



# Spokane Design Review Board

Wednesday, June 28, 2023

5:30-7:00 PM

Tribal Conference Room

Hybrid Meeting – Teleconference and In-person

**TIMES GIVEN ARE AN ESTIMATE AND ARE SUBJECT TO CHANGE**

## Board Briefing Session:

5:30 – 5:40	1) Call to Order	Chair
	2) Roll Call	Dean Gunderson
	3) Changes to the Agenda?	Chair

## Workshop:

5:40 – 7:15	4) <a href="#">Design Review of Adams Elementary Replacement</a>	Taylor Berberich
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## Board Business:

7:15 – 7:30	5) <a href="#">Approve Minutes from February 22, 2023</a>	Chair
	6) Old Business	
	7) New Business	
	8) Chair Report	Chair
	9) Secretary Report	Dean Gunderson
	10) Other	
	11) Adjourn	

**The next Design Review Board meeting is scheduled for Wednesday, July 12, 2023.**

The password for City of Spokane Guest Wireless access has been changed:

**Username:** COS Guest

**Password:** K8vCr44y

**AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION:** The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. The Tribal Conference Room in the first-floor lobby of City Hall, 808 W. Spokane Falls Blvd., is wheelchair accessible. Individuals requesting reasonable accommodations or further information may call, write, or email Risk Management at 509.625.6221, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or [mlowmaster@spokanecity.org](mailto:mlowmaster@spokanecity.org). Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.

# The Design Review Board meeting will be held in a hybrid format

Participants are able to join the meeting in-person (practicing safe distancing guidelines) in the Tribal Conference Room (City Hall, First Floor Lobby), or join the meeting on-line using the following information:

To participate via video follow the link on your computer (click on “Join meeting”)

## [Join Meeting](#)

### Microsoft Teams meeting

**Join on your computer, mobile app or room device**

[Click here to join the meeting](#)

Meeting ID: 212 030 123 881

Passcode: mfnAC9

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Video Conference ID: 114 278 844 9

[Alternate VTC instructions](#)

**Or call in (audio only)**

[+1 323-618-1887,,680560189#](#) United States, Los Angeles

Phone Conference ID: 680 560 189#

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While the meeting begins at 5:30pm, you can join as early as 5:15pm on the date of the meeting.

Dean Gunderson, Sr. Urban Designer

[dgunderson@spokanecity.org](mailto:dgunderson@spokanecity.org)

The audio proceedings of the Design Review Board meeting will be recorded, with digital copies made available upon request.

## **Meeting Process - Spokane Design Review Board**

### **Call to Order**

- Chair calls the meeting to order, noting the date and time of the meeting.
- Chair asks for roll call for attendance.
- Chair asks if there any changes to the agenda.

### **Board Workshop**

- Chair announces the first project to be reviewed and notes the following: a) the Board will consider the design of the proposal as viewed from the surrounding public realm; b) the Board does not consider traffic impacts in the surrounding area or make recommendations on the appropriateness of a proposed land use; c) the Board will not consider un-permitted, possible surrounding development(s) except those which are contemplated under the Comprehensive Plan and Development Code; c) it is the applicant's responsibility to meet all applicable Code requirements regardless of what might be presented or discussed during workshops.
- Chair asks for a staff report.

#### ***Staff Report***

- Staff report on the item, giving findings of fact. Presentation will be kept to 5-10 minutes.

#### ***Applicant Presentation***

- Chair invites the applicant(s) to introduce the project team and make a 10-15 minute presentation on the project.

#### ***Public Comment***

#### ***DRB Clarification***

- Chair may request clarification on comments.

#### ***Design Review Board Discussion***

- Chair will ask the applicants whether they wish to respond to any written public comments, after their response (if any) they are to return to their seats in the audience.
- The Chair will formally close public comments (unless motioned otherwise).
- Chair leads discussion amongst the DRB members regarding the staff topics for discussion, applicable design criteria, identification of key issues, and any proposed design departures.

#### ***Design Review Board Motions***

- Chair asks whether the DRB is ready to make a motion.
- Upon hearing a motion, Chair asks for a second. Staff will record the motion in writing.
- Chair asks for discussion on the motion.
- Chair asks the applicant if they would like to respond to the motion.
- After discussion, Chair asks for a vote.

#### ***Design Review Board Follow-up***

- Applicant is advised that they may stay or leave the meeting, and that the annotated & signed motion will be made available within five working days.
- Next agenda item announced.

### **Board Business**

- Meeting Minutes - Chair asks for comments on the minutes of the last meeting; Asks for a motion to approve the minutes.
- Chair asks is there any old business? Any old business is discussed.
- Chair asks is there any new business? Any new business is discussed.
- Chair Report – Chair gives a report.
- Secretary Report – Sr. Urban Designer gives a report.

### **Adjourn**

- Chair asks for a motion to adjourn. After the motion is seconded, and approved by vote, Chair announces that the meeting is adjourned, noting the time of the adjournment.

**Adams Elementary School****1 - Program Review/Collaborative Workshop****Design Review Staff Report**

June 28, 2023

**Staff:**

**Dean Gunderson**  
Senior Urban Designer

**Taylor Berberich**  
Urban Designer

Planning and Economic Development  
808 W. Spokane Falls Blvd.  
Spokane, WA 99201

**Applicants:**

**Greg Forsyth**  
SPS Capital Projects Director  
[GregoryF@spokaneschools.org](mailto:GregoryF@spokaneschools.org)

**Brooke Hanley, Architect**  
NAC Architecture  
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*When a Design Review application is received, city staff evaluate the project for compliance with all applicable regulatory documents. Should staff see a potential concern that falls within the purview of the Design Review Board, staff then present the board with Topics for Consideration. The purpose of these discussion points is to call attention to potential concerns and should not be viewed as required changes to the project.*



## Project Description

This project is to replace the existing school building for Adams Elementary School. Please see applicant's submittal for full project details.

## Quarter Mile Vicinity Map



### Large Vicinity Map

- 5-min. Walk Radius
- Subject Sites
- Building
- STA Bus Route
- STA Bus Stop
- City Park
- City Property

### Pedestrian Network

- Bike Friendly Route
- High Traffic (Bike Lane)
- High Traffic (Shared)
- Moderate Traffic (Bike Lane)
- Moderate Traffic (Shared)
- Shared Use Path
- Trail

### Zoning Designation

- Centers and Corridors
- Community Business
- Office / Office Retail
- RHD / RMF
- RSF / RTF



## **Location & Context (see map, previous page)**

### Site and Boundaries

Adams Elementary School is located on Spokane's South Hill, at 2909 E 37<sup>th</sup> Avenue. Zoned RSF, the site lies on the southernmost boundary of the Lincoln Heights Neighborhood Council. Southgate Neighborhood Council's northernmost boundary is to the south across 37<sup>th</sup> Avenue. Joel E. Ferris High School is directly south of the subject parcel.

### Transportation

Both Regal Street and 37<sup>th</sup> Avenue are designated as minor urban arterials. Two Spokane Transit Authority (STA) bus lines run near the school. The 43 bus runs east-west along 37<sup>th</sup> Avenue. The 4 (Monroe-Regal) High Performance Transit line runs north-south on Regal. STA operate stops for both lines at the northwest and southeast corners of the intersection of Regal and 37<sup>th</sup>. Adams Elementary maintains a mid-block stop with dedicated pull-out along 37<sup>th</sup> Avenue south & adjacent to the school site.

The City of Spokane Bike Plan recognizes Regal as a high non-motorized traffic route with a bike lane, and 37<sup>th</sup> as a moderate non-motorized traffic route with a bike lane. Daily motorized traffic counts on Regal are approximately 19,000 vehicles, while daily motorized traffic counts on 37<sup>th</sup> are approximately 11,000 vehicles.

The city has undergone transportation planning to analyze the capacities on several major arterials on the South Hill. The project will focus on the Ray, Freya, and Regal corridors. The intent is to look for alternatives to the Ray-Freya Crossover project, which had historically been part of the City's 6-Year Arterial Street Plan but was removed in 2017. The Ray-Freya Crossover would have moved traffic from Ray at 37<sup>th</sup> to Freya, running near Ferris High School, and was designed to ease traffic congestion. City staff is looking at other transportation improvements that could streamline traffic flow in the area. During the past few months City of Spokane staff held two online meetings to share the study results with the public and refine the alternatives. A final report is now available. The Spokane City Council endorsed the report's findings in Resolution 2021-0093, approved in November of 2021.

The recommended projects will be added to the 20-year street projects list and incorporated into the Bicycle Plan. Eventual construction work would be partially funded through Transportation Impact Fees, and the City would seek additional construction funding as needed. For more information, please visit <https://my.spokanecity.org/projects/ray-freya-alternatives-analysis/>

## **Character Assets (see map, following page)**

### Historic Significance

The school was built in 1910 but multiple renovations have removed much of the original façade. The current building is not considered a historical contributor to the area, nor is it on the historic register. No local or national historic districts or any registered historic structures are located within a ¼-mile radius (5-minute walk) of the site.

### Site Observations

The site is relatively flat. The Applicant is also requesting that the portion of the Fiske Street ROW located east of the site be vacated. The Applicant is requesting that the entirety of the ROW be aggregated to the site via a boundary line adjustment.

The intersection of Regal & 37<sup>th</sup> sees a high volume of vehicular traffic. Staff viewed the site at 2:30pm on a weekday when school had just ended, and there were many students waiting at the intersection to cross the street. Also, the intersection of 37<sup>th</sup> & Ray (a 4-way stop) saw long queues of students waiting for an opportunity to cross.

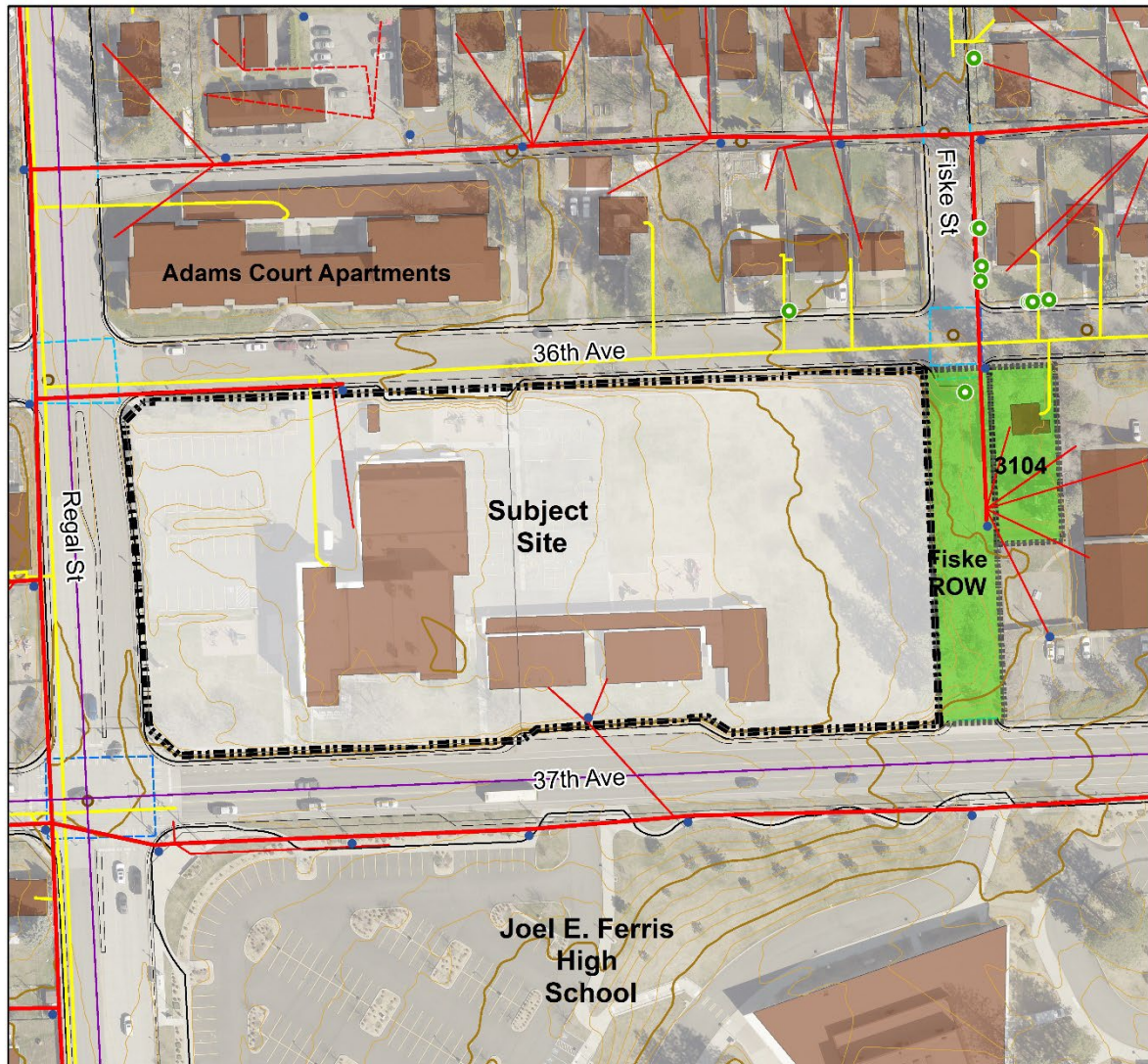
These observations led staff to consider the opportunity for a mid-block controlled crossing along 37<sup>th</sup> Avenue to lessen the pedestrian traffic at these two prominent intersections.

There are no public street trees on the site, however there are six mature and visibly healthy maple trees along the south property line. A concrete retaining wall separates the site from the sidewalk, which varies



in width from 5' to 8'. The mature pine trees along the eastern boundary of the site and along the Fiske Street right-of-way (to be vacated as part of this project) are part of the city's street tree inventory but have not been geo-located or added to the current map.

## Context Map



### Legend

- |                         |                             |
|-------------------------|-----------------------------|
| Subject Site            | Overhead Primary Electrical |
| Proposed Site Additions | Overhead Service Electrical |
| Existing Building       | Gas Pipe                    |
| Urban Local Access      | Utility Pole                |
| Urban Minor Arterial    | Sanitary Manhole            |
| Sidewalk                | Public Tree                 |
| Crosswalk               | 5-foot Conour               |
|                         | 1-foot Contour              |

## Topics for Consideration

*Should staff see a potential concern that falls within the purview of the Design Review Board, staff then present the board with Topics for Discussion. The purpose of these discussion points is to call attention to potential concerns and should not be viewed as required changes to the project.*

To address the Institutional Design Standards, Comprehensive Plan Policies, and Public Project Design Guidelines listed in the staff report, staff would offer the following for consideration and discussion:

1. Assuming that the vacation of the Fiske Street Right-of-Way is granted as requested by the applicant, is there an opportunity to create a bicycle/pedestrian Shared Use Path between 36<sup>th</sup> and 37<sup>th</sup> Avenues? Specifically, this will align with the current conversion of the Fiske Street greenway improvements from 29<sup>th</sup> Avenue to 35<sup>th</sup> Street (which includes a HAWK crossing installation at 29<sup>th</sup> Avenue to improve safe pedestrian crossings).
2. Is there an opportunity to create a safe mid-block pedestrian crossing of 37<sup>th</sup> Avenue connecting the Share Use Path addressed in Topic #1 with the pedestrian pathways through Joel E. Ferris High School to the south? Note, this would also align with future plans for a Shared Use Path along 37<sup>th</sup> Avenue and complete the Fiske Street Greenway connections.

