



Spokane Plan Commission Agenda

October 24, 2018

2:00 PM to 3:15 PM

City Council Briefing Center

808 W. Spokane Falls Blvd., Spokane WA 99201

TIMES GIVEN ARE AN ESTIMATE AND ARE SUBJECT TO CHANGE

Public Comment Period:

3 minutes each Citizens are invited to address the Plan Commission on any topic not on the agenda.

Commission Briefing Session:

2:00 -2:15	1) Approve October 10, 2018 meeting minutes	All
	2) City Council Report	Lori Kinnear
	3) Community Assembly Liaison Report	Patricia Hansen
	4) President Report	Dennis Dellwo
	5) Transportation Sub- Committee Report	John Dietzman
	6) Secretary Report	Heather Trautman

Workshops:

2:15 – 3:15	1) Infill – Parking Requirements, Lot Area, Building Coverage, Design Standards, Parking Requirements – RMF & RHD Zones	Tirrell Black
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Adjournment:

Next Plan Commission meeting will be on November 14, 2018 at 2:00 pm

Additional Information

1) [Plan Commission Retreat Agenda](#) – October 30, 2018 - 12:00 PM, City Conference Room 5A

The password for City of Spokane Guest Wireless access has been changed: Username: COS Guest Password: tPEB6sdP

AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION: The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. The Spokane City Council Chamber and the City Council Briefing Center in the lower level of Spokane City Hall, 808 W. Spokane Falls Blvd., is wheelchair accessible and also is equipped with an infrared assistive listening system for persons with hearing loss. Headsets may be checked out (upon presentation of picture I.D.) at the City Cable 5 Production Booth located on the First Floor of the Municipal Building, directly above the Chase Gallery or through the meeting organizer. Individuals requesting reasonable accommodations or further information may call, write, or email Human Resources at 509.625.6363, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or msteinolfson@spokanecity.org. Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.

Spokane Plan Commission - Draft Minutes

October 10, 2018

Meeting Minutes: Meeting called to order at 2:00

Attendance:

- Board Members Present: Dennis Dellwo, Carole Shook, Diana Painter, Greg Francis, John Dietzman, Michael Baker, Sylvia St. Clair, Patricia Kienholz, Christopher Batten, Todd Beyreuther, Lori Kinnear; Council Liaison
- Board Members Not Present: Patricia Hansen; Community Assembly Liaison
- Staff Members Present: Heather Trautman, Nathan Gwinn, Kevin Freibott, Kim Richards

Public Comment:

- None

Briefing Session:

Minutes from the September 26, 2018, meeting approved unanimously.

1. City Council Liaison Report - Councilmember Lori Kinnear
 - A mental health forum was held at the last City Council Meeting
 - A Property Crimes Initiative task force is being assembled to provide awareness and education to the community to help citizens avoid becoming a victim of property crimes.
 - Lori will be the recipient of the Spokane Preservation Advocates Advocacy Award for her work with Historic Preservation.
 - The Pedestrian, Traffic & Transportation (PeTT) committee is working with the street department on traffic calming.
2. Community Assembly Liaison Report
 - None
3. Commission President Report - Denny Dellwo
 - Denny announced his term will end on December 31, 2018, so another president will need to be elected.
4. Transportation Subcommittee Report-John Dietzman
 - At the meeting on October 9, 2018, the Streets Standards update continued. The committee will be working on updating the 20-year list of projects for rebuilds in the City of Spokane.
5. Secretary Report-Heather Trautman
 - Retreat date options were discussed. October 30, 2018, was selected by the commissioners present.
 - Elections will be added to the agenda in December.

Workshops:

1. Infill - Parking Requirements, Lot Area, Building Coverage, Design Standards - RMF & RHD Zones - Nate Gwinn
 - Presentation and overview given.
 - Questions asked and answered.
2. Update on Joint Design Review Board/Plan Commission Meeting and Process - Kevin Freibott
 - Update provided.
 - Questions asked and answered.

Plan Commission took a brief recess between the workshop and the hearing and moved to Council Chambers.

Hearing:

1. Citywide Capital Improvement Program - Crystal Marchand
 - Presentation and overview given.
 - Questions asked and answered.

Motion:

Todd Beyreuther made a motion that the City Plan Commission recommends that the 2019-2024 Six-Year Citywide Capital Improvement Program is in conformance with the City of Spokane's Comprehensive Plan. Seconded by Mike Baker. Motion passes unanimously 10/0.

Meeting Adjourned at 4:16

Next Plan Commission Meeting is scheduled for **October 24, 2018**

DRAFT

Subject	Section	Existing code	Items for consideration	Progress update
1. Lot width/front lot line for attached houses in RTF, RMF, RHD zones	17C.110.200	36 ft. min. lot width for lots without alley parking	Reduce to 25 ft. but require min. 36 ft. if garage faces front	Draft presented to PC Workshop Sept. 26, 2018
2. Primary building height	17C.110.200 17C.110.215	<ul style="list-style-type: none"> 35 ft. roof height 30 ft. wall height RMF zone Rezone required to increase height above limits 	Increase roof height from 35 ft. to 50 ft. in RMF, RHD zones	Exception to allow up to 50 ft. with a pitched roof - draft presented to PC Workshop Sept. 12, 2018
3. Lot area for attached houses in RMF zone	17C.110.200 17C.110.360 17G.080.065	1600 sq. ft. min. or alternative residential subdivision	Change from min. 1,600 sq. ft. to none	Draft presented to PC Workshop Sept. 26, 2018
4. Lot depth for attached houses in RMF and RHD zones	17C.110.200 17C.110.360 17G.080.065	25 ft. min. or alternative residential subdivision	Change from min. 25 ft. to none	Draft presented to PC Workshop Sept. 26, 2018
5. Building coverage for attached homes in RMF and RHD zones	17C.110.200 17C.110.360 17G.080.065	50 percent (RMF) or 60 percent (RHD) for lots 5,000+ sq. ft. Under alternative residential subdivision, applied to parent site instead of individual lots, which may be as small as the building footprint	Change from max. 50 or 60 percent to 100 percent	Draft presented to PC Workshop Sept. 26, 2018
6. Design standards for attached housing in RMF, RHD zones	17C.110.310 17C.110.360 17C.110.400 through .465 17G.080.065	Multidwelling structures use different design standards than single-family attached houses and Pocket Residential Development	Use multifamily design standards for attached housing subdivisions instead of Pocket Residential	Draft presented to PC Workshop Sept. 26, 2018
7. Minimum parking requirements for attached houses in RMF, RHD zones	17C.230.130	One space per unit, plus 1 space per bedroom after 3 bedrooms	Require no parking minimum for 6 units or less	Draft presented to PC Workshop Oct. 10, 2018

Attachment A Comment Log

Substantive Public Comments Received since July 11, 2018 hearing – updated 10/19/2018

Number	Date of Comment	Name/Event	Other Info/ Draft Version/ Section/ Page	Comment Summary	Comment Start Page
18	7/26/2018	Frank, Jim		Support additional changes to landscape area and lot dimensional requirements	2
19	9/8/2018	Frank, Jim Spokane Home Builders Association		Support height increase to 50 feet without pitched roof requirement above 35 feet	4
20	9/18/2018	Rae, Bonnie		Opposed to proposal	6
21	10/2/2018	Biggerstaff, Julie		Supports proposal, some changes	8
22	10/3/2018	Garcia, Luis City of Spokane Enforcement Supervisor		Comments relate to parking requirements	10
23	10/3/2018	Hughes, Rick City of Spokane Solid Waste Commercial Supervisor		Comments relate to parking requirements	12
24	10/3/2018	Kruger, Teresa City of Spokane Parking		Comments relate to parking requirements	13
25	10/11/2018	Schram, John		Opposed to proposal	15
26	10/11/2018	Ritter, Deborah		Concerns about impacts and proposed changes should include provisions for affordable housing for new development	16
27	10/15/2018	Carlberg, Karen		Concerns with impacts to transportation, open space	18
28	10/16/2018	Loux, Jan		Supports some proposed changes, but not building height or parking	19
29	10/16/2018	Marshall, Tod and Sinisterra, Amy		Supports some proposed changes, but not building height or parking	21
30	10/16/2018	Newsom, George		Agrees with comments submitted by Jan Loux	22
31	10/17/2018	Morrissey, Barbara		Opposed to changes to lot area, concerned about parking, open space impacts	23

From: [Gwinn, Nathan](#)
To: "Jim Frank"
Cc: [Black, Tirrell](#); [Palmquist, Tami](#)
Subject: RE: Front yard requirements for driveways and landscaping
Date: Friday, July 27, 2018 3:53:01 PM

Good afternoon Jim,

Thank you for your response. We will take these suggestions under consideration as the revised draft is prepared. I will also include this message in the public record for the file.

Sincerely,

Nathan Gwinn | Assistant Planner | City of Spokane

509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

From: Jim Frank [mailto:jfrank@greenstonehomes.com]
Sent: Thursday, July 26, 2018 10:45 AM
To: Gwinn, Nathan
Cc: Black, Tirrell; Palmquist, Tami
Subject: Re: Front yard requirements for driveways and landscaping

See notes below.

Jim Frank
Greenstone Corporation

Enriched Living. Lasting Value.
www.greenstonehomes.com

On 24/07/2018, at 3:04 PM, Gwinn, Nathan <ngwinn@spokanecity.org> wrote:

Hi Jim,

Below are existing code provisions I said I would follow up on from our discussion this morning.

- [SMC 17C.110.310](#)(E)(2)(b) requires 60 percent of area between the front lot line and the building to be landscaped, with up to one-third of this area for recreational use such as patios. This design standard applies in all residential zones to detached houses on lots 40 feet or less wide, duplexes, and attached housing. This design standard is repeated in [SMC 17C.110.350](#)(F)(2)(c) for cottage housing and [SMC 17C.110.360](#)(E)(5)(b) for pocket residential development.

This probably acceptable if the landscape area is dropped to 50% in the multifamily Zones.

- [SMC 17C.110.208](#)(E)(3)(a) requires lots to be configured so that new garage walls facing the street are limited to 50 percent of the length of the street-facing building façade. This standard applies in the RA, RSF, RSF-C, and RTF zones to garages on lots that are 36 feet or less wide and accessory to houses, attached houses, manufactured homes, and duplexes. Intent statements in the same section cite compatibility with existing lots, and avoiding having the garage door as the dominant feature of the front of a house. ***Note this requirement does not apply in the RMF or RHD zones.***

<image007.jpg>

Since this does not apply in RMF and RHD it is apparently not an issue.

- [SMC 17C.230.145](#)(C)(4)(b) limits driveways to 20 percent of the land area between the front lot line and the front building line, with an exception for at least a 9-foot-wide vehicle area. This requirement applies to residential uses in areas including the RTF, RMF, and RHD zones. A related intent statement in this section states that the size and placement of vehicle parking areas are regulated in order to enhance the appearance of neighborhoods.

This needs to be modified as part of the modification of lot dimensional requirements. If you limit the garage width to not more than 60% of the lot width (not building facade) on any lot smaller than 36 feet.

Sincerely,

<image001.jpg>

Nathan Gwinn | Assistant Planner | Planning & Development

509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

<image002.png> <image003.png> <image004.png>

Gwinn, Nathan

From: Arthur Whitten <AWhitten@shba.com>
Sent: Monday, September 10, 2018 10:59 AM
To: Jim Frank; Black, Tirrell
Cc: Stuckart, Ben; Trautman, Heather; Gwinn, Nathan; Rob Brewster; Josh Hissong; Gary Bernardo; Wolff, Charlie
Subject: RE: Infill Schedule at Plan Commission COS

SHBA agrees with these comments. It is inherently restrictive to regulate RMF and RHD like single family zonings or to only permit certain construction types through narrow exceptions in the residential zonings designated for the highest densities.

From: Jim Frank [mailto:jfrank@greenstonehomes.com]
Sent: Saturday, September 08, 2018 2:31 AM
To: Black, Tirrell
Cc: Stuckart, Ben; Trautman, Heather; Gwinn, Nathan; Arthur Whitten; Rob Brewster; Josh Hissong; Gary Bernardo; Charlie Wolff
Subject: Re: Infill Schedule at Plan Commission COS

Tirrell. I am out of town and will not be able to attend the September 12th meeting. Regarding the building height language (increase to 50 feet): It would be far better to change the number in table 17C rather than a new section requiring a pitched roof. I have had both an architect and a developer say this language is preventing flat roof 3 story building over a parking podium. Most parking podiums are not fully under ground. There is no apparent reason for this limitation of requiring pitched roofs over 35 feet in MF zones. We need to be encouraging both density and structured parking.

Sustainable mobility and transit require higher density. We need to “encourage” higher densities not just permit it under limited circumstances. Design is important and should be addressed in “design guidelines” uniformly applied.

Jim

Jim

Sent from my iPhone

On Sep 7, 2018, at 11:20 PM, Black, Tirrell <tblack@spokanecity.org> wrote:

Hi Jim,

We have a document prepared for the Plan Commission meeting next week that we wanted to share with you. I have also included the infill packet for the PC Agenda. That should be going out soon – I believe you are on the distribution list.

You can see the topics that we will be covering at the various plan commission workshops prior to November. At the upcoming Sept 12 meeting there is only 15 minutes to present so Nate is going to talk about the schedule and present that actual language re the height (also in the PC Packet).

Additionally Nate is working on community outreach at the Logan block party on Sept 13 and the Cliff-Cannon block party on September 15.

If you have time and/or interest in checking in with us on how the draft is progressing, please let us know a day/time that works to meet.

Sincerely,

<image005.jpg>

Tirrell Black, AICP | City of Spokane | Associate Planner

509.625-6185 | main 509.625-6300 | tblack@spokanecity.org | spokanecity.org

<image006.png> <image007.png> <image008.png>

This email is subject to Washington State Public Records Act, Chapter 42.56 RCW, and may therefore be subject to public disclosure.

From: Jim Frank <jfrank@greenstonehomes.com>

Sent: Saturday, August 25, 2018 10:10 AM

To: Gwinn, Nathan <ngwinn@spokanecity.org>; Black, Tirrell <tblack@spokanecity.org>; Trautman, Heather <htrautman@spokanecity.org>; Kinder, Dawn <dkinder@spokanecity.org>; Rob Brewster <rob.brewster@gmail.com>; Arthur Whitten <AWWhitten@shba.com>

Subject: Townhomes

Dealing with the entire urban infill code review process has been very frustrating. The photos below show the type of MF development permitted with virtually no regulatory barriers. However, regulatory barriers prevent development of townhomes for homeownership simply because subdivision is required.

You will get the type of development your code permits. This is what your code has permitted. I'm really glad we have the street frontage, lot size and site coverage standards to protect the neighborhood.

Jim

<image001.png>

<image002.png>

<image003.png>

<image004.png>

Jim Frank
Greenstone Corporation

Enriched Living. Lasting Value.
www.greenstonehomes.com

<PC_Infill_Schedule_Sept12_2018.docx>

<2018-09-12-pc-agenda-packet-infill-code-workshop.pdf>



RECEIVED

SEP 18 2018

PLANNING & DEVELOPMENT

COMMENT SHEET

Code Amendments for Attached Houses and Multifamily Zones

September 2018

For more project info visit:

my.spokanecity.org/projects/infill-housing-strategies-infill-development/

Name: Bonnie Rae

ADDRESS: 72518 Morton St.

PHONE NUMBER: 489-5116

E-MAIL CONTACT: The last thing this neighborhood needs is more rentals. -

When I bought this house 45 years ago you could walk three or four blocks in any direction and not see a rented property. - Lawns were greener, people knew each other, litter was practically non-existent... all the things you would expect when residents have a vested interest in the place they live. - And that, simply put, is a mortgage. - I've had it up to here with all this talk about "Affordable Housing." - You folks -- or people like you -- are responsible for draconian monstrosities like the North/South Freeway boondoggle that has already bulldozed 600 affordable homes (and counting!). - Homes that low-income people could actually buy. - You seem to think cramming more rentals in is going to solve that problem? - So developers get richer? - And these low-income people you pretend to be so concerned about get to pay rent... which even you will have to agree is akin to lighting a wad of money afire with a Bic lighter every month. - No. - A thousand times No. -

I've read over this propaganda you folks were handing out at

Please feel free to share your questions, comments or concerns with us!

(over)

Postal Mail – fold this comment card in thirds, add postage and drop in the mail

Phone – call us at #625-6983

E-mail – write to us at ngwinn@spokanecity.org

Thank you... We look forward to hearing from you!

the Logan Block Party. - I honestly had to read parts of it two and three times to believe what I was seeing. - Zoning laws that have stood ~~a~~ a century or more -- and served Spokane well -- are to be trashed? - Three story apartment houses? - Shorter distances between buildings? - Larger "foot prints" on conventional city lots? - Fewer drive-ways? Are you people kidding? - Go ahead and do it in your own neighborhoods. - I'm pretty certain none of you planning commission people live where I live. -

And before I run out of writing space, let me say one more thing. - Calling tacky apartments "Townhouses" doesn't make them any less tacky. - They're still apartments. - And they'd still be an abomination in this old neighborhood where a lot of us still take some pride in our property. - Our property. - Not some landlord's. -



Planning & Development Services
808 W. Spokane Falls Blvd.
Spokane, WA 99201-3343

Thanks for letting me vent. - I'm sure it won't make one iota of difference. -

Xannie Rae

City of Spokane
Planning & Development Services
808 W. Spokane Falls Blvd.
Spokane, WA 99201-3343

Gwinn, Nathan

From: Gwinn, Nathan
Sent: Tuesday, October 2, 2018 3:42 PM
To: 'JULIE BIGGERSTAFF'
Subject: RE: proposed infill revisions comments

Hi Julie,

Thank you for your comment. I will add it to the public record for this file.

