



# Spokane Design Review Board

Wednesday, September 13, 2023

5:30-7:00 PM

Tribal Conference Room

Hybrid Meeting – Teleconference and In-person

**TIMES GIVEN ARE AN ESTIMATE AND ARE SUBJECT TO CHANGE**

## Board Briefing Session:

5:30 – 5:40	1) Call to Order	Chair
	2) Roll Call	Dean Gunderson
	3) Changes to the Agenda?	Chair

## Workshop:

5:40 – 6:45	4) <a href="#">Adams Elementary School – Recommendation Meeting</a>	Taylor Berberich
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## Board Business:

6:45 – 7:00	5) <a href="#">Approve Minutes from August 23, 2023</a>	Chair
	6) Old Business	
	7) New Business	
	8) Chair Report	Chair
	9) Secretary Report	Dean Gunderson
	10) Other	
	11) Adjourn	

**The next Design Review Board meeting is scheduled for Wednesday, September 27, 2023.**

The password for City of Spokane Guest Wireless access has been changed:

**Username: COS Guest**

**Password: K8vCr44y**

**AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION:** The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. The Tribal Conference Room in the first-floor lobby of City Hall, 808 W. Spokane Falls Blvd., is wheelchair accessible. Individuals requesting reasonable accommodations or further information may call, write, or email Risk Management at 509.625.6221, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or [mlovmaster@spokanecity.org](mailto:mlovmaster@spokanecity.org). Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.

# The Design Review Board meeting will be held in a hybrid format

Participants are able to join the meeting in-person in the Tribal Conference Room (City Hall, First Floor Lobby), or join the meeting on-line using the following information:

To participate via video follow the link on your computer (click on “Join meeting”)

## [Join Meeting](#)

### Microsoft Teams meeting

**Join on your computer, mobile app or room device**

[Click here to join the meeting](#)

Meeting ID: 221 031 683 594

Passcode: KxxG7U

[Download Teams](#) | [Join on the web](#)

**Join with a video conferencing device**

cityofspokane@m.webex.com

Video Conference ID: 112 539 437 5

[Alternate VTC instructions](#)

**Or call in (audio only)**

[+1 323-618-1887,,943158105#](#) United States, Los Angeles

Phone Conference ID: 943 158 105#

[Find a local number](#) | [Reset PIN](#)

[Learn More](#) | [Meeting options](#)

While the meeting begins at 5:30pm, you can join as early as 5:15pm on the date of the meeting.

Dean Gunderson, Sr. Urban Designer

[dgunderson@spokanecity.org](mailto:dgunderson@spokanecity.org)

The proceedings of the Design Review Board meeting will be recorded, with digital copies made available upon request.

## **Meeting Process - Spokane Design Review Board**

### **Call to Order**

- Chair calls the meeting to order, noting the date and time of the meeting.
- Chair asks for roll call for attendance.
- Chair asks if there any changes to the agenda.

### **Board Workshop**

- Chair announces the first project to be reviewed and notes the following: a) the Board will consider the design of the proposal as viewed from the surrounding public realm; b) the Board does not consider traffic impacts in the surrounding area or make recommendations on the appropriateness of a proposed land use; c) the Board will not consider un-permitted, possible surrounding development(s) except those which are contemplated under the Comprehensive Plan and Development Code; c) it is the applicant's responsibility to meet all applicable Code requirements regardless of what might be presented or discussed during workshops.
- Chair asks for a staff report.

#### ***Staff Report***

- Staff report on the item, giving findings of fact. Presentation will be kept to 5-10 minutes.

#### ***Applicant Presentation***

- Chair invites the applicant(s) to introduce the project team and make a 10-15 minute presentation on the project.

#### ***Public Comment***

#### ***DRB Clarification***

- Chair may request clarification on comments.

#### ***Design Review Board Discussion***

- Chair will ask the applicants whether they wish to respond to any written public comments, after their response (if any) they are to return to their seats in the audience.
- The Chair will formally close public comments (unless motioned otherwise).
- Chair leads discussion amongst the DRB members regarding the staff topics for discussion, applicable design criteria, identification of key issues, and any proposed design departures.

#### ***Design Review Board Motions***

- Chair asks whether the DRB is ready to make a motion.
- Upon hearing a motion, Chair asks for a second. Staff will record the motion in writing.
- Chair asks for discussion on the motion.
- Chair asks the applicant if they would like to respond to the motion.
- After discussion, Chair asks for a vote.

#### ***Design Review Board Follow-up***

- Applicant is advised that they may stay or leave the meeting, and that the annotated & signed motion will be made available within five working days.
- Next agenda item announced.

### **Board Business**

- Meeting Minutes - Chair asks for comments on the minutes of the last meeting; Asks for a motion to approve the minutes.
- Chair asks is there any old business? Any old business is discussed.
- Chair asks is there any new business? Any new business is discussed.
- Chair Report – Chair gives a report.
- Secretary Report – Sr. Urban Designer gives a report.

### **Adjourn**

Chair asks for a motion to adjourn. After the motion is seconded, and approved by vote, Chair announces that the meeting is adjourned, noting the time of the adjournment.

# Design Review Board - Meeting Minutes

**August 23, 2023**

**Hybrid City Hall Tribal Room/Teams**

Meeting called to order at 5:30 PM by Mark Brower

## Attendance:

- *Board Members Present:* Mark Brower (Chair), Chuck Horgan (Vice-Chair & Arts Commission Liaison), Chad Schmidt, Ryan Brodwater, Drew Kleman, Grant Keller, Bob Scarfo
- *Board Members Not Present:*
- *Quorum Present:* Yes
- *Staff Members Present:* Dean Gunderson, Taylor Berberich, Ryan Benzie

## Changes to Agenda:

- None

## Workshop:

### **1. Collaborative Workshop for U-Haul - 1616 Rustle Road**

- Staff Report: Dean Gunderson
- Applicant Presentation: Casey Jones
- Questions asked and answered
- Public comments were solicited, public comment period was closed
- Discussion ensued

Chuck Horgan made a motion to approve the recommendations as read, Ryan Brodwater seconded. Motion passed unanimously (7/0).

## Board Business:

**Approval of Minutes:** Chad Schmidt made a motion to approve the minutes from the July 26, 2023 meeting approved; Drew Kleman seconded. Motion passed unanimously.

## **Old Business:**

- None

## **New Business:**

- None

## **Chair Report - Mark Brower**

- None

## **Secretary Report - Dean Gunderson**

- Dean reported that the applicant for Adams Elementary has submitted their recommendation meeting packet to be reviewed at the September 13, 2023 board meeting.
- Dean reported that staff is continuing to work on the Design Review Handbook.
- The permanent replacement for BOCA is in progress and will appear before the Plan Commission in the near future.

**Meeting Adjourned at 7:47 PM**

Next Design Review Board Meeting scheduled for Wednesday, September 13, 2023



# Project Name

## 1 – RECOMMENDATION MEETING

### Design Review Staff Report

September 8, 2023



#### Staff:

**Dean Gunderson**  
Senior Urban Designer

**Taylor Berberich**  
Urban Designer

Planning and Economic  
Development Services  
808 W. Spokane Falls Blvd.  
Spokane, WA 99201

#### Applicants:

##### ATTN:

Greg Forsyth, Spokane Public Schools

Melissa McFadgen, NAC Architecture

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## Background

The Design Review Board Collaborative Workshop was held on June 28, 2023.

*The following materials are supplemental to this report:*

- *Design Review Staff Report | Program Review/Collaborative Workshop, June 28, 2023;*
- *Design Review Board | Collaborative Workshop Advisory Actions, June 30, 2023;*

## Responses to Discussions Held at Collaborative Workshop

During the workshop, the applicant is encouraged to please describe changes to the design since the Collaborative Workshop including any changes made in response to advisory actions offered by the Design Review Board on June 28 2023 as follows:

### SUMMARY OF DESIGN CHANGES SINCE COLLABORATIVE WORKSHOP:

Since the Design Review Board Collaborative Workshop, the site design has been developed to consistently create a layer of landscaping to filter views between the building and the street. Paths through the site have a flow or “meander” to capture the feeling of walking on a path through clusters of trees in a grove. Additionally, the site structures such as the playground entry and reader board have been designed with materials, details, and ornament matching those used on the elementary school building.

The exterior materials and ornament of the building have been developed to create an orderly “frame and infill” rhythm of solid wall and window openings on the brick portions of the building. This “frame and infill” is emphasized through different brick textures, details, and ornament so it is clearly visible from the street. The overhanging portion of the building on the North elevation and the upper level portion of the building on the West and South elevation has a warm neutral color using a metallic panel to complement the brick color.

To create a welcoming presence for the elementary students attending the school, the building incorporates some accent colors and textures on the building exterior. These colors and textures appear together on the bay windows into classrooms. The colors appear alone in glazed brick bands and the textures appear alone in windows across the project. These “elementary” accent colors and textures appear in modest amounts so that they’re mainly experienced by students, parents, teachers, and visitors arriving at the school.

Additional changes are noted in the responses to the advisory actions below.

#### **DRB COLLABORATIVE WORKSHOP ADVISORY ACTIONS & APPLICANT RESPONSES FOR RECOMMENDATION MEETING:**

**1. The Applicant should continue investigating the opportunity to provide non-motorized access through the site along the vacated Fiske right-of-way (ROW).**

Applicant response:

In an effort to accommodate a future non-motorized pathway along the vacated Fiske ROW, the perimeter fence on the east side of the site has been re-located in the updated site plan design to allow space for this access.

*Bike and Pedestrian Coordinator comment:*

*Can a 10-12 foot swath or clear zone along the driveway access also be maintained for future construction of a wide sidewalk or pathway? Not requiring construction but creating the clear space for the pathway when the greenway is completed to the north.*

**2. The Applicant is strongly encouraged to retain existing mature trees where feasible.**

Applicant response:

The project will make every effort to retain existing mature trees where feasible. The Adams Elementary site is one of the smallest school sites in the district and the design is attempting to find the right balance between the need for student playground area, resolving the parking and drop-off/pick-up concerns, and maintaining existing mature trees. A handful of pine trees are being removed from the Fiske ROW to build the driveway to the main parking lot, more than three-quarters of the existing trees in that area will be protected. The deciduous trees along the west half of the site near 37th will be impacted by construction activities for the new building location. These trees are currently too far from the sidewalk to provide much pedestrian shade, especially because this side is exposed to the southern sun. The current plan will retain the three mature deciduous trees along 36th near the Regal intersection.

**3. The Board supports the Applicant’s intent to utilize clusters of trees in lieu of City standard street tree configuration. The Applicant is strongly encouraged to find opportunities to increase the quantity of trees above those otherwise required by code.**

Applicant response:

The current planting plan is provided in the recommendation meeting application. Thank you for supporting the intent to utilize clusters of trees. The new planting plan shows that the school will have many more trees than are required by code and will enhance the pedestrian experience along all street frontages. Screening parking and shading hardscape areas will also be provided by the new tree layout. The new student courtyard and playground will be enhanced with multiple types of plants and trees around the perimeters, leaving the center areas open for gathering, outdoor learning, PE, and recess activities.

**4. The Applicant shall return with street level perspectives of the project that capture pedestrian and vehicular views from the public realm.**

Applicant response:

Street level perspective renderings that capture the pedestrian and vehicular views from the public realm have been provided within this recommendation meeting application submittal. The design of the building and the site have been developed with further detail to create a welcoming and friendly streetscape presence along all three frontages while also maintaining a safe environment for the building's staff and students throughout the school day. The current design will greatly improve the experience of the entering the site and approaching the front door over the existing condition that is entirely surrounded by tall fences and retaining walls. Along 36th, the parent drop-off and pick-up lane and sidewalk has been enhanced with landscaping and street tree clusters, as well as a special entry gate that celebrates the arrival of the students to the playground each morning. This will also be the main entrance to the playground for the public when using the site like a neighborhood park if school is not in session.