Note: Both Topics 1 & 2 address inconsistencies with existing street design standards which prohibit block lengths in excess of 600'. The current block length between Regal and Ray Streets exceeds 1,200'. With the permanent vacation of the Fiske Street ROW, opportunity to bring this block frontage into compliance with standards would be eliminated. Current best practices encourage communities to consider min-block pedestrian crossings when block lengths approach 500'. Guidance for the creation of mid-block crossings (with and without pedestrian refuges) can be found in the NACTO Urban Street Design Guide (pages 114-115) and the City of Spokane Design Standards.

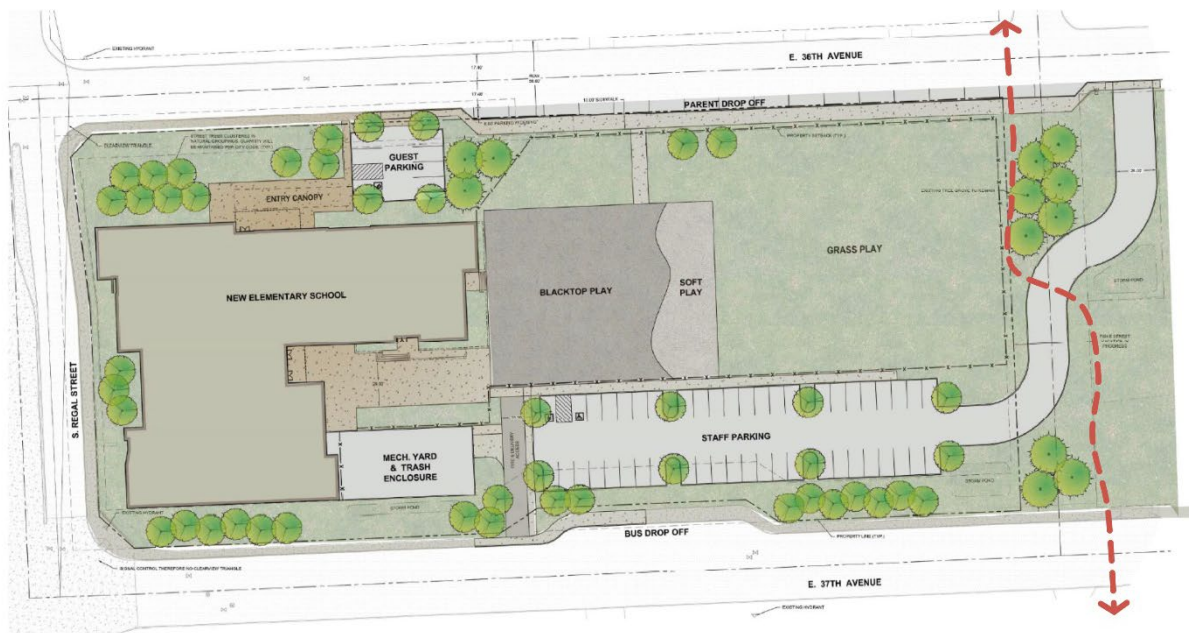


Figure 1- Proposed pedestrian pathway

3. Is there an opportunity to enhance the pedestrian realm along Regal Street and 37<sup>th</sup> Avenue? The current streetscape could benefit from amenities such as trees, landscaping, benches, improved retention wall aesthetics (see figure 4 for 36<sup>th</sup> Ave. treatment) and other rest areas, safety bollards, and lighting to name a few options. (See figures 2 and 3)

*Note: the following policies are required to be followed for this project:*



Separated Sidewalks with Landscape Buffer: [SMC 17H.010.190 Pedestrian Buffer Strips](#) – also City of Spokane Engineering Design Standard Plan F-102B (with pedestrian buffer strip), and City of Spokane Engineering Design Standards, Section 3.4-2 Buffer Zone and Section 3.14 Table 1 Street Dimensions. Street Trees: [SMC 17C.200.050 Street Tree Requirements](#) – note, per [Table 17C.200.050-1](#) a continuous planting strip in RSF zones is 6' in width, minimum. An exemption applies to the placement of required street trees only at designated loading zones for schools and churches (footnote 4).



Figure 2- Sidewalk on west side of Regal Street



Figure 2- Image superimposing the proposed site layout with existing tree locations

4. The applicant has indicated a desire to mimic the landscape and public realm surrounding Joel E. Ferris High School. Though the code has changed since that project was permitted, is there an opportunity to comply with the current street tree requirements (which requires street trees along all street frontages) while also achieving a similar contextual response?  
*\*Note: the current exceptions for street tree plantings at schools allow required street trees to be placed behind sidewalks where adjacent to bus drop-offs.*
5. The graphic in figure 4 superimposes the proposed site layout over the existing map. The green circles represent trees that would likely be safe from demolition, therefor retained. The red circles indicate mature trees that would likely have to be removed to accommodate the proposed layout. Given the applicant has indicated a desire to retain the stand of mature pines along the Fiske Street right-of-way, and the size and health of the mature maple trees along 37<sup>th</sup> Avenue, does the board have any advice regarding alternate site configurations to preserve the urban tree canopy?
6. As the 36<sup>th</sup> Avenue pavement width is already 36' in width (sufficient for two 11'-wide travel lanes, and a 7'-wide parallel parking bay on both the north and south sides of the street, see figure 4), there appears to be no need for the Applicant-proposed continuous vehicle pull-out along the south side of the street. What opportunities are there for minimizing pavement widths to keep the 36<sup>th</sup> Avenue streetscape pedestrian oriented?



Figure 3- Sidewalk on north side of 37th Avenue



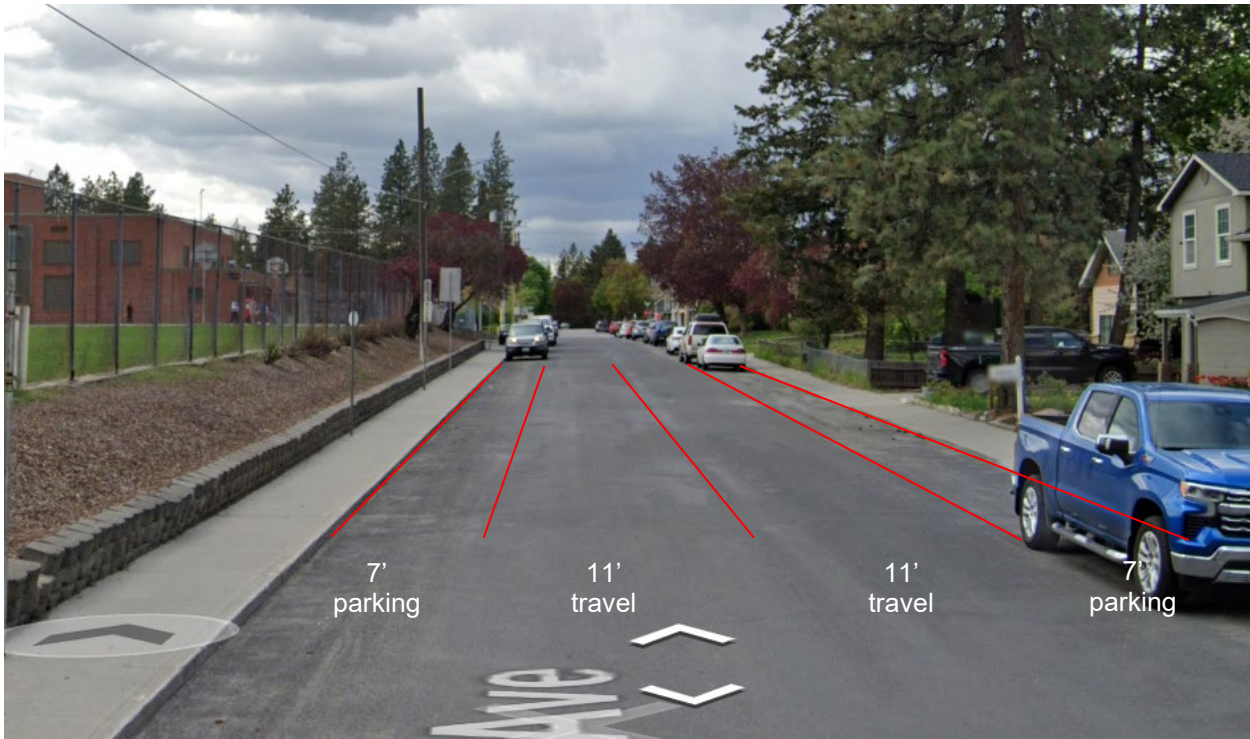


Figure 3. Street view of 36th Avenue, with Adams Elementary on the left (south)

## Regulatory Analysis

*Note: the applicant has done an exemplary job of responding to applicable policies and regulatory documents in pages 8-13 of their submittal packet.*

### Design Review Board Authority

#### Spokane Municipal Code [Chapter 04.13](#) Design Review Board

A. Purpose. The design review board is hereby established to:

1. improve communication and participation among developers, neighbors and the City early in the design and siting of new development subject to design review under the Spokane Municipal Code;
2. ensure that projects subject to design review under the Spokane Municipal Code are consistent with adopted design guidelines and help implement the City's Comprehensive Plan.
3. advocate for the aesthetic quality of Spokane's public realm;
4. encourage design and site planning that responds to context, enhances pedestrian characteristics, considers sustainable design practices, and helps make Spokane a desirable place to live, work and visit.
5. provide flexibility in the application of development standards as allowed through development standard departures; and
6. ensure that public facilities and projects within the City's right of way:
  - a. wisely allocate the City's resources,
  - b. serve as models of design quality

Under SMC [Section 17G.040.020](#) **Design Review Board Authority**, all public projects or structures are subject to design. Recommendations of the Design Review Board must be consistent with regulatory requirements per [Section 17G.040.080](#) **Design Review Board**

### Recommendations.

Recommendations of the Design Review Board will be forwarded to the Planning Director and the chair of the Lincoln Heights Neighborhood Council.

## **Zoning Code Requirements**

The site is zoned Residential Single Family (RSF). The applicant will be expected to meet zoning code requirements. Applicants should contact Current Planning Staff with any questions about these requirements.

**Recommendations of the Design Review Board must be consistent with adopted regulations.** The DRB may not waive any code requirements.

The applicant held a Pre-development conference with the City on June 15, 2023. Staff notes will be included in the project materials once they are available.

## **Institutional Design Standards**

Design standards in the code appear in the form of Requirements (R), Presumptions (P), and Considerations (C). Upon request of the applicant, the board may offer some flexibility from certain eligible code "design standards" if the board recommends that the proposed solution is equal or better than what is required, and still meets the purpose of the standard.

### **Section 17C.124.500 Design Standards Implementation:**

The design standards and guidelines found in SMC 17C.124.510 through SMC 17C.124.590 follow [SMC 17C.124.015](#), Design Standards Administration. All projects must address the pertinent design standards and guidelines. Design standards are in the form of Requirements (R), Presumptions (P), and Considerations (C). Regardless of which term is used, an applicant must address each guideline. An applicant may seek relief through [chapter 17G.030 SMC](#), Design Departures, for those eligible standards and guidelines contained in the zoning code.

### **Overview of analysis**

To determine if the proposed addition meet the design standards, the following project data is provided:

#### **SMC 17C.110.515 Buildings Along the Street**

The proposed building appears to ensure that the development will contribute to the liveliness of sidewalks along both Regal Street and 37<sup>th</sup> Avenue. During the June 16<sup>th</sup> Predevelopment meeting with the city, current planning staff indicated 37<sup>th</sup> would need to observe front yard setback standards. This would mean parking is not permitted within the setback. To comply with this requirement, the applicant is encouraged to work with current planning staff to address the staff parking lot configuration on 37<sup>th</sup> Avenue.

#### **SMC 17C.110.520 Lighting**

Staff will request lighting information when the applicant submits for the recommendation meeting.

#### **SMC 17C.110.525 Landscaped Areas**

Staff will request landscape details when the applicant submits for the recommendation meeting.

#### **SMC 17C.110.530 Street Trees**

At the time of writing, Urban Forestry has not had the opportunity to review the project for street tree requirements.

#### **SMC 17C.110.535 Curb Cut Limitations**

The proposed site plan appears to significantly limit curb cuts.

#### **SMC 17C.110.540 Pedestrian Connections in Parking Lots**

The staff parking lot south and east of the proposed building will need to provide pedestrian connections, however the guest parking lot north of the building does not reach the threshold of 30 or more parking spaces.

#### **SMC 17C.110.545 Transition Between Institutional and Residential Development**



The proposed building maintains a similar bulk to the previous building, however since it is closer to the street corner the upper floor has been stepped back to reduce the overall visual impact.

#### [SMC 17C.110.550 Treatment of Blank Walls](#)

The elevations provided by the applicant appear to meet this standard.

#### [SMC 17C.110.555 Prominent Entrances](#)

The addition's primary entrance appears to ensure that it is easily identifiable and clearly visible from the street and sidewalk.

#### [SMC 17C.110.560 Massing](#)

The proposed addition appears to reduce the apparent bulk of the building by providing a sense of "base" and "top".

#### [SMC 17C.110.565 Roof Form](#)

The proposed addition ensures that its roofline presents a distinct profile and appearance for the building.

#### [SMC 17C.110.570 Historic Context Considerations](#)

The building is not a historical contributor to the area nor is it on the historic register.

#### [SMC 17C.110.575 Screening](#)

The mechanical yard and trash enclosure will need to be screened from view, especially as it will be easily seen from 37<sup>th</sup> Avenue.

### **City of Spokane Comprehensive Plan**

[Comprehensive Plan link](#)

## **CHAPTER 3: LAND USE**

### **LU 2 PUBLIC REALM ENHANCEMENT**

LU 2.1 Public Realm Features: Encourage features that improve the appearance of development, paying attention to how projects function to encourage social interaction and relate to and enhance the surrounding urban and natural environment.

### **LU 4 TRANSPORTATION**

LU 4.4 Connections: Form a well-connected network which provides safe, direct, and convenient access for all users, including pedestrians, bicycles, and automobiles, through site design for new development and redevelopment.

### **LU 5 DEVELOPMENT CHARACTER**

LU 5.1 Built and Natural Environment: Ensure that developments are sensitive to the built and natural environment (for example, air and water quality, noise, traffic congestion, and public utilities and services), by providing adequate impact mitigation to maintain and enhance quality of life.

LU 5.2 Environmental Quality Enhancement: Encourage site locations and design features that enhance environmental quality and compatibility with surrounding land uses.

LU 5.5 Compatible Development: Ensure that infill and redevelopment projects are well-designed and compatible with surrounding uses and building types.

## **CHAPTER 4: TRANSPORTATION**

TR GOAL A: PROMOTE A SENSE OF PLACE: Promote a sense of community and identity through the provision of context-sensitive transportation choices and transportation design features, recognizing that both profoundly affect the way people interact and experience the city

TR GOAL B: PROVIDE TRANSPORTATION CHOICES: Meet mobility needs by providing facilities for transportation options – including walking, bicycling, public transportation, private vehicles, and other choices

**TR GOAL F: ENHANCE PUBLIC HEALTH & SAFETY:** Promote healthy communities by providing and maintaining a safe transportation system with viable active mode options that provides for the needs of all travelers, particularly the most vulnerable users.

**TR 1 Transportation Network For All Users:** Design the transportation system to provide a complete transportation network for all users, maximizing innovation, access, choice, and options throughout the four seasons. Users include pedestrians, bicyclists, transit riders, and persons of all abilities, as well as freight, emergency vehicles, and motor vehicle drivers. Guidelines identified in the Complete Streets Ordinance and other adopted plans and ordinances direct that roads and pathways will be designed, operated, and maintained to accommodate and promote safe and convenient travel for all users while acknowledging that not all streets must provide the same type of travel experience. All streets must meet mandated accessibility standards. The network for each mode is outlined in the Master Bike Plan, Pedestrian Master Plan, Spokane Transit's Comprehensive Plan, and the Arterial Street map.