Sincerely,
Nathan Gwinn



Nathan Gwinn | Assistant Planner | Planning & Development
509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org



From: JULIE BIGGERSTAFF <rbiggerstaff@comcast.net>
Sent: Tuesday, October 2, 2018 11:30 AM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Subject: proposed infill revisions comments

Hi Nathan,

I'm a member of the Browne's Addition Neighborhood Council (BANC) and am writing in general support of the in-fill revisions, WITH the caveat that they won't work well for all neighborhoods, specifically those of us with many historic homes and narrow streets where parking, car vandalism and snow removal are huge issues. I would wish that the city would be more amenable to working with neighborhoods for historic protection of structures, so as to protect the investment of folks who are already residents/owners. Browne' Addition is, as you may know, working for a local historic designation, to help incentivize owners to fix up, rather than tear down, historic structures, to keep the visual fabric and structural history of the neighborhood intact. Unfortunately, the city is requiring a 50%+1 vote, with non-votes (un-returned ballots) counted as a 'no'. The members of city council and the mayor did not achieve their offices with this type of voting system, yet that's what is required of us, a neighborhood with a high number/percentage of landlords who don't live in the city, let alone the neighborhood. We are frustrated as a neighborhood at the daunting task of getting landlords who won't even take care of their properties, to vote.

The revisions as I understand them, would not be helpful to the quality of life in our neighborhood, but may really help other neighborhoods revitalize, so again, my feelings are mixed. Putting more cars on our streets, particularly the N/S streets is hazardous due to limited emergency vehicle access being blocked (come drive around the neighborhood to see what I mean) and building large scale buildings that 'dwarf' the other buildings within a neighborhood like ours blocks people's view and constricts sense of space.....in a densely built neighborhood. Having good set-backs, porches, etc., even with dense building, helps maintain a sense of space...you'll feel the difference when you drive by the new development on Chestnut by Coeur d'Alene Park and the one of Coeur d'Alene Ave overlooking Latah creek; both have small set-backs and received exemptions to building height restrictions.

I do believe that vacant lots could and should be used to build affordable housing and that use of current city infrastructure (garbage collection, sewer and water) rather than further urban sprawl makes good economic sense; however, I would point out that in BA, of the two recent developments that resulted in historic structures being torn down, neither resulted in 'affordable housing'. If these revisions are going to pass, I believe there MUST be a requirement with them that a certain % of the units built be truly affordable and available to, for example, section 8 holders. I also believe that further exemptions to the revisions as passed should not be further possible; people trying to make money are always trying eke out just that little bit more.....

Can these be based on true in-fill only (vacant lot), versus the situation we will continue to have if the historic district project is not approved (tear down and re-build)?

Greed and money are powerful motivators and we know from current landlords in the neighborhood, that there are property owners in BA that would tear down anything to put in a 10 story apartment complex if they could get away with it.

Thanks much,

Julie Biggerstaff

Gwinn, Nathan

From: Garcia, Luis
Sent: Wednesday, October 3, 2018 10:50 AM
To: Gwinn, Nathan; Kruger, Teresa
Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Nathan,

Following up on our meeting for the Attached Housing provisions pertaining to parking changes. Parking would like to note that the relaxation of off-street parking requirements will certainly bring additional enforcement for the Parking Enforcement Officers as the struggle for access to the parking that is adjacent and in the immediate vicinity will increase from existing conditions. While it is understood that the intent is to maximize the land use and with the increase in mass transit may alleviate this conflict, the parking program will have an increase in budget needs to show attention to complaints as they are submitted. Parking therefore requests that this impact be noted in your staff report on potential budget impacts.

Let me know if you have any questions.

Luis Garcia CBO, CSBA | City of Spokane | Enforcement Supervisor
509.625.6850 | lgarcia@spokanecity.org | spokanecity.org

-----Original Message-----

From: Gwinn, Nathan
Sent: Tuesday, October 2, 2018 10:24 PM
To: Garcia, Luis <lgarcia@spokanecity.org>; Kruger, Teresa <tkruger@spokanecity.org>
Subject: FW: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Hey Luis and Teresa, attached is the PPT presentation and draft options 1 and 2 (underlined text in Word document on the bottom of page 1 and top of page 2-paragraph F). I would be pleased to share any comments you have with the Plan Commission. I'll be submitting their packet at the end of business Wednesday.

Thanks,
Nate

From: Gwinn, Nathan
Sent: Tuesday, October 02, 2018 10:30 AM
To: Hughes, Rick
Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Hi Rick,

Thanks for the call.

For reference, attached is April's powerpoint presentation from the meeting, as well as the draft text (bottom of page 1 and top of page 2) that the Plan Commission will review in the meeting next week.

Thanks again,

Nate Gwinn

From: Gunderson, April

Sent: Wednesday, September 26, 2018 5:06 PM

To: Gwinn, Nathan <ngwinn@spokanecity.org>; Garcia, Luis <lgarcia@spokanecity.org>; Trautman, Heather <htrautman@spokanecity.org>; Palmquist, Tami <tpalmquist@spokanecity.org>; Black, Tirrell <tblack@spokanecity.org>; Becker, Kris <kbecker@spokanecity.org>; Schenk, Andrew <aschenk@spokanecity.org>; Turner, Bob <bturner@spokanecity.org>; Kaatz, Robert <rkaatz@spokanecity.org>

Cc: Kruger, Teresa <tkruger@spokanecity.org>

Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Hi all,

Thank you for attending this meeting and providing feedback. Attached is the PowerPoint from today. If you have any comments, please provide them to Nathan Gwinn by Wednesday, October 3 at 5pm.

Thank you!

[City-Logo_2-color_jpg]

April Gunderson | Project Planner | Neighborhood and Planning Services

509.625.6965 | fax 509.625.6013 | agunderson@spokanecity.org<mailto:agunderson@spokanecity.org> | my.spokanecity.org<https://my.spokanecity.org/>

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-----Original Appointment-----

From: Gwinn, Nathan

Sent: Monday, September 17, 2018 9:34 AM

To: Gwinn, Nathan; Garcia, Luis; Gunderson, April; Trautman, Heather; Palmquist, Tami; Black, Tirrell; Becker, Kris; Schenk, Andrew; Turner, Bob; Kaatz, Robert

Subject: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

When: Wednesday, September 26, 2018 1:00 PM-2:00 PM (UTC-08:00) Pacific Time (US & Canada).

Where: City Conference Room 3B

Section 17A.020.010<<https://my.spokanecity.org/smc/?Section=17A.020.010>>(AR) Attached Housing.

Two or more dwelling units that are single-family residences on individual lots attached by a common wall at a shared property line. These include:

1. Townhouses,
2. Row houses, and
3. Other similar structures

Gwinn, Nathan

From: Hughes, Rick
Sent: Wednesday, October 3, 2018 1:56 PM
To: Gwinn, Nathan
Subject: Reducing Minimum Parking Standards

Nathan,

The following are important issues and concerns that Solid Waste Collection has with reducing attached housing minimum parking requirements in multi-family zones:

- 1) Reduction in Automation Efficiency: In 1997, the City of Spokane chose to move from two (2) man manual rear-loading routes to one (1) man automated routes. To be successful in keeping rates low and reducing employee injuries, the trucks must be able to drive along the curb. Street parking requires the driver to exit the vehicle and manually move the container within reach of the truck. This increases injuries and decreases the amount of work each truck can do.
- 2) Reduction in Service Delivery: In areas such as Browne's Addition and Gonzaga where parking is inadequate now, there are instances where the vehicles along the curb are so close together that the driver cannot get the containers out in between them for collection. The residents get upset and do not want to pay the return trip charges because the vehicles are not theirs and they have no control over where people park.
- 3) Parking Enforcement Issues: Currently in areas with high amounts of street parking, illegal parking is an issue. When vehicles park closer to an alley entrance than legally allowed, drivers cannot turn out of the alley to exit. In areas where there is currently not enough street parking for the amount needed, there is often illegally parked vehicles in the alleys. In these cases, we either do not collect waste in those alleys or the drivers have to back out into traffic with limited visibility.
- 4) Snow Removal Issues: Berms created by plowing in residential areas often narrow the streets. Vehicles parked alongside the berms must be far enough away to open their doors. At times residential streets with parking on both sides can become unpassable.

If you need additional information or have any questions, please contact me.

Thank you,

RICK HUGHES
CITY OF SPOKANE SWCD
COMMERCIAL SUPERVISOR
509-625-7871
509-343-9652
RHUGHES@SPOKANECITY.ORG

Gwinn, Nathan

From: Kruger, Teresa
Sent: Wednesday, October 3, 2018 4:15 PM
To: Gwinn, Nathan
Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Nate,

I see issues with both options.

When parking areas are reduced it only increases parking issues. More people will park illegally and unfortunately instead of changing behavior it becomes finger pointing to city govt. that allowed the reduction of parking spaces.

It is a no win situation.

Thank you.

Teresa

-----Original Message-----

From: Gwinn, Nathan
Sent: Tuesday, October 2, 2018 10:24 PM
To: Garcia, Luis <lgarcia@spokanecity.org>; Kruger, Teresa <tkruger@spokanecity.org>
Subject: FW: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

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Thanks,
Nate

From: Gwinn, Nathan
Sent: Tuesday, October 02, 2018 10:30 AM
To: Hughes, Rick
Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Hi Rick,

Thanks for the call.

For reference, attached is April's powerpoint presentation from the meeting, as well as the draft text (bottom of page 1 and top of page 2) that the Plan Commission will review in the meeting next week.

Thanks again,

Nate Gwinn

From: Gunderson, April
Sent: Wednesday, September 26, 2018 5:06 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>; Garcia, Luis <lgarcia@spokanecity.org>; Trautman, Heather <htrautman@spokanecity.org>; Palmquist, Tami <tpalmquist@spokanecity.org>; Black, Tirrell

<tblack@spokanecity.org>; Becker, Kris <kbecker@spokanecity.org>; Schenk, Andrew <aschenk@spokanecity.org>; Turner, Bob <bturner@spokanecity.org>; Kaatz, Robert <rkaatz@spokanecity.org>
Cc: Kruger, Teresa <tkruger@spokanecity.org>
Subject: RE: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

Hi all,

Thank you for attending this meeting and providing feedback. Attached is the PowerPoint from today. If you have any comments, please provide them to Nathan Gwinn by Wednesday, October 3 at 5pm.

Thank you!

[City-Logo_2-color_jpg]

April Gunderson | Project Planner | Neighborhood and Planning Services

509.625.6965 | fax 509.625.6013 | agunderson@spokanecity.org<mailto:agunderson@spokanecity.org> | my.spokanecity.org<https://my.spokanecity.org/>

[FindUs]<<http://www.spokanecity.org/>>[LikeUs]<<http://facebook.com/spokanecity>>[FollowUs]<<http://twitter.com/spokanecity>>

-----Original Appointment-----

From: Gwinn, Nathan

Sent: Monday, September 17, 2018 9:34 AM

To: Gwinn, Nathan; Garcia, Luis; Gunderson, April; Trautman, Heather; Palmquist, Tami; Black, Tirrell; Becker, Kris; Schenk, Andrew; Turner, Bob; Kaatz, Robert

Subject: Attached Housing (see notes) in Multifamily Zones - Parking Requirements

When: Wednesday, September 26, 2018 1:00 PM-2:00 PM (UTC-08:00) Pacific Time (US & Canada).

Where: City Conference Room 3B

Section 17A.020.010<<https://my.spokanecity.org/smc/?Section=17A.020.010>>(AR) Attached Housing.

Two or more dwelling units that are single-family residences on individual lots attached by a common wall at a shared property line. These include:

1. Townhouses,
2. Row houses, and
3. Other similar structures

From: Gwinn, Nathan
To: ["Office of John Schram"](#)
Cc: [Patricia Hansen](#)
Subject: RE: infill feedback
Date: Thursday, October 11, 2018 1:49:00 PM

Good afternoon Mr. Schram,

Yes, I will forward your comments to the Plan Commission and they will be made part of the public record for this file. Thank you for submitting them.

Sincerely,

Nathan Gwinn | Assistant Planner | Planning & Development
509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

-----Original Message-----

From: Office of John Schram <john.schram@lpl.com>
Sent: Thursday, October 11, 2018 1:04 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Cc: Patricia Hansen <patricia@pahansen.com>
Subject: infill feedback

Nate, I wanted to follow up from a presentation you did a while back to the Cliff Cannon neighborhood about the city's push to densify neighborhoods. Clearly the assumption that allowing bigger buildings on a neighborhood parcel will by default make any unit "affordable" is laughable at best and purposefully deceptive in the worse case. The infill changes proposed will not only NOT accomplish the desired affordable housing goals but will lead to an increase in street related parking issues in addition to allowing traditional single family home neighborhoods to retain their original and still desired feel. I as a business owner and neighborhood activist in the Cliff Cannon neighborhood respectfully ask the City of Spokane to cease this epic failure in the making. My general understanding is that having a denser neighborhood is not a goal of neighborhoods, only the city planners, politicians, and developers.

It also does not escape my notice that neighborhoods are now having to go down the path, with the city, to designate themselves as historic in nature just to try and stave off these types of efforts. I will encourage the planning commission as well to vote NO on these proposals as well and will trust you are able to forward my comments to them.

In your service,
John A. Schram, CFP®
Registered Principal
LPL Financial
Member FINRA/SIPC
917 S. Monroe St.
Spokane, WA 99204
509.328.5627
509.328.4634 (f)

Securities offered through LPL Financial Member FINRA/SIPC

From: Gwinn, Nathan
To: ["Deborah Ritter"](#)
Subject: RE: survey for code changes
Date: Friday, October 12, 2018 6:49:00 AM

Hi Deb,

Thanks for your message and feedback about the survey. I will include this comment in the public record for the file.

The options presented mirror the options the City Plan Commission is discussing, and responses should help Commission members evaluate the proposals as they prepare to make a recommendation to the City Council.

The connection to affordability is in the supply as a whole, and increasing the variety of choices and potentially smaller dwellings in all neighborhoods. For information, please see the vision, values, goals and policies starting on page 4 in the City's Comprehensive Plan, Chapter 6:

<https://static.spokanecity.org/documents/shapingspokane/comprehensive-plan/chapter-6-housing.pdf>

The City is developing a webpage to provide information on local supply and demand. I can provide that link to you when it goes online.

Sincerely,

Nathan Gwinn | Assistant Planner | Planning & Development

509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

From: Deborah Ritter <yuccaplants@gmail.com>
Sent: Thursday, October 11, 2018 2:07 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Subject: survey for code changes

Hi Nate,

I took the survey for code changes and found it pretty biased to choosing code changes vs not choosing them. Is the point that the code changes will happen, regardless, and the city is trying to get input on which changes they should make?

For instance, this question:

The City is looking at parking requirements in multifamily areas for townhouses. Should the City allow less parking for smaller homes, or no parking for up to six homes?

There was no checkbox option to choose "none" or "neither" -- there was just an option to choose "other" and write in an explanation.

For the first question about townhouses being narrower, there was no option asking if townhouses should be allowed to be narrower -- just once they are allowed, how do people want to see the design.

The logic for these code changes seems unsubstantiated in the materials I've seen. I would like to see data showing that smaller, taller buildings with less parking make housing more affordable. In order for available housing to drive prices down, the market must be flooded with available housing -- how is that expected to occur with occasional, sporadic infill development projects in a rapidly growing city?

The poster below presents data about a lack of affordable housing in Spokane. But it does not show data on how these code changes will create affordable housing. As far as I could see, ***there is no code in the proposal that requires the housing to be affordable if developers are allowed to build taller, smaller buildings with less parking. I am very concerned that the codes will simply create smaller, higher units with more street congestion due to lack of parking -- and the pricing for these units will still not be affordable for most.***

<https://static.spokanecity.org/documents/projects/infill-housing-strategies-infill-development/2018-09-25-handout-and-posters-attached-housing-multifamily-zones.pdf>

Thank you,
Deb

--

"they don't want tunas with good taste, they want tunas that taste good"-MLR

From: Gwinn, Nathan
To: ["Karen Carlberg"](#)
Subject: RE: Comments on infill
Date: Monday, October 15, 2018 11:04:00 AM

Hi Karen,

Thank you for your message. I will include these comments in the public record for the file.

Sincerely,

Nathan Gwinn | Assistant Planner | Planning & Development

509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

From: Karen Carlberg <karencarlberg@comcast.net>
Sent: Friday, October 12, 2018 7:42 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Subject: Comments on infill

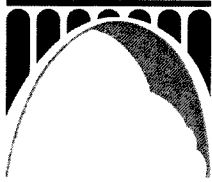
Hi Nate,

Your survey does not have space for comments, so here are a couple:

Sufficient roads and public transit need to be in place BEFORE there is a major population increase in an area.

Nearby green spaces are important for everyone's happiness and mental health. Parks and other green spaces need to be added, not eliminated, as infill occurs. Neighbors of new infill need to be consulted about which undeveloped areas are valuable to them as open space, and those wishes must be respected. Once open space is paved and destroyed, it tends to be gone forever. This is a major quality of life issue and impacts the social health of a community.

Karen



COMMENT SHEET

Code Amendments for Attached Houses and Multifamily Zones

September 2018

For more project info visit:

my.spokanecity.org/projects/infill-housing-strategies-infill-development/

Name: Jan Loux

ADDRESS: 1944 W Clarke Ave, 99201

PHONE NUMBER: 509-995-6747

E-MAIL CONTACT: twux11@msn.com

See attached comments.

Please feel free to share your questions, comments or concerns with us!

Postal Mail – fold this comment card in thirds, add postage and drop in the mail

Phone – call us at #625-6983

E-mail – write to us at ngwinn@spokanecity.org

Thank you...We look forward to hearing from you!

Planning & Development Services, City of Spokane

I am submitting my comments regarding the proposed changes to the Spokane Municipal Code to accommodate and encourage infill development. I live in Peaceful Valley, a neighborhood that is entirely zoned RMF (Residential Multi-Family). Located within easy walking and biking distance to the Downtown core and to the hospital district, and with many undeveloped properties, Peaceful Valley is a prime area for infill development.

Peaceful Valley is one of Spokane's oldest neighborhoods with houses dating from the 1890s. The neighborhood is plotted in 25 foot wide parcels and many of the old homes are small and built very close together. While there are a few multi-family buildings in the neighborhood, most of the residences are still single family one or two story homes. Peaceful Valley has a distinct history and character that are unique in Spokane. Certainly residents, and hopefully many others in the city, would like to see the uniqueness of Peaceful Valley preserved.