- 5. The Board appreciates the Applicant's intent to soften the grades around the perimeter of the site and provide adequate pedestrian queuing space at Regal St. & 37th Ave.**

Applicant response:

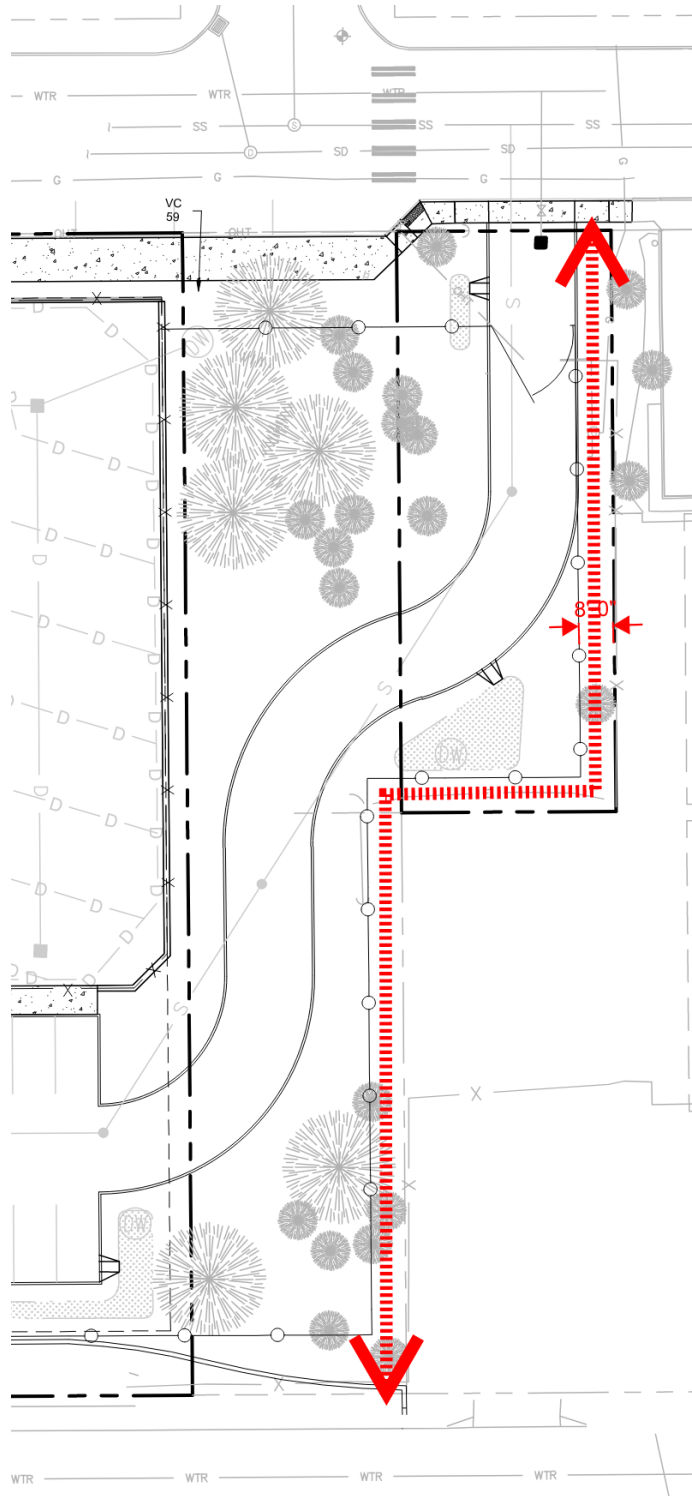
Thank you. Yes, the intention is to eliminate as many of the retaining walls as possible and increase the number of plantings and greenery that will enhance the pedestrian experience along the streetscape. New concrete sidewalk has been added to the corner of Regal & 37th to provide expanded pedestrian queuing.

- 6. The Applicant is strongly encouraged to continue investigating options to soften the vehicle pull-out area on 36th Ave. through landscaping, hardscape material change, or other designs.**

Applicant response:

Please review the current site plan and planting plan. The vehicle pull-out sidewalk width is generous enough for pedestrians to pass as cars are unloading occupants. The playground fence has been moved south to provide for a landscaping strip between the sidewalk and the fence. Trees have been added where appropriate, especially to highlight the playground entry gate experience. Brick pillars with integral lighting are proposed at the entry gate landing area, as well as seat wall benches for parents to wait for their children under the tree canopy. An ADA compliant access ramp has also been provided for safe travel from the sidewalk to the playground level.

At the request of ICM staff, the applicant provided an updated screenshot of the intended location of the shared use path. There is an 8' space outside the school fence on the west side of the proposed access drive. See image below:



# Staff Comments on Applicant's Submittal

## Current Planning Staff

At the June 15, 2023 pre-development conference with the City of Spokane, the applicant was informed that the project would be required to include separated sidewalks. The current proposal shows attached sidewalks throughout and little to no barriers between vehicular and pedestrian traffic. The final construction permit application will need to show separated sidewalks on Regal St, 36<sup>th</sup> Ave and 37<sup>th</sup> Ave. **See additional topic for consideration #1.**

## Urban Forestry Staff

1. "They are planning a lot of evergreens near the street along 36<sup>th</sup> & 37<sup>th</sup>. As these would likely be trees that are going to count as their street tree requirement, the species' need to be chosen from the Approved Street Tree list and Class II trees. We would also need a Street Tree Agreement signed by the school district stating that the trees will be maintained as if they were street trees (arborist with tree permit required for all work on them, including planting), permission from Urban Forestry prior to removal, replacement trees required.
2. Add more trees along Regal and 36<sup>th</sup> Avenue.
3. They will need to show all existing trees on their demo plans with each tree designated as 'protect in place' or 'remove'.
4. For the area to the east, this is the undeveloped extension of Fiske St. It shows up as right of way. Is this public land or school district land? If it is public, we would like to see retention and protection of the health native trees there. UF will visit the site to assess tree health and make recommendations on which trees should be retained and protected and which trees we would allow to be removed. Please have them design the pathway to retain healthy trees noted by UF.
5. All retained trees must have tree protection fencing installed around them per the City of Spokane Tree Protection Standards and Specifications prior to any site/soil/demo work and fencing must remain intact throughout all phases of construction. This note should be on the demo/civil plans as well as the Standards and Specs.
6. Trees that are within 10' feet of excavation for buildings, sidewalks, pathways, etc will require a city licensed arborist to perform root treatments on them prior to excavation for these features.
7. Urban Forestry will require a site visit for a pre-construction meeting with UF staff, the arborist, the GC, and a school district representative to talk about which trees will be impacted, how to minimize impacts, and a plan to care for the trees during construction."

Many of these points raised by staff will be addressed in the construction permit application and are outside the purview of the DRB. However, the need for more street trees along 36<sup>th</sup> Street could be a potential discussion point. **See additional topic for consideration #2.**

## Planning Staff- Bike and Pedestrian Coordinator

- The alignment shown looks great.
- Where possible, 10' width is considered minimum for a shared-use path.
- 8' is acceptable width around pinch-points or obstructions, such as possibly around the trees shown toward the north end of the path.

Details below from the AASHTO Guide for the Development of Bicycle Facilities:

See page 131 of the [AASHTO Guide for the Development of Bicycle Facilities](#), excerpt attached also.

The standard is:

- 10' minimum with 2' clear zones on both sides.
- 11-14' width recommended in high-demand locations.
- 8' width is acceptable for short sections in highly-constrained locations.

**See additional topic for consideration #3.**

## **Integrated Capital Management (ICM) Staff**

1. *That Fiske ROW is currently fenced off. We need to discuss placement of a pedestrian pathway through that ROW as a condition of the vacation or other agreement with the City to use the ROW. Without that walkway we have a 1300' block face and pedestrians can't go from one side to the other without walking on the arterials. Fiske is designated as a Neighborhood Greenway route, and while we weren't expecting to use this ROW for a bike route there could be foot-traffic that wants to use it. Especially since STA has a route with stops on 37th between Regal and Ray.*
2. *It would be helpful if they could share the designated walk routes to and from the site and whether any improvements should be rolled into the project, for example curb extensions at 36th/Fiske to shorten the crossing distance and make kids more visible.*
3. *36th Avenue is 35' wide. That's sufficient width to have the entire south side be a parent drop-off zone and they do not need the extra 8' deep pullout area. That will give them a little more space on-site for landscape buffer or grass play area.*
4. *Regarding the vacation of the ROW: per City code, a vacation splits the land down the middle. The Applicant will need to arrange an agreement with the adjacent apartment owners to buy the western portion of the vacated ROW.*

**See additional topic for consideration #4 and #5.**

## **Additional Suggested Topics for Consideration**

Posed by staff based on the August 21, 2023 submittal:

1. To address the concerns raised by Current Planning, is there an opportunity to provide a safer, more engaging pedestrian environment on the street frontages?
2. To address the concerns raised by Urban Forestry, what opportunities are available to add more street trees along 36th Street?
3. To address the concerns raised by the City's Bike and Pedestrian Coordinator, could the applicant maintain a 10-12 foot clear space on the north side of the drive access on the Fiske ROW to accommodate future construction of an adjacent pathway?
4. To address the concerns raised by Integrated Capital Management (ICM) staff, is there an opportunity to provide curb extensions at 36th/Fiske to shorten the crossing distance and make kids more visible?
5. Is there an opportunity to reconsider the parent drop off areas to allow more room for on-site landscape buffers or grass play area, as recommended by ICM staff?

## **Supplementary Documents**

Please see the meeting packet for the Pre-development Conference notes and the Applicant's submittal packet.

## **Note**

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

## **Policy Basis**

Spokane Municipal Codes  
City of Spokane Comprehensive Plan  
Public Project Design Guidelines



**NAME OF PROJECT:**

Adams Elementary School Replacement - RECOMMENDATION MEETING APPLICATION

**ADDRESS:**

2909 E 37th Ave, Spokane, WA 99223

**TYPE OF PROJECT:**

- |   |  |
|---|--|
| <input checked="" type="checkbox"/> Public Project        | <input type="checkbox"/> Required by CBD Zones and Downtown Plan |
| <input type="checkbox"/> Shoreline Conditional Use Permit | <input type="checkbox"/> Design Departure                        |
| <input type="checkbox"/> Skywalk Over Public ROW          |  |

**FEES:**

- |   |  |
|---|--|
| Standard Board Review   |  |
| <input checked="" type="checkbox"/> \$1275 (up to 3 meetings) | <input type="checkbox"/> \$500 per additional meeting if necessary |

**APPLICANT:**

**Name:** Greg Forsyth, SPS Capital Projects Director

**Address:** 2815 E. Garland Ave., Spokane, WA 99207

**Phone (home):**

**Phone (work):** (509) 354-5771

**Email address:** GregoryF@spokaneschools.org

**PROPERTY OWNER:**

**Name:** Spokane Public Schools (SPS)

**Address:** 200 N Bernard Street, Spokane, WA 99201

**Phone (home):**

**Phone (work):** (509) 354-5771

**Email address:** GregoryF@spokaneschools.org

**AGENT:**

**Name:** Brooke Hanley, Architect, NAC Architecture

**Address:** 1203 W Riverside Ave, Spokane, WA 99201

**Phone (home):**

**Phone (work):** (509) 838-8240

**Email address:** bhanley@nacarchitecture.com

**REPRESENTATIVE SIGNATURE:**

**DATE:**

*Brooke Hanley*

8/21/2023

**DEPARTMENT USE ONLY:**

**Submittal Date:**

**Accepted as Complete:**

**Design Review Committee Meeting Date:**



Planning and Development  
www.spokanecity.org

## Pre-Development Conference Notes

**Project Name:** Adams Elementary School Replacement

**To:** Greg Forsyth  
Spokane Public Schools  
2815 E. Garland Ave.  
Spokane, WA 99207  
GregoryF@spokaneschools.org

**Phone:** 509-354-5775

**From:** Patty Kells

**Phone:** 509-625-6447

**Project Name:** Adams Elementary School Replacement  
**Permit No.:** B23M0077PDEV  
**Site Address:** 2909 E. 37th  
**Parcel No.:** 35342.0001, 2901, 3022  
**Meeting Date:** Thursday, June 15, 2023

Thank you for attending a Pre-Development meeting with the City of Spokane. Below are notes summarizing the information that was presented to you at your meeting on Thursday, June 15, 2023. These notes are broken down into three sections:

- Section 1: This section describes those proposed items specific to the building improvements with directives for code compliance addressed by the Building and Fire Departments as well as Spokane Regional Health District when warranted.
- Section 2: This section describes all issues outside of the building within the property boundaries including landscaping, parking requirements and accessibility, utilities, traffic, and refuse addressed by Planning, Engineering, Traffic, and Solid Waste Departments.
- Section 3: This section contains information for permit submittal, our intake process, and general information.