**TR 2 Transportation Supporting Land Use:** Maintain an interconnected system of facilities that allows travel on multiple routes by multiple modes, balancing access, mobility and place-making functions with consideration and alignment with the existing and planned land use context of each corridor and major street segment.

**TR 15 Activation:** Build great streetscapes and activate public spaces in the right-of-way to promote economic vitality and a sense of place, with a focus on the designated Centers and Corridors identified in the Land Use chapter.

## **CHAPTER 8: URBAN DESIGN AND HISTORIC PRESERVATION**

### **DP 2 URBAN DESIGN**

**DP 2.5 Character of the Public Realm:** Enhance the livability of Spokane by preserving the city's historic character and building a legacy of quality new public and private development that further enriches the public realm.

**DP 2.6 Building and Site Design:** Ensure that a particular development is thoughtful in design, improves the quality and characteristics of the immediate neighborhood, responds to the site's unique features - including topography, hydrology, and microclimate- and considers intensity of use.

**DP 2.11 Improvements Program:** Facilitate improvements such as sidewalks, street improvements, street trees, sewers, and parks in neighborhoods and commercial areas designated for higher density development.

**DP 2.12 Infill Development:** Encourage infill construction and area redevelopment that complement and reinforce positive commercial and residential character.

**DP 2.21 Lighting:** Maximize the potential for lighting to create the desired character in individual areas while controlling display, flood, and direct lighting installations so as to not directly and unintentionally illuminate, or create glare visible from adjacent properties, residential zones, or public right-of-way.

### **DP 3 PRESERVATION**

**DP 3.4 Reflect Spokane's Diversity:** Encourage awareness and recognition of the many cultures that are an important and integral aspect of Spokane's heritage.

## **Design Guidelines for Public Projects**

[Guidelines PDF Link Here](#)

### **A: Urban Design**

#### **A-1 360-Degree Design**

The proposed building appears to respond to all surrounding frontages.

#### **A-2 Provide a Sustainable Framework**

#### **A-3 Accommodate the Multi-modal Transportation Network**

See Topic for Consideration #1.

#### A-4 Design for Change

The proposed project is designed to be flexible enough to respond to future changes in use and adapt to growing student enrollment.

### **B: Public Amenities**

#### B-1 Provide Elements that Define the Place

The project thematically gives reference to the historic educational brick styling of the original structure.

#### B-2 Provide Context-Sensitive Signage and Lighting

The signage and lighting for the proposed addition does appear appropriate for the scale and character of the project and immediate neighborhood. This will be addressed in more detail at the recommendation meeting but may be influenced at this point by members of the Design Review Board during the collaborative workshop.

#### B-3 Design for Personal Security

The layout of the proposed building appears to promote a sense of security for people visiting the facility in the evening through an adequate utilization of the four principles of Crime Prevention through Environmental Design (CPTED) – those being: natural surveillance, access control, territorial enforcement, and space management.

#### B-4 Universal Design

The layout of the proposed building and its relationship to the surrounding public realm accommodations appear to provide an overall barrier-free, ergonomic, and accessible extension of the public realm.

#### B-5 Provide Inviting and Usable Open Space

The proposed design provides open space that is generally visually pleasing, safe, and healthful. The applicant has indicated the site may be used by the community outside of school hours.

#### B-6 Enhance the Building and Site with Landscaping

Landscaping will be addressed at the recommendation meeting but may be influenced at this point by members of the Design Review Board during the collaborative workshop.

### **C: Pedestrian Environment**

#### C-1 Design Façades at Many Scales

The proposed building incorporates architectural features, fenestration patterns, and material composition that are sympathetic to the architecture of the existing facility and refer to the human activities contained within and surrounding the building.

#### C-2 Reinforce Primary Building Entries

The proposed building provides an entrance that promotes pedestrian comfort and orientation, without unduly detracting from the Primary Entrance to the school facility.

#### C-3 Develop Pedestrian-Oriented Spaces Along Street Frontages

The proposed design generally creates a series of human-scaled spaces that respond to how people will engage with their surroundings with clear paths of pedestrian travel, legible wayfinding, and enhanced connectivity.

*A key emphasis in this design guideline is the prioritization of such human-scaled improvements along the project's street frontage. As proposed the street frontage does require some additional treatments.*

#### C-4 Provide High Quality Walkable Design for the Public Realm

In general, the proposed design continues the existing level of walkable elements in the public realm. Connectivity to the surrounding site may need to be further explored. Please see topics for consideration.

C-5 Provide Appropriate Weather Protection

The proposed design provides adequate, well-lit weather protection at the principal pedestrian entry.

C-6 Enhance Alleyways

Not Applicable to this site.

**D: Architectural Expression**

D-1 Create Transitions in Bulk and Scale

The stepped roof form of the proposed building provides adequate transition from the taller institutional structure and the adjacent residential neighborhood.

D-2 Design a Well-Proportioned and Unified Building/Structure/Site

The proposed building organizes the massing of the publicly accessible interior and exterior spaces in a manner that is well-proportioned, while coherently extending the architectural vocabulary of the existing facility.

D-3 Maintain the Prevailing Street Edge

As the existing building is set back from the street edge, the proposed structure effectively realigns with the adjacent buildings to maintain the prevailing street edge.

D-4 Design with a Legible Part

The architectural expression was created through a shared vision of the design team and client. The parti is clearly legible and helped guide the applicant through the various design iterations.

D-5 Enhance the Skyline

While this guideline is normally applied to the upper constructed elements of a building, and how they may contribute to a larger regional context, the proposed building does maintain the skyline created by the current structure.

**E: Access & Screening**

E-1 Maximize Pedestrian Access to the Building and Site

The proposed site design does not propose any additional curb-cuts or drive-aisles into the existing pedestrian zones surrounding the site.

E-2 Minimize the Impact of Parking Facilities Along Street Frontages

The parking lot along 37<sup>th</sup> would have the greatest impact on the pedestrian realm. The applicant has indicated adequate screening in the form of landscaping will be used to reduce the visual impact. This is subject to change based on conversations with current planning and the need to ensure there are no parking facilities in the front yard setback along 37th Avenue.

E-3 Minimize the Presence of Service Areas

The proposed design will need to adequately screen service areas and mechanical equipment from the view of passersby. No rendering has been provided at this stage, but the applicant has indicated that solid masonry walls clad with brick veneer will be utilized to screen the equipment from view.

E-4 Design Sustainable Parking

The applicant has indicated on-site stormwater filtration and retention, as well as shade trees in the parking lots and electric vehicle charging infrastructure.

**Note**

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

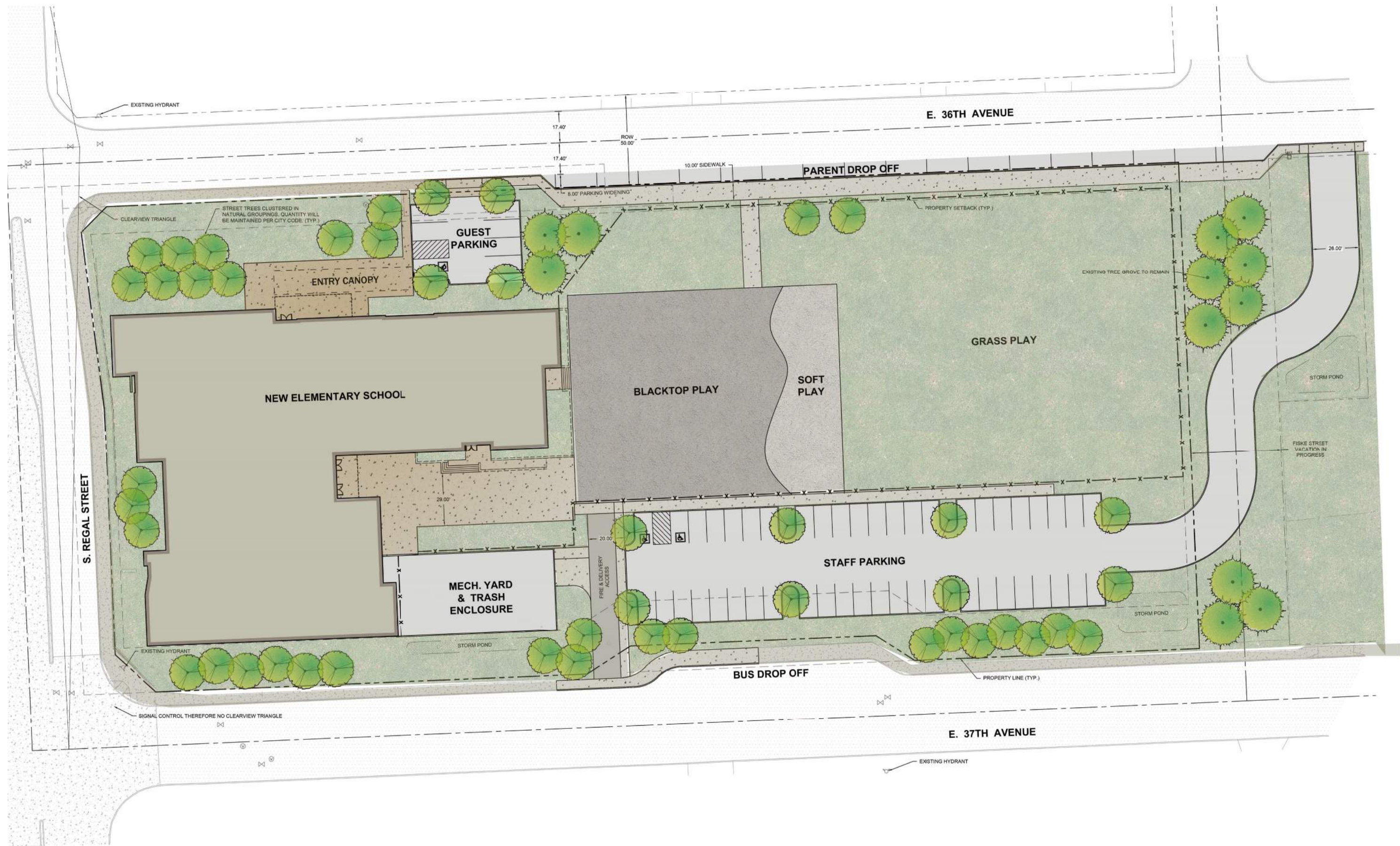
**Policy Basis**

Spokane Municipal Codes

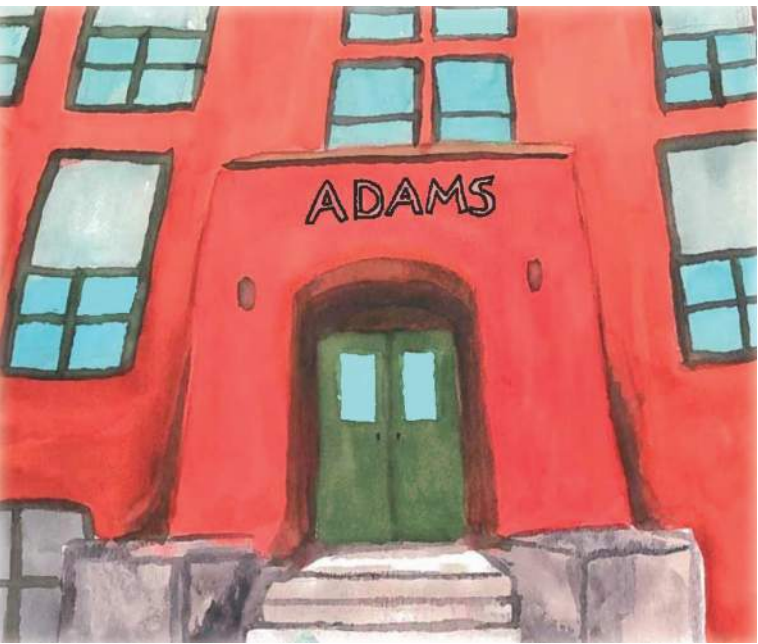
City of Spokane Comprehensive Plan

Public Project and Structures Design Guidelines









DEVELOPMENT OBJECTIVES:

Adams Elementary School is one of the first schools scheduled to be re-developed under the next Spokane Public Schools capital projects bond. Design is currently underway in order for construction activities to begin immediately after the bond is anticipated to be approved by Spokane voters in February 2024. This bond will fund demolition of the existing school and construction of the new Adams Elementary. During construction students will be relocated to “Camp Adams” at the old Jefferson Elementary School at 37th and Grand to allow for full access to the existing Adams site. Staff and students will be welcomed to their new school in January 2026 once construction is complete. The proposed new elementary school building will be a 60,000 square foot replacement of the existing 3-story brick structure which was originally built in 1910 with multiple renovations and additions throughout the years. All new outdoor playgrounds with age appropriate play structures, new grass play fields, and new hard surface play areas will be incorporated into the design for student recess during school and community access after hours. All new visitor and staff parking lots, as well as, parent pick-up/drop-off areas will be developed to increase traffic safety and efficiency.

DESIGN GOALS:

An important SPS goal is that each school is designed to meet the unique needs of the individual school’s community and culture. Since this project is a replacement of an existing school with an existing culture, the Adams design and planning group is tasked with guiding the vision and mission for the school’s current and future culture and goals. During the pre-design/educational specification phase of the process, the following goals and cultural principles were identified as the design committee’s shared vision for the project and they are organized by 5 key words developed by the staff:

- **Welcoming** - (approachable, neighborly) Focus on creating a welcoming environment both for the students and the community surrounding the school. The school should be prominently visible with the South Hill community.
- **Centered** - (cozy, secure, balanced) Create academic “villages” that are comprised of groupings of classrooms to create smaller scale learning communities within the overall school. The smaller scale fosters student to student, student to teacher and teacher to teacher engagement and connectivity. Building should be located on the site in a way that “shelters” the outdoor play areas from the busier streets along 37th and Regal.
- **Creative** - (playful, unexpected) The central shared spaces (Learning Commons, Art/Science, Performing Arts, Multipurpose, Gymnasium) are interconnected and more accessible than similar spaces in the existing building while each individual space has the opportunity to provide playful and unexpected moments that can be customized.
- **Versatile** - (adaptable, flexible, efficient) Provide flexible learning spaces beyond classrooms to promote collaboration. Long term adaptability to allow this facility to serve and adjust to future yet-to-be defined needs.
- **Empowering** - (self-reliant, accessible, inclusive, confident) Create an environment that is bright, warm and inclusive while developing a unique architectural character to deepen the feeling of ownership students have over their spaces. Students can easily access shared spaces from their classrooms, building self-reliance and confidence within the school community.

The “Welcoming” and “Centered” qualities of the building are qualities that the Adams community and culture has currently and wants preserved and carried forward in their new school. These are FAMILIAR qualities. The “Creative” and “Versatile” qualities are new qualities that the Adams community considers vital to the prosperity of their students in the future. These are NEW qualities. Overall guiding design statement: Adams Elementary School is a blend of FAMILIAR and NEW academic environments that EMPOWER students through the connections between these environments.

Program: The Adams program contains 23 teaching spaces for grades Pre-K through 5th Grade. These spaces vary from general classrooms, flex classrooms, and pre-school to special education classrooms, a project room (art/science), gym, music classroom/performance stage, and a learning commons (library). The building program also includes offices for administrative, counselors and itinerants, a multipurpose lunch room and kitchen for preparation, serving and eating meals. The total building area is targeted at 60,000 gross square feet and will serve a maximum of 450 students.