I support the concept of infill development in the Peaceful Valley neighborhood. In regard to the current infill development proposals, I support the proposed change to lot width, allowing for smaller required distances around homes and few driveways across sidewalks. I also support the proposed change for attached housing that would remove the requirement to double the distance between buildings and side lot lines to encourage townhouses.

I do not support two of the other proposed changes. I do not support the change to height limits which would increase the building height limit from 35 to 50 or 55 feet to accommodate pitched roofs and basement parking. As all of the single family homes in Peaceful Valley are one or two story structures that are significantly below the current 35 foot height limit, buildings 50 feet or taller would dwarf existing neighboring buildings. New, taller buildings would be incompatible and would detract from the historical charm and coherent character of the neighborhood. Also, because the additional allowance for pitched roofs would include roofs with dormers, the privacy of neighboring properties could be compromised.

I am also opposed to changing the minimum parking requirements for attached houses. Some older homes in Peaceful Valley have no off-street parking. Particularly on Water Ave and Main Ave, residents with cars park on the street. Also, because the Downtown core is an easy walk from Peaceful Valley, Downtown workers take advantage of free parking on the neighborhood streets. In addition, a multi-use trail through Peaceful Valley is under construction. The trail will reduce the width of certain streets and parking will be restricted to one side of certain streets. In light of these conditions, adding more parked cars to the streets of Peaceful Valley is a bad idea. I do not want to see Peaceful Valley looking like Browne's Addition with most streets reduced to one lane of traffic due to a solid wall of cars parked on both sides of the street. The goal of the proposed reduction in required parking might be to encourage alternate transportation – an admirable goal. However, I think most people will still own a car, whether they use it on a daily basis to commute or not. And those cars will need to park somewhere. I want that parking to occur off street.

These are our comments regarding the proposed changes to the Spokane Municipal Code to accommodate and encourage infill development. We live in Peaceful Valley, a neighborhood that is entirely zoned RMF (Residential Multi-Family). Located within easy walking and biking distance to the Downtown core and to the hospital district and with many undeveloped properties, Peaceful Valley is a prime area for infill development.

Peaceful Valley is one of Spokane's oldest neighborhoods with houses dating from the 1890s. The neighborhood is plotted in twenty-five foot parcels and many of the old homes are small and built very close together. While there are a few multi-family buildings in the neighborhood, most of the residences are still single family homes. Peaceful Valley has a distinct history and character that are unique in Spokane. Many of the residents would like to see that character preserved.

We support the concept of infill development in the Peaceful Valley neighborhood. In regard to the current infill development proposals, we support the proposed change to lot width, allowing for smaller required distances around homes and few driveways across sidewalks. We also support the proposed change for attached housing that would remove the requirement to double the distance between buildings and side lot lines to encourage townhouses.

We do not support two of the other proposed changes. We do not support the change to height limits which would increase the building height limit from 35 to 50 or 55 feet to accommodate pitched roofs and basement parking. As all of the single-family homes in Peaceful Valley are one or two story structures that are significantly below the current 35 foot height limit, buildings 50 feet or taller would dwarf existing neighboring buildings. New, taller buildings would be incompatible and would detract from the historical charm and coherent character of the neighborhood. Also, because the additional allowance for pitched roofs would include roofs with dormers, the privacy of neighboring properties could be compromised.

We are also opposed to changing the minimum parking requirements for attached houses. Some older homes in Peaceful Valley have no off-street parking. Particularly on Water Ave and Main Ave, residents with cars park on the street. Also, because the Downtown core is an easy walk from Peaceful Valley, downtown workers take advantage of free parking on the neighborhood streets. In addition, a multiuse trail through Peaceful Valley is under construction. The trail will reduce the width of certain streets and parking will be restricted to one side of certain streets. In light of these conditions, adding more parked cars to the streets of Peaceful Valley is a bad idea. We do not want Peaceful Valley reduced to one lane of traffic due to a solid wall of cars parked on both sides of the street. The goal of the proposed reduction in required parking might be to encourage alternative transportation – an admirable goal. However, We think most people will still own a car, whether they use it on a daily basis to commute or not. And those cars will need to park somewhere. We want that parking to occur off street.

Thank you for your time,

Tod Marshall and Amy Sinisterra
1629 W. Clarke Avenue
509 496 1251

From: Gwinn, Nathan
To: ["George Newsom"](#)
Subject: RE: Peaceful Valley Proposed Changes
Date: Wednesday, October 17, 2018 8:15:00 AM

Good morning Mr. Newsom,

Thank you for your comment. I will add it to the public record for the file.

Sincerely,

Nathan Gwinn | Assistant Planner | Planning & Development
509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

From: George Newsom <g_newsom@hotmail.com>
Sent: Tuesday, October 16, 2018 7:50 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Subject: Peaceful Valley Proposed Changes

Hello I just read what Jan Loux wrote you and I agree with her 100%

George Newsom

From: Gwinn, Nathan
To: ["Barbara Morrissey"](#)
Subject: RE: Infill housing
Date: Wednesday, October 17, 2018 4:36:00 PM

Good afternoon, Barbara:

Thank you for the message. I will add your comments to the public record for this file.

Sincerely,

Nathan Gwinn | Assistant Planner | Planning & Development
509.625.6893 | ngwinn@spokanecity.org | www.spokanecity.org

-----Original Message-----

From: Barbara Morrissey <taslin10@comcast.net>
Sent: Wednesday, October 17, 2018 4:20 PM
To: Gwinn, Nathan <ngwinn@spokanecity.org>
Subject: Infill housing

Hi Nathan

My comments are based on living since 1983 in Peaceful Valley

There is no need to change lot size requirements if Little Houses are allowed. Some people like room for gardening. Some people like to listen in to the neighbor fights.

Not everyone cares for multifamily housing. Not everyone likes to live in tall buildings. If that is all one can find in a City you will be sure people move outside when they have a chance. Why do you planners think suburbs exist?? The human species is adapted to prefer space. Most live in places like Hong Kong and Shanghai because they have no choice. Architects and developers love tall towers for financial reason, as well as a sense of personal pride. But most people who live in the area don't. Don't increase height allowances in R2 zones.

As far as off street parking on 25ft lots there are several two story townhouses on Clarke Ave and Wilson in Peaceful Valley which are examples of what can be done within the 25 ft limit. Bob Cooke built them. Developers need to continue to provide off street parking.

A 1/4 mile walking distance to a grocery, an office, etc is not feasible for handicapped people. I have noticed a lot more people in the neighborhood grocery in BA are getting greyer in the hair, using walkers and canes, like me. The inner city demo is not swinging to the young but toward the elders. As electric autos become more available the solution to greenhouse gasses becomes closer. Bikes are no transportation solution for those who are handicapped.

I agree with the lady who suggested that certain things which make a neighborhood, like trees, not be torn down to make more "dirt" for infill housing.

Getting to the Rosauers in BA will be harder once STA stops running down Clarke. Planners should bear in mind that Mass Transit can fall through

The early plans for the Great Gorge Park recommended keeping undeveloped open space. I especially like an area of spring we call the Swamp. A family of deer hang out there. I think city utilities would have something to say about keeping these springs intact since they flow directly into the river.

Spokane should not become like Portland or Seattle.

sincerely.

Barbara Morrissey
1647 west Clarke ave
Spokane, WA



Infill Development Code Amendments

October 24, 2018

Dimension and Transition Standards - Multifamily Zones

- Building Height
- Parking Requirements
- Lot Area/Building Coverage
- Design Standards





Workshop Overview

- Actions requested
 - Direction on options for hearing draft (RMF, RHD)
 - Building height: structured parking exception
 - Minimum attached housing parking requirements
 - Lot area and building coverage standards
 - Multifamily design standards for attached homes
- Review
 - Lot width and front lot line—attached homes (RTF)
 - Curb cut limitations – for hearing draft (RMF, RHD)



Public Outreach

- Plan Commission workshops
 - March-October, 2018
- Land Use Committee
 - May 18, September 18, 2018
- Community Assembly
 - May and June, 2018
- Neighborhood events, social media
 - Neighborhood Council meetings, block parties
- Plan Commission public hearing
 - July 11 continued to November 14, 2018

[SpokaneCity.org/projects](https://spokanecity.org/projects) > Infill Housing/Infill Development

RMF, RHD Zones and Centers/Corridors -North

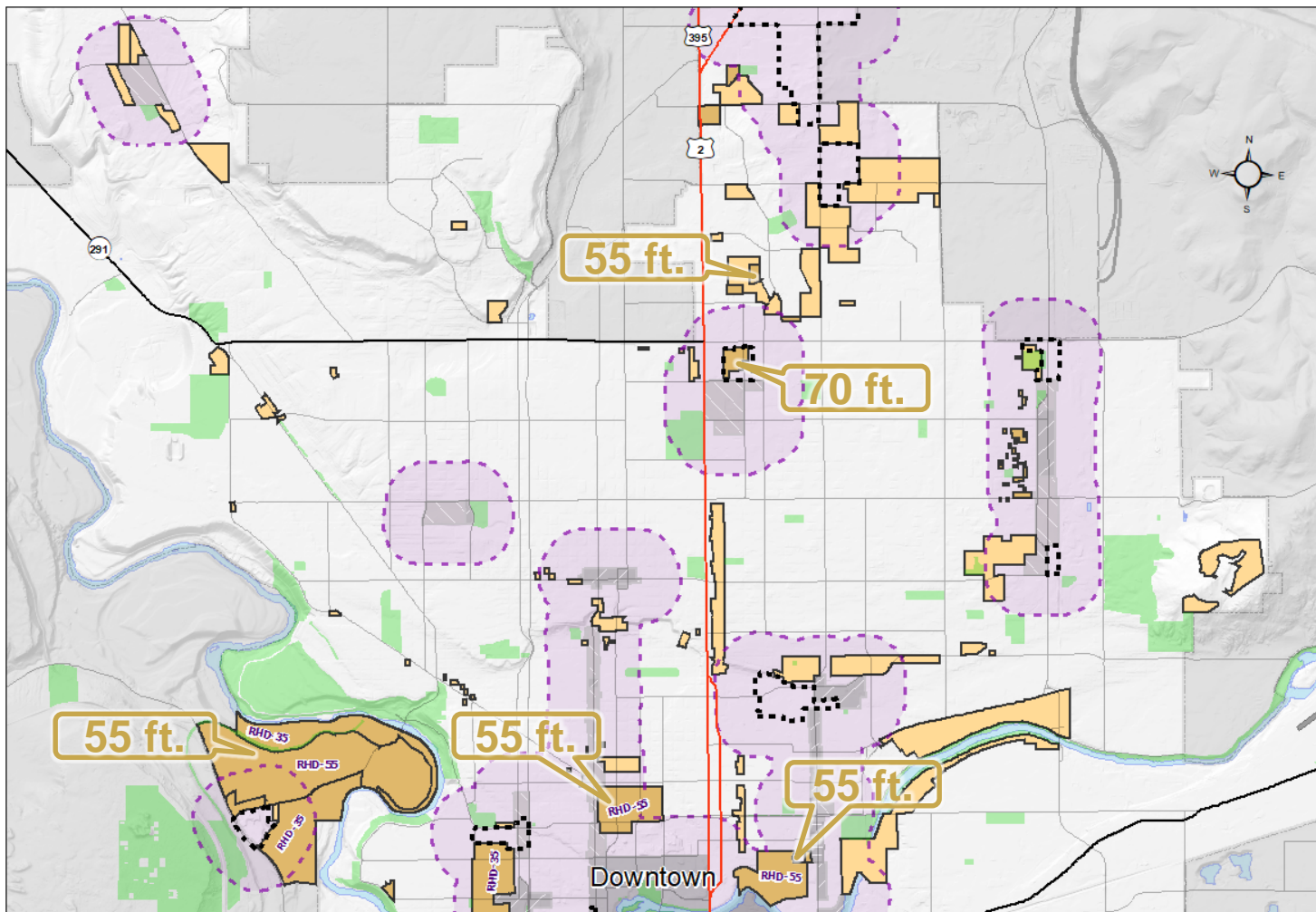
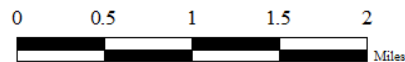
Print date: 10/10/2018

Enlarged
area



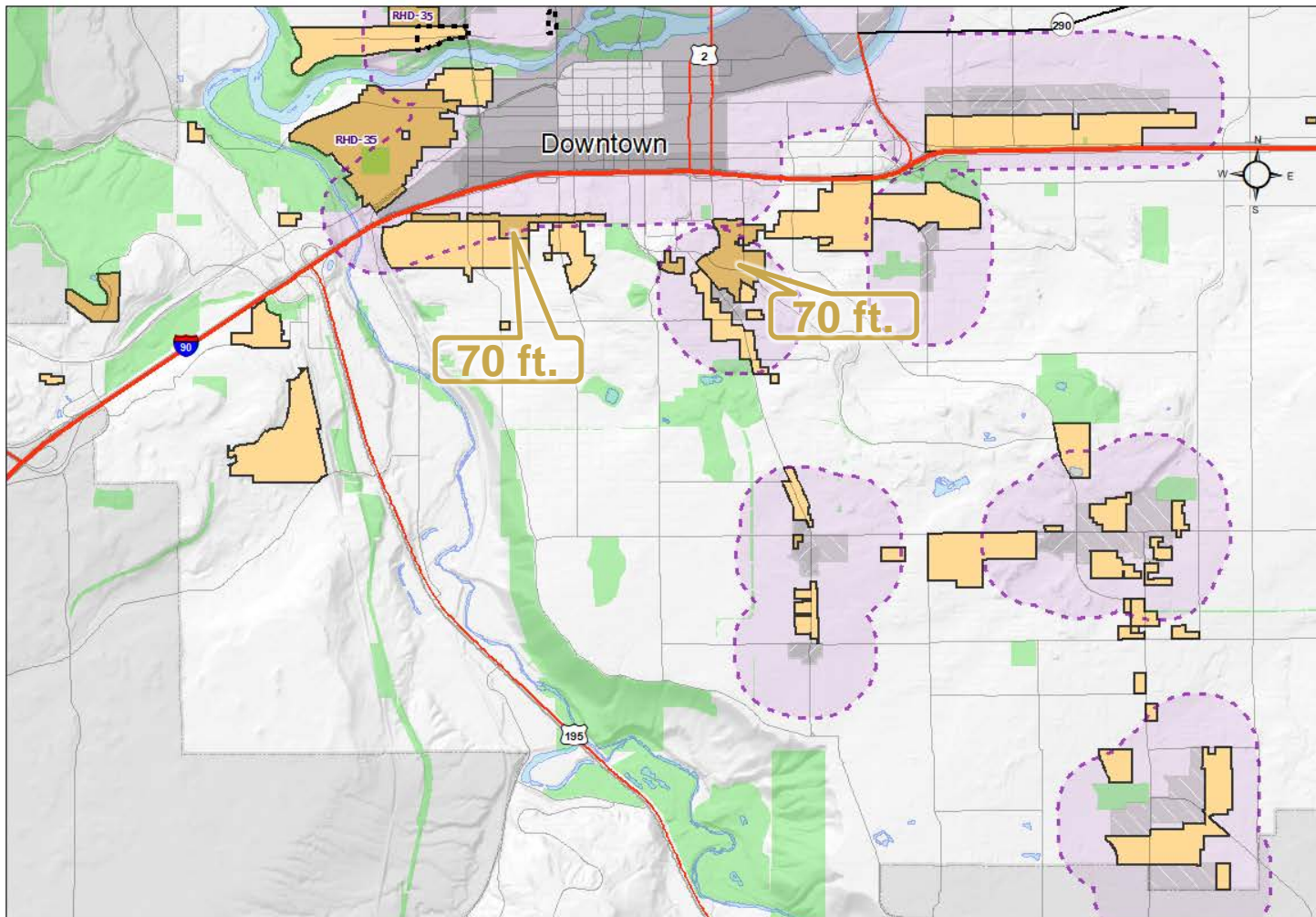
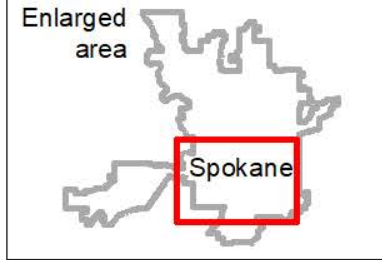
Spokane

- CC3 Zoning Overlay
- 1/4 Mile from CC
- Res. High Density (RHD)
- Res. Multifamily (RMF)
- City Park
- Center/Corridor 1-2,
Downtown,
Context Area 1-3



RMF, RHD Zones and Centers/Corridors -South

Print date: 10/10/2018



- CC3 Zoning Overlay
- 1/4 Mile from CC
- City Park
- Res. High Density (RHD)
- Res. Multifamily (RMF)
- Center/Corridor 1-2,
Downtown,
Context Area 1-3



RMF and RHD zones | SMC 17C.110.200, .215

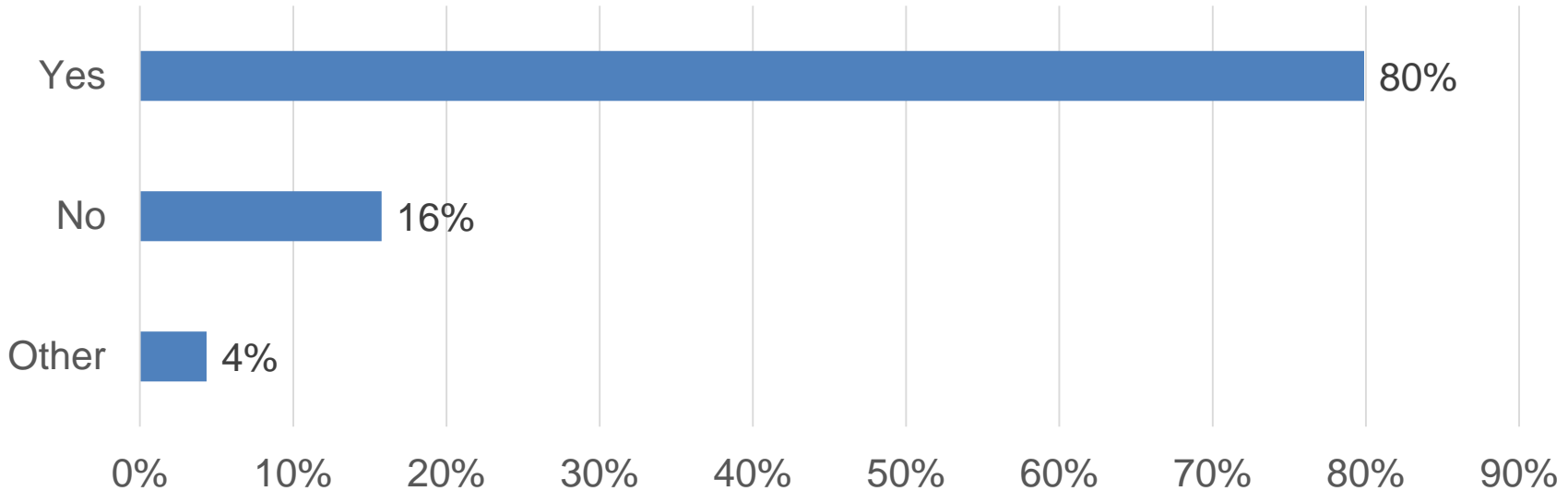
BUILDING HEIGHT



Survey Responses

Should the City allow taller buildings in multifamily zones if it is for parking under the structure?

