRUCTION SITE WASTE MANAGEMENT IS THE RESPONSIBILITY OF THE OWNER/CONTRACTOR. THE CONSTRUCTION SITE MUST BE KEPT CLEAN AND FREE FROM NON-STORM WATER MATERIAL DISCHARGES, INCLUDING BUT NOT LIMITED TO SOLID WASTE SUCH AS PACKAGING MATERIALS, WOOD, PAPER, AND PLASTIC; SCRAP OR SURPLUS BUILDING MATERIALS SUCH AS METALS, RUBBER, PLASTIC, GLASS AND MASONRY PRODUCTS; AND WASTES SUCH AS FOOD CONTAINERS, COFFEE CUPS, PAPER BAGS, AND CIGARETTES; CONSTRUCTION AND LANDSCAPING MATERIALS, AND HAZARDOUS WASTE SUCH AS PETROLEUM PRODUCTS, WOOD PRESERVATIVES, PAINTS, MORTARS, AND SOLVENTS. ANY ACTIVITY DURING CONSTRUCTION WHICH ACTUALLY AND/OR MAY POTENTIALLY RESULT IN THE DISCHARGE OF SUCH POLLUTANTS INTO THE CITY'S STORM WATER SYSTEM IS IN VIOLATION OF CITY AND STATE WATER ORDINANCE AND STATE BOARD REGULATIONS. THE SITE SHALL REMAIN NEAT AND FREE FROM ANY SUCH POLLUTANTS. THE SITE MUST HAVE AN ADEQUATE NUMBER OF WATERTIGHT CONTAINERS WITH LIDS OR COVERS THAT CAN BE PLACED OVER THE CONTAINER TO KEEP OUT RAIN OR TO PREVENT LOSS OF WASTE DUE TO WIND. THE CONTRACTOR MUST COLLECT SITE TRASH DAILY AND ARRANGE FOR REGULAR WASTE COLLECTION BEFORE CONTAINER OVERFLOWS.
- IN THE EVENT THAT ANY REMAINS OF PREHISTORIC OR HISTORIC HUMAN ACTIVITIES ARE ENCOUNTERED DURING PROJECT-RELATED ACTIVITIES, WORK IN THE IMMEDIATE VICINITY OF THE FINDS SHALL HALT AND THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT SUPERINTENDENT AND THE CITY AGENCY INSPECTOR. WORK SHALL NOT RESUME UNTIL A QUALIFIED ARCHAEOLOGIST OR HISTORIC ARCHAEOLOGIST, AS APPROPRIATE, APPROVED BY THE CITY AGENCY, HAS EVALUATED THE SITUATION AND MADE RECOMMENDATIONS FOR TREATMENT OF THE RESOURCE, AND WHOSE RECOMMENDATIONS ARE CARRIED OUT. IF HUMAN BURIALS ARE ENCOUNTERED, THE CONTRACTOR MUST ALSO NOTIFY THE COUNTY CORONER.
- A COMPLETE SET OF THE APPROVED AND PERMITTED PLANS ARE REQUIRED TO BE ON SITE AT ALL TIMES THROUGHOUT THE LIFE OF THE PROJECT.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN ALL REQUIRED PERMITS AND APPROVALS FROM THE CITY OF SPOKANE PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING A PRE-CONSTRUCTION MEETING WITH A CITY OF SPOKANE CONSTRUCTION INSPECTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
- A CITY OF SPOKANE CONSTRUCTION INSPECTOR IS REQUIRED TO PERFORM FORM AND SUBGRADE INSPECTION PRIOR TO THE POURING OF CONCRETE. THE CITY REQUIRES TWENTY FOUR HOURS' NOTICE PRIOR TO INSPECTION.
- ALL MAINS AND APPURTENANCES SHALL BE INSPECTED AND APPROVED BY THE CITY OF SPOKANE'S CONSTRUCTION INSPECTOR PRIOR TO BACKFILLING. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING THE CITY OF SPOKANE FOR ALL REQUIRED INSPECTIONS. SHOULD ADDITIONAL TESTING AND INSPECTIONS DETECT DEFICIENCIES AND/OR FAILURES, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CORRECT SAID DEFICIENCIES AND/OR FAILURES.
- THROUGHOUT THE LIFE OF PROJECT ALL TESTING SHALL BE DONE IN CONFORMANCE WITH WSDOT STANDARD SPECIFICATIONS, LATEST EDITION, AND THE PROVIDED GEOTECHNICAL REPORT.
- ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND TO BE BROKEN, HEAVED, SUNKEN, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION PER SPOKANE MUNICIPAL CODE 12.01.010. THE OWNER/CONTRACTOR WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509) 625-6300 TO ARRANGE A SITE VISIT.
- THE CONTRACTOR SHALL REVIEW THE CITY NOISE ORDINANCE AND PLAN HOURS OF CONSTRUCTION ACCORDINGLY.
- ANY AND ALL PROPOSED CHANGES TO PROVIDED APPROVED DESIGN SHALL BE REVIEWED AND APPROVED BY THE CIVIL ENGINEER AND THE CITY OF SPOKANE.
- THE CONTRACTOR SHALL KEEP A RECORD OF ANY AND ALL DEVIATIONS FROM THE APPROVED PLANS THAT OCCUR THROUGHOUT THE LIFE OF THE PROJECT FOR RECORD DRAWING PURPOSES.
- THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING THE AUTHORITY HAVING JURISDICTION FOR ALL PERMITS AND APPROVALS REQUIRED FOR ANY WORK THAT IS PROPOSED TO TAKE PLACE IN THE PUBLIC RIGHT-OF-WAY.
- PRIOR TO THE COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES. CALL THE UNDERGROUND UTILITY LOCATION SERVICE AT 811 BEFORE YOU DIG.
- LOCATIONS OF EXISTING UTILITIES SHOWN IN THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES. ANY CONFLICTING UTILITIES SHALL BE RELOCATED PRIOR TO CONSTRUCTION.
- IF THE CONTRACTOR DISCOVERS ANY DISCREPANCIES BETWEEN THE PLANS AND EXISTING FIELD CONDITIONS ENCOUNTERED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE DESIGN ENGINEER.

EROSION & SEDIMENT CONTROL NOTES:

- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE USED TO ENSURE THAT WATER ENTERING THE STORM DRAIN SYSTEM BELOW THE CONSTRUCTION SITE IS EQUIVALENT QUALITY AND CHARACTER AS THE WATER ABOVE THE SITE.
- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE PLACED IN FRONT OF INCOMPLETE STORM DRAIN SYSTEMS TO PREVENT DEBRIS AND SEDIMENT-LOADED WATER FROM ENTERING INTO THE PUBLIC STORM DRAIN SYSTEM. BEST MANAGEMENT PRACTICES SHALL BE USED WHEN DESIGNING AND INSTALLING SUCH DEVICES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTANT MAINTENANCE OF EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES TO THE SATISFACTION OF THE ENGINEER AND THE CITY OF SPOKANE. EROSION AND SEDIMENT CONTROL MEASURES AND THEIR INSTALLATION SHALL BE ACCOMPLISHED USING BEST MANAGEMENT PRACTICES.
- STORMWATER RUNOFF FROM THE CONSTRUCTION SITE SHALL BE DIRECTED TOWARD AN INLET WITH A SEDIMENT OR FILTRATION INTERCEPTOR PRIOR TO ENTERING THE STORM DRAIN SYSTEM.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR CLEANING WATER THAT HAS BECOME POLLUTED DUE TO NOT TAKING NECESSARY EROSION AND SEDIMENT CONTROL ACTIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANUP OF MUD AND DEBRIS CARRIED ONTO SURROUNDING STREETS AND ROADS AS A RESULT OF CONSTRUCTION ACTIVITY ON THE SITE TO THE SATISFACTION OF THE CITY OF SPOKANE.
- THE CONTRACTOR IS TO INFORM ALL CONSTRUCTION SITE WORKERS ABOUT THE MAJOR PROVISIONS OF THE EROSION AND SEDIMENT CONTROL PLAN AND SEEK THEIR COOPERATION IN AVOIDING THE DISTURBANCE OF THESE CONTROL MEASURES.
- UNSTABILIZED AREAS WILL BE REPAIRED AS SOON AS POSSIBLE AFTER BEING DAMAGED.
- ALL GRADED OR DISTURBED AREAS SHALL BE STABILIZED IMMEDIATELY AFTER GRADING IS COMPLETE.

CALL 811
2 BUSINESS DAYS
BEFORE YOU DIG
(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)

EROSION & SEDIMENT CONTROL NOTES CONT.:

- ENTRANCE TO THE PROJECT SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT INTO PUBLIC RIGHT-OF-WAY. WHEN NECESSARY, WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE OF PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED IT SHALL BE DONE IN AN AREA STABILIZED WITH CRUSHED ROCK THAT DRAINS INTO A SEDIMENT TRAP.
- ALL SEDIMENT SPILLED, DROPPED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY SHALL BE REMOVED IMMEDIATELY USING BEST MANAGEMENT PRACTICES.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR PURPOSE SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REPAIRED OR REPLACED WHEN THEY ARE NO LONGER FUNCTIONING PER BEST MANAGEMENT PRACTICES.
- AFTER CONSTRUCTION IS COMPLETE ALL STORM DRAIN SYSTEMS ASSOCIATED WITH THIS PROJECT SHALL BE INSPECTED AND CLEARED OF ACCUMULATED SEDIMENTS AND DEBRIS.
- GRADED AREAS TO BE SEED FOR EROSION CONTROL SHALL USE GRASS SEED AT THE RATE OF 75-100/LBS. PER ACRE. SEEDING AREAS SHALL BE IRRIGATED TO ENSURE COVER IS ROOTED PRIOR TO RAINY SEASON.
- STABILIZATION OF EXPOSED GRADED AREAS WITH STRAW MULCH SHALL BE APPLIED AT A RATE OF 2 TONS PER ACRE.
- TEMPORARY SILT AND DRAINAGE CONTROL FACILITIES SHALL BE INSTALLED TO CONTROL AND CONTAIN EROSION-CAUSED SILT DEPOSITS AND TO PROVIDE FOR THE SAFE DISCHARGE OF STORMWATER INTO EXISTING STORM WATER FACILITIES. DESIGN OF THESE FACILITIES MUST BE APPROVED BY THE CITY ENGINEER AND IN PLACE PRIOR TO THE START OF GRADING.
- FILL SLOPES: AVOID LEAVING SHINY, SMOOTH GRADED SURFACES. THE LAST GRADING OPERATION SHALL BE TO WALK A TRACK-TYPE TRACTOR UP AND DOWN THE SLOPE, CREATING CLEAT MARKS ON THE SLOPE WITH CONTOURS. THESE WILL PROVIDE SEED AND FIBER COLLECTION POINTS.
- CUT SLOPES: AVOID LEAVING SHINY, SMOOTH GRADED SURFACES. THE LAST GRADING OPERATION SHALL LEAVE THE SLOPE IN A ROUGHENED CONDITION WITH 2 INCHES OF LOOSENED MATERIAL FOR SEEDING.
- AFTER THE UNDERGROUND STORM DRAIN SYSTEM IS INSTALLED, THE CATCH BASINS WILL BE INSTALLED (AS SOON AS PRACTICAL) AND THE SEDIMENT CONTROL DEVICE WILL BE PLACED AROUND THOSE CATCH BASINS AS SHOWN ON THE EROSION CONTROL SHEET UNTIL THE SITE IS PAVED AND CLEANED.
- CONTRACTOR TO PROVIDE TEMPORARY SEDIMENT CONTROL DEVICE AT CATCH BASINS AS SHOWN IN THE DETAILS. CONTRACTOR MAY SUBSTITUTE OTHER SEDIMENT CONTROL DEVICES (GRAVEL BAGS, SILT TRAPS, ETC.) UNDER THE DIRECTION OF THE CIVIL ENGINEER.
- ACCESS ROADS: AS NECESSARY, ANY SEDIMENT OR OTHER CONSTRUCTION RELATED MATERIALS DEPOSITED ON ACCESS ROADS SHALL BE REMOVED PRIOR TO ANY RAIN EVENT BY VACUUMING OR SWEEPING.
- WIND EROSION CONTROL: STOCKPILED WASTE MATERIAL SHALL BE CONTAINED AND SECURELY PROTECTED FROM WIND EROSION AT ALL TIMES WHEN NOT IN USE.
- CONTRACTOR SHALL PROVIDE EFFECTIVE SOIL COVER FOR INACTIVE AREAS WHERE CONSTRUCTION ACTIVITY HAS DISTURBED SOIL BUT ARE NOT SCHEDULED TO RE-DISTURB SOIL FOR AT LEAST 14 DAYS.
- THE CONTRACTOR IS RESPONSIBLE FOR REVIEWING APPENDIX BA - EROSION/SEDIMENTATION CONTROL STANDARD PLAN NOTES FOUND IN THE SPOKANE REGIONAL STORMWATER MANUAL DATED APRIL 2008. THE CONTRACTOR SHALL REVIEW ALL 22 NOTES AND UTILIZE ANY AND ALL NOTES THAT APPLY TO THIS PROJECT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF THE CONTRACTOR CANNOT ACCESS APPENDIX 9A.

GRADING NOTES:

- NO GRADING, CLEARING OR GRUBBING SHALL BE PERFORMED PRIOR TO ISSUANCE OF A GRADING PERMIT FROM THE CITY OF SPOKANE.
- DUST CONTROL SHALL BE PROVIDED BY CONTRACTOR DURING ALL PHASES OF CONSTRUCTION.
- CONTRACTOR SHALL PROTECT EXISTING DRAINAGE FACILITIES FROM SEDIMENTATION DURING ALL PHASES OF CONSTRUCTION.
- ALL HAZARDOUS MATERIAL SHALL BE REMOVED AND LEGALLY DISPOSED OF. THE APPLICANT IS REQUIRED TO DEMONSTRATE COMPLIANCE WITH STATE AND LOCAL CODES FOR REMOVAL OF ASBESTOS CONTAINING MATERIALS DURING DEMOLITION OF THE STRUCTURES ON THE PROJECT SITE.
- ALL PADS SHALL BE CONSTRUCTED TO A TOLERANCE OF 0.1 FEET +/- FROM SHOWN PAD ELEVATION.
- EARTHWORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF SPOKANE STANDARDS AND THE PROVIDED GEOTECHNICAL REPORT.
- ALL INDEPENDENT LABORATORY INSPECTION AND TESTING REQUIRED BY THE CITY SHALL BE PAID FOR BY THE CONTRACTOR.
- SUBGRADE SHALL BE COMPACTED IN ACCORDANCE WITH THE CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION), WSDOT STANDARD SPECIFICATIONS (LATEST EDITION), AND THE GEOTECHNICAL REPORT.
- THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY FIELD CHANGES MADE WITHOUT WRITTEN AUTHORIZATION FROM THE ENGINEER.
- ALL AREAS ARE TO BE GRADED TO THE ELEVATIONS SHOWN.
- PROPOSED TOP OF CURB SHALL BE 0.50 FEET ABOVE PROPOSED ASPHALT/CONCRETE PAVEMENT UNLESS NOTED OTHERWISE.
- ALL PROPOSED SITE GRADING, REQUIRED EXCAVATION FOR PROPOSED IMPROVEMENTS, EMBANKMENTS, AND UTILITY TRENCHING SHALL BE COMPLETED IN ACCORDANCE TO THE CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION), WSDOT STANDARD SPECIFICATIONS (LATEST EDITION), AND THE GEOTECHNICAL REPORT.
- ALL PROPOSED VERTICAL AND HORIZONTAL IMPROVEMENTS (CURB, CURB AND GUTTER, SIDEWALKS, STREET GRADES, ETC.) SHALL BE STAKED BY AN ENGINEERING OR SURVEYING FIRM.
- ALL PROPOSED FINISHED GRADES SHALL MATCH EXISTING FINISHED GRADES ALONG THE SITE'S BOUNDARY. IF A CONFLICT EXISTS BETWEEN THE GRADING PLAN AND ACTUAL FIELD CONDITIONS THE CONTRACTOR SHALL NOTIFY THE CIVIL ENGINEER TO RESOLVE THE ISSUE PRIOR TO PROCEEDING.

PAVING NOTES:

- ALL PROPOSED PAVING SHALL BE COMPLETED IN ACCORDANCE TO THE CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION), WSDOT STANDARD SPECIFICATIONS (LATEST EDITION), AND RECOMMENDATIONS PROVIDED IN THE GEOTECHNICAL REPORT.
- WHERE PROPOSED ASPHALT PAVEMENT JOINS EXISTING ASPHALT PAVEMENT, THE EXISTING ASPHALT PAVEMENT SHALL BE SAWCUT TO A NEAT VERTICAL COMPETENT EDGE AND TACKED WITH ASPHALT EMULSION IN ACCORDANCE TO CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION) AND WSDOT STANDARD SPECIFICATIONS (LATEST EDITIONS).
- PRIOR TO PLACING BASE MATERIAL, THE CONTRACTOR SHALL ADHERE TO THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT IN REGARDS TO SUBGRADE PREPARATION.
- ALL EXISTING AND PROPOSED UTILITY LIDS, VALVES, MANHOLE COVERS SHALL BE ADJUSTED TO PROPOSED FINISHED PAVEMENT GRADE.
- ALL PROPOSED ASPHALT PAVEMENT SHALL BE INSTALLED 2" ABOVE PROPOSED OR EXISTING CONCRETE EDGES, UTILITY LIDS, MANHOLE COVERS, VALVES, ETC. UNLESS NOTED OTHERWISE, ALL PROPOSED CONCRETE EDGES SHALL HAVE A ROUNDED 2" RADIUS.
- ALL PHASES OF PAVING WORK SHALL BE COORDINATED, INSPECTED, AND APPROVED BY THE CITY OF SPOKANE. THE CONTRACTOR IS RESPONSIBLE FOR SCHEDULING ALL INSPECTIONS.
- IF REQUIRED, TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. ALL TRAFFIC CONTROL PLANS MUST BE SUBMITTED AND APPROVED BY THE CITY OF SPOKANE.

STORMWATER NOTES:

- ALL PROPOSED AND EXISTING STORMWATER SYSTEMS SHALL BE KEPT FREE OF DIRT AND DEBRIS DURING ALL PHASES OF CONSTRUCTION.
- STORMWATER FACILITIES, INCLUDING DRYWELLS, CATCH BASINS, AND PIPES MUST BE CONSTRUCTED UNDER THE SUPERVISION OF THE WASTEWATER MANAGEMENT DIVISION. STORMWATER TREATMENT FACILITIES (208 SWALES) SHALL BE INSPECTED PRIOR TO PLACEMENT OF TOPSOIL, PLANTINGS, OR GRASS. THE CONTRACTOR SHALL CONTACT THE WASTEWATER MAINTENANCE DIVISION OFFICE AT (509)625-7905 OR (509)625-7912 IN ORDER TO ARRANGE A MUTUALLY AGREEABLE INSPECTION SCHEDULE.
- ALL TRENCHING AND BEDDING SHALL BE COMPLETED IN ACCORDANCE TO CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION).
- PROPOSED SWALE BOTTOM ELEVATIONS ARE TO PROPOSED FINISHED GRADE. CONTRACTOR WILL NEED TO TAKE LANDSCAPE FINISHES INTO CONSIDERATION AND PREPARE SUBGRADE ACCORDINGLY. COORDINATE WITH LANDSCAPE PLANS AND LANDSCAPE ARCHITECT.
- ALL PROPOSED SWALES AND DRAINAGE FACILITY SHALL BE FINISHED WITH LANDSCAPE AND IRRIGATED. COORDINATE WITH LANDSCAPE ARCHITECT AND LANDSCAPE PLANS.
- THE CONTRACTOR SHOULD TAKE PRECAUTIONS TO PROTECT THE INFILTRATION CAPACITY OF STORMWATER FACILITIES (E.G., LINE THE FACILITY WITH FILTER FABRIC, OVER EXCAVATE UPON COMPLETION OF THE INFRASTRUCTURE, ETC.).
- BIO-INFILTRATION SWALES SHALL HAVE A MAXIMUM TREATMENT DESIGN DEPTH (FROM SWALE BOTTOM TO ELEVATION OF DRYWELL GRATE OR FIRST OVERFLOW/OUTFLOW MECHANISM) OF 8". EITHER ORGANIC MATTER CONTENT OR CATION EXCHANGE CAPACITY (CEC) TESTING SHALL BE COMPLETED IN ORDER TO SUBSTANTIATE THE TREATMENT SOIL COMPOSITION. THE TESTS SHALL BE PERFORMED ON COMPOSITE SAMPLES TAKEN FROM THE TREATMENT SOIL LAYER FROM THE CONSTRUCTED POND BOTTOM A COMPOSITE SAMPLE CONSISTS OF WELL-MIXED SOIL OBTAINED FROM AT LEAST FOUR CORES, TO A DEPTH OF AT LEAST 6", RANDOMLY DISTRIBUTED OVER THE POND BOTTOM TEST AREA. STOCKPILE SAMPLES FROM ON-SITE OR A MATERIAL SUPPLIER CAN BE TESTED FOR INFORMATIONAL PURPOSES TO DETERMINE INITIAL SUITABILITY AND POSSIBLE SOIL AMENDMENTS, BUT WILL NOT BE ACCEPTED IN-LEU OF IN-PLACE TESTING. A MINIMUM OF ONE TEST SHALL BE PERFORMED FOR EACH BIO-INFILTRATION SWALE. 1,500 SQUARE FEET OR LESS, WITH ONE ADDITIONAL TEST FOR EACH ADDITIONAL 2,000 SQUARE FEET OF SWALE BOTTOM, OR FRACTION THEREOF. "ONE TEST" IS EQUAL TO FOUR CORE SAMPLES TAKEN AS DESCRIBED ABOVE. TESTING RESULTS SHALL BE SUBMITTED AS PART OF THE CONSTRUCTION CERTIFICATION SUBMITTAL REQUIRED FOR RELEASE OF SURETY POSTED ON THE PROJECT.
- CONCRETE APRONS ARE REQUIRED AT THE INLET INTO ANY SWALE OR POND. THE PROPOSED FINISHED GRADE OF THE SWALE/POND SIDE SLOPE, WHERE THE CONCRETE APRON ENDS, SHALL BE A MINIMUM OF 2" BELOW THE FINISHED ELEVATION OF THE CONCRETE CURB APRON EXTENSION. THE INTENTION IS TO ALLOW STORMWATER RUNOFF TO ENTER THE SWALE/POND UNOBSTRUCTED, WITHOUT BACKING UP INTO THE STREET AND GUTTER DUE TO SOIL OVERGROWTH AT THE INLET.
- UNLINED POND AND BIO-INFILTRATION SWALE BOTTOMS ARE EXPECTED TO INFILTRATE VIA THE POND FLOOR, AND THEREFORE, SHALL NOT BE HEAVILY COMPACTED; EQUIPMENT TRAFFIC SHALL BE MINIMIZED ON THE POND BOTTOMS. THE FACILITY SUBGRADE SHALL BE A MEDIUM-TO WELL-DRAINING MATERIAL, WITH A MINIMUM THICKNESS OF 4" AND A MINIMUM INFILTRATION RATE OF 0.15 IN/HR. THE FACILITY SHALL DRAIN WITHIN 72 HOURS OF A STORM EVENT, IF THE POND ALSO SERVES AS A WATER QUALITY TREATMENT FACILITY, THE TREATMENT ZONE (SOD AND 6" OF TREATMENT SOIL) SHALL BE A MEDIUM-TO WELL-DRAINING MATERIAL, WITH A MINIMUM INFILTRATION RATE OF 0.25-0.50 IN/HR. SILTY LOAM OR LOAMY SOILS ARE PRESUMED TO HAVE AN INFILTRATIVE RATE THAT FALLS WITHIN THIS RANGE. SCARIFY THE FINISHED GRADE OF THE SWALE BOTTOM PRIOR TO HYDROSEEDING/SODDING. TESTING THAT VERIFIES SUBGRADE MINIMUM INFILTRATION RATE IS REQUIRED BY THE LOCAL JURISDICTION PRIOR TO CONSTRUCTION CERTIFICATION TO ENSURE ADEQUATE DRAINAGE. INFILTRATIVE TESTING OF THE TREATMENT ZONE IS ONLY REQUIRED IF SOILS OTHER THAN SILTY LOAM OR LOAMY SOILS ARE PROPOSED.
- IF, DURING THE FINAL INSPECTION, IT IS FOUND THAT THE CONSTRUCTION POND OR SWALE DOES NOT CONFORM TO THE ACCEPTED DESIGN, THE SYSTEM SHALL BE RECONSTRUCTED SO THAT IT DOES COMPLY.

STORM DRAIN MATERIALS

THE FOLLOWING STANDARD PIPE MATERIALS SHALL BE USED FOR STORMWATER CONSTRUCTION AND SHALL CONFORM TO THE LATEST EDITIONS OF CITY OF SPOKANE GENERAL PROVISIONS FOR PRIVATE CONTRACTS, AND AMERICAN SOCIETY OF TESTING MATERIALS STANDARDS:

MATERIAL	SPECIFICATION	DIAMETER
PVC PIPE	ASTM D3034-SDR 35	4"-15"
PVC PIPE	ASTM F789-PS 46	18"-36"
PVC PIPE FITTINGS	ASTM D3212 & F477	ALL

UTILITY NOTES:

- THE LOCATIONS OF UNDERGROUND OBSTRUCTIONS SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND SHOULD NOT BE TAKEN AS FINAL OR ALL INCLUSIVE. THE CONTRACTOR IS CAUTIONED THAT THE PLANS MAY NOT INCLUDE ALL EXISTING UTILITIES AND THAT THE OWNER, THE CITY AGENCY AND ENGINEER ASSUME NO RESPONSIBILITY OF OBSTRUCTIONS WHICH MAY BE ENCOUNTERED.
- RECORD PLAN DRAWINGS SHALL BE PROVIDED, IF REQUESTED, UPON COMPLETION OF PROJECT PRIOR TO FINAL ACCEPTANCE.
- ALL PIPE LENGTHS SHOWN ARE MEASURED HORIZONTALLY TO INSIDE EDGE OF MANHOLE STRUCTURES OR TO THE CENTER OF MINOR DEVICES SUCH AS INLETS OR CLEANOUTS.
- ALL PROPOSE WATER MAINS SHALL BE STAKED FOR HORIZONTAL AND VERTICAL ALIGNMENT BY A LICENSED ENGINEERING OR SURVEYING FIRM.
- WHEN INSTALLING PROPOSED UTILITIES (I.E. STORM, WATER, SANITARY SEWER), THE CONTRACTOR SHALL MAINTAIN UTILITY SEPARATION IN CONFORMANCE TO THE CITY OF SPOKANE STANDARD PLANS A-4, A-5, A-6, AND A-7.
- PROPOSED PIPE BEDDING SHALL CONFORM TO THE CITY OF SPOKANE STANDARD PLANS A-1, A-2, AND A-3.