Building Site: Adams Elementary will be replaced on the existing school site at 2909 East 37th Avenue, across the street and just north of Ferris High School on the South Hill. The compact 3.7 acre site is bounded by Regal Street on the west, 36th Ave to the north, Fiske easement to the east, and 37th Ave to the south. Most students live within a one-mile radius of the school, therefore only one bus serves the school for students attending before and after school care, all other students arrive via vehicle, bike, or walking. Due to the limited site access points, current parent pick-up/drop-off is severely congested with the main staff/visitor parking lot blocked by pick-up traffic backing up onto Regal. Addressing separation of staff and visitor parking, along with extending the vehicle pick-up lane along 36th was a high priority identified by the design committee and a key element in organizing the site. Focusing on maximizing the space available on the tight site, addressing traffic flow around and within the site, along with orienting outdoor play spaces to receive the best solar orientation were important factors during early design discussions.



PROJECT SUMMARY - DESIGN EVOLUTION

Through a series of community outreach activities, Spokane Public Schools and the design team have made a concerted effort to understand the culture and personality embedded in the long history of the existing Adams Elementary and surrounding neighborhood. The feedback, regardless of the format in which it was received, highlighted a deep sense of community within the school while featuring the traits of an inclusive, welcoming, kind, diverse, and well-connected culture. Families tend to stay in the neighborhood for generations; it is not uncommon that families will have 3 generations that attended the school at some time. The existing 3-story, red brick building is familiar and beloved by many due to the fond memories of favorite teachers, yearly school functions, eating lunch in the gym/cafeteria, the warm feeling of wood floors & trim molding, or having fun on the playground during recess or after hours.

Site Design: The new building is positioned along the west edge of the site with the 3-level mass of the building in the northwest corner. Locating the school to the west more directly engages the major street-scapes and defines the public way and primary pedestrian experience while also using the building to shelter the outdoor courtyard and play areas to the east from the noise and traffic. The east side of the school is predominately landscaped areas and playfields. Visitor parking and parent drop-off is located to the north near the main office entry, and staff parking is located to the southeast along 37th. 36th Ave will be widened to accommodate a longer parent drop-off/pick-up lane to relieve the current traffic congestion before and after school. A small bus pull-out remains along 37th. Fire and service access will also occur along the south edge of the site. The Adams front door is located on 36th Ave and a generous translucent canopy or “front porch” shelters visitors as they approach the main entry. Here visitors will enter the building during the school day via a secure vestibule. Building services and a utility yard will all be located on the south side of the building and will include walls to screen views away from the equipment.

Building Design: While still early in the schematic design process, the exterior design of the building is not only meant to spark feelings of familiarity in relation to the original building with its primarily red-brick facades and its orderly organization, but it also speaks to the present and future by introducing new, creative elements that express themselves in large and small moments throughout. The potentially imposing scale of a 3-story building mass is relieved by stepping the 3rd floor walls back from the 2-story volume and finely articulating the facades through window openings, brick detailing, and other moments of interest. The exterior material palette is still being developed to define the red-brick and complementary cladding. The non-traditional massing of the gym and multipurpose volumes is intended to reduce the scale and provide a friendly face to the community along 37th and Regal.

DESIGN REVIEW BOARD SUBMITTAL  
JUNE 7, 2023

ADAMS ELEMENTARY  
SPOKANE PUBLIC SCHOOLS

WELCOMING: APPROACHABLE, NEIGHBORLY



CENTERED: COZY, SECURE, BALANCED



ADAMS  
ELEMENTARY  
SCHOOL

EMPOWERING :  
SELF RELIANT, ACCESSIBLE, INCLUSIVE, CONFIDENT



CREATIVE: PLAYFUL, UNEXPECTED



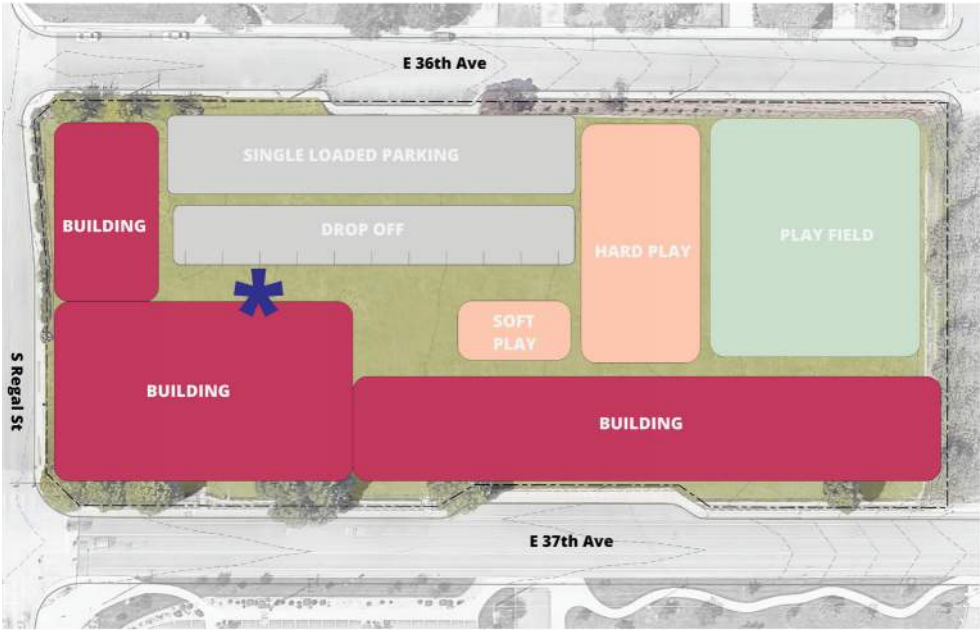
VERSATILE:  
ADAPTABLE FLEXIBLE, EFFICIENT





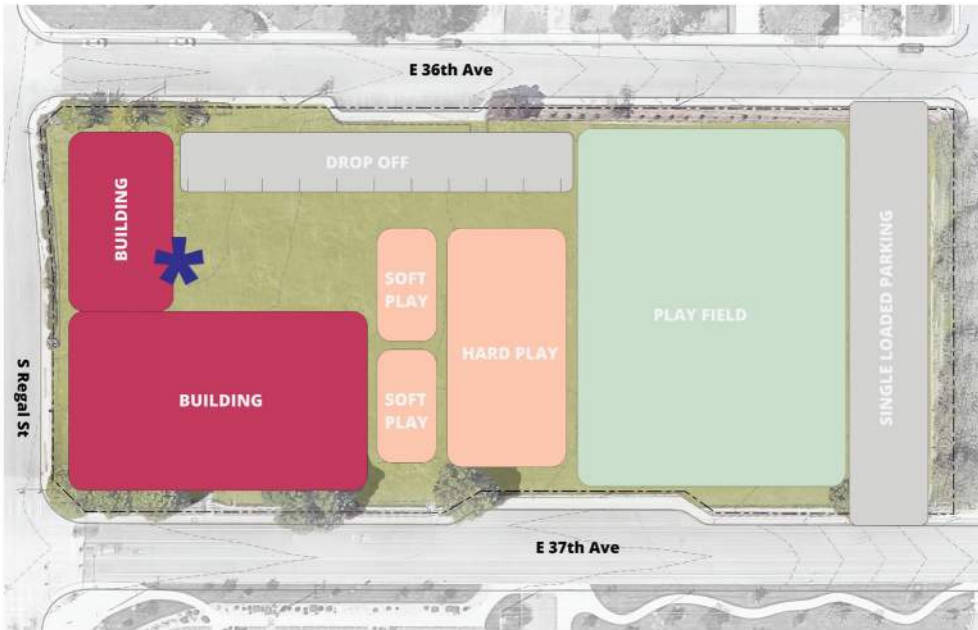
PROJECT SUMMARY - DESIGN EVOLUTION

The primary question of building organization was how would the building be orientated to address the primary streets, main entry, playfields, the surrounding neighborhood and views. Several adjacency options were studied with SPS. Shown below and on the following pages are a few samples of the various conceptual milestone diagrams that were studied.



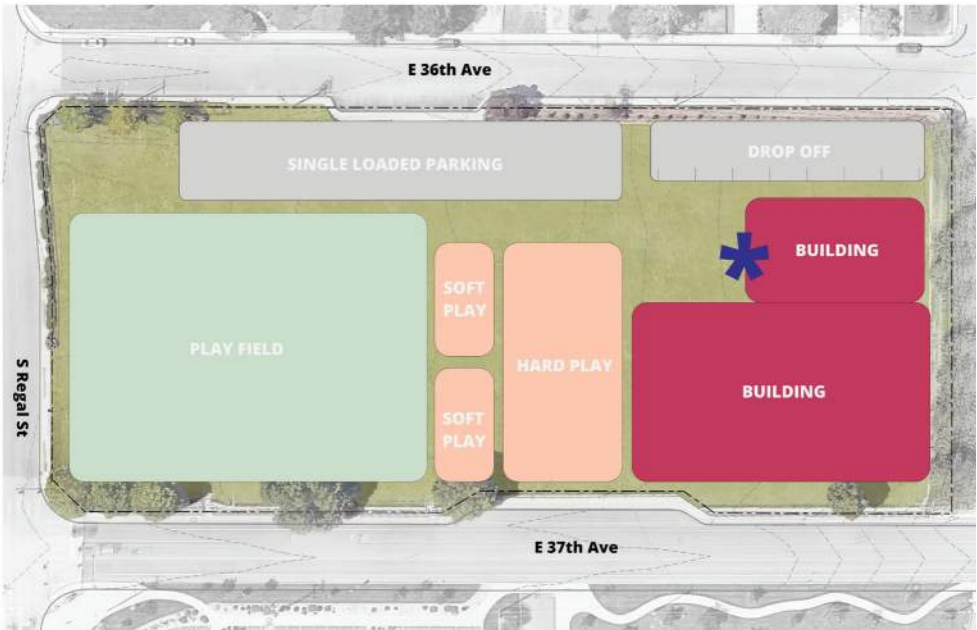
CONCEPT: 2-Story L-Shape West/South

A 2-story scheme, similar to other new SPS elementary schools, was explored but ultimately discarded due to the limited site available and the constraints this put on area left for parking and student play areas.



CONCEPT: 3-Story L-Shape West

This 3-story scheme has a much smaller building footprint on the site leaving more space for student playgrounds, parking and building service areas. This scheme also explored locating the building on the west side of the site to “shelter” the playgrounds from the noise and traffic of Regal and 37th and it also presents an urban street-scape opportunity along the street frontage. This scheme was ultimately preferred by the staff design committee.



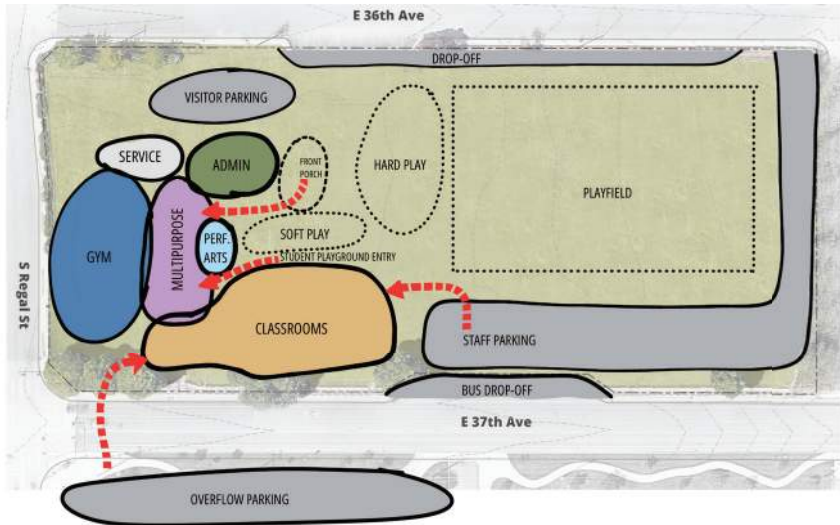
CONCEPT: 3-Story Compact East

This 3-story scheme also has a small building footprint but explores locating the building on the east side of the site. This orientation would be similar to some of the “rainbow” elementary schools where the grassy fields are up front and adjacent to the streets while the building is set back.

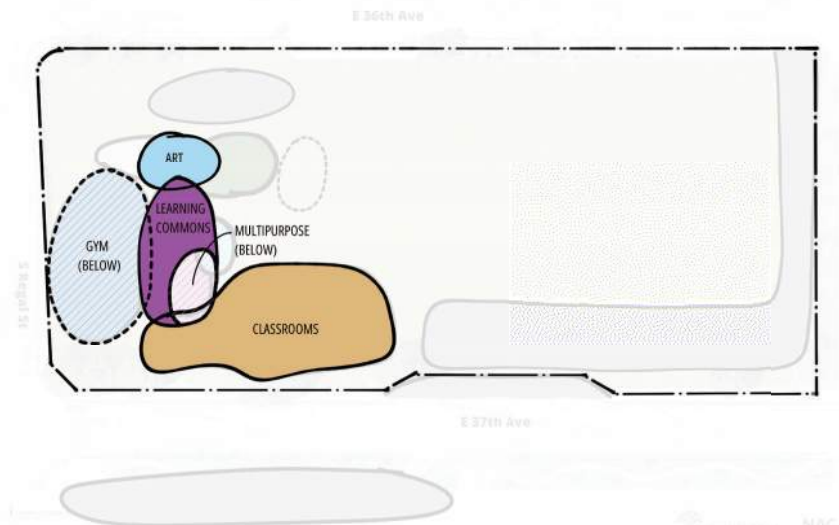




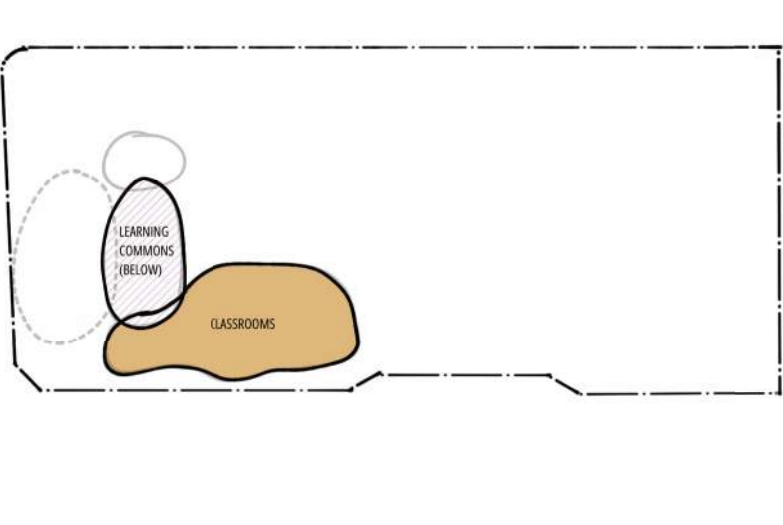
PROJECT SUMMARY - DESIGN EVOLUTION



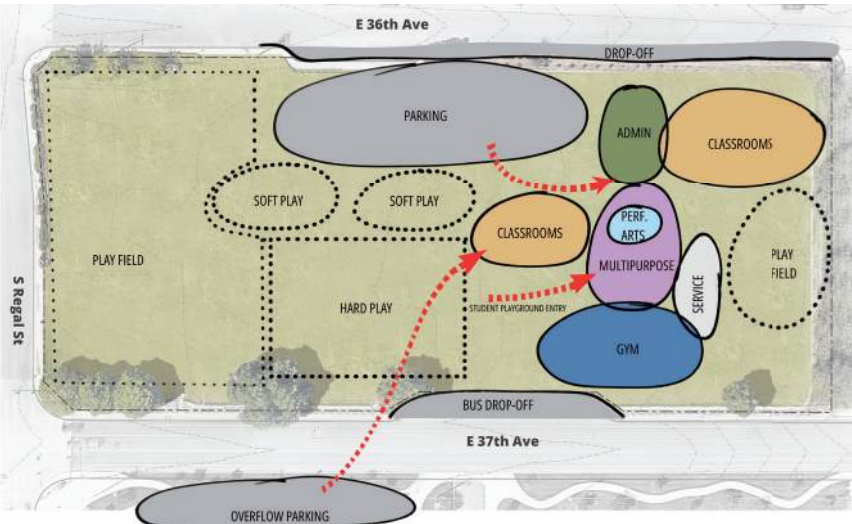
VERTICAL VILLAGES A - FIRST FLOOR (SELECTED CONCEPT)