Answered: 184 Skipped: 0



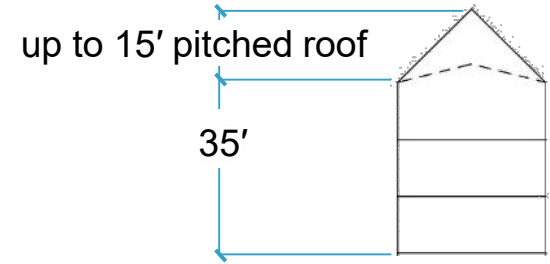


Building Height

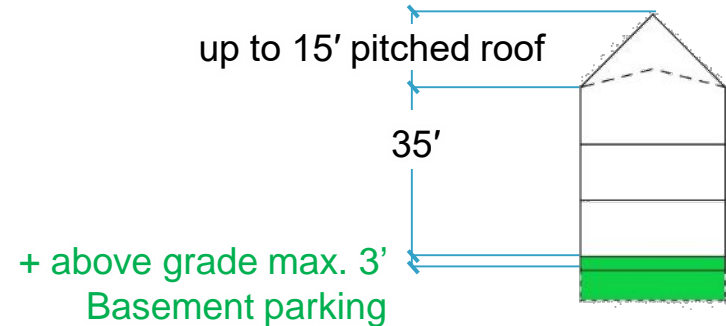
SMC 17C.110.215
pp. 11-13

Proposed building height in RMF and RHD-35 zones

- Option 1 : up to 50 ft.



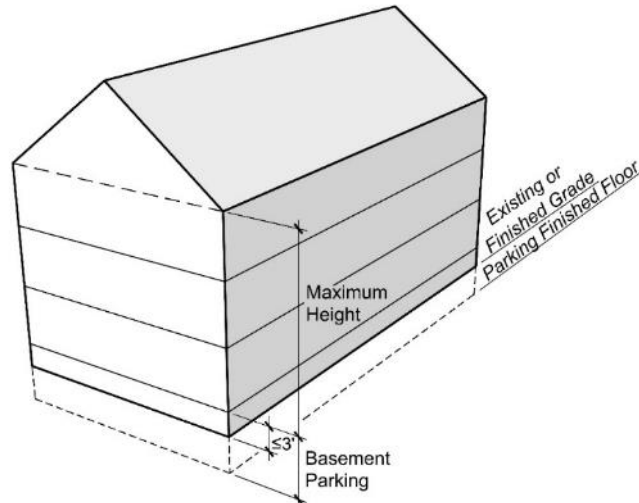
- Option 2 : up to 53 ft.



Building Height

3. In the RMF and RHD zones, height does not include up to three feet of the above-grade portions of basement parking, where the elevation of the first residential finished floor is three feet or less above the lowest elevation of the existing grade or finished grade, whichever is lower. See Figure 17C.110-D.

Figure 17C.110-D: Basement Parking Excluded from Height.



SMC 17C.230.130-140

PARKING REQUIREMENTS

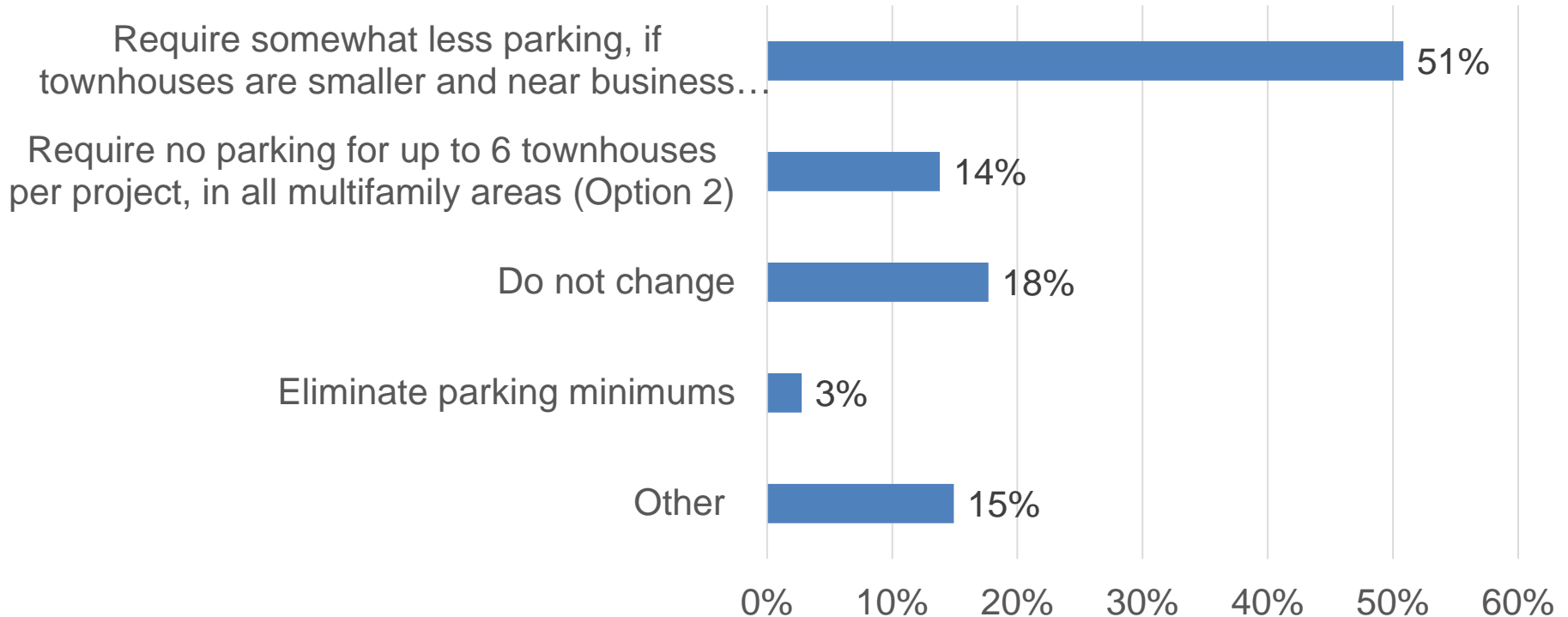


Survey Responses

Should the City allow less parking for smaller homes, or no parking for up to 6 homes?

Answered: 181

Skipped: 3

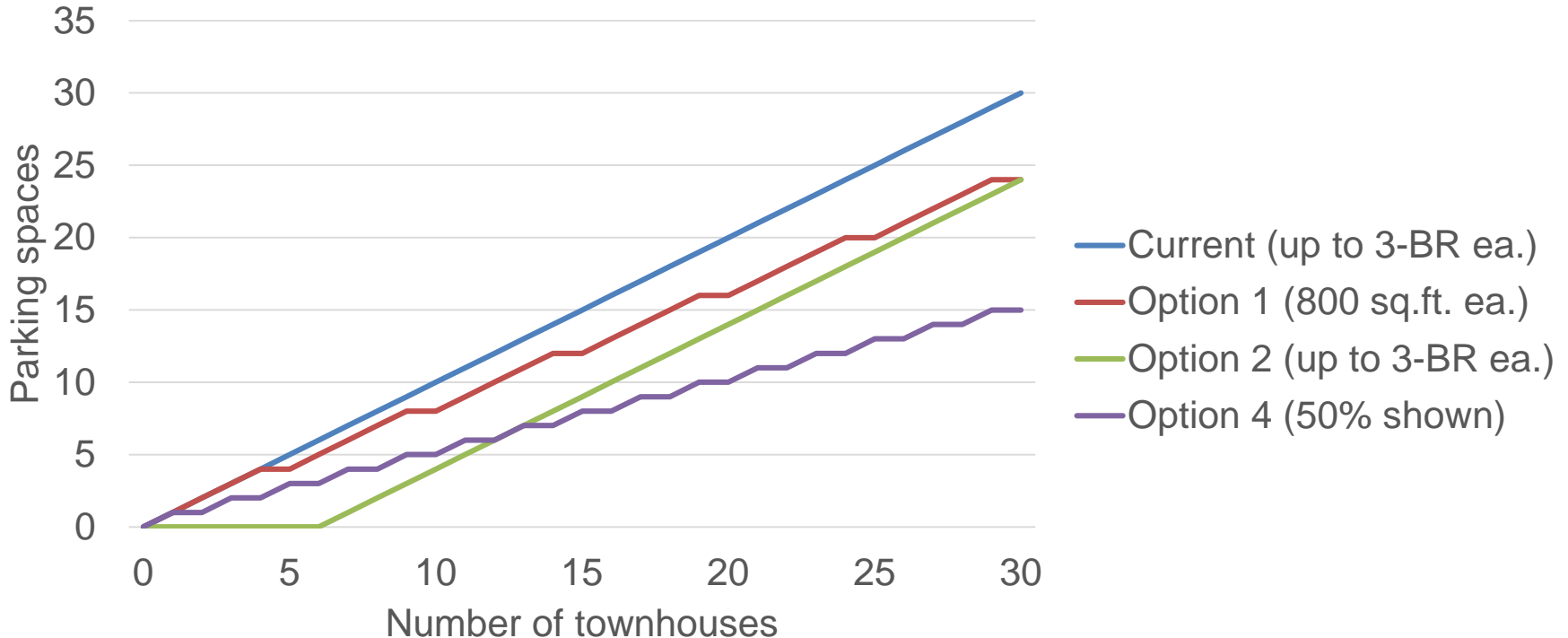




Parking Requirements

SMC 17C.230.130(F)
pp. 1-2

Proposed Townhouse Parking Spaces Required in RMF, RHD





Parking Requirements

SMC 17C.230.130(F)
pp. 1-2

Proposed minimum parking requirements in RMF and RHD zones

- Option 1 1:1,000 sq. ft. attached houses
w/in 1,320 ft. of CC, CA, DT zone
- Option 2 attached houses deduct up to 6 parking spaces
all RMF/RHD
- ADD Option 3 attached houses deduct up to 6 parking spaces
only w/in 1,320 ft. of CC, CA, DT zone
- ADD Option 4 ___% reduction in parking spaces for attached houses
all RMF/RHD

RMF, RHD Zones | 17C.110.200, Table 17C.110-3

LOT AREA AND BUILDING COVERAGE

Lot Area and Building Coverage

SMC 17C.110.200 pp. 4-5

17C.110.360 p. 20

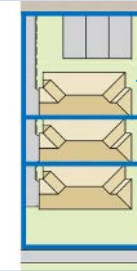
17G.080.065 pp. 1-2

Proposed attached housing lot area and building coverage

Option 1

Change Alternative Residential Subdivisions

- Remove homeowners' assoc.
- No 1.5-acre max. in RMF, RHD, Office, etc.



One Method

Parent Site

Area >1,600 sq.ft.

Coverage 50%

Option 2

Change Alternative Residential Subdivisions

- Remove homeowners' assoc.
- No 1.5-acre max. in RMF, RHD, Office, etc.

Change Table 17C.110-3

- RMF ~~1,600 sq.ft.~~ min. lot area → None
- All residential zones change max. bld. coverage → lot small as home
- Need dedication language

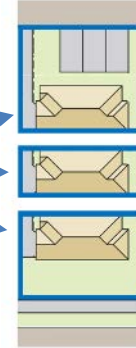
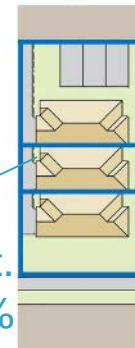
Two

Methods

A

or

B



By lot

35%

80%

40%

Parent Site

Area >1,600 sq.ft.

Coverage 50%

RMF, RHD zones | 17C.110.310, 17C.110.360, 17G.080.065

DESIGN STANDARDS



Design Standards

SMC 17C.110.310, 17C.110.360
17C.110.400 - .465

Proposed attached housing design standards in RMF/RHD zones

Option 1

Attached housing
RA, RSF, RTF
~~RMF, RHD~~

- Street-oriented standards

Pocket residential
RA, RSF, RTF
~~RMF, RHD~~

- Landscaping and lighting

Attached housing & pocket
residential
RMF, RHD

- Multifamily design standards

Option 2

Leave/no change

RTF, RMF, RHD Zones | 17C.110.200, Table 17C.110-3

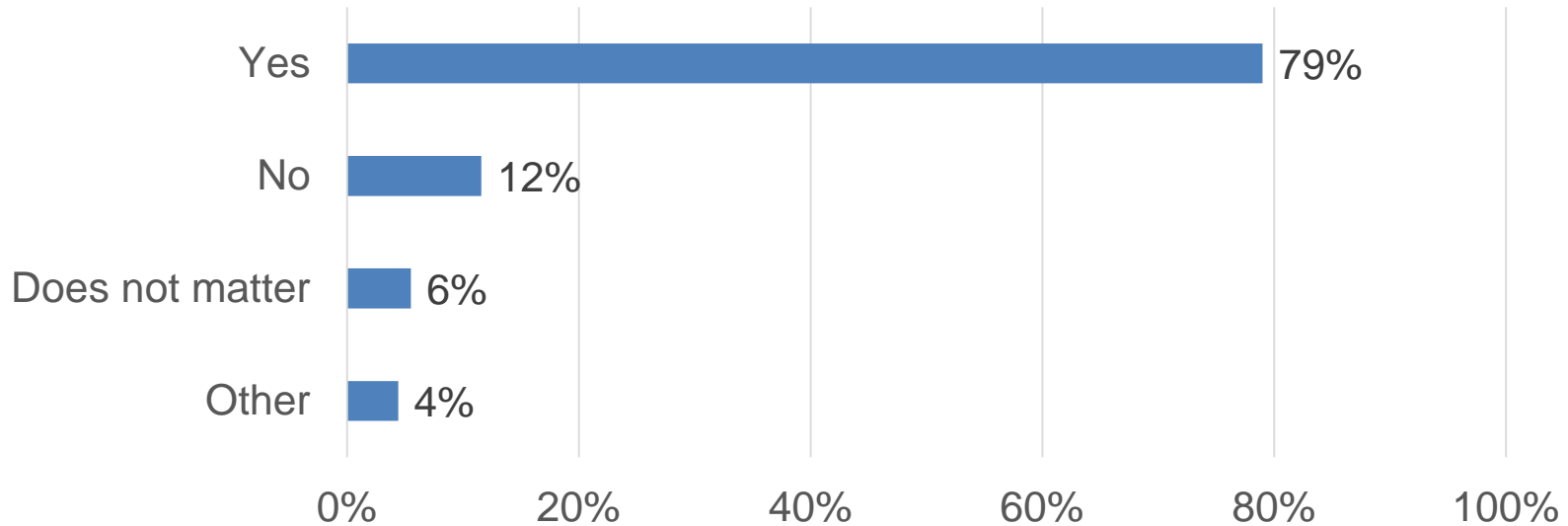
LOT WIDTH AND FRONT LOT LINE



Survey Responses

If townhouses facing the street are allowed to be narrower in multifamily zones, should the garage be moved to the side or back so that the front side of each townhouse is not dominated by a garage door?