SANITARY SEWER NOTES:

- THE CONTRACTOR SHALL EXPOSE ALL EXISTING SANITARY SEWER PIPES WHERE A CONNECTION IS TO BE MADE SO THAT THE ENGINEER CAN VERIFY EXISTING FLOWLINES AND LOCATIONS BEFORE START OF CONSTRUCTION.
- SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN WSDOT STANDARD SPECIFICATIONS (LATEST EDITION) AND THE CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION).
- ALL PROPOSED SANITARY SEWER LINES SHALL MEET THE SPECIFICATIONS SHOWN HEREON.
- ALL PROPOSED SEWER MAINS SHALL BE STAKED FOR VERTICAL AND HORIZONTAL ALIGNMENT BY A LICENSED ENGINEERING OR SURVEYING FIRM.
- THE CONTRACTOR SHALL INSTALL TRACER TAPE IN THE EXCAVATION TRENCH AT MID-DEPTH LOCATION FOR ALL PIPES AND SERVICES. THE TRACER TAPE SHALL READ "SEWER" AND BE DETECTABLE BY A STANDARD METAL DETECTOR.
- ALL PROPOSED SANITARY SEWER SIDE CONNECTIONS SHALL BE COMPLETED IN CONFORMANCE TO THE CITY OF SPOKANE STANDARD PLAN Z-116.

SANITARY SEWER MATERIALS

THE FOLLOWING STANDARD MATERIALS SHALL BE USED FOR GRAVITY SANITARY SEWER CONSTRUCTION AND SHALL CONFORM TO THE LATEST EDITION OF AMERICAN SOCIETY OF TESTING MATERIALS STANDARDS AND CITY OF SPOKANE GENERAL PROVISIONS FOR PRIVATE CONTRACTS:

MATERIAL	SPECIFICATION	DIAMETER
PVC PIPE	ASTM D3034-SDR 35	4"-15"
PVC PIPE	ASTM F789-PS 46	18"-36"
PVC PIPE FITTINGS	ASTM D3212 & F477	ALL

WATER NOTES:

- CONTRACTOR SHALL EXPOSE EXISTING WATER LINES TO VERIFY EXISTING ELEVATION AND LOCATION PRIOR TO START OF CONSTRUCTION.
- PROPOSED WATERLINES SHALL BE LAID AT A MINIMUM OF 5.50 FEET TO INVERT FROM PROPOSED FINISHED GRADE. PIPE DEPTH SHALL BE MAINTAINED THROUGH PROPOSED AND EXISTING 208 SWALE SYSTEMS.
- PIPE JOINTS WILL BE EITHER MECHANICAL JOINT OR TYTON SLIP JOINT. CHANGE IN DIRECTION SHALL NOT EXCEED 75 PERCENT OF MANUFACTURER'S MAXIMUM DEFLECTION STANDARDS.
- ALL PROPOSED WATERLINES SHALL BE BEDDED WITH A MINIMUM OF 6" OF SAND BELOW THE PROPOSED PIPE AND 12" OF SAND ABOVE THE PROPOSED PIPE.
- WHERE A PROPOSED WATERLINE ENTERS A BUILDING A DEPTH OF 5 FEET SHALL BE MAINTAINED. WHERE THERE IS NO BASEMENT I.E., CRAWL SPACE OR SLAB FLOOR, THE WATERLINE, INCLUDING FIRE LINES WILL MAINTAIN A 5 FOOT BURY AND EXTEND 2 FEET INSIDE THE FOOTING BEFORE RISING TO THE POINT OF USE OR METER.
- ALL PROPOSED UNDERGROUND FIRE LINES, OR FIRE SUPPRESSION SYSTEMS THAT ARE SEPARATED OR PROTECTED FROM POTABLE WATER SYSTEMS REQUIRES A STATE LEVEL III OR "U" LICENSED CONTRACTOR FOR INSTALLATION.
- ALL HYDRANTS SHALL BE PROPERLY RESTRAINED FROM THE MAIN TO THE HYDRANT (MEGA LUGS OR FIELD LOCK GASKETS)
- ALL TEES, PLUGS, CAPS, AND BENDS (HORIZONTAL AND VERTICAL) ON WATERLINES INSTALLED UNDERGROUND SHALL BE MECHANICALLY RESTRAINED. MEGA LUGS OR FIELD LOCK GASKETS OR OTHER RESTRAINT SYSTEMS APPROVED BY THE DIRECTOR OF THE CITY OF SPOKANE WATER DEPARTMENT SHALL BE USED. THRUST BLOCKING IS NOT ACCEPTABLE.
- PROPOSED WATERLINES AND APPURTENANCES 3" AND LARGER SHALL BE TESTED IN SECTION OF CONVENIENT LENGTH UNDER A HYDROSTATIC PRESSURE EQUAL TO 1.50 TIMES THAT UNDER WHICH THEY WILL OPERATE OR IN NO CASE SHALL THE TEST PRESSURE BE LESS THAN 175 PSI. FIRE LINES WILL BE TESTED AT 200 PSI OR 1.50 TIMES THE OPERATION PRESSURE, WHICHEVER IS GREATER. CONTRACTOR MUST PROVIDE RESTRAINED MJ CAP ON LAST PIPE (OR PLUG ON LAST NEW VALVE) NEAR CONNECTION POINT WITH EXISTING WATER SERVICE AND INCLUDE 2" THREADED PORT WITH BALL VALVE/CURB STOP ASSEMBLY FOR TESTING. CHLORINATION SHALL ONLY BE DONE BY CITY FORCES AT THE EXPENSE OF THE DEVELOPER AND ALL ARRANGEMENTS SHALL BE MADE THROUGH THE CITY OF SPOKANE WATER SERVICE INSPECTORS.
- ANY UNUSED WATER SERVICES MUST BE KILLED AT THE PUBLIC WATER MAIN AT THE DEVELOPER'S EXPENSE.
- A 2" ELECTRICAL CONDUIT IS REQUIRED FROM THE PROPOSED WATER VAULT TO A PROPOSED BUILDING STRUCTURE FOR A METER READING DEVICE.
- ALL PROPOSED CONNECTIONS MADE TO THE PUBLIC WATER MAIN ARE PERFORMED BY THE CITY OF SPOKANE. THE CONTRACTOR IS RESPONSIBLE FOR ALL FEES, TRENCHING, BACKFILL, AND SURFACE RESTORATION.
- PROPOSED DUCTILE IRON WATERLINES SHALL MEET THE SPECIFICATIONS SHOWN HEREON. PROPOSED DUCTILE IRON WATERLINES SHALL HAVE A CEMENT MORTAR LINING MEETING THE REQUIREMENTS OF AWWA C104.
- ALL PROPOSED GATE VALVES SHALL COMPLY WITH AWWA C509 OR C515 (LATEST REVISIONS), CLASS 150. RESILIENT SEATED GATE VALVES SHALL BE NON-RISING STEM TYPE SUITABLE FOR DIRECT BURIAL. SHAFT SEALS SHALL BE STANDARD "O" RING SEALS.
- ALL PROPOSED FIRE HYDRANTS SHALL CONFORM TO AWWA C502-94 (LATEST REVISION) AND THE STANDARD FOR DRY-BARREL FIRE HYDRANTS.
- THE CONTRACTOR SHALL INSTALL TRACER TAPE IN THE EXCAVATION TRENCH AT MID-DEPTH LOCATION FOR ALL PIPES AND SERVICES. THE TRACER TAPE SHALL READ "WATER" AND BE DETECTABLE BY A STANDARD METAL DETECTOR.
- ALL PROPOSED WATERLINE INSTALLATIONS AND MATERIALS, INCLUDING FIRE HYDRANTS AND VALVES, MUST CONFORM TO THE CITY OF SPOKANE STANDARD SPECIFICATIONS (LATEST EDITION), CITY OF SPOKANE WATER DEPARTMENT RULES AND REGULATIONS FOR WATER SERVICE INSTALLATIONS" (LATEST EDITION) AND THE CITY OF SPOKANE GENERAL SPECIAL PROVISIONS FOR PRIVATE CONTRACTS (LATEST EDITION). PIPE AND FITTINGS SHALL BE APPROVED DUCTILE IRON. ALL FIRE HYDRANTS MUST BE INDIVIDUALLY VALVED. WHEN IN FIRE DISTRICTS OUTSIDE OF CITY OF SPOKANE JURISDICTION, INSTALLATION, AND MATERIALS WILL CONFORM CITY OF SPOKANE WATER DEPARTMENT RULES AND REGULATIONS.
- WATER SERVICES SHALL MEET CURRENT BACKFLOW STANDARDS PER WAC-246-290-490 AND FOLLOW CITY OF SPOKANE WATER DEPARTMENT RULES AND REGULATIONS FOR WATER SERVICE INSTALLATIONS.
- PIPES SHALL BE CLEAN INSIDE WHEN INSTALLED AND OPEN ENDS SHALL BE PROTECTED WHEN WORK IS STOPPED, TO PREVENT FOREIGN MATERIAL FROM ENTERING PIPE.
- EARTH SHALL BE WELL TAMPED (PER STD. PLAN A-1 AND A-2) UNDER AND AROUND PIPES TO PREVENT SETTLING OR LATERAL MOVEMENT. CARE SHALL BE TAKEN TO PREVENT ROCKS, ETC. FROM DAMAGING PIPE WHILE BACKFILLING. FROZEN EARTH AND/OR ASPHALT SHALL NOT BE USED FOR BACKFILL MATERIAL. BACKFILLING WILL BE DONE ACCORDING TO APWA SPECIFICATIONS.
- ALL APPROVED WATER MAIN EXTENSION INSTALLATIONS WILL BE REQUIRED TO EXTEND 10' BEYOND PROPERTY LINE UNLESS OTHERWISE REQUIRED BY THE WATER DEPARTMENT.
- FIRE HYDRANT USE REQUIRES A CITY OF SPOKANE ISSUED REDUCED PRESSURE BACKFLOW PREVENTER (RPBA) AND FLOW METER ASSEMBLY FOR ALL FIRE HYDRANT WATER USAGE DURING CONSTRUCTION (E.G., CONSTRUCTION PHASE DUST CONTROL, ETC.). CALL 311 OR CITY WATER DEPARTMENT AT 509-625-7800 FOR MORE INFO ON FIRE HYDRANT USE.
- ALL FIRE HYDRANTS, METER VAULT/BOX, CURB BOXES, AND VALVE BOXES MUST MAINTAIN AN UNOBSTRUCTED 3' RADIUS.
- IF A BYPASS SERVICE LINE IS INSTALLED IT MUST BE METERED, AND WITH SAME BACKFLOW PROTECTION AS THE SERVICE BEING BYPASSED.
- WHEN THE METER OR DOUBLE CHECK ARE SUSCEPTIBLE TO BEING SUBMERGED IN GROUNDWATER, THEY MUST BE INSTALLED ABOVE GRADE OR IN APPROVED WATERTIGHT VAULT.

WATER MATERIALS

THE FOLLOWING PIPE MATERIALS SHALL BE USED FOR WATER MAIN CONSTRUCTION AND SHALL CONFORM TO THE LATEST EDITION OF APPROPRIATE AMERICAN WATER WORKS ASSOCIATION STANDARDS, CITY OF SPOKANE GENERAL PROVISIONS FOR PRIVATE CONTRACTS, AND AMERICAN SOCIETY OF TESTING MATERIALS STANDARDS:

MATERIAL	SPECIFICATION	DIAMETER
PVC PIPE	AWWA C900-CLASS 200 MIN.	ALL
DUCTILE IRON PIPE	AWWA C151	3"-64"
DUCTILE IRON PIPE FITTINGS	AWWA C110 OR C153	3"-48"

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, AND ALL OTHER EXISTING FEATURES AND CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

OWNER:
BOONE EXPRESS, LLC
2040 W BOONE AVE
SPOKANE, WA 99201

PROJECT:
BROADWAY GROCERY
2229 W BROADWAY AVE, SPOKANE, WA 99201

NOTES

PROJ. MANAGER: EF

DESIGNED BY: SS

DRAWN BY: SS

CHECKED BY: EF

SCALE: AS SHOWN

DATE: 11/07/2023

REV: 3

SHEET: 08 OF 12

SHEET NUMBER

CALL 811
2 BUSINESS DAYS
BEFORE YOU DIG
(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)

11/07/2023

LEED AP

LEED ACCREDITED PROFESSIONAL & THE RELATED ARCHITECT & THE CITY OF SPOKANE ARE TRULY OWNED BY THE U.S. GREEN BUILDING COUNCIL & ARE AWARDED TO INDIVIDUALS UNDER LICENSE BY THE GREEN BUILDING CERTIFICATION INSTITUTE.

CAD FILE NUMBER: P:\CLIENTS\CIVIL\COPELAND ARCHITECTS\2229 W BROADWAY AVE SPOKANE\DWG\DRAWING\2208.0142.00_EX SITE & DEMO PLAN.DWG
LAST MODIFIED BY: ERK.FUENTES - SAVE DATE: 11/7/2023 7:41 AM - SHEET SET: BROADWAY GROCERY - ORIGINAL SHEET SIZE: ARCH FULL BLEED D (24.00 X 36.00 INCHES)
AUTOCAD VERSION: CIVIL 3D 2017

LEGEND

- WATER SHUTOFF
- FIRE HYDRANT
- GAS VALVE
- GAS METER
- YARD LIGHT
- LIGHT POST
- POWER POLE
- POWER POLE W/ UNDERGROUND
- DRYWELL
- GUTTER INLET CATCH BASIN
- SANITARY SEWER MANHOLE
- CLEAN OUT
- TELEPHONE VAULT
- TRAFFIC SIGN
- OVERHEAD POWER
- OVERHEAD UTILITY
- SAINT LINE SEWER

BASIS OF BEARINGS:

BASIS OF BEARINGS FOR THIS SURVEY IS THE MONUMENT LINE ALONG BROADWAY AVENUE PER FOUND MONUMENTS, PER ROS BK. 90, PG. 32.

VERTICAL DATUM:

VERTICAL DATUM (NAVD 88)

BASIS OF ELEVATION:

BASIS OF ELEVATION IS DERIVED FROM WSDOT MONUMENT GP 320290-186, ELEVATION IS 1986.97' AND CHECKED TO WSDOT MONUMENT GP32395-175 AT THE SOUTHWEST CORNER OF CATALDO AVENUE AND RUBY STREET ELEV. = 1889.78'

SURVEYOR'S NOTES:

- PURPOSE OF SURVEY IS FOR FUTURE SITE IMPROVEMENTS.
- SEE RECORD OF SURVEY FOR BOUNDARY AND CONTROL.
- UTILITIES SHOWN ARE FROM FIELD LOCATED, ABOVE GROUND EVIDENCE, UNLESS NOTED OTHERWISE. OTHER UTILITIES MAY EXIST AND NEED FURTHER INVESTIGATION.

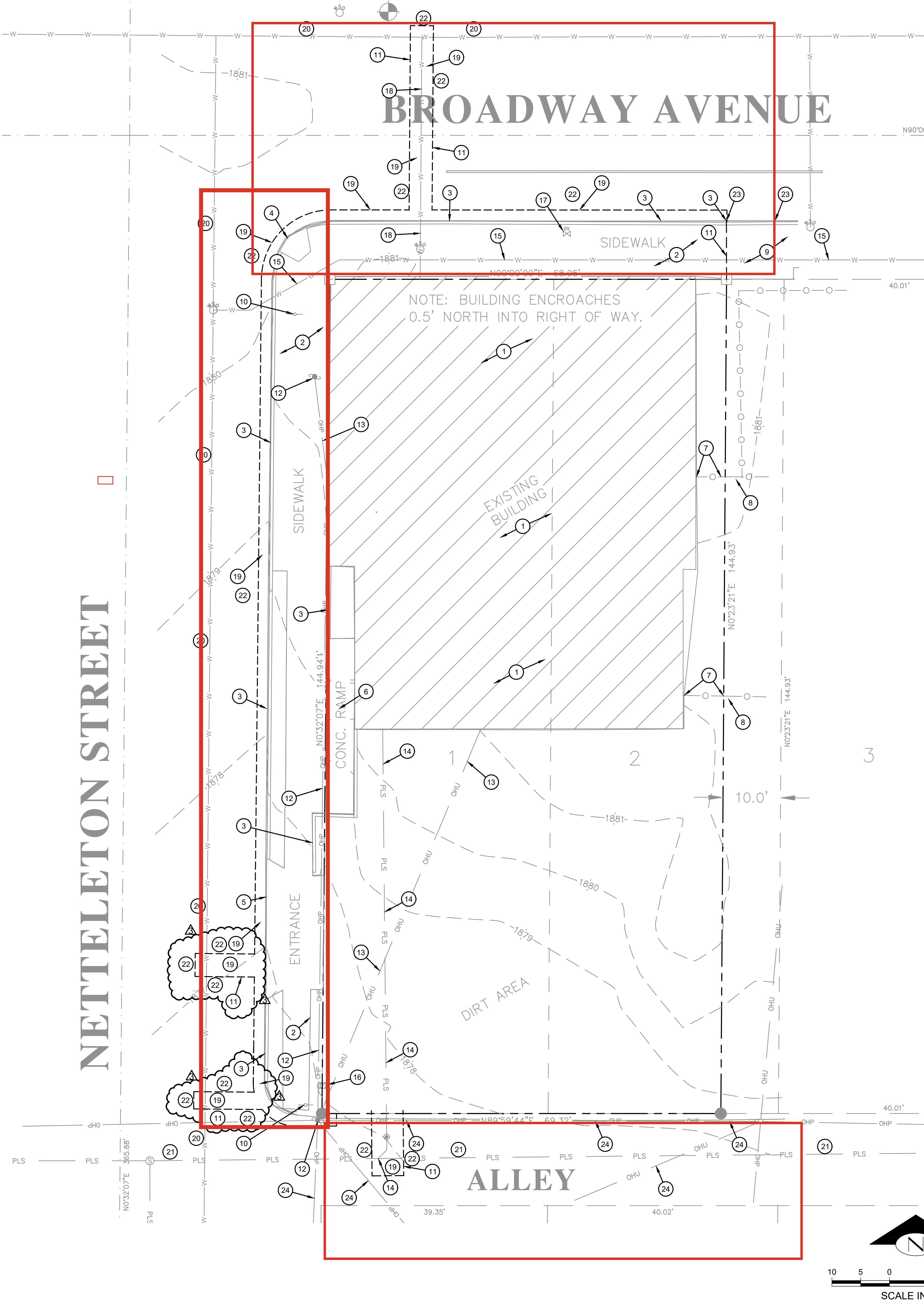
LEGEND:

- SET #4: REBAR WITH ORANGE PLASTIC CAP MARKED "DALE PLS 29270" (TYPICAL)
- SET CHISELED "+" FOR PROPERTY CORNERS

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, AND ALL OTHER EXISTING FEATURES AND CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

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(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)



KEY NOTES:

KEY	NOTE:	DETAIL/ SHEET
1	REMOVE EXISTING BUILDING.	-
2	REMOVE EXISTING CONCRETE SIDEWALK.	-
3	REMOVE EXISTING CONCRETE CURB.	-
4	REMOVE EXISTING CONCRETE CURB RAMP.	-
5	REMOVE EXISTING CONCRETE DRIVEWAY APPROACH.	-
6	REMOVE EXISTING CONCRETE RAMP AND LANDING.	-
7	REMOVE EXISTING CHAIN LINK FENCE TO THE PROPERTY LINE.	-
8	EXISTING CHAIN LINK FENCE TO REMAIN ON ADJACENT PROPERTY. PROTECT IN PLACE.	-
9	EXISTING CONCRETE SIDEWALK TO REMAIN. PROTECT IN PLACE.	-
10	REMOVE EXISTING SIGN. SEE SHEET C12, SIGNAGE AND STRIPING PLAN, FOR REPLACEMENT.	-
11	SAWCUT EXISTING HARDSCAPE SURFACE TO NEAT VERTICAL EDGE.	-
12	COORDINATE WITH UTILITY PURVEYOR TO RELOCATE EXISTING POWER POLE AND ASSOCIATED OVERHEAD POWER LINES.	-
13	REMOVE EXISTING OVERHEAD POWER LINES.	-
14	REMOVE EXISTING SANITARY SEWER STUB. CONTRACTOR SHALL REMOVE SANITARY SEWER STUB BACK TO THE SEWER MAIN.	-
15	EXISTING DOMESTIC WATER SERVICE TO REMAIN. PROTECT IN PLACE.	-
16	COORDINATE WITH UTILITY PURVEYOR TO REMOVE AND REPLACE UTILITY VAULT.	-
17	EXISTING NATURAL GAS SERVICE TO REMAIN. CONTRACTOR SHALL VERIFY EXISTING NATURAL GAS SIZE IS ADEQUATELY SIZED FOR PROPOSED IMPROVEMENTS.	-
18	REMOVE EXISTING DOMESTIC WATER SERVICE SERVING PROJECT SITE. CONTRACTOR SHALL KILL EXISTING WATER SERVICE AT THE EXISTING 6" CAST IRON WATER MAIN. EXCAVATION AND RESTORATION SHALL BE COMPLETED BY CONTRACTOR. EXISTING WATER SERVICE SHALL BE KILLED BY WATER DEPARTMENT. ALL WORK TO BE PAID FOR BY DEVELOPER/OWNER.	-
19	REMOVE EXISTING ASPHALT PAVEMENT.	-
20	EXISTING WATER MAIN TO REMAIN. PROTECT IN PLACE.	-
21	EXISTING SANITARY SEWER MAIN TO REMAIN. PROTECT IN PLACE.	-
22	EXISTING ASPHALT TO REMAIN. PROTECT IN PLACE.	-
23	EXISTING CONCRETE CURB TO REMAIN. PROTECT IN PLACE.	-
24	EXISTING OVERHEAD POWER LINE TO REMAIN. PROTECT IN PLACE.	-

GENERAL NOTES:

- ALL CLEARING, GRUBBING, SITE PREPARATION, OVER EXCAVATION, EARTHWORK, ENGINEERED FILL, AND MATERIAL TESTING SHALL BE DONE IN COMPLIANCE WITH THE GEOTECHNICAL ENGINEERING REPORT.
- ALL EXISTING IMPROVEMENTS SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
- EXISTING WATER LINES AND SANITARY SEWER LINES ARE SHOWN IN THEIR APPROXIMATE LOCATION. LOCATION WAS DETERMINED USING GIS INFORMATION FROM THE CITY OF SPOKANE.
- ANY AND ALL UNUSED WATER AND SEWER SERVICES SHALL BE KILLED AT THE MAIN.
- ALL STREET DEMOLITION AND PATCHING WITHIN THE ROW SHALL BE DONE IN CONFORMANCE WITH THE INLAND NORTHWEST PAVEMENT CUT POLICY (LATEST EDITION).
- ALL SIGNAGE, STRIPING, AND TRAFFIC SIGNS LOCATED WITHIN THE ROW SHALL BE COMPLETED/ INSTALLED PER CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS.
- ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND TO BE BROKEN, HEAVED, SUNKEN, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION. IF YOU WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509) 625-6300 TO ARRANGE A SITE VISIT.

REVISION	BY	DATE	NO.	1	2	3	4	5	6	7	8
CITY PERMIT COMMENTS #1	SS	08/25/2023	1								
CITY PERMIT COMMENTS #2	EF	09/22/2023	2								
CITY PERMIT COMMENTS #3	EF	11/07/2023	3								

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601 W Main Ave
Suite 617
Spokane, WA 99201
P: 509.600.3600
F: 206.523.1012
www.dcgengr.com

DCG
civil structural

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STATE OF WASHINGTON
ERK FUENTES
REGISTERED PROFESSIONAL ENGINEER
56555
11/07/2023

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OWNER:
BOONE EXPRESS, LLC
2040 W BOONE AVE
SPOKANE, WA 99201

PROJECT:
BROADWAY GROCERY
2229 W BROADWAY AVE, SPOKANE, WA 99201
EXISTING CONDITIONS AND DEMOLITION PLAN

PROJ. MANAGER: EF
DESIGNED BY: SS
DRAWN BY: SS
CHECKED BY: EF
SCALE: AS SHOWN
DATE: 11/07/2023
REV. 3
SHEET 1 OF 12

SHEET NUMBER
C03

CAD FILE NUMBER: P:\CLIENTS\CIVIL\COPELAND ARCHITECTS\2229 W BROADWAY AVE SPOKANE\DWG\DRAWING\2208.01\22.00 GRADING PLAN.DWG
LAST MODIFIED BY: ERK.FUENTES - SAVE DATE: 11/7/2023 7:29 AM - SHEET SET: BROADWAY GROCERY - ORIGINAL SHEET SIZE: ARCH FULL BLEED D (24.00 X 36.00 INCHES)
AUTOCAD VERSION: CIVIL 3D 2017

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, AND ALL OTHER EXISTING FEATURES AND CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

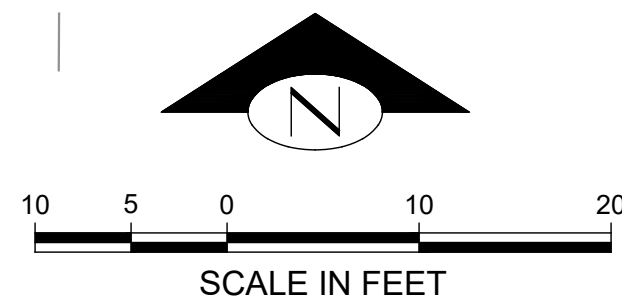
CALL 811
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(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)

NETTELETON STREET

BROADWAY AVENUE

MAIN FLOOR FFE 1879.50
BASEMENT FFE 1871.50

ALLEY



KEY NOTES:		
KEY	NOTE:	DETAIL/ SHEET
1	NEW ACCESSIBLE PARKING STALL PER CITY OF SPOKANE STANDARD PLANS G-54 AND G-80A. SLOPES WITHIN THE DESIGNATED STALL AND ACCESS AISLE SHALL NOT EXCEED 2% IN ANY DIRECTION.	4/C09
2	NEW 7' WIDE OFF-SITE CONCRETE SIDEWALK PER CITY OF SPOKANE STANDARD PLAN F-102.	8/C10
3	TAPER 6" CONCRETE CURB TO FLUSH WITH PAVEMENT OVER LAST 2'.	5/C09
4	NEW CONCRETE CURB PER CITY OF SPOKANE STANDARD PLAN F-106.	1/C09
5	NEW CONCRETE CURB DROP INLET PER CITY OF SPOKANE STANDARD PLAN F-109.	6/C10
6	NEW CONCRETE CURB RAMP TYPE 1 PER CITY OF SPOKANE STANDARD PLAN F-105.	3/C10
7	NEW CONCRETE DRIVEWAY PER CITY OF SPOKANE STANDARD PLAN F-104.	7/C10
8	NEW ASPHALT PAVEMENT.	4/C10
9	NEW CONCRETE RETAINING WALL.	2/C09
10	CONTRACTOR SHALL MATCH EXISTING ELEVATION ALONG SAWCUT LINE, TYPICAL.	-
11	TIE PROPOSED TOP OF CURB INTO EXISTING TOP OF CURB ELEVATION.	-
12	NEW ON-SITE CONCRETE SIDEWALK.	3/C09
13	NEW ASPHALT PAVEMENT. CONTRACTOR SHALL MATCH PAVEMENT DEPTH AND SECTION OF EXISTING ROADWAY.	-
14	NEW TRASH ENCLOSURE. CONTRACTOR SHALL COORDINATE WITH ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.	5/C10
15	NEW FLUSH CONCRETE CURB.	3/C11
16	NEW CONCRETE CURB WALL PER CITY OF SPOKANE STANDARD PLAN D-101B.	9/C10
17	CONTRACTOR SHALL TRANSITION FROM FLUSH CURB TO 6" REVEAL OVER THE LENGTH OF TWO PARKING STALLS.	-
18	NEW SEPARATED SIDEWALK ALLEY RETURN PER CITY OF SPOKANE STANDARD PLAN W-105.	1/C11
19	TRASH ENCLOSURE GATES SHALL REMAIN CLOSED AT ALL TIMES KEEPING THE DRIVE AISLE AND ADJACENT PARKING STALL UNOBSTRUCTED.	-
20	CONCRETE CURB RAMP - TYPE 3 PER CITY OF SPOKANE STANDARD PLAN F-105D.	5/C11

GENERAL NOTES:

- SEE UTILITY PLAN, SHEET C08, FOR ADDITIONAL INFORMATION.
- SEE STORMWATER PLAN, SHEET C07, FOR ADDITIONAL INFORMATION.
- ALL CLEARING, GRUBBING, SITE PREPARATION, OVER EXCAVATION, EARTHWORK, ENGINEERED FILL, AND MATERIAL TESTING SHALL BE DONE IN COMPLIANCE WITH THE GEOTECHNICAL ENGINEERING REPORT.
- CONTRACTOR SHALL ADJUST ALL EXISTING UTILITY SURFACE STRUCTURES (LIDS, CLEANOUTS, VALVE BOXES/COVERS, GRATES, ETC.) TO MATCH PROPOSED FINISHED GRADES.
- ALL STREET DEMOLITION AND PATCHING WITHIN THE ROW SHALL BE DONE IN CONFORMANCE WITH THE INLAND NORTHWEST PAVEMENT CUT POLICY (LATEST EDITION).
- ALL SIGNAGE, STRIPING, AND TRAFFIC SIGNS LOCATED WITHIN THE ROW SHALL BE COMPLETED/ INSTALLED PER CITY OF SPOKANE STANDARD PLANS AND SPECIFICATIONS.
- ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND TO BE BROKEN, HEAVED, SUNKEN, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION. IF YOU WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509) 625-6300 TO ARRANGE A SITE VISIT.