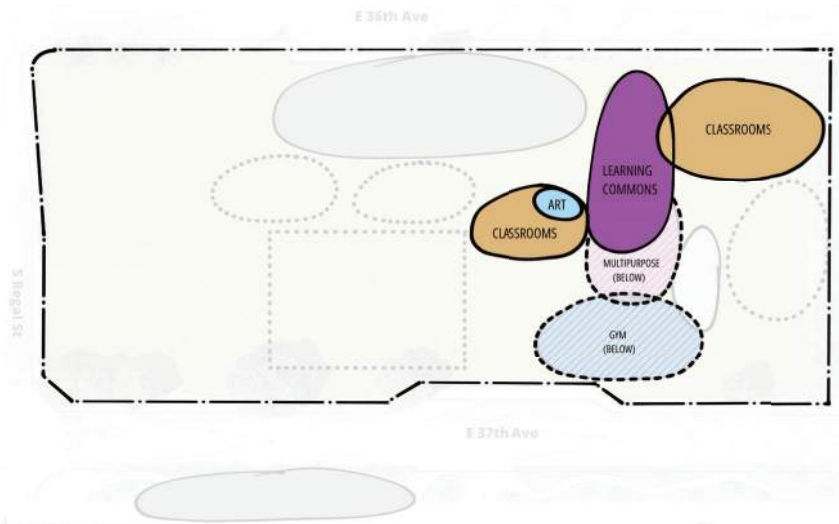
VERTICAL VILLAGES A - SECOND FLOOR



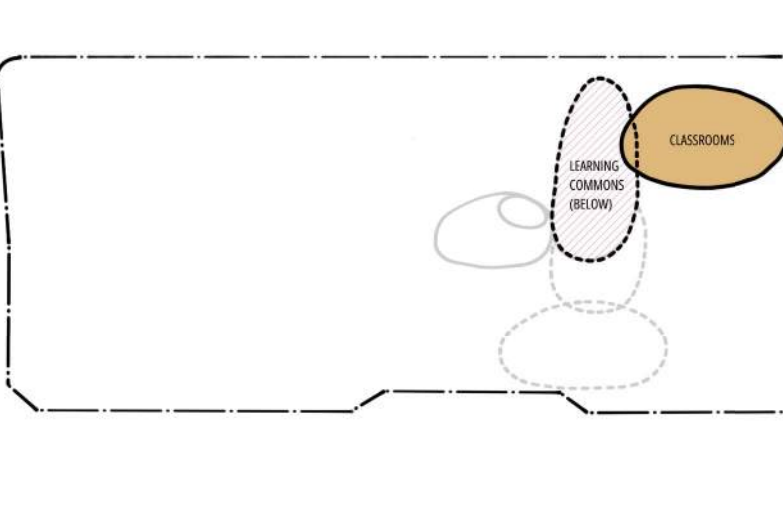
VERTICAL VILLAGES A - THIRD FLOOR



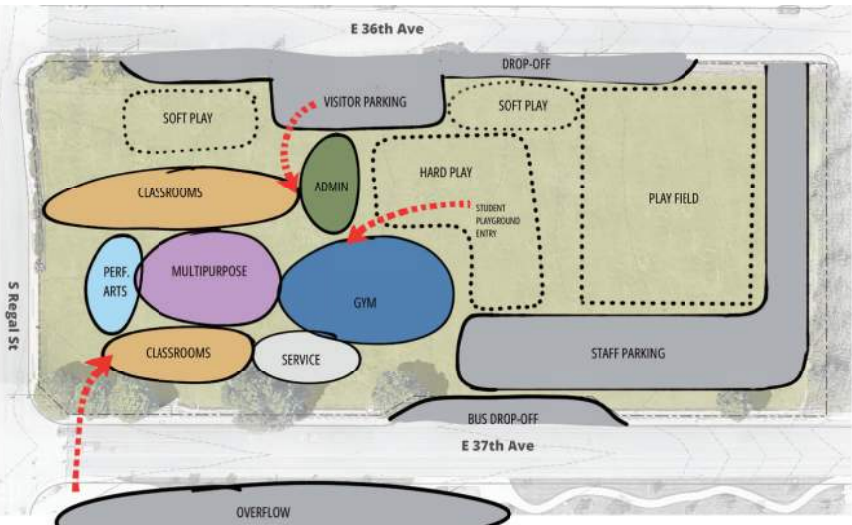
VERTICAL VILLAGES B - FIRST FLOOR



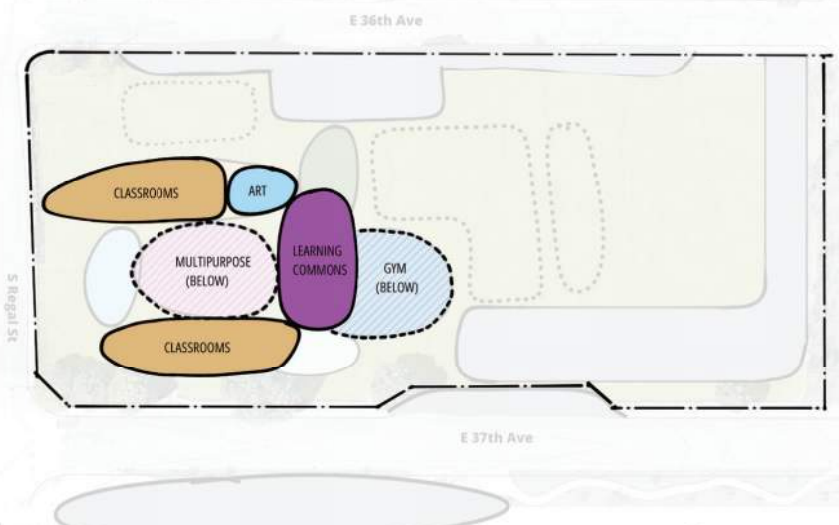
VERTICAL VILLAGES B - SECOND FLOOR



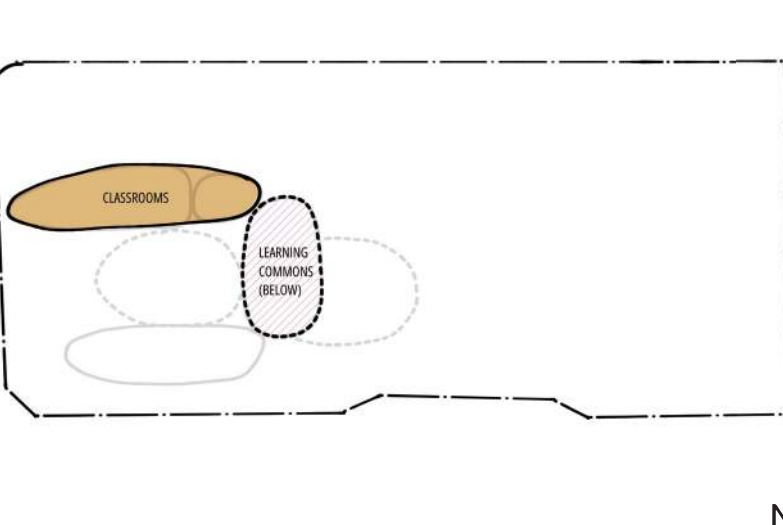
VERTICAL VILLAGES B - THIRD FLOOR



MIXING MARKET - FIRST FLOOR (SELECTED CONCEPT)



MIXING MARKET - SECOND FLOOR



MIXING MARKET - THIRD FLOOR



PROJECT SUMMARY - DESIGN EVOLUTION



VERTICAL VILLAGES - FIRST FLOOR (SELECTED CONCEPT)



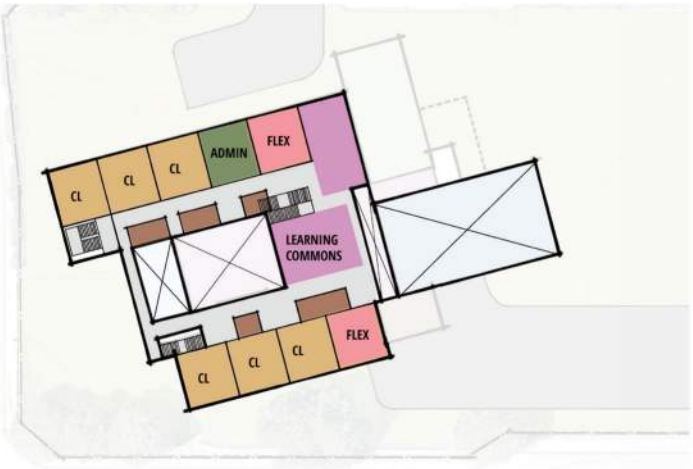
VERTICAL VILLAGES - SECOND FLOOR



VERTICAL VILLAGES - THIRD FLOOR



MIXING MARKET - FIRST FLOOR



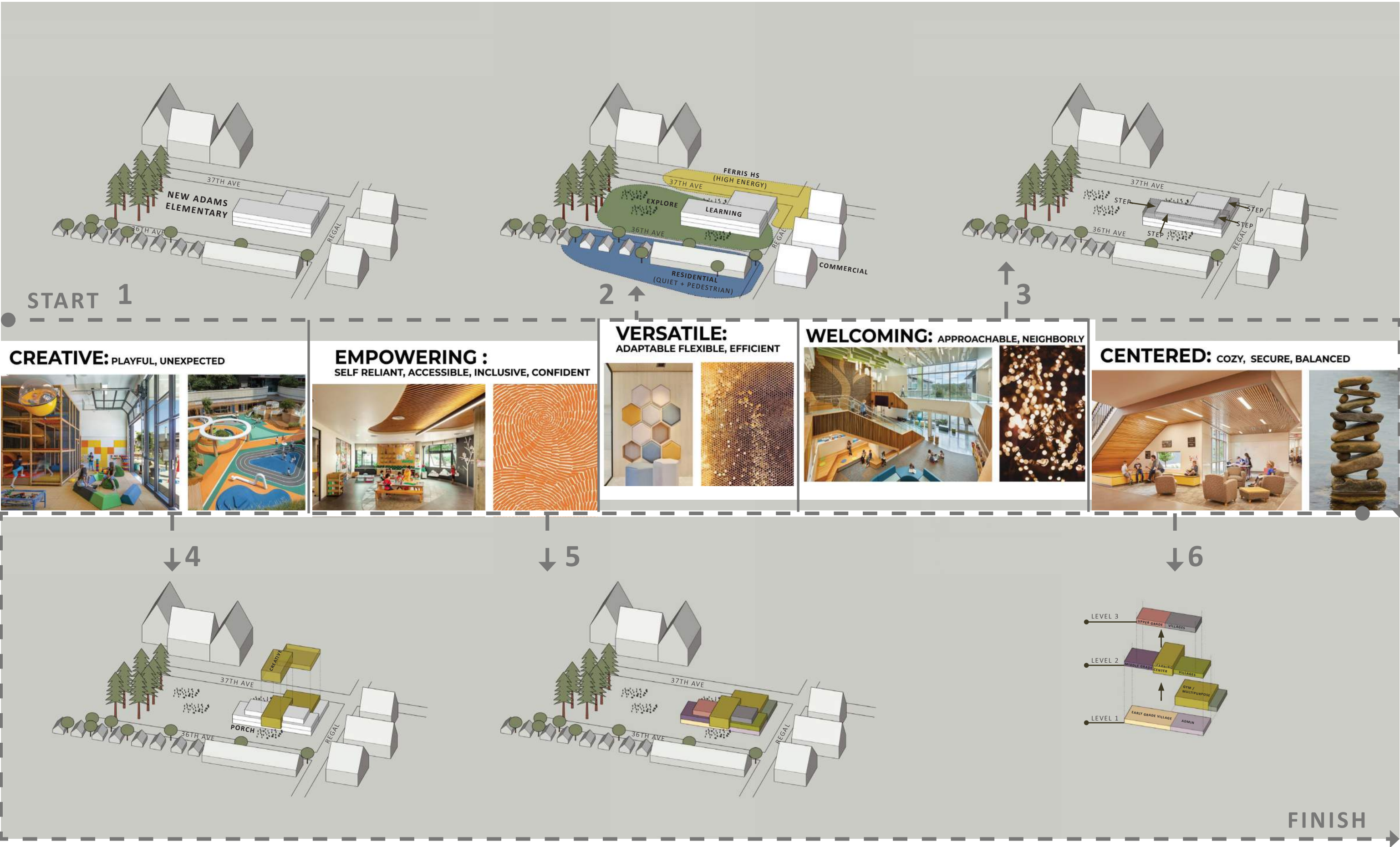
MIXING MARKET - SECOND FLOOR



MIXING MARKET - THIRD FLOOR









PROJECT SUMMARY



ADDRESS DESIGN GUIDELINES & SPOKANE MUNICIPAL CODE

Please review the following responses (in green below) to the design guidelines for public projects and applicable sections of SMC 17C.110. We feel the proposed design exceeds the minimum quality of design established by the adopted guidelines and that the proposed design extends and enhances the context of the Adams site to the surrounding neighborhood and community.

(A) URBAN DESIGN

A-1: 360 degree design:

-This project will be a prime example of a building where every facade is important and should be designed with the similar effort. The north side faces 36th and will host the main entry experience or “front porch”. The west and southern-most elevations face Regal and 37th and will be designed to address the busier, more commercial context along those sides. The west elevations face the student courtyard and playground and are intended to be appropriately scaled and detailed for elementary students.

A-2: Provide a Sustainable Framework

-The 3-story configuration maintains a compact footprint on the site, even though the overall building square footage will be larger. The building will exceed the requirements for the Washington State Energy Code and Washington Sustainable Schools Protocol. Reusing and improving an existing urban site is another sustainable approach to land-use within the city.

A-3: Accommodate the Multi-modal transportation network:

-The design of the project ties into existing sidewalks and crosswalks to accommodate and encourage pedestrian traffic from the surrounding neighborhood. The program of the project is specifically tailored to the immediate surrounding neighborhood and therefore helps to encourage multi-modal transportation. Spokane Public Schools also works with Safe Routes to School initiatives to improve safety and levels of physical activity for students by promoting walking and bicycling to school.

-The design will incorporate bike storage for staff, students, and visitors. The staff parking lot provides enough parking for daily use, but for events the parking across the street at Ferris can be used as overflow. Street parking is also available along 36th.

-Public transit stops are readily located on both sides of the intersection at Regal and 37th. The school district provides bus transportation for students that attend before and after school programs off-site.

A-4: Design for Change:

-The proposed project consolidates multiple buildings into one compact footprint at the west end of the site, leaving the east end of the site open for future infill development or future phased construction of a new school. The floor to floor heights are generous enough for 9’-0”+ ceilings throughout the spaces and even higher in the large spaces like the multipurpose or gymnasium. The large-bay steel framework of the structural system will allow for the flexibility of future interior/exterior renovations without demolishing the entire building.