Answered: 181 Skipped: 3





Lot Width/Front Lot Line

Table 17C.110-3
p. 5

	RA	RSF & RSF-C	RTF	RMF	RHD
Minimum Lot Width	40 ft.	40 ft.	36 ft. <u>with garage facing front lot line,</u> or 25 ft. <u>with Curb Cut Limitation [15],</u> or 16 ft. <u>with alley parking and no street curb cut</u>	((Same)) 36 ft. <u>with garage facing front lot line,</u> or 25 ft. <u>with Curb Cut Limitation [15],</u> or 16 ft. <u>with alley parking and no street curb cut</u>	((Same)) 36 ft. <u>with garage facing front lot line,</u> or 25 ft. <u>with Curb Cut Limitation [15],</u> or 16 ft. <u>with alley parking and no street curb cut</u>



Curb Cuts

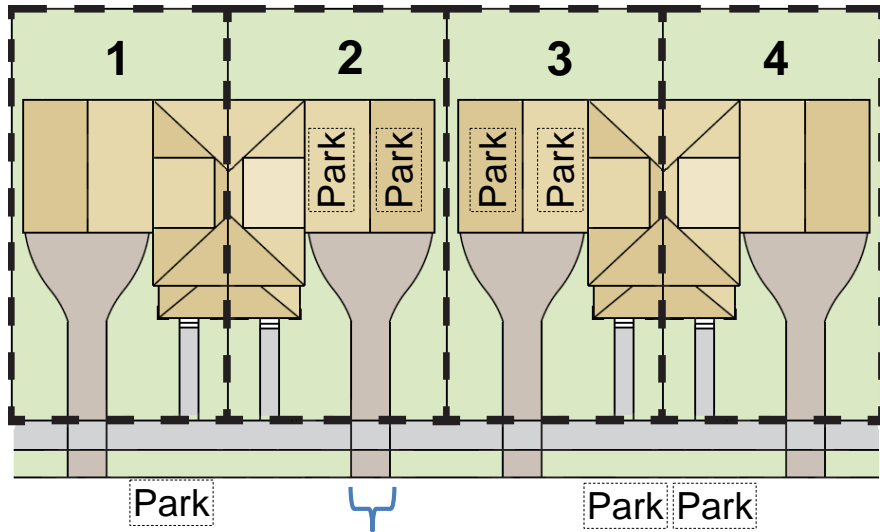




Attached Housing – Curb Cuts Proposed

RTF, RMF, RHD zones

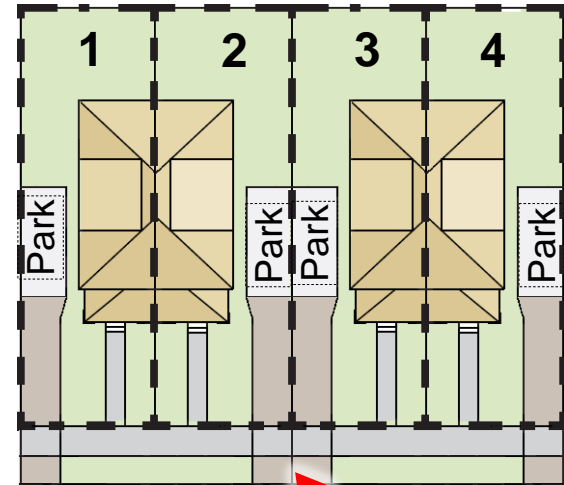
Existing Code



- Approach width (at curb) min. 12 ft.
- Driveway width min. 9 ft.
- Max. 20% of lot

Proposed Code

Lots less than 40 feet wide



**Limit one curb cut
per two dwellings**



Questions





Adopted Guidance

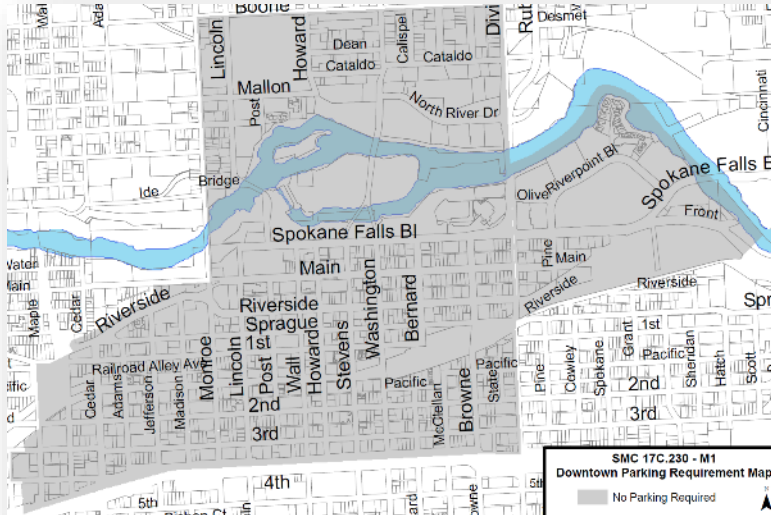
- Housing affordability and choice
 - Expand opportunities for homeownership
 - Build near centers for efficiency
- Clearer standards, easier permit approval
- Make infill more compatible with surrounding neighborhoods

[SpokaneCity.org/projects](https://spokane-city.org/projects) > Infill Housing/Infill Development

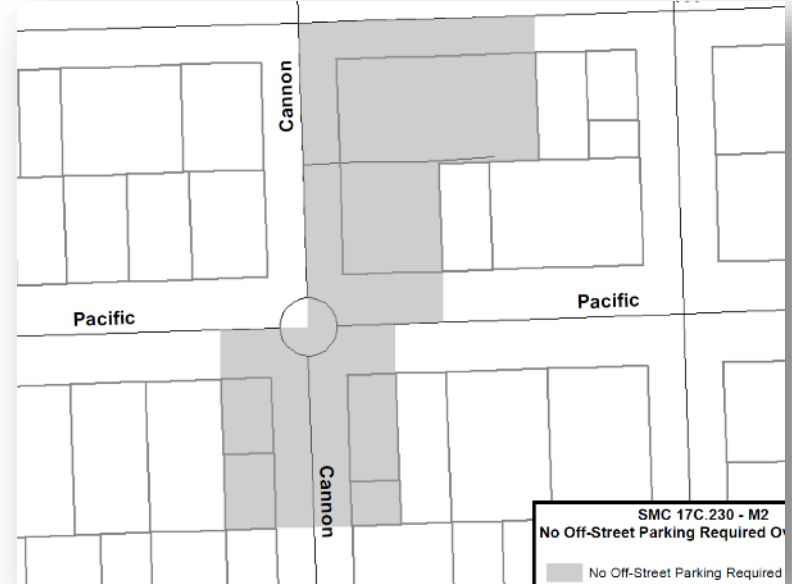


Special Parking Areas

SMC 17C.230.120
(Table 17C.230-2)



Downtown No Parking Required



No off-street parking overlays

- Browne's Addition: Pacific Avenue & Cannon Street
- East Central: 5th Avenue & Fiske Street



Parking for Attached Houses (RMF, RHD)

SMC 17C.230.130(F)
pp. 1-2

Current requirement

12 attached houses=12 spaces



Option 1

Use Centers reqt. – near CC zones



Option 2

Current reqt. minus 6





Design Standards

SMC 17C.110.310, 17C.110.360
17C.110.400 - .465

Existing **Attached Housing** (all lots front street)

Pocket Residential (w/o public street; detached or attached)

- Landscaping – 3-gal. shrub
- 60% front area
- Unified design
- Fire escape-stairs
- Orientation
- Porch 50 sq. ft.
- Modulation 30x4 ft.
- Elements/forms
- Vertical/horizontal patterns

Pocket Residential (w/o public street frontage; detached or attached)

- Ground level access
- Lighting
- Parking L2 screening
- Fencing

Proposed
to apply to Attached
Housing in RMF and RHD

Multifamily

- Sidewalks, pedestrian connections
- Landscaping and screening
- Articulation and details
- Front yards, entrances
- Pitched roofs-near SF
- Base, middle, top
- Windows–15% street
- Parking structures

ORDINANCE NO. _____

An ordinance relating to dimensional standards for attached housing and multifamily development in residential zones, amending Spokane Municipal Code (SMC) sections 17C.110.200, 17C.110.215, 17C.110.310, and 17C.110.360.

The City of Spokane does ordain:

Section 1. That SMC section 17C.110.200 is amended to read as follows:

17C.110.200 Lot Size

A. Purpose.

The standards of this section allow for development on lots, but do not legitimize lots that were divided in violation of [chapter 17G.080 SMC](#), Subdivisions. The required minimum lot size, lot depth, lot width and frontage requirements for new lots ensure that development will, in most cases, be able to comply with all site development standards. The standards also prevent the creation of very small lots that are difficult to develop at their full density potential. Finally, the standards also allow development on lots that were reduced by condemnation or required dedications for right-of-way.

B. Existing Lot Size.

1. Development is prohibited on lots that are not of sufficient area, dimension and frontage to meet minimum zoning requirements in the base zone. Except:
 - a. one single-family residence may be developed on a lot that was legally created under the provisions of chapter 58.17 RCW, Plats – Subdivisions – Dedications, or applicable platting statutes;
 - b. a PUD lot may be less than the minimum size of the base zone, if such lot is delineated on a PUD plan, which has been approved by the hearing examiner. All use and development standards of the zone wherein such lot is located, shall be complied with, unless modified through the PUD process by the hearing examiner. A PUD shall comply with the requirements of subsection (C) of this section.
2. No lot in any zone may be reduced so that the dimension, minimum lot area, frontage or area per dwelling unit is less than that required by this chapter, except as modified through the PUD process by the hearing examiner.
3. Lots Reduced by Condemnation or Required Dedication for Right-of-way. Development that meets the standards of this chapter is permitted on lots, or combinations of lots, that were legally created and met the minimum size requirements at the time of subdivision, but were reduced below one or

more of those requirements solely because of condemnation or required dedication by a public agency for right-of-way.

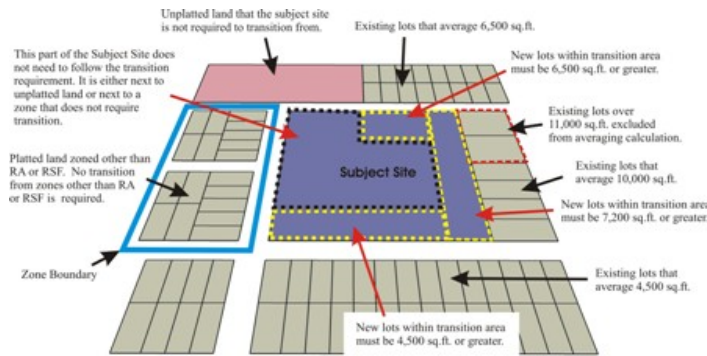
C. Land Division.

All new lots created through subdivision must comply with the standards for the base zone listed in Table 17C.110-3.

1. Transition Requirement.

For sites two acres or greater, transition lot sizes are required to be included as a buffer between existing platted land and new subdivision subject to the requirements of this section. The purpose of this section is to transition lot sizes between the proposed and existing residential developments in order to facilitate compatible development and a consistent development pattern. In the RA and RSF zones, the minimum lot size is subject to transitioning of lots sizes. Lots proposed within the initial eighty feet of the subject property are required to transition lot sizes based on averaging under the following formulas:

- a. Transitioning is only required of properties adjacent to or across the right-of-way from existing residential development. "Existing residential development" in this section shall mean existing lots created through subdivision or short plat.
- b. Lot size in the transition area is based on the average of the existing lot size in subdivisions adjacent to, or across the street from, the subject property. Lots greater than eleven thousand square feet are not counted in the averaging.
- c. If the existing average lot size is greater than seven thousand two hundred square feet, then the lot size in the transition area can be no less than seven thousand two hundred square feet.
- d. If the existing average lot size is less than seven thousand two hundred square feet, then the lot size in the transition area can be equal to or greater than the average.
- e. If the subject site shares boundaries with more than one subdivision, the minimum lot size in the transition area shall be based on the average lot sizes along each boundary. When two boundaries meet, the lot size shall be based on the larger of the two boundaries. See example below; and



- f. If the subject site shares a boundary with property zoned other than RA or RSF, then there are no transition requirements along that boundary.
 - g. After the first set of lots in the transition area, lot sizes may be developed to the minimum lot size of the base zone, i.e., four thousand three hundred fifty square feet in the RSF zone.
2. Planned unit developments, combined with a subdivision, may reduce the minimum lot size, lot width, lot depth and frontage requirements in the RA and RSF zones pursuant to [SMC 17G.070.030\(C\)\(1\)](#), except in the transition area required by subsection (C)(1) of this section.
- D. **Ownership of Multiple Lots.**
Where more than one adjoining lot is in the same ownership, the ownership may be separated as follows:
1. If all requirements of this chapter will be met after the separation, including lot size, density and parking, the ownership may be separated through either a boundary line adjustment (BLA) or plat, as specified under [chapter 17G.080 SMC](#), Subdivisions.
 2. If one or more of the lots does not meet the lot size standards in this section, the ownership may be separated along the original plat lot lines through a boundary line adjustment (BLA).
- E. **New Development on Standard Lots.** New development on lots that comply with the lot size standards in this section are allowed subject to the development standards and density requirements of the base zone as required under Table 17C.110-3.
- F. **Lot Frontage.** All residential lots shall front onto a public street and meet the minimum lot frontage requirements of Table 17C.110-3. Except, that frontage on a public street is not required for lots created through alternative residential subdivision under [SMC 17G.080.065](#), and lots approved in a planned unit

development or a manufactured home park may have lots or spaces fronting onto private streets, subject to the decision criteria of [SMC 17H.010.090](#).

TABLE 17C.110-3 DEVELOPMENT STANDARDS [1]					
DENSITY STANDARDS					
	RA	RSF & RSF-C	RTF	RMF	RHD
Density - Maximum	4,350 (10 units/acre)	4,350 (10 units/acre)	2,100 (20 units/acre)	1,450 (30 units/acre)	--
Density - Minimum	11,000 (4 units/acre)	11,000 (4 units/acre)	4,350 (10 units/acre)	2,900 (15 units/acre)	2,900 (15 units/acre)
MINIMUM LOT DIMENSIONS LOTS TO BE DEVELOPED WITH:					
Multi-Dwelling Structures or Development					
	RA	RSF & RSF-C	RTF	RMF	RHD
Minimum Lot Area				2,900 sq. ft.	2,900 sq. ft.
Minimum Lot Width				25 ft.	25 ft.
Minimum Lot Depth				70 ft.	70 ft.
Minimum Front Lot Line				25 ft.	25 ft.
Compact Lot Standards [2]					
Minimum Lot Area [3]		3,000 sq. ft.			
Minimum Lot Width		36 ft.			
Minimum Lot Depth		80 ft.			
Minimum Front Lot Line		30 ft.			
Attached Houses					
Minimum Lot Area [3]	7,200 sq. ft.	4,350 sq. ft.	1,600 sq. ft.	((1,600 sq. ft.)) <u>None</u>	None

ATTACHED HOUSES OPTION 1
No change to min. lot area in
RMF zone – instead, change text
of 17G.080.065

OPTION 2 (shown here) –
change 1,600 sq. ft. to none with
other code language to be
developed.

Minimum Lot Width	40 ft.	40 ft.	36 ft. with garage facing front lot line, or 25 ft. with Curb Cut Limitation [15], or 16 ft. with alley parking and no street curb cut	((Same)) 36 ft. with garage facing front lot line, or 25 ft. with Curb Cut Limitation [15], or 16 ft. with alley parking and no street curb cut	((Same)) 36 ft. with garage facing front lot line, or 25 ft. with Curb Cut Limitation [15], or 16 ft. with alley parking and no street curb cut
Minimum Lot Depth	80 ft.	80 ft.	50 ft.	((25-ft.)) None	((25-ft.)) None
Minimum Front Lot Line	40 ft.	40 ft.	Same as lot width	Same as lot width	Same as lot Width

**ATTACHED HOUSES
OPTION 1** – Do not add text related to maximum building coverage or min. lot area to this table (Table 17C.110-3), but change the text of 17G.080.065 as shown in separate companion ordinance

ATTACHED HOUSES OPTION 2 – Add the following statement about maximum building coverage in the next line in Table 17C.110-3, which is existing text adapted from 17G.080.065(D)(5). This option would bypass the process of 17G.080.065 and the provision to place plat notes on the subdivision map about limitation on building additions and repair.

<u>Maximum Building Coverage</u>	<u>All buildings located within the development shall not exceed the maximum building coverage stated below for primary structures. Lots within the development may be as small as the footprint of an individual attached home</u>
----------------------------------	---

Detached Houses					
Minimum Lot Area [3]	7,200 sq. ft.	4,350 sq. ft.	1,800 sq. ft.	1,800 sq. ft.	None
Minimum Lot Width	40 ft.	40 ft.	((36)) 25 ft.	25 ft.	25 ft.
Minimum Lot Depth	80 ft.	80 ft.	40 ft.	25 ft.	25 ft.
Minimum Front Lot Line	40 ft.	40 ft.	((30)) 25 ft.	25 ft.	25 ft.

Duplexes					
Minimum Lot Area			4,200 sq. ft.	2,900 sq. ft.	None
Minimum Lot Width			25 ft.	25 ft.	25 ft.
Minimum Lot Depth			40 ft.	40 ft.	25 ft.
Minimum Front Lot Line			25 ft.	25 ft.	25 ft.
PRIMARY STRUCTURE					
Maximum Building Coverage (except see above for attached houses) <i>[Option 2 text]</i>					
	RA	RSF & RSF-C	RTF	RMF	RHD
Lots 5,000 sq. ft. or larger	40%	2,250 sq. ft. +35% for portion of lot over 5,000 sq. ft.	2,250 sq. ft. +35% for portion of lot over 5,000 sq. ft.	50%	60%
Lots 3,000 - 4,999 sq. ft.	1,500 sq. ft. + 37.5% for portion of lot over 3,000 sq. ft.				
Lots less than 3,000 sq. ft.	50%				
Building Height					
Maximum Roof Height (([5]))	35 ft. [5]	35 ft. [5]	35 ft. [5]	35 ft. [6]	35 ft. [6]
Maximum Wall Height	25 ft.	25 ft.	25 ft.	((30 ft. [6])) --	--
Floor Area Ratio (FAR)					
FAR	0.5	0.5 [4]	0.5 [4]	--	--
Setbacks					
Front Setback [7, 8]	15 ft.				
Side Lot Line Setback – Lot width more than 40 ft.	5 ft.				
Side Lot Line Setback – Lot width 40 ft. or less	3 ft.				

Street Side Lot Line Setback [7]	5 ft.				
Rear Setback [9, 10]	25 ft.	25 ft. [11]	15 ft.	10 ft.	10 ft.
Required Outdoor Area					
Required Outdoor Area for attached and detached houses. Minimum dimension (See SMC 17C.110.223)	250 sq. ft. 12 ft. x 12 ft.	250 sq. ft. 12 ft. x 12 ft.	250 sq. ft. 12 ft. x 12 ft.	200 sq. ft. 10 ft. x 10 ft.	48 sq. ft. 7 ft. x 7 ft.
ACCESSORY STRUCTURES					
	RA	RSF & RSF-C	RTF	RMF	RHD
Maximum Roof Height	30 ft.	20 ft.	20 ft.	35 ft.	35 ft.
Maximum Wall Height	30 ft.	15 ft.	15 ft.	35 ft.	35 ft.
Maximum Coverage [12]	20%	15%	15%	See Primary Structure	See Primary Structure
Front Setback	20 ft.				
Side Lot Line Setback – Lot width 40 ft. or wider [13]	5 ft.				
Side Lot Line Setback – Lot width less than 40 ft. [13]	3 ft.				
Street Side Lot Line [14]	20 ft.				
Rear [13]	5 ft.				
Rear with Alley	0 ft.				

Notes:

-- No requirement

[1] Plan district, overlay zone, or development standards contained in SMC 17C.110.310 through 360 may supersede these standards.

[2] See SMC 17C.110.209, Compact Lot Standards.

[3] For developments two acres or greater, lots created through subdivision in the RA, RSF and the RSF-C zones are subject to the lot size transition requirements of SMC 17C.110.200(C)(1).

