



Spokane Design Review Board

Wednesday, December 09, 2020

5:30-8:00 PM

Teleconference

TIMES GIVEN ARE AN ESTIMATE AND ARE SUBJECT TO CHANGE

Board Briefing Session:

5:30 – 5:40	1) Call to Order	Chair
	2) Roll Call	Dean Gunderson
	3) Changes to the Agenda?	Chair
	4) Motion to Temporarily Suspend Rules	Chair

Workshop:

5:40 – 7:40	5) Latah Glen PUD – Recommendation Meeting	Taylor Berberich
	• Staff Report.....	15-20 m
	• Applicant Presentation.....	25 m
	• Public Comments and Board Q & A	25 m
	• Board Discussion and Motion(s).....	45 m

Board Business:

7:40 – 8:00	6) Approve the 11/16/2020 meeting minutes.	Chair
	7) Old Business	
	8) New Business	
	9) Chair Report	Chair
	10) Secretary Report	Dean Gunderson
	11) Other	
	12) Adjourn	

The next Design Review Board meeting is scheduled for Monday, December 14, 2020.

In order to comply with public health measures and Governor Inslee's *Stay Home, Stay Safe* order, the Design Review Board meeting will be held on-line

Members of the general public are encouraged to join the on-line meeting using the following information:

To participate via video follow the link on your computer (click on "Join meeting")

[Join meeting](#)

To participate by phone

Call: 1 (408) 418-9388

Enter: **146 780 9933** followed by # when prompted for a meeting number or access code. Enter # when prompted for an attendee ID

While the meeting begins at 5:30pm, you can join as early as 5:15pm on the date of the meeting.

Please note that public comments cannot be taken during the meeting, but the public is encouraged to continue to submit their comments or questions in writing to:

Dean Gunderson, Sr. Urban Designer
dgunderson@spokanecity.org

The audio proceedings of the Design Review Board meeting will be recorded, with digital copies made available upon request.

Meeting Process - Spokane Design Review Board

Call to Order

- Chair calls the meeting to order, noting the date and time of the meeting.
- Chair asks for roll call for attendance.
- Chair asks if there any changes to the agenda.
- Chair asks for motion to temporarily suspend the rules (see Agenda packet)

Board Workshop

- Chair announces the first project to be reviewed and notes the following: a) the Board will consider the design of the proposal as viewed from the surrounding public realm; b) the Board does not consider traffic impacts in the surrounding area or make recommendations on the appropriateness of a proposed land use; c) the Board will not consider un-permitted, possible surrounding development(s) except those which are contemplated under the Comprehensive Plan and Development Code; c) it is the applicant's responsibility to meet all applicable Code requirements regardless of what might be presented or discussed during workshops.
- Chair asks for a staff report.

Staff Report

- Staff report on the item, giving findings of fact. Presentation will be kept to 5-10 minutes.

Applicant Presentation

- Chair invites the applicant(s) to introduce the project team and make a 10-15 minute presentation on the project.

Public Comment *

** During the Stay Home, Stay Safe order, public comments are being accepted in writing.*

DRB Clarification

- Chair may request clarification on comments.

Design Review Board Discussion

- Chair will ask the applicants whether they wish to respond to any written public comments, after their response (if any) they are to return to their seats in the audience.
- The Chair will formally close public comments (unless motioned otherwise).
- Chair leads discussion amongst the DRB members regarding the staff topics for discussion, applicable design criteria, identification of key issues, and any proposed design departures.

Design Review Board Motions

- Chair asks whether the DRB is ready to make a motion.
- Upon hearing a motion, Chair asks for a second. Staff will record the motion in writing.
- Chair asks for discussion on the motion.
- Chair asks the applicant if they would like to respond to the motion.
- After discussion, Chair asks for a vote.

Design Review Board Follow-up

- Applicant is advised that they may stay or leave the meeting, and that the annotated & signed motion will be made available within five working days.
- Next agenda item announced.

Board Business

- Meeting Minutes - Chair asks for comments on the minutes of the last meeting; Asks for a motion to approve the minutes.
- Chair asks is there any old business? Any old business is discussed.
- Chair asks is there any new business? Any new business is discussed.
- Chair Report – Chair gives a report.
- Secretary Report – Sr. Urban Designer gives a report.

Other

- Chair asks board members if there is anything else.

Adjourn

- Chair asks for a motion to adjourn. After the motion is seconded, and approved by vote, Chair announces that the meeting is adjourned, noting the time of the adjournment.

DESIGN REVIEW BOARD
Latah Glen PUD
2 - RECOMMENDATION MEETING

FILE NO.DRB
2022_2014

Design Review Staff Report

December 4, 2020



Staff:
Dean Gunderson, Senior Urban Designer

Taylor Berberich, Urban Designer

Neighborhood & Planning Services
808 W. Spokane Falls Blvd.
Spokane, WA 99201

Applicants:

William Nascimento
Laguna Canyon Group, LLC

William Sinclair,
Storhaug Engineering

Background

The Design Review Board Collaborative Workshop was held on August 12, 2020.

The following materials are supplemental to this report:

- *Design Review Staff Report | Program Review/Collaborative Workshop, August 7, 2020*
- *Design Review Board | Collaborative Workshop Advisory Actions, August 12, 2020;*

Responses to Advisory Actions

During the workshop, the applicant is encouraged to please describe changes to the design since the Collaborative Workshop including any changes made in response to the advisory actions offered by the Design Review Board on August 12, 2020 as follows (Applicant's comments are provided in *italicized blue*, additional staff comments are in **bold blue**):

- 1. To promote connectivity and offer a neighborhood asset, the Applicant is encouraged to provide an intentional non-motorized connection from the site to the Fish Lake Trail.**

A proposed intentional trail alignment connecting the on-site pedestrian system of walks to Fish Lake Trail has been added to the attached Site Plans. A perimeter fence with a pedestrian gate and controlled access for this trail connection is also shown and noted.

Staff Comment: See Additional Topic for Discussion #3.

- 2. The Applicant is encouraged to evaluate the internal sidewalks and pathways and consider opportunities to elevate the pedestrian user experience by introducing benches, nodes, enhanced landscaping, or other means. A network of sidewalks and pathways connecting residents to common buildings, common spaces, and public ways may enhance the overall site design experience.**

The internal sidewalks and pathways have been evaluated for opportunities to elevate the pedestrian user experience. The results of this evaluation are shown on the attached 'Site Plan- Design Character' sheet. Locations for pedestrian wayfinding/project identity, seating nodes, with plantings, and recreational nodes are conceptually identified throughout the pedestrian system with referenced character images.

A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.

- 3. The Applicant is encouraged to return with a more fully developed plan illustrating intended innovation in stormwater treatment.**

The stormwater pond sizes and locations shown are based on Concept Stormwater Plan that has been prepared by our Civil Engineer informed by the completed preliminary Geotechnical Report- see attached Exhibit 'A'

Conceptual bioretention stormwater swales have been indicated on the attached 'Site Plan-Design Character' sheet. Character images of bioretention-style plantings instead of the ubiquitous '208' turf swale are included. Final planting design of bioretention swales will be in accord with the Eastern Washington Low Impact Development Guidance Manual.

Detailed Stormwater and Landscape Plans will be prepared for City approval, as required, for permitting and construction.

Staff Comment: See Additional Topic for Discussion #1.

- 4. The Applicant shall return with a proposed street tree palette.**

A tree palette with street trees included has been added and concept tree types and locations shown on the attached 'Site Plan-Design Character' sheet.

A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.

Staff Comment: See Additional Topic for Discussion #2.

- 5. The Applicant shall restore the landscape in the areas of the site beyond the lease areas in a manner consistent with the existing and preserved natural areas on site.**

A native dryland hydroseed is indicated and planned on the existing unvegetated slope area with Ponderosa Pines planted as feasible on the attached 'Site Plan - Design Character' sheet.

The Site Plan has been updated to include a note that the existing natural landscape areas to remain within 20' of the site boundary will be augmented, as needed, to achieve the design intent of the required L1 landscape buffer.

A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.

Staff Comment: See Additional Topics for Discussion #1 and #2.

- 6. The Applicant is encouraged to explore ways to massage the architectural aesthetic of the proposed structures into a cohesive theme that reflects and enhances the regional character of the area. The Board strongly recommends that a set of design standards for the development be crafted in order to maintain**

consistency with the established style as the project is built out, and to preserve the presumption of privacy between homes.

The architectural theme has been revised to “Regional Northwest” as depicted for the clubhouse - see sheet 4. Design guidelines, under development, will require consistency of homes with this style of architecture.

Design guidelines under development will include the requirement for privacy between the homes to be achieved by landscape buffer and or fencing as needed if it can't be achieved by appropriate window offsets.

- 7. The Applicant shall return with a developed entry design, gate design, landscaping and signage, along with the design for any proposed fencing or enclosure of the site.**

Entry Plan- The entry design area has been revised to include a landscaped keypad island with a three-car queue to it within the property, a large vehicle turnaround before the gate and greater detail of the entry/exit lanes that taper to the typical road section - please see Concept Entry Gate Layout With Vehicle Turnaround enlargement, see sheet 5.

Gate - The vehicle gate character image, which depicts single-swing gates attached to masonry columns with a column and planter between the in and out lanes is shown on sheet 5- the masonry columns depicted are now noted to compliment the project clubhouse as part of cohesive Northwest theme throughout the park.

Fencing - a 6' chain link fence has been added and is indicated within the L1 buffer area around the project perimeter on the Site Plan. The chain link fence is anticipated to be visually obscured from outside the project by the required continuous 6' tall plantings of the L1 buffer. A transition from chain link to the ornamental fence, i.e. more decorative tube steel, aluminum, or wrought iron fence, is indicated on the revised entry gate plan, where fencing will be highly visible and a pedestrian gate with controlled access has been indicated. The ornamental fence is noted to compliment the regional Northwest architectural style- see sheet 5, 'Concept Cohesive Entry/Site Character.'

Signage - The project signage character is shown on the attached Site Plan. The masonry monument sign depicted is noted to compliment the clubhouse architectural, color, material, and texture palette during the final design. The entry gate/drive design indicated encroachment into a small portion of the L1 Landscape Buffer around the property.

Detailed Civil Engineering and Landscape Plans will be prepared for City approval, as required for permitting and construction.

Staff Comment: See Additional Topics for Discussion #1 and #3.

- 8. The Board appreciates the introduction of additional affordable housing to the Spokane area.**

Noted and very much appreciated.

- 9. The Board finds the reclamation and renovation of the existing auto wrecking yard to be an innovative reuse of the land.**

Noted and very much appreciated.

We look forward to getting our Recommendation Meeting with the DRB scheduled as soon as possible.

Additional suggested topics for discussion by staff based on the November 11, 2020 submittal:

Innovative:

1. As indicated on the Applicant's site plan, there are three bioretention swales that fall within the L1 landscape buffer. Does the board have a recommendation regarding selection of plant species that will thrive in a damp/swamp environment which may not receive supplemental irrigation?

Aesthetic:

2. According to page 3 of the Applicant's Site Design PDF (*Site Plan- Design Character) street trees along the western-most north/south running street (Private Road-3) are only shown on the downhill/eastern slope. Does the uphill/western slope provide an opportunity for street trees, and does the board have a recommendation to provide?
3. Is there an opportunity to match the aesthetics of the westernmost emergency vehicle access gate and pedestrian gate leading to Fish Lake Trail to the aesthetics of the entry gate, in order to provide a cohesive site character?
4. As the Applicant's Site Plan is unclear if power and telecommunications is intended to be provided through overhead utility lines, does the board have a recommendation for reducing the visual impact of such overhead lines?

Energy Efficiency:

5. In previous PUDs, street and pedestrian site lighting utilized photovoltaics to offset energy use. Does the board have a recommendation regarding the use of such energy efficient of lighting to meet the PUD design review decision criteria?

Note

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

Policy Basis

Spokane Municipal Codes
City of Spokane Comprehensive Plan

November 4, 2020

Dean Gunderson
City of Spokane
808 W. Spokane Falls Blvd.
Spokane, WA 99201

RE: Latah Glen Residential Community – DRB Recommendation Meeting Submittal

Mr. Gunderson,

Along with this Written Project Summary, we have attached our revised Site Design (Exhibit ‘A’), Concept Utility and Roads (Exhibit ‘B’), Building Design (Exhibit ‘C’), and Design Details (Exhibit ‘D’).

Concept Utility and Road Plans are included to provide the conceptual stormwater design and concept plan for grading the property showing roadway profiles prepared using available County of Spokane topographic information. The final grading plan will be prepared, for City approval, following our conceptual plan using aerial topography recently flown for final design for permitting and construction.

The Site Design documents now include updates from the DRB that are detailed below. Lighting has been added to the typical lease space detail and concept plant information has also been added.

Building Design information includes a to-scale clubhouse plan view and elevations, the laundry facility has been removed from the plan – these are also included for review within the Site Design documents that also features regional Northwest design character images. The clubhouse design will inform the design character of the homes for material, color, and texture palette. There is limited design control over the mass-produced manufactured homes by limited number of manufacturers, but we are working on design guidelines that will be applied to the manufactured units, such as a 2-car garage requirement and minimum roof pitch.

The Design Details are a conceptual representation of the intended elements of cohesive design that will complement the clubhouse architecture, color, and texture palette. These sketches indicate entry gate and project signage conceptually with pedestrian-scale lighting that, together with standardized pedestrian wayfinding and seating elements, is intended to set a cohesive design theme throughout the development at each unit. The final design of these elements may evolve.

On 08.12.2020, we met with the Design Review Board for the Collaborative Workshop and received the following written Advisory Actions ([responses](#) added):

Advisory Actions

1. To promote connectivity and offer a neighborhood asset, the Applicant is encouraged to provide an intentional non-motorized connection from the site to the Fish Lake Trail.
 - a. A proposed intentional trail alignment connecting the on-site pedestrian system of walks to Fish Lake Trail has been added to the attached Site Plans. A perimeter fence with a pedestrian gate and controlled access for this trail connection is also shown and noted.

2. The Applicant is encouraged to evaluate the internal sidewalks and pathways and consider opportunities to elevate the pedestrian user experience by introducing benches, nodes, enhanced landscaping, or other means. A network of sidewalks and pathways connecting residents to common buildings, common spaces, and public ways may enhance the overall site design experience.
 - a. The internal sidewalks and pathways have been evaluated for opportunities to elevate the pedestrian user experience. The results of this evaluation are shown on the attached 'Site Plan – Design Character' sheet. Locations for pedestrian wayfinding/project identity, seating nodes with plantings, and recreational nodes are conceptually identified throughout the pedestrian system with referenced character images.

 - b. A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.

3. The Applicant is encouraged to return with a more fully developed plan illustrating intended innovation in stormwater treatment.
 - a. The stormwater pond sizes and locations shown are based on Concept Stormwater Plan that has been prepared by our Civil Engineer informed by the completed preliminary Geotechnical Report – see attached Exhibit 'A'

 - b. Conceptual bioretention stormwater swales have been indicated on the attached 'Site Plan – Design Character' sheet. Character images of bioretention-style plantings instead of the ubiquitous '208' turf swale are included. Final planting design of bioretention swales will be in accord with the Eastern Washington Low Impact Development Guidance Manual.

 - c. Detailed Stormwater and Landscape Plans will be prepared for City approval, as required, for permitting and construction.

4. The Applicant shall return with a proposed street tree palette.
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 - b. A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.

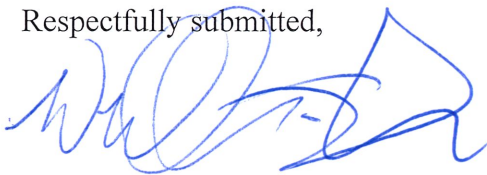
5. The Applicant shall restore the landscape in the areas of the site beyond the lease areas in a manner consistent with the existing and preserved natural areas on site.
 - a. A native dryland hydroseed is indicated and planned on the existing unvegetated slope area with Ponderosa Pines planted as feasible on the attached 'Site Plan – Design Character' sheet.
 - b. The Site Plan has been updated to include a note that the existing natural landscape areas to remain within 20' of the site boundary will be augmented, as needed, to achieve the design intent of the required L1 landscape buffer.
 - c. A detailed Landscape Plan will be prepared for City approval, as required, for permitting and construction.
6. The Applicant is encouraged to explore ways to massage the architectural aesthetic of the proposed structures into a cohesive theme that reflects and enhances the regional character of the area. The Board strongly recommends that a set of design standards for the development be crafted in order to maintain consistency with the established style as the project is built out, and to preserve the presumption of privacy between homes.
 - a. The architectural theme has been revised to regional Northwest as depicted for the clubhouse– see sheet 4. Design guidelines, under development, will require consistency of homes with the style of architecture.
 - b. Design guidelines under development will include the requirement for privacy between the homes to be achieved by landscape buffer and or fencing as needed if it can't be achieved by appropriate window offsets.
7. The Applicant shall return with a developed entry design, gate design, landscaping and signage, along with the design for any proposed fencing or enclosure of the site.
 - a. Entry Plan – The entry design area has been revised to include a landscaped keypad island with a three-car queue to it within the property, a large vehicle turnaround before the gate and greater detail of the entry/exit lanes that taper to the typical road section – please see Concept Entry Gate Layout With Vehicle Turnaround enlargement, sheet 5.
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continuous 6' tall plantings of the L1 buffer. A transition from chain link to ornamental fence, i.e. more decorative tube steel, aluminum, or wrought iron fence, is indicated on the revised entry gate plan, where fencing will be highly visible and a pedestrian gate with controlled access has been indicated. The ornamental fence is noted to compliment the regional Northwest architectural style – see sheet 5, 'Concept Cohesive Entry/Site Character'.

- d. Signage – The project signage character is shown on the attached Site Plan. The masonry monument sign depicted is noted to compliment the clubhouse architectural, color, material, and texture palette during final design. The entry gate/drive design indicates encroachment into a small portion of the L1 Landscape Buffer around the property.
 - e. Detailed Civil Engineering and Landscape Plans will be prepared for City approval, as required for permitting and construction.
8. The Board appreciates the introduction of additional affordable housing to the Spokane area.
- a. Noted and very much appreciated.
9. The Board finds the reclamation and renovation of the existing auto wrecking yard to be an innovative reuse of the land.
- a. Noted and very much appreciated.

We look forward to getting our Recommendation Meeting with the DRB scheduled as soon as possible.

Respectfully submitted,



William Sinclair, PLA, MURP

CC: William Nascimento, File

CONTEXT/SITE ANALYSIS LATAH GLEN RESIDENTIAL COMMUNITY

A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON

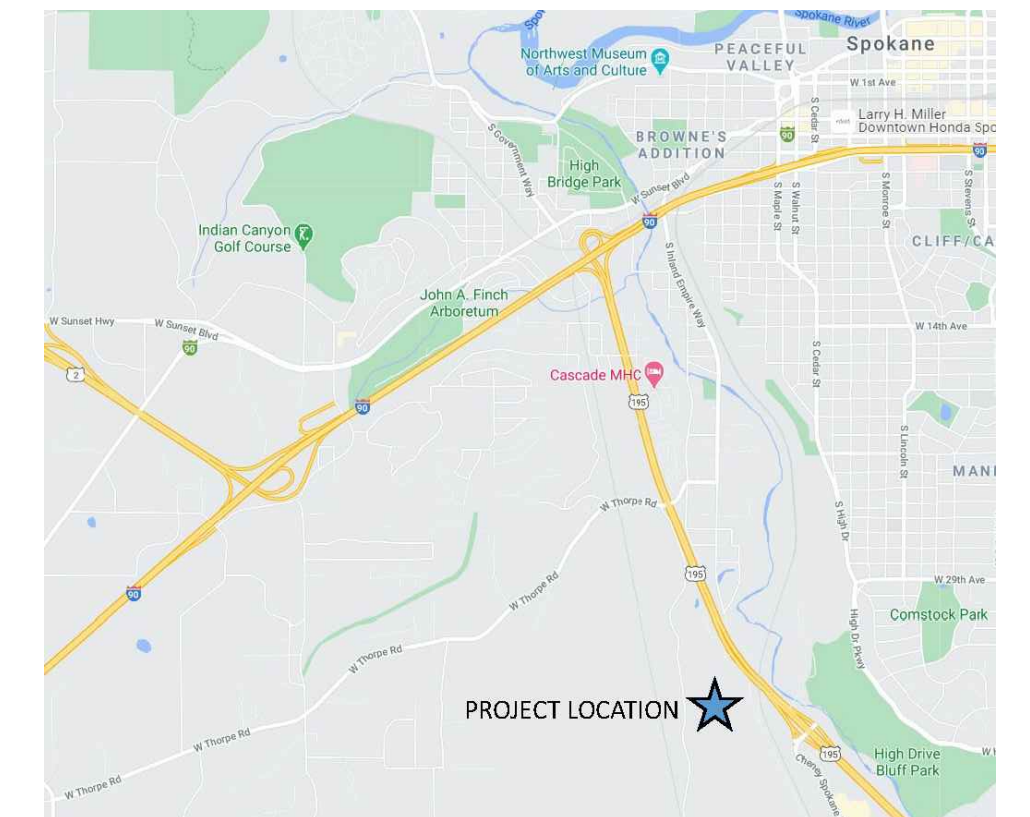


NOTES:

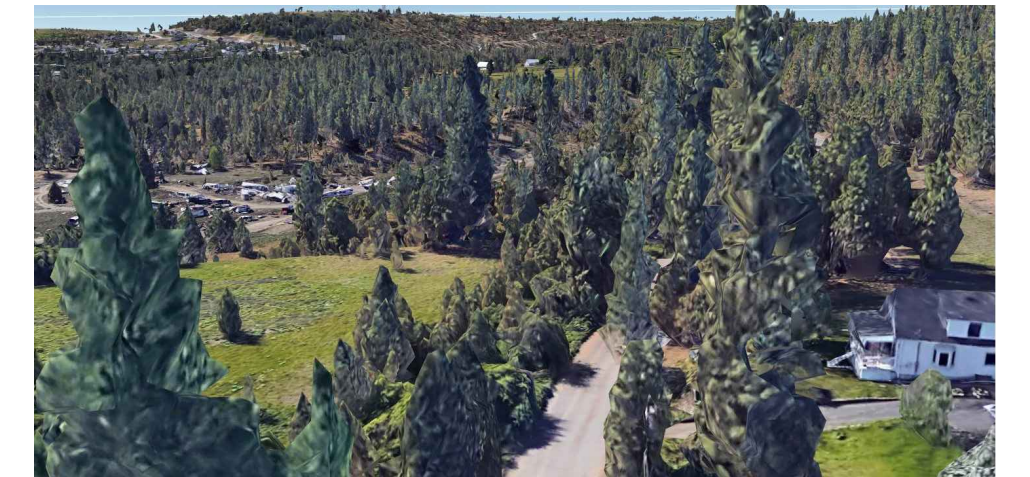
1. TOPOGRAPHICAL AND BOUNDARY INFORMATION SHOWN WAS OBTAINED FROM SPOKANE COUNTY PUBLIC INFORMATION AND DOES NOT REPRESENT ACTUAL SURVEY INFORMATION.
2. TOPOGRAPHICAL AND BOUNDARY INFORMATION SHOWN IS FOR CONCEPTUAL SITE PLANNING AND REVIEW ONLY.

LEGEND

- PROPOSED MANUFACTURED HOME PARK/PUD BOUNDARY
- EXISTING PROPERTY LINE
- MAJOR CONTOUR (10' C.I.)
- MINOR CONTOUR (2' C.I.)
- SLOPE DIRECTION AND GRADIENT
- EXISTING GRAVEL ROAD
- EXISTING ASPHALT ROAD
- VIEW DETAIL CALLOUT
- VIEW DETAIL NO.
- VIEW DIRECTION/ARC



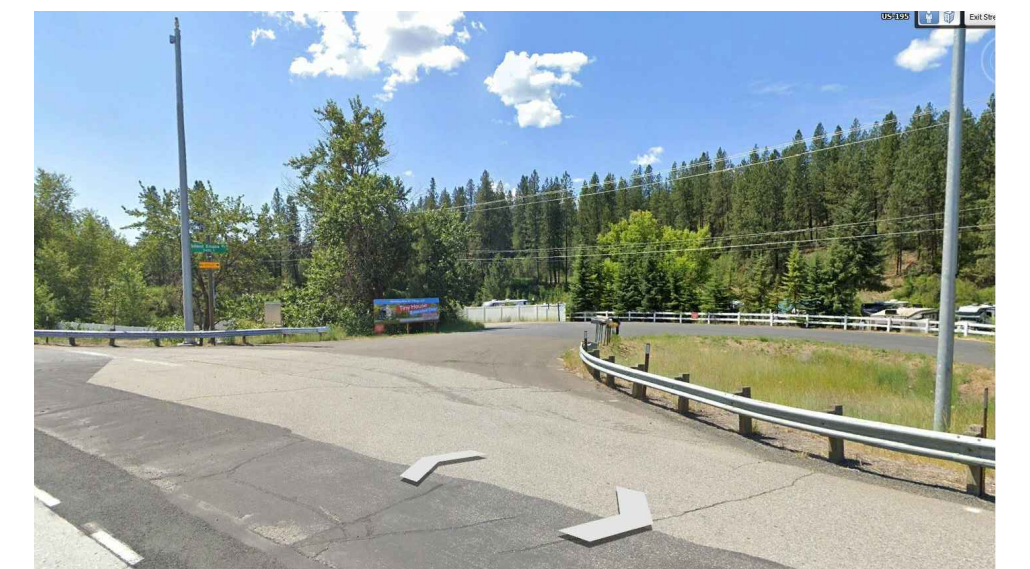
SOUTHWEST SITE FRONTAGE FROM MARSHALL ROAD (A)



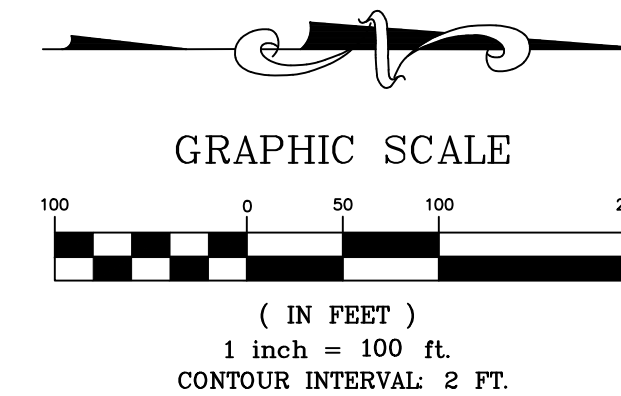
NORTHWEST SITE FRONTAGE FROM MARSHALL ROAD (B)



SITE FRONTAGE FROM SR 195 AT PRIMARY ACCESS (C)



SR 195 AT INLAND EMPIRE WAY (D)



storhäug
civil engineering planning
landscape architecture surveying
510 east third avenue | spokane, wa | 99202
p 509.242.1000 | f 509.242.1001

