



## Design Review Board

March 22, 2017

5:30 PM

City Council Briefing Center  
Lower Level, City Hall

**TIMES GIVEN ARE AN ESTIMATE AND ARE SUBJECT TO CHANGE**

### Commission Briefing Session:

- |             |  |               |
|-------------|--|---------------|
| 5:30 - 5:40 | 1) Chair Report  | Austin Dickey |
|             | 2) Staff Report  | Julie Neff    |
|             | 3) Approve <a href="#">February 22, 2017</a> & <a href="#">March 8, 2017</a> meeting minutes |               |

### Workshop:

- |             |   |             |
|-------------|---|-------------|
| 5:40 - 6:40 | 1) Recommendation meeting: CSO 26 Riverside Ave   | Omar Akkari |
|             | <ul style="list-style-type: none"><li>• <a href="#">Staff Report</a></li><li>• <a href="#">Applicant Presentation</a></li><li>• Public Comment</li><li>• Board Discussion and Motions</li></ul> |             |
| 6:40 - 7:40 | 2) Recommendation meeting: KXLY   | Julie Neff  |
|             | <ul style="list-style-type: none"><li>• <a href="#">Staff Report</a></li><li>• <a href="#">Applicant Presentation</a></li><li>• Public Comment</li><li>• Board Discussion and Motions</li></ul> |             |

### Board Business:

- 1) Old or New Business

### Adjournment:

Next Design Review Board meeting will be held on April 12, 2017

The password for City of Spokane Guest Wireless access has been changed:

**Username:** COS Guest

**Password:**

**AMERICANS WITH DISABILITIES ACT (ADA) INFORMATION:** The City of Spokane is committed to providing equal access to its facilities, programs and services for persons with disabilities. The Spokane City Council Chamber in the lower level of Spokane City Hall, 808 W. Spokane Falls Blvd., is wheelchair accessible and also is equipped with an infrared assistive listening system for persons with hearing loss. Headsets may be checked out (upon presentation of picture I.D.) at the City Cable 5 Production Booth located on the First Floor of the Municipal Building, directly above the Chase Gallery or through the meeting organizer. Individuals requesting reasonable accommodations or further information may call, write, or email Human Resources at 509.625.6363, 808 W. Spokane Falls Blvd, Spokane, WA, 99201; or [jjackson@spokanecity.org](mailto:jjackson@spokanecity.org). Persons who are deaf or hard of hearing may contact Human Resources through the Washington Relay Service at 7-1-1. Please contact us forty-eight (48) hours before the meeting date.

# Design Review Board

**February 22, 2017**

Meeting Minutes

Meeting called to order at 5:35 PM

## Attendance

- Board Members Present: David Buescher, Austin Dickey, Kathy Lang, Ted Teske, Steven Meek, Anne Hanenburg, Ryan Leong
- Board Members Not Present: Charlene Kay
- Staff Present: Julie Neff, Omar Akkari, Nathan Gwinn; City of Spokane Planning and Development

## Briefing Session:

1. Chair Comments-Austin Dickey
  - None
2. Staff Comments-Julie Neff
  - Next Design Review workshops will be held on March 22, 2017.

January 25, 2017 meeting minutes were approved unanimously with suggested changes to the Attendance.

## Board Business:

1. Design Review Board Mission & Purpose:
  - Board members reviewed the Design Review Board's mission statement.
  - Discussed the Board's role throughout the Design Review process.
2. Board Recommendations:
  - Discussed implementing improvements to the Board's recommendation to help strengthen and clarify next steps for the applicants.
3. Draft Options for DRB Updates:
  - Discussed implementing improvements that will strengthen the Boards processes.
  - Overviewed draft options for revising the Design Review Board's work plan.
4. 2017 Mayor's Urban Design Awards:
  - Opened dialogue on the 2017 Mayor's Urban Design Awards.
  - Discussed the current criteria for each Design Award.
  - Discussion to be continued at the regularly scheduled March 8, 2017 meeting.

## **Meeting Adjourned at 7:40 PM**

Next Design Review Board meeting is scheduled for **March 8, 2017**

Note: Minutes summarized by staff. An audiotape of the meeting is on file with the Planning & Development Department, City of Spokane.



# Design Review Board

**March 8, 2017**

Meeting Minutes

Meeting called to order at 5:36 PM

## Attendance

- Board Members Present: Austin Dickey, Kathy Lang, Steven Meek, Anne Hanenburg,
- Board Members Not Present: Charlene Kay, David Buescher, Ted Teske, Ryan Leong
- Staff Present: Julie Neff, Omar Akkari; City of Spokane Planning and Development

## Briefing Session:

1. Chair Comments-Austin Dickey
  - None
2. Staff Comments-Omar Akkari
  - Next Design Review workshops will be held on March 22, 2017.

February 22, 2017 meeting minutes were deferred till March 22, 2017 due to lack of quorum.

## Board Business:

1. Introduction to the 2017 Spokane Mayor's Urban Design Awards:
  - Introduced the history behind MUDA
2. Definition of Urban Design:
  - Omar Akkari presented the definition of Urban Design.
  - Discussed how Urban Design relates to the Public Realm.
  - Discussed utilizing the definition of Urban Design to guide the MUDA submission requirements.
3. Awards updates:
  - Discussed the potential updates to the Mayor's Urban Design award categories.
4. MUDA Jury Criteria:
  - Omar Akkari presented past and current Mayor's Urban Design Award Jury Criteria.
  - Discussed Staff and Board suggestions for revisions to the Jury Criteria.
5. Next Steps:
  - Omar Akkari will revise the MUDA Awards submission requirements and jury criteria based on feedback received. The revised documents will be sent out for Board Members to review.

**Meeting Adjourned at 7:45 PM**

Next Design Review Board meeting is scheduled for **March 22, 2017**

Note: Minutes summarized by staff. An audiotape of the meeting is on file with the Planning & Development Department, City of Spokane.