Please be advised that these notes are non-binding and do not constitute permit review or approval. The comments were generated based on current development standards and information provided by the applicant; therefore, they are subject to change. Comments on critical items will be highlighted in **bold** text.

### Project Information:

- A. Project Description: New elementary school
- B. Scope and Size: Project area is approximately 60,000 with an occupancy of E. Building will have three floors, no basement. Scope includes revisions to the site
- C. Special Considerations: Demolition permits, CUP with SEPA
- D. Estimated Schedule: Submit permit Oct. 2023 and construct June 2024
- E. Estimated Construction Cost: \$26,000,000.00



## **Section 1 – Comments Specific to the Building**

### **Katie Shaffer – Certified Plans Examiner (509-625-6993)**

1. Demolition of the existing structure(s) will require a demolition permit with a minimum 10-day waiting period. Coordination with the Spokane Regional Clean Air Agency is also required.
2. SEPA for the demolition and replacement of the building is required.
3. A BLA is required as buildings cannot cross property lines.
4. Projects accepted after October 29, 2023, will be governed by the 2021 editions of the building codes along with the 2017 edition of accessibility requirements (A117.1-2017).
5. A Washington State registered architect is required to stamp and sign permit drawings for commercial buildings larger than 4000 sf in area. When an Architect is required, we ask that the trade drawings (Mechanical, Electrical, and Plumbing) be prepared by licensed professionals as well.
6. Non-Residential Energy Code compliance is required. This requires a Special Plans Examiner and Special Inspector. Energy code review is conducted on the building envelope, mechanical and lighting systems.
7. Special Inspections will be required. Provide a signed Statement of Special Inspections form at time of submittal/prior to permit issuance.
8. A separate submittal and plan review from the Spokane Regional Health District (SRHD) is required for schools.
9. A site plan to include fire separation distances identified to proposed new exterior walls will be required at time of plan review submittal.
10. Plans to include a code summary with occupancy, occupant loads, construction type, life safety plan, etc.
11. Sprinklers for the new building will be required.
12. Provide a floor plan indicating all fire rated assemblies, with listed assembly details.
13. Provide details of firestopping systems (manufacturer and product, with details)
14. **\*\*Updated\*\*** WAC Amended IBC Section 429.1 Exception #2 exempts Group A, Group E, and Group M occupancies, except where employee parking spaces are designated from being required to provide EV charging. The provisions of Section 429 shall apply only to those designated employee parking spaces. Provide 10% of parking with EV charging stations. Note that raceways must be installed under the pavement to serve additional parking up to 20% of the total parking spaces, and the electrical room must have the capacity to serve the additional charging stations. One station located between the accessible and standard stalls would be adequate.
15. The building must be accessible for persons with disabilities. An accessible path is required from the public way, to parking, to the building entry, and to all primary function areas including restrooms.

### **Ali Brast – Associate Planner (509-625-6638):**

1. Development Standards:
  - a. Front yard setback: 15 feet from front property line – both 36<sup>th</sup> and 37<sup>th</sup> are designated as front property lines for this site.
  - b. Side yard setback: 5 feet
  - c. Overall setback: no structure shall be closer than 12ft to the face of curb.
  - d. Lot Coverage: 2,250 sq. ft. +35% for portion of lot over 5,000 sq. ft.
  - e. FAR: 0.5

2. Design Standards: Per *SMC 17C.110.500*

This project must address Institutional Design Standards. Please refer to *17C.120.500* for institution design standards, which address:

- a. Section 17C.110.515 Buildings Along the Street
- b. Section 17C.110.520 Lighting
- c. Section 17C.110.525 Landscaped Areas
- d. Section 17C.110.530 Street Trees
- e. Section 17C.110.535 Curb Cut Limitations
- f. Section 17C.110.540 Pedestrian Connections in Parking Lots
- g. Section 17C.110.545 Transition Between Institutional and Residential Development
- h. Section 17C.110.550 Treatment of Blank Walls
- i. Section 17C.110.555 Prominent Entrances
- j. Section 17C.110.560 Massing
- k. Section 17C.110.565 Roof Form
- l. Section 17C.110.570 Historic Context Considerations
- m. Section 17C.110.575 Screening

**Justin Cravalho – Fire Prevention Engineer (509-625-7057):**

1. The facility will be (is) of Type IIB construction.
2. Construction and demolition shall be conducted in accordance with IFC Chapter 33 and NFPA 241.
3. The building will be required to be provided with fire sprinklers. (IFC 903)
4. A fire alarm system with central monitoring is required throughout this building (including tenant spaces) in accordance with the Municipal Code including the provision for “Special Areas to be Protected”. (SMC 17F.080.110)
5. Smoke detectors are required above the panel, power supplies, annunciator, and other panels associated with the fire alarm system.
6. An emergency voice/alarm system is required for this building (IFC 907 amended with SMC 17F.080.110).
7. Where a commercial kitchen is provided with equipment that will produce grease vapors, a Class I kitchen hood is required and will be protected with a wet-chemical suppression system (IFC 609.2). In addition, a Class K fire extinguisher will be located no more than 30 feet from the area of grease cooking (IFC 906.1). The type of equipment that is considered to generate grease vapors is established by the International Mechanical Code.
8. Fire extinguishers are required for A, B, E, F, H, I, M, R-1, R-2, R-3, and S occupancies in accordance with IFC 906 – Table 906.3(1).
9. Construction separation meeting the requirements of IFC Chapter 33 and NFPA 241 are required if any portion of the building is occupied during construction. Plans will clearly show physical separation and pathways for occupant ingress and egress without going through construction areas without separation. Construction access pathways and material schedules /methods will be indicated. If the project is phased, a plan will be provided for each phase.
10. Address numbers or other approved signs are required to be provided on the building in a visible location (IFC 505).
11. If the building is equipped with a fire protection system, a Fire Department key box will be required (IFC 506).

**Sandra Phillips- Spokane Regional Health District (509-324-1004):**

1. See attached.

## Section 2 – Comments Specific to the Site

### Ali Brast - Associate Planner (509-625-6638):

1. **A Type II Conditional Use Permit for the new school will be required to be approved prior to any construction. A Community Meeting is required prior to submittal of the Type II application materials.**
2. **Design Review will be required as part of the CUP.**
3. **SEPA will be required for the project. SEPA can either be processed with the CUP, or if the School District chooses to be Lead Agency, the SEPA Determination would need to be submitted with the CUP materials.**
4. A Boundary Line Adjustment will be required to aggregate the two existing school parcels and the single parcel to the east after the street vacation is complete. Assuming there are some conditions of the vacation that cannot be satisfied until the construction is complete, the BLA will need to be submitted with the building permit application and recorded prior to CO.
5. Landscaping and Sidewalks:
  - a. Separated Sidewalks with planting strips adjacent to the street are required. Exceptions can be made for bus loading zones and parent drop off areas.
  - b. Sidewalks, including interior pathways, shall have the minimum dimension of five feet. This dimension shall be applied to the clear, unobstructed pathway between the planting zone for street trees per SMC 17C.200.050 and building facades or parking lot screening.
  - c. Irrigation is required as per 17C.200.100.
  - d. A six-foot wide planting area of L2 landscaping, including street trees as per 17C.200.050 are required along street frontages.
  - e. Building setbacks and all other portions of a site not covered by structures, hard surfaces, or other prescribed landscaping shall be planted in L3 open area landscaping until the maximum landscape requirement threshold is reached (see SMC 17C.200.080).
6. Pedestrian Connections in Parking Lots
  - a. Within parking lots containing more than thirty stalls, clearly defined pedestrian connections shall be provided:
    - i. between a public right-of-way and building entrances.
    - ii. between parking lots and building entrances, pedestrian connections can be counted toward the amount of required landscaping.
  - b. Pedestrian connections shall not be less than five feet wide.
  - c. Pedestrian connections shall be clearly defined by at least two of the following:
    - i. Six-inch vertical curb.
    - ii. Textured paving, including across vehicular lanes.
    - iii. A continuous landscape area at a minimum of three feet wide on at least one side of the walkway.
7. Parking:
  - a. Required parking is not permitted in the front 20ft of the property along 36<sup>th</sup> or 37<sup>th</sup> Ave. Through the CUP process, we are willing to consider the allowance of ADA parking near the entrance, so long as it is heavily screened from public view with landscaping inside the property line.
  - b. No more than 40% of the land area between the front lot line and the front building line may be paved or used for vehicle areas. On corner lots, not more than 20% of the land area between the side street lot line and the side street building line may be paved or used for vehicle areas.

- c. Please show parking calculations on your building plans when you submit for permit. Minimum and Maximum parking ratios are per *SMC 17C.230*.
    - i. Minimum Ratio for grade schools: one parking stall per classroom.
    - ii. Maximum Ratio for grade schools: 2.5 parking stalls per classroom
- 8. Refuse storage areas are not permitted within 15ft of a street lot line.
- 9. Any new fencing will require a separate permit.

**Patty Kells – Traffic Engineering Assistant (509-625-6447):**

1. AHBL has contacted our engineering group regarding vacating Fiske St and was given the application and checklist. If Fiske is vacated, the closure requirements will be removal of the existing curb returns to be replaced with City Standard curb and sidewalk to match existing. Adjustment of the existing street drainage will also need to be reviewed.
2. How will the access proposed on the plans off 37<sup>th</sup> Ave for the Fire Dept and deliveries logistically be constructed? The location of this access that close to the bus pull out could be a conflict.
3. All parking and maneuvering areas must be hard surfaced. All required parking, landscaping and onsite stormwater designs must be within the property lines. All unused driveway approaches must be removed and replaced with City standard curb and sidewalk.
4. Please provide a dimensioned site plan to include the property lines, buildings and setbacks, and all site improvements. Please dimension the parking stalls, accessible stalls and access aisles, travel lanes and driveway approaches on the site plan.
5. All parking onsite must be striped to current standards and comply with ADA requirements if five or more units are proposed. An accessible barrier free parking space and aisle will be required onsite and must comply with the updated City of Spokane Standard Plan G-54 & G-80A attached for signing and striping. An accessible route of travel connecting to the nearest accessible entrances and to the public sidewalk is required with a marked accessible route of travel. All barrier free spaces and aisles must be drawn and reference these standard plans and ***must be added as details on the plans***. Note on the site plan the van-accessible stall and the access aisle for van accessibility must be eight feet wide. Please note that it is now required to install a “No Parking Anytime” sign centered in the ADA aisle per Standard Plan G-80A. Please note both ADA sign locations on the site plan.
6. Any proposed on-site lighting must be confined to the site and cannot overspill into the public rights-of-way.
7. Please add all existing street signage on the site and landscape plans to verify any conflicts.
8. Adequate access and maneuvering for refuse/emergency vehicles is required per City Standards and must be maintained during construction.
9. Maintain clear view at intersections, alleys, and pedestrian ways.
10. *The City shall collect impact fees, based on the schedules in SMC 17D.075.180, or an independent fee calculation provided for in SMC 17D.075.050, from any applicant seeking development approval from the City.* A transportation impact fee will be assessed for the new elementary school and is assessed for the difference in the size of the old (34,332sf) and new (60,000sf) buildings (= 25,668sf) calculated at \$1.53/sf in the South Service Area. The estimated fee is \$39,272.04 + \$1,000 admin fee = **\$40,272.04**. This fee must be paid with the other permit fees prior to issuance of the building fee permit.

**Inga Note – Senior Traffic Planning Engineer - ICM (625-6331):**

Below are comments sent for Design Review Board:

1. That Fiske ROW is currently fenced off. I do think it's worth a discussion of providing a pedestrian pathway through that ROW as a condition of the vacation. Without that walkway we have a 1300' block face and pedestrians can't go from one side to the other without walking on the arterials. Fiske is designated as a Neighborhood Greenway route, and while we weren't expecting to use this ROW for a bike route there could be foot-traffic that wants to use it. Especially since STA has a route with stops on 37<sup>th</sup> between Regal and Ray.
2. It would be helpful if they could share the designated walk routes to and from the site and whether any improvements should be rolled into the project, for example curb extensions at 36<sup>th</sup>/Fiske to shorten the crossing distance and make kids more visible.
3. 36<sup>th</sup> Avenue is 35' wide. That's sufficient width to have the entire south side be a parent drop-off zone and they do not need the extra 8' deep pullout area. That will give them a little more space on-site for landscape buffer or grass play area.