[4] In the RSF-C and RTF zones, and sites in the RSF zone qualifying for compact lot development standards, described in SMC 17C.110.209, FAR may be increased to 0.65 for attached housing development only.

[5] No structure located in the rear yard may exceed twenty feet in height.

[6] Base zone height may be modified according to SMC 17C.110.215, Height.

[7] Attached garage or carport entrance on a street is required to be setback twenty feet from the property line.

[8] See SMC 17C.110.220(D)(1), setbacks regarding the use of front yard averaging.

[9] See SMC 17C.110.220(D)(2), setbacks regarding reduction in the rear yard setback.

[10] Attached garages may be built to five feet from the rear property line except, as specified in SMC 17C.110.225(C)(6)(b), but cannot contain any living space.

[11] In the RSF-C zone and sites in the RSF zone qualifying for compact lot development standards, described in SMC 17C.110.209, the rear setback is 15 feet.

[12] Maximum site coverage for accessory structures is counted as part of the maximum site coverage of the base zone.

[13] Setback for a detached accessory structure and a covered accessory structure may be reduced to zero feet with a signed waiver from the neighboring property owner, except, as specified in SMC 17C.110.225(C)(5)(b).

[14] The setback for a covered accessory structure may be reduced to five feet from the property line.

[15] *[Note: New note needed to refer to appropriate section for Curb Cut Limitations.]*

Section 3. That SMC section 17C.110.310 is amended to read as follows:

17C.110.215 Height

A. Purpose.

The height standards promote a reasonable building scale and relationship of one residence to another and they promote privacy for neighboring properties. The standards contained in this section reflect the general building scale and placement of houses in the City's neighborhoods.

B. Height Standards.

The maximum height standards for all structures are stated in [Table 17C.110-3](#). The building height shall be measured using the following method:

1. The height shall be measured at the exterior walls of the structure. Measurement shall be taken at each exterior wall from the existing grade or finished grade, whichever is lower, up to a plan essentially parallel to the existing or finished grade. For determining structure height, the exterior wall

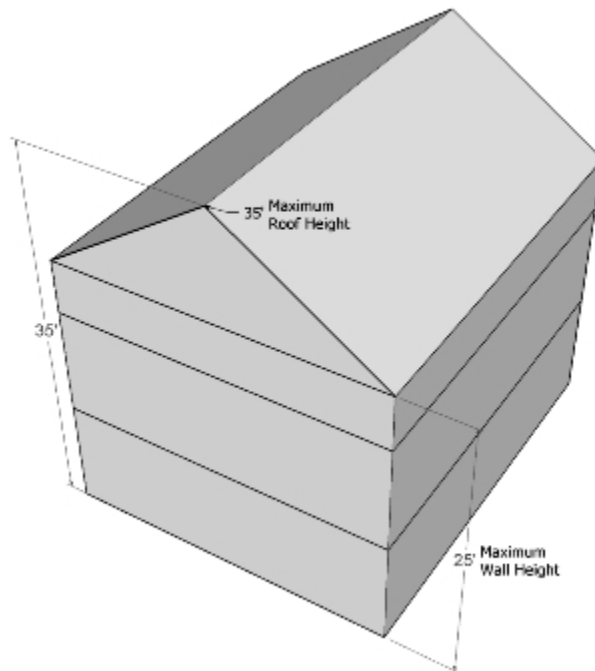
shall include a plane between the supporting members and between the roof and the ground. The vertical distance between the existing grade, or finished grade, if lower, and the parallel plane above it shall not exceed the maximum height of the zone.

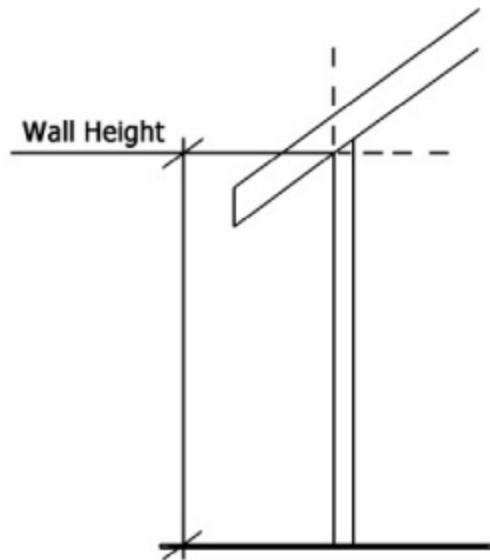
2. When finished grade is lower than existing grade, in order for an upper portion of an exterior wall to avoid being considered on the same vertical ((plan)) plane as a lower portion, it must be set back from the lower portion a distance equal to two times the difference between the existing and finished grade on the lower portion of the wall.
3. Depressions such as window wells, stairwells for exits required by other codes, “barrier free” ramps on grade, and vehicle access driveways into garages shall be disregarded in determining structure height when in combination they comprise less than fifty percent of the facade on which they are located. In such cases, the grade for height measurement purposes shall be a line between the grades on either side of the depression.
4. No part of the structure, other than those specifically exempted or excepted under the provisions of the zone, shall extend beyond the plan of the maximum height limit.
5. Underground portions of the structure are not included in height calculations. The height of the structure shall be calculated from the point at which the sides meet the surface of the ground.
6. For purposes of ((measure)) measuring building height in residential zones, the following terms shall be interpreted as follows:
 - a. “Grade” means the ground surface contour (see also “existing grade” and “finished grade”).
 - b. “Fill” means material deposited, placed, pushed, pulled or transported to a place other than the place from which it originated.
 - c. “Finished grade” means the grade upon completion of the fill or excavation.
 - d. “Excavation” means the mechanical removal of earth material.
 - e. “Existing grade” means the natural surface contour of a site, including minor adjustments to the surface of the site in preparation for construction.

<p style="text-align: center;">TABLE 17C.110.215-1 MAXIMUM HEIGHT</p>

Maximum Wall Height [1]	25 ft.
Maximum Roof Height [2]	35 ft.
<p>[1] The height of the lowest point of the roof structure intersects with the outside plane of the wall.</p> <p>[2] The height of the ridge of the roof.</p> <p>See “Example A” below.</p>	

((C-)) **Example A**





C. Exceptions to the maximum height standard are stated below:

1. Exceptions to the maximum structure height in the RMF and RHD zones are designated on the official zoning map by a dash and a height listed after the zone map symbol (i.e., ((CB)) RHD-150). Changes to the height limits in the RMF and RHD zones require a rezone. Height limits are ~~((thirty feet,))~~ thirty-five feet, forty feet, fifty-five feet, seventy feet, or one hundred fifty feet depending on location.

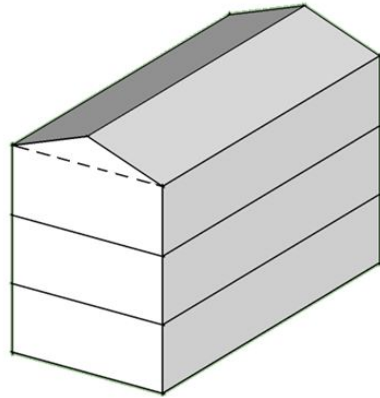
2. In RMF and RHD zones where the maximum structure height is thirty-five feet, pitched roof structures are allowed an additional fifteen feet above the maximum height standard stated in Table 17C.110-3, provided that the roof:

- a. incorporates pitched roof forms having slopes between 4:12 and 12:12; and

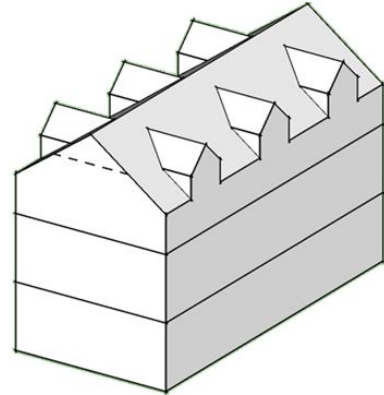
- b. is a gabled or hipped roof, which may include dormers (see Figure 17C.110-A).

**HEIGHT EXCEPTION
OPTION 1** - as
proposed by Plan
Commission, allows
15 additional feet to
base height of 35 ft.
(up to 50 ft.) with a
pitched roof.

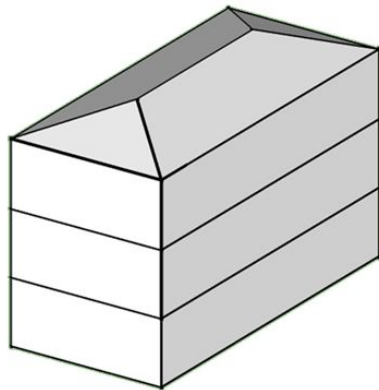
Figure 17C.110-A: Roof Types Eligible for Height
Exception.



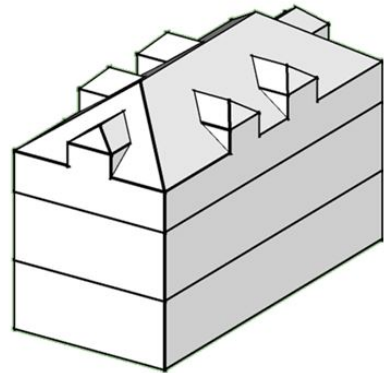
Gabled roof



Gabled roof & dormers



Hipped roof



Hipped roof & dormers

HEIGHT
EXCEPTION
OPTION 1
(continued)

[Note: Add the graphic above.]

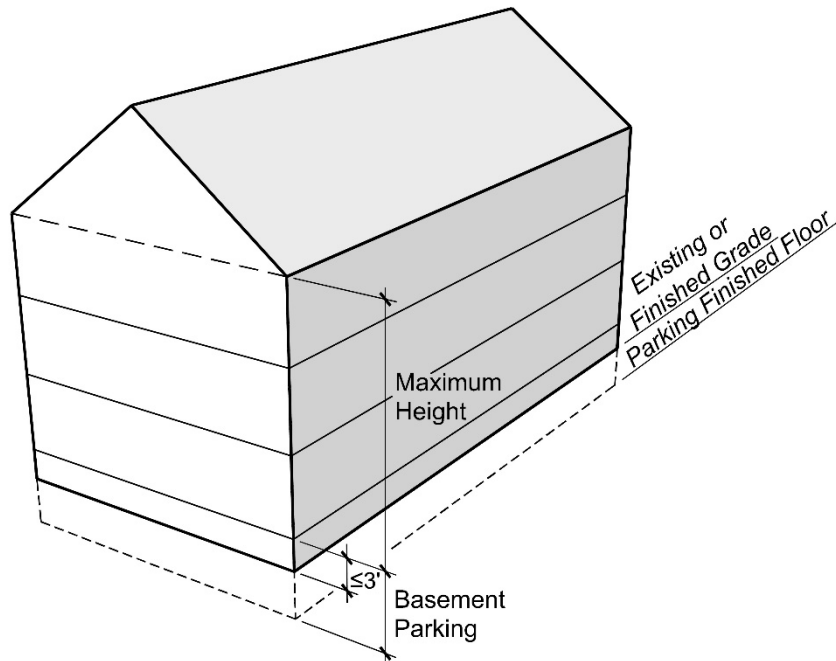
Note: Figure 17C.110-D and highlighted text in paragraph 3 below include changes to proposed Option 2 text to limit elevation of first floor.

HEIGHT EXCEPTION
OPTION 2 - includes
Option 1 above and
adds 3 ft. to
encourage and
accommodate
basement parking.

3. In the RMF and RHD zones, height does not include up to three feet of the above-grade portions of basement parking, where the elevation of the first residential finished floor is three feet or less above the lowest elevation of the existing grade or finished grade, whichever is lower. See Figure 17C.110-D.

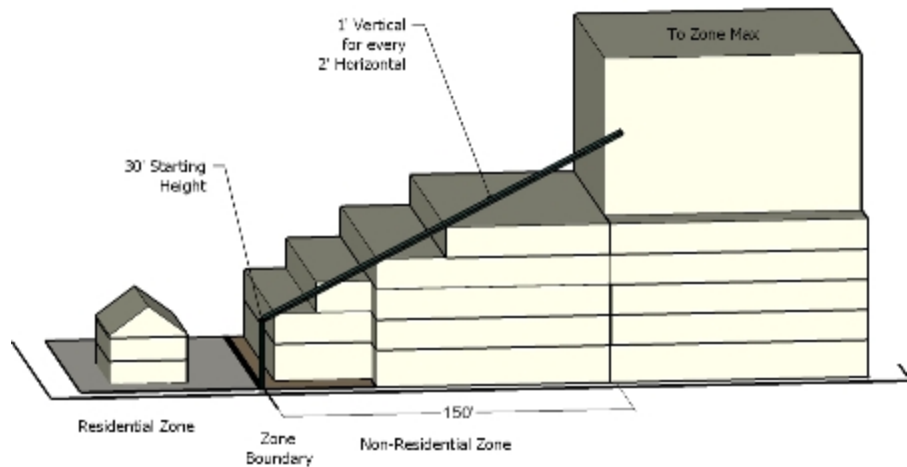
Figure 17C.110-D: Basement Parking Excluded from Height.

HEIGHT
EXCEPTION
OPTION 2
(continued)



[Note: Add the graphic above.]

- ((2)) 4. Buildings and structures over fifty feet in height must follow the design, setback and dimensional standards found in chapter 17C.250 SMC, Tall Building Standards.
- ((3)) 5. Adjacent to Single-family and Two-family Residential Zones.
To provide a gradual transition and enhance the compatibility between the more intensive commercial zones and adjacent single-family and two-family residential zones:
- a. for all development within one hundred fifty feet of any single-family or two-family residential zone the maximum building height is as follows:
 - i. Starting at a height of thirty feet (($\frac{1}{7}$)) at the residential zone boundary additional building height may be added at a ratio of one to two (one foot of additional building height for every two feet of additional horizontal distance from the closest single-family or two-family residential zone). The building height transition requirement ends one hundred fifty feet from the single-family or two-family residential zone and then full building height allowed in the zone applies.



((4)) 6. Projections Allowed.

Chimneys, flagpoles, satellite receiving dishes and other similar items with a width, depth or diameter of three feet or less may extend above the height limit, as long as they do not exceed three feet above the top of the highest point of the roof. If they are greater than three feet in width, depth or diameter, they are subject to the height limit.

((5)) 7. Farm Buildings.

Farm buildings such as silos, elevators and barns are exempt from the height limit as long as they are set back from all lot lines at least one foot for every foot in height.

((6)) 8. Utility power poles and public safety facilities are exempt from the height limit.

((7)) 9. Radio and television antennas are subject to the height limit of the applicable zoning category.

((8)) 10. Wireless communication support towers are subject to the height requirements of chapter 17C.355A SMC, Wireless Communication Facilities.

((9)) 11. Uses approved as a conditional use may have building features such as a steeple or tower which extends above the height limit of the underlying zone. Such building features must be set back from the side property line adjoining a lot in a residential zone a distance equal to the height of the building feature or one hundred fifty percent of the height limit of the underlying zone, whichever is lower.

D. Special Height Districts.

Special height districts are established to control structure heights under particular circumstances such as preservation of public view or airport approaches. See [chapter 17C.170 SMC](#), Special Height Overlay Districts.

E. Accessory Structures.

The height of any accessory structure located in the rear yard, including those attached to the primary residence, is limited to twenty feet in height, except a detached ADU above a detached accessory structure may be built to twenty-three feet in height.

Section 3. That SMC section 17C.110.310 is amended to read as follows:

17C.110.310 Attached Housing, Detached Houses on Lots Less than Forty Feet Wide, and Duplexes

A. Purpose.

Attached housing, detached houses on narrow lots and duplexes allow for energy-conserving housing and a more efficient use of land. See definition of attached housing under chapter 17A.020 SMC.



B. Qualifying Situations.

Sites located in the ~~((RSF))~~ RA through the RHD zones. All lots must be under the same ownership or a signed and recorded agreement to participate in an attached housing development must be submitted to the City by all property owners at the time of building permit application.

C. Lot Development Standards.

Each house must be on a lot that complies with the lot development standards in the base zone as provided in Table 17C.110-3.

D. Building Setbacks for Attached Housing.

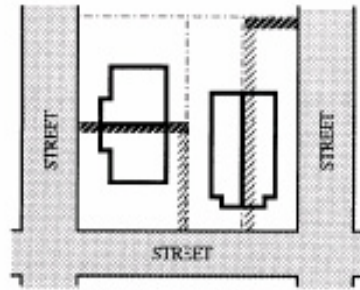
1. Interior Lots.

On interior lots, the side building setback on the side containing the common wall is reduced to zero. ~~((The side building setbacks on the side opposite~~

~~the common wall must be double the side setback standard of the base zone.))~~

2. Corner Lots.

On corner lots, either the rear setback or non-street side setback may be reduced to zero. However, the remaining street side lot line setback must comply with the requirements for a standard side or rear setback.



On corner lots, either the rear setback or the nonstreet side setback can be reduced to zero. However, the remaining nonstreet setback must comply with the requirements for a standard rear setback.

————— Rear lot line
----- Nonstreet side lot line

E. Design Standards.

This section is subject to the provisions of SMC 17C.110.015, Design Standards Administration.