# Spokane Falls CSO 26 Control Facility Plaza Project

## 2 - Recommendation Meeting

### Design Review Staff Report

March 10, 2016



**Staff:**  
Omar Akkari, Urban Designer  
Planning & Development Services Department

**Applicants:**  
Kyle Twohig  
City of Spokane  
  
Bill LaRue  
Bernardo | Wills Architects PC  
  
Steve Hansen  
Hansen Consulting

### Meeting Goals

At the March 22, 2016 Design Review Board (DRB) Recommendation Meeting, the DRB should:

- Determine how adopted plans and policy including the Downtown Design Guidelines, Shoreline Regulations and Comprehensive Plan affect or pertain to the proposed design and
- Identify opportunities for design modifications as appropriate to maintain consistency with adopted plans and policy and respond to public comment.

### Background

- The Design Review Board Program Review/Collaborative Workshop was held on July 27<sup>th</sup>, 2016.

The following materials are supplemental to this report:

- *Design Review Board Program Review/Collaborative Workshop Recommendation*, July 27, 2016;
- *Design Review Staff Report (Program Review/Collaborative Workshop)*, July 13, 2016;
- Additional information is on file and may be viewed at City Hall.

Recommendations of the Design Review Board will be forwarded to the Planning Director.

### Regulatory Requirements

#### Chapter 17E.060 Shoreline Regulations - Article VIII. Design Standards and Guidelines Specific to Shoreline Districts

The project is within the shoreline jurisdiction so the Design Standards and Guidelines Specific to the Shoreline Districts apply. The shoreline jurisdiction boundary is shown in applicant's submittals within attachment #10. It is important to reviewing the following guidelines, which describe the preferred character and relationship between new structures and the river. The full text of Section 17E.060.820 Standards and Guidelines Specific to the Downtown District is attached after the staff report.

#### Section 17E.060.810 Standards and Guidelines Applying to Downtown, Campus, and Great Gorge Districts

#### Section 17E.060.820 Standards and Guidelines Specific to the Downtown District

##### D. Building Design.

##### 1. Character Related to the Setting.

##### a. Purpose.

To ensure that new buildings are complementary or visually subordinate to the natural splendor of the river and falls, including its geologic features and native vegetation.

- b. The dramatic nature of the river and the falls define the image of downtown Spokane in a way unlike any other city. Therefore, new buildings should respect the importance of this magnificent and unique centerpiece by not attempting to compete with it visually. Buildings should be designed with respect to location, form and color, so that the river continues to be the emphasis. (C)
- c. Building facades should also incorporate elements that strengthen the natural setting, such as local stone, bay windows, fine-grained articulation, lower floors that frame public spaces, surrounding vegetation. The preference is to have buildings that seem like a built extension of rock, embankments, and outcroppings. (C)

## Topics for Discussion

During the workshop, the applicant is encouraged to please describe changes to the design since the Collaborative Workshop/Program Review including any changes made in response to recommendations offered by the Design Review Board on July 27, 2016.

Staff offers the following items for discussion:

1. **Views from the site and the relationship between the plaza, the Spokane River, and the Monroe St. Bridge.** The river is Spokane's most iconic natural feature. Introducing unique experiences, perspectives, or views that showcase the river from this vantage point are encouraged.

*Please See Downtown Design Guidelines A-1 and Spokane Municipal Code(SMC) [Section 17E.060.820](#) Standards and Guidelines Specific to the Downtown District.*

2. **Views of the site from surrounding vantage points including Veteran's Park and Monroe Street Bridge and Huntington Park.** How can the tank's bulk be further mitigated for view from across north side of the river? Can the concrete formliner walls facing north be stained or dyed a darker gray closer to basalt's natural color?

*Please See Downtown Design Guidelines A-1 and E-3 and SMC [Section 17E.060.820](#) Standards and Guidelines Specific to the Downtown District.*

3. **Mechanical Building.** Staff would recommend that the mechanical building be blended into its surroundings on the street facing portions rather than being wrapped in a mural or artwork as suggested in the submittal. If a mural is desired on the street facing portion of the building then the building should be flipped so that the service door and vents faces the boom truck access area, which would allow for an uninterrupted mural surface facing the street.

*Please See Downtown Design Guideline E-3*

4. **Service access / manholes.** Are there opportunities to minimize the visual impact of the many utility access hatches scattered throughout the upper plaza deck? Are recessed tray lids (Lids that integrate the paver and concrete patterns) able to be utilized?

*Please See Downtown Design Guideline E-3*

5. **Faux basalt rock.** Is the use of faux basalt rock carvings appropriate at the plaza level? Would it be more appropriately used below the plaza? If natural basalt is not an option above the tank, suggested solutions might include using abstracted geometric forms (reminiscent of basalt formations) made from steel or concrete rather than mimicking natural basalt.

*Please See Downtown Design Guideline C-7 SMC [Section 17E.060.820](#) Standards and Guidelines Specific to the Downtown District.*

6. **Place making approaches.** Are there opportunities to have interactive features or a place-making attraction in this plaza that would be simple and elegant, without detracting from the views of the falls?

*Please See Downtown Design Guideline C-3 and SMC [Section 17E.060.820](#) Standards and Guidelines Specific to the Downtown District.*

7. **Thematic ties to the poem plaza.** How might the story told in the poem plaza be reflected in other areas of the park? The poem plaza is in a somewhat secluded location. How might elements of that story be incorporated as wayfinding design cues leading visitors to the poem plaza?

*Please See Downtown Design Guideline C-1 and SMC [Section 17E.060.820](#) Standards and Guidelines Specific to the Downtown District.*

8. **Light box benches.** Staff has concerns with the light box benches being attractive targets for vandalism because of their white / semi-transparent surfaces. This concern is reiterated in the manufacturer's product description. Under lit benches or some other more resilient bench and light source combination is recommended.