**Joelie Eliason - Engineering Tech IV (509-625-6385):**

1. Addressing shall conform to [SMC 17D.050A](#). The address should reflect the main public entrance of the building. If the main entrance to the building is from 36<sup>th</sup>, the address should be changed/updated PRIOR to submitting for permits. If the address needs to be updated, please contact the Addressing Team at [addressing@spokanecity.org](mailto:addressing@spokanecity.org). Include a copy of the site plan to facilitate a quality response.
  - a. SMC 17D.050A.010: Proper addressing is important to facilitate the expedient emergency response by medical, law enforcement, fire, rescue, and any other emergency services.
  - b. SMC 17D.050A.020: The City of Spokane may reassign addresses as necessary to further the purpose of this chapter and that all non-conforming addresses may be changed to conform to the Code.
  - c. SMC 17D.050A.100: Commercial and Public Facility structures may be assigned an address based upon the roadway the main entrance faces and not necessarily the access roadway.
2. Regal Street is a Tier 2 street and 37<sup>th</sup> Avenue is a Tier 3 street under and subject to the [Spokane Regional Pavement Cut Policy](#).
3. Our records indicate the existing building is connected to the public sanitary main in Regal St. with a six-inch side sewer constructed in 1961. The pipe material was not listed. If you plan to reuse this service connection, we recommend the condition of the pipe be verified as part of your project. Any unused sewer services shall be capped with a water-tight seal at the property line.
4. New commercial side sewers shall be PVC at least six inches in diameter; shall have a minimum slope of two percent and 3.5 feet of cover where vehicular traffic passes over; two feet minimum in other areas. Sewer and Water service separation requirements are 18 inches minimum vertical, five feet minimum horizontal. Sewer cleanouts shall be installed at every 100 feet and every angle 45 degrees or greater. See the [City of Spokane Design Standards](#) Section 4 for additional information on Sewers.
5. General Facility Charges (GFCs) will apply. The sewer GFC is based on the size of the domestic water meter. Information regarding GFCs, including the most recent fee schedules, can be reviewed at [SMC 13.03.0730](#), [SMC 13.03.0732](#), and [SMC 13.03.0734](#). The sewer GFC rates at the time of this meeting are as follows but are subject to change:

#### Wastewater General Facility Charge Schedule

Meter Size	2023	2024
3/4"	\$3,984	\$7,461
1"	\$3,984	\$12,435
1.5"	\$11,266	\$24,870
2"	\$11,266	\$39,792
3"	\$20,697	\$87,046
4"	\$31,862	\$149,221
6"	\$58,540	\$335,747
8"	Based flow rates that utilizes sewer capacity	Will be calculated
10"		

Each year after year 1, will be annually adjusted based on a five year rolling average of the Engineering News Record Construction Costs Index pursuant to SMC 13.03.0732.

Numbers above are representative of phase in only. Actual numbers will be based on the ENRCCI indexed for inflation.

6. Please provide a water and sewer study that shows average and peaking daily demands and required fire flow for the project. Specify where the sewer and water connections to the existing system are expected. This information is required to maximize development approval while tracking total existing system demands and future development planned system demands.
7. All stormwater and surface drainage generated on-site must be disposed of on-site in accordance with [SMC 17D.060.140](#) "Stormwater Facilities" as per the Project Engineer's recommendations. Locate stormwater requirements in the [Spokane Regional Stormwater Manual \(SRSW\)](#) and [City of Spokane Design Standards](#) Section 6. Generally, new developments, additions, plats and binding site plans, addition, or replacement of any impervious surface, manufactured or mobile home parks, will require a geotechnical site characterization (report) and drainage report/plan. **Please include a detailed Civil Plans which show and clearly delineate existing and proposed sewer, water, drainage structures, dry well types, swale bottom areas, and property lines.** Show proposed and existing pavement. The geotechnical report, drainage report, and civil plan must be stamped and signed by an engineer licensed in the State of Washington.
  - a. The project site is located within the Moran Prairie Special Drainage District. A down-gradient analysis demonstrating that there will be no expected adverse impacts on down-gradient properties will be required. Additional information on Special Drainage Districts can be found in [SMC 17D.060](#) "Stormwater Facilities" and the [Spokane Regional Stormwater Manual \(SRSW\)](#).
  - b. The project site is located within a high Critical Aquifer Recharge Area and is considered to have high susceptibility for groundwater contamination.
  - c. Combining landscape and stormwater treatment areas per Eastern Washington Low Impact Development (LID) Guidance Manual is allowed. The link to DOE LID resources can be found at:  
<https://ecology.wa.gov/Regulations-Permits/Guidance-technical-assistance/Stormwater-permittee-guidance-resources/Low-Impact-Development-guidance>
  - d. Any drywells and subsurface drainage galleries (existing and proposed) for the site must be shown on the plans and registered with the Washington State Department of Ecology (DOE). Please send a copy of the completed registration form to the City of Spokane Development Services Center. See the following link at the Department of Ecology (DOE) website for information about the Underground Injection Control (UIC):

<https://ecology.wa.gov/Regulations-Permits/Guidance-technical-assistance/Underground-injection-control-program>, [Note all](#)

new projects must submit a UIC registration to Ecology at least 60 days prior to commencing UIC well construction.

Ecology's approval of the registration is required prior to construction of a new UIC well.

8. Most land-disturbing activities require an Erosion and Sediment Control (ESC) plan. Land-disturbing activities are activities that result in a change in existing soil cover (vegetative or non-vegetative) or site topography. Land-disturbing activities include, but are not limited to, demolition, construction, clearing and grubbing, grading, and logging. An ESC plan detailing how erosion and other adverse stormwater impacts from construction activities will be handled must be submitted to the Development Services Center for review and acceptance prior to construction of said phase. See Section 9 of the SRSM for ESC requirements and applicability. The following link provides information on ESC training and certification programs:  
<https://ecology.wa.gov/Regulations-Permits/Permits-certifications/Certified-erosion-sediment-control>.
9. **A construction stormwater general permit may need to be obtained from Ecology. See attached handout for additional information.**
10. Include a note stating that the contractor is responsible for designating a location where concrete trucks and equipment can be washed out. This area shall not be located near or draining into a storm drainage area, treatment area, or facility.
11. All sidewalks, curbs, and driveway approaches adjacent to the property will be reviewed at the end of the project when a Certificate of Occupancy is requested. If any are found to be broken, heaved, sunken, or missing, they must be repaired/replaced whether the damage was existing or caused by construction. If you would like a sidewalk inspection prior to requesting occupancy, please contact the City of Spokane (509) 625-6300 to arrange a site visit.

**Justin Cravalho – Fire Prevention Engineer (509-625-7057):**

1. An approximate site fire flow (obtained from IFC Table B105.1 and Table C105.1 using the total fire area and construction type) is 5,250 GPM without automatic sprinklers throughout and requires six fire hydrants. Site fire flow is 1,500 GPM with automatic sprinklers throughout and requires one fire hydrant.
2. There are five existing fire hydrants in the area that meet some or all the code requirements for this project.
3. Site fire flow will be required to be maintained or provided prior to delivery of building construction materials to the site (IFC 3312.1).
4. Fire hydrant spacing shall not be more than 500 feet (along an acceptable path of travel), within 500 feet of the property line for non-sprinklered buildings and 750 feet of the property line for fire sprinklered buildings (SMC 17F.080.030).
5. For commercial buildings, fire hydrants are required to be along an acceptable path of travel within 400 feet to all points around the building without fire sprinklers (IFC 507.5.1), and 600 feet for commercial buildings with fire sprinklers (IFC 507.5.1, exception 2).
6. Fire Department Connections for new fire sprinkler system installations shall be located no more than five hundred feet from a fire hydrant along an accessible path of travel unless where approved by the fire official.
7. Fire Department approved all-weather access must be provided to within 200 feet of any point around the outside of a building (IFC 503.1.1). For fully sprinklered buildings, this is extended to 240 feet (IFC 503.1.1, exception 1). Dead-end roads longer than 150 feet need approved fire apparatus turnarounds (IFC 503.2.5). Fire apparatus turning radius is 50 feet external, 28 feet internal (SMC 17F.080.030.D.3). Minimum height clearance is 13 feet-6 inches (IFC 503.2.1). Fire lanes will have a maximum slope of 10 percent (based on IFC 503.2.7). Minimum width for fire access is 20 feet, unobstructed (IFC 503.2.1). Fire aerial access lanes are limited to a maximum slope of 5%.

8. Streets with a minimum clear width less than 26 feet are required to be provided with “No Parking” signs on both sides of the street. Streets with a width more than 26’ to less than 32’ shall be provided with “No Parking” signs on one side of the street. Signs shall be spaced 50 feet apart.
9. Buildings exceeding 30 feet in height and will be required to have a Fire Aerial Access Lane of 26 feet wide along at least one full side of each building (IFC D105.2). The fire aerial lane is required to be a minimum of 15’ and a maximum of 30’ from the building along the full length of the side of the building.
10. Fire lanes will be maintained with an all-weather surface (IFC 3310.1) and provided prior to the delivery of building construction materials to the site.
11. The installation of security gates or barriers on fire access roads shall be approved by the Fire Department (IFC 503.6). If access to the site is required to comply with the distances around the building, at least one access gate will be setback a minimum of 48’ from the edge of pavement. Gate openings will be a minimum of 14’ wide, and open gates will not obstruct access to structures.
12. The proposal appears to meet the requirements of the Fire Code for fire access, but the turn-around on the blacktop area may need to move south to meet the length for the north leg of the hammerhead.

**Mathias Bauman – Water Department (509-625-7953):**

1. Our records show an existing 4-inch domestic water service running to 2909 E 37<sup>th</sup> Ave. There is also a ¾-inch copper domestic water service running to 3104 E 36<sup>th</sup> Ave. Your engineer may determine that the existing services may need to be replaced or upsized to meet the needs of the project. If any existing services are not utilized, they must be disconnected at the main.
2. Currently, you are serviced in the High-Pressure Zone (48 psi). Connecting to the Top Pressure Zone (83 psi) would be permitted if the current water services are disconnected at the main. We would not allow the connection of two separate pressure zones on a single parcel.
3. If additional water is needed, a 6-inch cast iron water distribution main in 36<sup>th</sup> Ave (High Pressure Zone) and Regal St (Top Pressure Zone) are available for the project. A 12-inch ductile iron main in 37<sup>th</sup> Ave (Top Pressure Zone) is also available.
4. A hydraulic model must be performed to prove that the design meets minimum standards and to show how this project affects our water system.
5. The City of Spokane Water Dept. does not allow water services to cross over property lines; therefore, the parcels must be aggregated.
6. The City of Spokane Water Department Cross Connection Control and Backflow program rules and regulations shall be followed in accordance with Washington Administrative Code (WAC 246-290-490) and the City of Spokane Municipal Code 13.04.0814.
7. General Facilities Charges will apply if new domestic or irrigation water taps are made. See Section 13.04.2042 in the Spokane Municipal Code.
8. Calculated static water pressure is approximately 48 psi in the High-Pressure Zone and 83 psi in the Top Pressure Zone. Pressures exceeding 80 psi require a pressure reducing valve to be installed.
9. A utility site plan illustrating new water lines and/or services to be installed shall detail the location of new tap(s) and meter(s) prepared by a Professional Engineer licensed in the State of Washington. Water Department plan reviewers and inspectors will ensure that any new water line(s) and Service line(s) needing backflow assemblies are installed in accordance with applicable rules and regulations. Water Department Water Service Inspectors, North side (509) 625-7845, South side (625-7844) will review submitted plans and inspect on-site construction. Water Department Cross Connection Control Specialists at (509) 625-7969, will review any backflow assemblies where required.



10. Taps and meters can be purchased at Developer Services Center, located on third floor of City Hall -Spokane. Size of service(s) shall comply with International Plumbing Code. Tap, meter, and connection fees will comply with section 13.04 of SMC. Tapping of the water main and installation of new meters shall be done by City forces. All excavation and restoration are the owner's responsibility. All trenches and/or excavations must comply with current W.A.C. #296-155-part N. No City of Spokane employee will be permitted into any trench and/or excavation without proper shoring or sloping, no exceptions. Please see Water Department Rules and Regulations for information about tap and meter sizes and sewer/water separation requirements.