DATE	11/03/2020	SCALE	1" = 100'
FIELD BOOK	N/A	DRAWN	WMS
PROJECT NUMBER	19-342	DRAWING NO.	1 OF 5

SITE PLAN LATAH GLEN RESIDENTIAL COMMUNITY

A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON

DEVELOPER
SYCAMORE GROUP, LLC
10 SYCAMORE CANYON DR
DOVE CANYON, CALIFORNIA 92679
(949) 357-9015

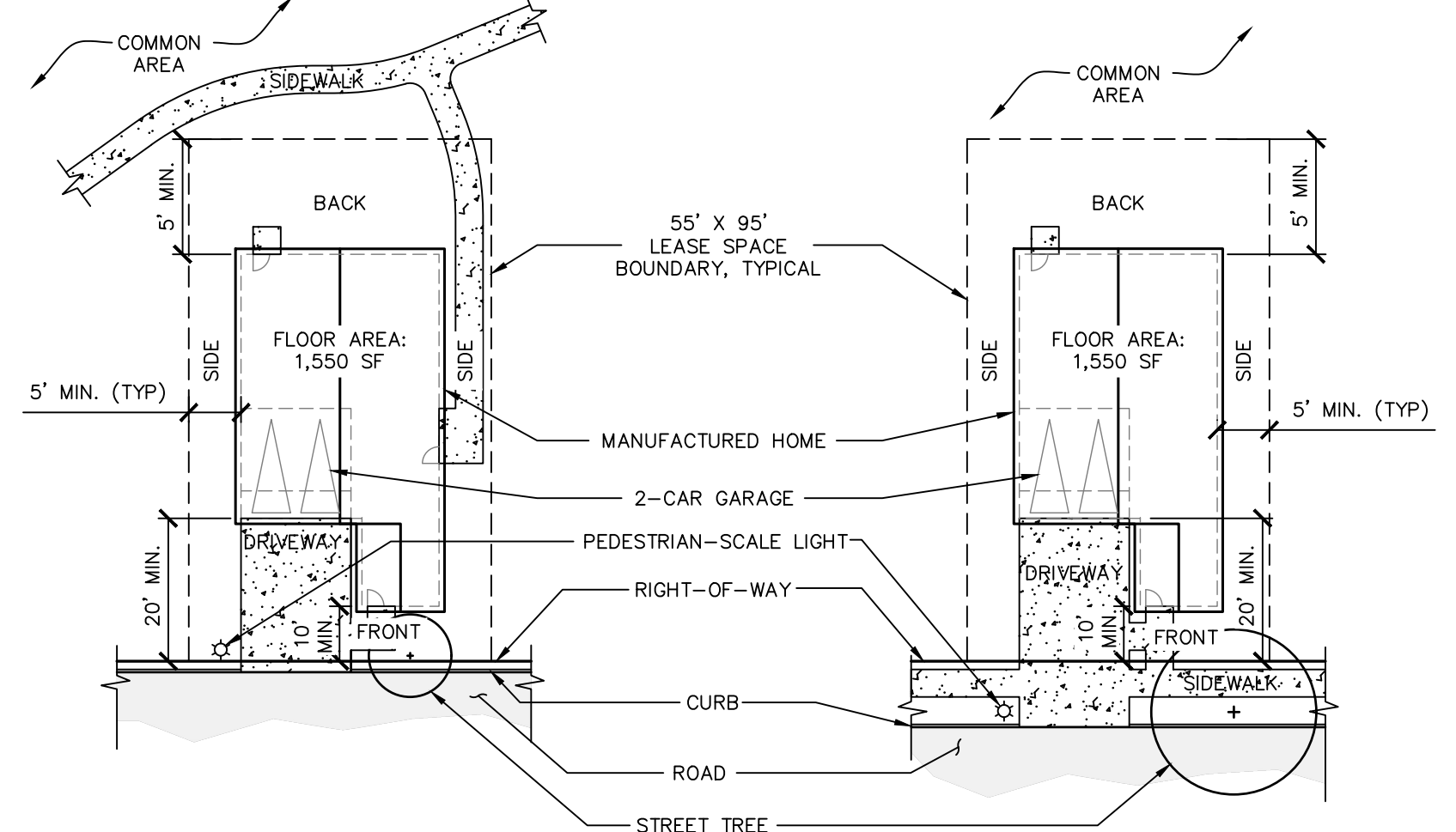
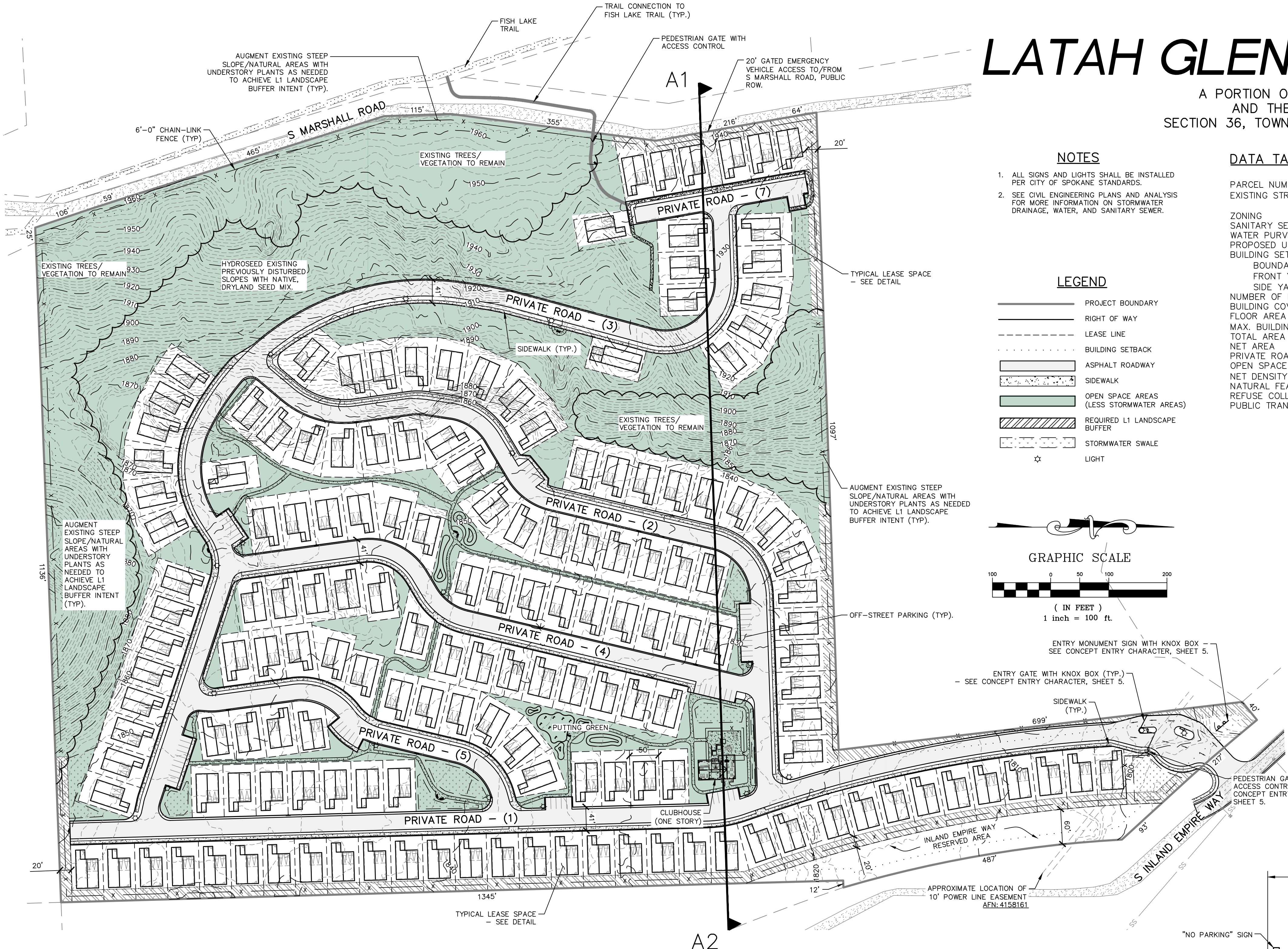
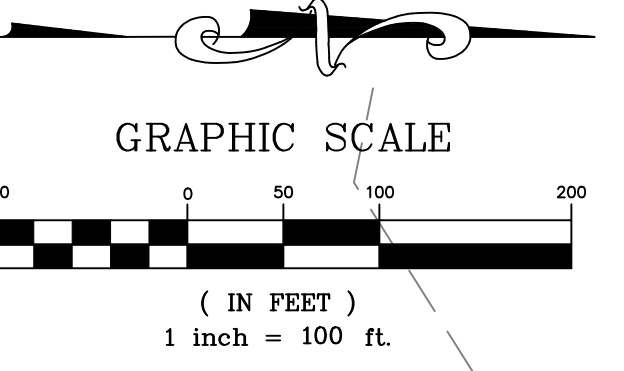
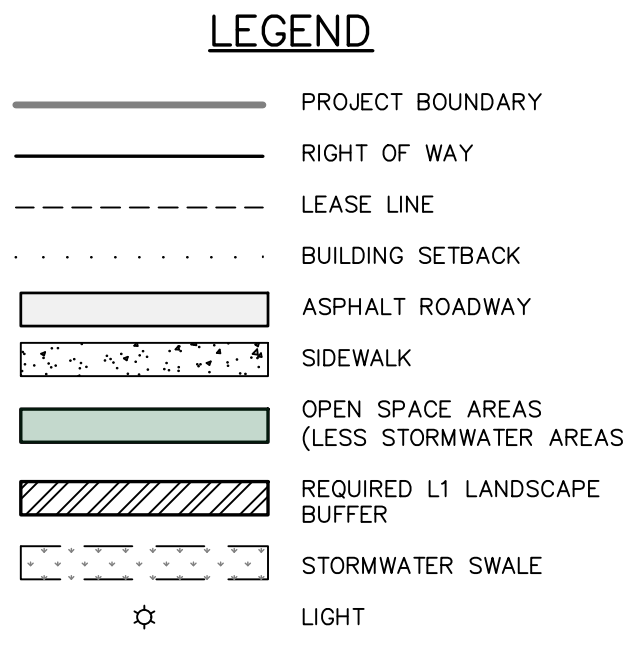
OWNER
ESTATE OF BETTIE SIMMONS
3504 S INLAND EMPIRE WAY
SPOKANE, WASHINGTON 99224

AGENT
STORHÄUG ENGINEERING
510 EAST THIRD AVENUE
SPOKANE, WASHINGTON 99202
(509) 242-1000

- NOTES**
- ALL SIGNS AND LIGHTS SHALL BE INSTALLED PER CITY OF SPOKANE STANDARDS.
 - SEE CIVIL ENGINEERING PLANS AND ANALYSIS FOR MORE INFORMATION ON STORMWATER DRAINAGE, WATER, AND SANITARY SEWER.

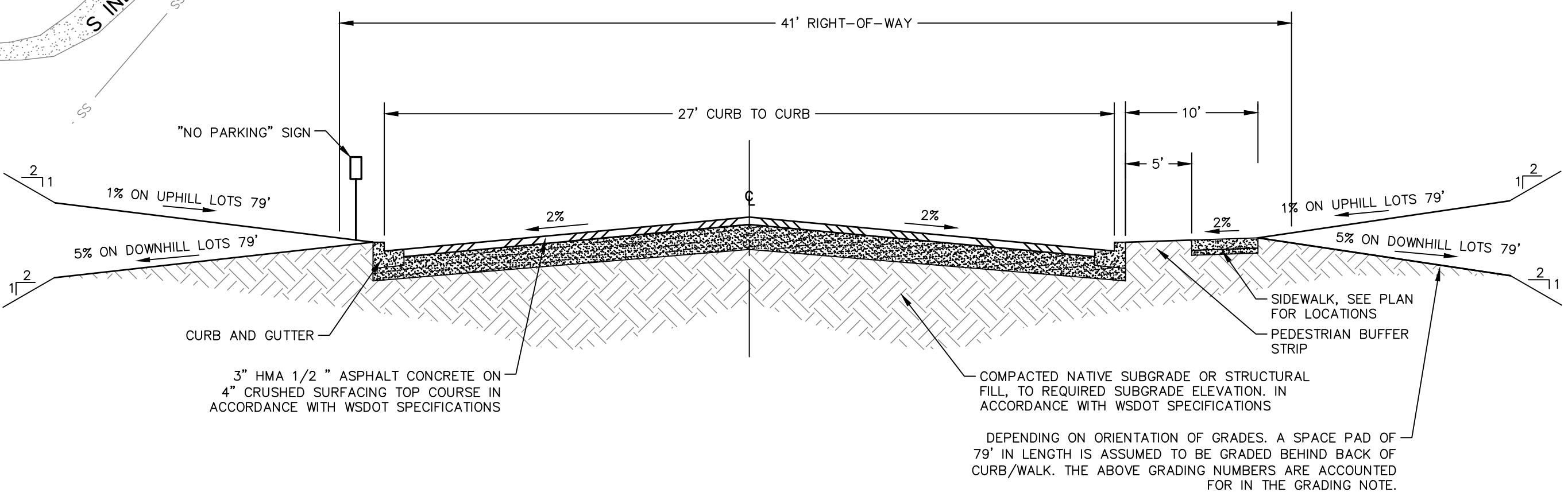
DATA TABLE

PARCEL NUMBERS	25364.0001 & 25361.0004
EXISTING STRUCTURES/USES	POLE BUILDING WITH ATTACHED MOBILE HOME TO BE REMOVED/AUTO SALVAGE YARD RESIDENTIAL SINGLE FAMILY
ZONING	CITY OF SPOKANE CITY OF SPOKANE MANUFACTURED HOME PARK
SANITARY SEWER	
WATER PURVEYOR	
PROPOSED USE	MANUFACTURED HOME PARK
BUILDING SETBACKS:	
BOUNDARY OF PARK SETBACK	20' MINIMUM
FRONT YARD SETBACK	10' MINIMUM
SIDE YARD SETBACK	10' MINIMUM FROM ANY OTHER HOME
NUMBER OF PROPOSED UNITS	157 ONE-STORY MANUFACTURED HOMES
BUILDING COVERAGE	313,020 SF
FLOOR AREA RATIO	.13:1
MAX. BUILDING HEIGHT	35'
TOTAL AREA	42.03 ACRES
NET AREA	34.99 ACRES
PRIVATE ROAD AREA	7.04 ACRES
OPEN SPACE & LANDSCAPED AREAS	14.92 ACRES OR 36% OF SITE
NET DENSITY	4.49 UNITS/ACRE
NATURAL FEATURES	STEEP SLOPES WITH TREES AND OTHER VEGETATION
REFUSE COLLECTION	INDIVIDUAL CONTAINERS WITH CURBSIDE PICK-UP
PUBLIC TRANSIT	NOT AVAILABLE

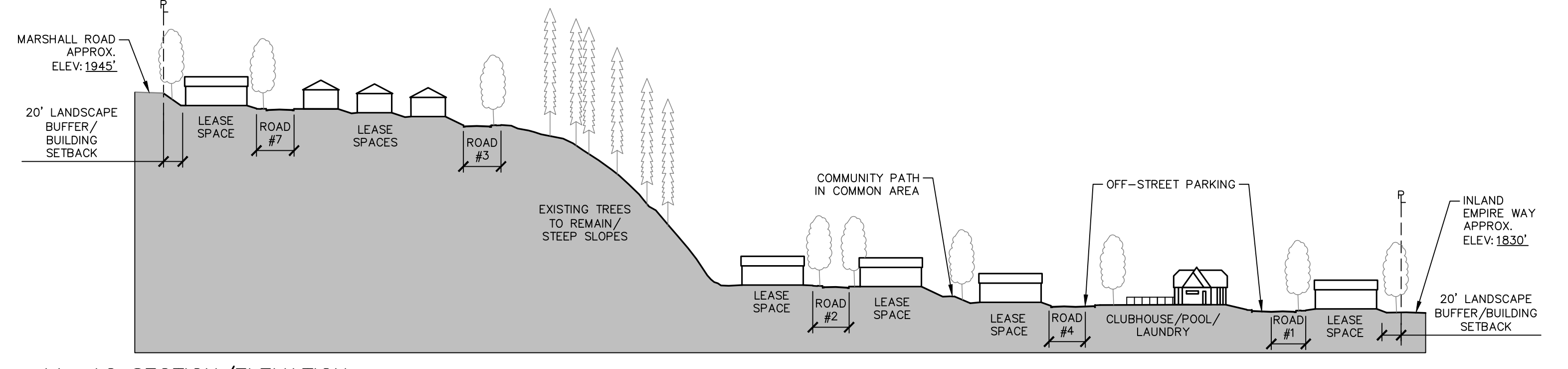


TYPICAL LEASE SPACE
SIDEWALK IN COMMON AREA
NTS

TYPICAL LEASE SPACE
SIDEWALK AT ROAD
NTS



TYPICAL ROAD SECTION
NTS



A1 - A2 SECTION/ELEVATION

HORIZONTAL SCALE: 1" = 100'
VERTICAL SCALE: 1" = 50'

storhäug
civil engineering planning
landscape architecture surveying
510 east third avenue | spokane, wa | 99202
p 509.242.1000 | f 509.242.1001

DATE	11/03/2020	SCALE	1" = 100'
FIELD BOOK	N/A	DRAWN	WMS
PROJECT NUMBER	19-342	DRAWING NO.	2 OF 6

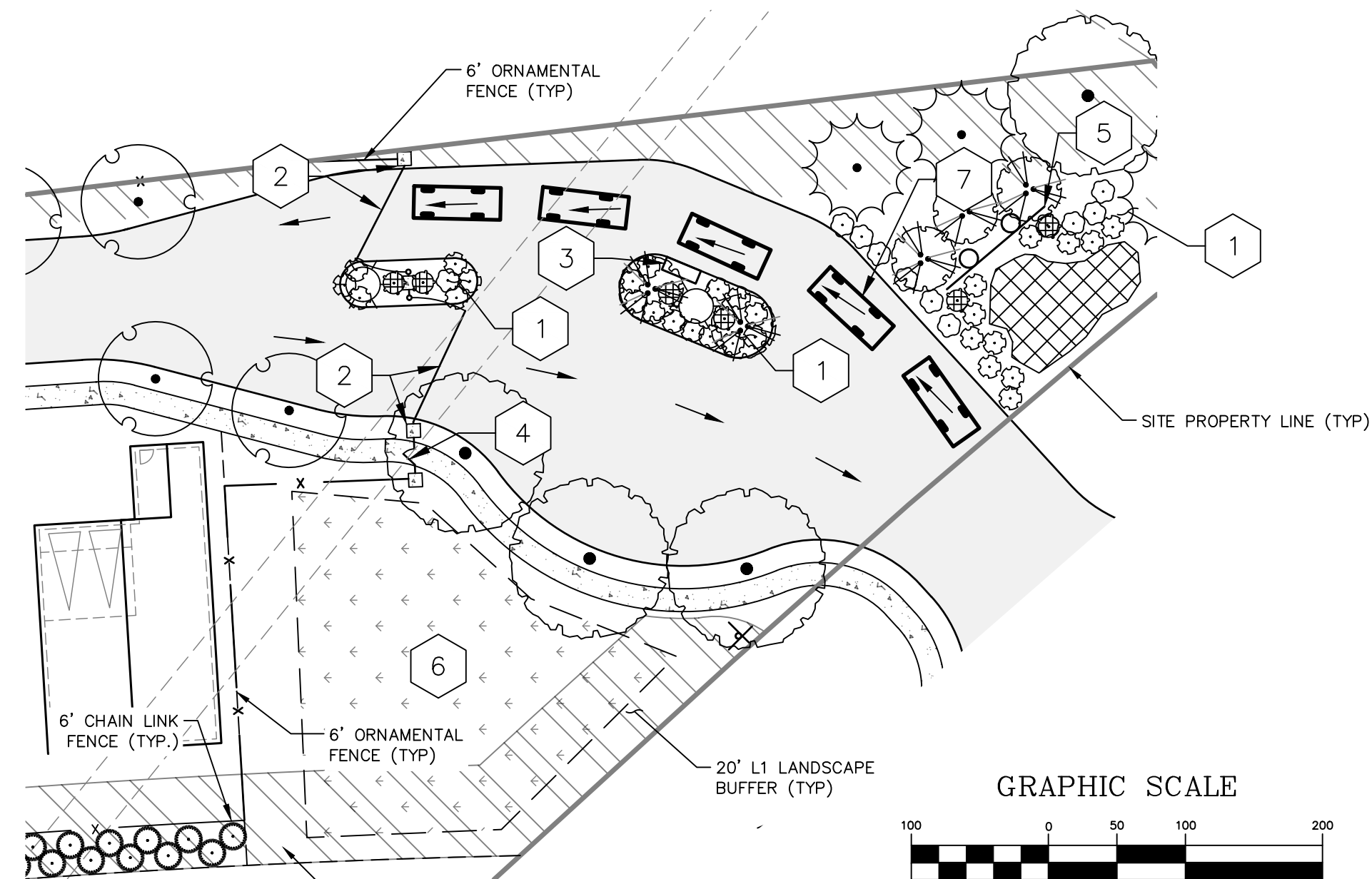
19-342 SITE PLAN EXHIBIT.dwg SITE PLAN

SITE PLAN - DESIGN CHARACTER

LATAH GLEN RESIDENTIAL COMMUNITY

CALLOUTS

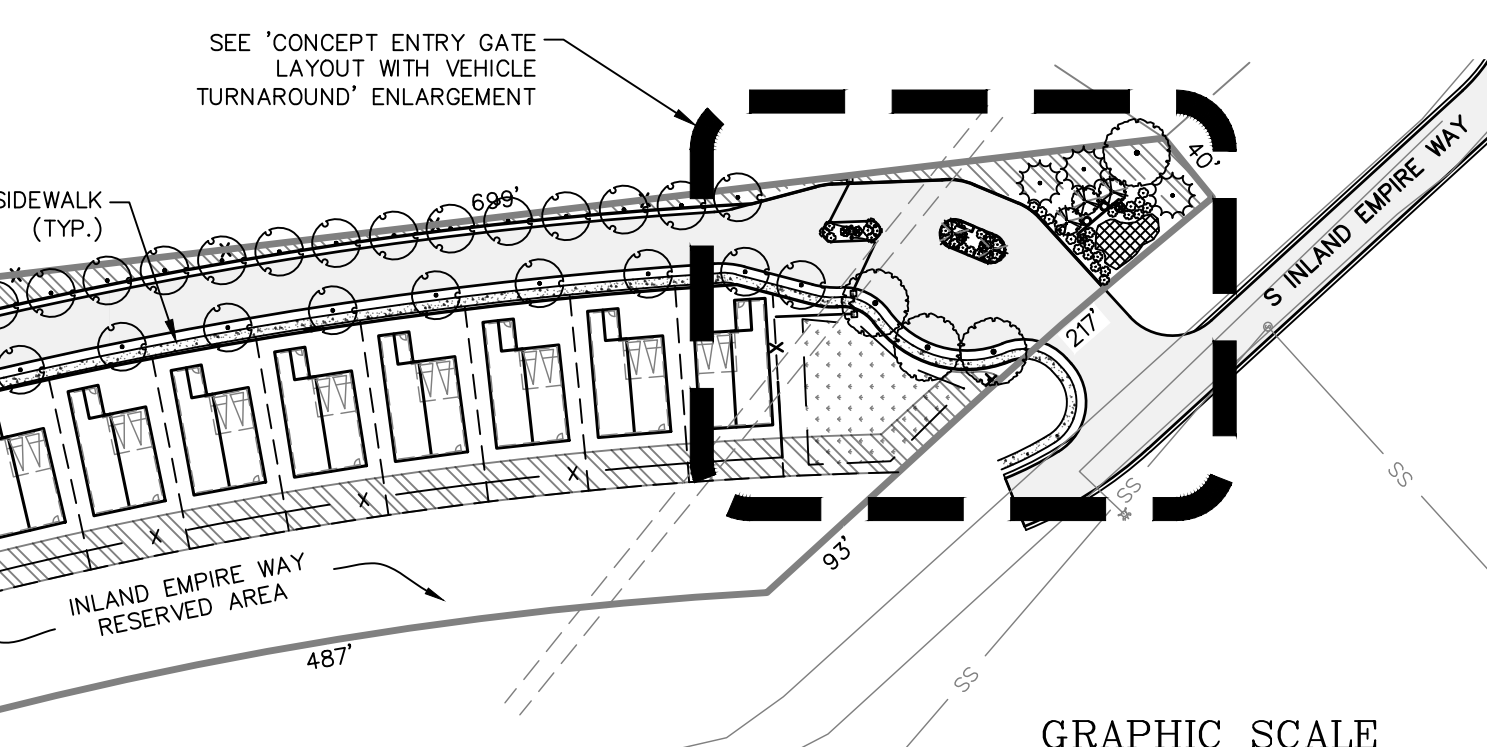
- 1 PLANTER - DECIDUOUS AND EVERGREEN TREES, SHRUBS, AND PERENNIALS (TYP)
- 2 ORNAMENTAL VEHICLE GATE WITH MASONRY COLUMNS - SEE SHEET 3 FOR CHARACTER IMAGE AND NOTES.
- 3 GATE KEYPAD (TYP)
- 4 ORNAMENTAL PEDESTRIAN GATE WITH CONTROLLED ACCESS (TYP)
- 5 MASONRY MONUMENT SIGN - SEE SHEET 2 FOR SIGN CHARACTER AND NOTES.
- 6 CONCEPT BIORETENTION STORMWATER SWALE - SEE 'BIORETENTION STORMWATER SWALE CHARACTER'
- 7 QUEUED VEHICLE (TYP)



CONCEPT ENTRY GATE LAYOUT WITH VEHICLE TURNAROUND



BIORETENTION STORMWATER SWALE CHARACTER



SEE 'CONCEPT ENTRY GATE LAYOUT WITH VEHICLE TURNAROUND' ENLARGEMENT

TREE PALLETTE

TREES	CODE	BOTANICAL NAME	COMMON NAME	SIZE
	AR2	Acer rubrum 'Bowhall'	Bowhall Maple	2" Cal.
	AR	Acer rubrum 'October Glory' TM	October Glory Maple	2" Cal.
	AC	Acer x freemanii 'Celebration' TM	Celebration Maple	2" Cal.
	GI	Gleditsia triacanthos inermis 'Sunburst'	Sunburst Thornless Honey Locust	2" Cal.
	MX	Malus x 'Spring Snow'	Spring Snow Crab Apple	2" Cal.
	PP	Pinus Ponderosa	Ponderosa Pine	6' TALL

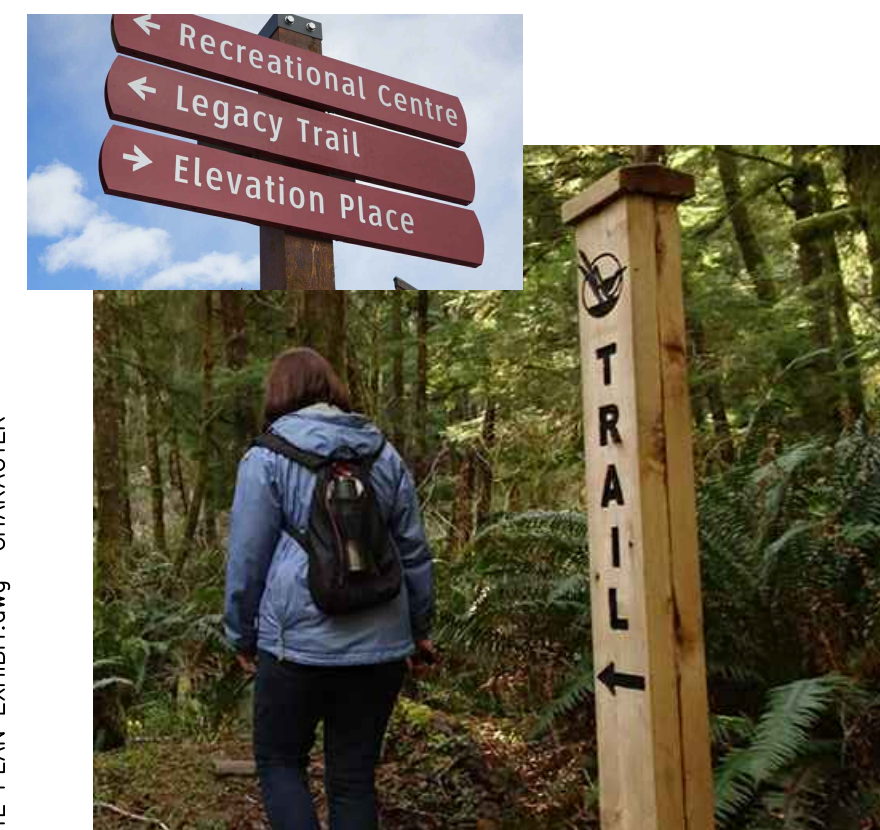


FISH LAKE TRAIL
CLASS 1 RECREATIONAL TRAIL
ALTERNATIVE TRANSPORTATION



PEDESTRIAN SYSTEM FEATURES/CHARACTER

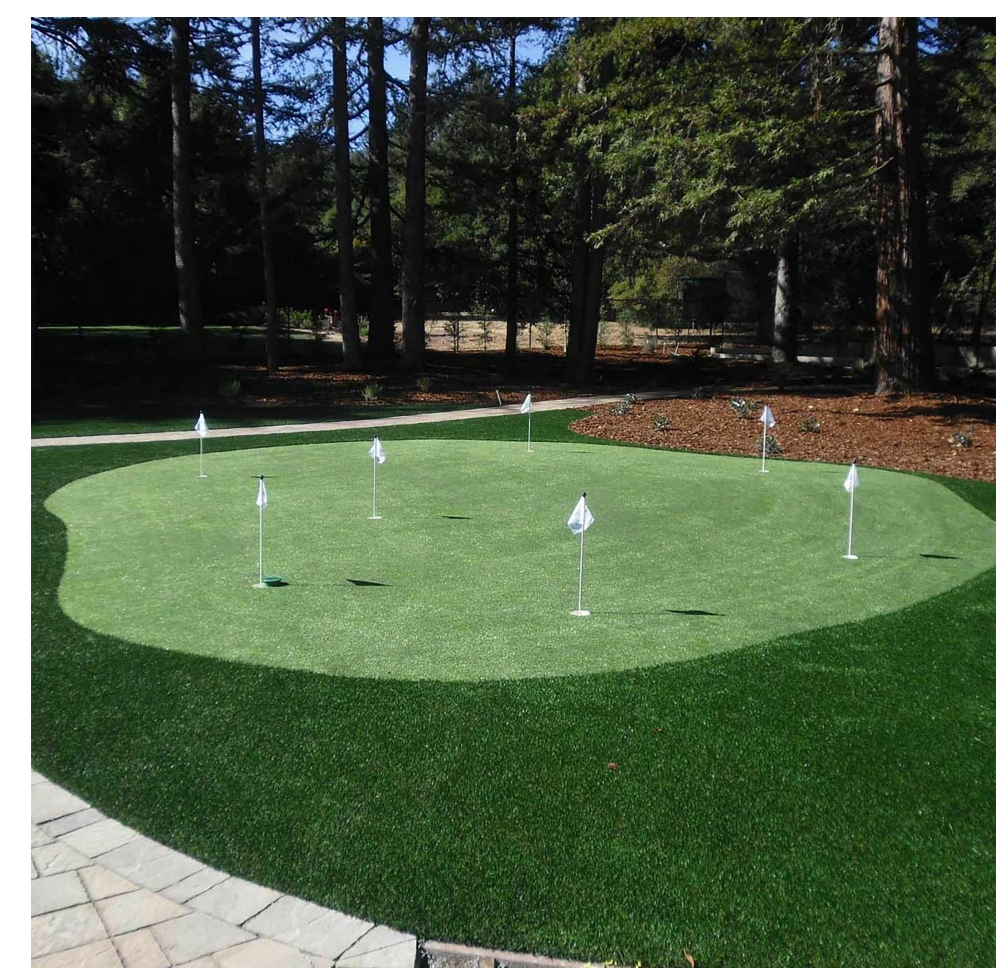
FINAL DESIGN OF PEDESTRIAN SYSTEM CHARACTER ELEMENTS TO COMPLIMENT NORTHWEST CHARACTER OF THE FINAL CLUBHOUSE DESIGN. STANDARD BENCH, SIGNAGE, AND PEDESTRIAN-SCALE LIGHTING WILL BE SPECIFIED AS PART OF A COHESIVE SITE CHARACTER - SEE SHEETS 4 AND 5 FOR CLUBHOUSE AND RELATED CONCEPT ENTRY CHARACTER.