*Please See Downtown Design Guidelines D-4 and D-6*

## **Note**

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

## **Policy Basis**

Spokane Municipal Codes  
Shoreline Regulations  
City of Spokane Comprehensive Plan  
Public Project Design Guidelines  
Downtown Design Guidelines

## Title 17E Environmental Standards

### Chapter 17E.060 Shoreline Regulations

#### Article VIII. Design Standards and Guidelines Specific to Shoreline Districts

##### Section 17E.060.820 Standards and Guidelines Specific to the Downtown District

###### A. Shoreline Relationships – Human Activity.

###### 1. Purpose.

To recognize that people's relationship to the river and anticipated uses in each district will inform the design of the built environment. Downtown is an intense urban environment where people and views of the falls create much of the interest in being there. New development along the riverfront that will facilitate desired activities is encouraged. These include shopping, dining, walking, biking, jogging, people watching, viewing the falls, and learning the stories of the river through interpretive displays.

###### 2. Along the river, new buildings shall provide at least three of the following (R):

- a. Outdoor seating or opportunities for outdoor dining.
- b. Building entry from the river side.
- c. Benches along pedestrian trail.
- d. Outdoor balconies.
- e. Public plaza with seating.
- f. Public viewpoint with interpretive signs (see [SMC 17E.060.810\(A\)\(2\)\(a\)\(iii\)](#), Overlooks and Public Development, and [SMC 17E.060.810\(A\)\(2\)\(b\)](#), Overlooks in Private Development).
- g. Public art.
- h. Opportunities for outdoor vending such as food or bicycle rental.

###### B. Streets, Sidewalks, and Trails – Pedestrian Pathways.

###### 1. Purpose.

To result in an urban pathway design reflecting the form and intensity of adjacent development, as well as a higher level of pedestrian activity.

###### 2. Pedestrian pathways shall be at least ten feet wide. (R)

###### 3. Pedestrian pathways shall be lighted either from nearby buildings or from pedestrian-scaled fixtures. (R)

###### 4. Seating should be provided at intervals. (C)

###### C. Site Design.

###### 1. Landscape Character Protection.

###### a. Purpose.

To ensure that development along the shoreline enhances the natural character of the river and falls including its geologic features and native vegetation.

###### b. Parking and service areas shall be located so they are screened from views along the shoreline, from opposite shorelines, and from bridges. (R)

###### 2. Pervious Plazas and Spaces.

###### a. Purpose.

To create a system of spaces integrated with the ecological systems of the shoreline.

- b. Development shall include one or more of the following (R):
  - i. Open spaces.
  - ii. Courtyards.
  - iii. Plazas.
  - iv. Forecourts; or
  - v. Other public spaces that allow for a seamless connection between streets and various uses.
- c. Surfacing of these spaces shall allow for the capture of rainwater and filtration into a natural cleansing system of vegetation and sub-grade materials. (R)

### 3. Impervious Surfaces.

- a. Purpose.

To reduce stormwater runoff rates and volumes, while recognizing that the downtown area will have more intense development and more hard surfaces than other districts.
- b. New development shall include at least ten percent pervious surfaces. This may be accomplished by using one or a combination of the following (R):
  - i. At-grade planted areas.
  - ii. Permeable paving systems in pedestrian surfaces such as pedestrian walkways or plazas.
  - iii. Green roofs.
  - iv. Each existing significant tree (over six inches caliper and in good health) protected during design and construction may reduce the total required pervious surface requirement by one percent per tree. The required pervious surface shall not be reduced to less than five percent. For example, an applicant retaining two significant trees shall be required to provide eight percent pervious surfaces; an applicant retaining six significant trees shall be required to provide five percent pervious surfaces. Please note that all landscape requirements shall be met regardless of pervious surface requirements.
- c. Stormwater runoff from impervious surfaces shall be directed to treatment or detention areas. For non-pollution generating impervious surfaces (generally this includes most surfaces not traveled on by automobiles) this may include one or a combination of the following as approved by director of engineering services (R):
  - i. Stormwater planters.
  - ii. Tree box filters.
  - iii. Water features.
  - iv. Landscaped areas including swales or rain gardens.
  - v. Cistern for later on-site irrigation.
  - vi. Other options as approved by engineering services.
- d. Raised curbs are discouraged in parking lots so that stormwater runoff can drain naturally into City approved percolation areas rather than be directed into a concentrated flow. At grade curb alternatives to finished asphalt edges are encouraged. (C)

### 4. Planting Palette.

- a. Purpose.  
To protect, enhance and restore native vegetation along the shorelines, while recognizing the more urban nature of vegetation in the downtown area.
- b. It is recognized that within downtown, there will likely be a somewhat more manicured and formal use of plantings to frame buildings, streets and spaces. However, landscape designs that rely upon lawns requiring fertilizers and herbicides, or vegetation that is highly consumptive of water should be avoided. (C)
- c. Lawns should be limited to areas where a green walking surface is necessary and desirable, in picnic areas for example. Plants should be chosen for year round interest and grouped according to their water needs for good water efficiency. Drought tolerant species and native plantings are highly encouraged. Native trees, shrubs and groundcover plants should dominate the landscape. (C)

#### D. Building Design.

##### 1. Character Related to the Setting.

- a. Purpose.  
To ensure that new buildings are complementary or visually subordinate to the natural splendor of the river and falls, including its geologic features and native vegetation.
- b. The dramatic nature of the river and the falls define the image of downtown Spokane in a way unlike any other city. Therefore, new buildings should respect the importance of this magnificent and unique centerpiece by not attempting to compete with it visually. Buildings should be designed with respect to location, form and color, so that the river continues to be the emphasis. (C)
- c. Building facades should also incorporate elements that strengthen the natural setting, such as local stone, bay windows, fine-grained articulation, lower floors that frame public spaces, surrounding vegetation. The preference is to have buildings that seem like a built extension of rock, embankments, and outcroppings. (C)

##### 2. Building Orientation.

- a. Purpose.  
To ensure that buildings along the shoreline have two public faces – one along the street, the other along the riverfront.
- b. While it is expected that buildings will be primarily oriented toward public streets, there shall also be features such as windows, secondary entries, balconies, and public spaces that are oriented to the river. (R)

##### 3. Ground Floor Animation.

- a. Purpose.  
To ensure that buildings take advantage of their unique shoreline setting and contribute to the vitality and activity of downtown.
- b. The ground floor of buildings shall incorporate a combination of at least three of the following features (R):
  - i. Windows covering more than thirty percent of the ground level façade facing the shoreline.
  - ii. Windows covering more than thirty percent of the ground level façade facing the street.
  - iii. Masonry or stone covering the ground level façade and producing a “plinth” effect.
  - iv. Ground level details such as accent lighting, decorative medallions, and canopies.
  - v. Sculpture, bas relief murals, art worked into paved surfaces.