**Kerry Deatrich – Solid Waste (509-625-7871):**

1. Prior to constructing a refuse location, the department will need to approve the layout on site.
2. Roll-off containers will require guide rails and back stops. As well as a minimum of 50' of unobstructed vehicle access (with room to maneuver) from the front of the container.
3. Front load containers:
  - a. An enclosure for refuse or recycling only, must be 12 feet wide by 10 feet deep with an interior clear width opening of 12 feet.
  - b. There are two options for enclosures with more than 3-95-gallon recycling carts.
    - Option 1. An enclosure which is 20 feet wide by 10 feet deep with a clear width opening of 20 feet.
    - Option 2. An enclosure which is 25 feet wide by 10 feet deep with **TWO** interior clear width openings of 12 feet.
4. Containers must be placed on a firm surface of concrete or asphalt at least 4 inches thick. Use of asphalt is discouraged.
5. Each gate leaf must include a mechanical stop to hold the leaf in the open position.
6. Each gate leaf (when open) cannot block lanes of travel, Fire Lanes or ADA parking stalls or ADA aisles. Gates may need to open greater than 90 degrees.
7. Nothing may be stored in the enclosure (e.g., pallets, mattresses, grease containers).
8. 50 feet of unobstructed access from the front of the enclosure & the width of the enclosure is required. Picture a rectangle 50 feet from the front, the width of the enclosure.
9. Overhead obstruction must be higher than 24 feet.

**Becky Phillips – Urban Forestry (509-363-5495):**

1. Comments will be sent later.

**Tara Limon – Spokane Transit (509-343-1692):**

1. Adams Elementary: Spokane Transit serves this site with a bus stop on 37<sup>th</sup> at Regal in the westbound direction. Please add this bus stop to your site plan as noted in the attachment and with this construction project please add an 8 ft by 8 ft ADA boarding and alighting pad and a shelter pad if site conditions are suitable. Please coordinate the improvements to the bus stop and any changes to the sidewalk, curb, or intersection.

### **Section 3 – General Information and Submittal Requirements**

1. Plan requirements are as shown on the attached "Commercial Application Submittal Requirements". For the permit intake submittal, please provide an electronic copy of the **All**

**plan sets along with reports and supporting documents.** Plan sets shall include all plans created for this project: cover sheet, architectural, structural, plumbing, mechanical, electrical, civil engineered plans, landscaping, and irrigation drawings. Plans are required to be stamped and sealed by an architect, landscape architect, or engineer licensed to do business within the State of Washington. All reports and supporting documentation noted in departmental comments will also be required for the permit intake submittal (i.e., NREC, drainage report, geotechnical site characterization, critical materials list, etc.). Please note that plans may be provided in multiple logically separated files to help manage files sizes as excessively large (i.e., separated by discipline, by building vs site, etc.).

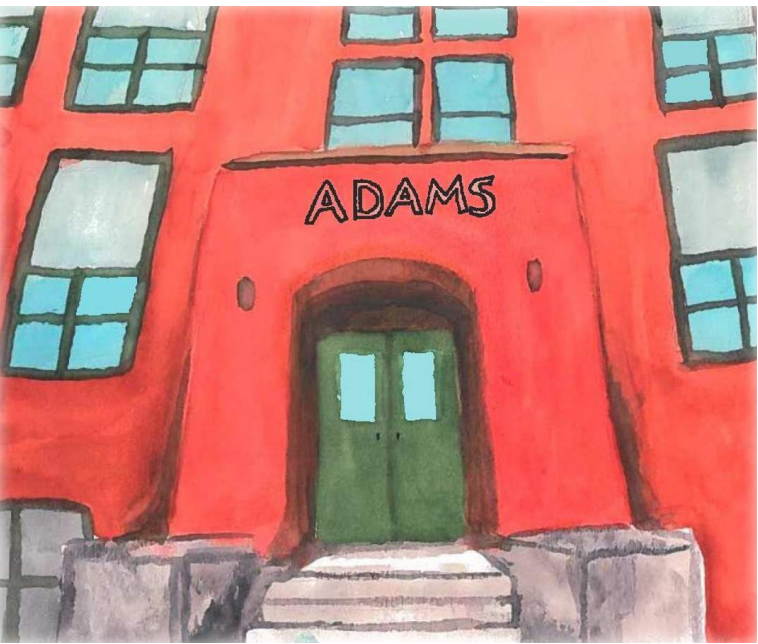
2. Please provide an electronic copy of site plans showing dimensions, ***property lines, and City Limits***, relative topography, all on-street signs and street markings, any new and existing frontage improvements, all structures, on-street storm drainage facilities, sidewalks, curbs, parking calculations and dimensions, dimension existing roadway, new and existing driveways and their locations, and other relative information. Show all existing topography in the public right-of-way such as street signs, water valves, hydrants, etc. All required landscaping must be within the property lines and not in the public right-of-way.
3. An Intake Meeting handout was provided to you in your packet at the Pre-Development meeting. Please call (509) 625-6300 to schedule an Intake Meeting to submit plans for a new commercial/industrial building, an addition to an existing building, a change-of-use, or a parking lot. Appointments must be made at least 24 hours in advance and can be scheduled for Monday through Thursday.
4. Please provide a complete set of plans to Spokane Regional Health District if food and/or beverage handling business is planned.
5. If you would like a full Certificate of Occupancy on any portion of the permit prior to completion of the other phases, it is required to file separate permits for each phase. An additional \$250 fee will be assessed for a Temporary Certificate of Occupancy and/or a Temporary Certificate of Occupancy extension per SMC 8.02.031M.
6. For additional forms and information, see [my.spokanecity.org](http://my.spokanecity.org).







PROJECT SUMMARY



SUMMARY OF DESIGN CHANGES SINCE COLLABORATIVE WORKSHOP:

Since the Design Review Board Collaborative Workshop, the site design has been developed to consistently create a layer of landscaping to filter views between the building and the street. Paths through the site have a flow or “meander” to capture the feeling of walking on a path through clusters of trees in a grove. Additionally, the site structures such as the playground entry and reader board have been designed with materials, details, and ornament matching those used on the elementary school building.

The exterior materials and ornament of the building have been developed to create an orderly “frame and infill” rhythm of solid wall and window openings on the brick portions of the building. This “frame and infill” is emphasized through different brick textures, details, and ornament so it is clearly visible from the street. The overhanging portion of the building on the North elevation and the upper level portion of the building on the West and South elevation has a warm neutral color using a metallic panel to complement the brick color.

To create a welcoming presence for the elementary students attending the school, the building incorporates some accent colors and textures on the building exterior. These colors and textures appear together on the bay windows into classrooms. The colors appear alone in glazed brick bands and the textures appear alone in windows across the project. These “elementary” accent colors and textures appear in modest amounts so that they’re mainly experienced by students, parents, teachers, and visitors arriving at the school.

Additional changes are noted in the responses to the advisory actions below.

DRB COLLABORATIVE WORKSHOP ADVISORY ACTIONS & APPLICANT RESPONSES FOR RECOMMENDATION MEETING:

1. The Applicant should continue investigating the opportunity to provide non-motorized access through the site along the vacated Fiske right-of-way (ROW).

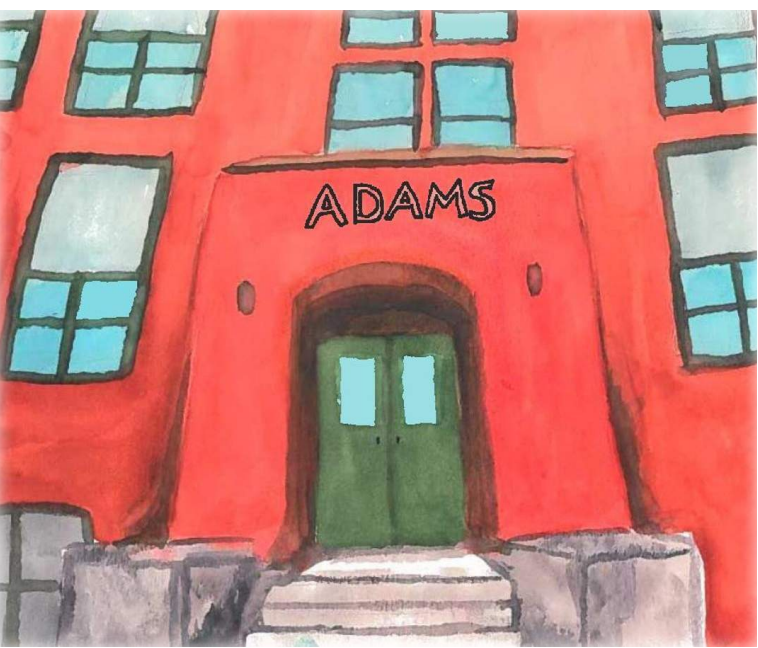
In an effort to accommodate a future non-motorized pathway along the vacated Fiske ROW, the perimeter fence on the east side of the site has been re-located in the updated site plan design to allow space for this access.

2. The Applicant is strongly encouraged to retain existing mature trees where feasible.

The project will make every effort to retain existing mature trees where feasible. The Adams Elementary site is one of the smallest school sites in the district and the design is attempting to find the right balance between the need for student playground area, resolving the parking and drop-off/pick-up concerns, and maintaining existing mature trees. A handful of pine trees are being removed from the Fiske ROW to build the driveway to the main parking lot, more than three-quarters of the existing trees in that area will be protected. The deciduous trees along the west half of the site near 37th will be impacted by construction activities for the new building location. These trees are currently too far from the sidewalk to provide much pedestrian shade, especially because this side is exposed to the southern sun. The current plan will retain the three mature deciduous trees along 36th near the Regal intersection.

3. The Board supports the Applicant’s intent to utilize clusters of trees in lieu of City standard street tree configuration. The Applicant is strongly encouraged to find opportunities to increase the quantity of trees above those otherwise required by code.

The current planting plan is provided in the recommendation meeting application. Thank you for supporting the intent to utilize clusters of trees. The new planting plan shows that the school will have many more trees than are required by code and will enhance the pedestrian experience along all street frontages. Screening parking and shading hardscape areas will also be provided by the new tree layout. The new student courtyard and playground will be enhanced with multiple types of plants and trees around the perimeters, leaving the center areas open for gathering, outdoor learning, PE, and recess activities.



(continued) **DRB COLLABORATIVE WORKSHOP ADVISORY ACTIONS & APPLICANT RESPONSES FOR RECOMMENDATION MEETING:**

4. The Applicant shall return with street level perspectives of the project that capture pedestrian and vehicular views from the public realm.

Street level perspective renderings that capture the pedestrian and vehicular views from the public realm have been provided within this recommendation meeting application submittal. The design of the building and the site have been developed with further detail to create a welcoming and friendly streetscape presence along all three frontages while also maintaining a safe environment for the building’s staff and students throughout the school day. The current design will greatly improve the experience of the entering the site and approaching the front door over the existing condition that is entirely surrounded by tall fences and retaining walls. Along 36th, the parent drop-off and pick-up lane and sidewalk has been enhanced with landscaping and street tree clusters, as well as a special entry gate that celebrates the arrival of the students to the playground each morning. This will also be the main entrance to the playground for the public when using the site like a neighborhood park if school is not in session.

5. The Board appreciates the Applicant’s intent to soften the grades around the perimeter of the site and provide adequate pedestrian queuing space at Regal St. & 37th Ave.

Thank you. Yes, the intention is to eliminate as many of the retaining walls as possible and increase the number of plantings and greenery that will enhance the pedestrian experience along the streetscape. New concrete sidewalk has been added to the corner of Regal & 37th to provide expanded pedestrian queuing.

6. The Applicant is strongly encouraged to continue investigating options to soften the vehicle pull-out area on 36th Ave. through landscaping, hardscape material change, or other designs.

Please review the current site plan and planting plan. The vehicle pull-out sidewalk width is generous enough for pedestrians to pass as cars are unloading occupants. The playground fence has been moved south to provide for a landscaping strip between the sidewalk and the fence. Trees have been added where appropriate, especially to highlight the playground entry gate experience. Brick pillars with integral lighting are proposed at the entry gate landing area, as well as seat wall benches for parents to wait for their children under the tree canopy. An ADA compliant access ramp has also been provided for safe travel from the sidewalk to the playground level.