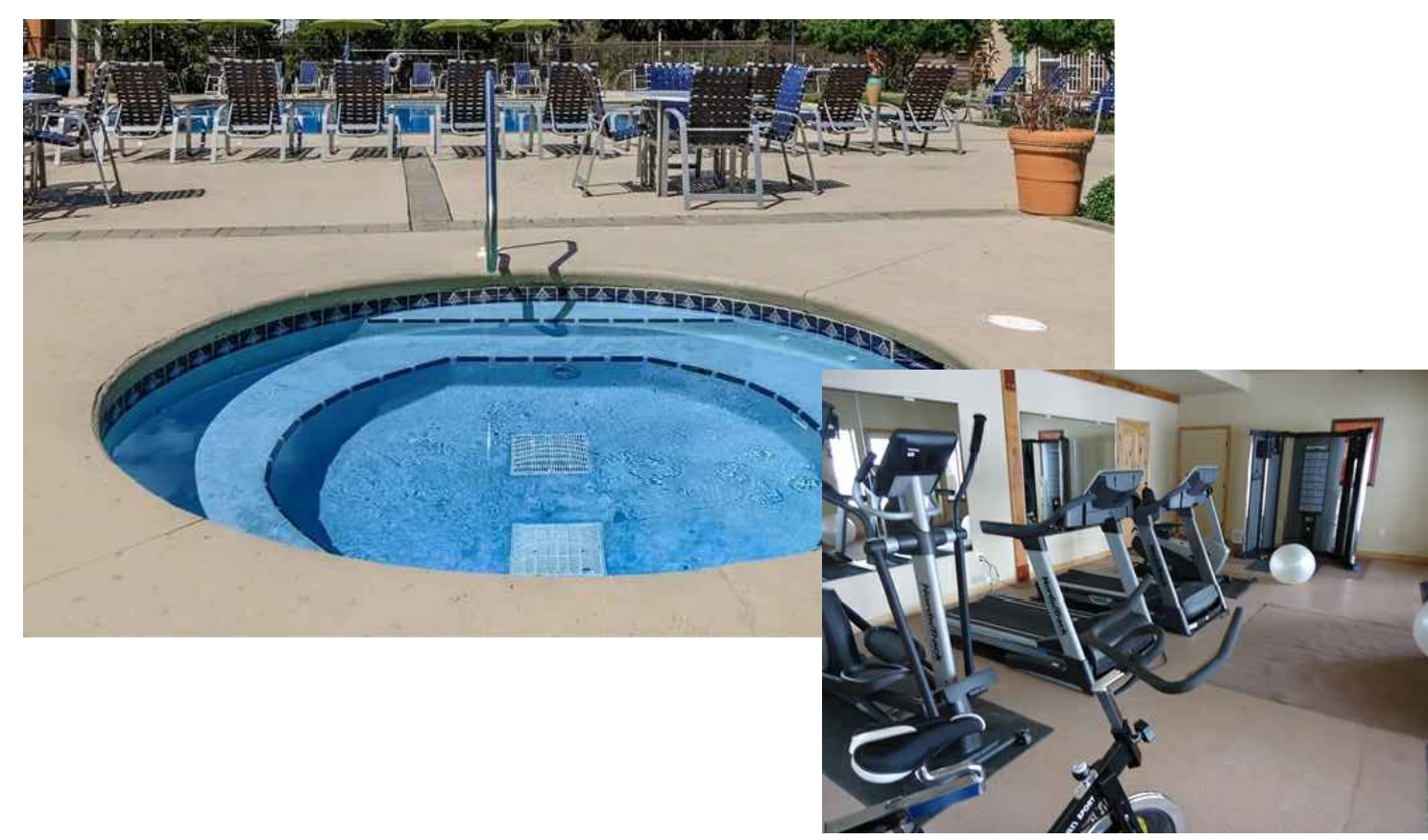
WAYFINDING/IDENTITY SIGNAGE (A)



SEATING NODE WITH PLANTINGS (B)



RECREATION NODE - PUTTING GREEN (C)



RECREATION NODE - CLUBHOUSE (D)

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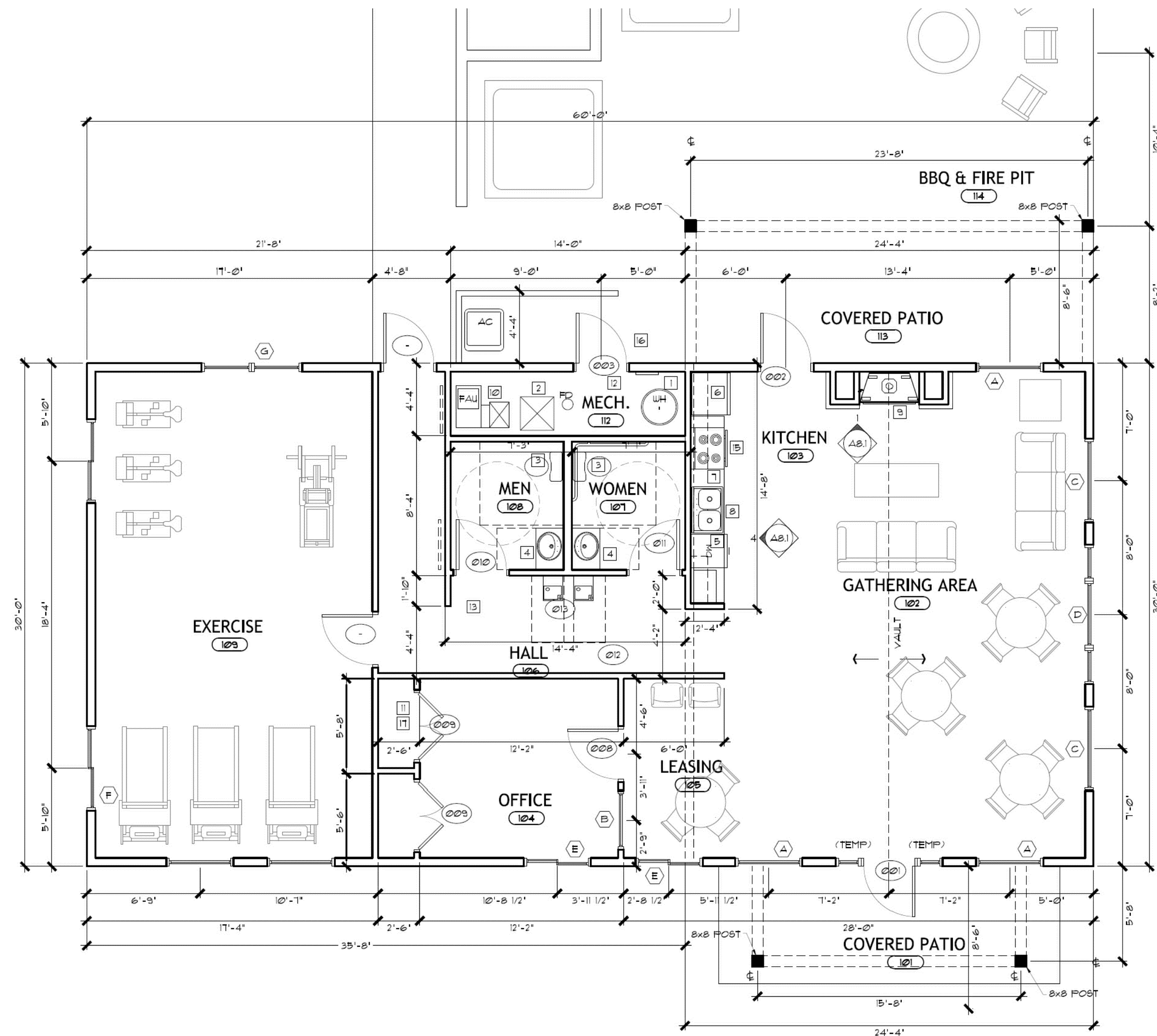
510 east third avenue | spokane, wa | 99202
p 509.242.1000 | f 509.242.1001

DATE	11/03/2020	SCALE	1" = 100'
FIELD BOOK	N/A	DRAWN	WMS
PROJECT NUMBER	19-342	DRAWING NO.	3 OF 6

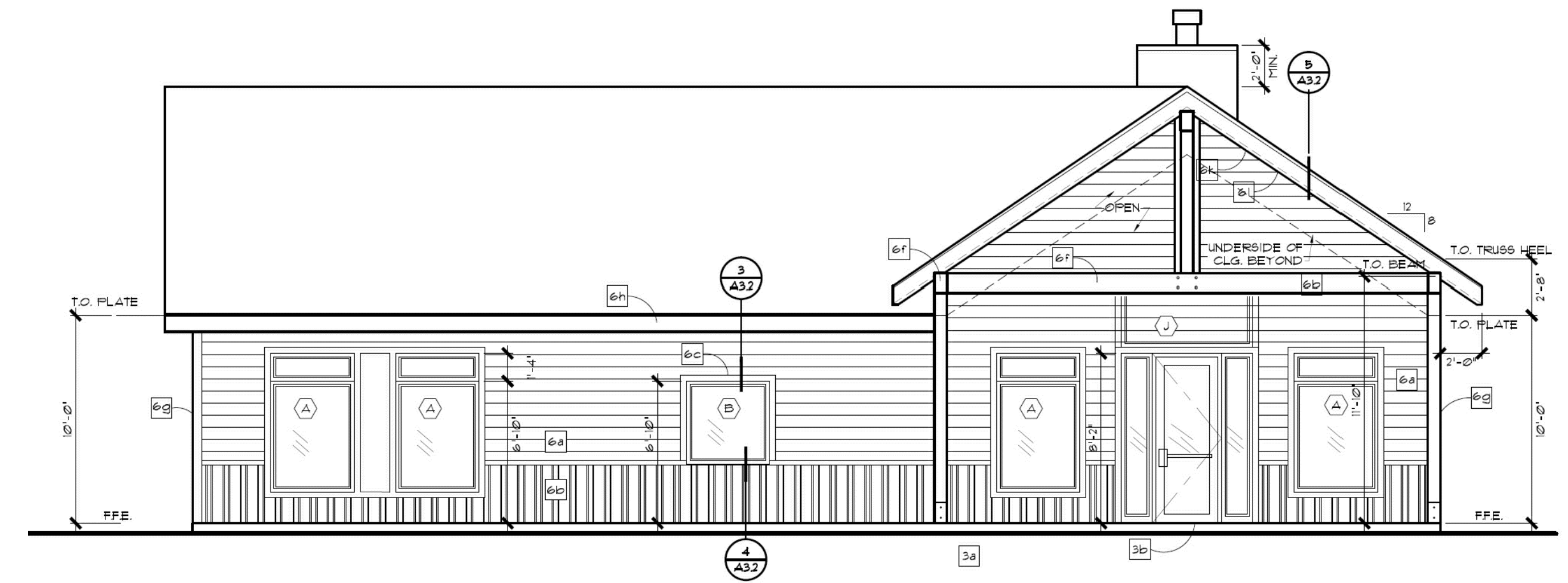
19-342 SITE PLAN EXHIBIT.dwg CHARACTER

CONCEPT BUILDING DETAILS LATAH GLEN RESIDENTIAL COMMUNITY

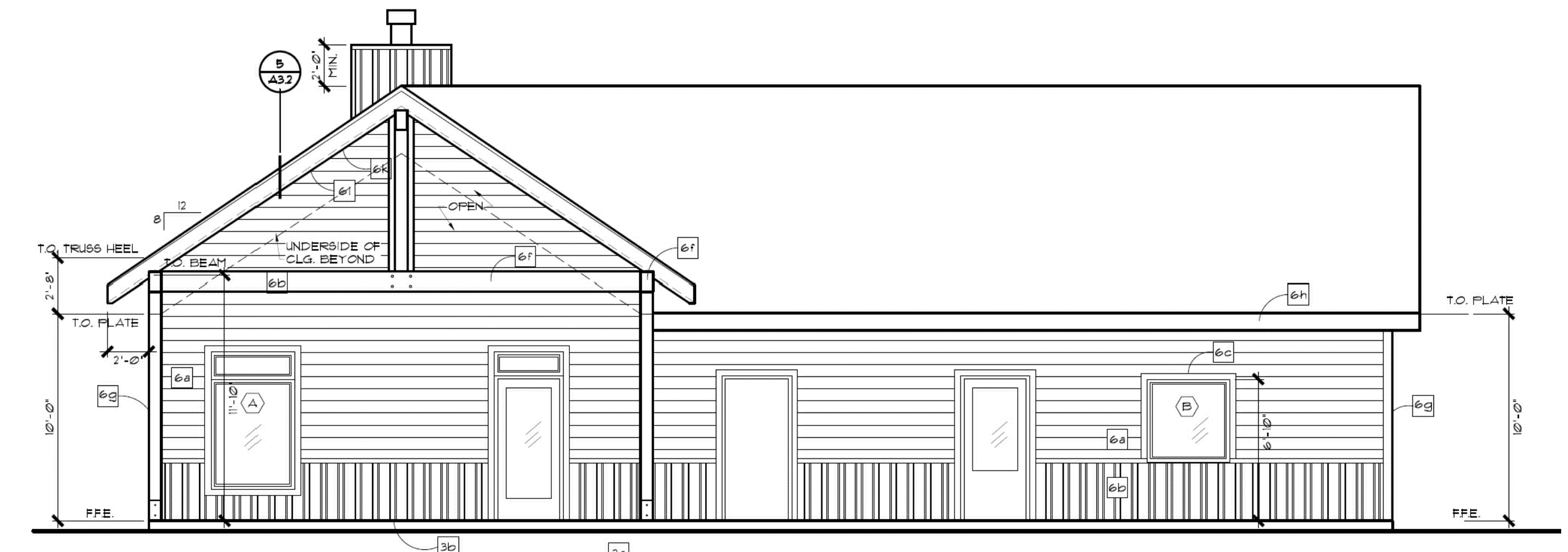
A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON



CONCEPT CLUBHOUSE – FLOOR PLAN

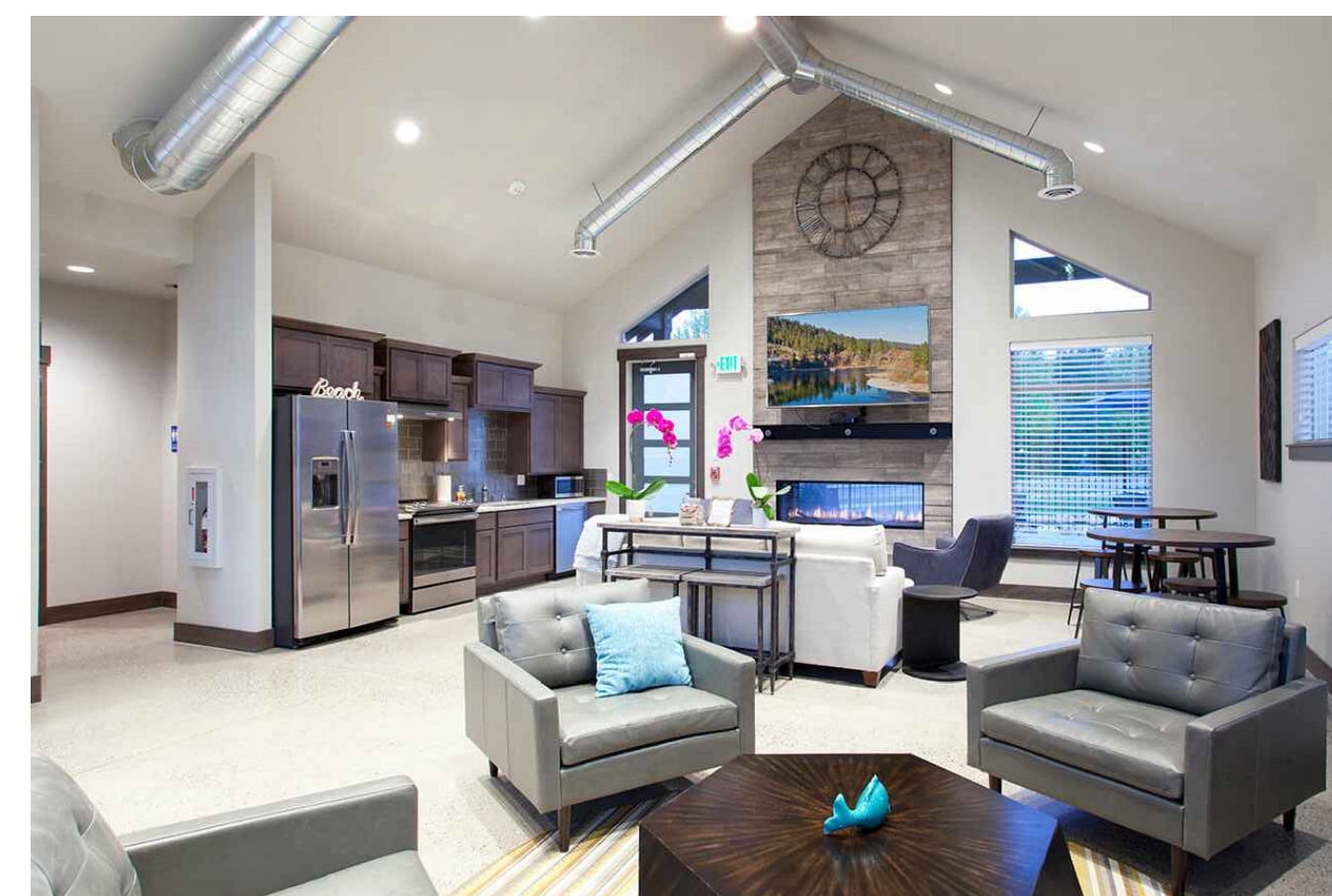


CONCEPT CLUBHOUSE – FRONT ELEVATION



CONCEPT CLUBHOUSE – REAR ELEVATION

CONCEPT CLUBHOUSE – 1,800 SF



CONCEPT CLUBHOUSE – NORTHWEST CHARACTER

CLUBHOUSE NOTES:

1. FINAL DESIGN OF CLUBHOUSE WILL INFORM SITE CHARACTER ELEMENTS INTO A COHESIVE THEME – SEE SHEET 5, CONCEPT ENTRY CHARACTER.
2. FINAL DESIGN OF CLUBHOUSE IN NORTHWEST CHARACTER MAY VARY.

19-342 SITE PLAN EXHIBIT.dwg CLUBHOUSE

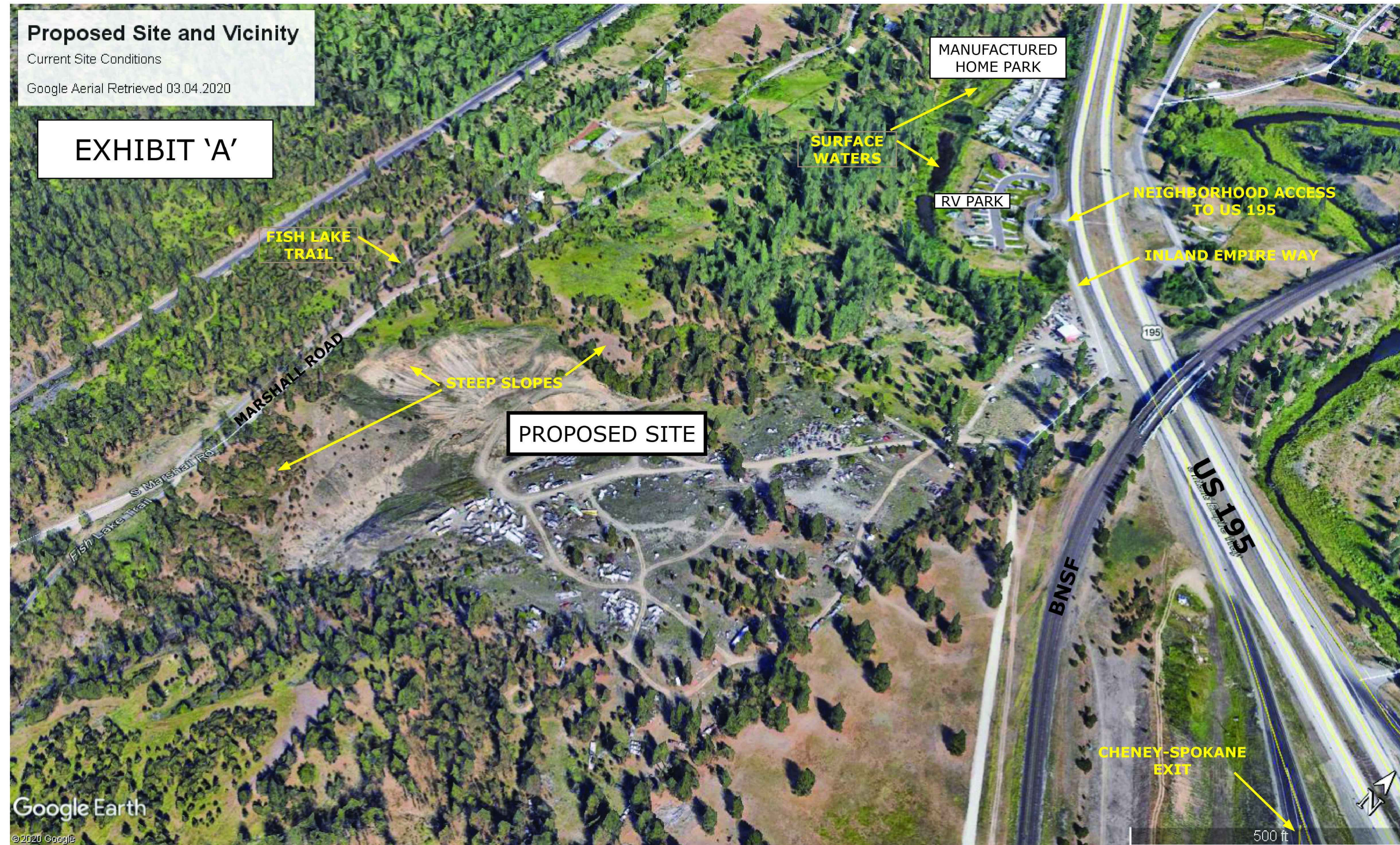
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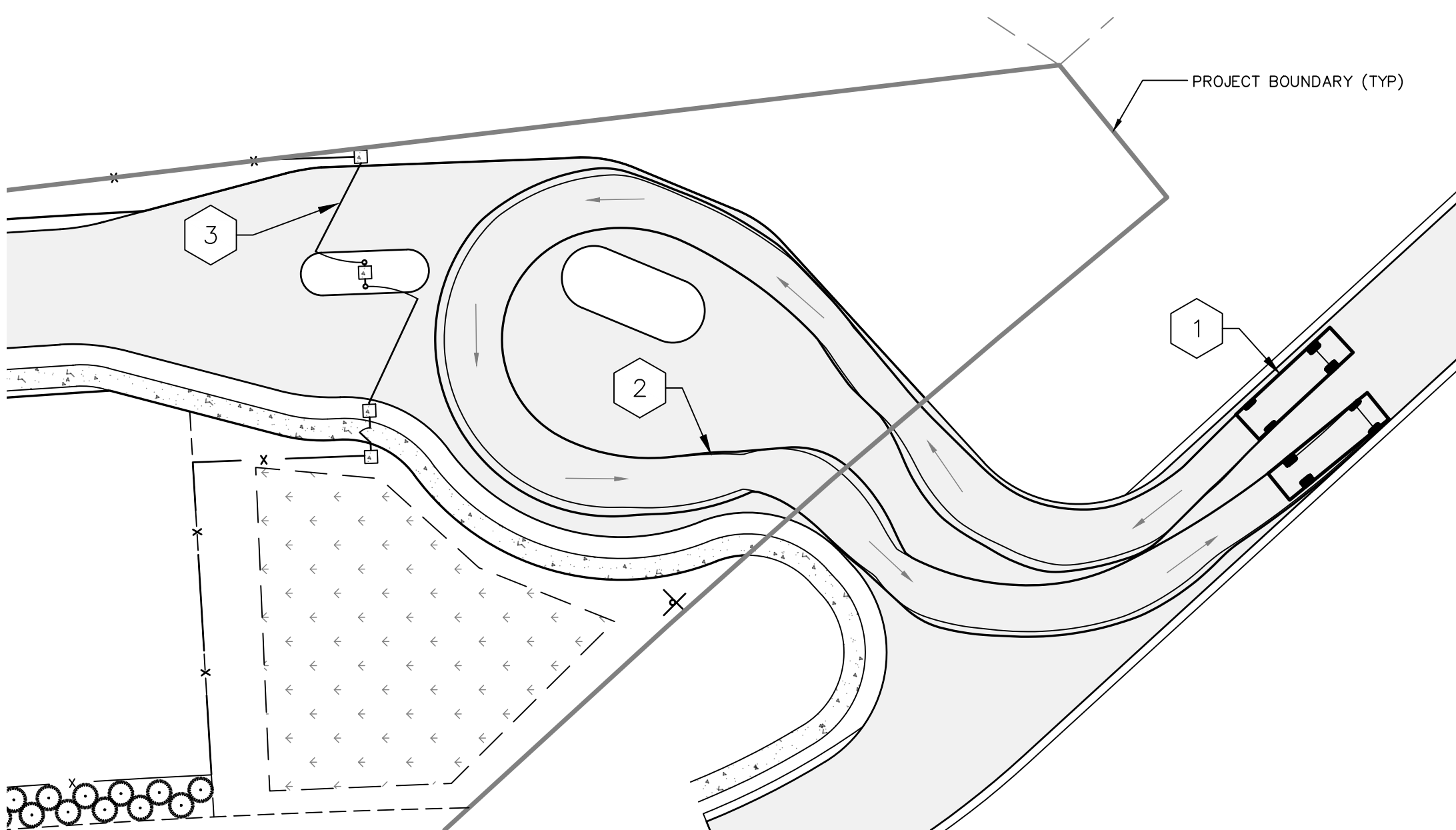
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FIELD BOOK	N/A	DRAWN	WMS
PROJECT NUMBER	19-342	DRAWING NO.	4 OF 5