REVISION

No.	DATE	BY	CITY PERMIT COMMENTS #1	CITY PERMIT COMMENTS #2	CITY PERMIT COMMENTS #3
1	08/25/2023	SS			
2	09/22/2023	EF			
3	11/07/2023	EF			

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ERK M. FUENTES

STATE OF WASHINGTON
REGISTERED
PROFESSIONAL ENGINEER
56555
11/07/2023

OWNER:

BOONE EXPRESS, LLC
2040 W BOONE AVE
SPOKANE, WA 99201

PROJECT:

BROADWAY GROCERY
2229 W BROADWAY AVE, SPOKANE, WA 99201
SITE PLAN

PROJ. MANAGER:

DESIGNED BY:

DRAWN BY:

CHECKED BY:

SCALE:

DATE:

REV

SHEET

SHEET NUMBER

C05

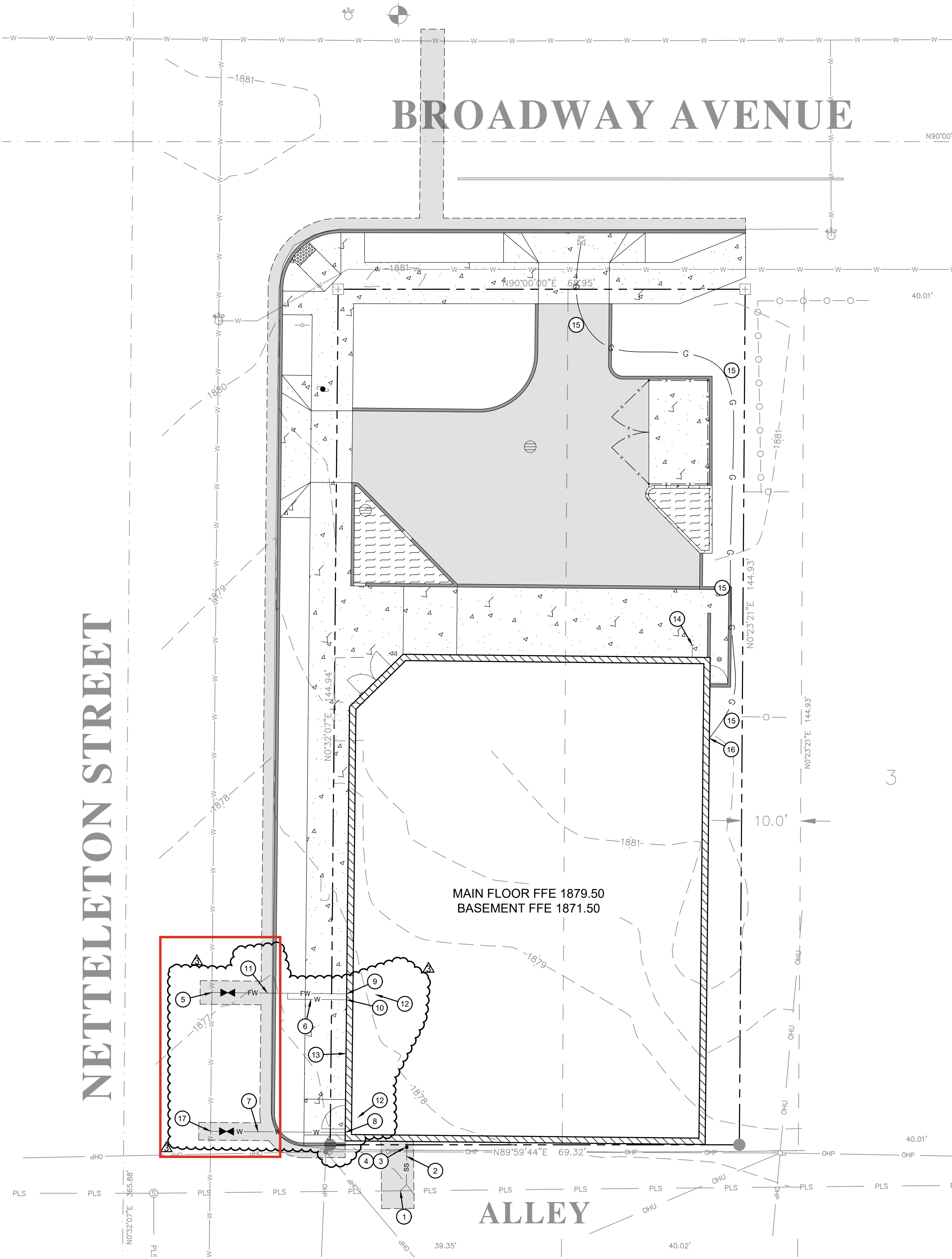
CAD FILE NUMBER: P:\CLIENTS\CIVIL\COPELAND ARCHITECTS\2229 W BROADWAY AVE SPOKANE\DWG\DRAWING\2208.01\22.00 UTILITY PLANDWG
LAST MODIFIED BY: ERK-FUENTES - SAVE DATE: 11/7/2023 7:29 AM - SHEET SET: BROADWAY GROCERY - ORIGINAL SHEET SIZE: ARCH FULL BLEED D (24.00 X 36.00 INCHES)
AUTOCAD VERSION: CIVIL 3D 2017

GENERAL NOTES:

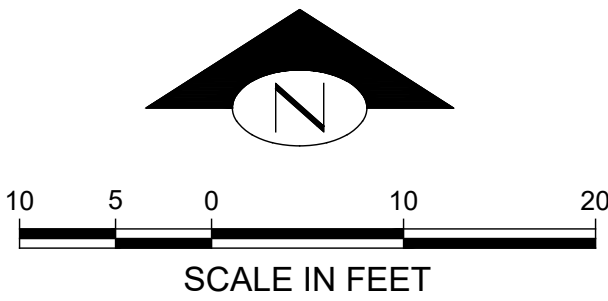
- FOR PIPE MATERIALS AND ADDITIONAL UTILITY NOTES, SEE SHEET C02.
- FOR FIRE PROTECTION CONTRACTOR REQUIREMENT INFORMATION AND DESIGNATION, SEE SHEET C02.
- FOR TRENCH SECTION, BACKFILL, AND SURFACE REPLACEMENT SEE CITY OF SPOKANE STANDARD PLANS A-1, A-2, AND A-3.
- PRIOR TO CONSTRUCTION, CONTRACTOR SHALL VERIFY ALL INVERTS OF EXISTING UTILITIES AT POINTS OF CONNECTION AND PROPOSED UTILITY CROSSINGS BY OBSERVATION OR POTHOLING METHODS. NOTIFY CIVIL ENGINEER OF ANY CONFLICTS OR DISCREPANCIES IN THESE PLANS AND ACTUAL FIELD INFORMATION.
- CONTRACTOR SHALL COORDINATE WITH AVISTA UTILITIES AND ELECTRICAL ENGINEERING PLANS FOR SITE ELECTRICAL AND POWER POLE RELOCATION.
- CONTRACTOR SHALL COORDINATE WITH AVISTA UTILITIES FOR NATURAL GAS INFORMATION.
- CONTRACTOR SHALL COORDINATE WITH LANDSCAPE PLANS FOR POINT OF CONNECTION, IRRIGATION LOCATIONS, SLEEVING, ETC.

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, AND ALL OTHER EXISTING FEATURES AND CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

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(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)



KEY NOTES:		
KEY	NOTE:	DETAIL/ SHEET
①	TIE NEW 6" PVC SANITARY SEWER INTO EXISTING 8" SANITARY SEWER MAIN PER CITY OF SPOKANE STANDARD PLAN Z-116. EX 8" IE 1861.13± 6" IE 1861.20	2/C11
②	9 LF± 6" PVC SANITARY SEWER PIPE @ 2.00% MIN.	-
③	APPROXIMATE 6" SANITARY SEWER STUB OUT FROM BUILDING. CONTRACTOR SHALL COORDINATE EXACT LOCATION WITH PLUMBING PLAN SHEET P1.01.	-
④	6" SSCO RIM 1877.40± 6" IE 1867.50	6/C09
⑤	TIE NEW 4" FIRE WATER SERVICE INTO EXISTING 6" CAST IRON WATER MAIN. CONNECTION TO PUBLIC WATER MAIN SHALL BE PERFORMED BY CITY FORCES, PAID FOR BY THE DEVELOPER/OWNER/CONTRACTOR WHICH ALSO INCLUDES ALL EXCAVATION, BACKFILL, AND SURFACE RESTORATION, BY THE CONTRACTOR. ALL EXCAVATIONS WILL BE SLOPED, SHORED, OR BENCHED ACCORDING TO DOSH DEPARTMENT OF LABOR AND INDUSTRY STANDARDS TO PROVIDE SAFE ACCESS PER WAC 296 155 PART N.	-
⑥	NEW 1" SEAMLESS SOFT-ANNEALED TYPE "K" COPPER IRRIGATION PIPE	-
⑦	NEW 2" SEAMLESS SOFT-ANNEALED TYPE "K" COPPER DOMESTIC WATER PIPE.	-
⑧	2" DOMESTIC WATER BUILDING STUB OUT LOCATION. CONTRACTOR SHALL COORDINATE EXACT LOCATION WITH PLUMBING PLAN SHEET P1.01.	-
⑨	APPROXIMATE 4" FIRE WATER BUILDING STUB OUT LOCATION. CONTRACTOR SHALL COORDINATE EXACT LOCATION WITH PLUMBING PLAN SHEET P1.01.	-
⑩	APPROXIMATE 1" IRRIGATION SERVICE BUILDING STUB OUT LOCATION. CONTRACTOR SHALL COORDINATE EXACT LOCATION WITH PLUMBING PLAN SHEET P1.01.	-
⑪	NEW 4" DUCTILE IRON WATER PIPE TO BE USED FOR FIRE WATER.	-
⑫	PROPOSED BUILDING IS LOCATED WITHIN 20' OF PROPERTY LINE. ALL METERS, BACKFLOW DEVICES, AND MISC. APPURTENANCES SHALL BE LOCATED INSIDE BUILDING FOR NEW DOMESTIC, FIRE, AND IRRIGATION SERVICES.	-
⑬	CONTRACTOR SHALL PROVIDE NEW WALL MOUNTED FIRE DEPARTMENT CONNECTION (FDC) AND POST INDICATOR VALVE (PIV). WALL MOUNTED PIV SHALL BE LOCATED DOWNSTREAM OF DOUBLE CHECK DETECTOR VALVE ASSEMBLY.	-
⑭	1" HDPE IRRIGATION SERVICE POINT OF CONNECTION. CONTRACTOR SHALL COORDINATE EXACT POINT OF CONNECTION WITH LANDSCAPE PLANS AND PLUMBING PLANS.	-
⑮	NEW NATURAL GAS SERVICE. NATURAL GAS TRENCH SHALL HAVE 12" OF SAND AT BOTTOM OF TRENCH. TRENCH SHALL BE BACKFILLED WITH SAND. CONTRACTOR SHALL COORDINATE WITH MECHANICAL PLAN SHEET M1.02 AND AVISTA FOR SERVICE SIZING.	-
⑯	APPROXIMATE NATURAL GAS METER LOCATION. CONTRACTOR SHALL VERIFY METER LOCATION WITH AVISTA AND MECHANICAL PLAN SHEET M1.02.	-
⑰	TIE NEW 2" DOMESTIC WATER SERVICE INTO EXISTING 6" CAST IRON WATER MAIN. CONNECTION TO PUBLIC WATER MAIN SHALL BE PERFORMED BY CITY FORCES, PAID FOR BY THE DEVELOPER/OWNER/CONTRACTOR WHICH ALSO INCLUDES ALL EXCAVATION, BACKFILL, AND SURFACE RESTORATION, BY THE CONTRACTOR. ALL EXCAVATIONS WILL BE SLOPED, SHORED, OR BENCHED ACCORDING TO DOSH DEPARTMENT OF LABOR AND INDUSTRY STANDARDS TO PROVIDE SAFE ACCESS PER WAC 296 155 PART N.	-



REVISION

NO.	DATE	BY	
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CAD FILE NUMBER: P:\CLIENTS\CIVIL\COPELAND ARCHITECTS\2229 W BROADWAY AVE SPOKANE\DWG\DRAWING\2208.01\22.00 SIGNAGE & STRIPING PLAN.DWG
LAST MODIFIED BY: ERK.FUENTES - SAVE DATE: 11/7/2023 6:49 AM - SHEET SET: BROADWAY GROCERY - ORIGINAL SHEET SIZE: ARCH FULL BLEED D (24.00 X 36.00 INCHES)
AUTOCAD VERSION: CIVIL 3D 2017

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, AND ALL OTHER EXISTING FEATURES AND CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

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BEFORE YOU DIG
(UNDERGROUND UTILITY LOCATIONS ARE APPROX.)

NETTELETON STREET

BROADWAY AVENUE

MAIN FLOOR FFE 1879.50
BASEMENT FFE 1871.50

ALLEY

KEY NOTES:		
KEY	NOTE:	DETAIL/ SHEET
1	REMOVE PORTION OF EXISTING PAVEMENT MARKING FROM WEST EXTENT TO 5' EAST OF NEW DRIVEWAY OPENING. PAVEMENT MARKINGS SHALL BE REMOVED BY WATER BLASTING PER CITY OF SPOKANE STANDARD SPECIFICATIONS.	-
2	EXISTING PAVEMENT MARKING TO REMAIN. PROTECT IN PLACE.	-
3	REMOVE EXISTING SIGN AND INSTALL NEW SIGNAGE AND SIGN POST PER CITY OF SPOKANE STANDARD PLANS. "NETTLETON" (MUTCD D3-1), FACING EAST AND WEST "700N", FACING EAST AND WEST "BROADWAY" (MUTCD D3-1), FACING NORTH AND SOUTH "2200W", FACING NORTH AND SOUTH "STOP" (MUTCD R1-1), FACING SOUTH	-
4	NEW SIGN AND SIGN POST PER CITY OF SPOKANE STANDARD PLANS "NO PARKING ANY TIME" (MUTCD R7-1A), FACING WEST	-
5	NEW CUSTOM SIGN ("ENTER ONLY") AND SIGN POST PER CITY OF SPOKANE STANDARD PLANS, FACING WEST	-
6	NEW CUSTOM SIGN ("EXIT ONLY") AND SIGN POST PER CITY OF SPOKANE STANDARD PLANS, FACING NORTH	-
7	PREFORMED THERMOPLASTIC DIRECTIONAL ARROW.	-
8	INSTALL THERMOPLASTIC END CAP PER CITY OF SPOKANE STANDARD PLAN G-60.	-
9	NEW SIGN AND SIGN POST PER CITY OF SPOKANE STANDARD PLANS "NO PARKING BEYOND THIS POINT" (MUTCD R8-3), FACING WEST	-

- GENERAL NOTES:**
- TRAFFIC SIGNPOST LOCATIONS SHALL BE FIELD-LOCATED BY THE CITY OF SPOKANE, PRIOR TO INSTALLATION. THE CONTRACTOR SHALL CONTACT THE CITY OF SPOKANE TRAFFIC SIGNS AND MARKERS SUPERVISOR AT 509-232-8800, AT LEAST FIVE CITY WORKDAYS PRIOR TO INSTALLATION TO ARRANGE FOR A CITY REPRESENTATIVE TO FIELD-LOCATE THE SIGNPOSTS.
 - FOR ALL TRAFFIC CONTROL SIGNS WITHIN THE RIGHT-OF-WAY, A SPECIFICATION DRAWING SHALL BE SUBMITTED FOR APPROVAL PRIOR TO MANUFACTURE. COMPLETED SIGNS SHALL BE PRESENTED FOR INSPECTION, PRIOR TO INSTALLATION, TO THE CITY OF SPOKANE TRAFFIC SIGNS AND MARKERS SUPERVISOR AT 901 N NELSON STREET. AN APPOINTMENT IS REQUIRED. THIS INCLUDES ALL SIGNS ATTACHED TO SIGNALS, PEDESTRIAN HYBRID BEACONS AND RECTANGULAR RAPID FLASH BEACONS.
 - ALL SIGNAGE AND STRIPING WITHIN THE RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF SPOKANE STANDARDS AND SPECIFICATIONS.
 - TRAFFIC SIGNS REMOVED DURING ANY PHASE OF THE CONSTRUCTION PROCESS ARE THE CONTRACTOR'S PROPERTY AND ARE TO BE DISPOSED OF BY THE CONTRACTOR. THESE SIGNS SHALL NOT BE REUSED.
 - PAVEMENT MARKINGS SHALL BE REMOVED BY WATER-BLASTING PER CITY OF SPOKANE STANDARD SPECIFICATIONS. PAINTING OVER EXISTING MARKINGS IS NOT ALLOWED.
 - ANY DAMAGE TO EXISTING MARKINGS DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR.



REVISION

NO.	DATE	BY	SS	EF	CITY PERMIT COMMENTS #1	CITY PERMIT COMMENTS #2	CITY PERMIT COMMENTS #3
1	08/25/2023	SS					
2	09/22/2023	EF					
3	11/07/2023	EF					

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LEED AP

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ERK.FUENTES

STATE OF WASHINGTON
REGISTERED
PROFESSIONAL ENGINEER
56555
11/07/2023

BASE MAP/TOPOGRAPHY PROVIDED BY OTHERS. DCG CANNOT BE HELD LIABLE FOR ACCURACY. CONTRACTOR SHALL FIELD VERIFY GRADES, UTILITIES, & ALL OTHER EX FEATURES & CONDITIONS. IF CONDITIONS ARE NOT AS SHOWN AND/OR PLANS CANNOT BE CONSTRUCTED AS SHOWN, CONTACT DCG PRIOR TO CONSTRUCTION.

OWNER:
BOONE EXPRESS, LLC
2040 W BOONE AVE
SPOKANE, WA 99201

PROJECT:
BROADWAY GROCERY
2229 W BROADWAY AVE, SPOKANE, WA 99201
SIGNAGE & STRIPING PLAN

PROJ. MANAGER: EF

DESIGNED BY: SS

DRAWN BY: SS

CHECKED BY: EF

SCALE: AS SHOWN

DATE: 11/07/2023

REV: 3

SHEET: 12 OF 12

SHEET NUMBER

C12

Broadway Foods

2229 West Broadway Ave, Spokane, WA 99201

The Landscape-Irrigation Contractor is to provide the following:

Lawn areas are to be sodded.

Plastic Lawn edging at planting edges.

Install all Plant Material as listed (with all tree permits required)

Finish grading, hand raking and provide/Install topsoil 2" thick in all landscape areas.

Install 2"-3" thick of Basalt Chip Rock Mulch in all planting areas, except for the (6) street trees.

Provide a 1-year warranty of all landscape and irrigation items from the time of acceptance of work.

PLANT LIST

Street Trees

5 STREET TREES – Class I Parrotia persica (2" cal.)

1 Street Trees – Class II Forest Green Oak (2" cal.)

TREES

2 EWRC 'Excelsior' Western Red Cedar (6' high) 3 ARM 'Armstrong' Red Maple (2" cal.)

1 FLC Flowering Crab Apple 'Prairie Fire' (2" cal.) 1 BAC Blue Atlas Cedar (6' high)

Shrubs

10 EGA Emerald Green Arborvitae (4' high)

12 SL Shipka Laurel (2 gal.)

3 GBS Globe Blue Spruce (2 gal.)

3 CHY Columnar Hicks Yew (2 gal.)

12 OLL Otto Luyken Laurel (2 gal.)

3 CBB Compact Burning Bush (2 gal.)

1 CR Carpet Rose (2 gal.)

Grasses / Perennials

2 SDL Stella d Oro Daylily (1 gal.)

3 JBG Japanese Blood Grass (1 gal.)

5 BES Black Eyed Susan (1 gal.)

2 MNS May Night Salvia (1 gal.)

19 SG Switch Grass 'Summer Sunrise' (1 gal.)

2 SD Shasta Daisy (1 gal.)

11 KFG Karl Foerster Grass (1 gal.)

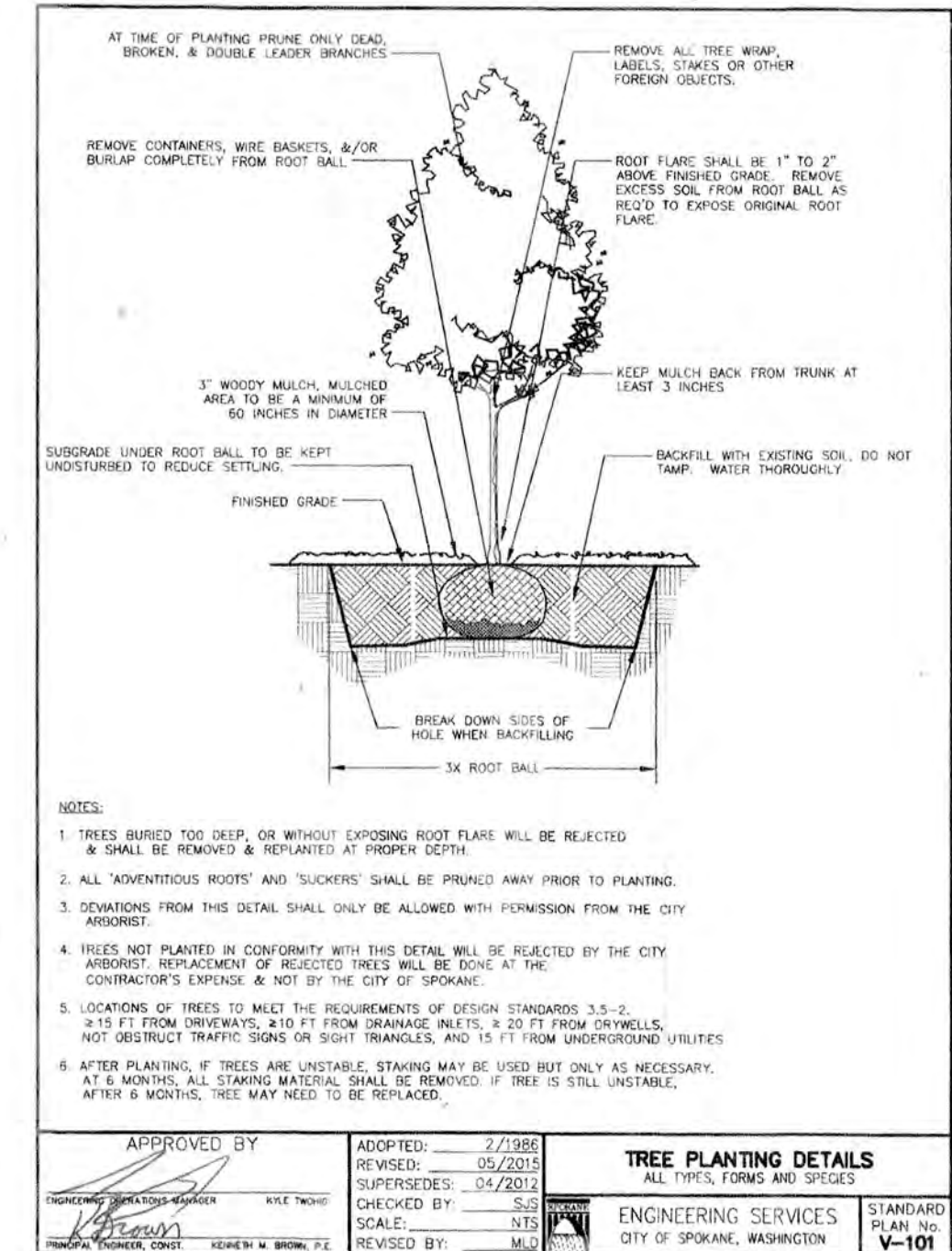
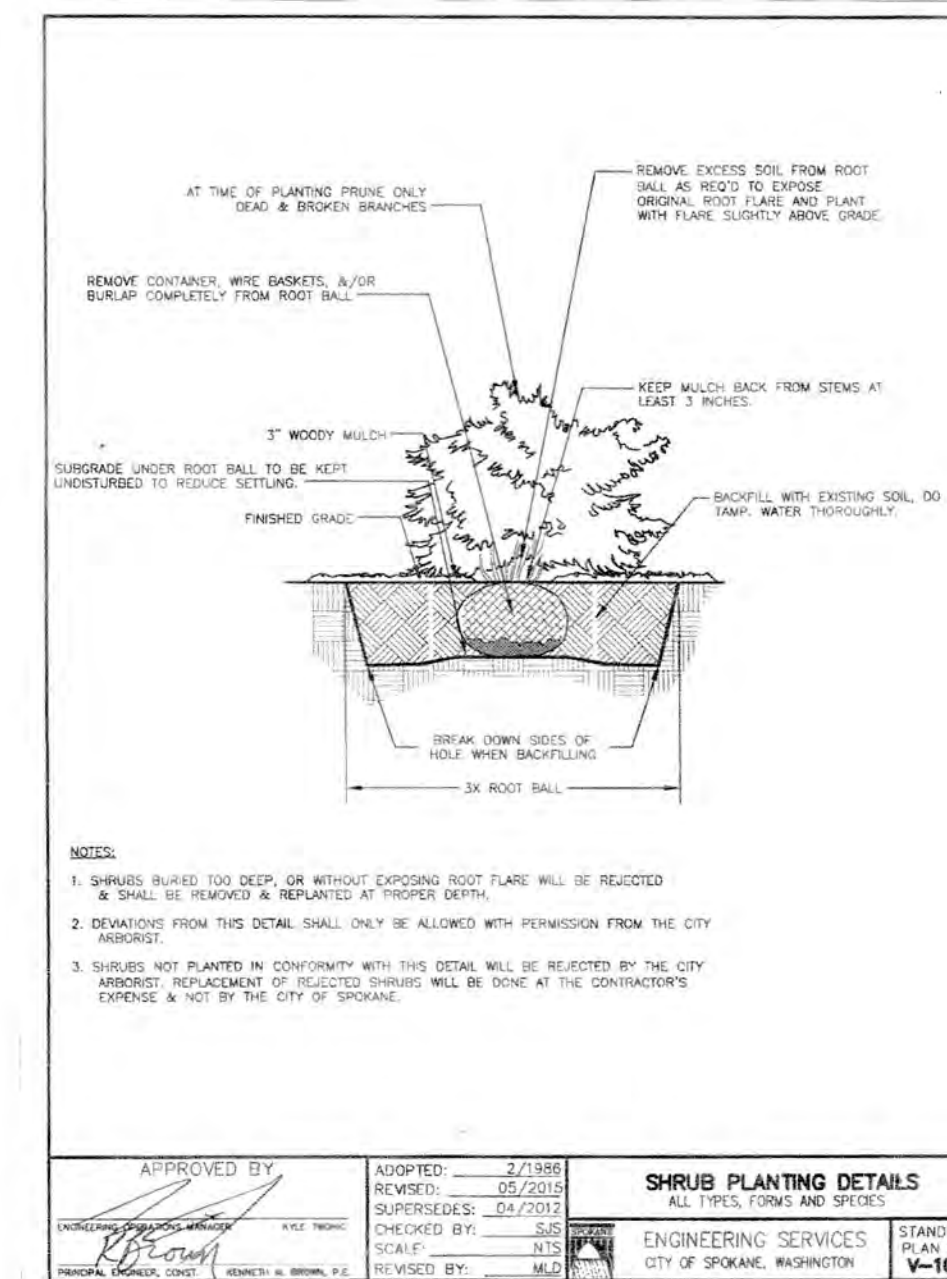
Irrigation Notes

Provide a complete, 100% irrigation coverage to all landscape areas.