SITE DESIGN:



CONCEPT SITE PLAN

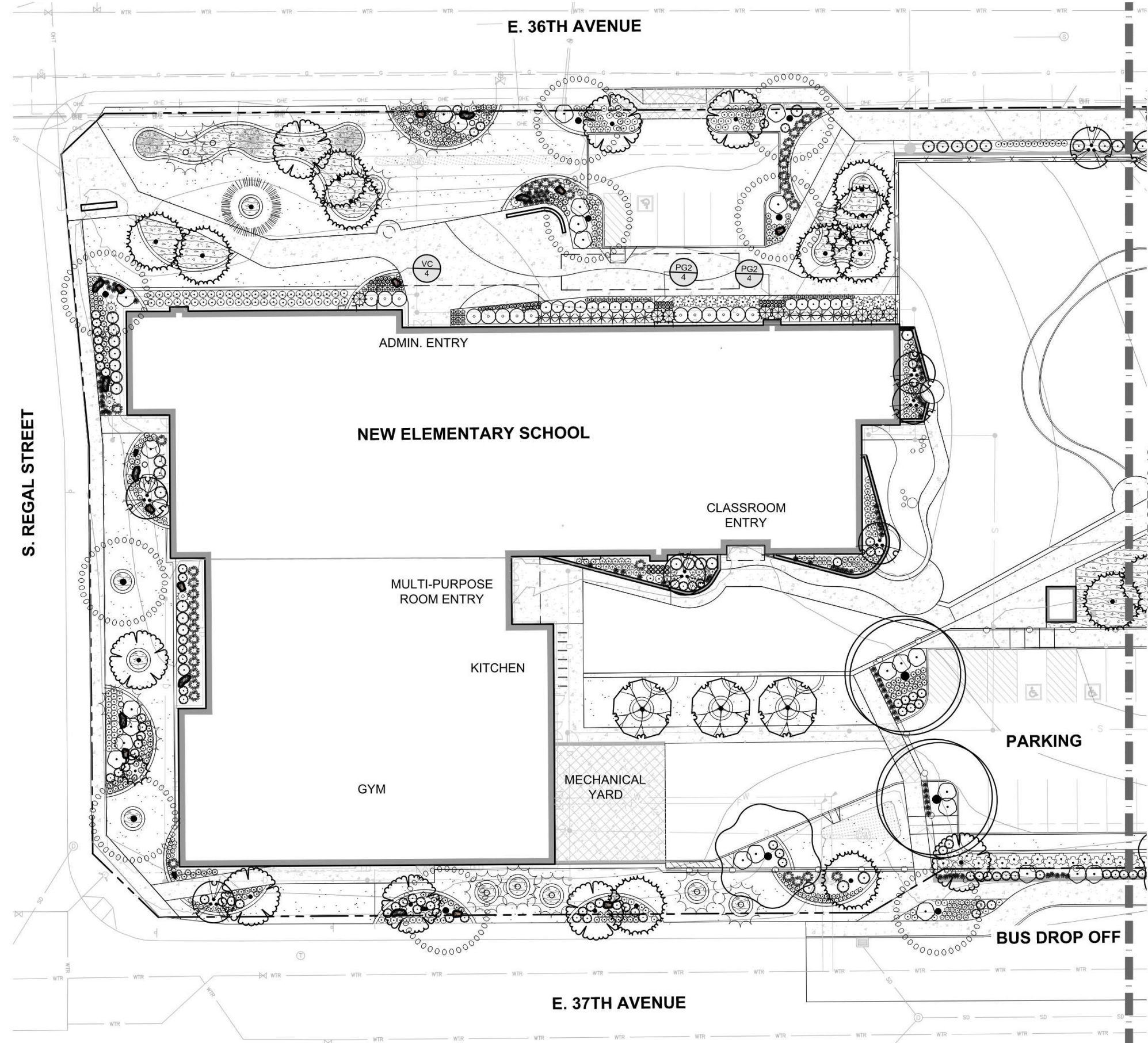
DESIGN REVIEW BOARD SUBMITTAL  
RECOMMENDATION MEETING  
SEPTEMBER 13, 2023

ADAMS ELEMENTARY  
SPOKANE PUBLIC SCHOOLS





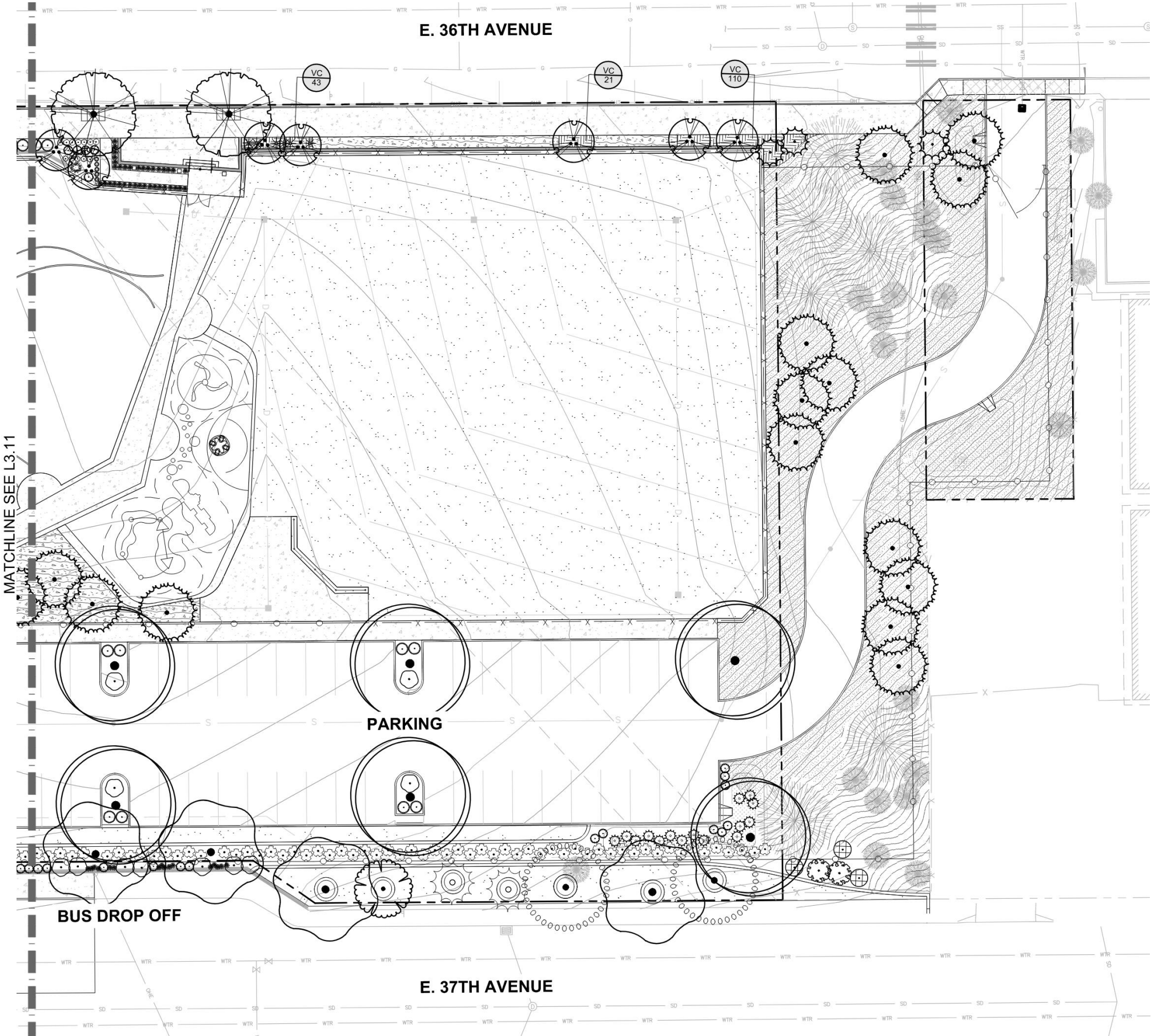
SITE DESIGN: PLANTING PLANS

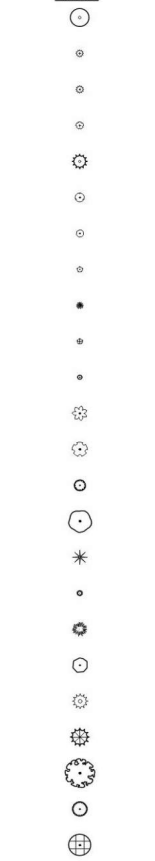
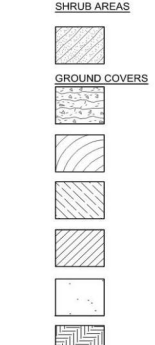
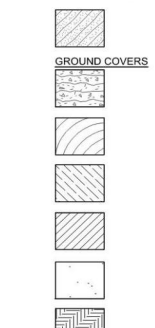


PLANT SCHEDULE				
TREES	QTY	BOTANICAL NAME	COMMON NAME	PLANTING SIZE
	8	ACER X FREEMANII 'JEFFERSRED'	AUTUMN BLAZE® FREEMAN MAPLE	2" CAL.
	14	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE'	AUTUMN BRILLIANCE APPLE SERVICEBERRY	7-8' HT. MULTI TRUNK
	5	CERCIS CANADENSIS	EASTERN REDBUD	2" CAL.
	11	GINKGO BILOBA 'PRINCETON SENTRY'	PRINCETON SENTRY MAIDENHAIR TREE	2" CAL.
	2	NYSSA SYLVATICA	TUPELO	2" CAL.
	1	PICEA PUNGENS 'GLAUCO'	BLUE COLORADO SPRUCE	8' HT. MIN.
	16	PINUS FLEXILIS 'VANDERWOLF'S PYRAMID'	VANDERWOLF'S PYRAMID LIMBER PINE	8' HT. MIN.
	5	PINUS LEUCODERMIS	BOSNIAN PINE	8' HT. MIN.
	25	PINUS PONDEROSA	PONDEROSA PINE	8' HT. MIN.
SHRUBS	9	PRUNUS X 'SNOW GOOSE'	SNOW GOOSE CHERRY	2" CAL.
	3	PRUNUS X YEDOENSIS 'CASCADE SNOW'	CASCADE SNOW CHERRY	2" CAL.
	QTY	BOTANICAL NAME	COMMON NAME	PLANTING SIZE
	14	AZALEA X 'HINO-CRIMSON'	HINO-CRIMSON AZALEA	3 GAL.
	55	BERBERIS THUNBERGII 'CRIMSON PYGMY'	CRIMSON PYGMY JAPANESE BARBERRY	1 GAL.
	141	BERBERIS THUNBERGII 'GORUZAM'	GOLDEN RUBY® JAPANESE BARBERRY	2 GAL.
	108	CALLUNA VULGARIS 'FIREFLY'	FIREFLY HEATHER	1 GAL.
	15	CORNUS SERICEA 'ISANTI'	ISANTI RED TWIG DOGWOOD	2 GAL.
	49	CORNUS SERICEA 'KELSEY'	KELSEY'S DWARF RED TWIG DOGWOOD	1 GAL.
	106	ERICA CARNEA 'SPRINGWOOD PINK'	SPRINGWOOD PINK WINTER HEATH	1 GAL.
169	ERICA X DARLEYENSIS 'MARY HELEN'	MARY HELEN DARLEY HEATH	1 GAL.	
136	GERANIUM X 'ROZANNE'	ROZANNE CRANESBILL	1 GAL.	
61	HAKONECHLOA MACRA 'ALL GOLD'	ALL GOLD JAPANESE FOREST GRASS	1 GAL.	
59	HEUCHERA X 'TNHEUSSOD'	SIRENS' SONG™ ORANGE DELIGHT HEUCHERA	1 GAL.	
12	HOSTA X 'FRANCEE'	FRANCEE HOSTA	2 GAL.	
77	ILEX GLABRA 'CHAMZIN'	NORDIC™ INKBERRY HOLLY	3 GAL.	
107	JUNIPERUS HORIZONTALIS 'LIMEGLOW'	LIMEGLOW CREEPING JUNIPER	2 GAL.	
38	JUNIPERUS SABINA 'MONARD'	MOOR-DENSE® JUNIPER	2 GAL.	
24	MATTEUCCIA STRUTHIOPTERIS	OSTRICH FERN	1 GAL.	
15	MISCANTHUS SINENSIS 'LITTLE KITTEN'	LITTLE KITTEN EULALIA GRASS	1 GAL.	
37	MISCANTHUS SINENSIS 'MORNING LIGHT'	MORNING LIGHT EULALIA GRASS	2 GAL.	
16	PIERIS JAPONICA 'PURITY'	PURITY JAPANESE PIERIS	3 GAL.	
4	PINUS MUGO 'MOPS'	MOPS MUGO PINE	3 GAL.	
8	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	1 GAL.	
2	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	1 GAL.	
41	SPIRAEA X BUMALDA 'GOLDFLAME'	GOLDFLAME SPIREA	3 GAL.	
3	VACCINIUM OVALIFOLIUM	OVAL-LEAF BLUEBERRY	1 GAL.	
SHRUB AREAS				
QTY	BOTANICAL NAME	COMMON NAME	CONTAINER	SPACING
9,866 SF	MEADOW GRASS MIX			
GROUND COVERS				
QTY	BOTANICAL NAME	COMMON NAME	PLANTING SIZE	SPACING
2,310 SF	PINE STRAW MULCH AREAS			
5,773 SF	NATIVE TO REMAIN			
10	ARCTOSTAPHYLOS UVA-URSI	KINNIKINNICK	4" POT	36" o.c.
8	PACHYSANDRA TERMINALIS 'GREEN CARPET'	GREEN CARPET JAPANESE PACHYSANDRA	4" POT	24" o.c.
43,375 SF	TURF HYDROSEED	DROUGHT TOLERANT FESCUE BLEND	HYDROSEED	
178	VINCA MINOR	COMMON PERIWINKLE	4" POT	18" o.c.