NEIGHBORHOOD AND SITE DETAILS LATAH GLEN RESIDENTIAL COMMUNITY

A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON

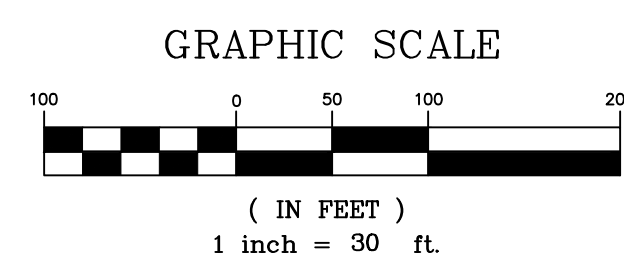


VICINITY CHARACTER



FIRE TRUCK TURNAROUND AT ENTRY GATE

19-342 SITE PLAN EXHIBIT.dwg NEIGH + SITE INFO



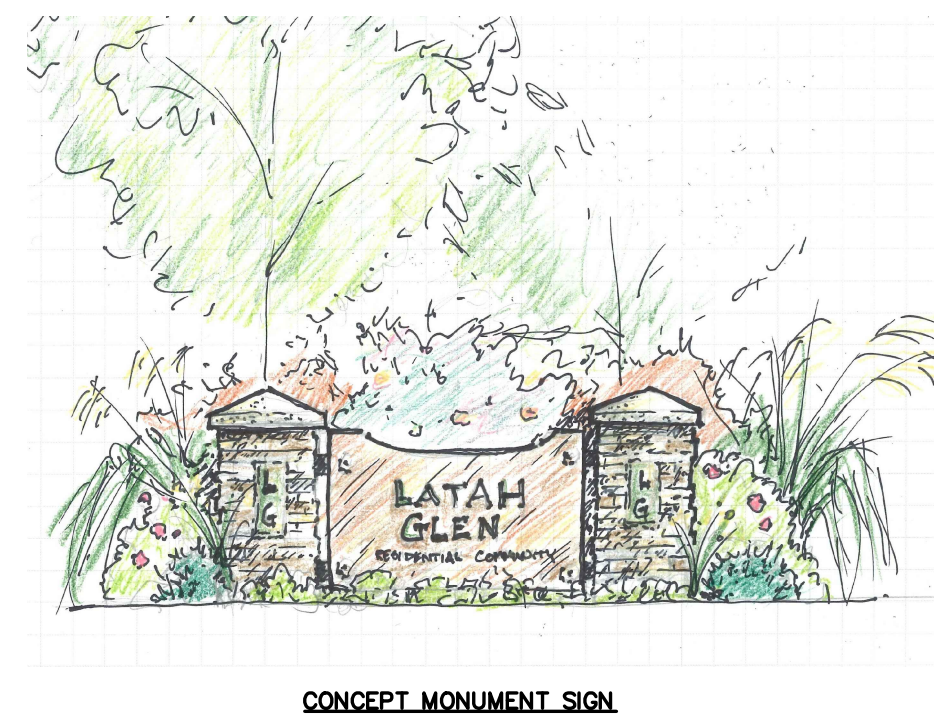
NEIGHBORHOOD CHARACTER

SHRUBS	CODE	BOTANICAL NAME	COMMON NAME
AP		Acer circinatum "Pacific Fire"	Vine Maple
BS		Banksia x "Green Mountain"	Bonewood
CS3		Cornus verticillata	Red Twig Dogwood
CS3		Cornus sericea "Flaviramosa"	Yellow Twig Dogwood
HB		Hibiscus syriacus "Blue Satin"	Rose of Sharon
		Hibiscus diversus	Oxeye
HA2		Hydrangea arborescens pendula	Climbing Hydrangea
IL		Ilex x meserveae "Blue Boy" TM	Blue Boy Holly
IS2		Juniperus horizontalis "Wichita Blue"	Wichita Blue Juniper
IS		Juniperus squarrosa	Oregon Grape
RF		Rubus idaeus "Gold Star"	Gold Star Rubus
RL		Rosa rugosa "Otto Luyken"	Luyken's Rose
		Rosa rugosa "Double Red"	Double Red Rose
SA		Spiraea x "Anthony Waterer"	Anthony Waterer Spiraea
TX		Taxus x media "Desformosa"	Desformosa
UD		Ulmus americana "Emerald"	Emerald Green Elm
VP		Viburnum opulus "Nanum"	Nanum Viburnum
VP		Viburnum acerifolium "Doublefile"	Doublefile Viburnum
WF		Wegelia florida "Evers" TM	Wegelia
ANNUALS/PERENNIALS	CODE	BOTANICAL NAME	COMMON NAME
DF		Dryopteris filix-mas "Robusta"	Robust Male Fern
HC2		Hemerocallis x "Alabama Jubilee"	Jubilee
HS		Hemiphragma x "Soleil de Cho"	Soleil de Cho Daylily
PA		Panicum x "Emerald Blue"	Emerald Blue Paspalum
PC3		Perovskia atrorubra	Russian Sage
PE2		Pilea subulata "Cotton Candy"	Cotton Candy Pilea
PE2		Pilea subulata "Emerald Blue"	Emerald Blue Pilea
PH2		Polystichum maritimum	Western Sword Fern
RF		Rudbeckia fulgida "Goldstrum"	Goldstrum
GRASSES	CODE	BOTANICAL NAME	COMMON NAME
CX		Calamagrostis x acutiflora "Avalanche"	Avalanche
HS		Hiscandrus sinensis "Morning Light"	Morning Light
SHRUB AREAS	BOTANICAL NAME	COMMON NAME	
	Juniperus horizontalis "Blue Chip"	Blue Chip Juniper	
GROUND COVERS	BOTANICAL NAME	COMMON NAME	
	Antrodiaefolia vicia-vari "Massachusetts"	Massachusetts Manzanita	
	Poa pratensis	Kentucky Bluegrass	

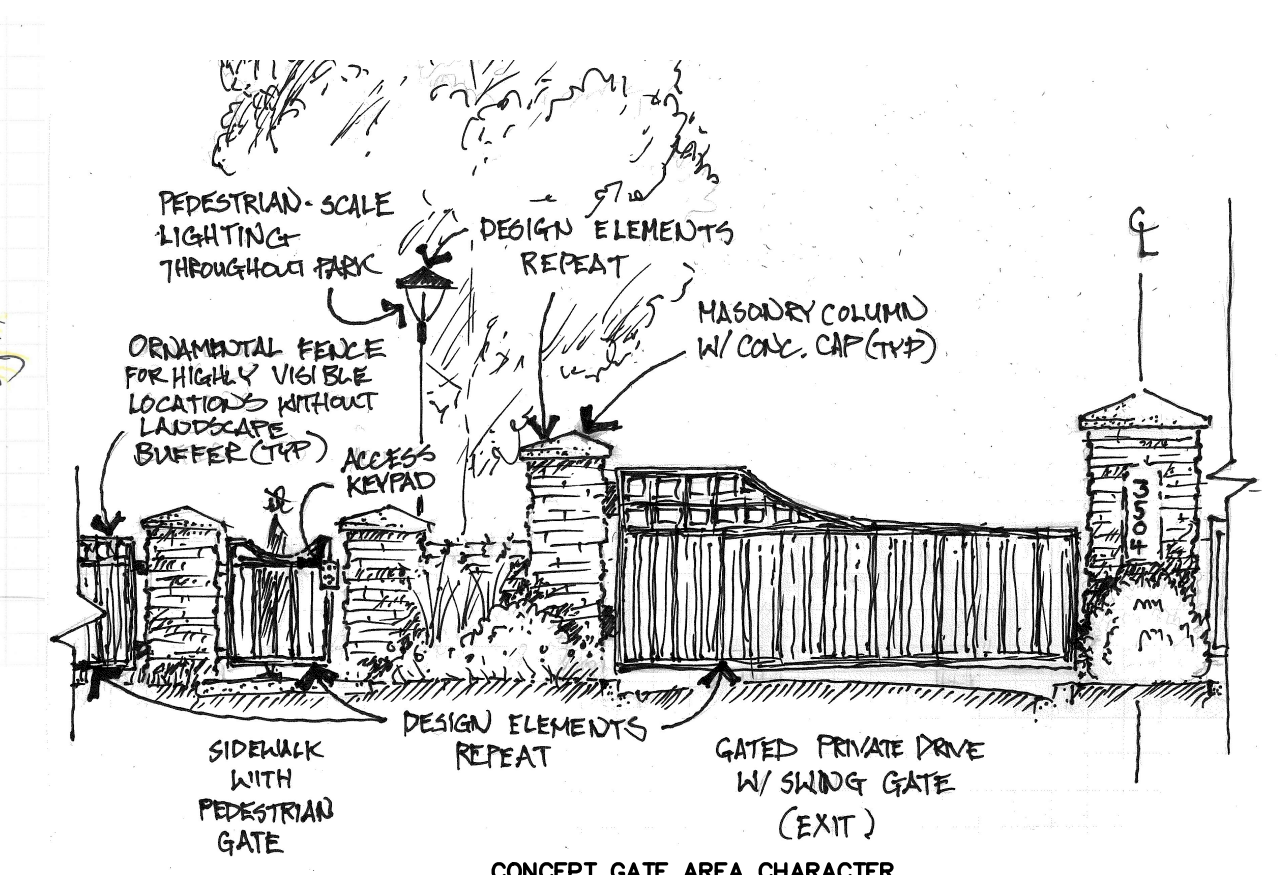
PLANT LIST

CONCEPT ENTRY/SITE CHARACTER NOTES:

- GATE EQUIPPED WITH KNOX BOX FOR EMERGENCY VEHICLE ACCESS.
- SITE SIGNAGE, GATE, VISIBLE FENCING, LIGHTING, AND SEATING TO COMPLEMENT THE NORTHWEST CHARACTER OF THE CLUBHOUSE FINAL DESIGN.
- FINAL DESIGN MAY VARY.



CONCEPT MONUMENT SIGN



CONCEPT GATE AREA CHARACTER

CONCEPT COHESIVE ENTRY/SITE CHARACTER

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DATE	11/03/2020	SCALE	N/A
FIELD BOOK	N/A	DRAWN	WMS
PROJECT NUMBER	19-342	DRAWING NO.	5 OF 5

EXHIBIT 'B'

CONCEPT STORMWATER PLAN LATAH GLEN RESIDENTIAL COMMUNITY

A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON



PROJECT INFORMATION

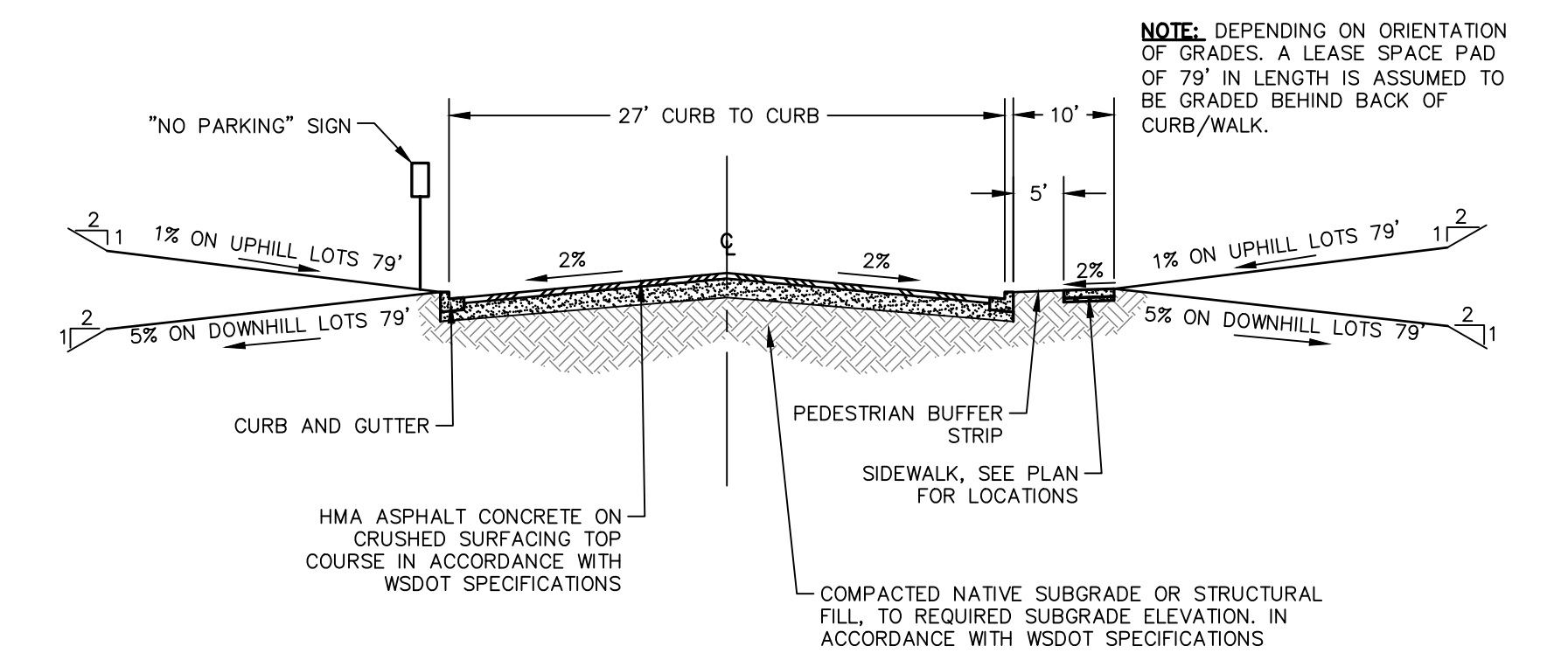
STORM DRAIN QUANTITIES:
CATCH BASIN 75
DRY WELLS 7
STORM DRAIN PIPE 6250 LF
(SEE PLAN FOR SIZES)

STORM DRAIN NOTES:

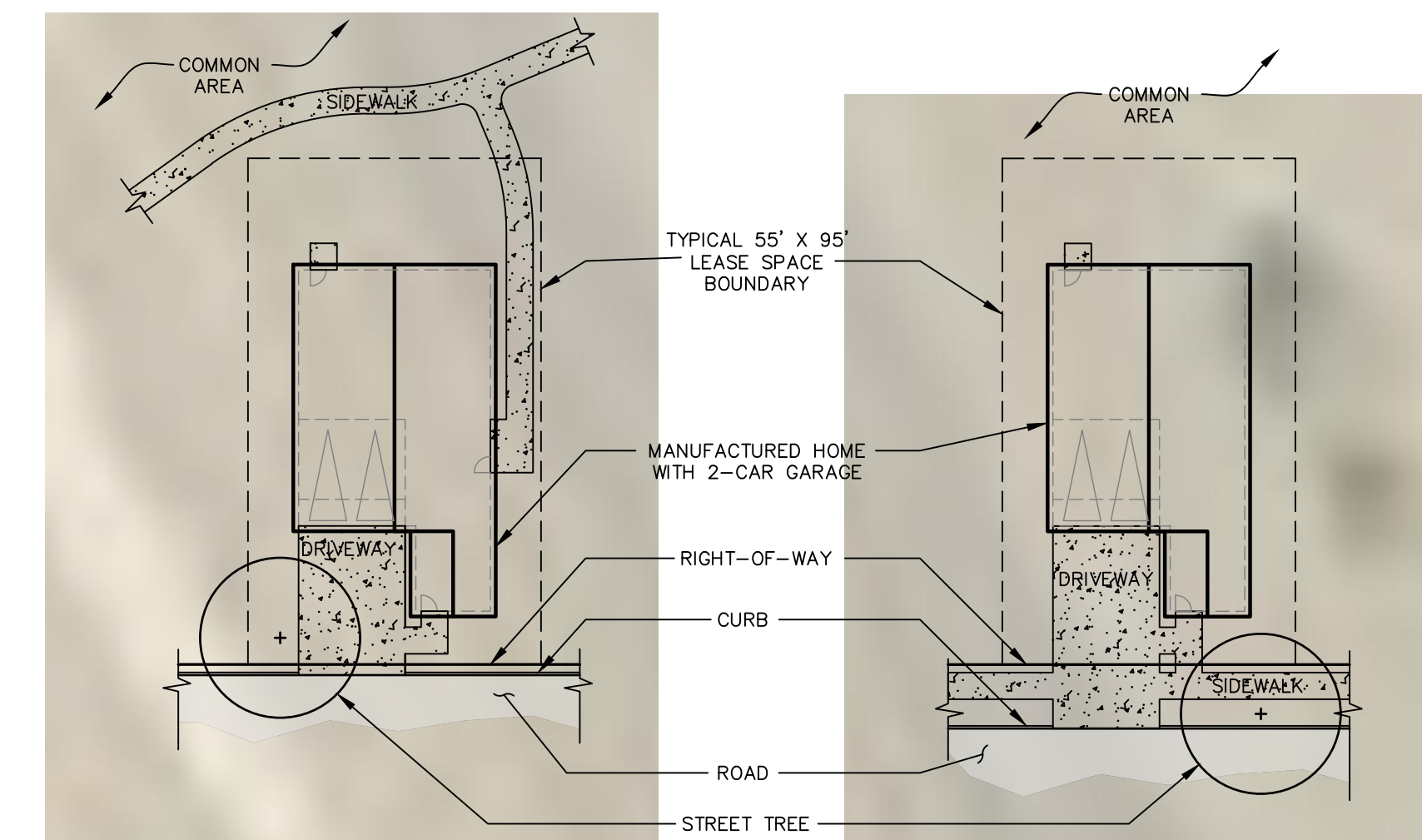
- CATCH BASIN AS SHOWN TO BE CITY OF SPOKANE STD. PLAN B-101C "TYPE 1 - CATCH BASIN"
- STORM DRAIN PIPE SHALL BE CPEP (CORRUGATED POLYETHYLENE PIPE) PER ADVANCED DRAINAGE SYSTEM (ADS) N-12, CONSTRUCTED PER ISPSWC SPECIFICATIONS.
- DRY WELLS AS LISTED TO BE CITY OF SPOKANE STD. PLAN B-102D "DRYWELL - TYPE 2"

SHEET INDEX

STORM DRAIN PLAN	SHEET 1 OF 3
UTILITY PLAN	SHEET 2 OF 3
PROFILE PLAN	SHEET 3 OF 3



TYPICAL PRIVATE ROAD SECTION
NTS



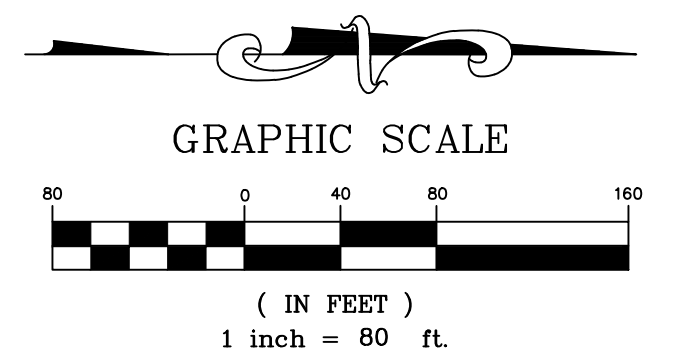
TYPICAL LEASE SPACE
SIDEWALK IN COMMON AREA
NTS

TYPICAL LEASE SPACE
SIDEWALK AT ROAD
NTS

STORM PIPE LEGEND



L.I.D. STORM WATER SWALE, TYP.
APPROXIMATE AREA SHOWN.



19-342 UTILITY CONCEPTS.dwg - STORM DRAIN PLAN

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DATE	09/30/2020	SCALE	1" = 80'
FIELD BOOK	NA	DRAWN	LMC
PROJECT NUMBER	19-342	DRAWING NO.	1 OF 3

CONCEPT UTILITY PLAN LATAH GLEN RESIDENTIAL COMMUNITY

A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER
AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
SECTION 36, TOWNSHIP 25 NORTH, RANGE 43 EAST, W.M., SPOKANE, WASHINGTON

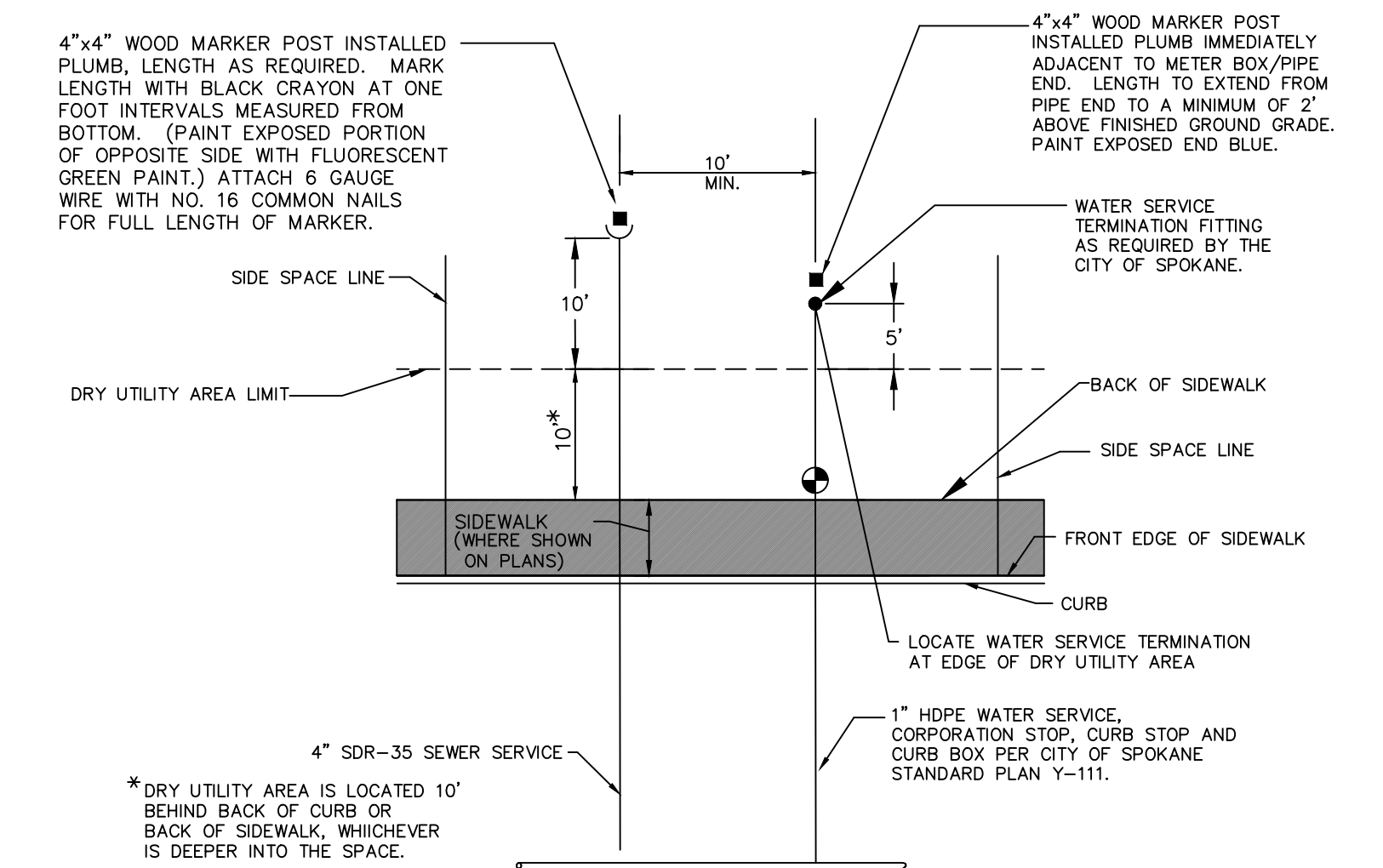
WATER MAIN 6880 LF
FIRE HYDRANTS 16
SANITARY SEWER MANHOLE 51
SANITARY SEWER MAIN (8") 6710 LF

UTILITY NOTES:

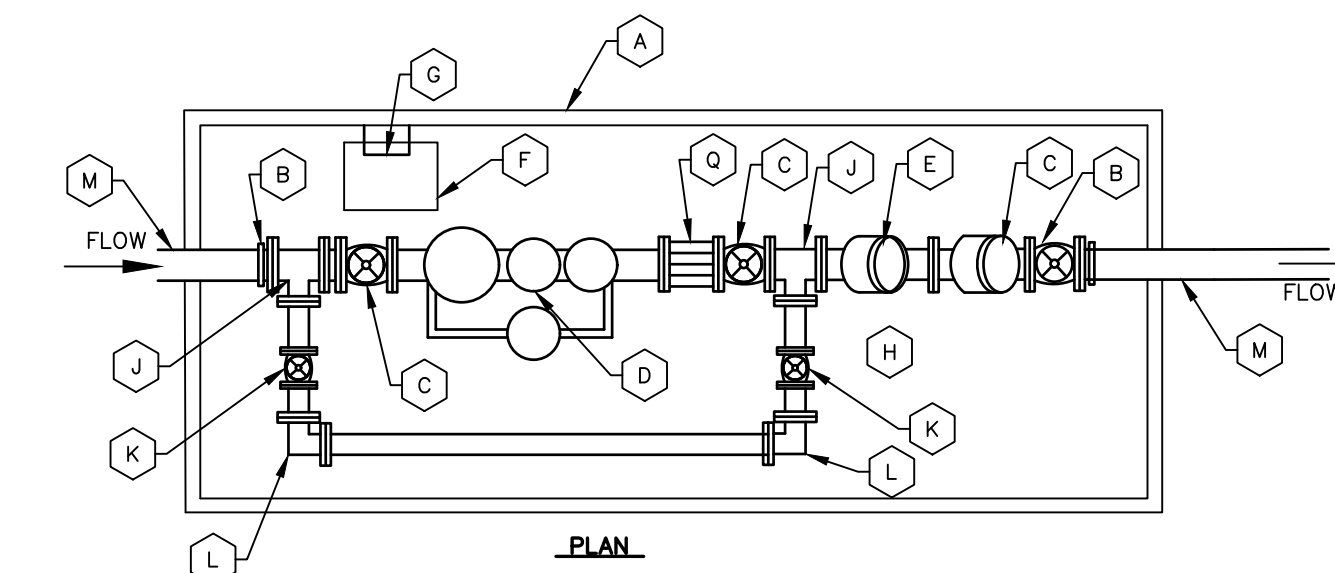
1. WATER PIPE, VALVES, AND FITTINGS (NOT INCLUDING SERVICES) SHALL BE DUCTILE IRON AND MECHANICAL JOINTS.
2. SANITARY SEWER MAINS AND SERVICES SHALL BE PVC, ASTM D 3034, SDR 35 WITH FLEXIBLE GASKET JOINTS. SEWER SERVICE CONNECTIONS SHALL BE MADE BY A TAP TO AN EXISTING MAIN, OR A TEE BRANCH FROM A NEW MAIN CONNECTED ABOVE THE SPRING LINE OF THE PIPE. SERVICE CONNECTIONS TO MANHOLES ARE PROHIBITED. SEWER SERVICES SHALL BE AT RIGHT ANGLES TO THE SEWER MAIN.