- vi. Retail uses, such as cafes and restaurants, bike rental, and brew pubs.
- vii. Publicly accessible gardens, courtyards, or plazas.

E. Lighting – Dark Sky.

1. Purpose.  
To reduce glare and spillover from lighting associated with parking lots or buildings.
2. All lighting shall be directed downwards, with cut-off designs that prevent light from being cast horizontally or upward. (R)

Date Passed: Monday, November 3, 2008

Effective Date: Monday, July 26, 2010

ORD C34326 Section 2



## DESIGN REVIEW BOARD

# Spokane Falls CSO 26 Control Facility Plaza Project

## 1 - Program Review/Collaborative Workshop

July 27, 2016



**From :**  
Design Review Board  
Austin Dickey, Chair

c/o Omar Akkari  
Urban Designer  
Planning & Development  
808 W. Spokane Falls Blvd.  
Spokane, WA 99201

**To :**  
Bill LaRue and Dell Hatch  
Bernardo | Wills Architects PC

Steve Hansen  
Hansen Consulting

**CC :**  
Julie Neff, DRB Secretary  
Lisa Key, Planning Director  
Ali Brast, Assistant Planner  
Kyle Twohig, Project

**Based on review of the materials submitted by the applicant and discussion during the July 27th, 2016 Collaborative Workshop the Design Review Board recommends the following:**

1. Applicant shall consider further articulation of the north wall to better incorporate man-made structures with the natural landscaping, mitigating the bulk of the tank, and elevating the design quality of the multi-use pathway to same degree as the plaza.
2. Applicant shall consider odor control and the location of exhaust venting.
3. Applicant shall consider opportunities within the design and budget to provide pedestrian connection directly to the lower Huntington Park.
4. Applicant shall consider engaging artists, as early as possible, for visioning and creation of artistic components within this project.
5. Applicant shall consider the priority of a retail space on the site in lieu of expanding the plaza.

Austin Dickey, Chair, Design Review Board

Note: Supplementary information, audio tape and meeting summary are on file with City of Spokane Design Review Board.

# Spokane Falls CSO 26 Control Facility Plaza Project

## 1 – COLLABORATIVE WORKSHOP/PROGRAM REVIEW

### Design Review Staff Report

July 13<sup>th</sup>, 2016

**Staff:**  
Omar Akkari, Urban Designer  
Ali Brast, Assistant Planner  
  
Planning & Development Services Department

#### Applicants:

Kyle Twohig  
City of Spokane  
  
Bill LaRue  
Bernardo | Wills Architects PC  
  
Steve Hansen  
Hansen Consulting

## Design Review Board Authority

### Spokane Municipal Code [Chapter 04.13](#) Design Review Board

A. Purpose. The design review board is hereby established to:

1. improve communication and participation among developers, neighbors and the City early in the design and siting of new development subject to design review under the Spokane Municipal Code;
2. ensure that projects subject to design review under the Spokane Municipal Code are consistent with adopted design guidelines and help implement the City's Comprehensive Plan.
3. advocate for the aesthetic quality of Spokane's public realm;
4. encourage design and site planning that responds to context, enhances pedestrian characteristics, considers sustainable design practices, and helps make Spokane a desirable place to live, work and visit.
5. provide flexibility in the application of development standards as allowed through development standard departures; and
6. ensure that public facilities and projects within the City's right of way:
  - a. wisely allocate the City's resources,
  - b. serve as models of design quality

Under SMC [Section 17G.040.020](#) **Design Review Board Authority**, all public projects or structures are subject to design review. Recommendations of the Design Review Board must be consistent with regulatory requirements per [Section 17G.040.080](#) **Design Review Board**.

### Recommendations

Recommendations of the Design Review Board will be forwarded to the Planning Director and the Hearing Examiner.

## Project Description

Please see applicant's submittal information.

Please note that due to a very recent design change the vehicle access to the maintenance level of the structure has been eliminated. The submitted drawings do not reflect this design change. The Applicant noted that the eastern side of the plaza level would more closely resemble the design dated December of 2015.

## **Location & Context**

CSO Project 26 will be located on the north side of Spokane Falls Blvd. between Spokane's City Hall and the Monroe Street Bridge. The 325' long by 65' wide tank will extend under Riverside Ave and 22' will be exposed on the north side along the south bank of the Spokane River above Huntington Park. The Downtown Spokane Library and River Park Square Mall are located across the street from the project.

The section of Spokane Falls Boulevard adjacent to this project is classified at Type II Pedestrian Street and is marked with sharrows. The project area falls within the Riverside Neighborhood Council District and within the Downtown Planning Area.

## **Character Assets**

### **Poem Plaza**

At the Spokane Public Library's Overlook Park there is a platform overlooking Spokane Falls. It was here that artist and former Spokane resident Sherman Alexie was inspired to write his poem, "That Place Where Ghosts of Salmon Jump." It is "an environmentally sensitive poem that celebrates the creation of the falls and the river - and laments what has become of this former river of nurture and the people who inhabited its banks." The poem is written in its entirety on a polished granite spiral set in concrete, right at the spot where Alexie was inspired to write it.

### **Scenic Views**

The dramatic scenic view of the Spokane River Gorge and the Lower Falls should be protected. Views from across the river of the site are also important. Care should be taken to obscure and blend any visible utility structures in to the surrounding landscape.

### **Spokane Falls Boulevard**

This street provides a connection between the future plaza, Riverfront Park and Spokane's Downtown Core. Spokane Falls Blvd. is designated a Type II Complete Street. *Type II streets move traffic and pedestrians into and around downtown. These streets provide some of the major pedestrian connections to surrounding neighborhoods and districts.*

### **Civic Buildings and Places**

The future plaza will be flanked by the Downtown Library to the south and City Hall and Riverfront Park to the east and Huntington Park to the north. Design queues should be considered from these prominent structures and parks.