(B) PUBLIC AMENITIES

B-1: Provide elements that define the place:

-The design of the building is proposed to have subtle nods to the history of the site with a familiar scale and masonry materials. New elements that speak to the context of today and look toward the future are also intended to be included. “Front porch” elements such as a generous, welcoming canopy at the main entry and careful attention to articulation and details will increase the sense of place. The pine tree groves are another defining element in this neighborhood and the landscaping design will be organized around keeping the existing and creating new groves to continue that language.

B-2: Provide Context Sensitive Signage and Lighting:

-Signage for the main entry will be easily identifiable to pedestrian and vehicular traffic along 36th but also scaled appropriately for that facade. The traditional monument sign and electronic reader board will be located on the more public corner of Regal and 37th (sized per code requirements) to catch the attention of the

PROJECT SUMMARY



most traffic in either direction since this is the most visible location. Site lighting will be Dark Sky compliant and building lighting will be minimal to meet accessible path requirements around the site while also being purposeful to denote entry points.

B-3: Design for Personal Safety and Security:

-The proposed project strongly emphasizes crime prevention through environmental design through concentric circles of protection. The visitor and parent drop-off approach to the building is directly in the line of sight of the main office of the school to provide an additional level of natural surveillance or “eyes on the street” approach to access control. Once inside the main entry secure vestibule, all other doors are locked and the visitor must use the video intercom to explain their purpose for visiting. The number of exterior access doors are minimized and also secured via electronic card reader access control. The playground is completely fenced during school hours for student safety and also feels sheltered from the busy streets by the placement of the building.

B-4: Universal Design:

-The design of the project is barrier free and easily accessible by all people regardless of physical ability level or impairment. Any changes in level will be accessed via sloping walks for all users, while true ramps and stairs will be avoided. Accessible parking stalls will be easy to find and distributed throughout the lots.

B-5: Provide Inviting and Usable Open Space

-The new playgrounds, including grass fields, sports fencing, play structures, and hard surface play areas will be accessible to students during the school day and to the community outside of school hours increasing the amount of park-like, inviting, and usable open space available to the surrounding neighborhood.

B-6: Enhance the building and site with Landscaping.

-The project will provide beautiful landscaping for users of the school as well as the surrounding neighbors. Site elements such as benches, bike racks, play structures, and screen walls will all be incorporated into the final design. Landscaping will be used to enhance the sense of place, provide multiple levels of scale, and soften the architecture, as well as be an opportunity for students to learn about native and naturalized plants. Landscaping choices will also be easy to maintain for Spokane Public Schools.

(C) PEDESTRIAN ENVIRONMENT

C-1: Design Facades at Many Scales:

-The facade of the new building responds to the activities within and helps to announce entrances using a canopy, glazing, and site circulation. The scale of the building steps down on all sides to help create an approachable scale. As one gets closer to the building the facade articulation in the masonry and detailing will reinforce the human-scale pedestrian experience.

C-2: Reinforce Primary Building Entries:

-Hierarchical components such as the canopy, signage, use of glazing, scale of landscaping, and building form help reinforce and distinguish individual building entrances at a scale appropriate to their need for public access. The main public entry is located along 36th while the student entries are located along the south elevation of the classroom wing and the east entrance to the multipurpose room with generous space for queuing in the exterior landscaped courtyard.

C-3: Develop Pedestrian-oriented Spaces Along Street Frontages:

-Connections to sidewalks along the streets will be maintained and new site landscaping will provide a shade and interest to the pedestrian experience.

C-4: Provide High Quality Walkable Design for the public Realm:

-In keeping with the street-scape approach to trees and landscaping that was designed at Ferris High School, the Adams project will have a similar high quality walkable design around the perimeter of the site.

C-5: Provide appropriate weather protection:

-The entrance to the elementary school includes a building overhang and additional length of canopy to shelter users and visitors. Trees along the sidewalks will enhance the urban forest canopy. Sidewalks and entry courts will be well-maintained during snow events.



PROJECT SUMMARY



C-6: Enhance Alleyways:  
-No alleyways are present in the surrounding context of the project.

(D) ARCHITECTURAL EXPRESSION

D-1 Create Transitions in Bulk and Scale:  
-Although the building is 3-stories, the 3rd floor walls “step back” from the face of the 2-story volume in an effort to reduce the scale of the facades. The massing and articulation around the multipurpose and gymnasium breaks up those large spaces into smaller expressions on the exterior while maintaining the volume needed on the interior.

D-2: Design a Well-proportioned and Unified Building/Structure/Site:  
-The parti diagrams speak to a combination of the familiar and new architectural expressions for this site as a nod to the past but also looking forward to the future. The familiar elements use more traditional materials, massing, and details, while the new, unexpected elements get a little more playful in their massing and materials. The two languages are intended to work together in a well-proportioned and unified design which will extend to the site and landscaping.

D-3: Maintain the Prevailing Street Edge:  
-By locating the building relatively close to the street (while maintaining required setbacks) along Regal and 37th this creates active and direct engagement to the street, continues an urban fabric language, and supports pedestrian-oriented activity. The building is set a little further back from the street along 36th to allow space for a generous grove of trees and a small visitor parking lot near the front door. By locating the larger staff parking lot along 37th instead of in the front yard of the building, the parked car is lower on the hierarchical scale than the prominence of the building along the street-scape.

D-4: Design with a Legible Parti:  
-Please review the parti diagrams in the Design Evolution section of this application. The parti is based on the shared vision of the client and design team.

D-5: Enhance the Skyline:  
-As a nod to the existing building, the new elementary school design is also the tallest structure (3-story) in the otherwise low-scale commercial and residential neighborhood. This will feel familiar to the existing community.

(E) ACCESS & SCREENING

E-1: Maximize Pedestrian Access to the Building and Site:  
-The proposed project will have no negative impact on the free flow of pedestrians onto and into the site from the public realm to the appropriate entry points. School safety and security will remain an important focus for the facility and certain areas of the site will only be accessible after hours as they are now.

E-2: Minimize the impact of parking facilities along street frontages:  
-The proposed parking lots minimize the number of parking stalls to as few as are needed for daily operations. Landscaping will be used to screen the parking lots from view of the surrounding sidewalks and playgrounds.

E-3: Minimize the presence of service Areas:  
-The proposed utility yard will be minimized in size to the extent possible. Solid masonry walls clad with brick veneer will be utilized to screen the equipment from view.

E-4: Design Sustainable Parking:  
-All stormwater infiltration/retention will occur on-site. Parking lots will be designed to include shade trees and infrastructure for electric vehicle charging stations.



SPOKANE MUNICIPAL CODE - LAND USE STANDARDS IN RESIDENTIAL ZONES 17C.110.510- .575 RESPONSES

.515: Buildings Along the Street

Design Standards.

1. New Development Shall not have only parking between buildings and Street. (P)  
Parking is minimized to a small visitor lot between the front of the building and the street along 36th Ave.
2. Buildings placed Along Sidewalks shall have windows and doors facing the street and shall incorporate other architectural features. (P)  
Proposed project design has windows and/or doors on entire facade along streets. Portions of wall with limited windows will be articulated with additional detailing to create interest.
3. Gardens, Plazas, or other open space shall meet the L3 landscape standards of chapter 17C.200 SMC, Landscaping and Screening.  
Proposed landscaping is similar in nature to the approach at Ferris High School where a deviation was granted to plant trees in groupings along the property instead of regularly spaced along the street. This approach fits within the existing context of the neighborhood and the greater South Hill natural areas.

.520: Lighting

1. Lighting shall be provided within parking lots, along pedestrian walkways and accessible routes of travel. (R) Bollard or pole lights and/or lighting along building will be provided along pedestrian walkways and accessible routes of travel. Pole lights will be provided in the parking lots and will meet the Dark Sky compliance.
2. Lighting fixtures shall be limited to heights of twenty-four feet for parking lots and sixteen feet for pedestrian walkways. (P) Bollard or pole lights will be below 16' for pedestrian walkways and 24' for parking lots.
3. All lighting shall be shielded from producing off-site glare, either through exterior shields or through optical design inside the fixture, so that the direction of light is downwards. (R) This requirement will be met.

.525 Landscaped Areas

1. Building setbacks.  
The required building setbacks must be landscaped to at least the L3 standard of chapter 17C.200 SMC, Landscaping and Screening. Parking, access, and maneuvering areas, plazas, detached accessory structures and other allowed development are exempt from this standard. Sites developed with a detached house, attached house or duplex are also exempt from this standard. (R) Landscaping will occur in the required building setbacks. Variances will be requested to continue the landscaping approach that is installed in the neighboring property at Ferris High School.
2. Parking Areas. Perimeter and internal parking area landscaping standards are stated in chapter 17C.200 SMC, Landscaping and Screening. (R) Landscaping will occur in the parking areas. Variances will be requested to continue the landscaping approach that was allowed in the neighboring property at Ferris High School.
3. Utility Substations. N/A

.530: Street Trees

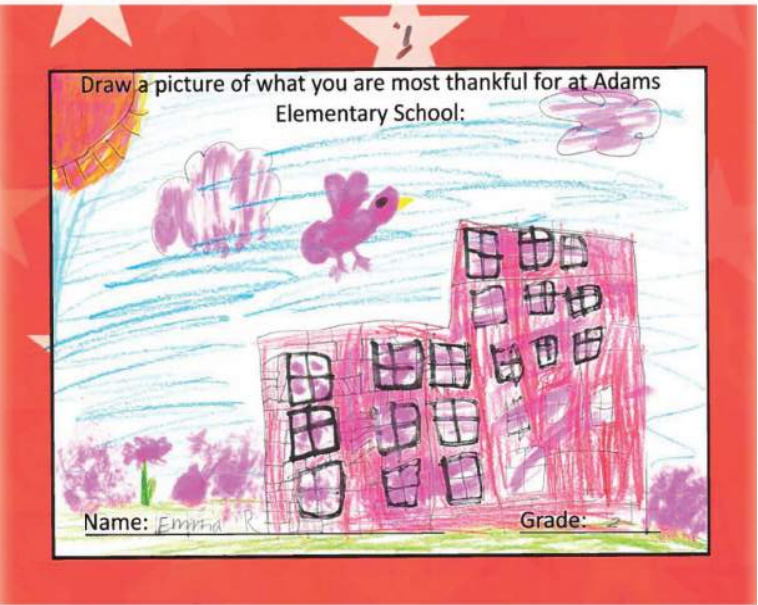
Variances will be requested to continue the landscaping approach that was allowed in the neighboring property at Ferris High School.

.535: Curb Cut Limitations

1. A curb cut for a nonresidential use shall not exceed thirty feet for combined entry/exits. Driveway width where the sidewalk crosses the driveway shall not exceed twenty-four feet in width. (R)  
No curb cuts will exceed this requirement.
2. The sidewalk pattern shall carry across the driveway. (R)  
This requirement will be met.
3. Adjacent developments shall share driveways, to the greatest extent possible. (P)  
N/A
4. Vehicular access shall be designated so that traffic is not directed through an adjoining residential zone. (P)  
Vehicular traffic will be maintained as is currently used.



PROJECT SUMMARY



.540: Pedestrian Connections in Parking Lots

- 1. Within parking lots containing more than thirty stalls, clearly defined pedestrian connections shall be provided: (R)
  - a. between a public right-of-way and building entrances. Pedestrian connections are included in these areas in proposed design.
  - b. between parking lots and building entrances pedestrian connections can be counted toward the amount of required landscaping.
- 2. Pedestrian connections shall not be less than five feet wide. (R) The proposed site design complies with this requirement
- 3. Pedestrian connections shall be clearly defined by at least two of the following: (R) All new pedestrian connections will meet at least two of these requirements.
  - a. Six-inch vertical curb.
  - b. Textured paving, including across vehicular lanes.
  - c. A continuous landscape area at a minimum of three feet wide on at least one side of the walkway.

.545: Transition Between Institutional and Residential Development

B. Design Standards. Code provisions require lower heights for portions of buildings that are close to single-family residential zones. In addition, any side of the building visible from the ground level of an adjacent single-family residential zone shall be given architectural treatment using two or more of the following: (P) All elevations of the building along Regal and 36th are zoned Multi-Family Residential and/or Office. Although the property across 37th to the south is zoned single family residential, it is a high school that is a non-residential use.

- 1. Architectural details such as:
  - a. projecting sills;
  - b. canopies;
  - c. plinths;
  - d. containers for season plantings;
  - e. tilework;
  - f. medallions.
- 2. Pitched roof form.
- 3. Windows.
- 4. Balconies.

.550: Treatment of Blank Walls:

Design Standards. Code provisions require lower heights for portions of buildings that are close to single-family residential zones. In addition, any side of the building visible from the ground level of an adjacent single-family residential zone shall be given architectural treatment using two or more of the following: (P) Please see attached renderings. Each elevation in the project includes windows or additional detailing. Also landscaping in the setbacks will soften any areas that do not have as many windows.

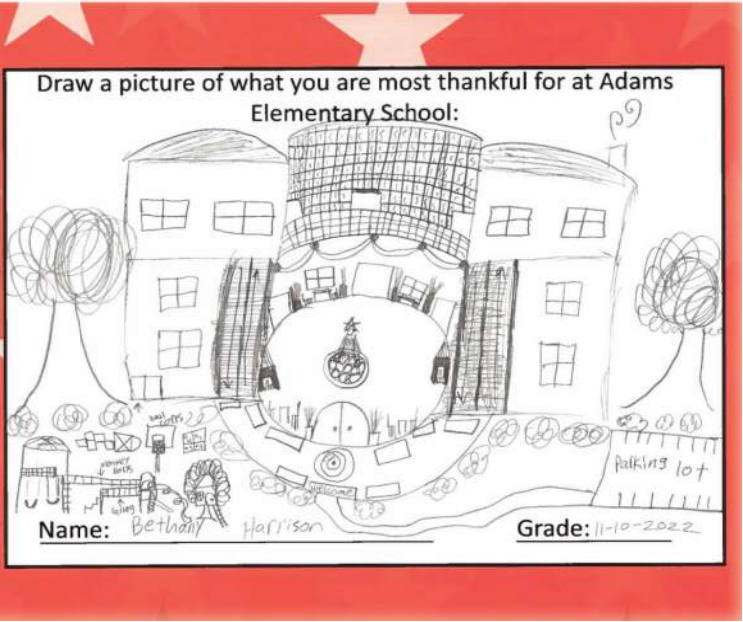
- 1. Architectural details such as:
  - a. projecting sills;
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  - c. plinths;
  - d. containers for season plantings;
  - e. tilework;
  - f. medallions.
- 2. Pitched roof form.
- 3. Windows.
- 4. Balconies.

.555: Prominent Entrances

The principal entry to a store/building shall be marked by: (P) Please see included renderings. The new Adams Elementary main entry point is recessed below a 2nd floor volume protrusion and also has a canopy leading up to the protruding element.

- a. ornamentation around the door; and
- b. at least one of the following:
  - i. Recessed entrance (recessed at least three feet).
  - ii. Protruding entrance (protruding at least three feet) .

PROJECT SUMMARY



- iii. Canopy (extending at least five feet).
- iv. Portico (extending at least five feet).
- v. Overhang (extending at least five feet).

.560: Massing

- 1. Buildings shall have a distinct “base” at the ground level, using articulation and materials such as stone, masonry or decorative concrete. (P)  
Please see attached perspectives. The project has a regular rhythm of first floor windows and prominent sills, as well as, larger glazed areas that help define the base of the building from the upper portions. The sidewalks have also been pulled away from the face of the building to allow for a landscaped area that “grounds” the building as a “base”.
- 2. The “top” of the building shall be treated with a distinct outline with elements such as a projecting parapet, cornice, or projection. (P)  
The proposed design has a distinct 3rd floor level that serves as the “top” of the 3-story mass. All roofs will have a projecting parapet or defined coping edge, as well.