TABLE

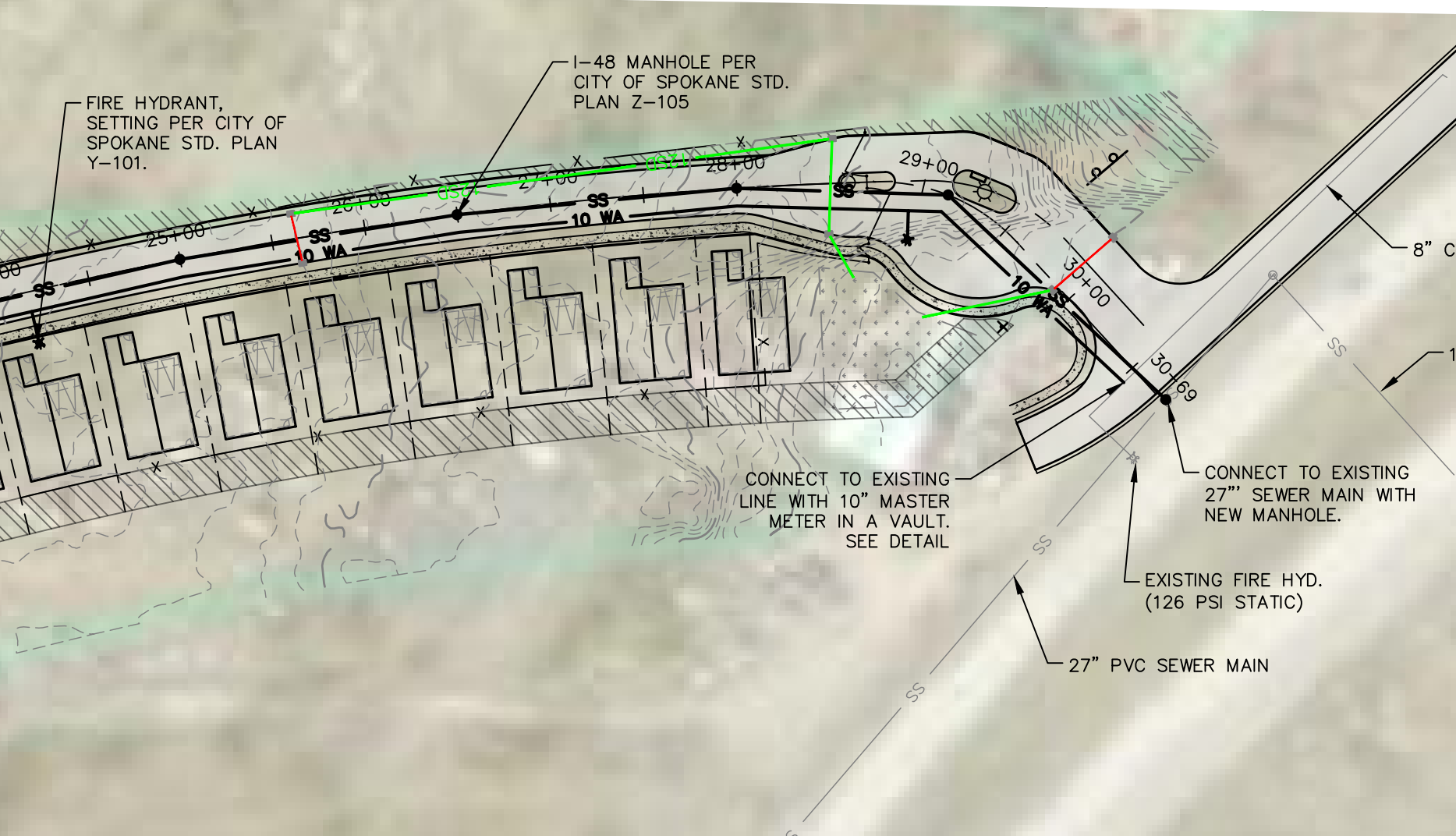
- A PRECAST CONCRETE METER VAULT, 7'W X 12'L X 6.5'H LARGE UTILITY VAULT, WILBERT PRECAST 1909 OR APPROVED EQUAL, SET ON 6" OF CRUSHED ROCK, VERIFY VAULT SIZE AND SPEC WITH CITY OF SPOKANE PRIOR TO CONSTRUCTION.
- B 10" MUXFL ADAPTER W/ MEGA-LUG MJ GLAND KIT
- C 10" GATE VALVE
- D 10" NEPTURE PROTECTUS III 3 COMBINATION FIRE/DOMESTIC METER PROVIDED BY CITY OF SPOKANE, INSTALLED BY CONTRACTOR
- E 10" WATTS DOUBLE CHECK VALVE ASSEMBLY
- F 60"x36" DOUBLE DOOR ALUMINUM ACCESS HATCH HALLIDAY S2R6036 OR APPROVED EQUAL
- G POLY COATED STEEL STEPS
- H PROVIDE ADJUSTABLE STEEL PIPE SUPPORTS SUPPORTED ON CONCRETE FOOTING IN 3 LOCATION BELOW ASSEMBLY
- J 10"x10"x4" TEE FL
- K 4" GATE VALVE
- L 4" 90° ELBOW
- M EXTEND 10" DUCTILE IRON PIPE 5' BEYOND OUTER WALL OF VAULT.
- O FLANGED DISMANTLING JOINT, ROMAN DJ400 OR EQUAL.



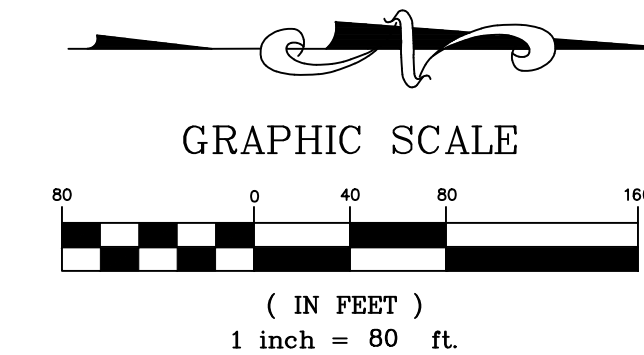
TYPICAL WATER AND SEWER SERVICE LOCATION



METER VAULT



EXISTING UTILITIES SHOWN IN THEIR APPROXIMATE LOCATION PER CITY OF SPOKANE GIS



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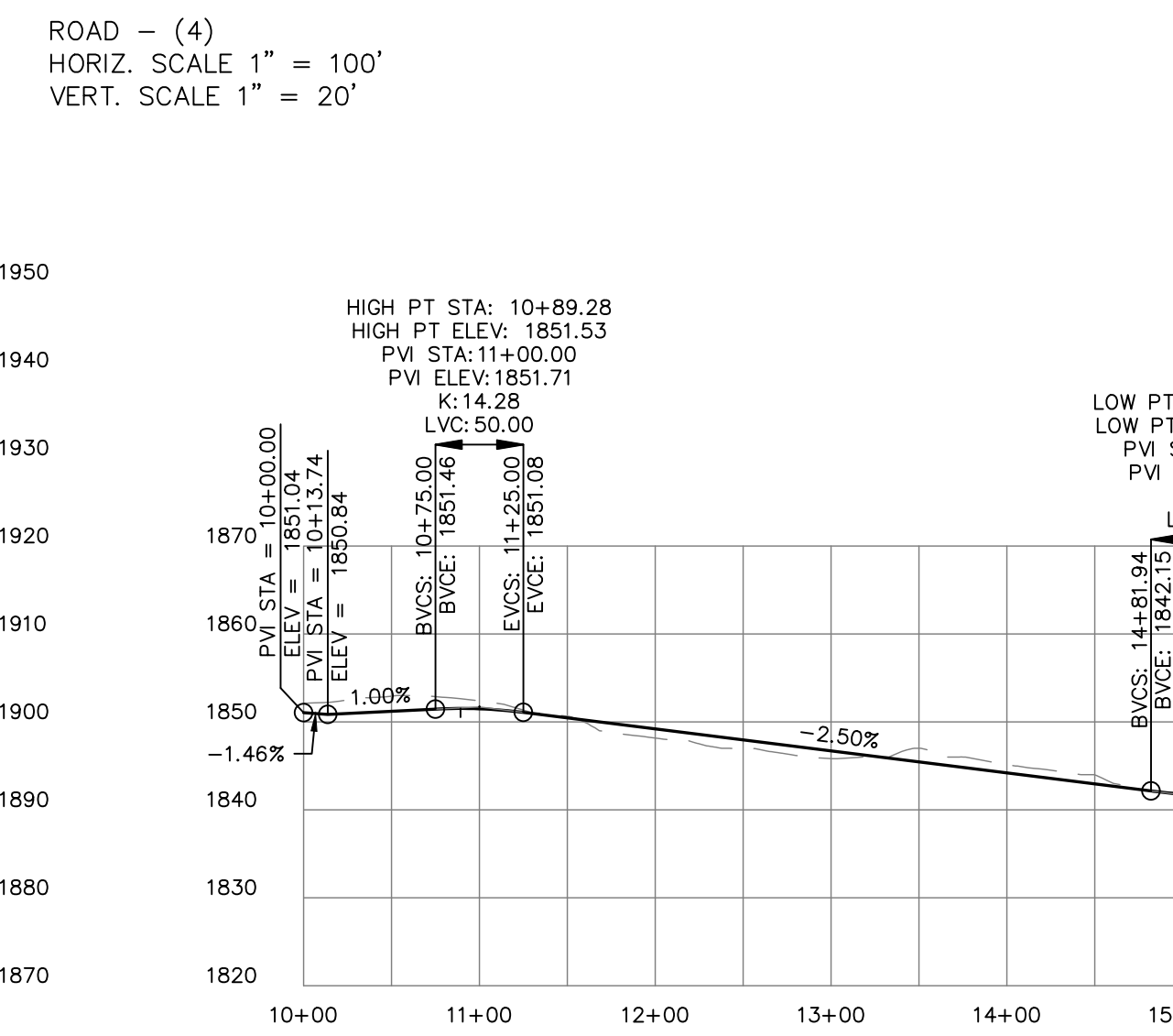
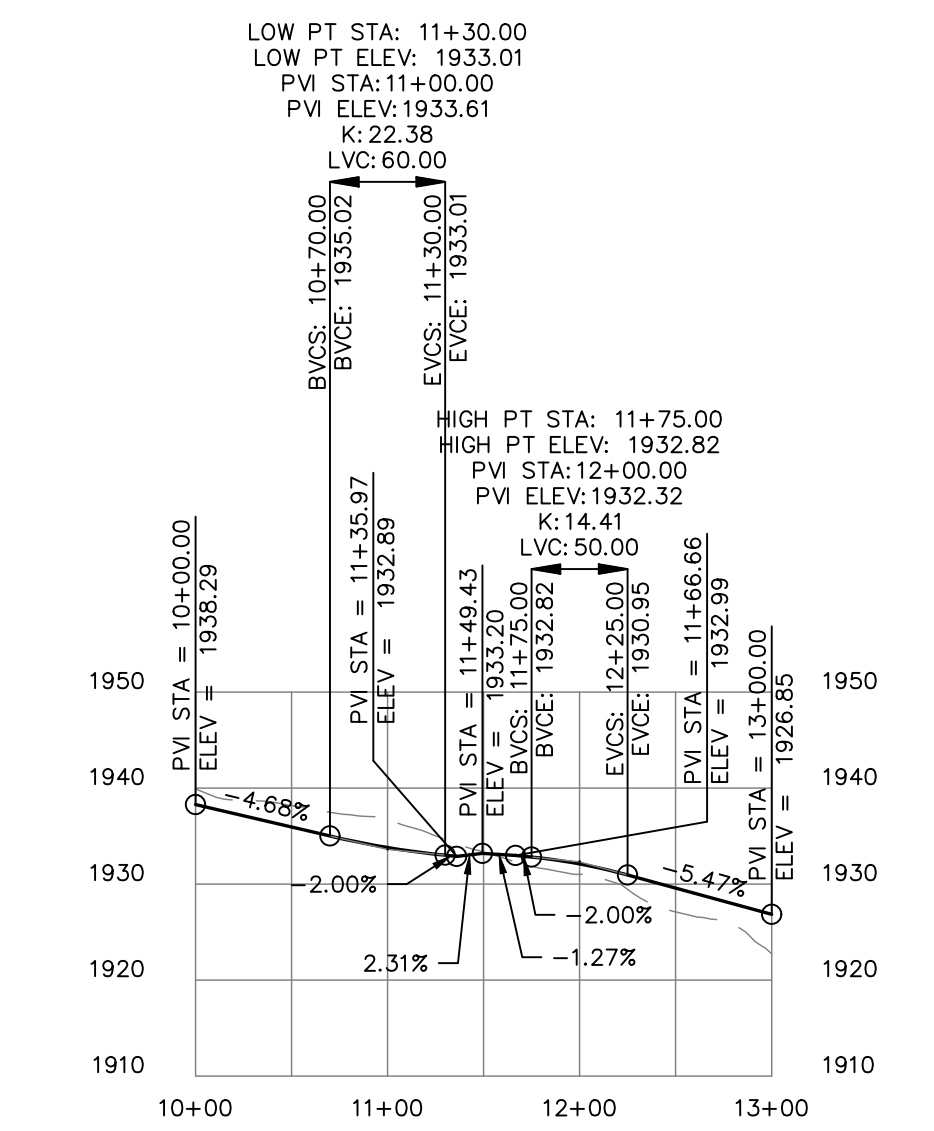
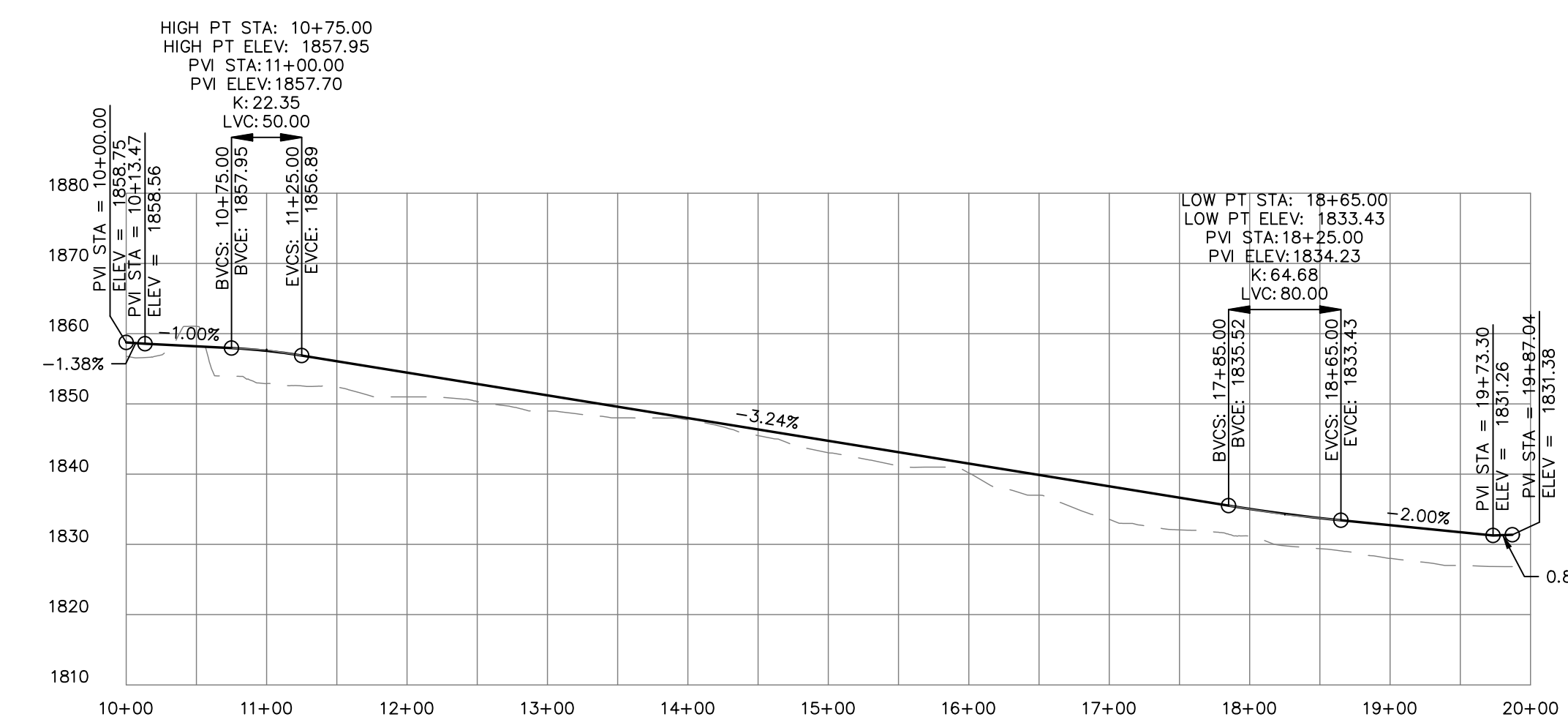
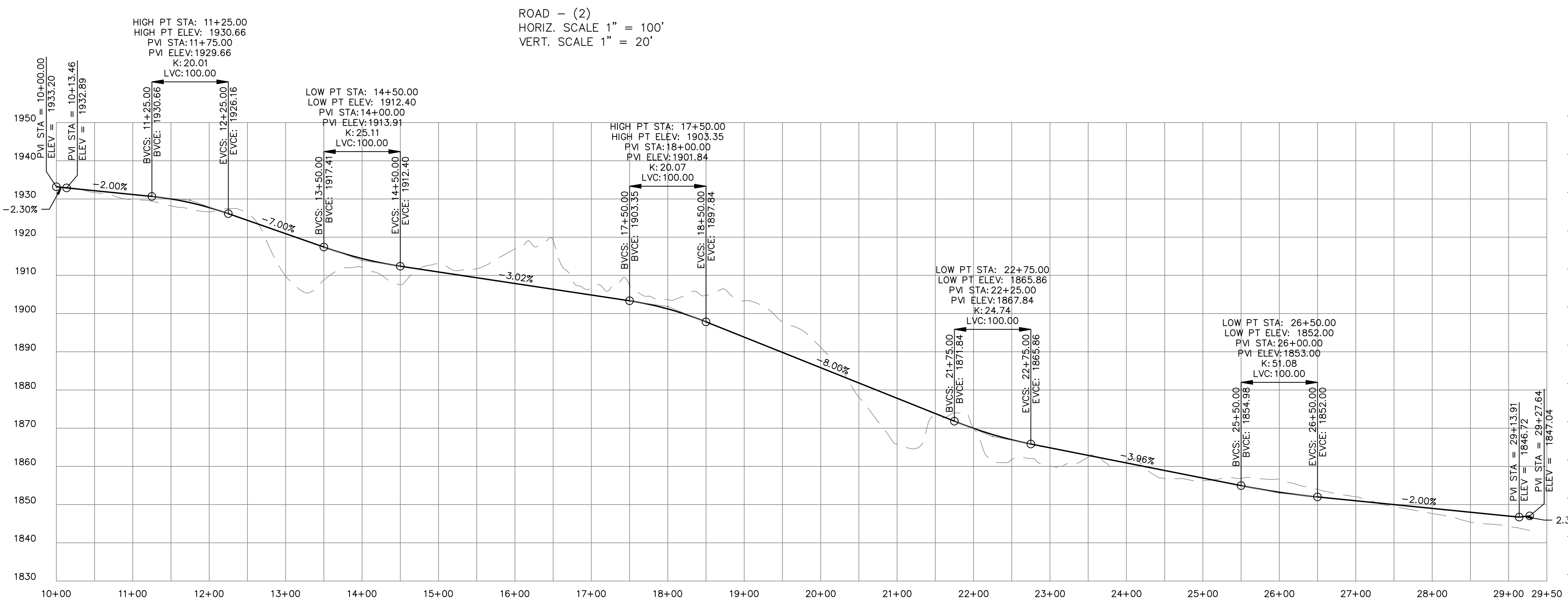
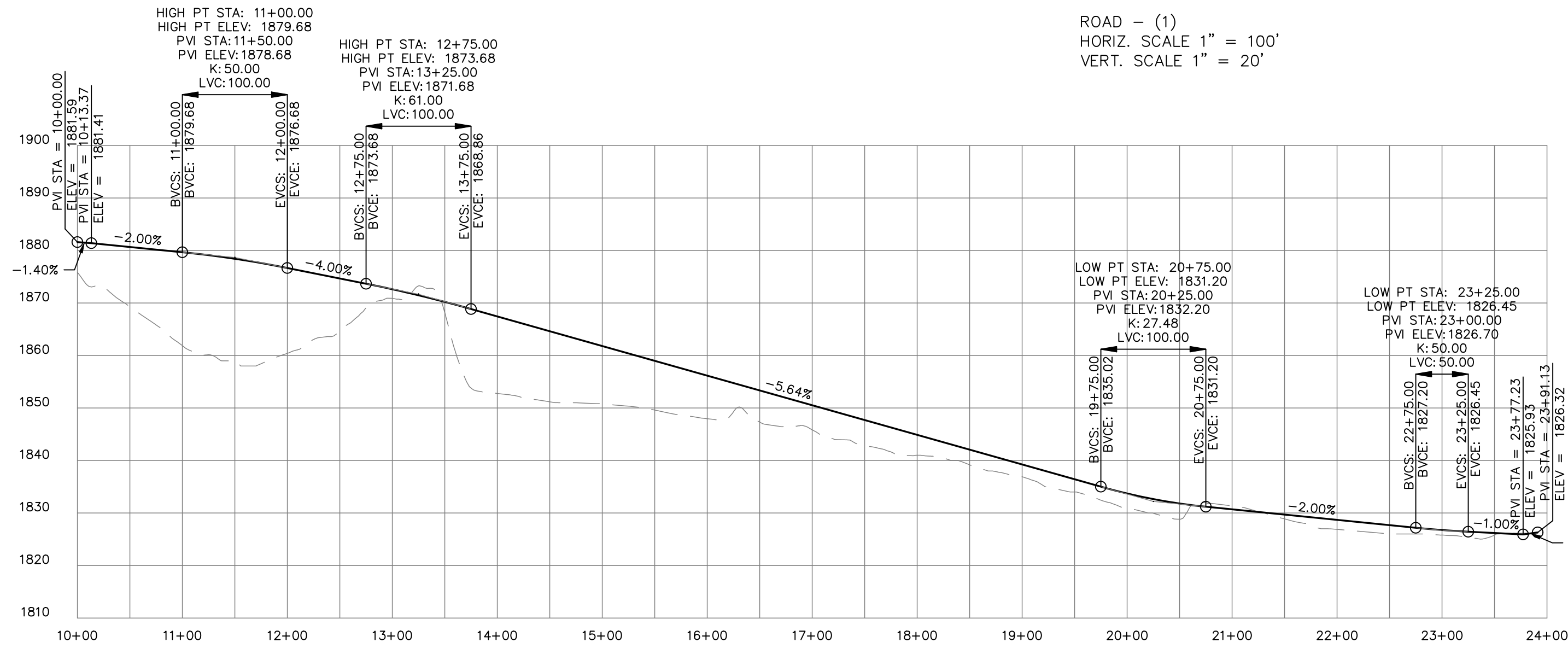
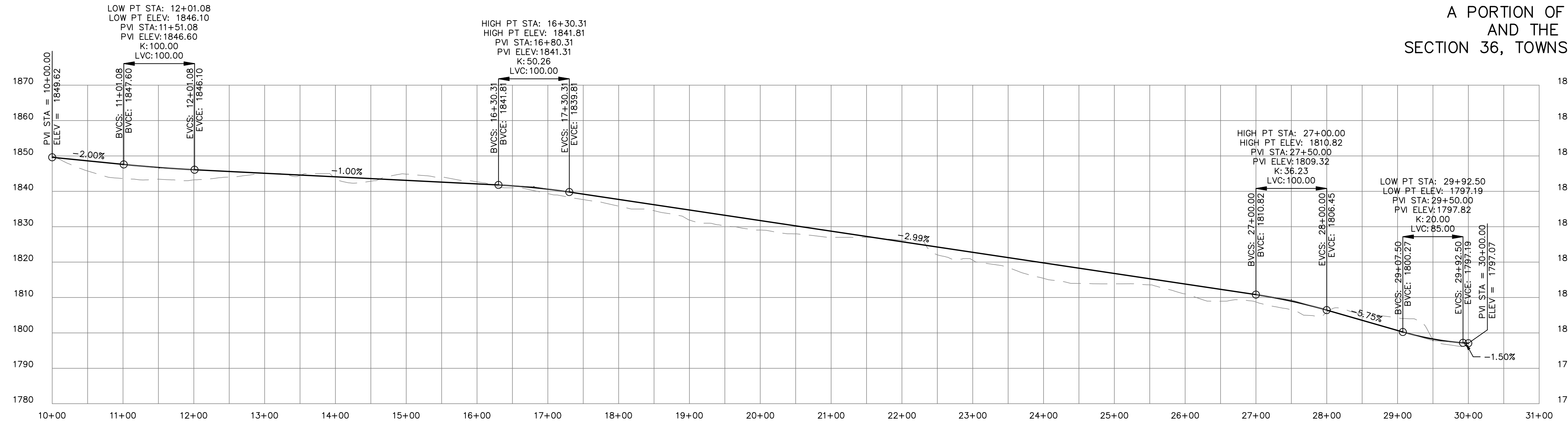
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DATE	09/30/2020	SCALE	1" = 80'
FIELD BOOK	NA	DRAWN	LMC
PROJECT NUMBER	19-342	DRAWING NO.	2 OF 3

CONCEPT ROAD PROFILES

LATAH GLEN RESIDENTIAL COMMUNITY

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AND THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER,
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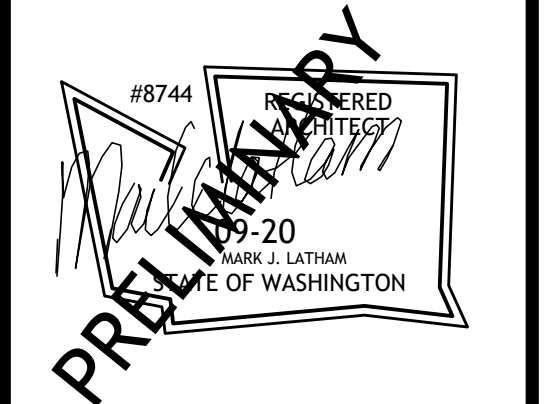
19-342 UTILITY CONCEPT S.dwg PROFILE



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DATE	09/30/2020	SCALE	1" = 80'
FIELD BOOK	NA	DRAWN	LMC
PROJECT NUMBER	19-342	DRAWING NO.	3 OF 3



PROJECT NUMBER: 20-69
DATE: 09-08-20
DRAWN: MJL
REVISIONS: ...