## **Regulatory Analysis**

### **Zoning Code Requirements**

**Zoning Designation:** The site is zoned DTG-70 (Downtown General – with a height limit of 70 feet)

**Land Use Designation:** Downtown and Conservation Open Space

**Note: Conservation Open Space:** Conservation Open Space includes areas that are publicly owned, not developed, and designated to remain in a natural state. It is intended to protect areas with high scenic value, environmentally sensitive conditions, historic or cultural values, priority animal habitats, and/or passive recreational features. It is expected that improvements in these areas would be limited to those supporting preservation or some passive recreation activities, like soft trails and wildlife viewpoints.

## **Downtown Design Standards**

Design standards in the code appear in the form of Requirements (R), Presumptions (P), and Considerations (C). Upon request of the applicant, the board may offer some flexibility from certain eligible code “design standards” if the board recommends that the proposed solution is equal or better than what is required, and still meets the purpose of the standard.

### **Section 17C.124.500 Design Standards Implementation:**

The design standards and guidelines found in SMC 17C.124.510 through SMC 17C.124.590 follow [SMC 17C.124.015](#), Design Standards Administration. All projects must address the pertinent design standards and guidelines. Design standards are in the form of Requirements (R), Presumptions (P), and Considerations (C). Regardless of which term is used, an applicant must address each guideline. An applicant may seek relief through [chapter 17G.030 SMC](#), Design Departures, for those eligible standards and guidelines contained in the zoning code.

[Section 17C.124.580\(B3\)](#) Plazas and Other Open Spaces Plazas, courtyards, and other pedestrian space shall include at least three of the following: (P)

- a. Special interest landscape.
- b. Pedestrian scale bollard or other accent lighting.
- c. Special paving, such as colored/stained concrete, brick, or other unit paver.
- d. Artwork.
- e. Seating, such as benches, tables, or low seating walls.
- f. Water feature.

**Shoreline Jurisdiction:** The project will be built in the Shoreline Jurisdiction, but will keep above-ground improvements out of the Shoreline Buffer. Because the proposal falls within the Shoreline Jurisdiction, a Shoreline Conditional Use Permit will be required.

Codes related to this project include: [Chapter 17E.060](#) Shoreline Regulations

Shoreline Buffer: 50 feet

Shoreline Environmental Designation: Urban Intensive

Shoreline District: Downtown

### **Section 17E.060.650 Shoreline Environment Designation: Intensive Urban**

“The purpose of the “intensive urban” environment is to ensure optimum, intensive public utilization of shorelines by providing high-intensity public use and managing development so that it enhances and maintains the shorelines for a variety of urban uses. Existing ecological functions within the shoreline area must be protected, and areas that have been previously degraded must be restored. Urban use of shorelines in this environment should be limited to water-oriented uses in developed areas with adequate building setbacks from the top of the riverbanks. Priority will be given to public access, both visual and physical. Pedestrian paths and cycle paths should connect to access points. Public ownership of land should be maintained and expanded along both riverbanks.”

[Section 17E.060.600\(G\)](#): Pedestrian and bicycle linkages to existing or planned transportation networks shall be allowed in the shoreline jurisdiction and shall incorporate scenic views, viewpoints, and the preservation of significant natural features in their design, if feasible.

### **Section 17E.060.820 Standards and Guidelines Specific to the Downtown District**

B. Streets, Sidewalks, and Trails – Pedestrian Pathways.

1. Purpose.

To result in an urban pathway design reflecting the form and intensity of adjacent development, as well as a higher level of pedestrian activity.

2. Pedestrian pathways shall be at least ten feet wide. (R)
  3. Pedestrian pathways shall be lighted either from nearby buildings or from pedestrian-scaled fixtures. (R)
  4. Seating should be provided at intervals. (C)
- C. Site Design.
1. Landscape Character Protection.
    - a. Purpose.  
To ensure that development along the shoreline enhances the natural character of the river and falls including its geologic features and native vegetation.
    - b. Parking and service areas shall be located so they are screened from views along the shoreline, from opposite shorelines, and from bridges. (R)
  2. Pervious Plazas and Spaces.
    - a. Purpose.  
To create a system of spaces integrated with the ecological systems of the shoreline.
    - c. Surfacing of these spaces shall allow for the capture of rainwater and filtration into a natural cleansing system of vegetation and sub-grade materials. (R)
- E. Lighting – Dark Sky.
1. Purpose.  
To reduce glare and spillover from lighting associated with parking lots or buildings.
  2. All lighting shall be directed downwards, with cut-off designs that prevent light from being cast horizontally or upward. (R)

## **Critical Areas**

This area is identified as the following priority habitat and species area: *Urban Natural Open Space*. Associated with regular individual occurrences of wintering bald eagles (bald eagle-10 or less), also individual occurrence of nesting osprey; winter waterfowl concentrations nesting red-tailed hawks.

### **Chapter 17E.020 Fish and Wildlife Conservation Areas**

A City of Spokane, **Critical Areas Checklist** must be completed at time of application.

The site is located adjacent to a **Riparian Habitat Zone 2 (Table 17E.020-3)** and is within an **Urban Natural Open Space (SMC 17E.020.030.B.12)** Habitat and Species Zone

A Shoreline Restoration Plan will be submitted to meet the intent of a Habitat Management Plan described in **Section 17E.020.090** and the goal of No Net Loss described in 17E.060.