Automatic controller, 1" double check assembly (with permit and inspection).

Automatic valves, mainline, sleeving as needed, 1" poly lateral lines.

1 year warranty on the irrigation system.



NOTE

The contractor shall plant all trees and shrubs on site according to detail V-101 and V-102.

Any substitutions of public/street trees and plant list must have written approval from Urban Forestry and City Planning and Landscape Architect, prior to installation.

Rock mulch at base of trees to be pulled away from the base of the tree 3" to 6", so that the root flares are visible.

Planting Notes

("do not install plants until all irrigation is installed and tested to ensure it is functioning properly")

Trees Staking – install (2) wood stakes and plastic tree tie per tree.

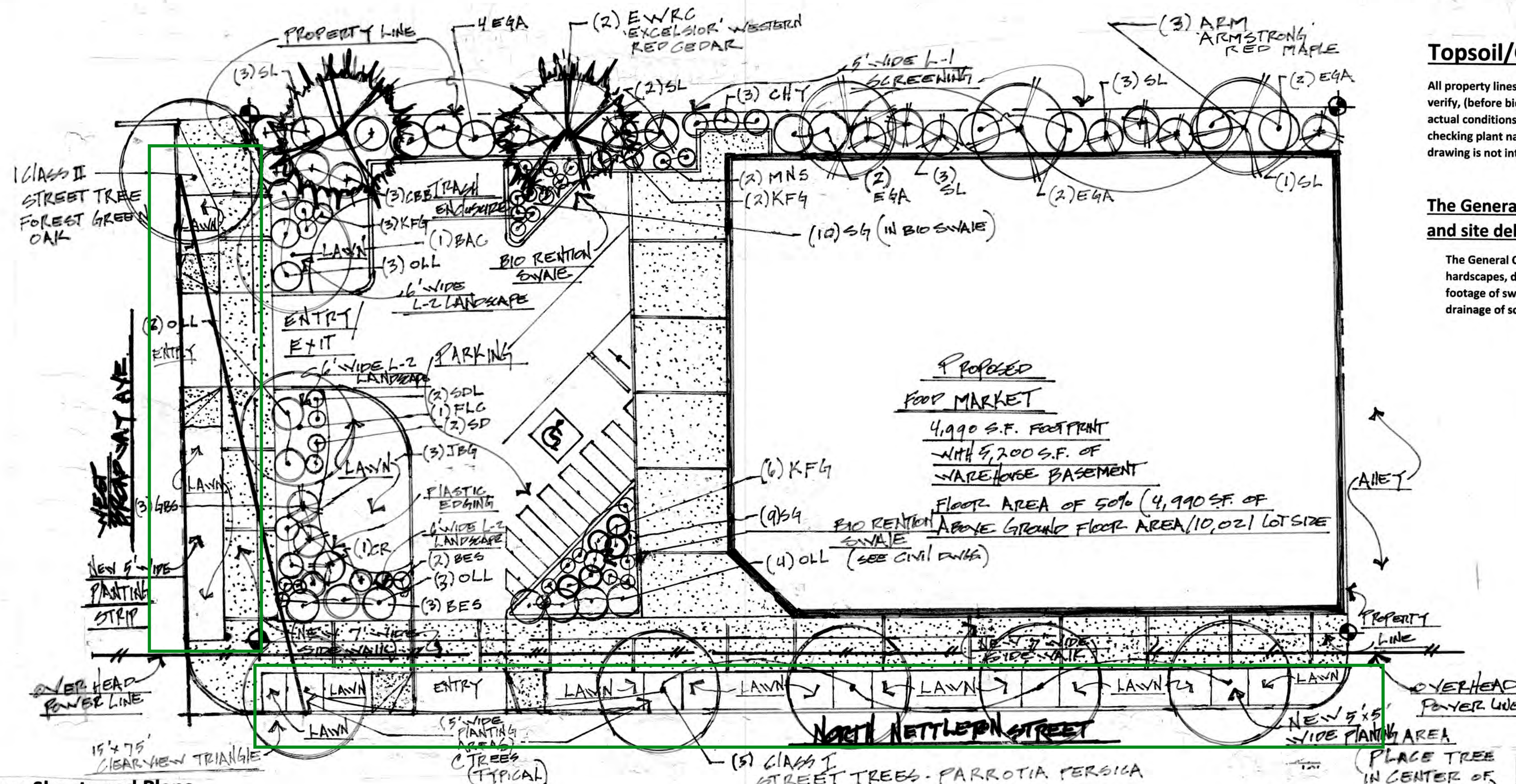
Soak all plants immediately upon planting. Provide a 1-year warranty/replace/ment of all plant material.

Topsoil/Grading/Landscape Notes

All property lines were established from information provided by the owner/architect/engineer. Check and verify, (before bidding). This drawing may need to be adjusted to meet actual property line, boundaries or actual conditions. The Contractor and Landscape/Irrigation Contractor is responsible for reading all notes, checking plant names, numbers and sizes and inspecting the site for actual conditions prior to bidding. This drawing is not intended to constitute a legal survey.

The General Contractor is to remove asphalt, curbs, concrete as shown and site debris/elements not to remain.

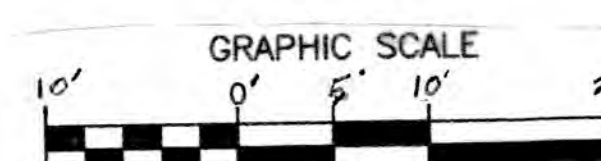
The General Contractor is to install all retaining walls, provide handrails, etc., as per code, install all hardscapes, dig out and establish drainage swales to required grades, elevations and required sq. footage of swales. (to within 2" of finish grade), test all soil drainage and remedy, if needed for required drainage of soils.



Other Sheets and Plans

(Check Site Plan Civil Sheets for fencing, hardscaping, curb requirements, traffic control, site utilities, such as overhead and underground gas, electric, water, phone, power lines, etc.) Call for locate of utilities before digging.

Landscape Plan



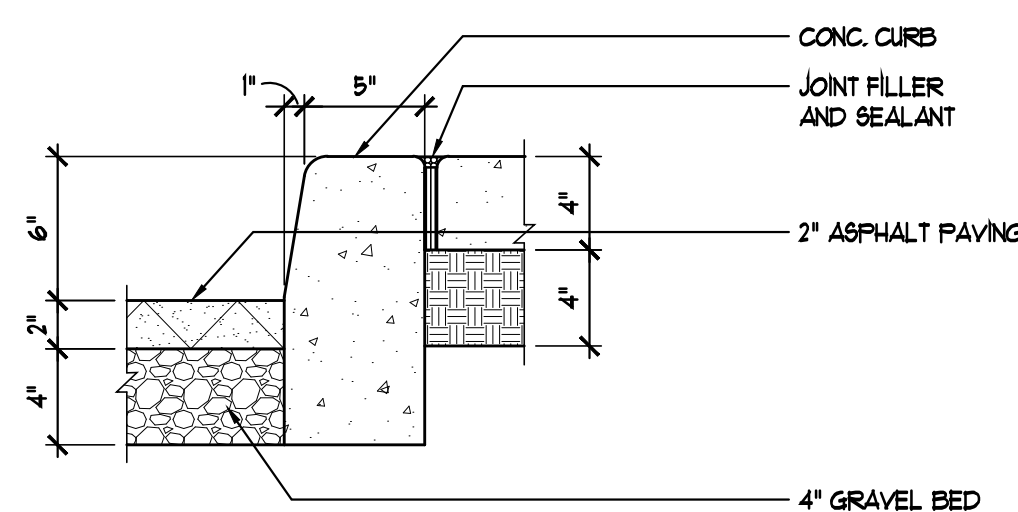
CONTRACTOR NOTE
ALL EXISTING UTILITIES SHOWN ON PLANS ARE TO BE VERIFIED HORIZONTALLY AND VERTICALLY PRIOR TO ANY CONSTRUCTION. ALL EXISTING FEATURES INCLUDING BURIED UTILITIES ARE SHOWN AS INDICATED ON RECORD MAPS AND SURVEYS FURNISHED BY OTHERS. WE ASSUME NO LIABILITY FOR THE ACCURACY OF THOSE RECORDS AND SURVEYS. CONTACT THE UTILITY OWNER/AGENCY FOR THE FINAL LOCATION OF EXISTING UTILITIES IN AREAS CRITICAL TO CONSTRUCTION.

UNDERGROUND SERVICE ALERT
ONE-CALL NUMBER
811
CALL TWO BUSINESS DAYS BEFORE YOU DIG

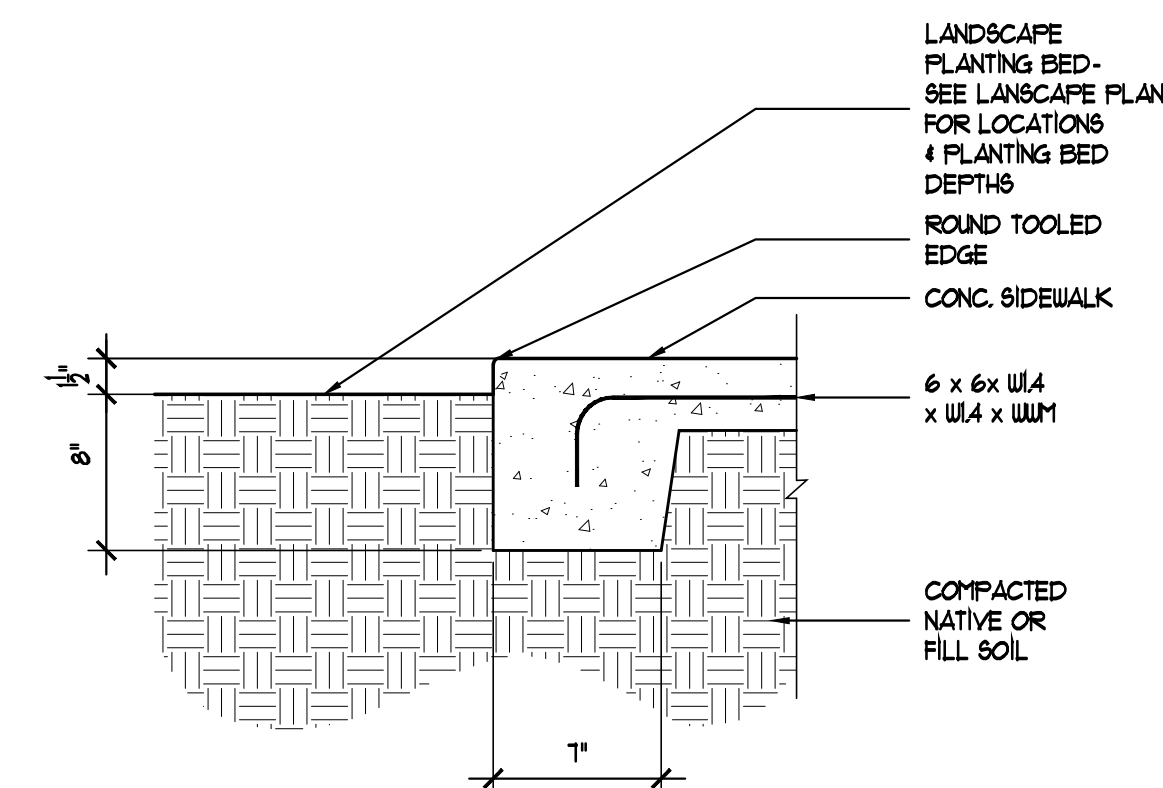
L-1



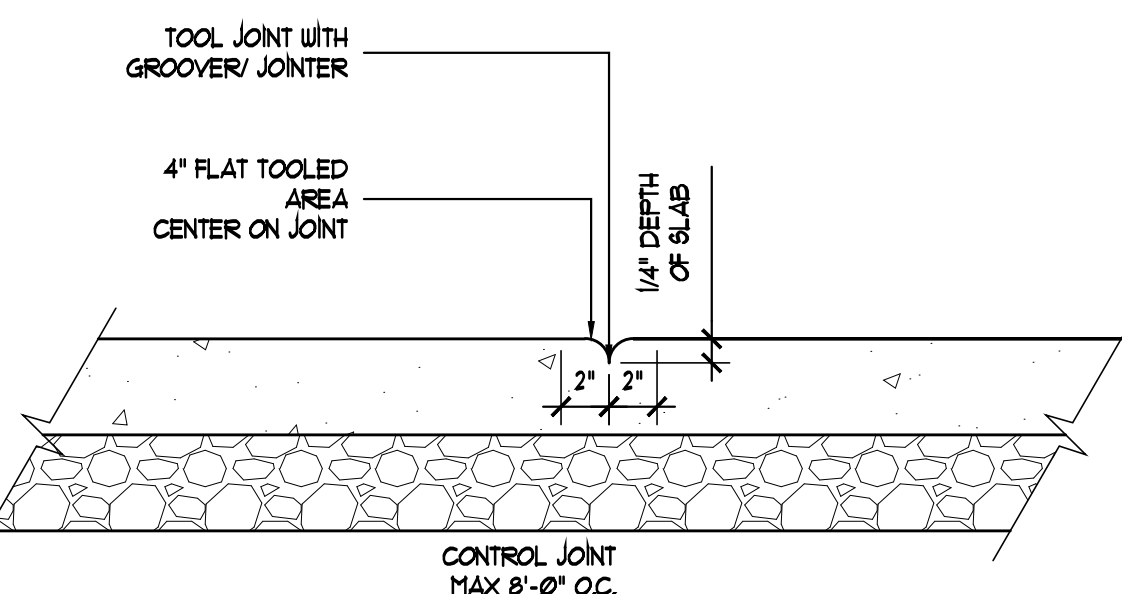
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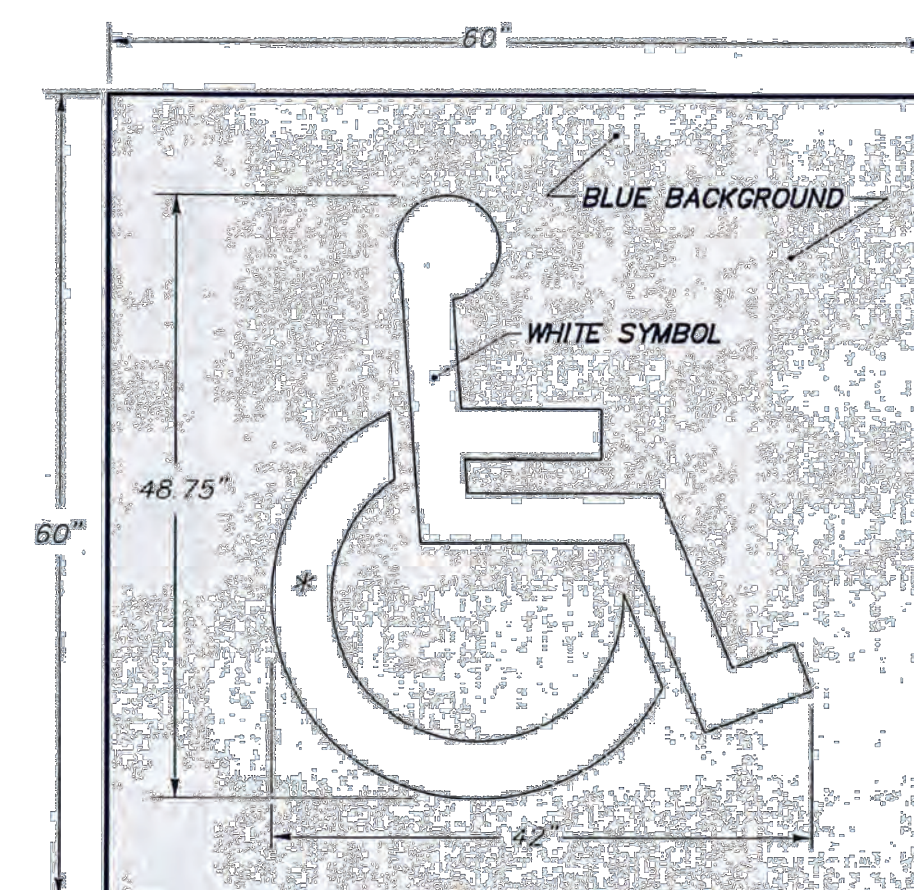
CURB DETAIL
1-1/2" = 1'-0"



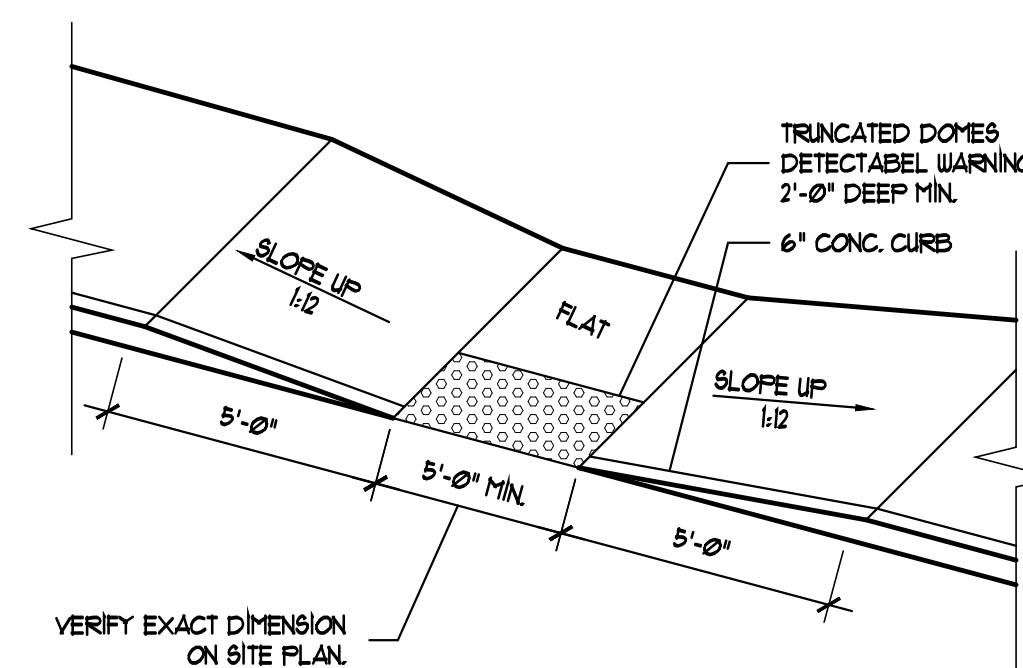
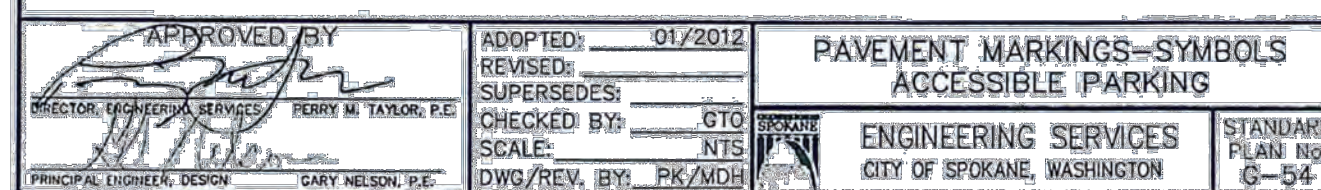
2 THICKENED WALK @ GRADE
A1.0 1'-1/2" = 1'-0"



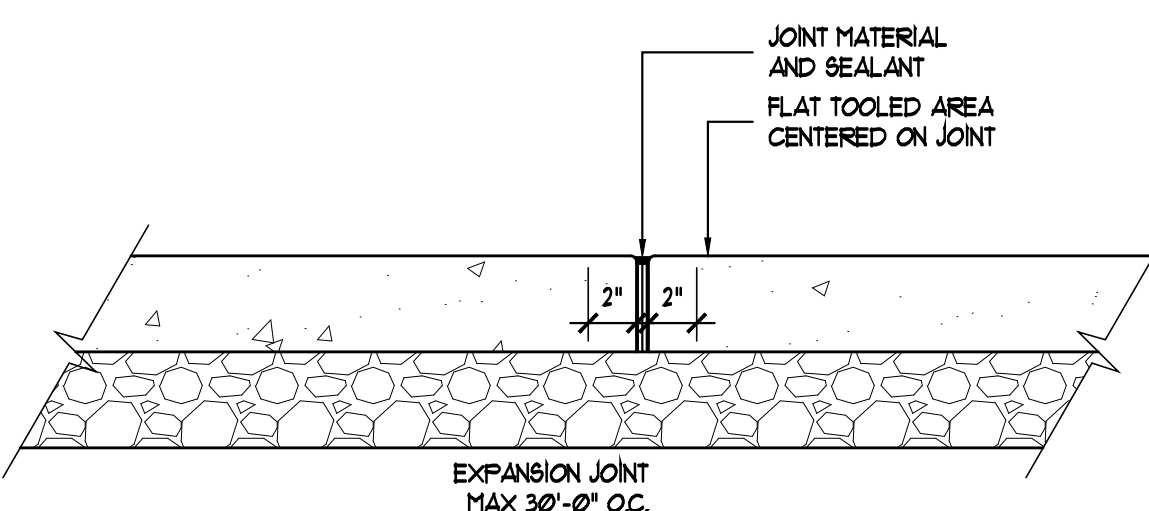
4 CONTROL JOINT DETAIL
1-1/2" = 1'-0"



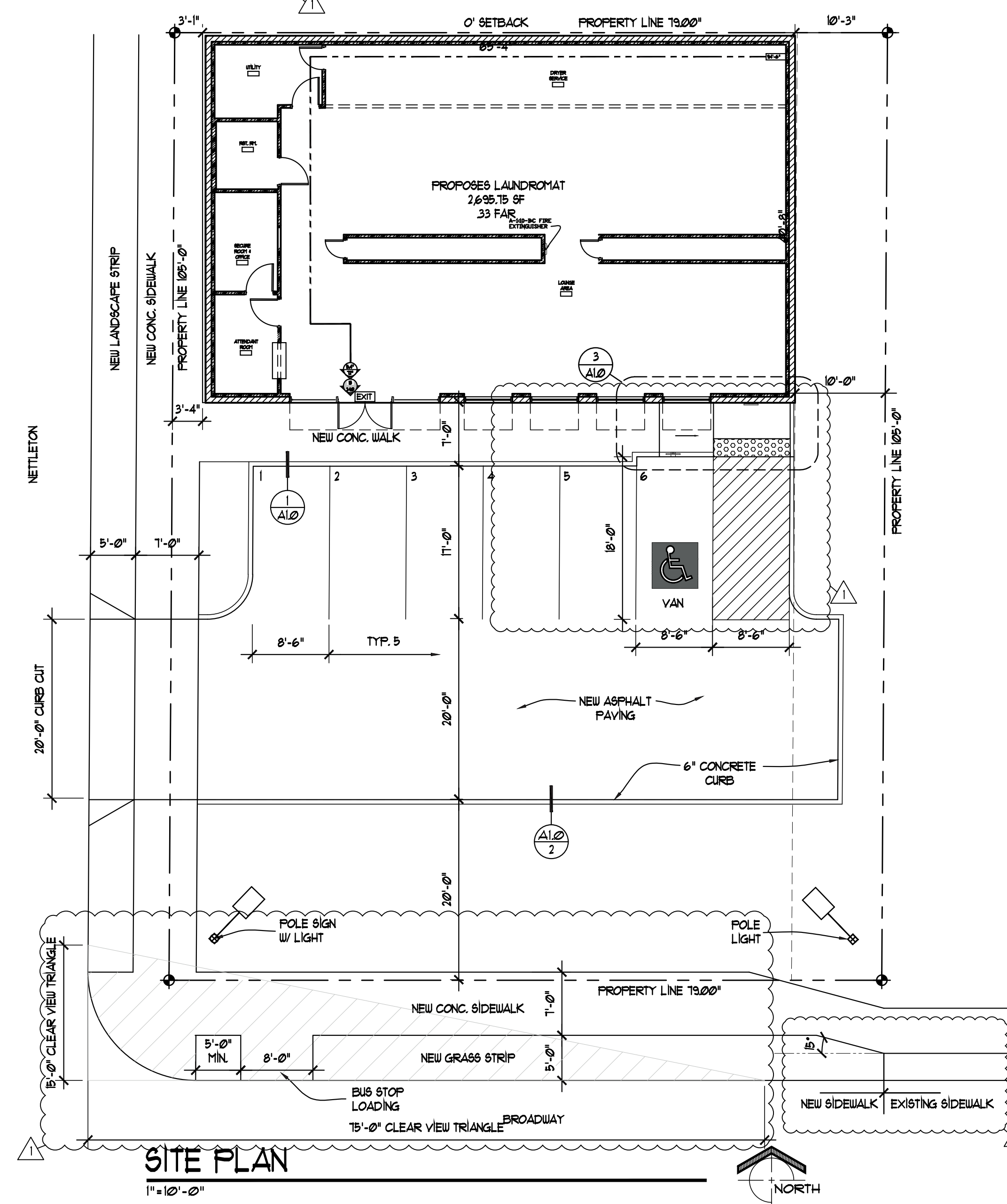
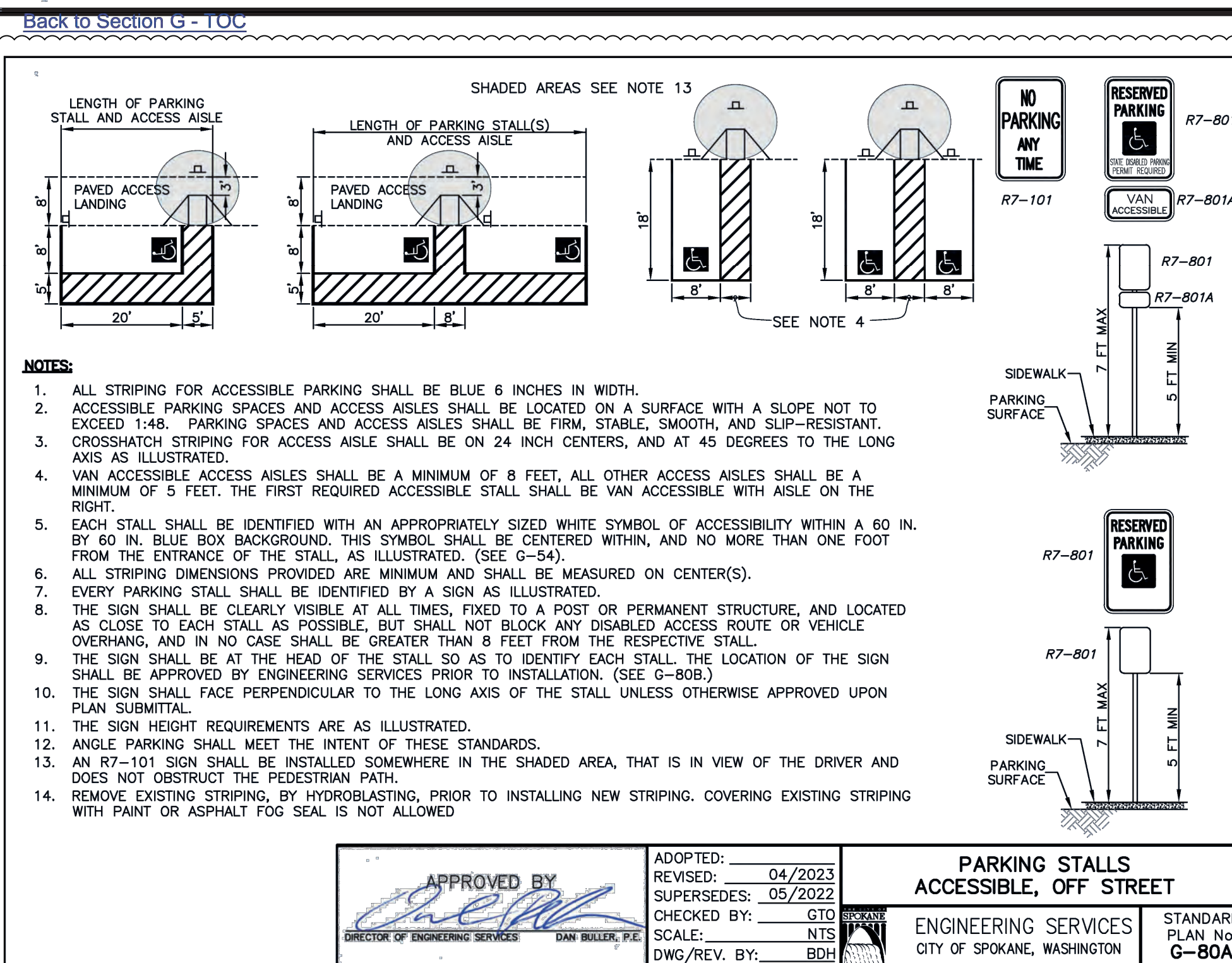
* SEE STATE FABRICATION MANUAL APPENDIX D-12