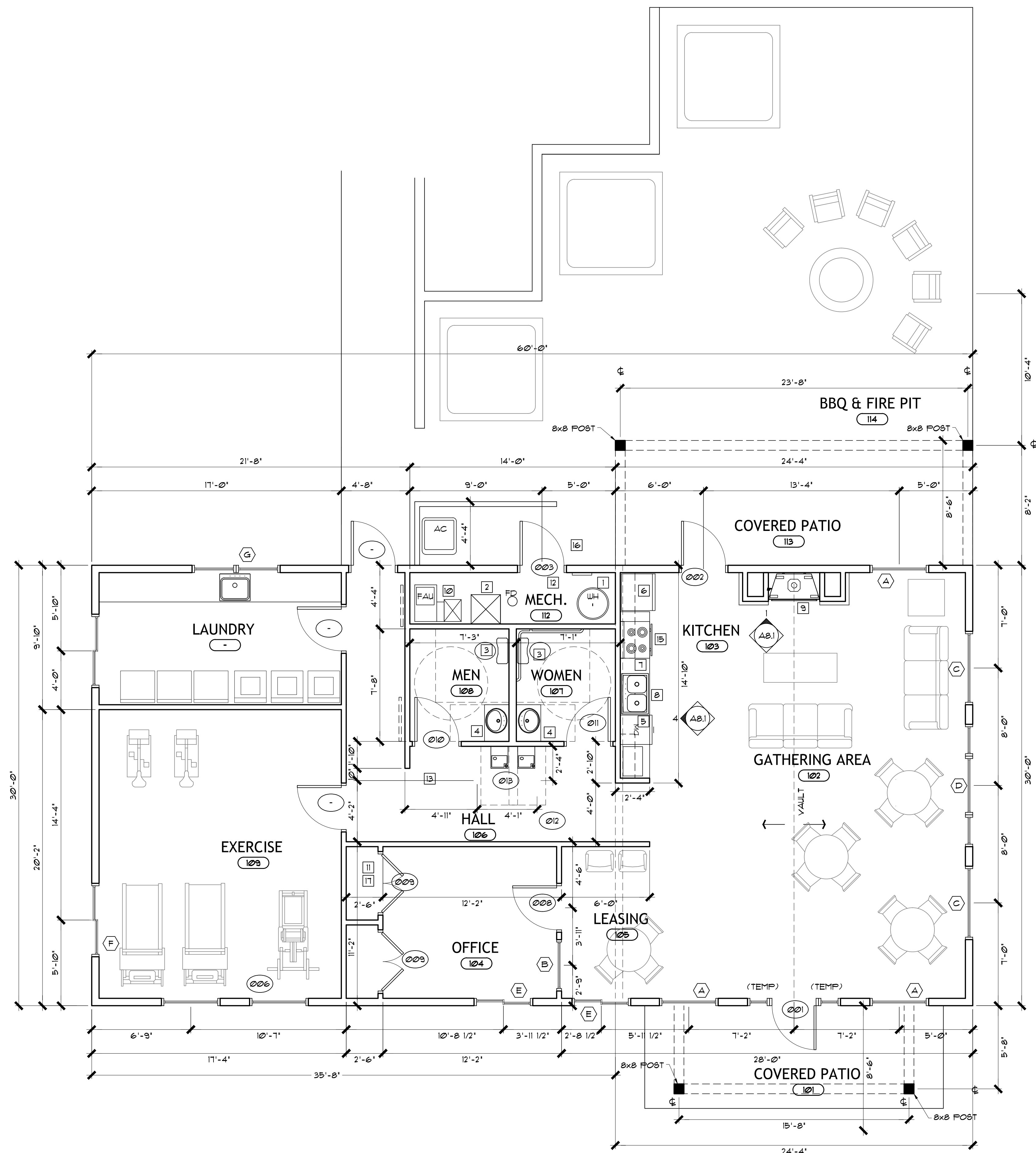
**COMMUNITY CLUBHOUSE FOR
LATAH GLEN
S. INLAND EMPIRE WAY
SPOKANE, WA**

FLOOR PLAN
KEYED NOTES

A2.1

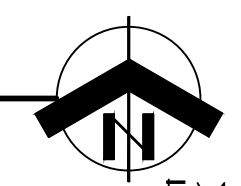
KEYED NOTES

- 1 ELECTRIC WATER HEATER - SEE PLUMBING FOR SIZE
 - 2 24" X 24" MAX. SIZE FIBERGLASS OR ACRYLIC JANITOR SINK - EXTRA DEEP, INTEGRAL LEGS, GOOSENECK FAUCET.
 - 3 ADA ELONGATED TOILET - COLOR: WHITE.
 - 4 SELF RIMMING SINK ON WOOD VANITY. PROVIDE SLAB MIRROR.
 - 5 24" UNDER COUNTER DISHWASHER.
 - 6 30" WIDE TOP/BOTTOM REFRIGERATOR
 - 7 STANDARD 2'-10" TALL ACCESSIBLE WOOD BASE CABINET WITH 30" TALL UPPER CABINETS, PLASTIC LAMINATE COUNTER.
 - 8 DOUBLE BASIN S.S. SINK, ADA LEVER STYLE HANDLE, GOOSENECK FAUCET, SPRAY WAND. NO DISPOSAL.
 - 9 PROPANE GAS ZERO CLEARANCE FIREPLACE, TELEVISION ABOVE. MAINTAIN CLEARANCE FROM TV TO FIREPLACE PER MFR RECOMMENDATIONS. DIRECT VENT FP TO EXTERIOR AND PROVIDE COMBUSTION AIR.
 - 10 FORCED AIR UNIT HEATING/COOLING SYSTEM. REFER TO MECHANICAL
 - 11 FIRE CONTROL AND ALARM SYSTEM FOR THIS BUILDING AND ENTIRE DEVELOPMENT. SURFACE MOUNT ON WALL.
 - 12 ELECTRICAL PANEL.
 - 13 ACCESSIBLE HIGH/LOW DRINKING FOUNTAINS.
 - 14 PROPANE GAS ZERO CLEARANCE FIREPLACE. PROVIDE EXPANDED METAL LATH PROTECTION OVER GLASS. TV ABOVE W/STEEL DOOR, LOCK.
 - 15 30" RANGE W/OVEN BELOW.
 - 17 PORTABLE 20LB OR 40LB PROPANE TANK ON CONCRETE PAD, DIRECT FLUMB GAS LINE TO (2) FIREPLACES WITH SHUTOFFS PER CODE.
- IRRIGATION SPRINKLER CONTROLS FOR ENTIRE DEVELOPMENT.



FLOOR PLAN
SCALE: 1/4" = 1'-0"

CLUBHOUSE: 1,800 SF.



F.V. w/SITE

PRELIMINARY - NOT FOR CONSTRUCTION

KEYED NOTES

DIVISION 2 - SITE WORK - Refer to Site Plan

DIVISION 3 - CONCRETE - Refer to Foundation Plan

3a FOUNDATION DAMP PROOFING

At all Foundation walls: Collection system at bottom consisting of 4" plastic perforated drainpipe with filter fabric wrapped up wall.

3b CONCRETE DECK SURFACE:

Standard Weight Concrete over rubber membrane over plywood. See details. Concrete Finish: Broom or Broom Surl. Tint: None. Stain: None. OPTION: Waterproofing Membrane Product.

DIVISION 4 - MASONRY

4a STONE VENEER:

El Dorado or equal synthetic brick veneer or Thin Brick Product as shown on drawings. Style and color to be selected during construction. Standard tooled grout joint. No sealant. OPTION: $\frac{3}{8}$ " LP Composite Wood sheet w/ corner trim. Paint.

4b CAPS AND SILLS:

Use synthetic stone caps by El Dorado. Color: Beige or Tan.

DIVISION 5 - METALS

DIVISION 6 - WOOD AND PLASTICS

AIR BARRIER - 'Tyvek' Weather Resistive barrier or approved equal building wrap at all exterior walls.

6a SIDING: Cement Fiber or Composite Wood Lap Siding as selected by owner. Style to be 'woodgrain.' Exposure: 1 inches. Stain or paint per division 9

6b SIDING: 7/8" deep wavy corrugated metal. Color: Patina Green or Antique Bronze.

6c WINDOW TRIM: Heads, Jambes, Sills: 5/4 X 4 composite wood w/ mitred corners. No Sill Projection.

6d BELLY BAND: 5/4 X 12 Composite Wood trim - paint.

6e TRIM BANDS: 5/4 X 6 Composite Wood trim - paint.

6f EXPOSED TIMBERS: 8 X 12 Beams 4 8 X 8 Posts, Doug fir #1 or better S4S - stain.

6g TIMBER COLUMNS: 8 X 8 S4S Doug fir #1 or better. Stain.

6h FASCIA BOARDS: 1 X 10 Composite wood trim. Woodgrain texture

6j SOFFIT: 3/8" thick Composite Wood sheet product (LP Smartite or similar) with grooved vents.

6k EXPOSED FACIA: Drip Edge over 1x3 or 1x4 over exposed 4x10 Timber Rafters.

6l SOFFIT: T4G Fir or Cedar - Stain.

DIVISION 7:

ROOFING - Refer to roof plan

RAIN GUTTERS - Refer to roof plan

DIVISION 8 - DOORS AND WINDOWS

DOORS, WINDOWS, GARAGE DOORS, HARDWARE: Refer to Door, Window and Hardware schedule.

DIVISION 9 - FINISHES

EXTERIOR PAINTING

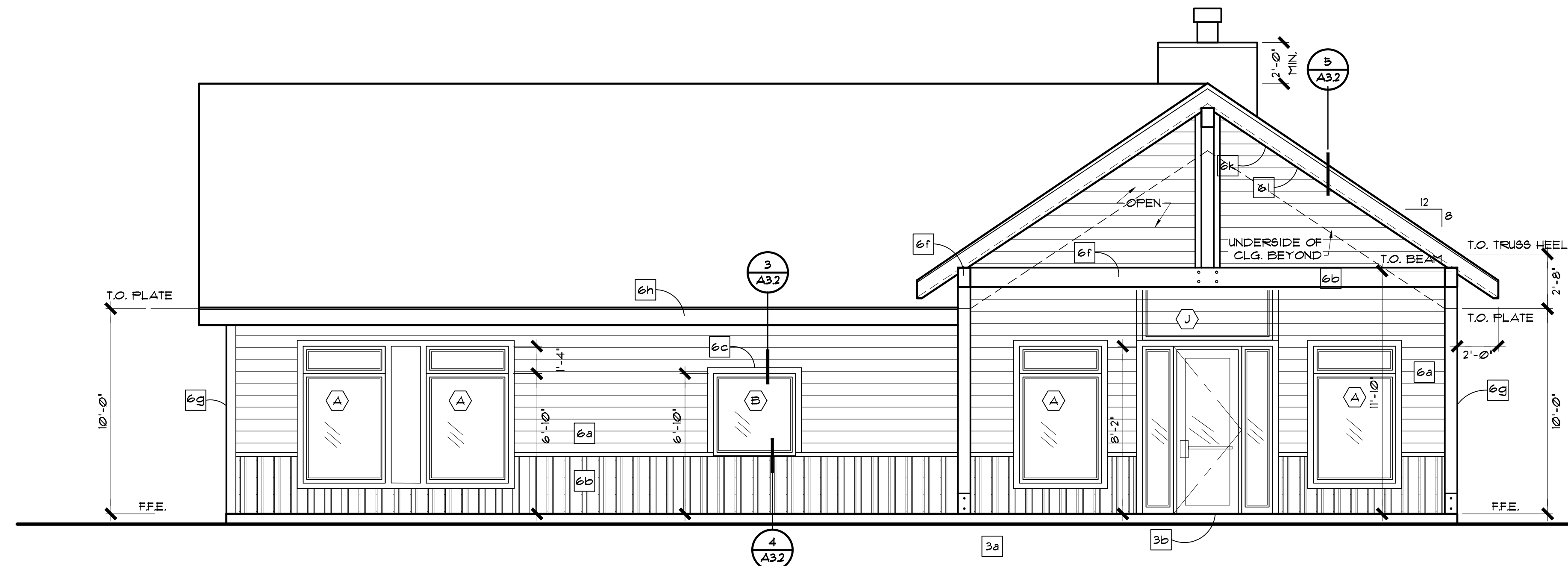
SIDING AND TRIM: Shall be pre-stained both sides with one (1) coat semi-transparent stain. Provide one (1) coat semi-transparent stain after siding and trim has been installed. Color to be selected.

SIDING AND COMPOSITE TRIM: One (1) coat primer and two (2) coats exterior grade 100% acrylic latex paint, sprayed, rolled or brushed as per manufacturer's recommendations. Color to be selected.

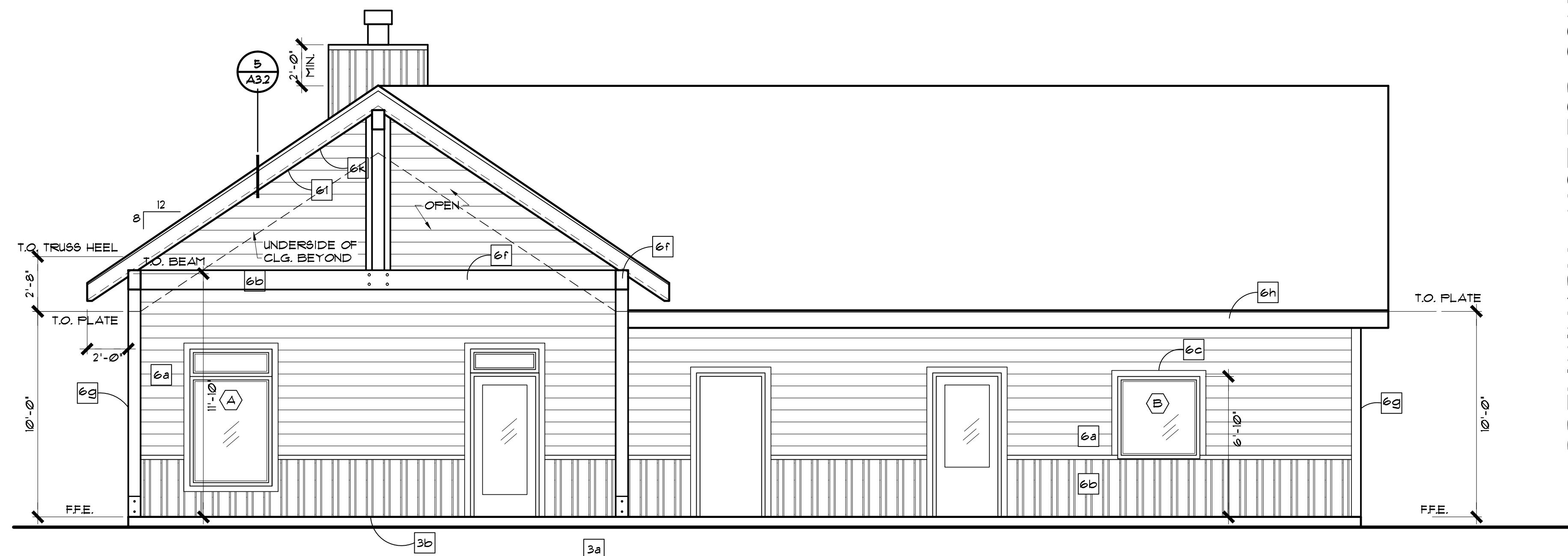
WOOD TIMBERS: Shall be painted to match trim with two (2) coats solid body pigmented stain.

EXTERIOR GRILLES AND VENTS: grilles and vents in walls and roofs shall be painted to match color of roof or flashing.

STEEL & FIBERGLASS ENTRY DOORS: shall be painted with color to match trim.



1
A3.1 FRONT ELEVATION
SCALE: 1/4" = 1'-0"



2
A3.1 BACK ELEVATION
SCALE: 1/4" = 1'-0"

PRELIMINARY - NOT FOR CONSTRUCTION

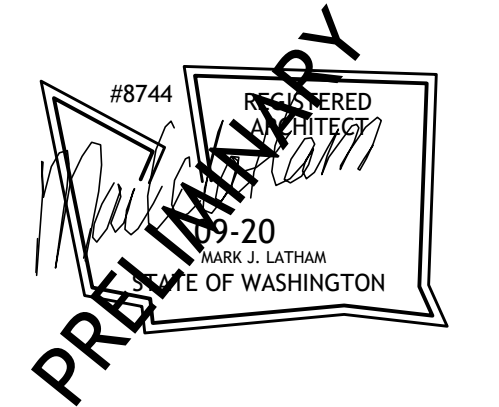
COMMUNITY CLUBHOUSE FOR
LATAH GLEN
S. INLAND EMPIRE WAY
SPOKANE, WA

ELEVATIONS
KEYED NOTES

A3.1

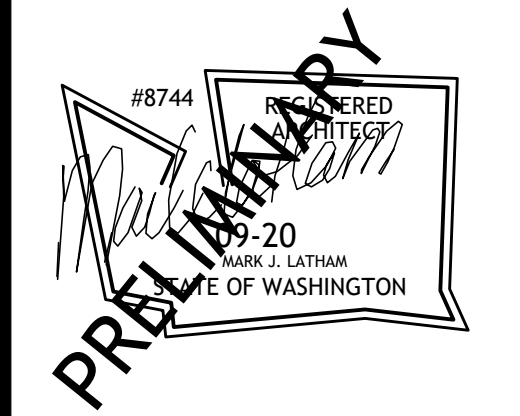
ML
ARCHITECT
& ASSOCIATES

607 E. 6th Ave #101
Post Falls, Idaho 83854
PH: (208) 773-9864
mark@ml-architect.com



PROJECT NUMBER: 20-69
DATE: 09-08-20
DRAWN: MJL
REVISIONS: ...

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PROJECT NUMBER: 20-69
DATE: 09-08-20
DRAWN: MJL
REVISIONS: ---

COMMUNITY CLUBHOUSE FOR
LATAH GLEN
S. INLAND EMPIRE WAY
SPOKANE, WA

ELEVATIONS
DETAILS

A3.2

1 LEFT SIDE ELEVATION

SCALE: 1/4" = 1'-0"



2 RIGHT SIDE ELEVATION

SCALE: 1/4" = 1'-0"

PRELIMINARY - NOT FOR CONSTRUCTION

LATAH GLEN PUD**1 - Program Review/Collaborative Workshop****Design Review Staff Report**

August 7, 2020

**Staff:**

Dean Gunderson, Senior Urban Designer

Taylor Berberich, Urban Designer

Neighborhood & Planning Services
808 W. Spokane Falls Blvd.
Spokane, WA 99201**Applicants:**William Nascimento
Laguna Canyon Group, LLCAgent:
William Sinclair
Storhaug Engineering**Design Review Board Authority****Spokane Municipal Code [Chapter 04.13](#) Design Review Board**

A. Purpose. The design review board is hereby established to:

1. improve communication and participation among developers, neighbors and the City early in the design and siting of new development subject to design review under the Spokane Municipal Code;
2. ensure that projects subject to design review under the Spokane Municipal Code are consistent with adopted design guidelines and help implement the City's Comprehensive Plan.
3. advocate for the aesthetic quality of Spokane's public realm;
4. encourage design and site planning that responds to context, enhances pedestrian characteristics, considers sustainable design practices, and helps make Spokane a desirable place to live, work and visit.
5. provide flexibility in the application of development standards as allowed through development standard departures; and
6. ensure that public facilities and projects within the City's right of way:
 - a. wisely allocate the City's resources,
 - b. serve as models of design quality

Under SMC [Section 17G.040.020](#) **Design Review Board Authority**, other developments or projects listed within the Unified Development Code that require design review, are subject to design review. More specifically, the following section of code specifies the requirement of the design review process for Planned Unit Developments as a requirement of the decision criteria.

[Section 17G.060.170\(4\)\(b\)](#) Decision Criteria | PUD and Plans-in-lieu

Architectural and Site Design.

The proposed development has completed the design review process and the design review committee/staff has found that the project demonstrates the use of innovative, aesthetic, and energy-efficient architectural and site design.

Recommendations of the Design Review Board must be consistent with regulatory requirements per [Section 17G.040.080](#) **Design Review Board**.

Recommendations.

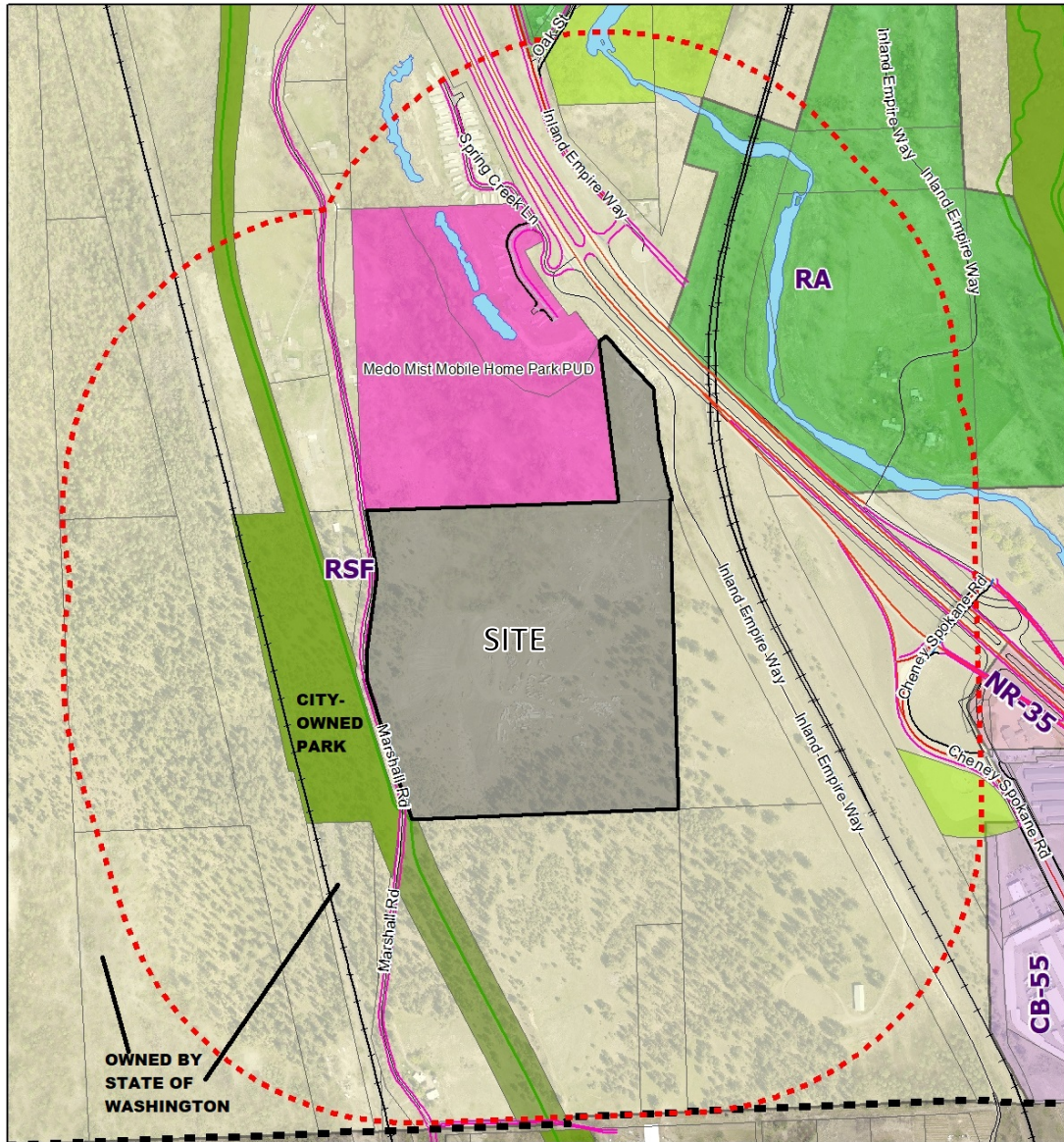
Recommendations of the Design Review Board will be forwarded to the Planning Director < Hearing Examiner, and the chair of the Latah/Hangman Neighborhood Council.

Project Description

Please see applicant's submittal information. It should be noted that the applicant is requesting a PUD approval (and therefor subjecting the development to design review) as the applicant is requesting

variances to several Street Design Standards. As such variances are not permitted under a Manufacture Home Park permit, the applicant is seeking approval of the development through the PUD process. As the site will not be subdivided, no plat application is being reviewed with this PUD application.

Location & Context



LATAH GLEN PUD- 1/4 MILE BUFFER

Legend

- City of Spokane
- Medo Mist MHP
- 1/4 Mile Radius
- Site
- Trail
- Railroad
- City Park
- City Owned Property

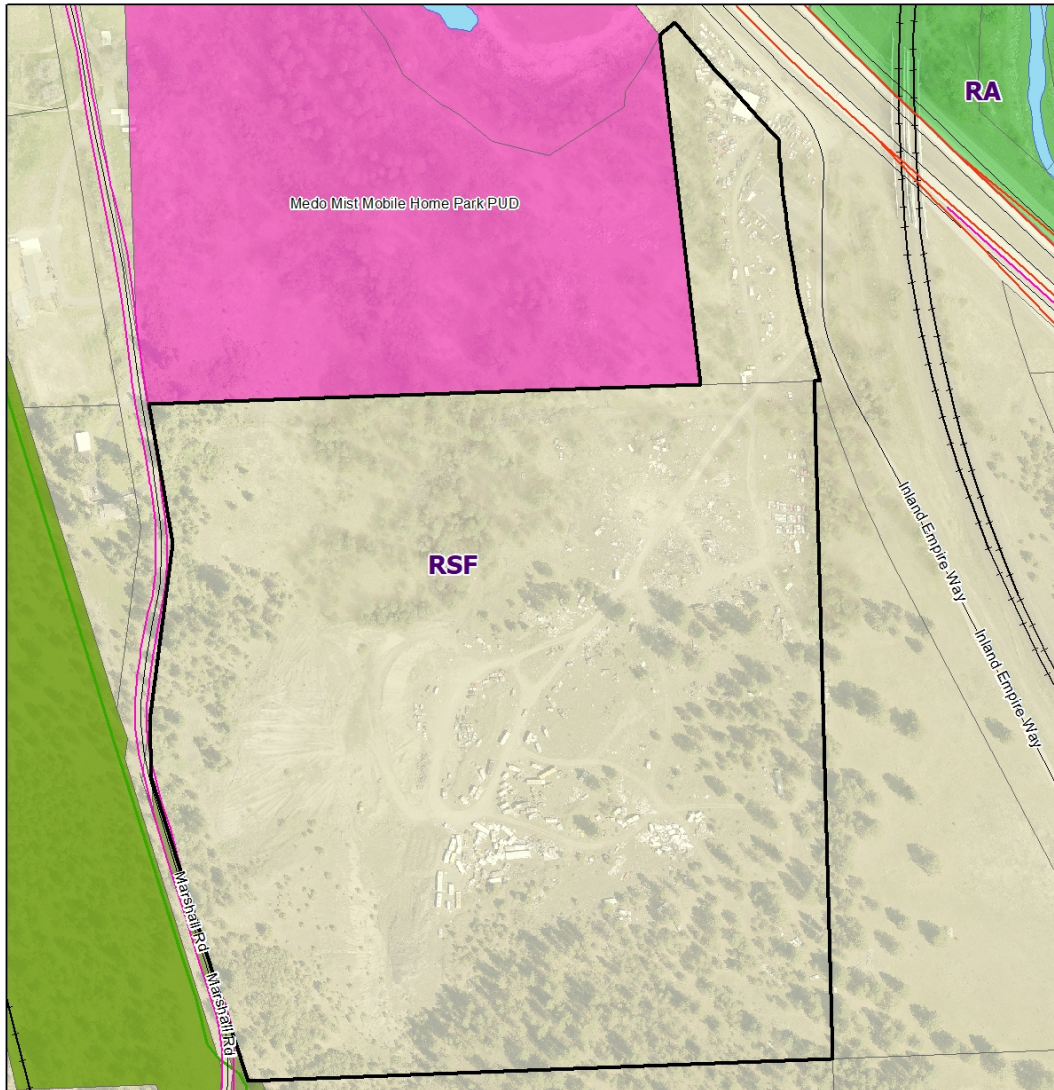
Zoning

- Community Business
- Neighborhood Retail
- Residential Agricultural
- Residential Single-Family

The Subject Site can be identified as parcel numbers 25364.0001 (the large main parcel) and 25361.0004 (the small parcel northeast of the main parcel) located on South Inland Empire Way in the Latah/Hangman Neighborhood Council. The parcel directly north contains the Medo-Mist Mobile Home










Park (approved as a Manufactured Home PUD in 2008). The Fish Lake Trail abuts the main parcel to the southwest. The Subject Site is zoned Residential Single Family (RSF) and is surrounded by RSF zoning. The BNSF Railroad does not directly border the site, but is near the east property line, and the Union Pacific Railroad is located near the western property line.