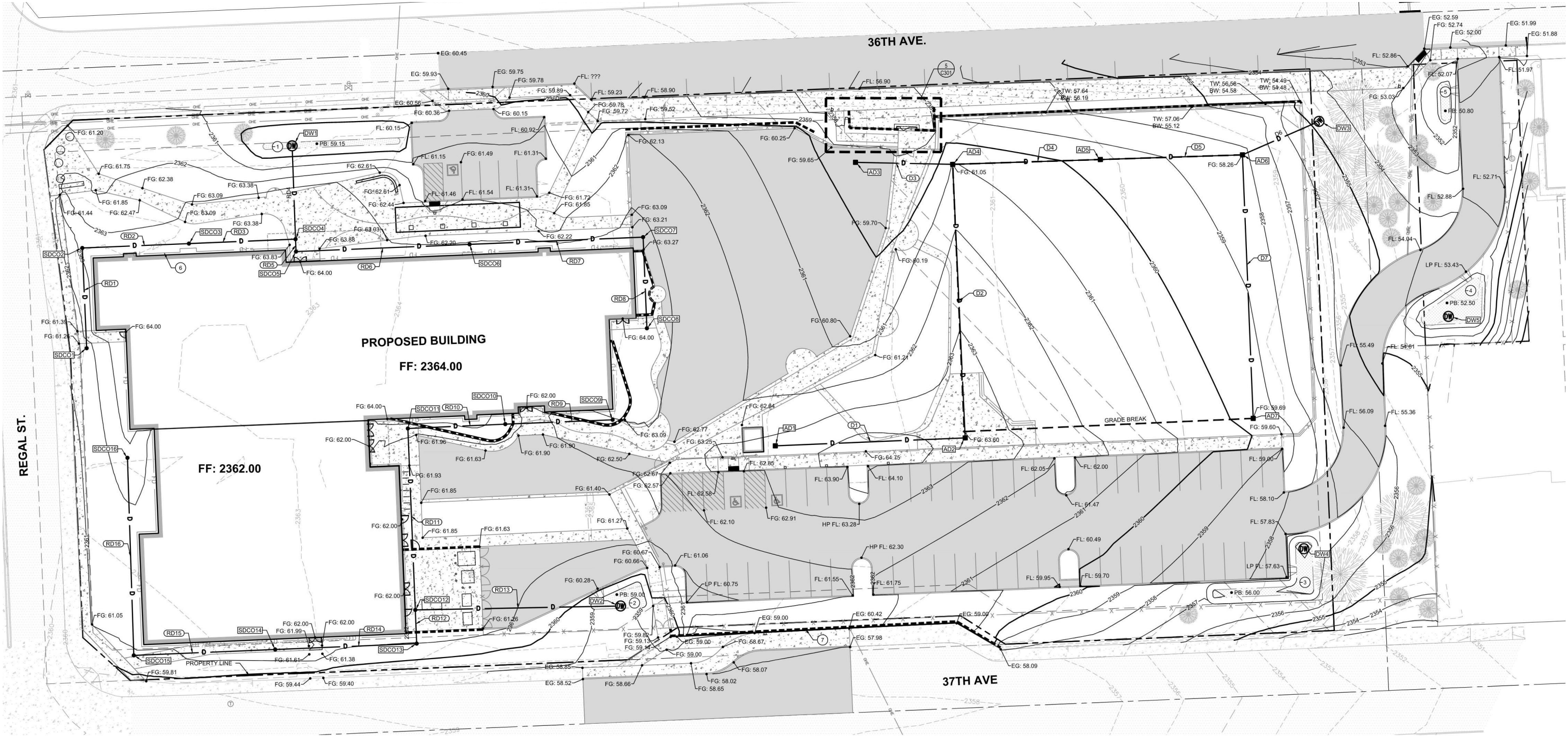
SITE DESIGN: PLANTING PLANS



PLANT SCHEDULE						
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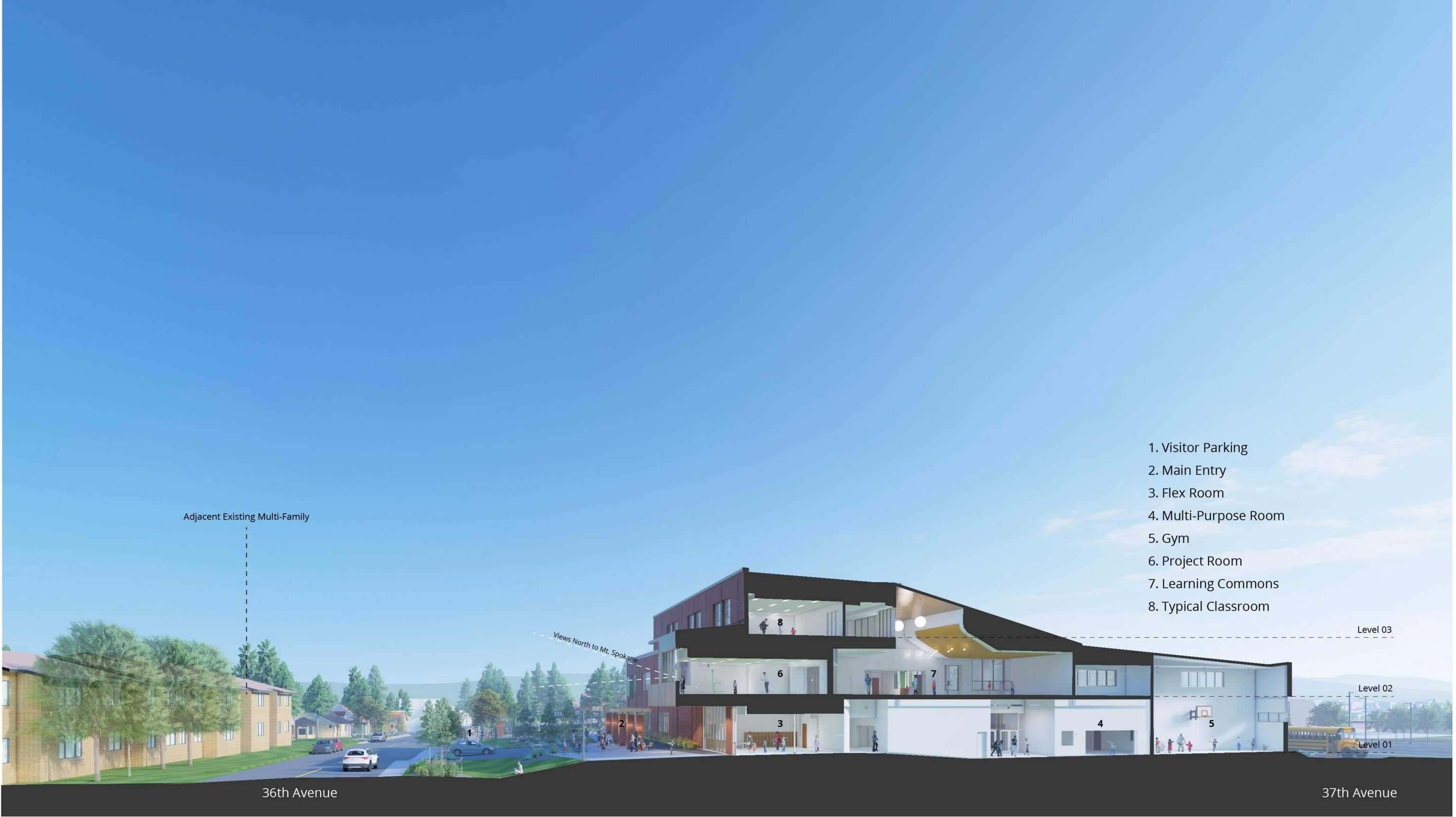


SITE DESIGN: CONCEPTUAL GRADING PLAN





SITE DESIGN: SITE SECTION



SITE CROSS SECTION RENDERING

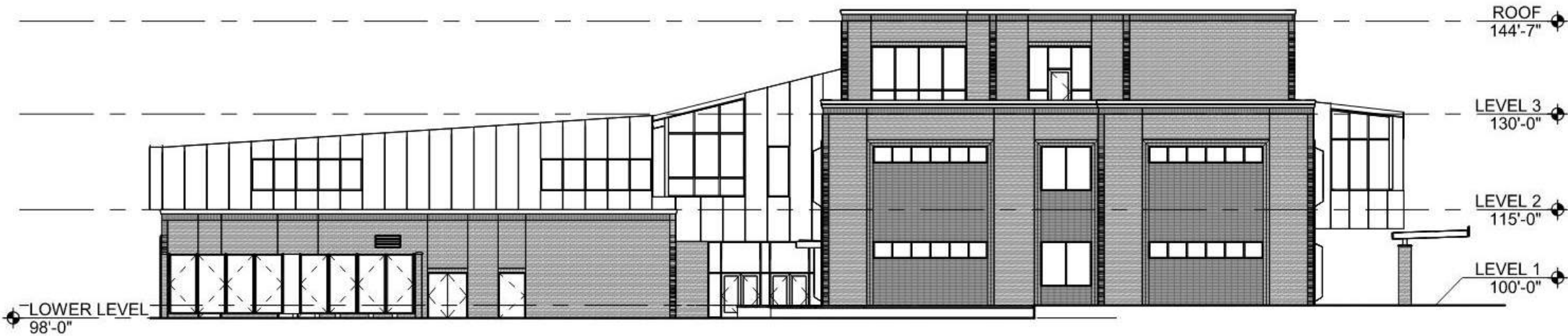


NORTH ELEVATION

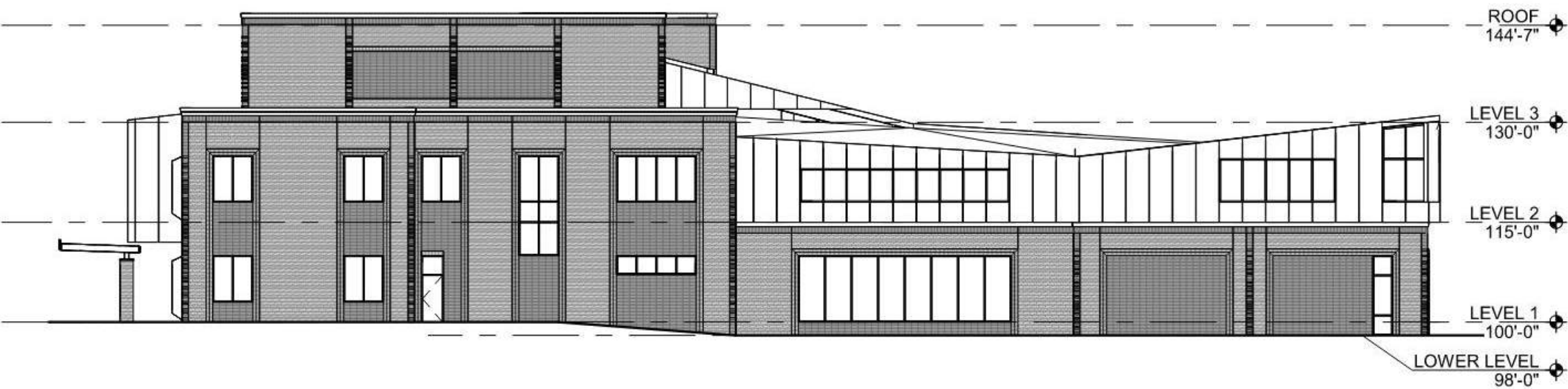


SOUTH ELEVATION





EAST ELEVATION



WEST ELEVATION

BUILDING DESIGN: FLOOR PLANS

Design Development level floor plans shown here for reference. The general layout has remained the same as shown during the collaborative workshop but more detail has been added.