3 CURB RAMP DETAIL
A10 1/4" = 1'-0"



5 EXPANSION JOINT DETAIL
A1.0 1'-1/2" = 1'-0"



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3736

REGISTERED
ARCHITECT

RUSSELL C. PAGE
STATE OF WASHINGTON

REVISIONS:	DATE	REVISION	BY
	8-3-23	PER BLDG DEPT	MEK

RCP Russell C. Page
ARCHITECTS
1101 N. ARCADE, SUITE 107
PORTLAND, OREGON 97202
TEL: 503/228-1101
FAX: 503/228-1102
WWW.RCPARCHITECTS.COM

PROJECT: **BROADWAY LAUNDROMAT**
2228 W BROADWAY
SPOKANE, WA
DRAWINGS:

DATE:
2-15-23

PLOT SCALE:
1"=10'-0"

DRAWN BY:
MEK

JOB NO.:
23-112

SHEET NO.:

A1.0

GENERAL NOTES:

1. ALL METHODS AND MATERIALS SHALL COMPLY WITH THE LATEST EDITIONS OF THE UNIFORM BUILDING CODE; THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD BRIDGE AND MUNICIPAL CONSTRUCTION; THE CITY OF SPOKANE SUPPLEMENTAL SPECIFICATIONS TO WSDOT'S STANDARD SPECIFICATIONS; AWWA; APWA; AND ALL OTHER APPLICABLE LOCAL, STATE AND FEDERAL CODES, RULES, AND EQUATIONS.
2. THE CONTRACTOR SHALL HAVE A RESPONSIBLE PARTY WHO SHALL HAVE THE AUTHORITY TO REPRESENT AND ACT FOR THE CONTRACTOR AT THE JOB SITE DURING ALL WORKING HOURS.
3. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE ALL MATERIALS REQUIRED FOR CONSTRUCTION OF THE IMPROVEMENTS SHOWN IN THIS PLAN SET.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TWO (2) DAYS NOTICE TO CITY OF SPOKANE ENGINEERING SERVICES PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY.
5. CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS AT THE PROJECT SITE BEFORE STARTING WORK AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
6. UNLESS OTHERWISE INDICATED, ALL DISTURBED AREAS SHALL BE RETURNED TO THE EQUIVALENT OF THEIR PRECONSTRUCTION CONDITION.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION, MAINTENANCE, AND REMOVAL OF SHORING AND BRACING NECESSARY TO PROTECT WORKERS, EXISTING BUILDINGS, STREETS, WALKWAYS, UTILITIES AND OTHER IMPROVEMENTS AND EXCAVATIONS AGAINST LOSS OF GROUND OR CAVING EMBANKMENTS.
8. SURVEY INFORMATION AND UNDERGROUND UTILITY LOCATIONS USED IN THE PREPARATION OF THESE DRAWINGS WERE FURNISHED BY OTHERS. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR COMPLETENESS OR ACCURACY.
9. ALL LOCATIONS OF EXISTING UTILITIES SHOWN HEREON HAVE BEEN ESTABLISHED BY FIELDS SURVEY OR OBTAINED FROM AVAILABLE RECORDS AND SHOULD THEREFORE BE CONSIDERED APPROXIMATE ONLY AND NOT NECESSARILY COMPLETE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, DEPTH, SIZE, TYPE AND CONDITION OF UTILITIES SHOWN ON THESE DRAWINGS AND TO HAVE AN APPROVED LOCATING SERVICE (811) LOCATE AND IDENTIFY UTILITIES WITHIN THE CONSTRUCTION AREA.
10. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY EROSION AND SEDIMENTATION CONTROL FACILITIES AS REQUIRED TO PREVENT EROSION AND TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT LEAVE THE CONSTRUCTION SITE.
11. EXISTING UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND SHALL BE INSPECTED BY THE ENGINEER OF RECORD.
12. PIPE LENGTH SHOWN ON THE PLANS ARE APPROXIMATE AND MAY CHANGE DUE TO FIELD CONDITIONS.
13. CONTRACTOR SHALL OBTAIN AND PAY FOR ALL PERMITS REQUIRED FOR CONSTRUCTION IDENTIFIED ON THESE PLANS.
14. ALL DISTURBED SOIL AREAS SHALL BE SEEDED OR STABILIZED BY OTHER APPROVED METHODS FOR PREVENTION OF ON-SITE EROSION AFTER THE COMPLETION OF CONSTRUCTION.
15. CONTRACTOR SHALL HAVE A PERMIT AND AN APPROVED SET OF PLANS ON-SITE AT ALL TIMES DURING CONSTRUCTION.
16. ALL SUBGRADE PREPARATION, COMPACTION, ROAD BASE, AND ASPHALT CONCRETE PAVEMENT SHALL BE IN ACCORDANCE WITH CITY OF SPOKANE STANDARDS.
17. ALL SWALES SHOWN ON THE PLANS SHALL BE INSTALLED INCLUDING THE ROUGH GRADING FOLLOWING THE PAVING, THE SWALE SHALL BE HYDROSEEDED OR SODDED WITH TURF GRASS AND PROVIDED WITH AN IRRIGATION SYSTEM.
18. ANY WORK IN THE PUBLIC RIGHT-OF-WAY WILL REQUIRE AN OBSTRUCTION PERMIT TO BE OBTAINED BY A LICENSED AND BONDED CONTRACTOR. THE OBSTRUCTION PERMIT WILL REQUIRE A TRAFFIC CONTROL PLAN.

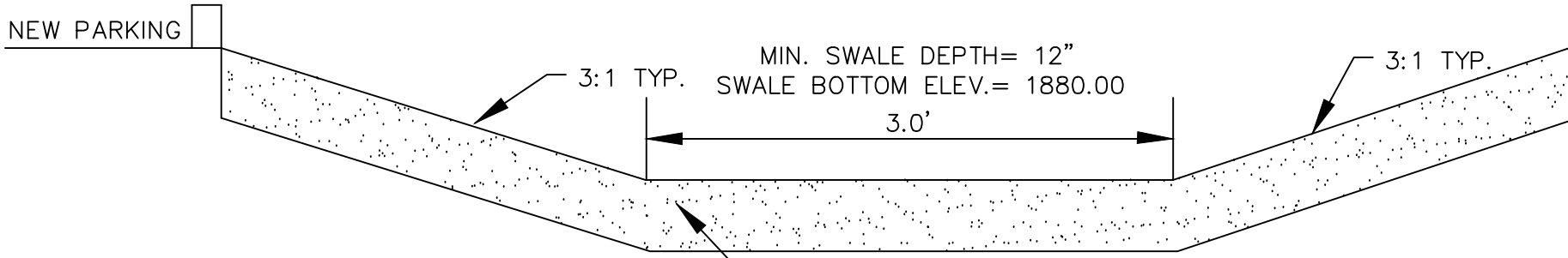
DRAINAGE NOTES

1. 10 YR DESIGN STORM STORAGE VOLUME PROVIDED FROM BOTTOM OF SWALES.
2. SEE DRAINAGE REPORT FOR BOWSTRING ANALYSIS OF STORM WATER RUNOFF.

ROOF AREA TIGHTLINE CALCUALTIONS

ROOF AREA TIGHTLINED = 100%
TOTAL ROOF AREA = 2700 S.F.
ROOF AREA TIGHTLINED TO SWALE = 2700 S.F.
Q10 FOR ROOF AREA = 0.15 CFS
CAPACITY OF 6-INCH PIPE @1% = 0.70 CFS

A CITY OF SPOKANE OBSTRUCTION PERMIT WILL BE REQUIRED FOR ALL WORK IN THE RIGHT-OF-WAY. PLEASE CONTACT THE INSPECTOR LISTED ON THE OBSTRUCTION PERMIT FOR RIGHT-OF-WAY INSPECTIONS INCLUDING THE ALLEY PAVING.

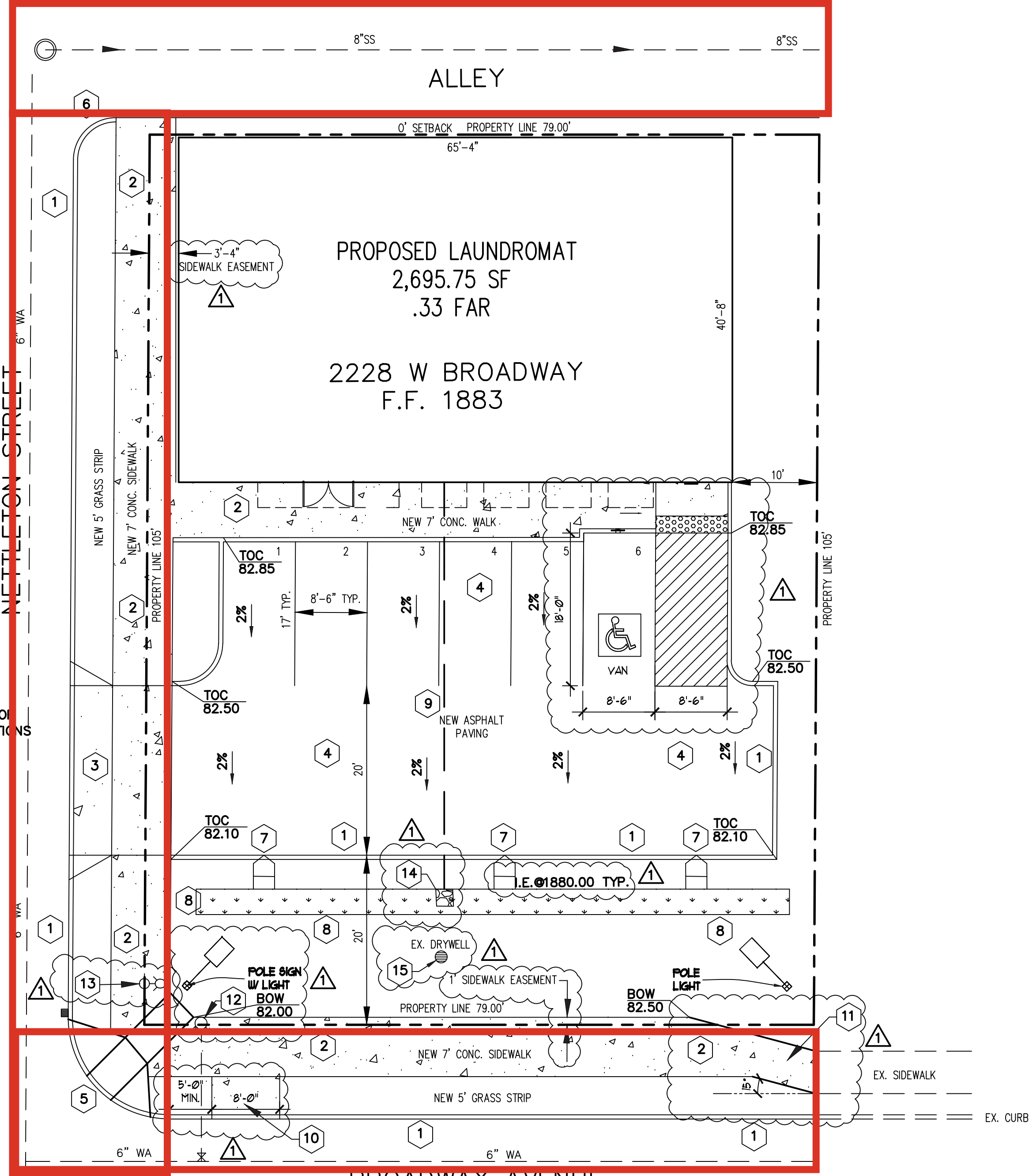


SWALE BOTTOM AND SIDE SLOPES SHALL HAVE A MINIMUM 12" OF TREATMENT SOIL MEETING SRSM REQUIREMENTS IN CHAPTER 6 AND BE HYDROSEEDED OR SODDED.

NOTES:

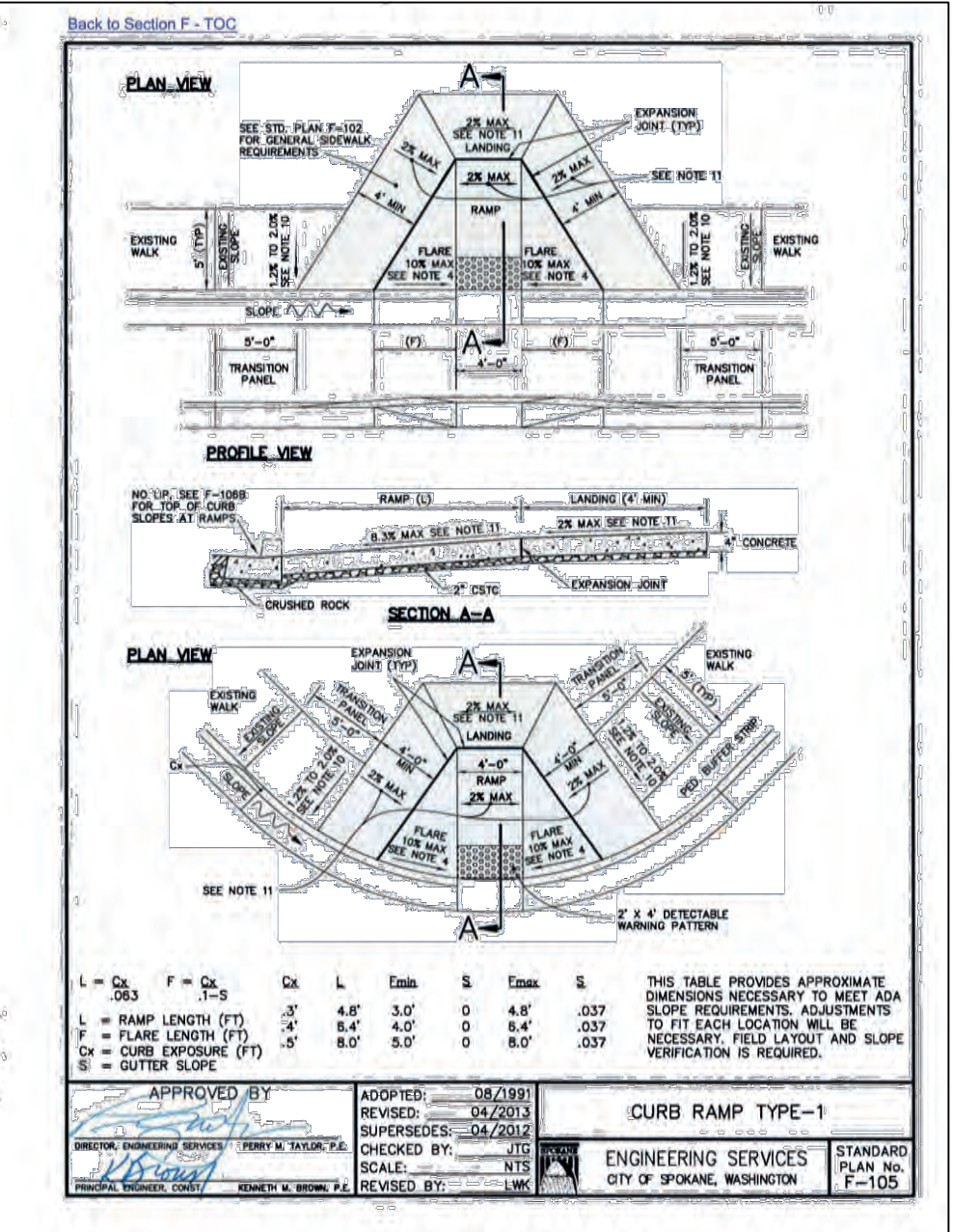
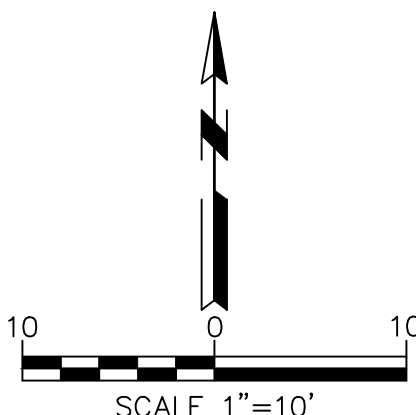
1. MINIMUM 12" OF TREATMENT SOIL.
2. NO MORE THAN 4" OF TOPSOIL OVER UNCOMPACTED NATIVE SOIL.
3. NO COMPACTION IN SWALE BOTTOM.
4. SWALES ARE TO BE GRASSED AND IRRIGATED AS NECESSARY. SWALES WITHOUT IRRIGATION SHALL HAVE DROUGHT-TOLERANT DRYLAND GRASS.

1 TYPICAL SWALE SECTION NOT TO SCALE



ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND BROKEN, HEAVED, SUNKEN, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION. IF YOU WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509)625-6300 TO ARRANGE A SITE VISIT.

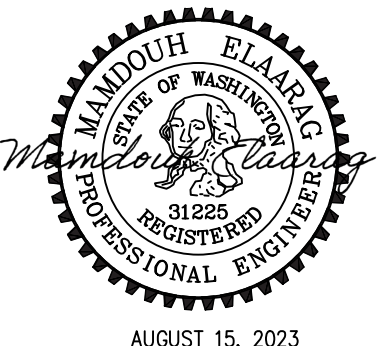
GRADING/DRAINAGE PLAN



2 CURB RAMP TYPE-1 NOT TO SCALE

CONSTRUCTION KEY NOTES:

- 1 REMOVE EXISTING CURB AND INSTALL NEW CONCRETE CURB PER CITY STD. PLAN F-106.
- 2 REMOVE EXISTING SIDEWALK AND INSTALL NEW 7' CONCRETE SIDEWALK PER CITY STD. PLAN F-102.
- 3 INSTALL NEW 20' CONCRETE DRIVEWAY PER CITY STD. PLAN F-104.
- 4 NEW PARKING LOT 3" ACP OVER 4" CSTC.
- 5 INSTALL CURB RETURNS & HANDICAP CURB RAMPS W/ C.O.S. APPROVED TRUNCATED DOMES. SEE DETAIL 2
- 6 INSTALL ALLEY RETURN PER CITY STD. PLAN W-104.
- 7 INSTALL TYPE 2 CURB INLET PER CITY STD. PLAN F-109.
- 8 '208' DRAINAGE SWALE BOTTOM AREA=70'X3' BOTTOM ELEV. 1880.00.
- 9 INSTALL 50' OF 6-INCH D.I. RAINWATER LEADER TIGHTLINED TO NEW SWALE. MINIMUM SLOPE = 1%. INV. @ BLDG = 1880.50 INV. @ SWALE = 1880.00
- 10 INSTALL CONCRETE BOARDING AND ALIGHTING PAD PER STA STANDARDS.
- 11 TRANSITION NEW SIDEWALK TO MATCH EXISTING SIDEWALK.
- 12 RELOCATE EXISTING FIRE HYDRANT 2' BEHIND NEW SIDEWALK.
- 13 RELOCATE EXISTING LIGHT POLE 2' BEHIND NEW SIDEWALK.
- 14 INSTALL 2'X2' RIP RAP PAD.
- 15 EXISTING SINGLE DEPTH DRYWELL TO BE PROTECTED. ADJUST RIM ELEVATION TO 1880.50.



REV	DATE	DESCRIPTION	DWN BY	DES BY	CHK BY	APP BY
1	8-15-23	REPONE TO CITY COMMENTS 7-19-23	MHE	MHE		
DATE OF ISSUE			DWN BY		CHK BY	
AUGUST 15, 2023			DES BY		APP BY	
			MHE			
			MHE			

MHE ENGINEERING
9702 W. Masters Lane
Cheney, WA 99004
(509) 230-1960
MELAARAO@YAHOO.COM

BROADWAY LAUNDROMAT
GRADING/DRAINAGE PLAN

DRAWING NO
C1.0

CALL BEFORE YOU DIG: 811

AUGUST 15, 2023

B2310052

GENERAL NOTES:

1. ALL METHODS AND MATERIALS SHALL COMPLY WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODE & UNIFORM PLUMBING CODE; THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD BRIDGE AND MUNICIPAL CONSTRUCTION; THE CITY OF SPOKANE SUPPLEMENTAL SPECIFICATIONS; AWWA; APWA, AND ALL OTHER APPLICABLE LOCAL, STATE AND FEDERAL CODES, RULES, AND REGULATIONS.
2. THE CONTRACTOR SHALL CONTACT THE DEPARTMENT OF ENGINEERING SERVICES CONSTRUCTION MANAGEMENT OFFICE, 1225 EAST MARIETTA AVENUE, 625-7722 TO ARRANGE A MUTUALLY AGREEABLE START WORK DATE.
3. THE CONTRACTOR SHALL PROVIDE THE DESIGN ENGINEER WITH RECORD DRAWINGS PRIOR TO FINAL APPROVAL. ALL DEVIATIONS FROM THE ORIGINAL PLANS MADE DURING THE COURSE OF CONSTRUCTION, INCLUDING LOCATIONS, INVERTS AND DEPTHS OF UTILITIES SHALL BE CLEARLY MARKED ON THE RECORD DRAWING.
4. AN OBSTRUCTION PERMIT IS REQUIRED FOR ALL WORK IN THE PUBLIC RIGHT OF WAY WHICH INCLUDES PUBLIC SIDEWALK AND TIME RESTRICTIONS WILL APPLY FOR LANE CLOSURES ON ARTERIAL STREETS.

SEWER NOTES:

1. ALL SEWER MAINS SHALL BE 8" SDR35 PVC PIPE.
2. ALL SIDE SEWERS SHALL BE 6" SDR35 PVC WITH CONNECTION AT SEWER MAIN PER CITY OF SPOKANE STANDARD PLAN Z-116.
3. ALL SIDE SEWERS SHALL HAVE 2% MIN. SLOPE, UNLESS NOTED OTHERWISE.
4. LOCATE SIDE SEWERS AS SHOWN ON THE PLANS.
5. EXTEND SIDE SEWERS TO 10' BEHIND BACK OF SIDEWALK.
6. PLACE WATER TIGHT PLUG AND GREEN 4"x4" MARKER WITH 18" LONG 1/2" REBAR SET AT MAX. 6" BELOW EXISTING GROUND @ END OF SIDE SEWER. EXTEND 4"x4" MARKER 2' MIN. ABOVE FINISH GRADE.
7. MAINTAIN 9.5' HORIZONTAL SEPARATION BETWEEN SEWER & WATER MAINS. MAINTAIN 10' HORIZONTAL SEPERATION BETWEEN SEWER & WATER SERVICES.
8. MAINTAIN 18" VERTICAL SEPERATION BETWEEN SEWER & WATER MAINS AND SERVICE LINES WHEN POSSIBLE. WHERE VERTICAL SEPERATION IS LESS THAN 18" OR SEWER (NON-POTABLE WATER) IS ABOVE THE WATER LINE SLEEVE LINES PER CITY OF SPOKANE STD. PLAN A-4. IN NO CASE SHALL THE CLEAR VERTICAL DISTANCE BETWEEN CROSSING LINES OR THEIR CASINGS BE LESS THAN 6".
9. CONTRACTOR SHALL PROVIDE EXCAVATION, BEDDING, BACKFILL AND COMPACTION IN ACCORDANCE TO CITY OF SPOKANE STANDARD PLANS A-1 THROUGH A-3.
10. CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF ALL EXISTING STRUCTURES.
11. CONTRACTOR SHALL COORDINATE WITH THE SITY OF SPOKANE FOR INSPECTION OF ALL SEWER WORK.