Character Assets



LATAH GLEN PUD- Site Context

Legend

	Medo-Mist MHP	Zoning
	Site	 Community Business
	Trail	 Neighborhood Retail
	Railroad	 Residential Agricultural
	City Park	 Residential Single-Family

The site has widely varying topography (with nearly 180' of elevation change) with intermittent stands of Ponderosa Pine. There is an existing pole building on the site that is used by the auto salvage business currently using the property. The site plan indicates this building will be removed, as well as all salvage

vehicles. The applicant is proposing that an unpaved portion of Marshall Road (along the western boundary of the site) will be utilized as secondary gated emergency vehicle access. The fully improved Fish Lake Trail is located within 35' of the Subject Site for nearly 700' of the site's western parcel line, with a trailhead connection intersecting Marshall Road near the site's southwest corner.

Regulatory Analysis

Zoning Code Requirements

The two parcels are zoned Residential Single Family (RSF). The applicant will be expected to meet zoning code requirements. Applicants should contact Current Planning Staff with any questions about these requirements.

Recommendations of the Design Review Board must be consistent with adopted regulations. The DRB may not waive any code requirements.

The PREDEV report is attached at the end of this staff report.

Note that some additional discussions between the applicant and Development Services Center staff have occurred since the Pre-Development Meeting. The applicant's submittal for design review is the applicant's attempt to reflect these subsequent discussions.

The Urban Forestry Report is attached, following the PREDEV report.

PUD Code Requirements

The applicant is seeking permission to develop the Subject Site as a Planned Unit Development (PUD) / Manufacture Home Park (MHP); which, if granted, will permit some flexibility in the development's design elements. This latitude is provided for in the following portions of development code:

Section 17G.070.010 Purpose

A. General Purpose.

The purpose of the planned unit development provisions are to encourage innovative planning and flexible design standards that results in more infill and mixed use development; economically diverse and affordable housing options; improved protection of open space and critical areas and transportation options and preserve the existing landscape and amenities that may not otherwise be protected through conventional development. These provisions provide:

1. Flexibility.
Provide a means for creating planned environments through the application of flexible standards, such as modifications to permitted uses and site development standards that facilitates development that is of a type, scale, orientation and design that maintains or improves the character, economic development and aesthetic quality and livability of the neighborhood.
2. Efficiency.
Design that facilitates the efficient use of land, urban infill, transportation alternatives that promotes pedestrian, bicyclist and public transit and encourages energy conservation.
3. Affordable Housing.
Flexible design standards that encourage affordable housing in all types of neighborhoods that is in an environment that is safe, clean and healthy. This is accomplished through the provision of flexibility in utility design standards, road design standards, site development standards, zoning density and permitted uses.
4. Diverse Housing.
Promote urban infill and a wide range of housing types and housing diversity to meet the social, economic and functional needs of our community in all areas of the City.
5. Open Space.
To acquire, operate, enhance and protect a diverse system of parks, trails, view sheds,

corridors, parkways, urban forests, recreational, cultural, historic and open space areas for the enjoyment and enrichment of all.

6. Economic Feasibility.
Increase economic feasibility and encourage revitalization and investment by fostering the efficient arrangement of land use allowing flexible site circulation and road standards; and allowing flexibility in utility design.
7. Resource Preservation.
Preserve critical areas and agriculture through the use of a planning procedure that can tailor the type and design of a development to a particular site.

Section 17G.070.100-150 Design Standards

Section 17G.070.120 Significant Features

Section 17G.070.125 Site Preparation

Section 17G.070.130 Landscaping

Section 17G.070.135 Compatibility with Surrounding Areas

Section 17G.070.140 Community Environment

Section 17G.070.145 Circulation

Section 17G.070.150 Lighting

Manufactured Home Parks Code Requirements

Chapter 17C.345 Manufactured Homes and Mobile Home Parks

Section 17C.345.120 Development Standards for Mobile Home Parks

- A. Uses.
 1. Manufactured homes or mobile homes, on a condominium basis, or on leased lots.
 2. Accessory buildings, such as laundry, grounds maintenance shop, recreation, restroom and swimming pool.
 3. Motor homes, recreational or camping vehicles and trailers are not permitted.
- B. Lot Area.

A park must have a minimum area of ten acres.
- C. Setbacks.
 1. All manufactured homes, and extensions thereof, accessory structures and other buildings must be set back on a leased lot as follows:
 - a. Twenty feet from the boundary of the park.
 - b. Twenty feet from a public street.
 - c. Ten feet from an interior private or public street, walking or parking area; and
 - d. Ten feet from any other manufactured home.
- D. Open Space.

At least fifteen percent of the gross site area must be in open space or recreational areas available for use by all residents. Parking, driving and setback areas and areas less than five thousand square feet do not count as required open space.
- E. Density.

The density minimum is four units per acres to a maximum density of ten units per acre.
- F. Buffer Strips.

A twenty foot strip around the boundary of the manufactured home park or manufactured home subdivision must be landscaped to provide a visual screen. All open spaces and other unimproved areas must be suitably landscaped. All landscaping must be maintained and furnished with an automatic sprinkler system.
- G. Landscaping Areas.

Requirements for landscaping are stated in [chapter 17C.200 SMC](#), Landscaping and Screening.
- H. Signs.

One freestanding identification sign may be erected along each major approach to the park so long as such sign:

- a. does not exceed an area of fifteen square feet;
- b. does not exceed twenty feet in height;
- c. sets back from the street at least twenty feet; and
- d. is of low-intensity illumination and not flashing or animated.

I. Parking.

Paved off street parking must be provided at the ratio of one and one half space per manufactured home. At least one space must be at the manufactured home or mobile home stand. Other spaces may be in a common parking area so long as each space is within two hundred feet of the manufactured home or mobile home stand to which it relates.

J. Pedestrian Access.

There must be a paved or graveled system of walkways, which gives safe and convenient access to every manufactured home and all common facilities. Sidewalks developed in conjunction with public or private streets may meet this requirement.

K. Utilities.

The park must be connected to the city water and sewer systems, individual on-site wells and septic tanks are not allowed. Utility lines are required to be under ground.

L. Streets.

Each lease lot, space or unit must be adjacent to a public or private street. Both public and private streets are approved by the director of engineering services and are required to meet the requirements of [chapter 17G.010 SMC](#). Deviations to the public or private street standards, curbing, sidewalks, lighting, pedestrian buffer strips and other street standards are not permitted through a mobile home park approval.

M. Installation.

Each manufactured home or mobile home must be securely installed upon a stand and must be skirted to conceal the undercarriage.

N. Accessory Structures.

Requirements for accessory structures are stated in [SMC 17C.110.225](#), Accessory Structures.

City of Spokane Comprehensive Plan

[Comprehensive Plan link](#)

Urban Design Staff finds the following chapters and goals from the Spokane Comprehensive Plan relevant to the project and/or within the project's potential to implement:

Chapter 3: Land Use

LU 1 CITYWIDE LAND USE

LU 1.16 Mobile Home Parks: Designate appropriate areas for the preservation of mobile and manufactured home parks.

LU 2 PUBLIC REALM ENHANCEMENT

LU 2.1 Public Realm Features: Encourage features that improve the appearance of development, paying attention to how projects function to encourage social interaction and relate to and enhance the surrounding urban and natural environment.

LU 5 DEVELOPMENT CHARACTER

LU 5.1 Built and Natural Environment: Ensure that developments are sensitive to the built and natural environment (for example, air and water quality, noise, traffic congestion, and public utilities and services), by providing adequate impact mitigation to maintain and enhance quality of life.

LU 5.2 Environmental Quality Enhancement: Encourage site locations and design features that enhance environmental quality and compatibility with surrounding land uses.

LU 5.3 Off-Site Impacts: Ensure that off-street parking, access, and loading facilities do not adversely impact the surrounding area.

LU 6 ADEQUATE PUBLIC LANDS AND FACILITIES

LU 6.9 Facility Compatibility with Neighborhood: Ensure the utilization of architectural and site designs of essential public facilities that are compatible with the surrounding area.

Chapter 4: Transportation

TR GOAL B: PROVIDE TRANSPORTATION CHOICES

TR GOAL C: ACCOMMODATE ACCESS TO DAILY NEEDS AND PRIORITY DESTINATIONS

TR 1 Transportation Network For All Users: Design the transportation system to provide a complete transportation network for all users, maximizing innovation, access, choice, and options throughout the four seasons. Users include pedestrians, bicyclists, transit riders, and persons of all abilities, as well as freight, emergency vehicles, and motor vehicle drivers. Guidelines identified in the Complete Streets Ordinance and other adopted plans and ordinances direct that roads and pathways will be designed, operated, and maintained to accommodate and promote safe and convenient travel for all users while acknowledging that not all streets must provide the same type of travel experience. All streets must meet mandated accessibility standards. The network for each mode is outlined in the Master Bike Plan, Pedestrian Master Plan, Spokane Transit's Comprehensive Plan, and the Arterial Street map.

TR 14 Traffic Calming: Use context-sensitive traffic calming measures in neighborhoods to maintain acceptable speeds, manage cut-through traffic, and improve neighborhood safety to reduce traffic impacts and improve quality of life.

Chapter 8: Urban Design & Historic Preservation

DP 1.3 Significant Views and Vistas: Identify and maintain significant views, vistas, and viewpoints, and protect them by establishing appropriate development regulations for nearby undeveloped properties.

DP 2 URBAN DESIGN

DP 2.5 Character of the Public Realm: Enhance the livability of Spokane by preserving the city's historic character and building a legacy of quality new public and private development that further enriches the public realm.

DP 2.6 Building and Site Design: Ensure that a particular development is thoughtful in design, improves the quality and characteristics of the immediate neighborhood, responds to the site's unique features - including topography, hydrology, and microclimate - and considers intensity of use.

DP 2.15 Urban Trees and Landscape Areas: Maintain, improve, and increase the number of street trees and planted areas in the urban environment.

DP 2.21 Lighting: Maximize the potential for lighting to create the desired character in individual areas while controlling display, flood and direct lighting installations so as to not directly and unintentionally illuminate, or create glare visible from adjacent properties, residential zones or public right-of-way.

Chapter 9: Natural Environment

NE 4.3 Impervious Surface Reduction: Continue efforts to reduce the rate of impervious surface expansion in the community.

NE 5 CLEAN AIR

NE 5.5 Vegetation: Plant and preserve vegetation that benefits local air quality.

NE 12 URBAN FOREST

NE 12.1 Street Trees: Plant trees along all streets.

NE 13 CONNECTIVITY

NE 13.1 Walkway and Bicycle Path System: Identify, prioritize, and connect places in the city with a walkway or bicycle path system.

NE 13.2 Walkway and Bicycle Path Design: Design walkways and bicycle paths based on qualities that make them safe, functional, and separated from automobile traffic where possible.

NE 13.3 Year-Round Use: Build and maintain portions of the walkway and bicycle path systems that can be used year-round.

NE 15 NATURAL AESTHETICS

NE 15.1 Protection of Natural Aesthetics: Protect and enhance nature views, natural aesthetics, sacred areas, and historic sites within the growing urban setting.

NE 15.2 Natural Aesthetic Links: Link local nature views, natural aesthetics, sacred areas, and historic sites with the trail and path system of the city.

Chapter 10: Social Health

SH 3 ARTS AND CULTURAL ENRICHMENT

SH 3.4 One Percent for Arts: Encourage private developers to incorporate an arts presence into buildings and other permanent structures with a value of over \$25,000 by allocating one percent of their project's budget for this purpose.

Chapter 11: Neighborhoods

N 2 NEIGHBORHOOD DEVELOPMENT

N 2.1 Neighborhood Quality of Life: Ensure that neighborhoods continue to offer residents transportation and living options, safe streets, quality schools, public services, and cultural, social, and recreational opportunities in order to sustain and enhance the vitality, diversity, and quality of life within neighborhoods.

N 2.4 Neighborhood Improvement: Encourage revitalization and improvement programs to conserve and upgrade existing properties and buildings.

N 2.5 Neighborhood Arts: Devote space in all neighborhoods for public art, including sculptures, murals, special sites, and facilities.

N 4 TRAFFIC AND CIRCULATION

N 4.5 Multimodal Transportation: Promote a variety of transportation options to reduce automobile dependency and neighborhood traffic.

N 4.6 Pedestrian and Bicycle Connections: Establish a continuous pedestrian and bicycle network within and between all neighborhoods.

N 4.7 Pedestrian Design: Design neighborhoods for pedestrians.

N 4.9 Pedestrian Safety: Design neighborhoods for pedestrian safety.

Topics for Discussion

The following Topics for Discussion are broken into two categories for ease of consideration. The first set of topics address the Design Review Board's role in determining whether the proposed development achieves "innovative, aesthetic, and energy-efficient architectural and site design" per [SMC 17G.060.170.D.4\(b\)](#).

The second set of topics deal mostly with the various Design Standards for both Planned Unit Developments and Manufactured Home Parks. The focus in these topics rests on the possibility that Design Departures may be sought by the applicant for some of the (R) requirements or (P) presumptions per [SMC 17G.030](#). The applicant may already be aware of some of these criteria and intends to address these as the development's design evolves, with solutions provided in the subsequent Recommendation Meeting(s) submission. There are some overlaps between the two categories of topics.

Note: If any Design Departures are contemplated by the applicant these should be proposed with sufficient time to ensure that the Design Review Board's findings meeting on the request can coincide with the project's public Recommendation Meeting(s).

Applicant's responses to the Topics are in **blue**, any staff additional comments are italicized and are in **green**.

Innovative, Aesthetic, and Energy-efficient Architectural and Site Design

1. Is there an opportunity to increase pedestrian and bicycle connectivity for future residents to the Fish Lake Trail? While this may address the pedestrian circulation connectivity requirement (R) in [SMC 17G.070.145.B.1 Circulation](#) this topic mostly deals with innovation and energy-efficiency as Fish Lake Trail is a Class 1 Shared Use Path within the City and offers the potential for access to alternative transportation residents.

Applicant's response: The Concept Site Plan depicts pedestrian access to the Marshall Road public right-of-way. A resident may use this public right-of-way to access the Fish Lake Trail and other interconnected public rights-of-way as desired. For community security and privacy, it's our preference to avoid direct connection.

2. The applicant is required to depict a higher level of aesthetic performance for a PUD than would be obtained with a typical residential development. What additional aesthetic contributions are proposed for the homes – beyond those already required of such structures located in a Manufactured Home Park? Are there additional architectural features that could be provided to meet a PUD's aesthetic demands (e.g., larger covered front porches)?

Applicant's response: We have proposed homes with high aesthetic contribution as these are equivalent to site built homes in design incorporating porches and design features. Given that, pricing is also very important as we are striving for affordability. Therefore, we've struck a balance between aesthetics and price. Also, these are home concepts and homeowners will have a choice of which home to buy. We are not planning to restrict buyers in selecting their homes.

Staff comment: *It should be noted that the applicant is choosing to submit the development to the higher aesthetic standards of a Planned Unit Development. Such aesthetic performance criteria are not relative to what might be expected in a Manufactured Home Park, or attenuated based on unit cost.*

3. The applicant is required to depict a greater level of energy-efficiency performance for a PUD than in a typical residential development. What additional energy efficient attributes are proposed for the manufactured homes and site development – beyond those already required by compliance with the Energy Code? Previous PUD applicants have proposed photovoltaic energized street lights, and "solar-ready" structures.

Applicant's response: The manufactured homes have a small footprint and will have significantly lower energy consumption than the typical single-family home. We support solar power where feasible and allowed and are committed to supplementing the park power demand with solar including site lighting and the clubhouse.

Staff comment: The energy-efficiency performance for structures in a PUD are in comparison to identically-sized structures constructed outside of a PUD. That is, similarly sized homes outside a PUD would not have a higher energy-efficiency obligation beyond that required by the energy code.

4. The applicant is required to depict a greater level of innovation for a PUD than would be obtained with a conventional residential development. As the development layout poses significant grade changes and stormwater management will offer a unique set of challenges, what innovative stormwater techniques is the applicant proposing (e.g., permeable pavement, stormwater swales and weirs adjacent to the roadways, Spokanescape landscaping in common areas, etc.)? Such innovative improvements may also help the project comply with the requirements (R) and presumptions (P) of [SMC 17G.070.125.B Site Preparation](#).

Applicant's comment: The stormwater facilities on the project site will be designed per the Spokane Regional Stormwater Manual (SRSM). The majority of the site will have stormwater collected via catch basins and pipes, which will carry the stormwater to the treatment swales as shown on the conceptual stormwater plan. Treatment swales are placed throughout the site, favoring areas with slopes that are conducive to swale grading. Low Impact Development stormwater features may be used, where appropriate.

Design Criteria

5. Although Spokane Transit Authority (STA) does not currently provide bus service along Highway 195, because the development is proposed as a 55+ community is there an opportunity to provide adequate on-street queuing for shuttle buses near the Community Center? Would a bench or shelter be a community benefit? ([SMC 17G.070.145.B.6 Circulation](#), [SMC 17G.070.010.A.2 Purpose, and Efficiency](#), [SMC 17G.070.115.B.3 Plan and Code Conformance](#))

Applicant's response: The majority of folks needing pick up service need to be picked up at their front door.

Staff comment: It should be noted that the need to accommodate alternative transportation in the PUD are code-mandated obligations. Paratransit and other such senior service shuttles available throughout the city that provide door-to-door transport are not a means for a PUD applicant to demonstrate how the development's site design will accommodate alternative transportation.

6. The layout of proposed development depicts considerable cuts & fills with the potential of impacting the site's "significant features" (as defined in [SMC 17G.070.120.B Significant Features](#)). What appropriate mitigation is proposed for these impacts? Is there additional information (if any) that could be submitted by the applicant in the Recommendation Meeting submission packet to address this design criteria?

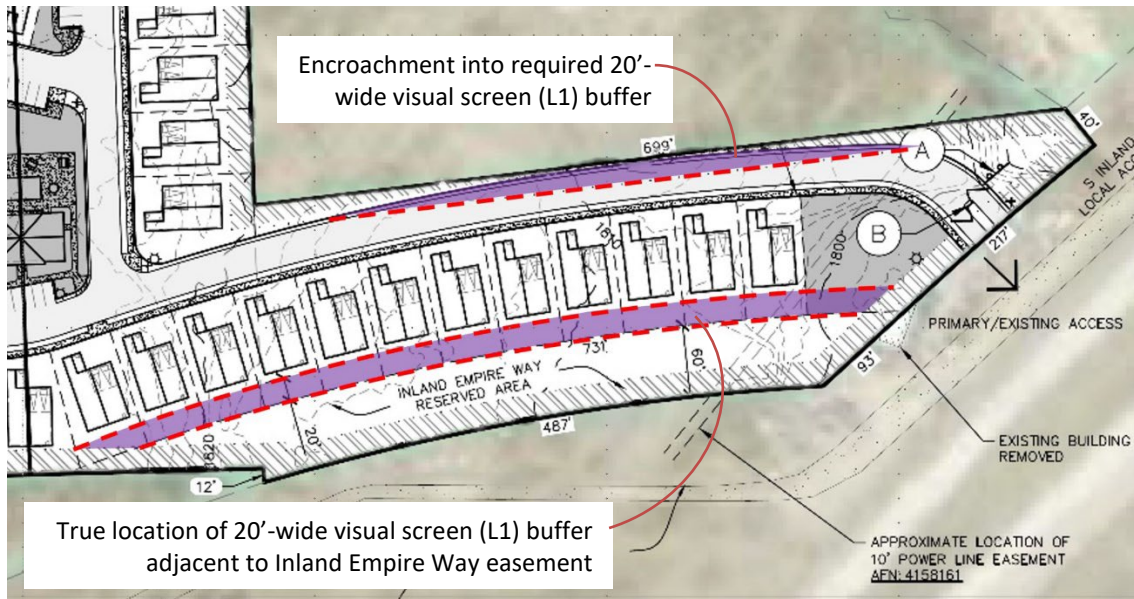
Applicant's response: Grading is needed for the road to access a buildable area near Marshall Road and provides pedestrian and emergency access for the community to the Marshall Road public ROW that does not currently exist. We understand the design proposed to be in an area of lower significance that will limit impacts to significant site features such as mature trees and other existing vegetation with associated existing topography around the property boundary. The project otherwise has been planned to minimize grading.

Private Roads are proposed to service and front the living units in the PUD. Some of the benefits of the private roads include a smaller impervious area footprint and the ability to more closely follow the existing contours to minimize large cuts and fills.

Staff comment: It should be noted that the conservation of existing topography is only one of the "significant features" listed in the code. The range of features to be preserved or whose impact will receive appropriate mitigation are, "areas that are geologically hazardous, wetlands, recharge the aquifer, conserve wildlife habitat or prone to flooding" (Design Standard B.2) – this obligation is a requirement (R).

7. The proposed entrance drive to the PUD/MHP encroaches on the required 20'-wide visual screen landscape buffer (L1) surrounding the Manufacture Home Park per [SMC 17C.345.120.F Buffer Strips](#).

Other than entrance drives that run perpendicular to such a buffer, paved streets are not permitted within the buffer. What opportunities are there to comply with the buffer requirement? (see figure)



Applicant's response: The Inland Empire Way 'Reserved Area' was requested by the City for potential extension of the Inland Empire Way ROW, however, expansion of Inland Empire Way is not a certainty at this time. The adjacent land is currently vacant and would be required to provide a 20' set back. Should the City prefer, we can move our entry road east to provide the 20' but will be unable to provide future open area for Inland Empire Way ROW extension.

Staff comment: The proposed future extension of Inland Empire Way (IEW) is depicted by the applicant as a means to demonstrated the development's compliance with the PUD requirement (R) found in [SMC 17G.070.145.B.2 Circulation](#) and the non-discretionary Street Development Standards found in [SMC 17H.010.030.F Street Layout Design](#). The encroachment of the development's access drive into the 20'-wide visual screen landscape buffer is not driven by the requirement for the IEW extension. There are a number of ways to depict the access drive while continuing to preserve the IEW extension.

8. The development's layout of the homes on the lease areas is not consistent with the applicant's proposed floor plans and site-built garage configurations. Would there be value in the applicant providing the specific home type and garage configuration on the lease areas in the Recommendation Meeting submission packet to ensure that all development and design standards are being addressed?

Applicant's response: The floor plans and graphics depict the developer's vision for the aesthetic of the homes and are not final. It is noted on the plans that actual units may vary and that the garages would be set back to accommodate driveways, more in-line with the typical lease space layout.

We may wish to include small/medium/large lease spaces where a variety of potential homes could fit on the spaces, if they meet minimum aesthetic requirements not unlike the samples provided.

9. It should be noted that the disbursed small parking areas throughout the proposed development would need to comply with the screening and shading requirements (R) in [SMC 17G.070.130.B.3 & 4 Landscaping](#). How is the applicant proposing to meet the 75% shading of the paved parking surfaces in these disbursed parking areas?

Applicant's response: A landscape plan will be prepared as part of the PUD/CUP application to address this; we envision deciduous trees will be planted to provide the needed shade at the parking locations in question.

10. It should be noted that per [SMC 17G.070.135.B.3](#) the Institutional Design Standards found in [SMC 17C.110.500 thru .575](#) apply to all common buildings located within the PUD – this includes both the Clubhouse and the Laundry Building (which has not been shown). What additional information can be provided by the applicant in the Recommendation Meeting submission packet to insure that these common buildings meet these requirements?

Applicant’s response: The clubhouse building depicted offers an aesthetic vision; design of the common buildings will conform to City requirements.

11. Depending on their overall dimensions the built-on-site garages may have to meet the articulation requirement (R) for garages and the privacy preservation presumption (P) for occupants listed in [SMC 17G.070.140.B.3 & 5 Community Environment](#). What additional information can be provided by the applicant in the Recommendation Meeting submission packet to ensure these criteria are being addressed?

Applicant’s response: It is our intent to have the homes oriented toward the street and garages set back from the front of the home. We plan to orient pedestrian access to the side or rear of the homes when they connect to greenbelt walkways. Garages will not exceed 25’ width.

Note

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

Policy Basis


Spokane Municipal Codes
City of Spokane Comprehensive Plan

DESIGN REVIEW BOARD

LATAH GLEN PUD

1 - Program Review/Collaborative Workshop

August 12, 2020

	From : Design Review Board Mark Brower, Vice-Chair (Dean Gunderson, serving as acting Chair for signature purposes) c/o Dean Gunderson, DRB Secretary Neighborhood & Planning Services 808 W. Spokane Falls Blvd. Spokane, WA 99201	To : William Nascimento Laguna Canyon Group, LLC William Sinclair, Storhaug Engineering	CC : Brian T. McGinn , Hearings Examiner Louis Meuler, Interim Planning Director Tami Palmquist, Development Services Melissa Owen, Development Services
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Based on review of the materials submitted by the applicant and discussion during the August 12, 2020 Collaborative Workshop the Design Review Board recommends the following Advisory Actions:

- 1. To promote connectivity and offer a neighborhood asset, the Applicant is encouraged to provide an intentional non-motorized connection from the site to the Fish Lake Trail.**

Please see the following Comprehensive Plan Goals and Policies: LU 2.1 Public Realm Features, LU 5.1 Built and Natural Environment, DP 2.5 Character of the Public Realm, NE 13.1 Walkway and Bicycle Path System, NE 13.2 Walkway and Bicycle Path Design, NE 13.3 Year-Round Use, N 2.1 Neighborhood Quality of Life, and N 4.6 Pedestrian and Bicycle Connections,

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.2 Efficiency, SMC 17G.070.010.5 Open Space, SMC 17G.070.120 Significant Features, SMC 17G.070.135 Compatibility with Surrounding Areas, and SMC 17G.070.145.B.1 Circulation

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.120.D Open Space, and SMC 17C.345.120.J Pedestrian Access.

- 2. The Applicant is encouraged to evaluate the internal sidewalks and pathways and consider opportunities to elevate the pedestrian user experience by introducing benches, nodes, enhanced landscaping, or other means. A network of sidewalks and pathways connecting residents to common buildings, common spaces, and public ways may enhance the overall site design experience.**

Please see the following Comprehensive Plan Goals and Policies: LU 2.1 Public Realm Features, LU 5.1 Built and Natural Environment, DP 2.5 Character of the Public Realm, NE 13.1 Walkway and Bicycle Path System, NE 13.2 Walkway and Bicycle Path Design, NE 13.3 Year-Round Use, N 2.1 Neighborhood Quality of Life, N 4.6 Pedestrian and Bicycle Connections, N 4.7 Pedestrian Design, and N 4.9 Pedestrian Safety.