.565: Roof Form

- Design Standards. Buildings shall incorporate one of the following roof forms: (P)
- 1. Pitched roofs with a minimum slope of 4:12 and maximum slope of 12:12, especially to highlight major entrances.
  - 2. Projecting cornices to create a prominent edge when viewed against the sky.
  - 3. Terraced roof forms that complement adjoining development.  
Roof forms incorporate one or more of these roof requirements.

.570: Historic Context considerations

- 1. The new development of public structures shall incorporate historic architectural elements that reinforce the established character of a center or corridor. New semi-public structures should consider this design standard. The following elements constitute potential existing features that could be reflected in new buildings: (P) The new Adams design will have nods to the historic architecture of the existing building while also balancing elements that speak to current day or contemporary architecture.
  - a. Materials.
  - b. Window proportions.
  - c. Cornice or canopy lines.
  - d. Roof treatment.
  - e. Colors.
- 2. When rehabilitating existing historic buildings, property owners are encouraged to follow the Secretary of the Interior’s Standards for Rehabilitation. (P) Not applicable
- 3. If a proposed building is not adjacent to other buildings having a desirable architectural character, it may be necessary to look at contextual elements found elsewhere within the area. (C) Not applicable

.575: Screening

- 1. Garbage and Recycling Collection Areas.  
All exterior garbage cans, garbage collection areas, and recycling collection areas must be screened from the street and any adjacent properties. Trash receptacles for pedestrian use are exempt. Screening must comply with at least the standards of chapter 17C.200 SMC, Landscaping and Screening. (R) Trash and recycling equipment will be screened within the utility yard area.
- 2. Mechanical Equipment.  
Mechanical equipment located on the ground, such as heating or cooling equipment, pumps or generators must be screened from the street and any adjoining residential zones by walls, fences or vegetation tall enough to screen the equipment. Mechanical equipment on roofs must be screened from the ground level of any adjoining R-zoned lands. (P) Mechanical equipment will be located behind screen walls on the second floor roof or within the screened utility yard.
- 3. Cell Phone Transmission Equipment.  
Cell phone equipment should be blended in with the design of the roofs, rather than being merely attached to the roof-deck. (C) Not Applicable
- 4. Other Screening Requirements.  
The screening requirements for parking, exterior storage, and exterior display areas are stated with the standards for those types of development.  
Noted



CONTEXT / SITE ANALYSIS

Adams Elementary School is located on one of the smaller sites in the Spokane Public Schools inventory at slightly under 4 acres. Located at the corner of 37th Ave and Regal St on the South Hill, these two streets are arterials with limited access points to the site. The north edge of the site is bordered by 36th street with a wooded easement for a future road to the east. SPS recently purchased an available property on the northeast corner of the site. The existing site has relatively little change in topography but is elevated approximately 3'-5' above the adjacent roadways as they slope downhill toward the east end of the site. Primary views into the site are from Regal & 37th. Views out of the site are to the residential neighborhood to the north, a mature grove of pine trees to the east, south to Ferris High School and the Dishman Hills beyond. From the upper floors there is a beautiful 360 degree view of the surrounding areas.



VICINITY MAP (NTS)



APPROACH FROM E 37TH ST - EAST BOUND



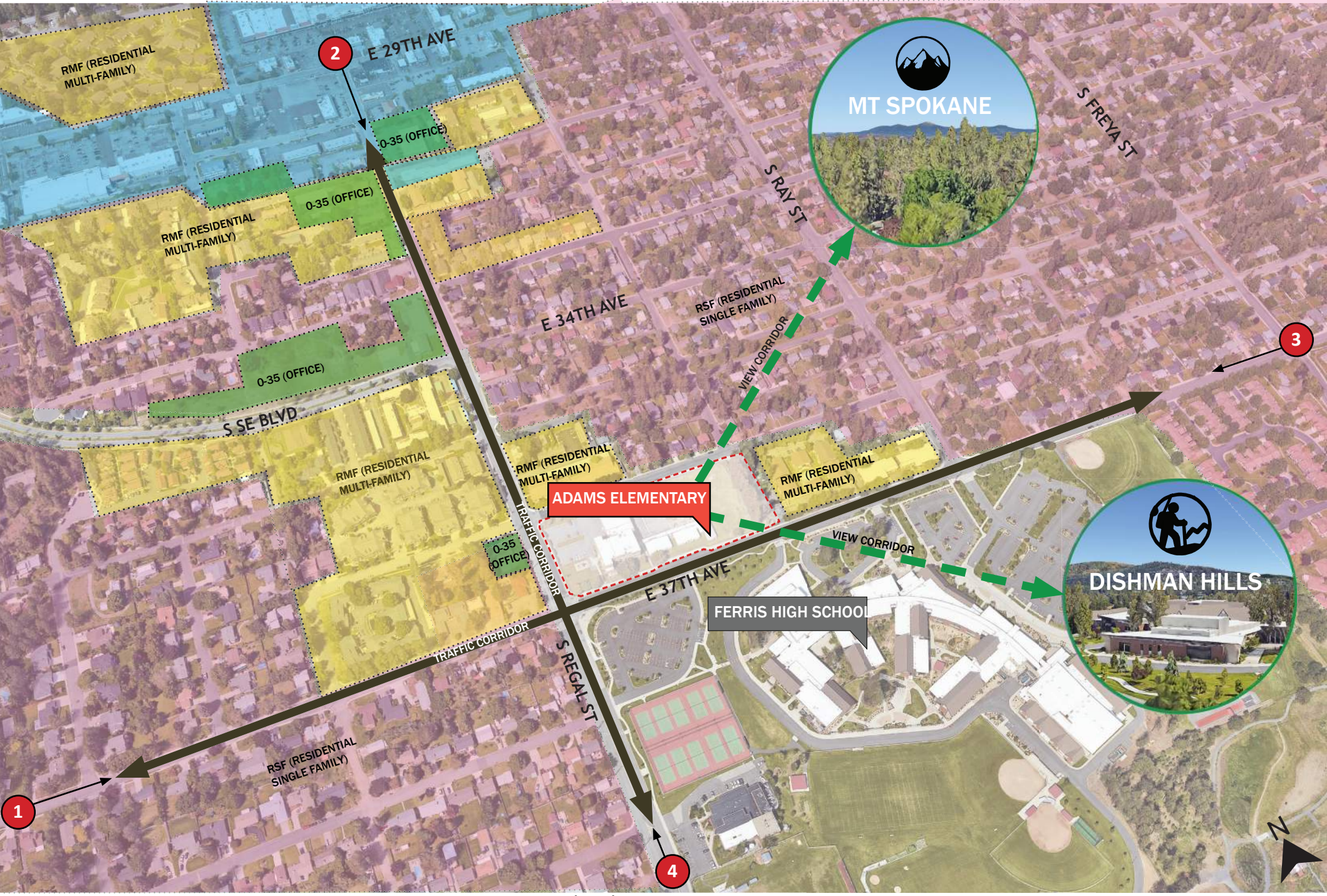
APPROACH FROM S REGAL ST - SOUTH BOUND



APPROACH FROM E 37TH ST - WEST BOUND



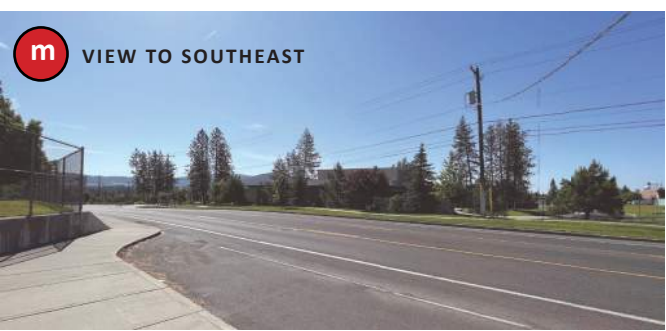
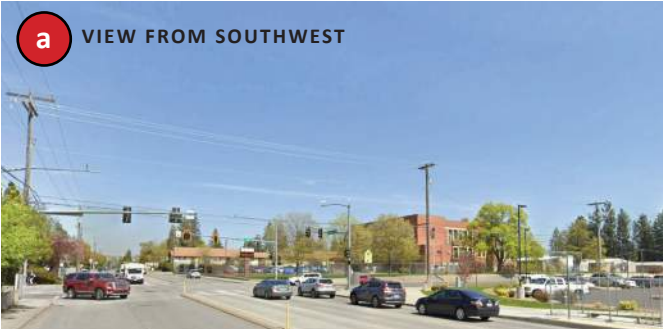
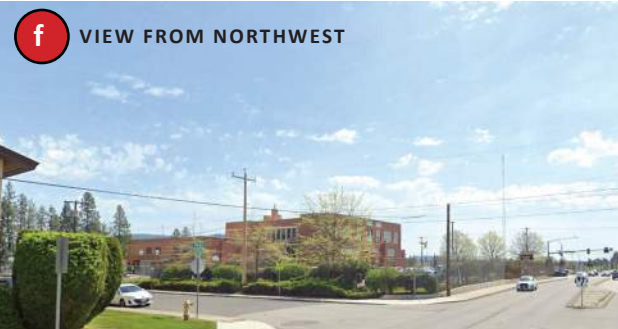
APPROACH FROM S REGAL ST - NORTH BOUND



AERIAL MAP - TRAFFIC CORRIDORS, ZONING, & PUBLIC VIEWPOINTS (NTS)



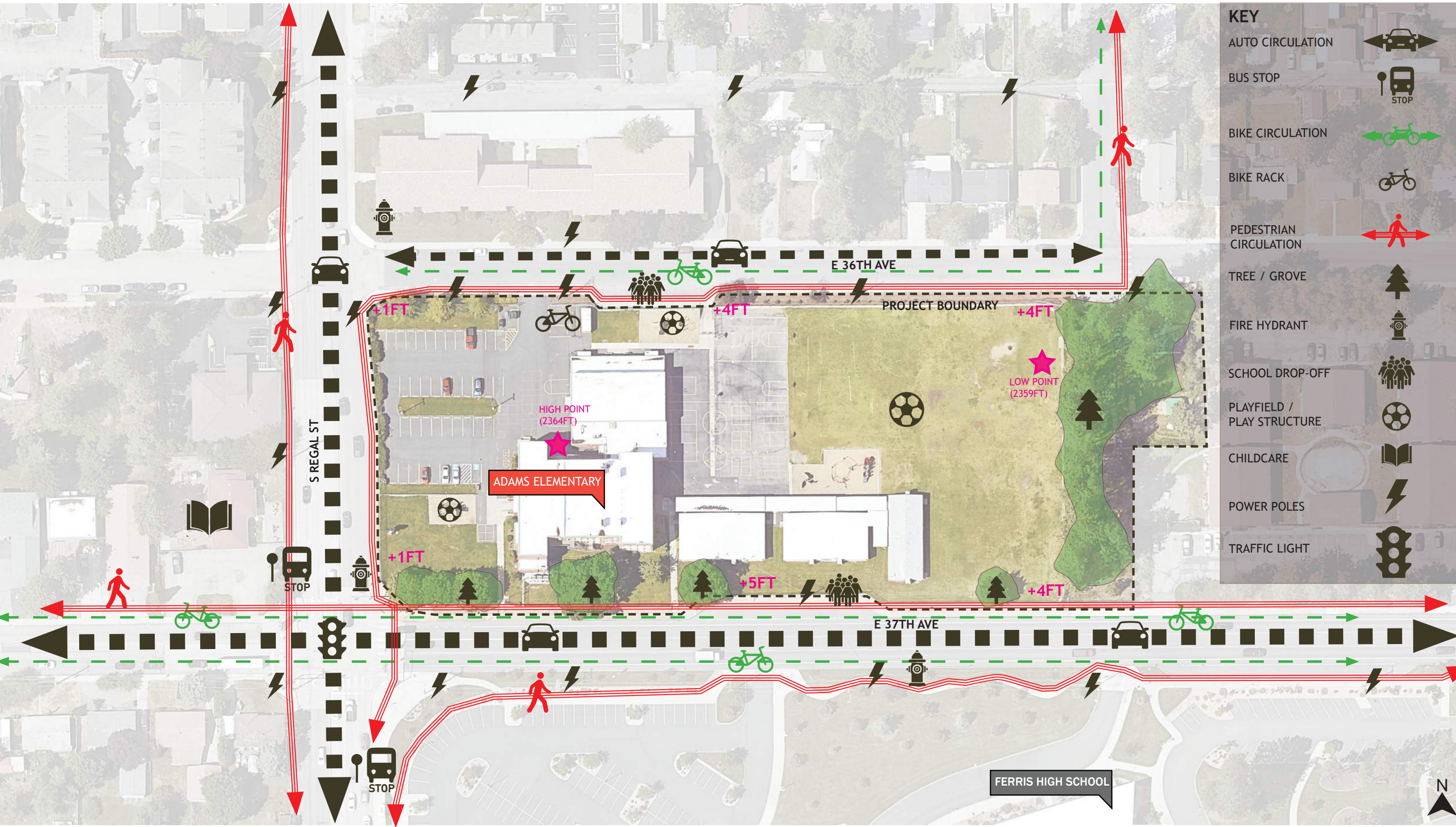
CONTEXT ANALYSIS: ADJACENT PROPERTIES & STREETSCAPES



SITE MAP - SITE + SURROUNDING PROPERTIES WITHIN 200' (NTS)



CONTEXT / SITE ANALYSIS



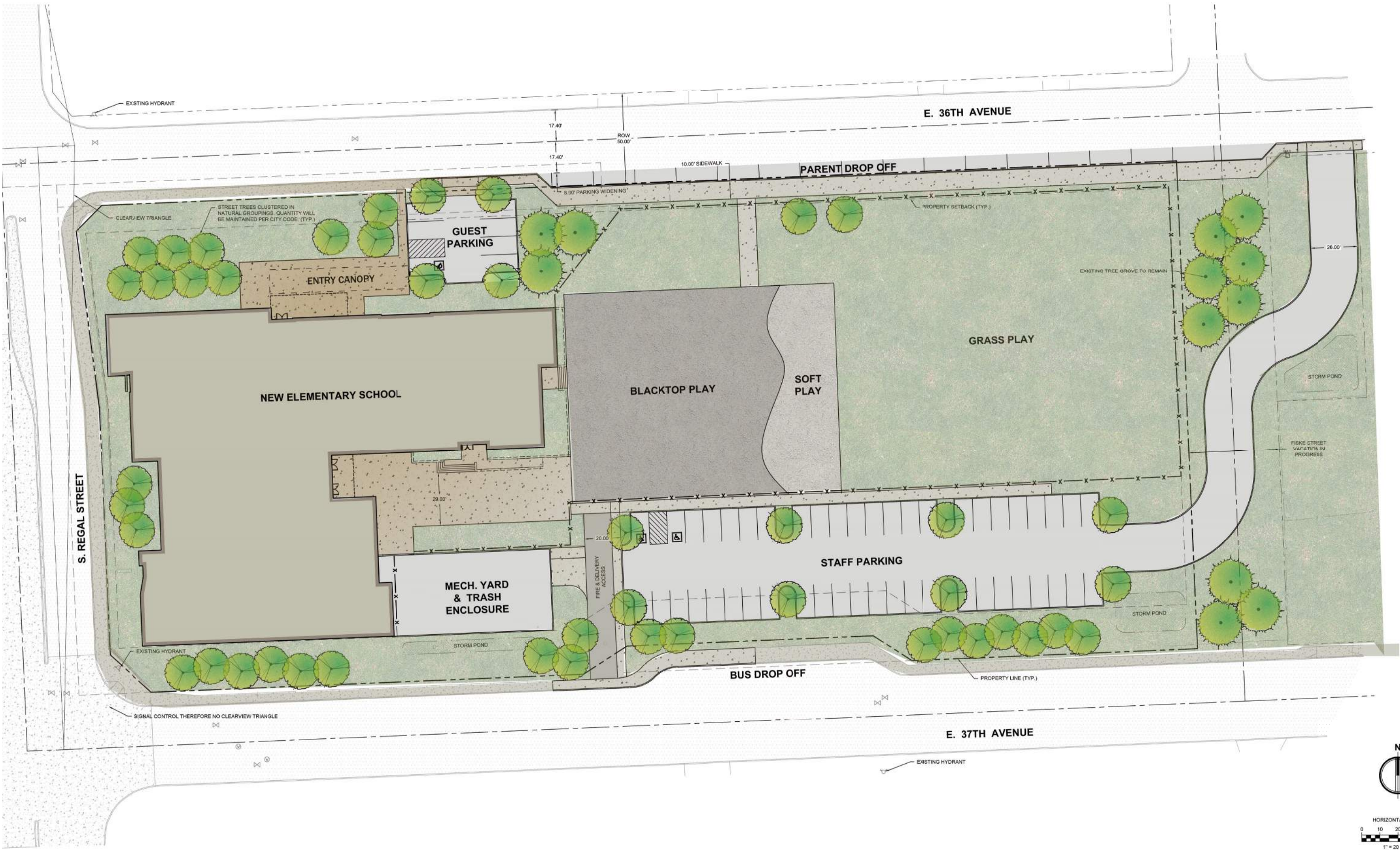
SITE MAP - EXISTING CONDITIONS (NTS)