## **City of Spokane Comprehensive Plan**

<https://static.spokanecity.org/documents/planning/2012/01/comprehensive-plan-revised-edition.pdf>

- **Urban Design and Historic Preservation Policy 6.3 Transit and Pedestrian-Oriented Development** – *Encourage attractive transit and pedestrian-oriented development.*
- **Urban Design and Historic Preservation Policy 6.7 - Design Standards for Public Projects and Structures.** *Design all public projects and structures to uphold the highest design standards and neighborhood compatibility.*
- **Natural Environment Goal 1** – *Protect the Spokane Valley-Rathdrum Prairie Aquifer and other water sources so they provide clean, pure water.*
- **Natural Environment Policy 5.6 Barrier Free Environments** – *Create barrier free walking and bicycling environments throughout the city in order to make alternative transportation a viable option.*

- **Natural Environment Policy 6.1 Native and Non-Native Adaptive Plants and Trees** – *Encourage the use of and develop standards for using native and non-native adaptive plants and trees in landscape designs for public and private projects.*
- **Natural Environment Policy 13.2 Walkway and Bicycle Path Design** – *Design walkways and bicycle paths based on qualities that make them safe, functional, and separated from automobile traffic where possible.*
- **Natural Environment Policy 15.5 Nature Themes** – *Identify and use nature themes in large scale public and private landscape projects that reflect the natural character of the Spokane region.*
- **Neighborhoods Goal N 4 Traffic and Circulation** – *Provide Spokane residents with clean air, safe streets, and quiet, peaceful living environments by reducing the volume of automobile traffic passing through neighborhoods and promoting alternative modes of circulation.*
- **Neighborhoods Policy 4.5 Multimodal Transportation** – *Promote alternative forms of transportation.*
- **Neighborhoods Policy 4.6** – *Establish a continuous pedestrian and bicycle network within and between all neighborhoods.*
- **Neighborhoods Policy 4.13 Pedestrian Safety** – *Design neighborhoods for pedestrian safety.*

## **Downtown Design Guidelines**

<https://static.spokanecity.org/documents/projects/divisiongateway/2010-downtown-design-guidelines.pdf>

The three overarching principles supported throughout the guidelines are:

1. **Contextual Fit**
2. **Pedestrian Friendly Streets**
3. **Sustainability**

## **DRC Public Project Guidelines**

### **Introduction** (excerpts)

Public projects and structures, whether they are parks, streets, bridges, governmental buildings, or above ground utility installations are the most physical and visual aspect of the roles government plays in the lives of the citizens of Spokane.

In current times it is all too often that budgetary constraints become the overriding concern dictating the design and the resulting “presence” of today’s public projects and structures. Instead of being a source of pride they may become a source of criticism and project an image of impermanence that is destructive to the particular public institution. For these reasons public projects and structures should be held to a higher standard of design, be required to comply with all adopted standards and policies, be consistent with adopted Neighborhood or District Plans, as well as serve as a positive example of how to incorporate resource conservation into a project.

### **Guidelines** (excerpts)

- **A. Site Design and Orientation**
- **Building Design (if applicable)**
  - B. Landscape Design**
  - C. Infrastructure Design**
  - D. Public Space**

Guidelines that may be especially relevant to the CSO 26 Project include:

- **DRC Public Project Guidelines - A.3 Pedestrian Access & Amenities. 3.** Layout of sidewalks should respond to direct movement patterns. Circulation patterns and design forms should be very clear and understandable. Walkways and pedestrian areas should be defined with elements such as trees, shrubs, lighting, and hard-scape.

- **DRC Public Project Guidelines - B.4 Signage. Design Objective:** The design and use of signage is a critical element in the overall appearance and function of a project and shall be designed to be functional, subtle, and efficient, with the purpose of way-finding and identifying uses.
- **DRC Public Project Guidelines - B.5 Lighting. Design Objective:** Lighting shall be provided for public projects and structures to improve the safety and security during the evening hours and enhance the character and quality of the facility. The form, quantity and character of lighting and the quality of light shall establish an attractive, distinctive and safe environment, but shall not create an unwanted nuisance for residential or other sensitive areas.
- **DRC Public Project Guidelines - C.1 General Landscape Design. Discussion:** Significant landscape elements such as mature trees, stands of native vegetation, topography, and rock outcroppings should be considered as design determinants and incorporated into the site and landscape design, particularly if due to a unique specimen, size, or historical association. Landscape plantings shall be incorporated into projects to define and emphasize building entrances, enhance and define pedestrian areas, and screen obtrusive elements.
- **DRC Public Project Guidelines - E.1 Public Spaces Design. Design Objective:** Public Spaces shall be developed in a manner that promotes social interaction, and makes the safety, convenience and enjoyment of the user the primary design parameters.

## Topics for Discussion

To address the Downtown and Shoreline Design Standards, Comprehensive Plan Policies, and Public Project Design Guidelines listed in the staff report, staff would offer the following for consideration and discussion:

1. **Views of the site from surrounding vantage points including Veteran's Park and Monroe Street Bridge and Huntington Park.** How will views from these areas be enhanced? Will material and vegetation complement the natural characteristics of the shoreline? Will these views express that this is a public area to be explored? How will the design frame and provide focus on newly created views/focal points from the plaza and bikeway?

*Please See Downtown Design Guideline A-1 and B-3*

2. **Pedestrian circulation and grading.** The proposal as submitted includes a series of steps ramps and landings. Are there other more seamless ways of integrating ADA access to the poem plaza, proposed lower overlook and bike trail extending to the westward from the project site? An example of this type of approach to ADA access is shown in the 7.14.2015 concept plan.

In light of the recent updates to the design, how can the access ramp that leads the multi-use trail be designed to be more pedestrian oriented and help to guide users to the trail?

*Please See Downtown Design Guideline C-1, C-4, C-7 and D-5*

3. **Public Art Ventilation Stack.** Several options have been shown to conceal the ventilation stacks within the public plaza. Which concepts are being most strongly considered?

*Please See Downtown Design Guideline E-3*

4. **CPTED principles employed?** Will the bike trail access feel too hidden or dark to feel safe? Will there be security cameras located on or near the site?

*Please See Downtown Design Guideline D-6 and D-7*

5. **Street Trees.** What creative options are available to meet the code requirement for street trees along Spokane Falls Blvd.?"

*Please See Downtown Design Guideline D-8*

6. **Materials and lighting.** During the recommendation meeting please present the proposed built elements including walls, lighting, handrails, paving, and interpretive signage (as applicable). An elevation or additional information on the proposed character along Spokane Falls Blvd and the proposed bike trail would be helpful.

*Please See Downtown Design Guidelines C-6 and D-7*

## **Note**

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of Planning and Development Services.