WATER NOTES

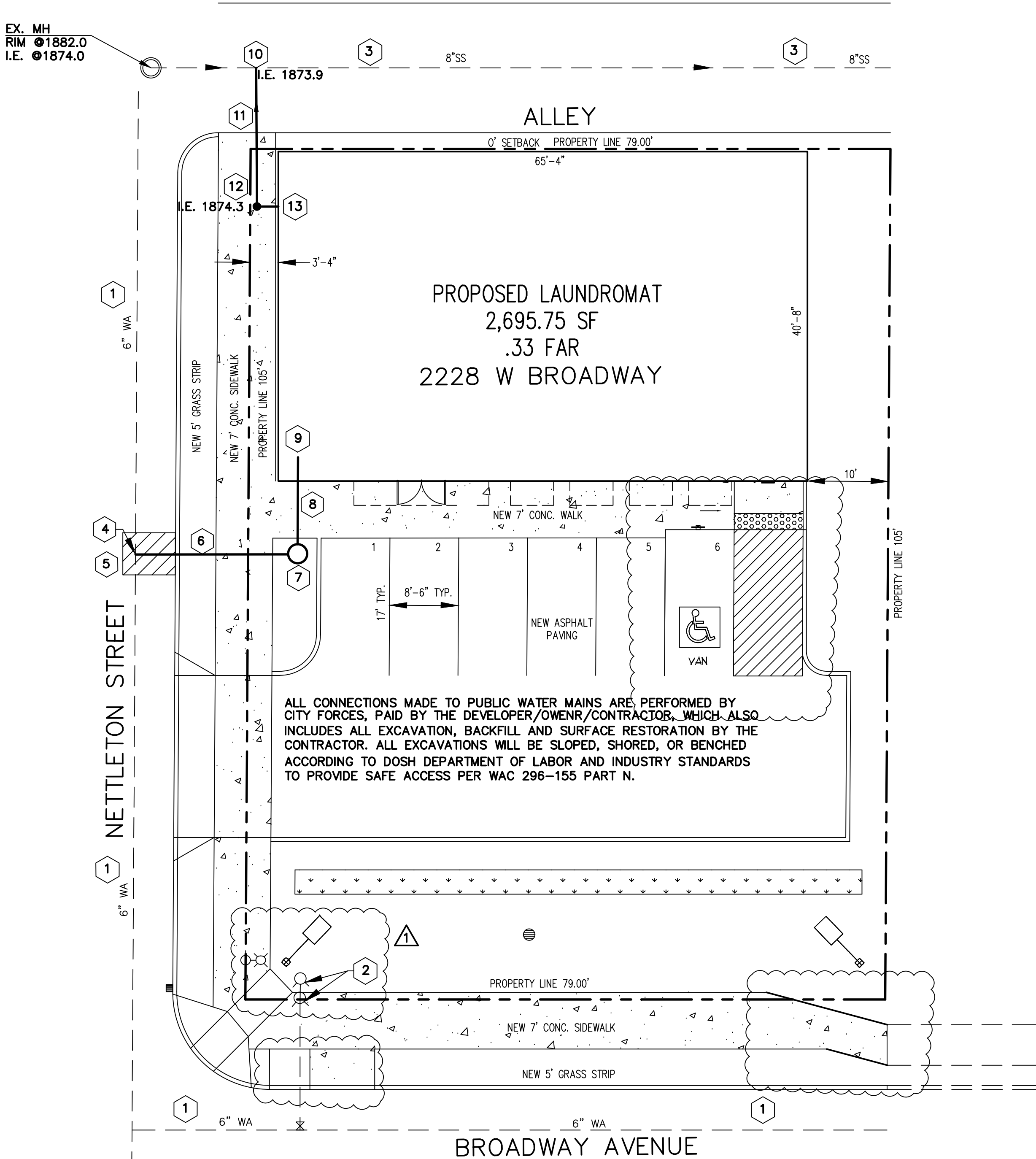
1. WATER MAIN SHALL BE CLASS 50 DUCTILE IRON PIPE W/ MECHANICAL JOINT FITTINGS.
2. ALL WATER SERVICES SHALL BE INSTALLED PER CITY OF SPOKANE SPECIFICATIONS.
3. ALL WATER SERVICES SHALL BE EXTENDED 10' BEHIND BACK OF SIDEWALK PER STANDARD PLAN Y-111.
4. ALL MATERIAL MUST BE NEW, INSPECTED, AND APPROVED BY CITY PRIOR TO INSTALLATION.
5. CONTRACTOR SHALL PROVIDE EXCAVATION, BEDDING, BACKFILL AND COMPACTION TO CITY OF SPOKANE STANDARDS.
6. MAINTAIN A CLEARANCE OF 5' BETWEEN UTILITY LINES IN A COMMON TRENCH.
7. ALL D.I. FITTINGS TO BE MECHANICAL JOINT (MJ).
8. ALL WATER MAINS AND SERVICES SHOULD BE A MINIMUM 5.5 FEET FROM FINISHED GRADE TO INVERT.
9. WATER MAINS AND SERVICES SHALL BE A MINIMUM OF 9.5' FROM SANITARY SEWER LINES & SIDE SERVICES.
10. ALL SERVICES SHALL HAVE A 1" CURB STOP & VALVE BOX ON PROPERTY WITHIN 3 FEET OF PROPERTY LINE PER STANDARD PLAN Y-111.
11. PIPES AND FITTINGS SHALL BE RESTRAINED PER THE ATTACHED RESTRAINT JOINT SCHEDULE.
12. PLACE WATER TIGHT CAP AND BLUE 4"x4" MARKER WITH 18" LONG 1/2" REBAR SET AT MAX. 6" BELOW EXISTING GROUND @ END OF WATER SERVICE. EXTEND 4"x4" MARKER 2' MIN. ABOVE FINISH GRADE.
13. ALL MAIN LINE GATE VALVES AND WATER SERVICE CURB STOPS SHALL BE PLACED IN CAST IRON VALVE BOX PER CITY OF SPOKANE STANDARD PLAN NO. Y-109.
14. ALL CONNECTIONS TO EXISTING WATER MAINS BY CITY FORCES.

ADDITIONAL WATER NOTES:

- I. WATER SERVICES SHALL MEET CURRENT BACKFLOW STANDARDS PER WAC 246-290-490 AND FOLLOW CITY OF SPOKANE WATER DEPARTMENT RULES AND REGULATIONS FOR WATER SERVICE INSTALLATIONS.
- II. MUST PROVIDE PASSING TESTING REPORTS FOR ALL BACKFLOW ASSEMBLIES (E.G. IRRIGATION AND FIRE SYSTEMS) TO THE CITY OF SPOKANE WATER DEPARTMENT. SEND ALL REPORTS TO WATERCROSSCONNECTION@SPOKANECITY.ORG BEFORE SITE SURVEY/INSPECTION, AND ALSO BEFORE CERTIFICATE OF OCCUPANCY CAN BE ISSUED.
- III. COORDINATE A SITE SURVEY/INSPECTION WITH THE CITY WATER DEPARTMENT (AFTER BACKFLOW DEVICES HAVE BEEN TESTED) NO LESS THAN 48 HOURS PRIOR TO REQUESTING CERTIFICATE OF OCCUPANCY. CROSS CONNECTION LINE TO SCHEDULE A WATER USE SURVEY/INSPECTION IS 509-625-7969.
- IV. CITY OF SPOKANE HAS A NEW HYDRANT USE PERMIT PROGRAM THAT REQUIRES AN RPBA AND FLOW METER ASSEMBLY FOR ALL FIRE HYDRANT WATER USAGE IF NEEDED FOR CONSTRUCTION PHASE OF THE PROJECT (E.G. DUST CONTROL, ETC.). THIS HYDRANT PROGRAM REQUIRES A NEW PERMIT FEE, CONSUMPTION BILLING, AND DEPOSIT FOR THE RPBA AND FLOW METER ASSEMBLY. PLEASE CALL 311 OR CITY WATER DEPARTMENT AT 509-625-7800 FOR MORE INFORMATION.

WATER METER SIGNAL NOTES:

1. CONDUIT FOR METER SIGNAL SHALL BE PROVIDED FROM CONCRETE VAULT AND STUBED UP MIN. 3' ABOVE GROUND (I.E. NEXT TO STREET FACING WALL OF THE BUILDING OR NEXT TO A POST/BOLLARD).
2. ALL CONDUIT BENDS MUST BE SEWEEPS NOT ELBOWS.
3. FOR METER SIGNAL POST: SHALL PROVIDE AND ATTACHE A 16' LONG COMPOSITE DECKING 2X6 TO FACE OF POST JUST BELOW ITS TOP (BOARD LENGTH PARALLEL TO GROUND, FACING NEAREST STREET) TO MOUNT THE METER TRANSMITTERS.



WATER / SEWER PLAN

ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND BROKEN, HEAVED, SUNKEN, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION. IF YOU WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509)625-6300 TO ARRANGE A SITE VISIT.

LEGEND

- LP LIGHT POLE
- P-O UTILITY POLE
- G- GUY ANCHOR
- FH FIRE HYDRANT
- WV WATER VALVE
- WM WATER METER
- HB HOSE BIB
- GV GAS VALVE
- CMH COMMUNICATION MANHOLE
- SMH SEWER MANHOLE
- 8"SS EXISTING SEWER LINE
- W EXISTING WATER LINE

UTILITIES NOTE

SURFACE UTILITIES SHOWN ARE FROM VISIBLE EVIDENCE COLLECTED BY SURVEY ONLY. SUBSURFACE UTILITY LINES ARE NOT SHOWN. INVESTIGATION INTO RECORDS HELD BY UTILITY PURVEYORS HAS NOT BEEN PERFORMED. UTILITIES WERE NOT SURFACE-MARKED FOR LOCATION PRIOR TO SURVEY.

EXISTING NOTES:

- 1 EXISTING 6" CAST IRON WATER LINE.
- 2 RELOCATE EXISTING FIRE HYDRANT TO 2' BEHIND NEW SIDEWALK.
- 3 EXISTING 8" SEWER LINE AT 0.0047.

WATER KEY NOTES:

- NOTE:**
2" HDPE SDR 9 CTS 200PSI IS AN ACCEPTABLE PIPE MATERIAL.
- 4 NEW 2-INCH TYPE K COPPER WATER SERVICE WITH CURB STOP PER CITY OF SPOKANE STD. PLAN Y-111.
 - 5 ALL PAVEMENT CUTS FOR UTILITY WORK AND PATCHES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE SPOKANE REGIONAL PAVEMENT CUT POLICY AND THE CITY OF SPOKANE STANDARDS.
 - 6 20' OF 2-INCH TYPE K COPPER WATER SERVICE.
 - 7 NEW 2-INCH METER IN CONCRETE WATER METER BOX 3' FROM PROPERTY LINE PER CITY OF SPOKANE STD. PLANS Y-112A.
 - 8 10' OF 2-INCH TYPE K COPPER WATER SERVICE.
 - 9 REFER TO MECHANICAL PLANS FOR WATER DETAILS AT BLDG.

- NOTE:**
ALL SEWER FITTINGS MUST BE SDR26.
- SEWER KEY NOTES:**
- 10 NEW 6-INCH SIDE SEWER SERVICE PER CITY OF SPOKANE STANDARDS.
 - 11 20'-6-INCH PVC SANITARY SIDE SEWER. 2% MIN. SLOPE.

- 12 6-INCH CLEANOUT TO GRADE WITH CAST IRON ACCESS LID SET 3' FROM BLDG. PER CITY STD. PLANS Z-114 AND Z-114A.
- 13 REFER TO MECHANICAL PLANS FOR SEWER DETAILS AT BLDG.

THE NEW SIDE SEWER SHALL BE 6-INCH PVC, HAVE A MINIMUM SLOPE OF TWO PERCENT AND 3.5 FEET OF COVER WHERE VEHICULAR TRAFFIC PASSES OVER, TWO FEET MINIMUM IN OTHER AREAS. THE TAP MUST BE IN THE MAINLINE, NOT TO A MANHOLE. SEWER AND WATER SEPARATION REQUIREMENTS ARE 18" MINIMUM VERTICAL, FIVE-FOOT MINIMUM HORIZONTAL. SEWER CLEANOUTS SHALL BE INSTALLED EVERY 100' AND AT EVERY ANGLE 45 DEGREES OR GREATER.

CALL BEFORE YOU DIG: 811



AUGUST 15, 2023

REV	DATE	DESCRIPTION	DWN BY	DES BY	CHK BY	APP BY
1	8-15-23	RESPONE TO CITY COMMENTS 7-19-23	MHE	MHE		
DATE OF ISSUE			DWN BY		CHK BY	
AUGUST 15, 2023			DWN BY		APP BY	
			DES BY			

MHE ENGINEERING

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BROADWAY LAUNDROMAT
WATER / SEWER PLAN

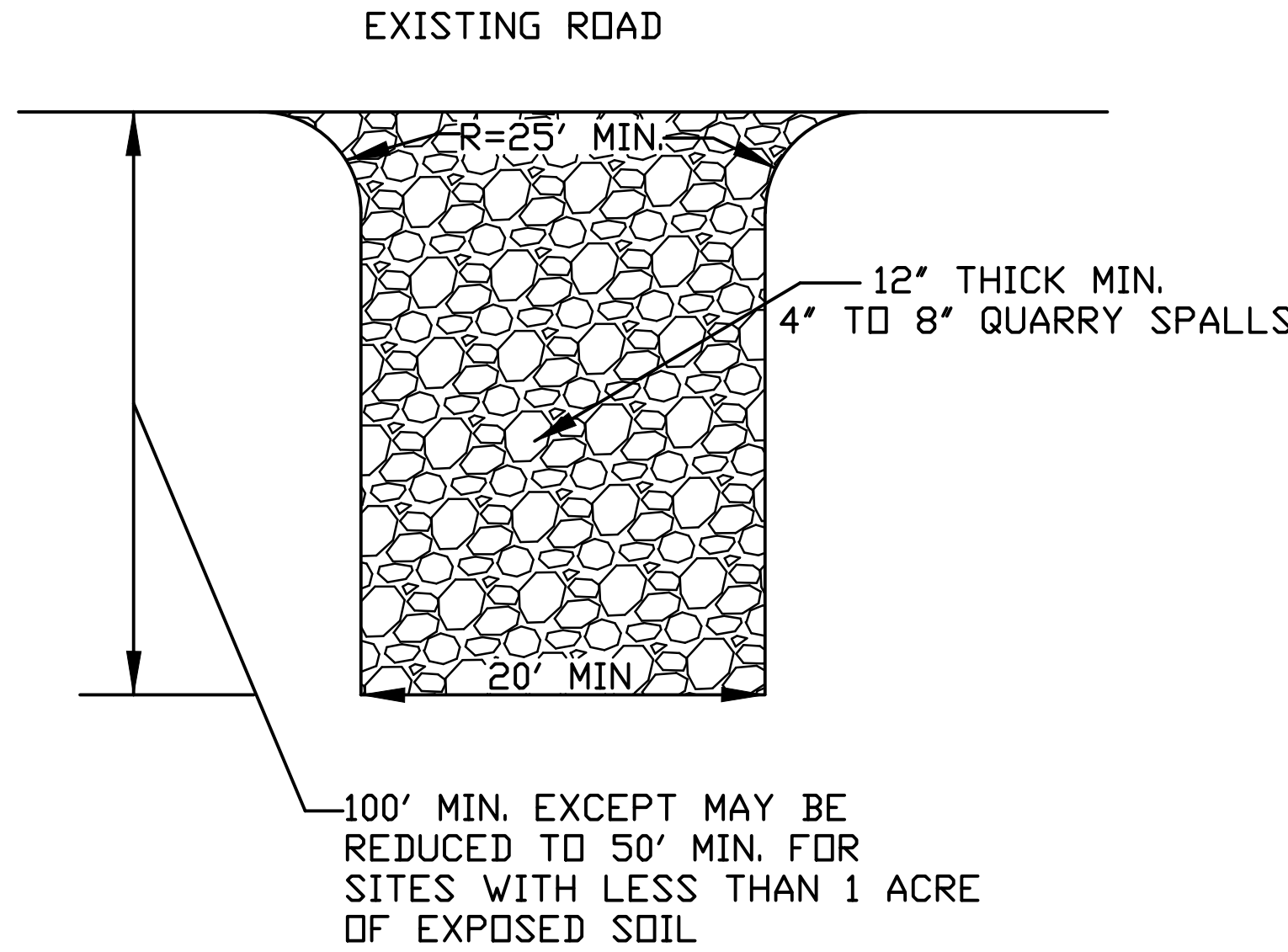
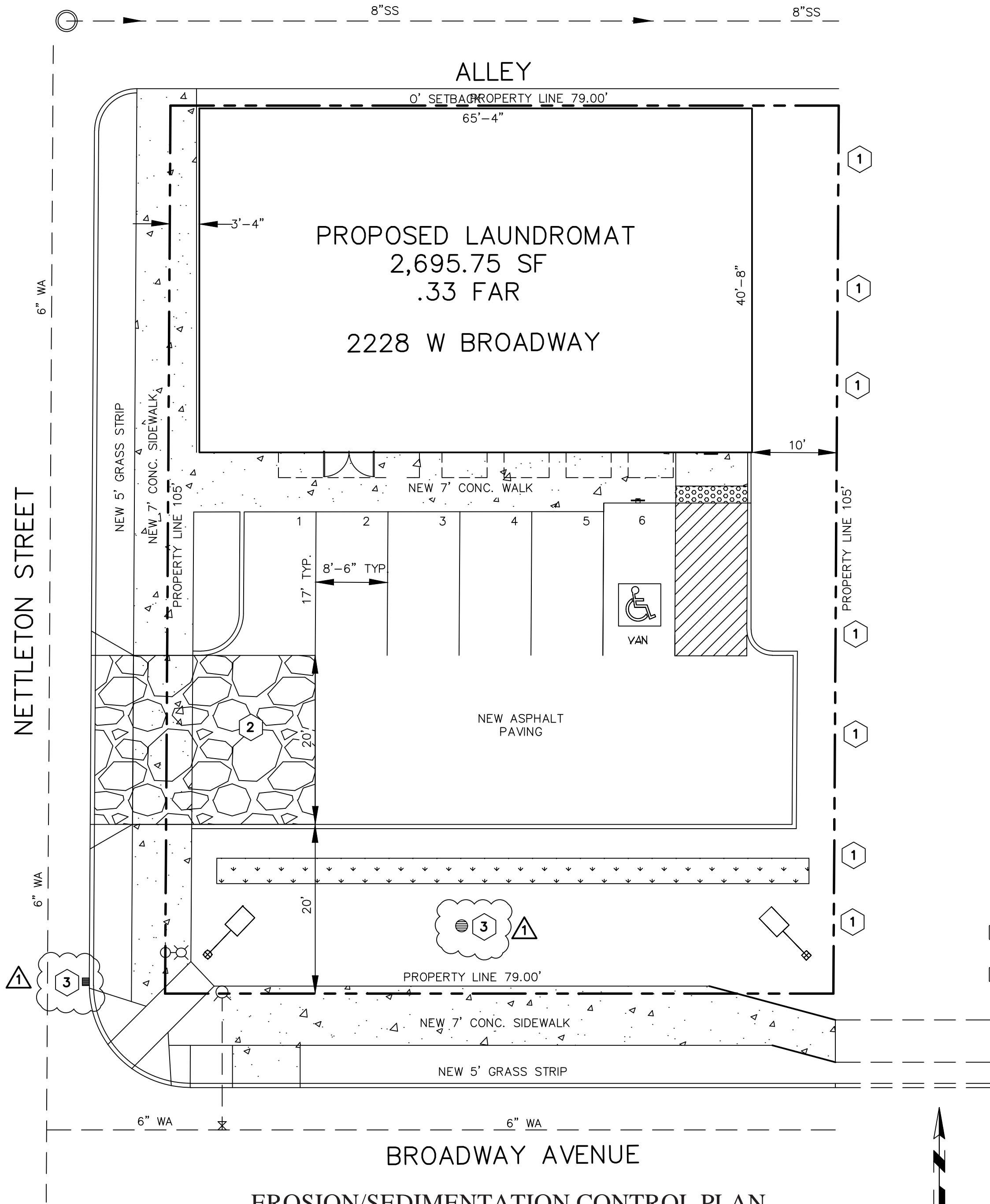
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EROSION/SEDIMENTATION CONTROL NOTES

1. THE FOLLOWING CONSTRUCTION SEQUENCE SHALL BE FOLLOWED IN ORDER TO BEST MINIMIZE THE POTENTIAL FOR EROSION AND SEDIMENTATION CONTROL PROBLEMS:
 - a. CLEAR AND GRUB SUFFICIENTLY FOR INSTALLATION OF TEMPORARY ESC BMPs;
 - b. INSTALL TEMPORARY ESC BMPs, CONSTRUCTING SEDIMENT TRAPPING BMPs AS ONE OF THE FIRST STEPS PRIOR TO GRADING;
 - c. CLEAR, GRUB AND ROUGH GRADE FOR ROADS, TEMPORARY ACCESS POINTS AND UTILITY LOCATIONS;
 - d. STABILIZE ROADWAY APPROACHES AND TEMPORARY ACCESS POINTS WITH THE APPROPRIATE CONSTRUCTION ENTRY BMP;
 - e. CLEAR, GRUB AND GRADE INDIVIDUAL LOTS OR GROUPS OF LOTS;
 - f. TEMPORARILY STABILIZE, THROUGH RE-VEGETATION OR OTHER APPROPRIATE BMPs, LOTS OR GROUP OF LOTS IN SITUATION WHERE SUBSTANTIAL CUT OR FILL SLOPES ARE AS A RESULT OF THE SITE GRADING;
 - g. CONSTRUCT ROADS, BUILDINGS, PERMANENT STORMWATER FACILITIES (I.E. INLETS, PONDS, UIC FACILITIES, ETC.);
 - h. PROTECT ALL PERMANENT STORMWATER FACILITIES UTILIZING THE APPROPRIATE BMPs;
1. INSTALL PERMANENT ESC CONTROLS, WHEN APPLICABLE; AND,
- i. REMOVE TEMPORARY ESC CONTROL WHEN:
2. PERMANENT ESC CONTROLS, WHEN APPLICABLE, HAVE BEEN COMPLETELY INSTALLED;
3. ALL LAND-DISTURBING ACTIVITIES THAT HAVE THE POTENTIAL TO CAUSE EROSION OR SEDIMENTATION PROBLEMS HAVE CEASED; AND,
4. VEGETATION HAD BEEN ESTABLISHED IN THE AREAS NOTED AS REQUIRING VEGETATION ON THE ACCEPTED ESC PLAN ON FILE WITH THE LOCAL JURISDICTION.
5. INSPECT ALL ROADWAYS, AT THE END OF EACH DAY, ADJACENT TO THE CONSTRUCTION ACCESS ROUTE. IF IT IS EVIDENT THAT SEDIMENT HAS BEEN TRACKED OFF SITE AND/OR BEYOND THE ROADWAY APPROACH, CLEANING IS REQUIRED.
6. IF SEDIMENT REMOVAL IS NECESSARY PRIOR TO STREET WASHING, IT SHALL BE REMOVED BY SHOVELING OR PICKUP SWEEPING AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
7. IF STREET WASHING IS REQUIRED TO CLEAN SEDIMENT TRACKED OFF SITE, ONCE SEDIMENT HAS BEEN REMOVED, STREET WASH WASTEWATER SHALL BE CONTROLLED BY PUMPING BACK ON-SITE OR OTHERWISE PREVENTED FROM DISCHARGING INTO SYSTEMS TRIBUTARY TO WATERS OF THE STATE.
8. RESTORE CONSTRUCTION ACCESS ROUTE EQUAL TO OR BETTER THAN THE PRE-CONSTRUCTION CONDITION.
9. RETAIN THE DUFF LAYER, NATIVE TOPSOIL AND NATURAL VEGETATION IN AN UNDISTURBED STATE TO THE MAXIMUM EXTENT PRACTICAL.
10. INSPECT SEDIMENT CONTROL BMPs WEEKLY AT A MINIMUM, DAILY DURING A STORM EVENT, AND AFTER ANY DISCHARGE FROM THE SITE (STORMWATER OR NON-STORMWATER). THE INSPECTION FREQUENCY MAY BE REDUCED TO ONCE A MONTH IF THE SITE IS STABILIZED AND INACTIVE.
11. CONTROL FUGITIVE DUST FROM CONSTRUCTION ACTIVITY IN ACCORDANCE WITH THE STATE AND/OR LOCAL AIR QUALITY CONTROL AUTHORITIES WITH JURISDICTION OVER THE PROJECT AREA.
12. STABILIZE EXPOSED UNWORKED SOILS (INCLUDING STOCKPILES), WHETHER AT FINAL GRADE OR NOT, WITHIN 10 DAYS DURING THE REGIONAL DRY SEASON (JULY 1 THROUGH SEPTEMBER 30) AND WITHIN 5 DAYS DURING THE REGIONAL WET SEASON (OCTOBER 1 THROUGH JUNE 30). SOILS MUST BE STABILIZED AT THE END OF A SHIFT BEFORE A HOLIDAY WEEKEND IF NEEDED BASED ON THE WEATHER FORECAST. THIS TIME LIMIT MAY ONLY BE ADJUSTED BY A LOCAL JURISDICTION WITH A "QUALIFIED LOCAL PROGRAM," IF IT CAN BE DEMONSTRATED THAT THE RECENT PRECIPITATION JUSTIFIES A DIFFERENT STANDARD AND MEETS THE REQUIREMENTS SET FORTH IN THE CONSTRUCTION GENERAL PERMIT.
13. PROTECT INLETS, DRYWELLS, CATCH BASINS AND OTHER STORMWATER MANAGEMENT FACILITIES FROM SEDIMENT, WHETHER OR NOT FACILITIES ARE OPERABLE.
14. KEEP ROADS ADJACENT TO INLETS CLEAN.
15. INSPECT INLETS WEEKLY AT A MINIMUM AND DAILY DURING STORM EVENTS.
16. CONSTRUCT STORMWATER CONTROL FACILITIES (DETENTION/RETENTION STORAGE POND OR SWALES) BEFORE GRADING BEGINS. THESE FACILITIES SHALL BE OPERATIONAL BEFORE THE CONSTRUCTION OF IMPERVIOUS SITE IMPROVEMENTS.
17. STOCKPILE MATERIALS (SUCH AS TOPSOIL) ON SITE, KEEPING OFF ROADWAY AND SIDEWALKS.
18. COVER, CONTAIN AND PROTECT ALL CHEMICALS, LIQUID PRODUCTS, PETROLEUM PRODUCT, AND NON-INERT WASTES PRESENT ON SITE FROM VANDALISM (SEE CHAPTER 173-304 WAC FOR THE DEFINITION ON INERT WASTE), USE SECONDARY CONTAINMENT FOR ON-SITE FUELING TANKS.
19. CONDUCT MAINTENANCE AND REPAIR OF HEAVY EQUIPMENT AND VEHICLES INVOLVING OIL CHANGES, HYDRAULIC SYSTEM REPAIR, SOLVENT AND DE-GREASING OPERATIONS, FUEL TANK DRAIN DOWN AND REMOVAL, AND OTHER ACTIVITIES THAT MAY RESULT IN DISCHARGE OR SPILLAGE OF POLLUTANTS TO THE GROUND OR INTO STORMWATER RUNOFF USING SPILL PREVENTION MEASURES, SUCH AS DRIP PANS. CLEAN ALL CONTAMINATED SURFACES IMMEDIATELY FOLLOWING ANY DISCHARGE OR SPILL INCIDENT. IF RAINING OVER EQUIPMENT OR VEHICLE, PERFORM EMERGENCY REPAIR ON SITE USING TEMPORARY PLASTIC BENEATH THE VEHICLE.
20. CONDUCT APPLICATION OF AGRICULTURAL CHEMICALS, INCLUDING FERTILIZER AND PESTICIDES, IN SUCH A MANNER, AND APPLICATION RATES, THAT INHIBITS THE LOSS OF CHEMICALS INTO STORMWATER RUNOFF FACILITIES. AMEND MANUFACTURER'S APPLICATION RATES AND PROCEDURES TO MEET THIS REQUIREMENT, IF NECESSARY.
21. INSPECT ON A REGULAR BASIS (AT A MINIMUM WEEKLY, AND DAILY DURING/AFTER A RUNOFF PRODUCING STORM EVENT) AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL BMPs TO ENSURE SUCCESSFUL PERFORMANCE OF THE BMPs. NOTE THAT INLET PROTECTION DEVICES SHALL BE CLEANED OR REMOVED AND REPLACE BEFORE SIX INCHES OF SEDIMENT CAN ACCUMULATE.
22. REMOVE TEMPORARY ESC BMPs WITHIN 30 DAYS AFTER THE TEMPORARY BMPs ARE NO LONGER NEEDED. PERMANENTLY STABILIZE AREAS THAT ARE DISTURBED DURING THE REMOVAL PROCESS.
23. CONTROL FUGITIVE DUST FROM CONSTRUCTION ACTIVITY IN ACCORDANCE WITH THE STATE AND/OR LOCAL AIR QUALITY CONTROL AUTHORITIES WITH JURISDICTION OVER THE PROJECT AREA.
24. THE CONTRACTOR IS RESPONSIBLE FOR DESIGNATING A LOCATION WHERE CONCRETE TRUCK AND EQUIPMENT CAN BE WASHED OUT. THIS AREA SHALL NOT BE LOCATED NEAR OR DRAINING INTO A STORM DRAINAGE AREA, TREATMENT AREA, OR FACILITY.