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.1 Flexibility, SMC 17G.070.010.2 Efficiency, SMC 17G.070.010.5 Open Space, SMC 17G.070.140 Community Environment, and SMC 17G.070.145 Circulation.

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.120.J Pedestrian Access, and SMC 17C.345.120.L Streets.

- 3. The Applicant is encouraged to return with a more fully developed plan illustrating intended innovation in stormwater treatment.**

Please see the following Comprehensive Plan Goals and Policies: LU 5.1 Built and Natural Environment, LU 5.2 Environmental Quality Enhancement, and DP 2.6 Building and Site Design.

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.2 Efficiency, SMC 17G.070.010.7 Resource Preservation, SMC 17G.070.125 Site Preparation, and SMC 17G.070.140 Community Environment.

- 4. The Applicant shall return with a proposed street tree palette.**

Please see the following Comprehensive Plan Goals and Policies: LU 2.1 Public Realm Features, LU 5.1 Built and Natural Environment, LU 5.2 Environmental Quality Enhancement, DP 2.5 Character of the Public Realm, DP 2.15 Urban Trees and Landscape Areas, NE 5.5 Vegetation, NE 12.1 Street Trees, and N 2.1 Neighborhood Quality of Life.

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.5 Open Space, SMC 17G.070.130 Landscaping, and SMC 17G.070.140 Community Environment.

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.120.D Open Space, SMC 17C.345.120.G Landscaping Areas, and SMC 17C.345.120.L Streets.

- 5. The Applicant shall restore the landscape in the areas of the site beyond the lease areas in a manner consistent with the existing and preserved natural areas on site.**

Please see the following Comprehensive Plan Goals and Policies: LU 2.1 Public Realm Features, LU 5.1 Built and Natural Environment, LU 5.2 Environmental Quality Enhancement, DP 2.5 Character of the Public Realm, DP 2.15 Urban Trees and Landscape Areas, NE 5.5 Vegetation, and N 2.1 Neighborhood Quality of Life.

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.5 Open Space, SMC 17G.070.130 Landscaping, and SMC 17G.070.140 Community Environment.

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.120.D Open Space.

- 6. The Applicant is encouraged to explore ways to massage the architectural aesthetic of the proposed structures into a cohesive theme that reflects and enhances the regional character of the area. The Board strongly recommends that a set of design standards for the development be crafted in order to maintain consistency with the established style as the project is built out, and to preserve the presumption of privacy between homes.**

Please see the following Comprehensive Plan Goals and Policies: LU 6.9 Facility Compatibility with Neighborhood, and DP 2.6 Building and Site Design.

Please see the following Planned Unit Development Code Requirements: SMC 120 Significant Features, SMC 17G.070.135 Compatible with Surrounding Areas, and SMC 17G.070.140.B.4 & 5 Community Environment

- 7. The Applicant shall return with a developed entry design, gate design, landscaping and signage, along with the design for any proposed fencing or enclosure of the site.**

Please see the following Comprehensive Plan Goals and Policies: LU 2.1 Public Realm Features, DP 2.5 Character of the Public Realm, and DP 2.18 Bus Benches and Shelters

Please see the following Planned Unit Development Code Requirements: 17G.070.010.1 Flexibility, 17G.070.120 Significant Features, 17G.070.130 Landscaping, 17G.070.140 Community Environment, and 17G.070.145 Circulation.

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.120.D Open Space, SMC 17C.345.120.G Landscaping Ares, and SMC 17C.345.120.H Signs.

- 8. The Board appreciates the introduction of additional affordable housing to the Spokane area.**

Please see the following Planned Unit Development Code Requirements: SMC 17G.070.010.3 Affordable Housing, and SMC 17G.070.010.6 Economic Feasibility.

Please see the following Development Standards for Mobile Home Parks: SMC 17C.345.010 Purpose

- 9. The Board finds the reclamation and renovation of the existing auto wrecking yard to be an innovative reuse of the land.**

Please see the following Comprehensive Plan Goals and Policies: DP 2.6 Building and Site Design, and DP 2.12 Infill Development

Please see the following Planned Unit Development Code Requirements: 17G.070.010.1 Flexibility, 17G.070.010.6 Economic Feasibility, and 17G.070.135 Compatibility with Surrounding Areas.

Advisory Actions were approved by unanimous vote (7/0)



Dean Gunderson, Secretary, Design Review Board (serving as signatory under DRB Rules of Procedure 9.4.G)

Note: Supplementary information, audio tape and meeting summary are on file with City of Spokane Design Review Board.

Design Review Board - Meeting Minutes Draft

November 16, 2020

Online via WebEx

Meeting called to order at 5:30 PM by Kathy Lang

Attendance:

- *Board Members Present:* Chuck Horgan (Arts Commission Liaison), Drew Kleman, Chad Schmidt, Ted Teske, Kathy Lang (Chair & CA Liaison), Mark Brower (Vice-Chair), Anne Hanenburg
- *Board Members Not Present:* Grant Keller
- *Quorum Present:* Yes
- *Staff Members Present:* Dean Gunderson, Taylor Berberich

Kathy Lang moved for the suspension of certain meeting rules due to the COVID-19 teleconference; Chuck Horgan seconded. Motion Carried. (7-0)

Changes to Agenda:

- None

Workshops:

- **Northeast Middle School - Recommendation Meeting**
- Staff Report: Taylor Berberich
- Applicant Presentation: Greg Forsyth (Spokane Public Schools), Walt Huffman & Craig Conrad (MMEC Architecture), Mike Terrell (MTLA Landscaping)
- Kathy Lang closed public comment
- Questions asked and answered
- Discussion ensued

Based on review of the materials submitted by the Applicant and discussion during the November 16, 2020 Recommendation Meeting the Design Review Board recommends the approval of the project subject to the following conditions:

Design Departures:

1. Buildings Along Street:

- The Board finds that the alternative design *meets* the intent of the design standard.
- The Board finds that the alternative design *is superior* in quality to what would be achieved if the standard were followed.

Please see the following Comprehensive Plan Goals and Policies: LU 1.1 Neighborhoods, LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, LU 6.9 Facility Compatibility with Neighborhood, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures, DP 2.6 Building and Site Design, DP 2.15 Urban Trees and Landscape Areas, NE 13.1 Walkway and Bicycle Path System, N 2.1 Neighborhood Quality of Life, and N 5.3 Linkages.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Buildings Along Street, SMC 17C.122.060 Lighting, SMC 17C.122.060 Ancillary Site Elements, SMC 17C.122.060 Treatment of Blank Walls, SMC 17C.122.060 Façade Transparency, SMC 17C.122.060 Massing, and SMC 17C.122.060 Pedestrian Oriented Signs.

2. Buildings at Intersection Corners:

- The Board finds that the alternative design *meets* the intent of the design standard.
- The Board finds that the alternative design *is superior* in quality to what would be achieved if the standard were followed.

Please see the following Comprehensive Plan Goals and Policies: LU 1.1 Neighborhoods, LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, TR GOAL A: PROMOTE A SENSE OF PLACE, TR GOAL B: PROVIDE TRANSPORTATION CHOICES, TR GOAL C: ACCOMMODATE ACCESS TO DAILY NEEDS AND PRIORITY DESTINATIONS, TR 1 Transportation Network For All Users, TR 7 Neighborhood Access, TR 14 Traffic Calming, TR 20 Bicycle/Pedestrian Coordination, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures, DP 2.6 Building and Site Design, DP 2.15 Urban Trees and Landscape Areas, NE 13.1 Walkway and Bicycle Path System, N 2.1 Neighborhood Quality of Life, N 4.6 Pedestrian and Bicycle Connections, and N 5.3 Linkages.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Buildings Along Intersection Corners, SMC 17C.122.060 Sidewalk Encroachments, SMC 17C.122.060 Lighting, SMC 17C.122.060 Ancillary Site Elements, SMC 17C.122.060 Curb Cut Limitations, and SMC 17C.122.060 Pedestrian Oriented Signs.

Chuck Horgan moved to approve the recommendations; Chad Schmidt seconded.

Ted Teske made a friendly amendment to vote first on the design departures and have a second vote on the recommendations; Drew Kleman seconded.

The motion on the design departures carried with a non-unanimous vote. (5/2)

General

3. Per the Lighting Design Standards for Centers and Corridors, the Applicant shall provide additional pedestrian-scale lighting along pedestrian paths 1) between Marietta Avenue and the playground/outdoor basketball courts and 2) at the proposed plaza at the intersection of North Perry Street and North Foothills Drive.

Please see the following Comprehensive Plan Goals and Policies: LU 1.1 Neighborhoods, LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, TR GOAL A: PROMOTE A SENSE OF PLACE, TR GOAL C: ACCOMMODATE ACCESS TO DAILY NEEDS AND PRIORITY DESTINATIONS, TR 1 Transportation Network For All Users, TR 7 Neighborhood Access, TR 14 Traffic Calming, TR 20 Bicycle/Pedestrian Coordination, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures, DP 2.6 Building and Site Design, NE 13.1 Walkway and Bicycle Path System, N 2.1 Neighborhood Quality of Life, N 4.6 Pedestrian and Bicycle Connections, and N 5.3 Linkages.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Lighting and SMC 17C.122.060 Ancillary Site Elements.

4. The Applicant is strongly encouraged to integrate the building materials and architectural detailing into the site gateway elements.

Please see the following Comprehensive Plan Goals and Policies: LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, TR GOAL A: PROMOTE A SENSE OF PLACE, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures, DP 2.6 Building and Site Design, and N 2.1 Neighborhood Quality of Life.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Buildings Along Intersection Corners, SMC 17C.122.060 Sidewalk Encroachments, SMC 17C.122.060 Lighting, SMC 17C.122.060 Ancillary Site Elements, SMC 17C.122.060 Pedestrian Oriented Signs, SMC 17C.122.060 Integration with Architecture, SMC 17C.122.060 Creative Graphic Design, SMC 17C.122.060 Unique Landmark Signs, SMC 17C.122.060 Ground Signs, and SMC 17C.122.060 Materials.

Ted Teske moved to approve the recommendations as written; Chuck Horgan seconded. Motion carried unanimously. (7/0)

Approval of Minutes:

- Minutes from the November 11, 2020 meeting approved unanimously.

Old Business:

- None

New Business:

- None

Chair Report:

- None

Secretary Report - Dean Gunderson

- The December 9th DRB Meeting will be a final recommendation meeting for the Latah Glen Manufactured Home Development.
- The Riverside Apartments project have expressed a desire to come back in December.
- Dean will send out a Doodle Poll to choose a date to replace the meeting originally scheduled for December 23rd.
- The written [dissenting opinion](#) will be attached to the meeting minutes.

Meeting Adjourned at 8:38 PM

The next Design Review Board Meeting is scheduled for Wednesday, December 9, 2020.

DISSENTING OPINION

Northeast Middle School

November 24, 2020



From :
Design Review Board Members
 Drew Kleman, Architect
 Theodore Teske, Citizen at Large

 c/o Dean Gunderson, DRB Secretary
 Planning Services
 808 W. Spokane Falls Blvd.
 Spokane, WA 99201

To :
 Louis Meuler, Spokane Interim Planning Director

 Spokane Design Review Board

 Greg Forsyth, Spokane Public Schools, Director of Capital Projects
 and Planning, w/ Walt Huffman, MMEC Architecture

 Donna deBit, Spokane Development Services Center, Associate
 Planner

The following Dissenting Opinion is provided by the Architect Member (Drew Kleman) and the Citizen at Large Member (Theodore Teske) of the Design Review Board (DRB).

As members of the DRB who did not vote with the majority on the motion regarding the recommendation to approve the Design Departures for the proposed development project, Mr. Kleman and Mr. Teske are entitled to submit a Dissenting Opinion under the Design Review Board's Rules of Procedure 6.4.1 and 6.4.2. This opinion will be transmitted to the Design Review Board, the City's Action Approving Authority for the project, and the Applicant.

A. The Majority Opinion of the Design Review Board for the two Design Departures
 (with Comprehensive Plan policies and Design Standards citations)

Buildings Along Street:

- The Board finds that the alternative design *meets* the intent of the design standard.
- The Board finds that the alternative design *is* superior in quality to what would be achieved if the standard were followed.

Please see the following Comprehensive Plan Goals and Policies: LU 1.1 Neighborhoods, LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, LU 6.9 Facility Compatibility with Neighborhood, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures, DP 2.6 Building and Site Design, DP 2.15 Urban Trees and Landscape Areas, NE 13.1 Walkway and Bicycle Path System, N 2.1 Neighborhood Quality of Life, and N 5.3 Linkages.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Buildings Along Street, SMC 17C.122.060 Lighting, SMC 17C.122.060 Ancillary Site Elements, SMC 17C.122.060 Treatment of Blank Walls, SMC 17C.122.060 Façade Transparency, SMC 17C.122.060 Massing, and SMC 17C.122.060 Pedestrian Oriented Signs.

Buildings at Intersection Corners:

- The Board finds that the alternative design *meets* the intent of the design standard.
- The Board finds that the alternative design *is* superior in quality to what would be achieved if the standard were followed.

Please see the following Comprehensive Plan Goals and Policies: LU 1.1 Neighborhoods, LU 1.12 Public Facilities and Services, LU 5.1 Built and Natural Environment, TR GOAL A: PROMOTE A SENSE OF PLACE, TR GOAL B: PROVIDE TRANSPORTATION CHOICES, TR GOAL C: ACCOMMODATE ACCESS TO DAILY NEEDS AND PRIORITY DESTINATIONS, TR 1 Transportation Network For All Users, TR 7 Neighborhood Access, TR 14 Traffic Calming, TR 20 Bicycle/Pedestrian Coordination, DP 1.2 New Development in Established Neighborhoods, DP 2.3 Design Standards for Public Projects and Structures,

DP 2.6 Building and Site Design, DP 2.15 Urban Trees and Landscape Areas, NE 13.1 Walkway and Bicycle Path System, N 2.1 Neighborhood Quality of Life, N 4.6 Pedestrian and Bicycle Connections, and N 5.3 Linkages.

Please see the following Design Standards and Guidelines for Centers and Corridors: SMC 17C.122.060 Buildings Along Intersection Corners, SMC 17C.122.060 Sidewalk Encroachments, SMC 17C.122.060 Lighting, SMC 17C.122.060 Ancillary Site Elements, SMC 17C.122.060 Curb Cut Limitations, and SMC 17C.122.060 Pedestrian Oriented Signs.

These recommendations were approved via a non-unanimous vote of 5/2.

Yeas: Kathy Lang (Chair and Community Assembly Liaison), Mark Brower (Vice-Chair and Engineer Member), Anne Hanenburg (Landscape Architect Member), Chuck Horgan (Arts Commission Member), and Chad Schmidt (Urban Designer Member)

Nays: Drew Kleman (Architect Member), and Theodore Teske (City at Large Member)

B. Statement of Dissent Regarding the *Intent* and *Quality* Questions for the Departures

As regard the two distinct questions for each Design Departure, Mr. Kleman and Mr. Teske submit their dissenting opinion for various reasons (see Table 1):

Table 1. Reasons for Dissenting Opinion

Design Criteria	Drew Kleman	Ted Teske
Has the Intent of Buildings Along Street been met?	No	No
Has the Superior Design Quality of the Alternative proposal for Buildings Along Street been established?	N/A ¹	N/A ¹
Has the Intent of Buildings at Intersection Corners been met?	Yes	No
Has the Superior Design Quality of the Alternative proposal for Buildings Intersection Corners been established?	No	N/A ¹

Note:

1. As the Intent of the referenced Design Standard has not been met, the question of whether the superior design quality of the proposed alternative has been established by the Applicant is rendered moot (that is, no determination on this could be made).

C. Statement of Dissent Regarding the Nature of the Opposition to the Majority Opinion

Drew Kleman finds:

This dissenting opinion questions the applicability of the Design Departure process to those requested Design Departures on this project. As stated in Section 17G.030.020 Applicable Standards:

A. *Design Departures.*

Design departures may be sought for design standards that are identified as Requirements (R) or Presumptions (P). Design departures are not for development standards (i.e., floor area ratio, building height, setbacks and sidewalks, etc.). The sections that allow for design departures include:

1. *site and building design standards (i.e., **ground floor windows, base/middle/top, articulation, etc.**) contained in chapter 17C.120 SMC.*

Under the Land Use code as written, the Applicant can request a Design Departure for design standards except as limited by the code. That the Land Use code specifically goes out of its way to identify **ground floor windows, base/middle/top, and articulation** in its text warrants further clarity of the intent of Design Departures by the City. It is this opinion that the intent of the Design Departure code is not to alleviate the Applicant from meeting those design standards foundational to a successful public environment, such as *Buildings Along Street* and *Buildings at Intersection Corners*. As the Land Use code is silent nor does not differentiate between those design standards potentially having greater impact on the public environment versus those with less significant impact, this opinion cannot hold the Applicant at fault for requesting said departures and yet finds the threshold for meeting intent and superior quality to be high. This opinion also finds that the threshold set by the Land Use code for the Design Review board to review and render an opinion on the intent and superior quality of Design Departure is inexhaustive. Prescriptive design criteria, such as submitting drawings, diagrams, perspectives, data, or other means depicting the application of Design Standards, how they hinder the project, and how the proposed alternate design meets the design intent and is of superior quality. The materials provided by City staff and the Applicant to the Design Review Board were exhaustive in their narrative but lacking in visuals to validate those texts.

The *Design Standards and Guidelines for Centers and Corridors* notes the intent of *Buildings at Intersection Corners* to "...create an environment that frames the public realm and creates an urban street edge and contributes to the liveliness of sidewalks." Additionally, Guideline 1 of *Buildings at Intersection Corners* "...shall hold the street corner...". The Applicant described the challenges created by the site boundary at the intersection corner of North Foothills Drive and Perry Street, to which this dissenting opinion does not object to the findings that this particular intersection corner would prove difficult to achieve a successful and logical design of meeting the Guideline. However, the proposed ornamental plaza and representative materials did not, in this opinion, portray a design of superior quality. This opinion questions the purpose of an ornamental plaza on this particular intersection corner where traffic noise may be expected to be consistent and objectionable to pedestrians, and thus questions if this plaza will contribute to the "...liveliness of sidewalks." However, it is the design of the plaza and components thereof that fail to "...frame() the public realm..." nor "...hold the street corner...". There is an apparent lack of rigor in the design of the plaza, including the pedestrian experience as one comes upon, passes through, and exits. This is not to suggest that the Applicant's overall design approach is invalid as evidenced in the bulk building. This opinion finds that additional time and design reviews would render a more

complete and agreeable solution. To that end, the Design Departure process would benefit from a more robust review and collaborative process that includes City staff and Design Review Board members. The Design Departure process may in fact warrant an independent review track separate from the general Design Review process.

The *Design Standards and Guidelines for Centers and Corridors* notes the intent of *Buildings Along Street* to "...ensure...part of the development of a site contributes to the liveliness of sidewalks along the street." Additionally, the Guideline 1 states "...at least 30% of the frontage of the site shall consist of building facades." This opinion finds the bar extremely high for requesting a Design Departure for *Buildings Along Street*, which as a Design Standard has significant impact on the liveliness of sidewalks and the pedestrian experience thereof. The Applicant described the site and grading challenges; the materials presented did not appear exhaustive in their investigation of design(s) that would more closely meet the intent of this Design Standard. The landscaping and entry plaza presented by the Applicant are good design features that will most likely benefit the project but are found to not be of superior quality.

Institutional uses are not mentioned in Chapter 17C.122 Centers and Corridors Zones; however, Section 17C.122.010 Intent includes, "...New development and redevelopment is encouraged in these areas that promotes a relatively cohesive development pattern with a mix of uses, higher density housing, **buildings oriented to the street, screened parking areas behind buildings**, alternative modes of transportation with a **safe pedestrian environment**, quality design, smaller blocks and relatively narrow streets with on-street parking." This opinion finds that the proposed alternate design does not meet the basic and underlying intent of the zoning code, particularly buildings oriented to the street, screened parking areas behind buildings, and safe pedestrian environments. The proposed building itself makes little to no acknowledgement to the street, parking is evident at the front of the building, and the back-of-curb sidewalk makes no attempt to buffer portions of the pedestrian environment from the street. These three elements in combination prove too big a hurdle to allow a vote in the affirmative.

Ted Teske finds:

The main reason for this dissenting opinion centers on the decision that the proposed design departures do not meet the intent of the standards from which the Applicant is seeking a departure. Therefore a determination of superior quality is rendered moot. The moment the Applicant sought a design departure, it raised the bar for their proposed design because it needed to "meet the intent and the general direction set forth by the Requirement" by providing a "specific change superior in design quality to that potentially achieved by the Requirement." (SMC 17G.030.040). Having in this member's opinion not met the first criteria, the second does not need to be considered in this decision.

In this case the Applicant was seeking a departure from two foundational design standards of the Center and Corridor zone: buildings placed along the street and buildings placed at street corners. According to the Center and Corridor Design Standards, the intent of both of these standards is to "contribute to the liveliness of sidewalks" adjacent to the project. Additionally, the standard for buildings along the corners is intended to frame pedestrian realm. (SMC 17A.122.060, Attachment A) This is in support of the overall intent of the Center and Corridor zones to, "promote a relatively cohesive development pattern with a mix of uses, higher density housing, buildings oriented to the street, screened parking areas behind buildings, alternative modes of transportation with a safe pedestrian environment, quality design, smaller blocks and relatively narrow streets with on-street parking." (SMC 17C.122.010)

These two standards are critical to creating the pedestrian-oriented urban environment envisioned by the Comprehensive Plan. By not meeting these standards outright, or at

least meeting their intent, the overall intent of this zone becomes nearly impossible to implement as envisioned in the built environment.

As regards the departure for *Buildings at Intersection Corners* – looking at the design as presented at the DRB Recommendation Meeting, the Applicant attempted to create a lively streetscape by developing a small plaza at the corner of North Foothills Boulevard and Perry Street to substitute for placing their building at the corner of the intersection. When comparing this to the environment created by the intent of the standard from other CC-1 zoned areas, it would not in my opinion, “create an environment that frames the public realm and creates an urban street edge and contributes to the liveliness of sidewalks.”

The low hardscape features and wide-open spaces adjacent to the corner did not create the urban effect seen in other CC-1 zones. It seemed to lack the appropriate scale that a building on the corner would provide and would not start creating the urban environment envisioned by the Center and Corridor zoning around the intersection as the Neighborhood Center continues to develop.

As regards the departure for *Buildings Along the Street* – the second design departure dealt with avoiding placing their building along any of the adjacent streets around the property by adding additional features to their main school entrance along Perry Street. When comparing the Applicant submittals for each design review meeting, the school building footprint and location did not materially change between the Collaborative Workshop and the Recommendation Meeting. The building’s narrowest face was placed closest to Perry Street and efforts to meet the intent of the standard were centered around hardscape treatments and fixed-location seating elements.

The overall effect was not measurably different than the final design of the entryway at Glover Middle School along Longfellow Avenue, a property designed and built in a Single-family Residential zone. Center and Corridor Zoning is the City’s most pedestrian-oriented zoning and building placement is a key component of meeting the intent to create an urban environment that frames the public realm at a pedestrian scale that attempts to replicate the scale and features of older commercial districts found in the Perry District, Garland Avenue, Monroe Street, and Market Street in Hillyard. The project as designed does not create that type of environment.

This is not an indictment of the building design overall. The building itself is well-designed and will function well in its intended purpose as will the other new middle-schools currently under design or under construction by Spokane Public Schools.

D. Conclusion and Proposed Remediation

Drew Kleman poses certain process improvements for Design Departures to resolve future conflicts, to wit:

In conclusion, this opinion questions the underlying intent of the Design Departure process in its applicability to this project's requested departures from *Buildings Along Street* and *Buildings at Intersection Corners*. This opinion finds that the *Design Standards and Guidelines for Centers and Corridors* may warrant reevaluation of and perhaps hierarchical importance of design standards, identifying those that have more impact on the public realm and pedestrian environment than others. It appears the Land Use code may suggest this through the text provided in Section 17G.030.020.A.1 but is silent nonetheless.

The Design Departure process may be improved by the following:

- Inclusion of prescriptive requirements for Design Departures, such as drawings, diagrams, perspectives, data, or other methods deemed appropriate to communicate:
 - The project as if meeting the intent of the design standard
 - Why the design standard is a hurdle to the project's success
 - The project's proposed alternate design
 - The project's superior quality
- Promote visual depictions over exhaustive narrative
- Clarification by the City, through Land Use code update, that Design Departures are to be engaged only if the Applicant can prove that a Design Standard is an impediment to a project. This opinion infers that the Design Departure process may be used when a Design Standard is objectionable, and that the Design Departure process is an 'easy out' for an Applicant to not address the basic intent of the Land Use code(s).
- A separate and distinct process for Design Departure review and approval from the standard Design Review Board review process
- Collaborative Workshop(s) with the Applicant, City staff, and Design Review Board prior to an Applicant requesting a Design Departure to discuss the project, challenges, and potential solution(s) for the Applicant
 - The intent here would be to provide the Applicant with clearer direction and criteria for proving the Design Departure need, including what materials the Action Approving Authority requires to review the request
 - This process may warrant a multi-step process where the review and approval of a Design Departure *meeting the design standard intent* would be approved prior to determining *superior quality*
 - This opinion finds that meeting of the design intent to be the highest hurdle, and that determination of superior quality to be secondary to that finding

Under the current code the Applicant does have the right to request a Design Departure for the design standards *Buildings Along Street* and *Buildings at Intersection Corners*. This opinion finds the bar extremely high to which an Applicant must show (a) design intent is met and (b) that the proposed design is of superior quality for the requested Design Departures. This opinion finds that bar not met and thus rendered a dissenting vote.

Ted Teske proposes that the City's various departments work more closely with an Applicant to more fully ensure that future development projects' proposed in the Centers and Corridors zones more fully embrace the pedestrian-oriented nature of these districts (regardless of the character of the existing surrounding built environment), while the staff within these departments work to understand that this zone category is the most pedestrian-oriented land use tool at the City's disposal, to wit:

After sitting through the two meetings on this project, it seems to me there was a big assumption on the part of the Applicant and many board members when considering these design departures. It was assumed that auto-oriented transportation would drive many of

the design decisions for developing this project in our City's most pedestrian-oriented zoning. Building placement seemed to be determined by a number of auto-oriented factors including:

- moving the building away from adjacent streets because road character was perceived to be immutable,
- determining building placement to accommodate parking lots with "enough" parking,
- moving or adjusting street tree locations to prioritize auto use along property frontages,
- declining to provide plans for safe crossings of North Foothills Blvd because of existing road character.

It is a theme the Design Review Board has seen repeatedly in projects in Center and Corridor zones throughout the city. Applicants seem unwilling to address broader changes to urban character needed to fully realize the intent of Center and Corridor zoning because it appears the City of Spokane is unwilling to fulfill the policies of the Comprehensive Plan as it relates to transportation and land use. The developers, for the most part rightly, see their responsibility stopping at the curb line, but the City is not shouldering their responsibility to make sure these projects are implemented in such a way as to meet the overall intent of the Center and Corridor zoning. So instead the community continues to see suburban-style Center and Corridor design that continues to tacitly emphasize auto-use instead of mixed-use and multi-modal development as envisioned in the Comprehensive Plan and called for by the Spokane Municipal Code.

I do not know what the specific remedy is for this situation. I would like to see more engagement and discussion between the engineering department and urban planning department at the City to see how changes to the streetscape could be included in Center and Corridor projects (or projects that are subject to design review more broadly). Perhaps including a transportation section in staff reports that talk about how the streets could be adapted to help meet the overall vision of the Center and Corridor design standards for calm, pedestrian-friendly streetscapes. There remains a vast amount of potential to use Centers and Corridors zoning to realize the vision of the Comprehensive Plan, continuing to not fully implement the intent continues to undermine the vision of that plan for Spokane and its future.

Respectfully,



Drew Kleman



Theodore (Ted) Teske