## **Policy Basis**

Spokane Municipal Codes  
City of Spokane Comprehensive Plan  
Downtown Design Guidelines  
Public Project Design Guidelines



Spokane Design Review Board Submittal for:  
**Spokane Falls Combined Sewer Overflow  
(CSO 26) Control Facility  
Public Space and Surface Treatments**

**DRB Meeting #2**

Submittal Date: March 1, 2017

Meeting Date: March 22, 2017



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### Graphic Attachments

Attachment #1 – Rendered Site Plan
Attachment #2- North Elevation Character
Attachment #3 – East Section/Elevation Character
Attachment # 4 – Ventilation Location and Materials Plan
Attachment #5 – Grading Plans (In Progress)
Attachment #6 - Planting Plans (In Progress)
Attachment #7 – Mechanical Enclosure Plan (In Progress)
Attachment #8 – Lighting Models

City of  
Spokane  
Planning Services  
Department



# Design Review

## Standard Board Review Application

<b>NAME OF PROJECT:</b>	
Spokane Falls Combined Sewer Overflow (CSO 26) Control Facility - PLAZA	
<b>ADDRESS:</b>	
218 N. Monroe Street	
<b>TYPE OF PROJECT:</b>	
<input checked="" type="checkbox"/> Public Project <input type="checkbox"/> Shoreline Conditional Use Permit <input type="checkbox"/> Skywalk Over Public ROW	<input type="checkbox"/> Required by CBD Zones and Downtown Plan <input type="checkbox"/> Design Departure
<b>FEES:</b>	
Standard Board Review Paid for by Interfund Charge - Utilities	
<input type="checkbox"/> \$1275 (up to 3 meetings)	<input type="checkbox"/> \$500 per additional meeting if necessary

<b>APPLICANT:</b>	
<b>Name:</b> City of Spokane - Utilities	
<b>Address:</b> 808 W. Spokane Falls Blvd.	
<b>Phone (home):</b>	<b>Phone (work):</b> (509) 625-6000
<b>Email address:</b>	
<b>PROPERTY OWNER:</b>	
<b>Name:</b> City of Spokane	
<b>Address:</b> 808 W. Spokane Falls Blvd.	
<b>Phone (home):</b>	<b>Phone (work):</b> (509) 625-6000
<b>Email address:</b>	
<b>AGENT:</b>	
<b>Name:</b> BWA Architects PC - Bill LaRue - Hansen Consulting, Steve Hansen	
<b>Address:</b> 153 S. Jefferson Street	
<b>Phone (home):</b>	<b>Phone (work):</b> (509) 838-4511
<b>Email address:</b> blarue@bwarch.com shansen2162@gmail.com	

<b>REPRESENTATIVE SIGNATURE:</b>	<b>DATE:</b>
	July 6, 2016

<b>DEPARTMENT USE ONLY:</b>
<b>Submittal Date:</b>
<b>Accepted as Complete:</b>
<b>Design Review Committee Meeting Date:</b>

## OVERVIEW OF DRB MEETING #1

The CSO 26 Plaza is a complex and very public project. At the first design review board meeting several topics were thoroughly discussed. Focus was generally centered on views of the tank from surrounding vantage points, concerns for pedestrian circulation, questions on incorporation and opportunities for public art, security and safety principles, options for including street trees and the materials and lighting being used.

## STAFF RECOMMENDATION RESPONSE

Following the first DRB meeting in July, five recommendations were made by staff regarding the CSO 26 Plaza and its design. Below are those recommendations and the design team's response. In addition, a series of attachments have been included at the back of this submittal which aid in the understanding of the project's progress and aid in answering the below questions. Included in those attachments is an updated, rendered site plan, showing the current layout for the plaza with updates in surface materials and patterning (Attachment #1).

1. Applicant shall consider further articulation of the north wall to better incorporate man-made structures with the natural landscaping, mitigating the bulk of the tank, and elevating the design quality of the multi-use pathway to same degree as the plaza.

- Response: See Attachment #2, North Elevation Character, for current design articulation for the north wall of the CSO tank. Exposed tank walls are currently planned to be constructed with a formliner texture for the entirety. The texture planned for the north tank wall is a 2" basalt relief pattern. In addition to the formliner texture, faux ballast rock carvings are intended for installation along the north bank to soften and naturalize the appearance of the tank structure. Faux basalt carvings shall be similar to those shown in adjacent images (Exhibits #1 and #2). Trellis structures previously shown along the north wall of the tank have been removed. There has been some concern that people could climb such structures. The trail and plaza guardrail design has developed into a decorative railing with wave pattern, similar to the ones found at the Gathering Place near City Hall. See Attachment #3, East Section/Elevation Character for additional articulation and massing imagery. Native plantings have been included at the base of the tank structure and at the east end of the design. Ornamental plantings have been included at the west end of the design and atop the plaza. A current layout of plantings at the base of the tank can be found on draft planting plans, Attachment #6.

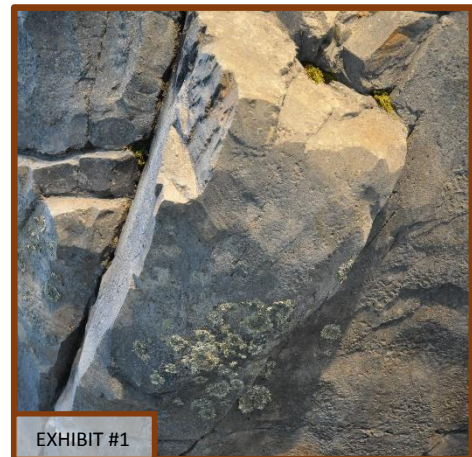
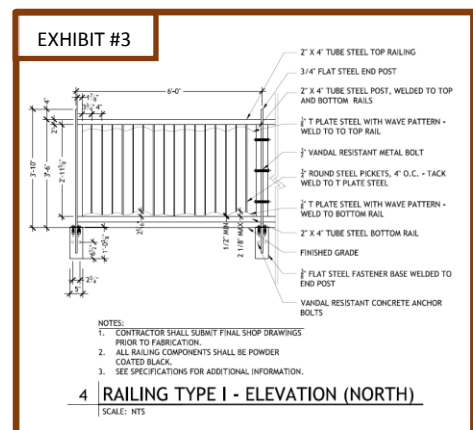