DESIGN REVIEW BOARD SUBMITTAL  
JUNE 7, 2023

ADAMS ELEMENTARY  
SPOKANE PUBLIC SCHOOLS



CONTEXT: SITE PLAN



CONCEPT PLAN

DESIGN REVIEW BOARD SUBMITTAL  
JUNE 7, 2023

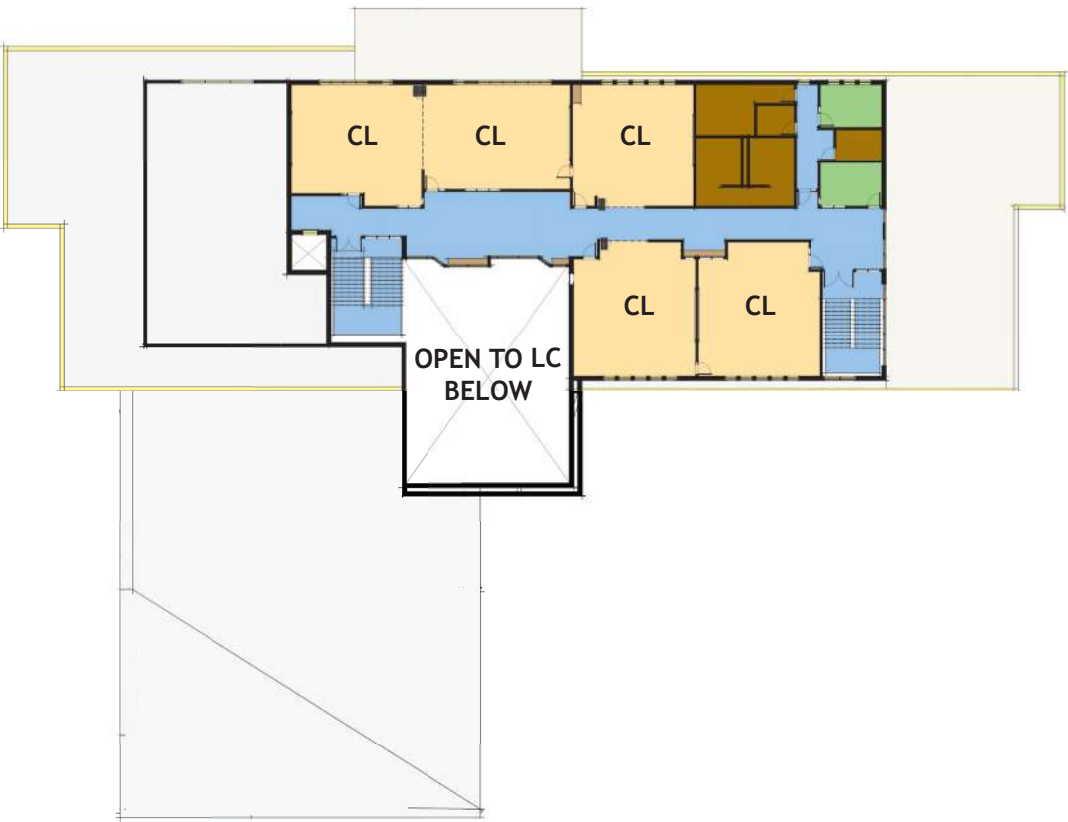
ADAMS ELEMENTARY  
SPOKANE PUBLIC SCHOOLS



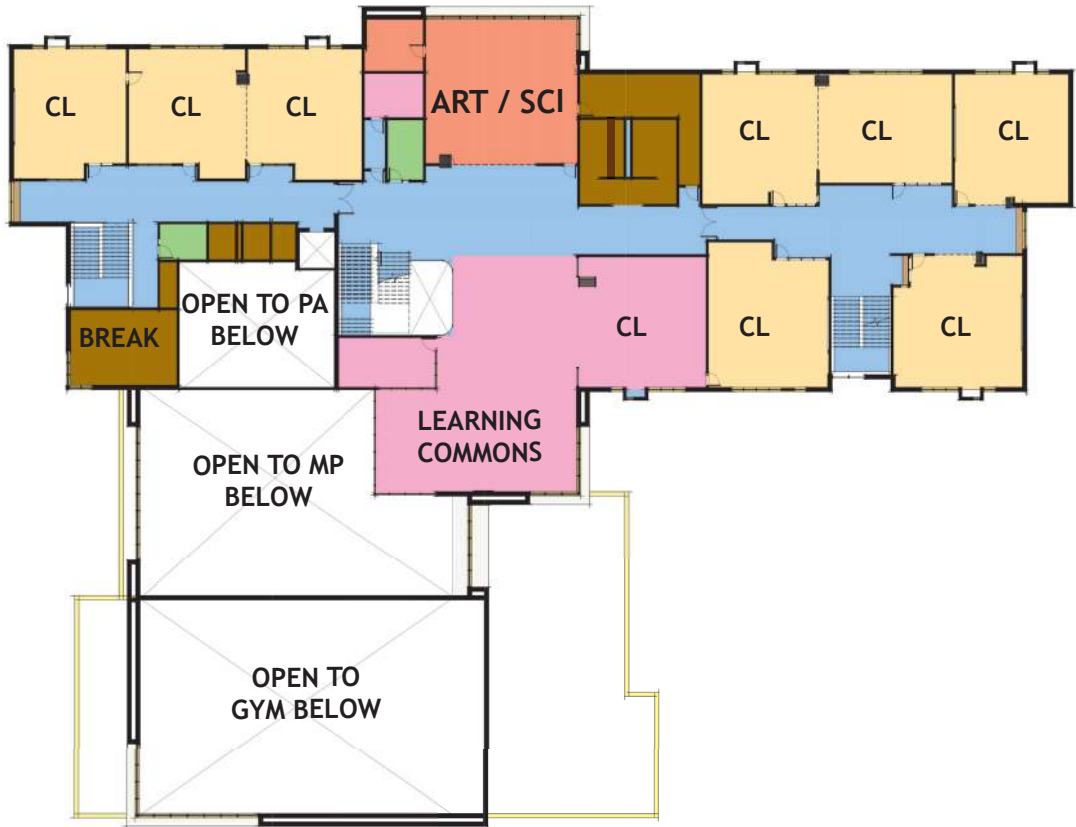


CONCEPT: FLOOR PLANS

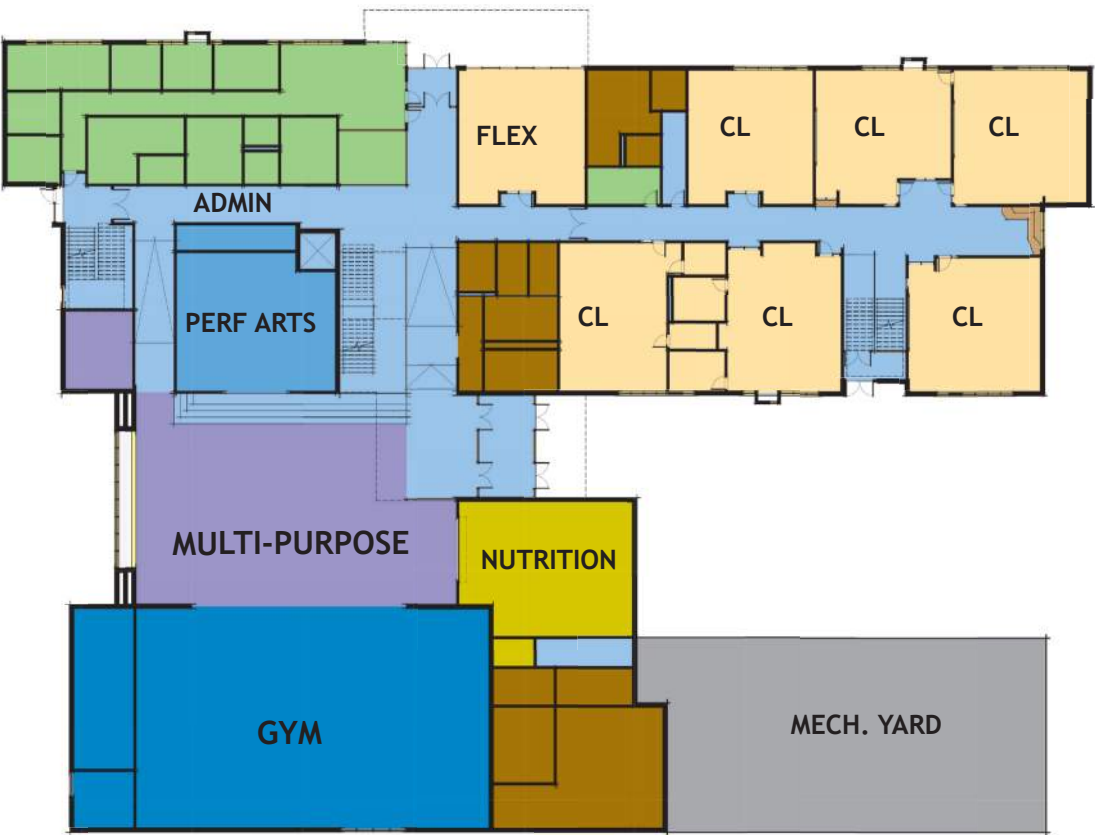
In the ongoing exploration through schematic design, the conceptual plans shown here and the following conceptual exterior renderings offer a snapshot of the continued development of the “Vertical Villages” scheme for the new Adams Elementary School.



THIRD FLOOR



SECOND FLOOR



FIRST FLOOR





VIEW FROM NORTHEAST FROM PARENT DROP-OFF





MAIN ENTRY @ CANOPY





VIEW FROM SOUTHWEST FROM 37TH & REGAL





VIEW FROM STUDENT COURTYARD



# Design Review Board - Meeting Minutes

**May 10, 2023**

**Hybrid City Hall Tribal Room/Teams**

Meeting called to order at 5:30 PM by Mark Brower

## **Attendance:**

- *Board Members Present:* Mark Brower (Chair), Chuck Horgan (Vice-Chair & Arts Commission Liaison), Chad Schmidt, Ryan Brodwater, Bob Scarfo, Drew Kleman, Grant Keller
- *Board Members Not Present:*
- *Quorum Present:* Yes
- *Staff Members Present:* Dean Gunderson, Taylor Berberich, Ryan Benzie

## **Changes to Agenda:**

- None

## **Workshop:**

### **1. Recommendation Meeting for PFD Parking Lot**

- Staff Report: Dean Gunderson
- Applicant Presentation: Ken Murphy, ALSC Architects
- Questions asked and answered
- Public comments were solicited, public comment period was closed
- Discussion ensued
- Chuck Horgan made a motion to approve the recommendations as read, Ryan Brodwater seconded. Motion passed unanimously. See Supplement A for more information.

## **Board Business:**

**Approval of Minutes:** Chad Schmidt made a motion to approve the minutes from the February 22, 2023 meeting approved; Drew Kleman seconded. Motion passed unanimously.

## **Old Business:**

- None

## **New Business:**

- None

## **Chair Report:**

- None

## **Secretary Report – Dean Gunderson**

- Dean informed the board that Joe Tovar, FAICP is presenting information on recent legislative actions to City staff on May 24th at noon.
- Dean will be giving a presentation in Charlotte on good design review and attending a joint workshop with Liberty House Plans concerning stock house plans.

## **Meeting Adjourned at 7:50 PM**

Next Design Review Board Meeting scheduled for Wednesday, May 24, 2023

## Public comments for the Adams Elementary School Design Review Collaborative Workshop

June 27, 2023

Carol Tomsic, chair of the Lincoln Heights Neighborhood Council

Sent June 25, 2023

The school district stated in their project summary design evolution (page 17) that the existing school building is familiar and beloved by many. It was also stated in the Historic Content Consideration (page 30) that the designs will have nods to the historic architecture of the building. However, I didn't see the historic nods anywhere in the packet. The proposed concept renderings of the building do not reflect any similarities to the existing school building.

I'd like to have the nods specifically defined, especially the façade. I'd like to know what historic preservation was requested in their community outreach.

### Concerning Topics for Discussion

1 - I am in favor of a bicycle/pedestrian shared use path in the proposed vacation of Fiske St, between 36th to 37th. I am sure there would have been historically used pathways if the parcel had not been completely fenced off.

Our council advocated for the Fiske St. Greenway, between 27th to 35th. An extension to 37th would be fabulous. It is our desire to have a greenway that connects the Ben Burr Trail to Thornton Murphy Park. Then, connect to the Fiske St. Greenway at 27th.

And, I appreciate the mention of the HAWK light at Fiske/29th. Our council has worked hard on safe pedestrian/bicycle crossings on 29th!

2 - A mid-block controlled crossing on 37th would also connect to a Ray-Freya shared use pathway, from 37th/Freya to 44th, through Ferris, down to the Palouse. The shared pathway was recommended in the 2021 Ray-Freya Alternative Analysis final report.

4- I walk across Ferris, from Ray St. to Regal. I would recommend more tree coverage. It is a hot walk without any tree coverage. Regal St. is a tree desert for pedestrians and bicyclists.

Municipal Code 12.02.905 states a goal that 30 percent of total land area within the City of Spokane has a healthy and functioning tree canopy by 2030. There should be street trees along all street frontages.

An alternate site plan to retain and preserve the urban tree canopy should be considered. It doesn't seem appropriate to remove trees for an auto path to staff parking. Especially since the school district is asking for a street vacation only to use the land for a 'street'.

Paul Kropp, Chair of the Community Assembly Liaison Committee

June 26, 2023

I would like to associate myself completely with Carol Tomsic's "few comments" on the Adams Elementary project.

She succinctly covers three topics:

1 The indeterminate - and perhaps contradictory (see the upper-left images on PDF pages 25-28) - character of the school building's massing and facade

2 The substantial long-run benefit of completing the Fisk St greenway concept to 37th Avenue

3 The vital importance of adding street trees when the opportunity arises, and of preserving and extending the urban tree canopy

For the record, I am a member of the Southgate Neighborhood Council.