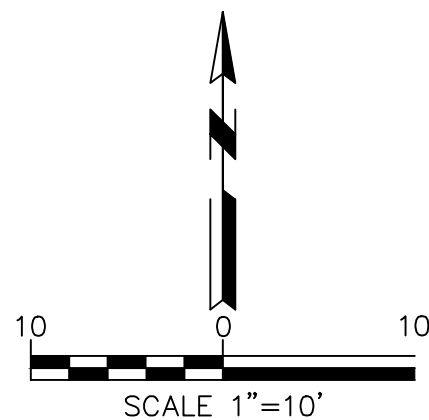
B STABILIZED CONSTRUCTION ENTRANCE
NOT TO SCALE

NOTES:

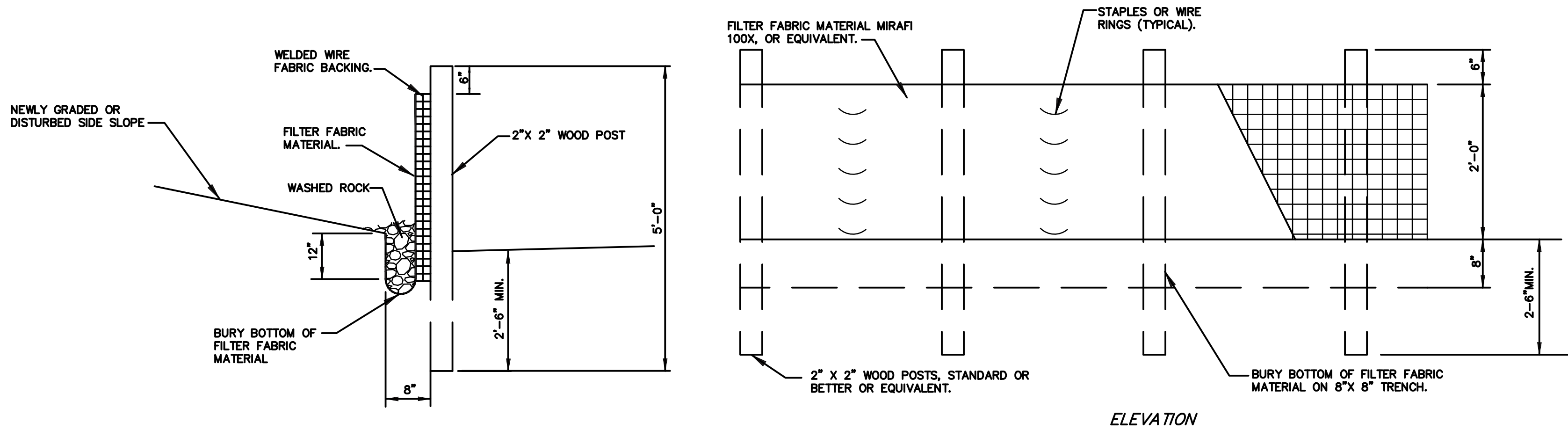
1. THE PROJECT CONSISTS OF THE CONSTRUCTION OF NEW 2700 SF LAUNDROMAT, PAVED PARKING LOT AND UTILITIES.
2. EROSION AND SEDIMENT CONTROL MEASURES WILL CONSIST OF THE INSTALLATION OF SILT FENCES.
3. SOILS AT THIS SITE ARE CLASSIFIED POORLY GRADED GRAVEL WITH SILT AND SAND (GP-GM).

E.S.C. KEY NOTES:

1. INSTALL SILT FENCE ALONG EAST PROPERTY LINE PER DETAIL A, THIS SHEET. TYP. WHERE SHOWN.
2. INSTALL ROCK CONSTRUCTION ENTRY PER DETAIL B, THIS SHEET. TYP. WHERE SHOWN.
3. PROVIDE INLET PROTECTION FOR EXISTING STRUCTURES PER DETAIL C, THIS SHEET.



EROSION/SEDIMENTATION CONTROL PLAN



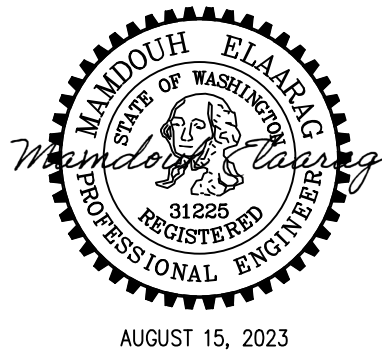
CROSS SECTION

ELEVATION

A SILT FENCE
NOT TO SCALE

C STORM DRAIN INLET PROTECTION
NOT TO SCALE

CALL BEFORE YOU DIG: 811



AUGUST 15, 2023

REV	DATE	DESCRIPTION	DWN BY	DES BY	CHK BY	APP BY
1	8-15-23	RESPONE TO CITY COMMENTS 7-19-23	MHE	MHE		
DATE OF ISSUE			CHK BY			
AUGUST 15, 2023			APP BY			

MHE ENGINEERING

9702 W. Masters Lane
Cheney, WA 99004
(509) 230-1960
MELAAARAG@YAHOO.COM

BROADWAY LAUNDROMAT
EROSION CONTROL PLAN

DRAWING NO

C3

B2310052

Broadway Laundromat

2228 West Broadway Ave, Spokane, WA 99201

Topsoil/Grading/Landscape Notes

All property lines were established from information provided by the owner/architect/engineer. Check and verify, (before bidding). This drawing may need to be adjusted to meet actual property line, boundaries or actual conditions. The Contractor and Landscape/Irrigation Contractor is responsible for reading all notes, checking plant names, numbers and sizes and inspecting the site for actual conditions prior to bidding. This drawing is not intended to constitute a legal survey.

The General Contractor is to remove asphalt, curbs, concrete as shown and site debris/elements not to remain.

The General Contractor is to install all retaining walls, provide handrails, etc., as per code, install all hardscapes, dig out and establish drainage swales to required grades, elevations and required sq. footage of swales. (to within 2" of finish grade), test all soil drainage and remedy, if needed for required drainage of soils.

PLANT LIST

Street Trees

- 3 STREET TREES – Class I Parrotia persica (2" cal.)
- 2 STREET TREES – Class II Forest Green Oak (2" cal.)

TREES

- 2 EWRC 'Excelsior' Western Red Cedar (6' high)
- 1 ARM 'Armstrong' Red Maple (2" cal.)
- 2 FLC Flowering Crab Apple 'Prairie Fire' (2" cal.)
- 1 BAC Blue Atlas Cedar (6' high)

Shrubs

- 7 EGA Emerald Green Arborvitae (4' high)
- 7 SL Shipka Laurel (2 gal.)
- 1 GFS Gold Flame Spirea (2 gal.)
- 3 DY Densiformis Yew (2 gal.)
- 5 OLL Otto Luyken Laurel (2 gal.)
- 3 RH Rhododendron (2 gal.)
- 2 CR Carpet Rose (2 gal.)
- 3 RH Rhododendron (2 gal.)
- 2 HP Hydrangea 'Tuff Stuff' Mt. Hydrangea (2 gal.)

Grasses / Perennials

- 3 LBS Little Blue Stem Grass (1 gal.)
- 2 SDL Stella d Oro Daylily (1 gal.)
- 8 KFG Karl Foerster Grass (1 gal.)
- 3 BES Black Eyed Susan (1 gal.)
- 3 JBG Japanese Blood Grass (1 gal.)
- 2 PCF Purple Cone Flower (1 gal.)
- 4 BOG Blue Oat Grass (1 gal.)
- 2 SD Shasta Daisy (1 gal.)
- 5 SG Switch Grass 'Summer Sunrise' (1 gal.)
- 1 MNS May Knight Salvia (1 gal.)

NOTE

The contractor shall plant all trees and shrubs on site according to detail V-101 and V-102.

Any substitutions of public/street trees and plant list must have written approval from Urban Forestry and City Planning and Landscape Architect, prior to installation.

Rock mulch at base of the street trees to be pulled away from the base of the tree 3" to 6", so that the root flares are visible.

Planting Notes

("do not install plants until all irrigation is installed and tested to ensure it is functioning properly")

Trees Staking – install (2) wood stakes and plastic tree tie per tree.

Soak all plants immediately upon planting. Provide a 1-year warranty/replacement of all plant material.

The Landscape-Irrigation Contractor is to provide the following:

Lawn areas are to be sodded.

Plastic Lawn edging at planting edges.

Install all Plant Material as listed (with all tree permits required)
HIRE A CITY LICENSED TREE SERVICE TO SUBMIT TREE PERMIT REQUEST TO PLANT STREET TREES PER PLAN AND ACCORDING TO V-101.
Finish grading, hand raking and provide/install topsoil 2" thick in all landscape areas.

Install 2"-3" thick of Basalt Chip Rock Mulch in all planting areas, except for the (5) street trees.
The (5) street trees to have rock mulch (2" minus rock) in a tree well at the base that is (5' diameter, free of turf and other vegetation) and the rest of the planting bed is to be Basalt Chip Rock Mulch.

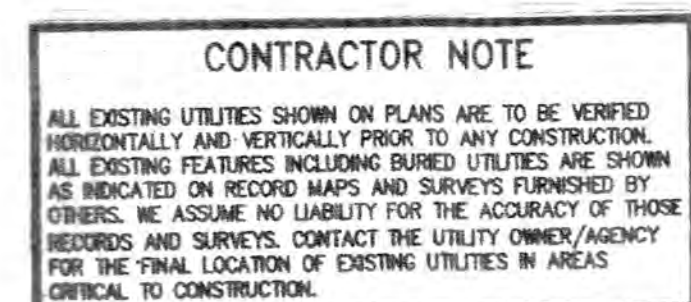
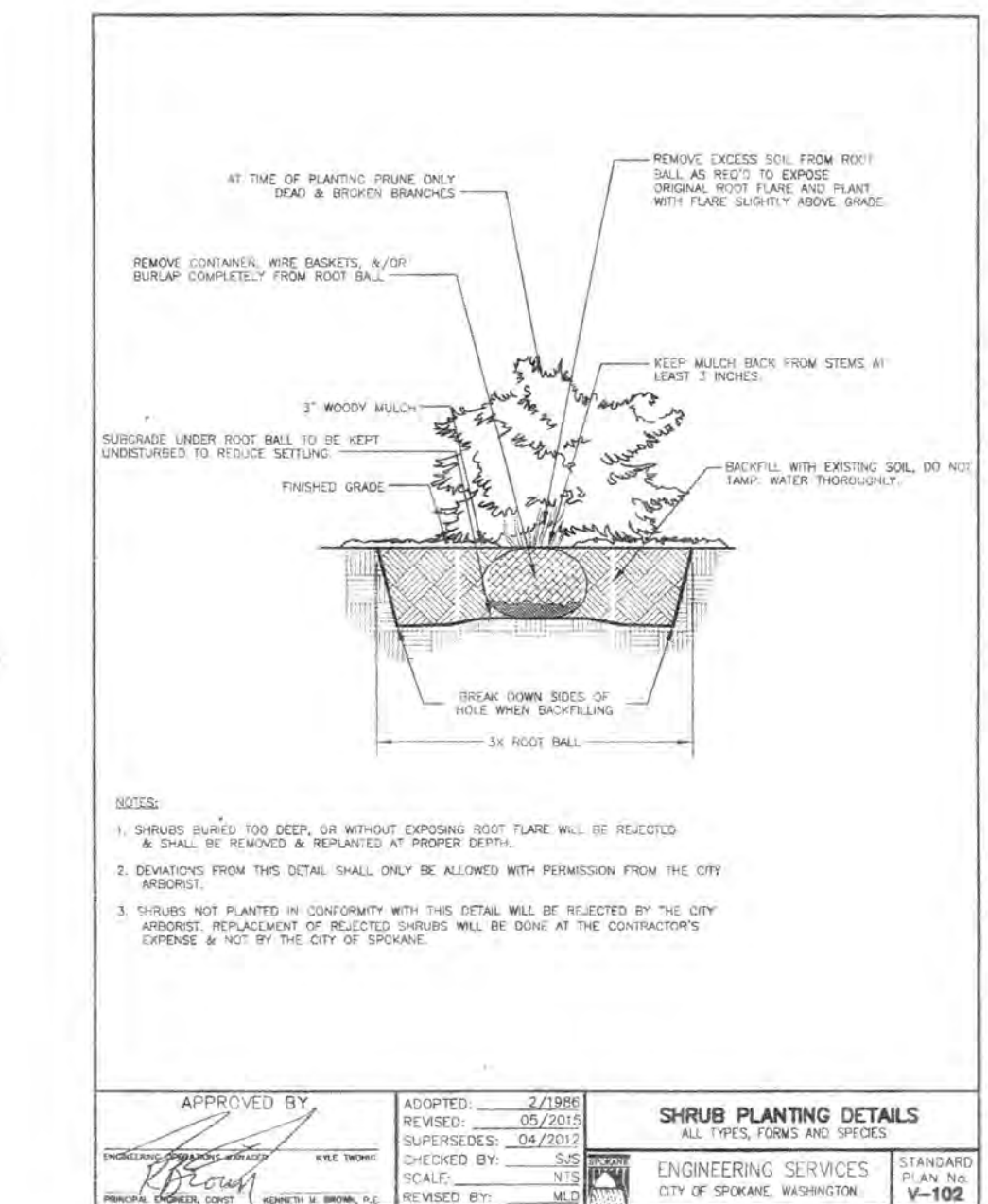
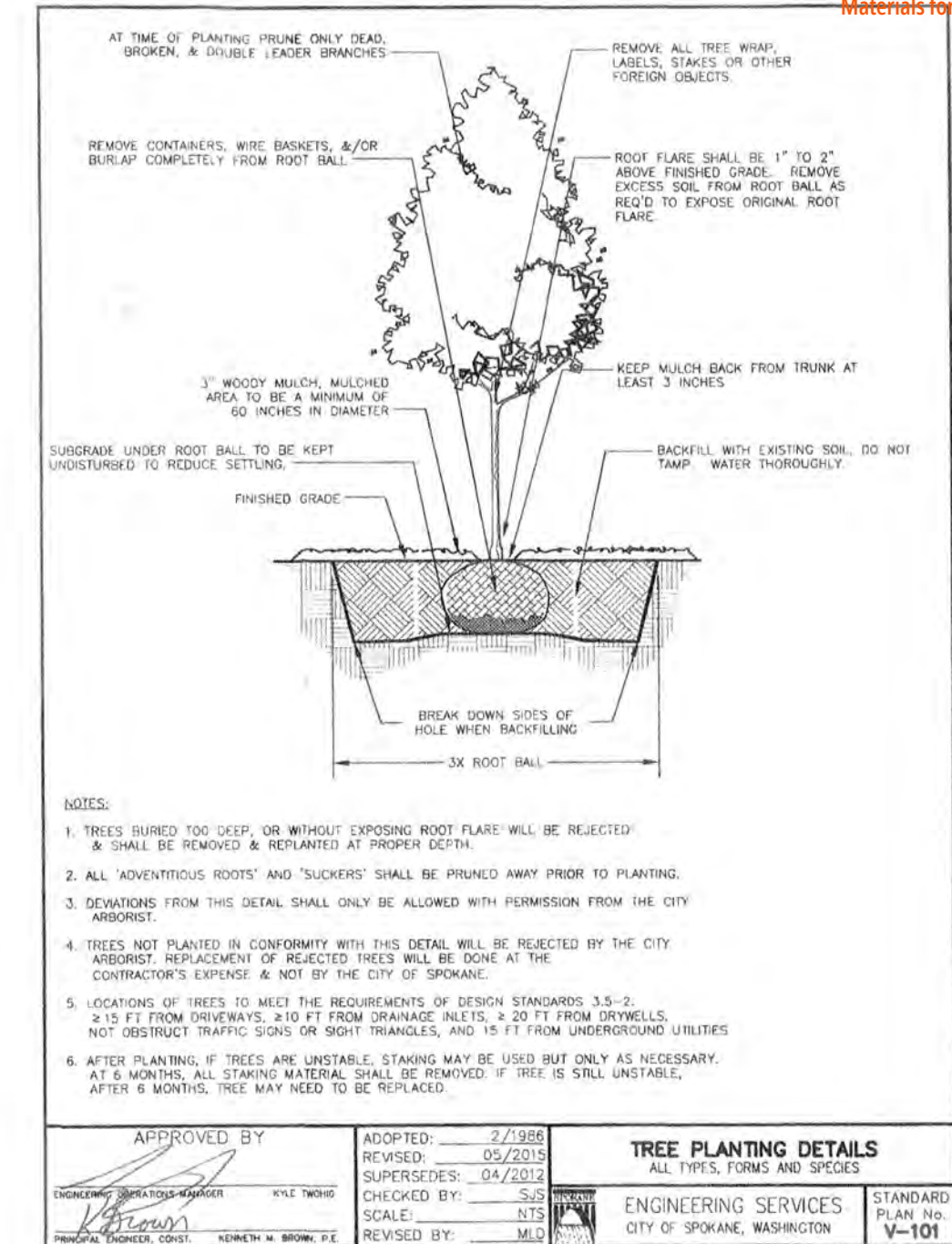
Provide a 1-year warranty of all landscape and irrigation items from the time of acceptance of work.

Other Sheets and Plans

(Check Site Plan Civil Sheets for fencing, hardscaping, curb requirements, traffic control, site utilities, such as overhead and underground gas, electric, water, phone, power lines, etc.) Call for locate of utilities before digging.

Irrigation Notes

- Provide a complete, 100% irrigation coverage to all landscape areas.
- Automatic controller, 1" double check assembly (with permit and inspection).
- Automatic valves, mainline, sleeving as needed, 1" poly lateral lines.
- 1 year warranty on the irrigation system.



Indian Trail Lawn and Landscape

Joseph Ferguson – Landscape Architect
5605 West Pacific Park Drive, Spokane, WA 99208
(509) 879-7358 / indiantrailandlandscape@yahoo.com

Broadway Laundromat

2228 West Broadway Ave, Spokane, WA 99201

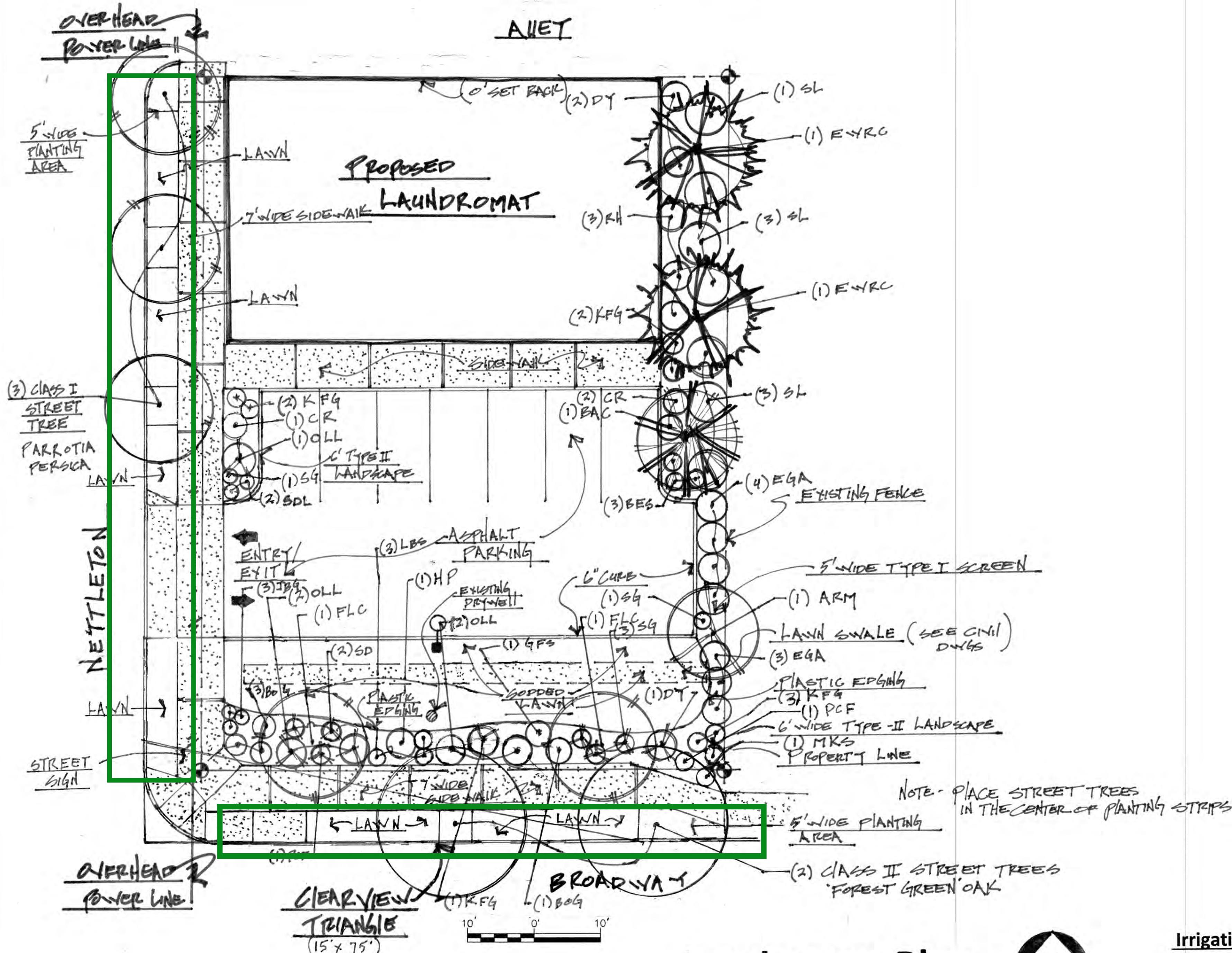
Landscape Plan

Project

DATE 4/3/23
REVIEWS
8/15/23

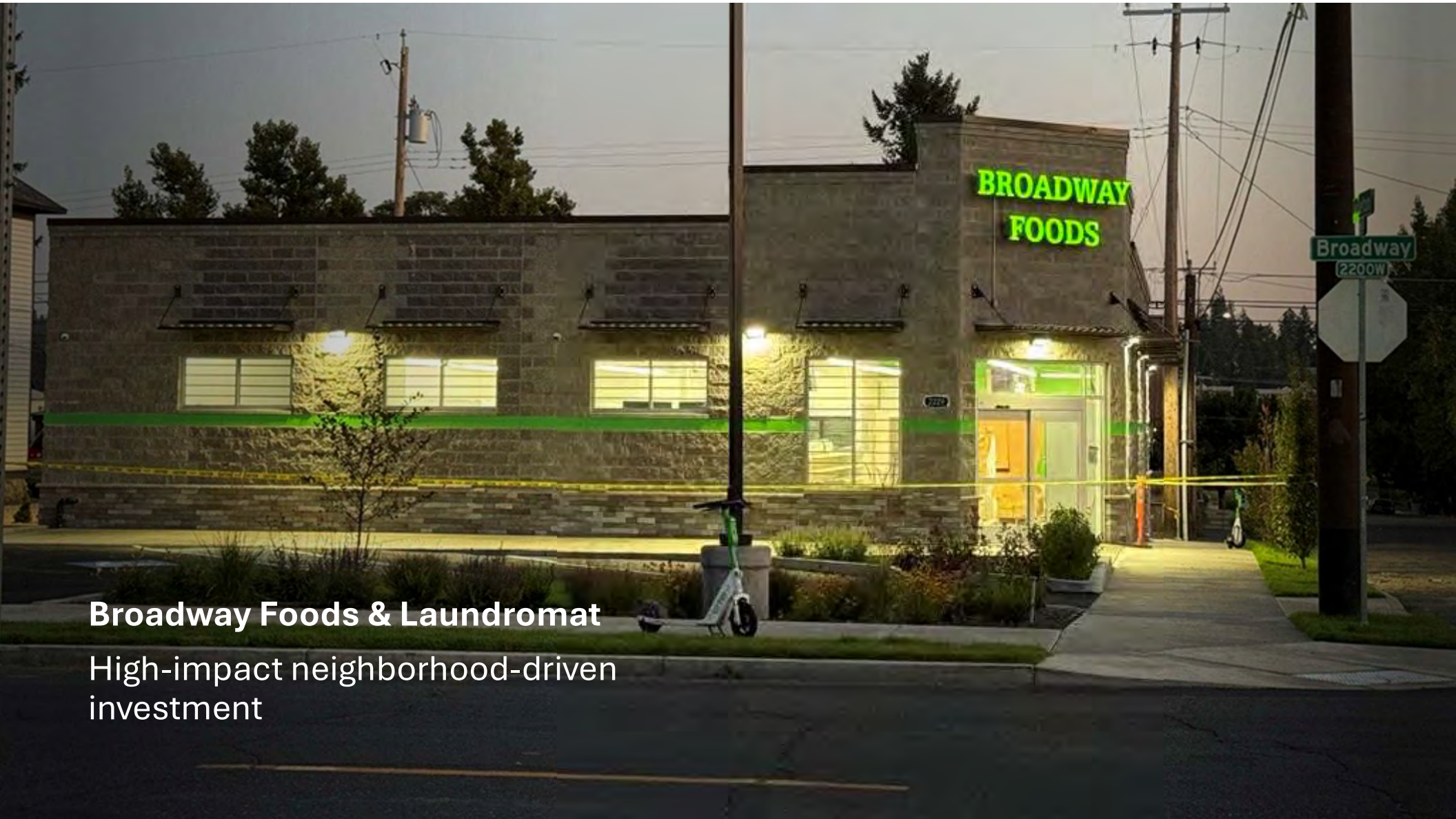
SCALE: 1"=10'
D.B. - J.F.
C.B. - J.F.
SHEET

L-1



Landscape Plan





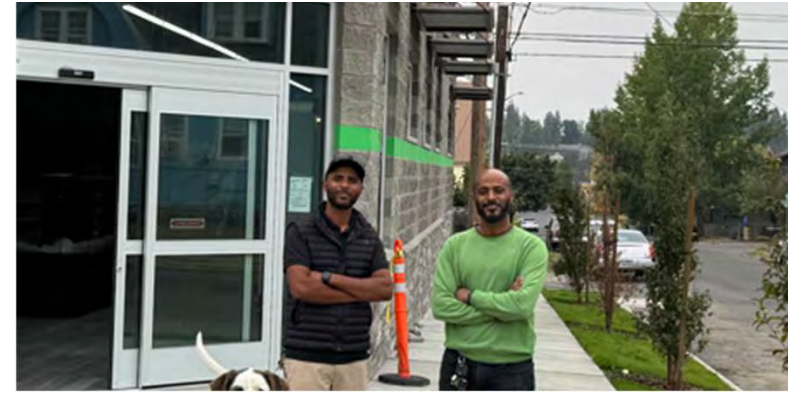
Broadway Foods & Laundromat

High-impact neighborhood-driven investment

Who We Are: Jolly & Daniel Ghebream

Business owners in West Central since
2010

*“What you do for the community, you are
doing for yourself.”*



Reclaiming Our Food Deserts

ighting food insecurity in West Central and greater Spokane means more than just adding
grocery stores

By Eliza Billingham



**Bong's Grocery and Deli owner keeps prices low, says now
not the time to make money**

March 31, 2020 | Updated Tue., March 31, 2020 at 9:39 p.m.