1. A multi-family residential building of three or more units ~~((is))~~ and attached housing in the RMF and RHD zones are subject to the design standards of SMC 17C.110.400 through 17C.110.465.
2. For detached houses on lots forty feet or less wide ~~((and attached housing))~~ and duplexes, where permitted, in the RSF, RSF-C, RTF, RMF and RHD zones, as well as attached housing in the RA, RSF, RSF-C, and RTF zones, the following design standards must be met:
 - a. All street-facing facades must have landscaping along the foundation. There must be at least one three-gallon shrub for every three lineal feet of foundation. (R)
 - b. Sixty percent of the area between the front lot line and the front building line must be landscaped. At a minimum, the required landscaped area must be planted with living ground cover. Up to one-third of the required landscaped area may be for recreational use, or for use by pedestrians. Examples include walkways, play areas, or patios. (R)

- c. ((~~Generous~~)) Use of planting materials and landscape structures such as trellises, raised beds and fencing to unify the overall site design is encouraged, with plantings consistent with L3 open area landscaping standard of SMC 17C.200.030. (P)
- d. Front facade.
Fire escapes, or exterior stairs that provide access to an upper level are not allowed on the front facade of the building. (R)
- e. Duplexes and attached houses on corner lots ((~~shall~~)) should be designed so each unit is oriented towards a different street. This gives the structure the overall appearance of a house when viewed from either street. (R)
- f. Detached houses on lots forty feet or less wide and both units of a duplex or attached houses must meet the following standards to ensure that the units have compatible elements. Adjustments to this paragraph are prohibited, but modifications may be requested through a design departure. The standards are:
 - i. Entrances. Each of the units must have its address and main entrance oriented toward a street frontage. Where an existing house is being converted to two units, one main entrance with internal access to both units is allowed. (R)
 - ii. Each unit must have a covered, main entry-related porch or stoop area of at least fifty square feet with no dimension less than five feet. (R)
 - iii. Buildings must be modulated along the public street at least every thirty feet. Building modulations must step the building wall back or forward at least four feet. (R)
 - iv. Reduce the potential impact of new duplex and attached housing development on established and historic neighborhoods by incorporating elements and forms from nearby buildings. This may include reference to architectural details, building massing, proportionality, and use of high-quality materials such as wood, brick, and stone. (P)
 - v. Create a human scale streetscape by including vertical and horizontal patterns as expressed by bays, belt lines, doors and windows. (P)
- g. Garages are subject to the garage limitation standards of SMC 17C.110.208(E). (R)

- h. Where off-street parking for two or more dwellings will be developed on abutting lots that are each less than forty feet in width, only one curb cut and sidewalk crossing for each two lots may be permitted, to promote pedestrian-oriented environments along streets, reduce impervious surfaces, and preserve on-street parking and street tree opportunities. (P)

F. Number of Units.

1. RA, RSF and RSF-C Zones.
A maximum of two houses may be with a common wall. Structures made up of three or more attached houses are prohibited unless approved as a planned unit development.
2. RTF Zone.
Up to eight attached houses may have a common wall. Structures made up of nine or more attached houses are prohibited unless approved as a planned unit development.
3. RMF and RHD zones.
There is no limit to the number of attached houses that may have common walls.

Section 4. That SMC section 17C.110.360 is amended to read as follows:

17C.110.360 Pocket Residential Development

A. Purpose.

The purpose of the pocket residential development is to:

1. Encourage greater efficiency of land use by allowing compact infill development on aggregate sites.
2. Stimulate new housing that is compatible in scale and character to established surrounding residential areas.
3. Produce a broader range of building forms for residential development.
4. Expand opportunities for affordable home ownership.
5. Promote high quality housing of a character compatible with existing neighborhoods.
6. Encourage adequate, usable open space.

B. Applicability.

Pocket residential development is permitted within the RSF, RSF-C, RTF, RMF, RHD, O, OR, CC, NR, CB, and GC zones.

C. Application Procedure.

Pocket residential development is allowed outright with a building permit. When pocket residential development involves subdivision of land, the application shall be processed in accordance with the procedures of chapter 17G.080 SMC, Subdivisions.

D. Basic Development Standards.

1. Maximum Building Height.
The maximum height of structures within a pocket residential development is as allowed in the underlying zone.
2. Maximum Building Coverage.
The maximum building coverage of the aggregate buildings located upon the parent site shall not exceed the maximum building coverage permitted by the underlying zone. Maximum building coverage is not limited in the O, OR, CC, NR, CB, and GC zones.
3. Setbacks.
Setbacks in a pocket residential development are measured from the exterior boundary of the parent site. The following setbacks are required except in commercial and center and corridor zones where the setbacks are as required in the underlying zoning district.
 - a. Front Setback.
The front yard requirement for the parent site shall be fifteen feet except as allowed under the front yard averaging provisions of SMC 17C.110.220(D)(1).
 - b. Side Setback, Abutting a Residential Zoning District.
If the side yard of the site is adjacent to other residentially zoned property the side yard shall be a minimum of five feet.
 - c. Side Setback, Interior to Parent Site.
If platted, the side yard, interior to the parent site, may be zero, provided, however, that any structure located upon a lot created under SMC 17G.080.065 shall comply with applicable building and fire code and the setbacks applicable to the underlying site development plan.
 - d. Side Setback, Street.
The street side yard requirement for the parent site shall be a minimum of five feet.
 - e. Rear Setback of the Parent Site.
Twenty-five feet or as required in the underlying zoning district.

4. Minimum and Maximum Parent Site Size:
 - a. The minimum parent site size for a pocket residential development is as follows:
 - i. RSF and RSF-C zone: Eight thousand seven hundred square feet.
 - ii. RTF zone: Four thousand two hundred square feet.
 - iii. RMF, RHD zones: Two thousand nine hundred square feet.
 - iv. O, OR, CC, NR, CB, and GC zones: No minimum parent site size.
 - b. The maximum parent site size for a pocket residential development is as follows:
 - i. RSF, RSF-C, and RTF zones: One and a half acres. Pocket residential developments in the RSF, RSF-C, and RTF zones over one and a half acres must be approved as a planned unit development.
 - ii. RMF, RHD, O, OR, CC, NR, CB, and GC zones: No maximum parent site size.
5. Density.

The maximum density allowed in a pocket residential development is limited to that allowed in the underlying zoning district in which the parent site is located, except as permitted by SMC 17C.110.330(C) for transitional sites.
6. Frontage and Access.

Frontage on a public street is not required for lots created in a pocket residential development. Private streets or private access may be used to provide lot frontage when a private street or private access is approved in accordance with chapter 17H.010 SMC. The parent site shall have frontage on a public street sufficient for adequate access and utilities.
7. Parking.

The minimum required off-street parking for a pocket residential development shall comply with the required parking standards of the underlying zone for residential uses in chapter 17C.230 SMC Parking and Loading.
8. Required Outdoor Area.

Pocket residential developments shall comply with the required outdoor area standards of the underlying zone in accordance with SMC 17C.110.223 and Table 17C.110-3 Development Standards. Common outdoor areas designated to meet this requirement will be permanently maintained by the owner or an appropriate property management entity, if under singular ownership. ~~((In the event that the development is subdivided or condominium platted, a homeowners' association is required to be created for the maintenance of the common open space within the development.))~~ This requirement shall be included in deed restrictions as required in SMC 17G.080.065(D).

9. Permitted Housing Types.

The housing types allowed in a pocket residential development are those allowed in the underlying zone in accordance with Table 17C.110-2.

10. Lot Size.

There is no minimum lot size for lots created within a pocket residential development.

E. Design Standards.

This section is subject to the provisions of SMC 17C.110.015, Design Standards Administration. A multi-family residential building of three or more units and attached housing in the RMF and RHD zones are subject to the design standards of SMC 17C.110.400 through 17C.110.470.

1. Ground Level Access.

In order to create the appearance of individual homes, rather than apartments, each attached dwelling unit shall have its own individual access from grade. Stacked units are permitted to have one main entrance with an internal stair accessed from grade to internal individual unit entrances.

Individual Access from Grade



Example of Individual Access for Each Unit

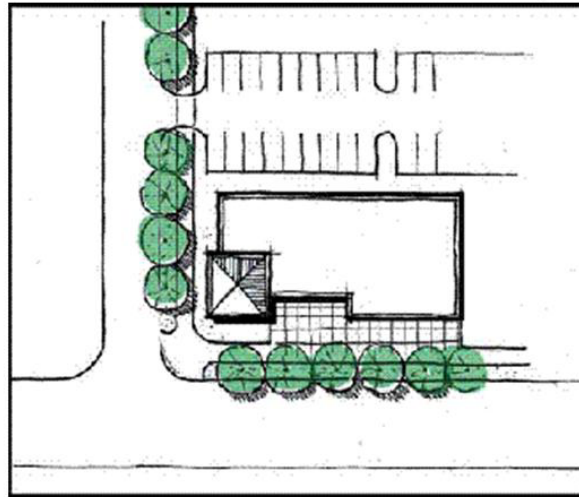


Example of Individual Access with Shared Open Space



2. Parking Lots.
To ensure that parking is as unobtrusive as possible the following standards must be met:
 - a. Alley Access.
If the development abuts an alley, parking must be accessed from the alley.

Example of Surface Parking Accessed from Alley



- b. Screening: Surface parking lots shall be screened both from the street and adjacent residential development by landscape type L2 see-through buffer in SMC 17C.200.030, Landscape Types. Decorative walls or fences no more than forty-two inches in height may be used in lieu of shrubs. Parking is not allowed in a required front yard setback area.

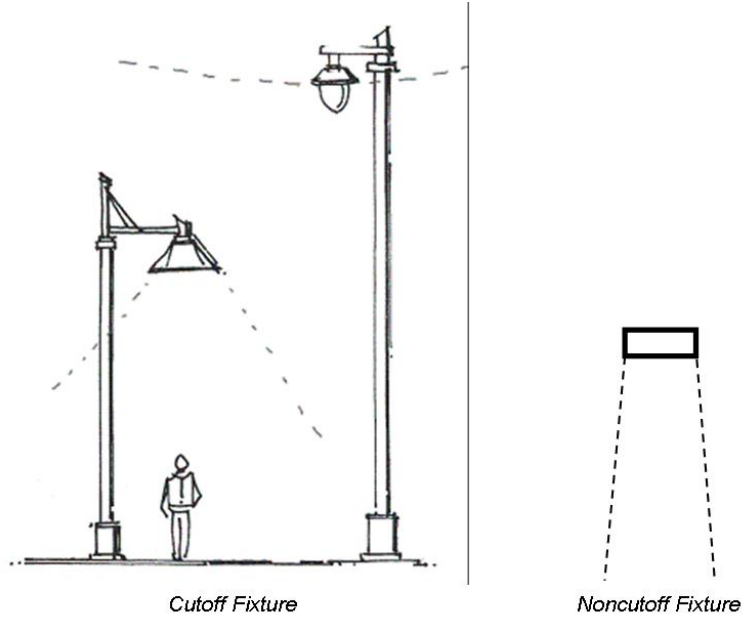
Planting Material Screen



Example of Surface Parking Screened from Street

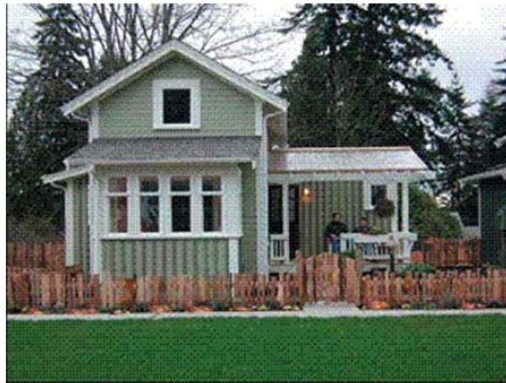


- c. Paving: All surface parking shall be improved in accordance with the standards of SMC 17C.230.140.
- 3. Lighting.
To diminish the amount of glare and spillover from lighting, the following standards shall apply:
 - a. Intensity: Exterior lighting fixtures shall not exceed one foot-candle in intensity.
 - b. Cutoffs Required: Lighting fixtures shall comply with the standards of SMC 17C.220.080



4. Fencing: To ensure a residential atmosphere, fencing higher than forty two inches shall not be permitted along any street frontage.

Residential Fencing



Residential Fence Along Street Frontage No Higher than 42 inches



5. Residential Building Design.

This section is subject to the provisions of SMC 17C.110.015, Design Standards Administration. For pocket residential development, the following design standards must be met:

- a. All street-facing facades must have landscaping along the foundation. There must be at least one three-gallon shrub for every three lineal feet of foundation. (R)
- b. Sixty percent of the area between the front lot line and the front building line must be landscaped. At a minimum, the required landscaped area must be planted with living ground cover. Up to one-third of the required landscaped area may be for recreational use, or for use by pedestrians. Examples include walkways, play areas, or patios. (R)
- c. Use of planting materials and landscape structures such as trellises, raised beds and fencing to unify the overall site design is encouraged, with plantings consistent with L3 open area landscaping standard of SMC 17C.200.030.(P)
- d. Front facade. Fire escapes, or exterior stairs that provide access to an upper level are not allowed on the front facade of the building. (R)
- e. Duplexes and attached houses on corner lots shall be designed so each unit is oriented towards a different street. This gives the structure the overall appearance of a house when viewed from either street. (R)
- f. All units must meet the following standards. Adjustments to this paragraph are prohibited, but modifications may be requested through a design departure. The standards are:
 - i. Entrances. Each of the units fronting on the street must have its address, windows, and main entrance oriented toward a street frontage. Units that are on the interior of a parent site may be oriented toward a private access or shared open space. Where an existing house is being converted to two units, one main entrance with internal access to both units is allowed. (R)
 - ii. Each unit must have a covered, main entry-related porch or stoop area of at least fifty square feet with no dimension less than five feet. (R)
 - iii. Attached units must be modulated along the public street at least every thirty feet. Building modulations must step the building wall back or forward at least four feet. (R)

- iv. Reduce the potential impact of new Pocket Residential Development on established and historic neighborhoods by incorporating elements and forms from nearby buildings. This may include reference to architectural details, building massing, proportionality, and use of high-quality materials such as wood, brick, and stone. (P)
- v. Create a human scale streetscape by including vertical and horizontal patterns as expressed by bays, belt lines, doors and windows. (P)

ORDINANCE NO. _____

The City of Spokane does ordain:

Section ____ That SMC section 17C.230.130 is amended to read as follows:

17C.230.130 Parking Exceptions

- A. In center and corridor downtown, and FBC CA1, CA2, and CA3 zones any new building or building addition with a floor area less than three thousand square feet shall have no parking requirement.
- B. In the neighborhood retail zone, any existing building, new building, or building addition, having a floor area less than three thousand square feet shall have no parking requirement. In addition, if a building has a floor area of five thousand square feet or less, the parking requirement will be determined after deducting the three thousand square foot exemption from the building's floor area. For example, the parking requirement for a four thousand square foot building would be based on one thousand square feet of floor area – i.e., a four thousand square foot building size minus the three thousand square foot exemption.
- C. The director may approve ratios that are higher than the maximum or lower than the minimum if sufficient factual data is provided to indicate that a different amount is appropriate. The applicant assumes the burden of proof. Approval of parking above the maximum shall be conditioned upon increasing the amount of required landscaping by thirty percent. Approval of parking below the minimum shall be conditioned upon the project contributing towards a pedestrian and transit supportive environment both next to the immediate site and in the surrounding area. When determining if a different amount of parking is appropriate, the director shall consider the proximity of the site to frequent transit service, the intensity of the zoning designation of the site and surrounding sites, and the character of the proposed use.
- D. If property owners and businesses establish a parking management area program with shared parking agreements, the director may reduce or waive parking requirements.
- E. Except in the residential single-family and residential two-family zones, existing legal nonconforming buildings that do not have adequate parking to meet the standards of this section are not required to provide off-street parking when remodeling which increases the amount of required parking occurs within the existing structure.

Option 1. F. In the RMF and RHD zones, attached housing built on a lot at least partially within one thousand three hundred twenty feet of a CC, CA, or DT zone or CC3

zoning overlay may use the minimum number of parking spaces required for residential developments in CC1, CC2, or CC3 zones in Table 17C.230-1.

Option 2. F. Attached housing in the RMF and RHD zones may deduct up to six parking spaces from the calculated minimum parking requirements, in addition to other reductions, such as for on-street parking or bicycle parking.

TABLE 17C.230-2 PARKING SPACES BY USE [1] (Refer to Table 17C.230-1 for Parking Space Standards by Zone) CU = Conditional Use			
RESIDENTIAL CATEGORIES			
USE CATEGORIES	SPECIFIC USES	MINIMUM PARKING	MAXIMUM PARKING
Group Living		1 per 4 residents	None
Residential Household Living		1 per unit plus 1 per bedroom after 3 bedrooms; 1 per Accessory Dwelling Unit (ADU); Single Resident Occupancy (SRO) are exempt	None
COMMERCIAL CATEGORIES			
USE CATEGORIES	SPECIFIC USES	MINIMUM PARKING	MAXIMUM PARKING
Adult Business		1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Commercial Outdoor Recreation		20 per acre of site	30 per acre of site
Commercial Parking		Not applicable	None
Drive-through Facility		Not applicable	None
Major Event Entertainment		1 per 8 seats or per CU review	1 per 5 seats or per CU review
Office	General Office	1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
	Medical/Dental Office	1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area

Quick Vehicle Servicing		1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Retail Sales and Service	Retail, Personal Service, Repair-oriented	1 per 330 sq. ft. of floor area	1 per 200 sq. ft. of floor area
	Restaurants and Bars	1 per 250 sq. ft. of floor area	1 per 60 sq. ft. of floor area
	Health Clubs, Gyms, Lodges, Meeting Rooms and similar continuous entertainment, such as Arcades and Bowling Alleys	1 per 330 sq. ft. of floor area	1 per 180 sq. ft. of floor area
	Temporary Lodging	1 per rentable room; for associated uses such as Restaurants, see above	1.5 per rentable room; for associated uses such as Restaurants, see above
	Theaters	1 per 4 seats or 1 per 6 feet of bench area	1 per 2.7 seats or 1 per 4 feet of bench area
	Retail sales and services of large items, such as appliances, furniture and equipment	1 per 1,000 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Mini-storage Facilities		Same as Warehouse and Freight Movement	Same as Warehouse and Freight Movement
Vehicle Repair		1 per 750 sq. ft. of floor area	1 per 200 sq. ft. of floor area
INDUSTRIAL CATEGORIES			
USE CATEGORIES	SPECIFIC USES	MINIMUM PARKING	MAXIMUM PARKING
Industrial Services, Railroad Yards, Wholesale Sales		1 per 1,000 sq. ft. of floor area	1 per 200 sq. ft. of floor area

Manufacturing and Production		1 per 1,000 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Warehouse and Freight Movement		1 per 1,000 sq. ft. of floor area for the first 3,000 sq. ft of floor area and then 1 per 3,500 sq. ft. of floor area thereafter	1 per 200 sq. ft. of floor area
Waste-related		Per CU review	Per CU review
INSTITUTIONAL CATEGORIES			
USE CATEGORIES	SPECIFIC USES	MINIMUM PARKING	MAXIMUM PARKING
Basic Utilities		None	None
Colleges		1 per 600 sq. ft. of floor area exclusive of dormitories, plus 1 per 4 dorm rooms	1 per 200 sq. ft. of floor area exclusive of dormitories, plus 1 per 2.6 dorm room
Community Service		1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Daycare		1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Medical Centers		1 per 500 sq. ft. of floor area	1 per 200 sq. ft. of floor area
Parks and Open Areas		Per CU review for active areas	Per CU review for active areas
Religious Institutions		1 per 100 sq. ft. of main assembly area or per CU review	1 per 60 sq. ft. of main assembly area
Schools	Grade, Elementary, Junior High	1 per classroom	2.5 per classroom
	High School	7 per classroom	10.5 per classroom
OTHER CATEGORIES			
USE CATEGORIES	SPECIFIC USES	MINIMUM PARKING	MAXIMUM PARKING
Agriculture		None or per CU review	None or per CU review

Aviation and Surface Passenger Terminals		Per CU review	Per CU review
Detention Facilities		Per CU review	Per CU review
Essential Public Facilities		Per CU review	Per CU review
Wireless Communication Facilities		None or per CU review	None or per CU review
Rail Lines and Utility Corridors		None	None
[1] The director may approve different amounts of parking spaces under the exceptions listed in SMC 17C.230.130.			