THIRD FLOOR



SECOND FLOOR



FIRST FLOOR



Concept Lighting Plan







VIEW FROM NORTHWEST TOWARD CORNER OF 36TH & REGAL AND NORTHWEST CORNER OF BUILDING





VIEW FROM NORTHWEST CORNER OF SITE TOWARD MAIN PUBLIC ENTRY ALONG NORTH SIDE OF BUILDING





VIEW FROM 36TH TOWARD MAIN ENTRY CANOPY AND VISITOR PARKING ALONG NORTH SIDE OF BUILDING





VIEW FROM EAST END OF SITE ALONG 36TH LOOKING TOWARD PLAYGROUND ENTRY NORTHEAST SIDE OF BUILDING





BIRD’S EYE VIEW FROM 36TH & FISKE LOOKING OVER PLAYGROUND, PARKING, & NEW BUILDING





VIEW FROM 37TH TOWARD BUS DROP, STUDENT COURTYARD, EAST CLASSROOM WING, & GYM





VIEW FROM STUDENT COURTYARD TOWARD STUDENT ENTRIES, LEARNING COMMONS, & SOUTH FACING CLASSROOMS





VIEW FROM INTERSECTION OF REGAL & 37TH TOWARD GYM & MULTI-PURPOSE SPACES



ADAMS ELEMENTARY SCHOOL

REVISIONS

DESIGN DOCUMENT

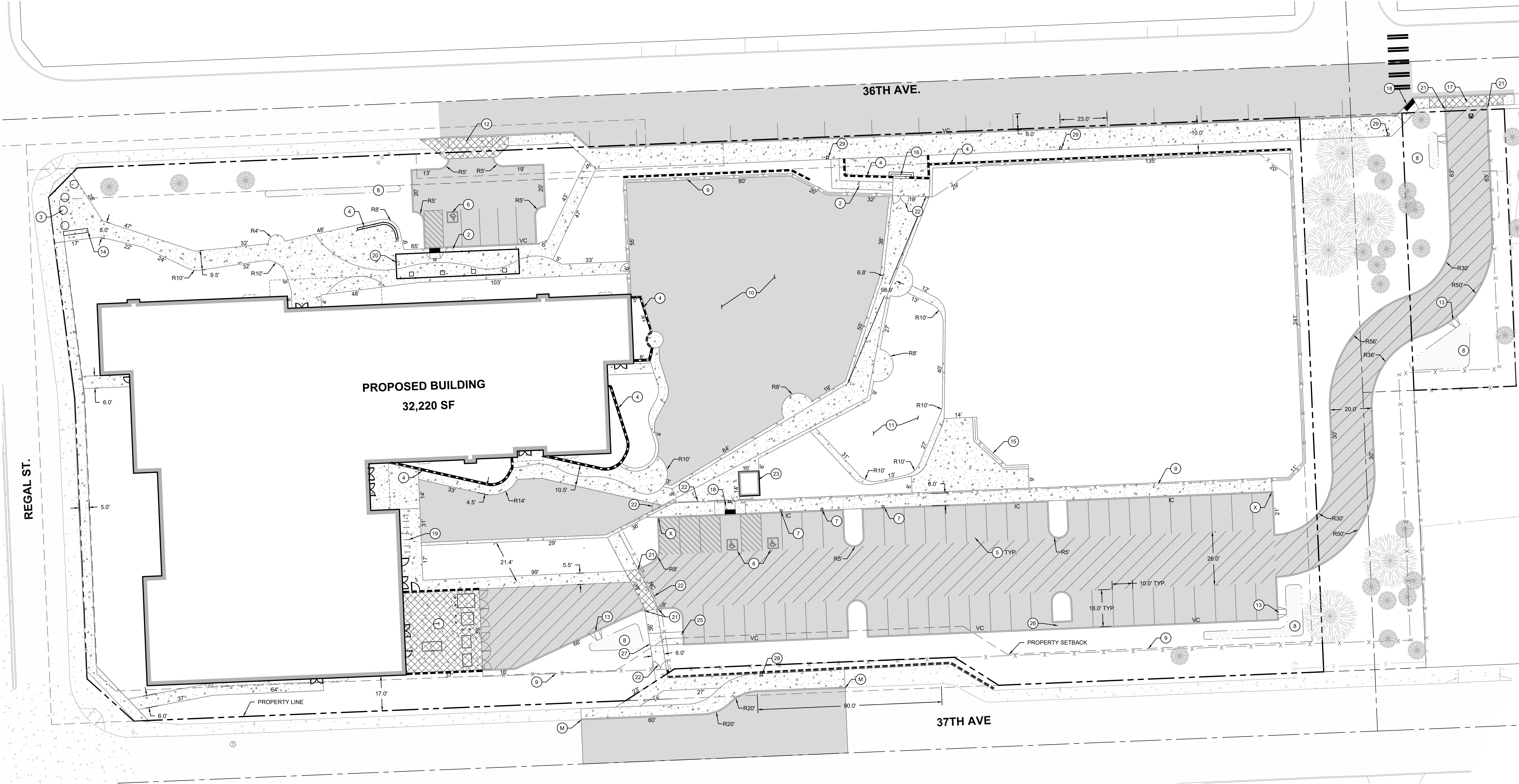
SPOKANE PUBLIC SCHOOLS  
ADAMS ELEMENTARY SCHOOL  
2909 EAST 36TH AVE, SPOKANE, WA 99207

NAC  
ARCHITECTURE  
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1303 WEST RIVERSIDE AVE  
SPOKANE, WA 99201  
P: 509.838.8240

NAC NO  
DRAWN: KTM  
CHECKED: EMF  
DATE: 08/11/23

CIVIL  
SITE  
PLAN

DD  
C200



KEYNOTES

- 1

MECHANICAL YARD; SEE ARCHITECTURAL PLAN FOR DETAILS
- 2

TYPE 1 CURB RAMP PER COS STD PLAN F-105
- 3

(4) 2Ø FIXED BOLLARDS; SEE LANDSCAPE PLAN FOR DETAILS
- 4

SITE WALL; SEE LANDSCAPE PLAN FOR DETAILS
- 5

STANDARD PARKING STALL
- 6

BARRIER FREE PARKING STALL PER COS STD PLAN G-80A
- 7

EV PARKING STATION
- 8

STORMWATER POND
- 9

CHAINLINK FENCE
- 10

HARD TOP PLAY AREA; SEE LANDSCAPE PLAN FOR DETAILS
- 11

SOFT TOP PLAY AREA; SEE LANDSCAPE PLAN FOR DETAILS
- 12

DRIVEWAY DROP PER COS STD PLAN F-103
- 13

CURB INLET PER COS STD PLAN F-109
- 14

MONUMENT SIGN; SEE LANDSCAPE PLAN FOR DETAILS.
- 15

BACKSTOP FENCING; SEE LANDSCAPE PLAN FOR DETAILS.
- 16

(3) 4" STAIRS
- 17

DRIVEWAY DROP PER COS STD PLAN F-103A
- 18

TYPE 2 CURB RAMP PER COS STD PLAN F-105
- 19

BIKE RACK; SEE LANDSCAPE PLAN FOR DETAILS
- 20

ENTRANCE CANOPY; SEE ARCHITECTURAL AND STRUCTURAL PLAN FOR DETAILS
- 21

CURBNOSE DOWN 

5

C201
- 22

GATE WITH KNOX BOX; SEE LANDSCAPE PLAN FOR DETAILS
- 23

STORAGE SHED
- 24

SITE WALL; SEE C300 FOR VARYING HEIGHTS
- 25

CONCRETE "V" GUTTER PER COS STD PLAN F-106A
- 26

CUSTOM CURB AND GUTTER PASSTHROUGH 

4

C201
- 27

CURB INLET TYPE 2 PER SPOKANE COUNTY STD PLAN B-9
- 28

PROPOSED SIGN, "NO PARKING EXCEPT SCHOOL BUS 8AM-5PM"
- 29

PROPOSED SIGN, "NO PARKING LOADING ZONE" PER MUTCD R7-6

SURFACING LEGEND

- CONCRETE SIDEWALK

2

C201
- HEAVY DUTY CONCRETE

3

C201
- HMA PAVEMENT

1

C201
- HEAVY DUTY HMA PAVEMENT

1

C201
- VC

VERTICAL CURB
- IC

INTEGRAL CURB
- NC

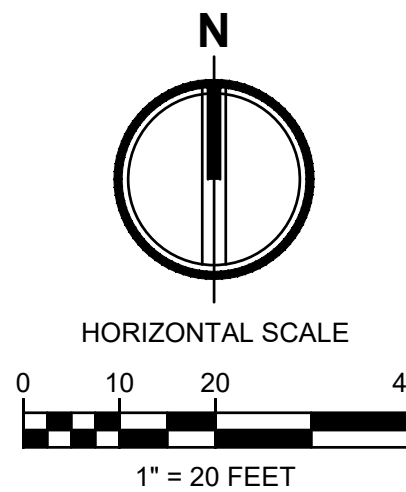
NO CURB
- X

CHANGE IN CURB TYPE
- M

MATCH EXISTING CURB TYPE

SITE PLAN NOTES

1. REFER TO SHEET C002 FOR CIVIL STANDARD NOTES.
2. CONTACT SANDIS FOR TEMPORARY BENCH MARKS (TBM) SHOWN.
3. REFER TO LANDSCAPE AND ARCHITECTURAL PLANTING PLANS FOR ADDITIONAL HORIZONTAL CONTROL, SITE FURNISHINGS, CONCRETE SCORING, PLANTING SCHEDULE, AND RELATED DETAILS NOT SHOWN ON THIS PLAN.
4. ALL DIMENSIONS TO FACE OF CURB UNLESS NOTED OTHERWISE.
5. ALL SIGNS TO BE INSTALLED PER CITY OF SPOKANE STANDARDS.
6. ALL NEW SIDEWALKS AND CONNECTIONS TO EXISTING SIDEWALKS SHALL BE BARRIER FREE PATHWAYS (I.E. ADA COMPLAINT).
7. VERIFY LOCATION OF ALL STRIPING WITH OWNER PRIOR TO INSTALLING. ALL STRIPING SHALL BE WHITE PAINT UNLESS NOTED OTHERWISE.
8. PAVEMENT PATCHING WITHIN THE RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE CITY OF SPOKANE'S REGIONAL PAVEMENT CUT POLICY.
9. VERTICAL CURB SHALL BE PER COS STD PLAN F-106.
10. MOUNTABLE CURB SHALL BE PER WSDOT STD PLAN F-10.64-03.
11. ROLLED CURB DRAIN SHALL BE PER WSDOT STD PLAN B-30.05-00.
12. CURB & GUTTER SHALL BE PER COS STD PLAN F-106.
13. INTEGRAL CURB AND GUTTER SHALL BE ADJACENT TO CURB OR MONOLITHIC CEMENT CONCRETE CURB AND SIDEWALK PER WSDOT STD PLAN F-30.10-04.
14. SEE ELECTRICAL PLANS FOR SITE LIGHTING DETAILS AND INFORMATION.
15. SITE IMPROVEMENTS SHALL COMPLY WITH RECOMMENDATIONS FROM APRIL 21, 2023 "GEOTECHNICAL EVALUATION REPORT" BY STRATA.
16. ALL SIDEWALKS, CURBS, AND DRIVEWAY APPROACHES ADJACENT TO THE PROPERTY WILL BE REVIEWED AT THE END OF THE PROJECT WHEN A CERTIFICATE OF OCCUPANCY IS REQUESTED. IF ANY ARE FOUND TO BE BROKEN, HEAVED, SUNKED, OR MISSING, THEY MUST BE REPAIRED/REPLACED WHETHER THE DAMAGE WAS EXISTING OR CAUSED BY CONSTRUCTION. IF YOU WOULD LIKE A SIDEWALK INSPECTION PRIOR TO REQUESTING OCCUPANCY, PLEASE CONTACT THE CITY OF SPOKANE AT (509)625-6300 TO ARRANGE A SITE VISIT.



Know what's below.  
Call before you dig.