Our Why

Problems we are solving

- Easy access to quality food and services for all residents, including those who are low-income or without cars
- Safer egress between Kendall Yards and West Central neighborhoods and businesses
- Encouraging a shared identity between Kendall Yards and West Central
- Revitalizing a dilapidated location in the community
- Increase overall connectivity and safety



Kendall Yards

There is a lack of connectivity between...

- Approximately 1,911 residents
- Vibrant Businesses
- Centennial Trail
- Night Market
- Green space

...and...

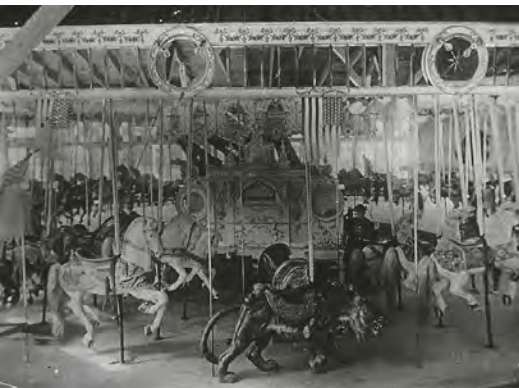




West Central

... the vibrant assets of West Central

- ~7,000+ residents
- West Central Community Center
- Local businesses and nonprofits
- Arts & culture
- Spokane heritage

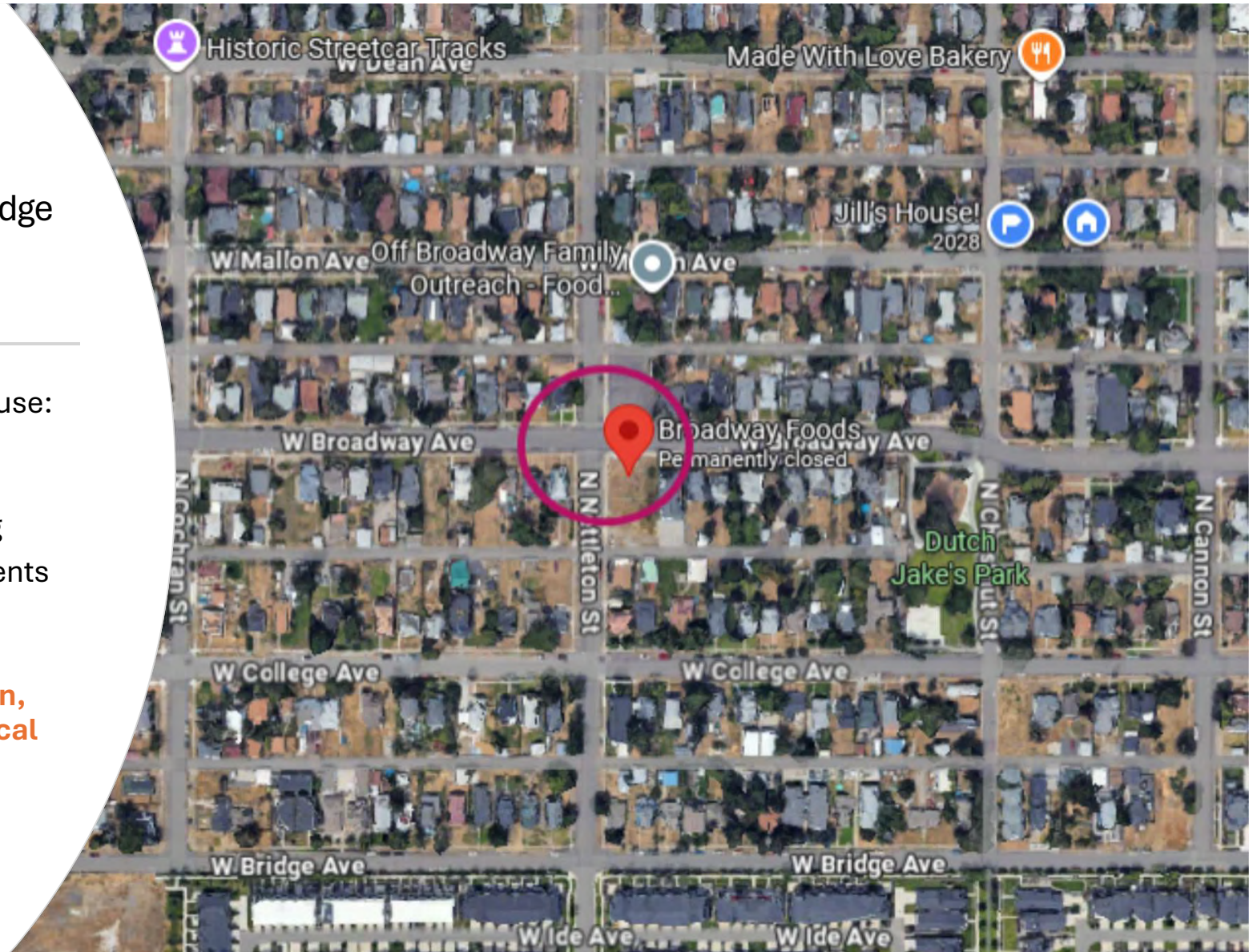


How do we cross the Bridge economically, culturally, psychologically?

This block was unsafe because:

- Vacant and dilapidated buildings
- Lack of adequate lighting
- Lack of street improvements

It is the keystone to neighborhood revitalization, connectivity, and hyper-local economic development!



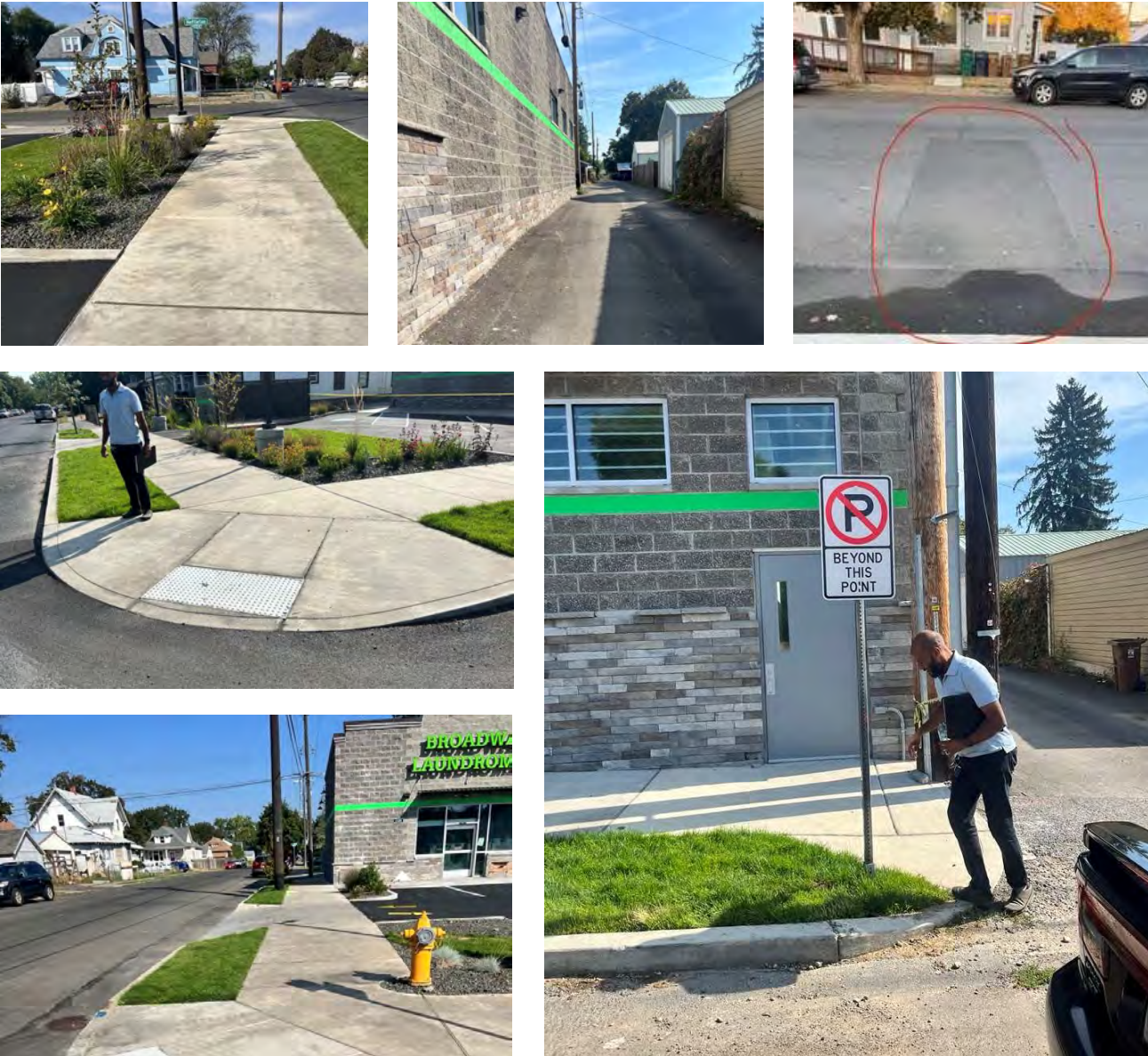
How

- **Listen to the community:** the owners have a history of serving the neighborhood through two local businesses and several affordable housing rentals.
- **Respond to local needs** for food justice, community connection, and health.
- **Revitalize blight:** Establish two businesses on the dilapidated intersection of Nettleton & Broadway, one of two connection points between the neighborhoods.
- **Improve & repair infrastructure** that increases multi-modal connectivity (bus, walking, Lime scooters, bikes, cars, etc.) and encourages additional revitalization efforts



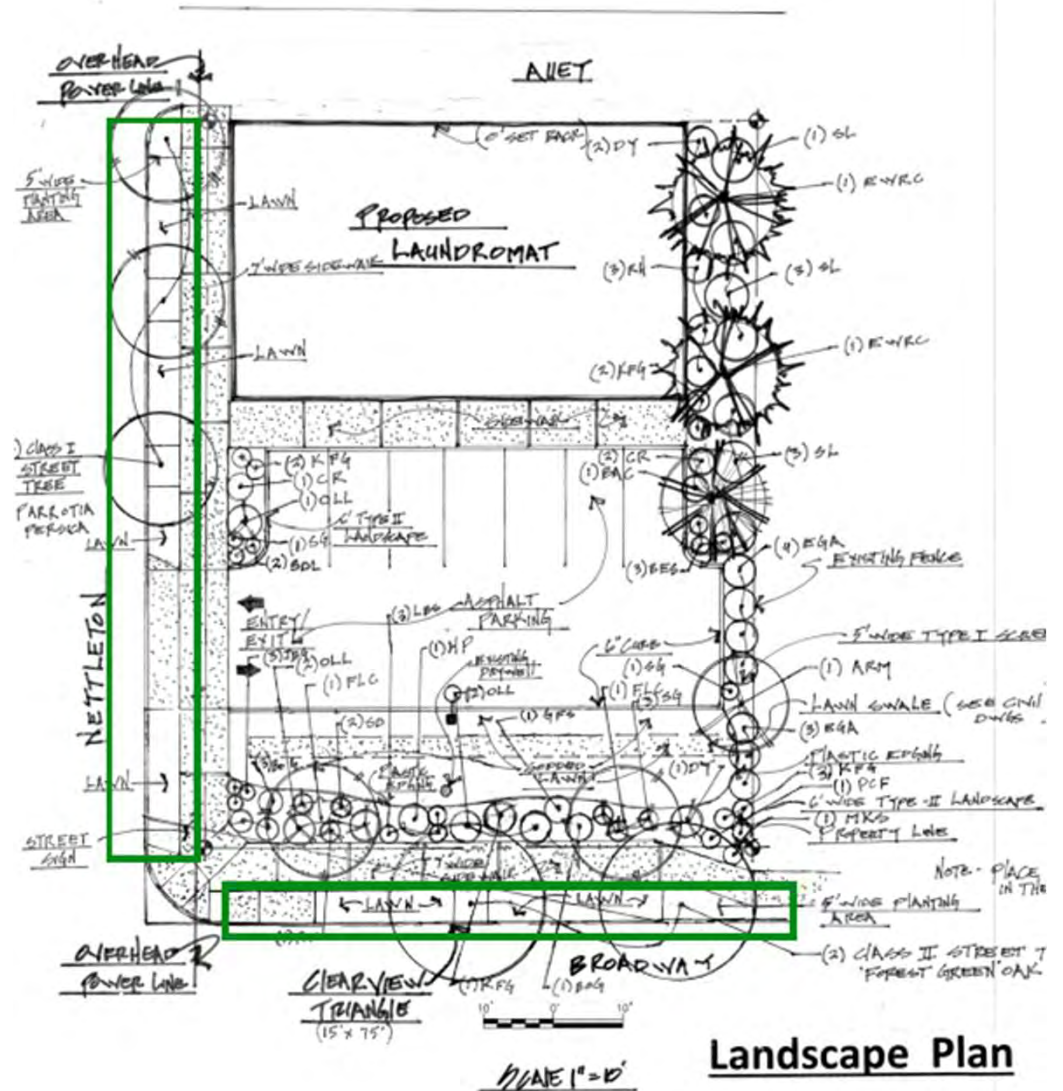
City-Owned Infrastructure Investment

- Installed new, wider sidewalks on both corners
- Moved Avista poles from the center of sidewalks to accommodate families and people with disabilities
- Upgraded infrastructure connections and signage
- Asphalted alleyways that were previously gravel
- Installed street trees and landscaping

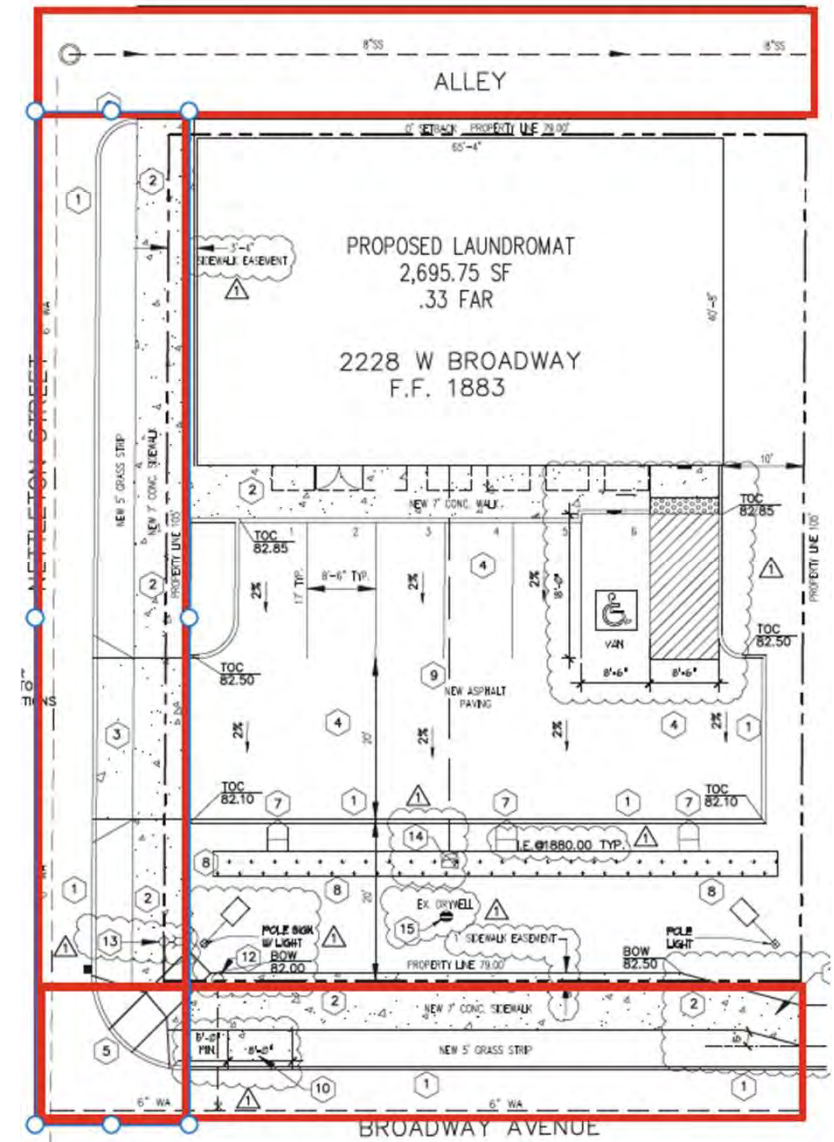
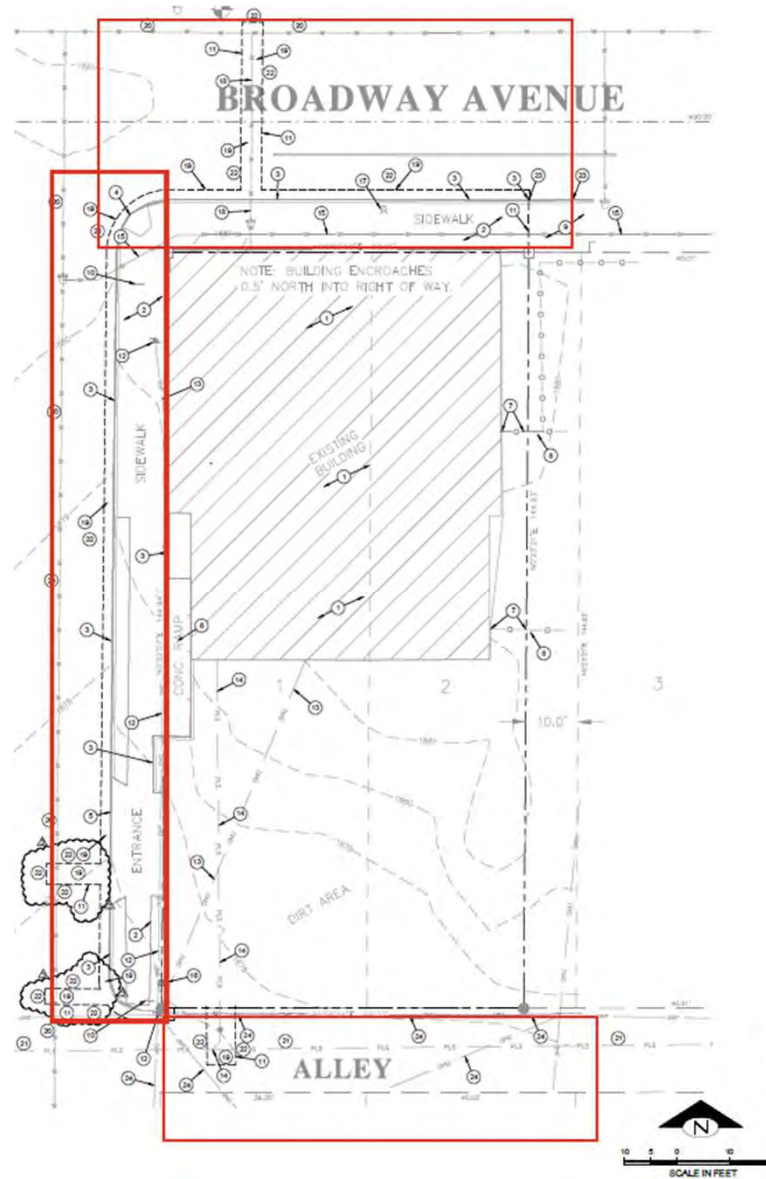




Laundromat Landscaping



Civil:





City-Owned Infrastructure Cost

Work Performed	Broadway Foods	Laundromat	Total
Remove existing sidewalk & install new concrete w/prep and compaction	\$21,700	\$23,000	\$44,700
Remove existing curb & install new concrete	\$11,950	\$11,500	\$23,450
Export all extra dirt (50% city)	\$9,000	\$0	\$9,000
New concrete driveway	\$5,500	\$4,500	\$10,000
5 Tie in new 4" domestic water	\$9,000	\$0	\$9,000
Install water services		\$12,000	\$12,000
Prep & Install asphalt in alley	\$12,000	\$12,000	\$24,000
Replace and relocate Avista poles from center of sidewalks		\$21,794	\$21,794
City Tree Permit	\$375	\$375	\$750
Street Trees (arborist)	\$3,750	\$3,750	\$7,500
Landscaping - Labor & Materials	\$4,100	\$3,400	\$7,500
Asphalt Improvements over connections	\$1,300	\$3,200	\$4,500
Parking Signage	\$520		\$520
Total	\$79,195	\$95,519	\$174,714



We also need a crosswalk!

Outcomes



- Deliver access to **quality food and services for all residents**, including low-income residents and those without cars
- **Connect and leverage** community assets and investments in Kendall Yards and West Central
- **Add 2 new businesses** to West Central
- **Revitalize** a dilapidated intersection
- **Enable** safe pedestrian pathways to bridge the neighborhoods
- **Empower local** small businesses and developers who are already connected to their communities



Jill Yotz <jillyotz@gmail.com>

Support for Jolly to Receive Available TIF Funds

1 message

Katie Salisbury <ksalisbu@gmail.com>

Mon, Sep 29, 2025 at 9:52 PM

To: jillyotz@gmail.com

Cc: James Halttunen <jhalttun@gmail.com>

Hello Jill,

Nice meeting you at Emily's barbecue on Saturday!

Thank you for your work to support Jolly Ghebreab and his business ventures in the neighborhood. Emily mentioned that you are collecting letters of support for some TIF funds being able to go to infrastructure near his businesses. We're not sure if this is exactly what you're looking for, but we just want to voice our gratitude for Jolly and his work to provide high quality, affordable, fresh food in the neighborhood. We go to Jolly Mart multiple times per week for last-minute staples or treats, and they almost always have what we need. Our daughter has recently taken pride in being able to do these trips herself, but the only thing that gives us pause about this is her needing to navigate traffic to get there. Any sort of infrastructure improvements that make his businesses more accessible and safe for community members to get to is a public good, in addition to improving walkability and safety more generally. We think that a flashing crosswalk at Nettleton and Broadway is a great idea, and we would also support improvements across Boone at Chestnut.

Take care,
Katie and James

September 29, 2025

To the NPAC:

I am writing this letter in support of Jolly Ghebreab to receive reimbursement funds from the West Quadrant TIF.

I'm a resident of the neighborhood (I live at 1838 W Gardner) and I'm the pastor of the West Central Abbey on the corner of Elm and Dean. My congregation has been involved in numerous efforts to make the West Quadrant TIF more responsive to the actual needs of West Central. We led a successful campaign in 2019 to change Washington state law so that affordable housing could be an allowable use of TIF funds. We have also been involved in efforts over the years at the city level to have the TIF extended and expanded.

Jolly invests in West Central because he cares deeply about our neighborhood. Unlike other developers who have projects all over town, Jolly chooses to invest in West Central, despite the challenges. He hires neighborhood residents. He has even worked to find his employees housing when they were in danger of losing it. When residents said they wanted a laundromat, he changed his plans to incorporate what our neighborhood needs. Without Bong's and Jollymart, West Central would be entirely reliant on My Fresh Basket for groceries, a business that is far too expensive for most of our residents. The new Broadway Foods that he is bringing to life is exactly what our neighborhood needs.

If Jolly had a large staff, like other developers, perhaps he would have known to ask about the TIF in advance, but that's not his situation. Instead, when he went to go get the required permits, and was given a list of infrastructure improvements he'd need to make, he simply took on the cost of doing those things. As someone who has personally spent a great deal of time and effort to make the TIF work better for West Central, I cannot think of a better recipient of TIF funds than Jolly Ghebreab.

I hope the NPAC does the right thing in this situation and recommends full reimbursement from the TIF to Jolly for the infrastructure improvements he's made on the corner of Broadway and Nettleton.

Sincerely,

Rev. Katy Shedlock

September 30, 2025

Neighborhood Project Advisory Committee (NPAC)
City of Spokane

Dear Members of the NPAC,

I am writing as both a neighbor in West Central and as the Executive Director of River City Youth to express my strong support for using West Quadrant Tax Increment Finance (WQTIF) funds to install a flashing pedestrian crosswalk in front of the new Broadway Foods at Broadway Avenue and Nettleton Street, as well as to provide reimbursement for the sidewalk and greenscape improvements already completed at that site.

Broadway Foods, owned by Jolly Ghebreab, is a vital addition to our neighborhood. Jollys' investment in reopening this corner store brings much-needed food access to West Central residents, many of whom walk, bike, or rely on transit to reach their daily needs. The upgrades he has already made to the property (including new sidewalks, trees, and landscaping) improve the streetscape and set a high standard for neighborhood business investment. Supporting reimbursement for those improvements would recognize this private commitment while encouraging future investment in our community.

Equally important is the installation of a flashing crosswalk at this busy intersection. As a neighbor, I know how difficult and unsafe it can be for pedestrians to cross Broadway in this area. As the leader of River City Youth, I also know how much our young people would benefit from safer access. Many of the youth we serve travel by foot or bike through this corridor. A pedestrian-activated signal would ensure their safety and promote walkability, equity, and access to essential services for all residents.

The WQTIF was designed to support infrastructure improvements that strengthen neighborhoods. This project represents an ideal use of those funds: it directly improves safety, leverages private investment, and ensures that critical community assets are accessible to everyone.

Thank you for considering this request and for your continued work to make West Central a safer, healthier, and more connected neighborhood. I urge you to prioritize this project in upcoming funding decisions.

Sincerely,

Kate Burke
Executive Director, River City Youth
West Central Neighbor
kate@rcyouth.org