Section __. That SMC section 17C.230.140 is amended to read as follows:

17C.230.140 Development Standards

- A. Purpose
The parking area layout standards are intended to promote safe circulation within the parking area and provide for convenient entry and exit of vehicles.
- B. Where These Standards Apply
The standards of this section apply to all vehicle areas whether required or excess parking.
- C. Improvements
 - 1. Paving.
In order to control dust and mud, all vehicle areas must be surfaced with a minimum all-weather surface. Such surface shall be specified by the city engineer. Alternatives to the specified all-weather surface may be provided, subject to approval by the city engineer. The alternative must provide results equivalent to paving. All surfacing must provide for the following minimum standards of approval:
 - a. Dust is controlled.
 - b. Stormwater is treated to City standards; and
 - c. Rock and other debris is not tracked off-site.

The applicant shall be required to prove that the alternative surfacing provides results equivalent to paving. If, after construction, the City determines that the alternative is not providing the results equivalent to

paving or is not complying with the standards of approval, paving shall be required.

2. Striping.

All parking areas, except for stacked parking, must be striped in conformance with the parking dimension standards of subsection (E) of this section, except parking for single-family residences, duplexes, and accessory dwelling units.

3. Protective Curbs Around Landscaping.

All perimeter and interior landscaped areas must have continuous, cast in place, or extruded protective curbs along the edges. Curbs separating landscaped areas from parking areas may allow stormwater runoff to pass through them. Tire stops, bollards or other protective barriers may be used at the front ends of parking spaces. Curbs may be perforated or have gaps or breaks. Trees must have adequate protection from car doors as well as car bumpers. This provision does not apply to single-family residence, duplexes and accessory dwelling units.

D. Stormwater Management

Stormwater runoff from parking lots is regulated by the engineering services department.

E. Parking Area Layout

1. Access to Parking Spaces.

All parking areas, except stacked parking areas, must be designed so that a vehicle may enter or exit without having to move another vehicle.

2. Parking Space and Aisle Dimensions.

a. Parking spaces and aisles in RA, RSF, RSF-C, RTF, RMF, RHD, FBC CA4, O, OR, NR, NMU, CB, GC, and industrial zones must meet the minimum dimensions contained in Table 17C.230-3.

b. Parking spaces and aisles in Downtown CC, and FBC CA1, CA2, CA3 zones must meet the minimum dimensions contained in Table 17C.230-4.

c. In all zones, on dead end aisles, aisles shall extend five feet beyond the last stall to provide adequate turnaround.

3. Parking for Disabled Persons.

The city building services department regulates the following disabled person parking standards and access standards through the building code and the latest ANSI standards for accessible and usable buildings and facilities:

- a. Dimensions of disabled person parking spaces and access aisles.
 - b. The minimum number of disabled person parking spaces required.
 - c. Location of disabled person parking spaces and circulation routes.
 - d. Curb cuts and ramps including slope, width and location; and
 - e. Signage and pavement markings.
4. A portion of a standard parking space may be landscaped instead of paved, as follows:
- a. The landscaped area may be up to two feet of the front of the space as measured from a line parallel to the direction of the bumper of a vehicle using the space, as shown in Figure 17C.230-3. Any vehicle overhang must be free from interference from sidewalks, landscaping, or other required elements.

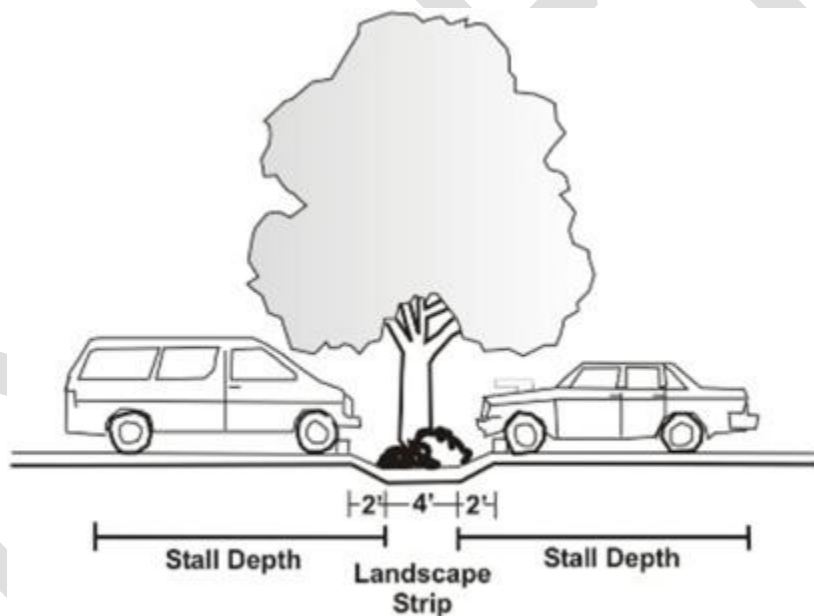


Figure 17C.230-3 Landscaped area at front of parking space

- b. Landscaping must be ground cover plants; and
 - c. The landscaped area counts toward parking lot interior landscaping requirements and toward any overall site landscaping requirements. However, the landscaped area does not count toward perimeter landscaping requirements.
5. Engineering Services Department Review

The engineering services department reviews the layout of parking areas for compliance with the curb cut and access restrictions of [chapter 17H.010 SMC](#).

Table 17C.230-3
RA, RSF, RSF-C, RTF, RMF, RHD, FBC CA4, O, OR, NMU, CB, GC and Industrial Zones
Minimum Parking Space and Aisle Dimensions [1, 2]

Angle (A)	Width (B)	Curb Length (C)	1-way Aisle Width (D)	2-way Aisle Width (D)	Stall Depth (E)
0° (Parallel)	8 ft.	20 ft.	12 ft.	22 ft.	8 ft.
30°	8 ft. 6 in.	17 ft.	12 ft.	22 ft.	15 ft.
45°	8 ft. 6 in.	12 ft.	12 ft.	22 ft.	17 ft.
60°	8 ft. 6 in.	9 ft. 9 in.	16 ft.	22 ft.	18 ft.
90°	8 ft. 6 in.	8 ft. 6 in.	22 ft.	22 ft.	18 ft.

Notes:

[1] See Figure 17C.230-4.

[2] Dimensions of parking spaces for the disabled are regulated by the building code.
 See [SMC 17C.230.140\(E\)\(3\)](#).

Table 17C.230-4
Downtown, CC, NR, FBC CA1, CA2, and CA3 Zones
Minimum Parking Space and Aisle Dimensions [1, 2]

Angle (A)	Width (B)	Curb Length (C)	1-way Aisle Width (D)	2-way Aisle Width (D)	Stall Depth (E)
0° (Parallel)	8 ft.	20 ft.	12 ft.	20 ft.	8 ft.
30°	8 ft. 6 in.	17 ft.	12 ft.	20 ft.	15 ft.
45°	8 ft. 6 in.	12 ft.	12 ft.	20 ft.	17 ft.
60°	8 ft. 6 in.	9 ft. 9 in.	16 ft.	20 ft.	17 ft. 6 in.
90°	8 ft. 6 in.	8 ft. 6 in.	20 ft.	20 ft.	16 ft.

Notes:

[1] See Figure 17C.230-4.

[2] Dimensions of parking spaces for the disabled are regulated by the building code. See [SMC 17C.230.140\(E\)\(3\)](#).

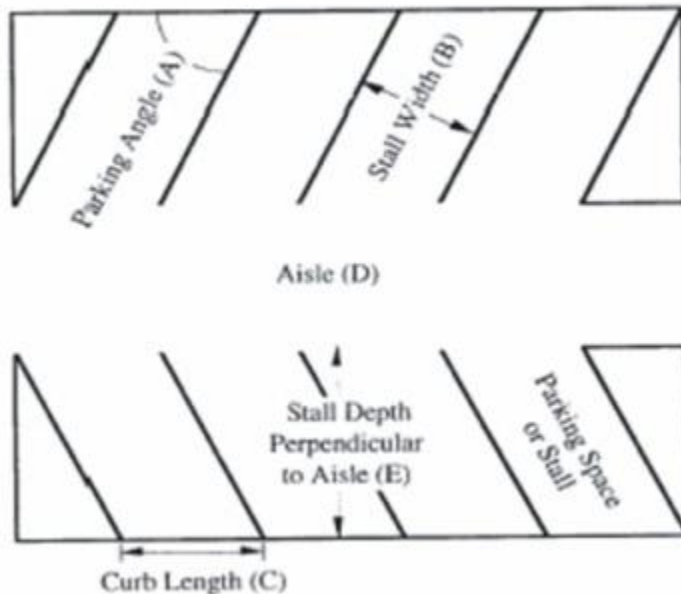
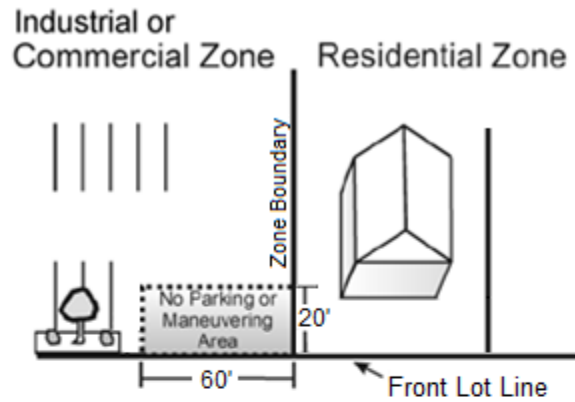


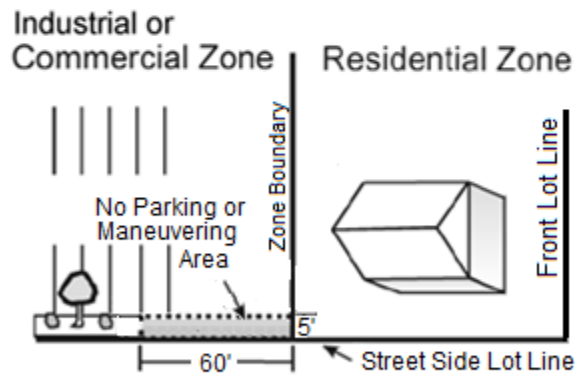
Figure 17C.230-4 Parking Dimension Factors

F. Parking Area Setbacks and Landscaping

1. For parking areas on sites abutting residential zoning districts, parking spaces or maneuvering areas for parking spaces, other than driveways that are perpendicular to the street, are ~~((not allowed within the first twenty feet from a street lot line for the first sixty feet from the boundary of))~~ required to be setback a distance equal to the setback specified in SMC 17C.230.145(C)(1) of the adjacent residential zoning district for the first sixty feet from the zoning district boundary (Figure 17C.230-5).



A. Setback adjacent to front lot line.



B. Setback adjacent to street side lot line.

Figure 17C.230-5 Parking Area Setback

[Note: Add the graphic above.]

2. All landscaping must comply with the standards of [chapter 17C.200 SMC](#), Landscaping and Screening.

ORDINANCE NO. _____

An ordinance relating to alternative residential subdivisions, amending Spokane Municipal Code (SMC) sections 17G.080.065.

The City of Spokane does ordain:

Section 1. That SMC section 17G.080.065 is amended to read as follows:

17G.080.065 Alternative Residential Subdivisions

A. Purpose.

The purpose of these provisions is to allow for the creation of lots for alternative residential development as described in SMC 17C.110.300, including attached housing, cottage housing, and similar developments with multiple dwelling units on a parent site, while applying only those site development standards applicable to the parent site as a whole, rather than to individual lots resulting from the subdivision.

B. Applicability.

The types of ~~((existing))~~ development that may use the alternative residential subdivision are:

1. Cottage housing projects approved under SMC 17C.110.350;
2. Housing developed under SMC 17C.110.360 Pocket Residential Development; or
3. A similar existing development that consists of multiple dwelling units on a single parcel or site, provided that such existing structures shall comply with applicable building and fire code.

C. Application Procedure.

Alternative residential subdivisions of nine or fewer lots shall be processed as short plats and all others shall be processed as subdivisions according to the associated permit types in SMC chapter 17G.060.

D. General Regulations.

**ATTACHED HOUSES
OPTION 1 - Change
requirements for
attached houses to
follow multifamily
design standards
instead of pocket
residential.**

1. An alternative residential subdivision shall meet development standards applicable to the underlying site development plan approval, if any, the basic development standards and design standards of SMC 17C.110.350 Cottage Housing, ~~((or))~~ SMC 17C.110.360 Pocket Residential Development, or design standards of SMC 17C.110.400 through 17C.110.465 for attached housing in

RMF and RHD zones, and the provisions of this section. As a result of the alternative residential subdivision, development on individual lots may be nonconforming as to some or all of the development standards based on analysis of the individual lot. So long as the parent site meets the criteria of the underlying site development plan or the dwelling units are already in existence, each lot will be deemed to be in conformance. If existing dwelling units do not comply with development standards (i.e.: minimum building setbacks, maximum density, etc.), a lot may be created for each existing dwelling unit. Subsequent platting actions, additions or modifications to the structure(s) may not create or increase any nonconformity of the parent site;

2. Alternative residential subdivisions shall be subject to all applicable requirements of Title 17 SMC, except as otherwise modified by this section;
3. Each lot's area and width for purposes of subdivision may be as small as the footprint of the individual dwelling unit;

**ATTACHED HOUSES
OPTION 1 - Change
requirements for
subdivisions to
respond to situations
where smaller
developments will not
require homeowners'
associations.**

4. Portions of the parent site not subdivided for individual lots shall be owned in common by the owners of the individual lots, or by a homeowners association comprised of the owners of the individual lots located within the parent site. ~~((A homeowners' association is required to be created for the maintenance of any shared required outdoor area or other open space, shared parking areas, and other common use areas, buildings, and utilities within the development.))~~ This requirement shall be included in deed restrictions as required in paragraph 7;

5. Maximum building coverage of the aggregate buildings located upon the parent site shall not exceed the maximum building coverage permitted by the underlying zone;
6. Except for existing nonconforming development, building setbacks shall be as required for the zone as applied to the underlying parent site as a whole. There shall be no setback required from individual lot lines which are interior to the perimeter of the parent site; provided, however, that any structure located upon a lot created hereunder shall comply with the setbacks applicable to the underlying site development plan;
7. Access easements, joint use and maintenance agreements, and covenants, conditions and restrictions identifying the rights and responsibilities of

property owners and/or the homeowners association shall be executed for use and maintenance of common garage, parking and vehicle access areas; on-site recreation; landscaping; utilities; common open space; exterior building facades and roofs; and other similar features, and shall be recorded with the county auditor's office. Separation requirements for utilities must be met. Each alternative residential subdivision shall make adequate provisions for ingress, egress and utilities access to and from each lot created by reserving such common areas or other easements over and across the parent site as deemed necessary to comply with all other design and development standards generally applicable to the underlying site development plan ((:)) .

8. Notes shall be placed on the plat recorded with the county auditor's office to acknowledge the following:
 - a. Approval of the design and layout of the development was granted by the review of the development, as a whole, on the parent site by the site development plan approval (stating the subject project file number if applicable);
 - b. Subsequent platting actions, additions or modifications to the structure(s) may not create or increase any nonconformity of the parent site as a whole, and shall conform to the approved site development plan;
 - c. If a structure or portion of a structure has been damaged or destroyed, any repair, reconstruction or replacement of the structure(s) shall conform to the approved site development plan;
 - d. Additional development of the individual lots may be limited as a result of the application of development standards to the parent site.

E. Conflicts.

Any conflicts between the provisions of this section and the text of other sections in the Unified Development Code shall be resolved in favor of the text of this section.

City of Spokane Plan Commission Training
Spokane City Hall
Tuesday, October 30, 2018
12:00 pm to 4:00 pm

AGENDA

I. Welcome and Introductions - 12:00 pm to 12:15 pm

II. Planning Theory and Practice for Planning Commissions – Joe Tovar, FAICP, Tovar Planning

Part I Overview of Planning in the State of Washington - 12:15 pm to 1:15 pm

- Why is land use so important? It shapes, and is shaped by, almost every aspect city life
- How did we get here? A brief history of land use planning – Hammurabi to Euclid to Hoover
- Washington State - diverse physical, economic and social geographies – so what?
- Washington's GMA approach – a middle path between bottom up and top down
- Q & A

Part II Organizing for a Shared Mission – Planning Commissions Best Practices - 1:15 pm- 3:15pm

- The shared mission
- City Council, Planning Commission, and City staff roles
- City Council, Planning Commission, and City staff prerogatives and obligations
- Public input - how to be both responsive and responsible
- Q & A

Break 2:15

- Ways to maintain communication and build trust
- The planning commission public meetings – do's and don'ts
- Don't be this guy
- Q & A

Part III Perspectives, Trends, Challenges and Innovations - 3:15 pm to 4:00 pm

- The perspective of planning – the big picture, the long-term, the inter-relatedness of decisions
- Global and National trends
- Washington specific challenges
- Innovations in Planning Practice
- Q & A

III. Adjournment – 4:00 pm