EXHIBIT #1



EXHIBIT #2



2. Applicant shall consider odor control and the location of exhaust venting.

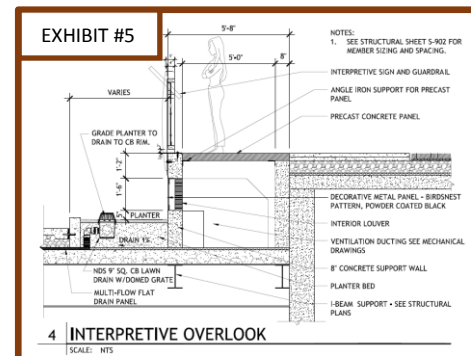
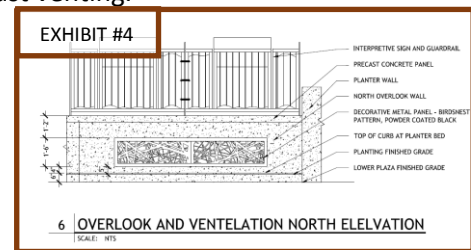
- Response: See Attachment #4 for the updated odor control ventilation outlet location, highlighted in yellow, which is now shown below the tank on the hillside near the west end of the design. Fresh air intake venting has been located just under the interpretative overlook areas (shown in orange on attachment), on the face of the wall between the upper and lower plazas. Fresh air intake location construction is shown in the north elevation and adjacent section (Exhibit #4 and #5). Decorative metal louvers from Revamp are planned to screen ventilation louvers (Exhibit #6).

3. Applicant shall consider opportunities within the design and budget to provide pedestrian connection directly to the lower Huntington Park.

- Response: After further exploration of the topographical survey and site visits it was determined that a pedestrian connection to Huntington Park would not be feasible. Grades are 1.5:1 +/- below the tank in most locations (Exhibit #7). The design team suggests that the city pursue a holistic wayfinding signage package that would work to navigate people between Riverfront Park, Huntington Park and the CSO plaza.

4. Applicant shall consider engaging artists, as early as possible, for visioning and creation of artistic components within this project.

- Response: The design team and the Spokane Arts Commission, led by Karen Mobley, are currently in the processes of selecting an artist to produce work for the CSO 26 public plaza. Following the recent call to artists, three individuals have been shortlisted and given a design contract, complete with a set of parameters the artist must meet. The Arts Commission, design team and Spokane Tribe plan to meet with the artists to review the project, site and answer questions in early March. Final locations for art are still under review and will be better established after the previously mentioned March meeting. Ultimately, the art pieces created will be integrated into the overall plaza design with accommodations for accent lighting. The three short listed artists selected are Jeff Ferguson teamed with Smoker Marchand, Sarah Thompson Moore and Joseph Rastovich. Artist bios and sample work can be found below and on the following page.
  - **Jeff Ferguson** (Spokane) and **Smoker Marchand** (Colville) team: This is a collaborative effort between two local artists. Both Jeff and Smoker are members of local tribes and committed to telling the story of the Spokane areas native people and our natural history. Smoker has completed several metal sculptures reflecting Native American culture (Exhibits #8 and #9).





- **Sarah Thompson Moore** (Cow Creek): Sarah's most recent work is an iconic structure named Everything In Between, in New York. She is an experienced artist decorated with several awards with professional experience dates back to 2008 and establishes roots back in the Inland Northwest. Sarah is a sculptor who uses a variety of materials including metal, concrete and even textile. (Exhibit #10)
- **Joseph Rastovich**: Joseph is a young artist out of the Tri City area. He began creating art professionally at the age of 15 and has continued since. Joseph specializes in sculpture that is bold and tells a story, and enjoys the exploration of finishes and multi-medium pieces (Exhibits #11 and #12).

5. Applicant shall consider the priority of a retail space on the site in lieu of expanding the plaza.

- Based on input from the city, retail space has been removed from the design for the CSO 26 plaza space. Trail access via Spokane Falls Boulevard is now located near where the retail space previously was placed. See Attachment #1 for the most current rendered site plan and Attachment #3 for trail character on the east end of the design.



EXHIBIT #8 - Smoker Marchand



Exhibit #9 - Smoker Marchand



EXHIBIT #10 - Sarah Thompson Moore

## UPDATED PLAZA DEVELOPMENT AND PROJECT OVERVIEW

The City of Spokane has committed to reduce the amount of untreated overflows from combined sanitary and stormwater that reach the Spokane River. As part of city's Integrated Clean Water Plan, one strategy the city is implementing with the Wastewater Combined Sewer Overflow (CSO) program is building underground concrete tanks strategically located in the city. These tanks hold the combined wastewater during a storm event and meter it back to the wastewater treatment plant when the storm surge subsides. Several of these tanks have already have been built as part of an overall network throughout the city.



EXHIBIT #11 - Joseph Ratstovich



EXHIBIT # 12 -  
Joseph Rastovich

The location for the Spokane Falls CSO 26 Control Facility was chosen due to a variety of factors such as geographic requirements, property the city already owns, cultural and regulatory sensitivities, soil contamination etc. The 2.2 million gallon CSO 26 Control Facility is approximately 300 feet long and 62 feet wide. It will be built partially under Spokane Falls Boulevard between city hall and the Monroe Street Bridge overlooking the Spokane River and Huntington Park. Existing utilities under Spokane Falls

Boulevard restrict the tank from being totally located under the roadway so approximately 22 feet of the tank will be exposed on the north side, along the south river bank.

Bernardo Wills Architects was contracted, as a sub-consultant to AECOM, to design the plaza space above the tank, at surface or street level. The plaza is two tiered, with an upper and lower plaza deck, separated by 3.5' in elevation change. The upper plaza level being at the same elevation as the adjacent Spokane Falls Boulevard. The maintenance space and odor control equipment sits below the plaza and on top of the tank. The design takes the exposed tank elevation into account and uses various design elements and techniques to minimize any adverse visual impact the tank may have.

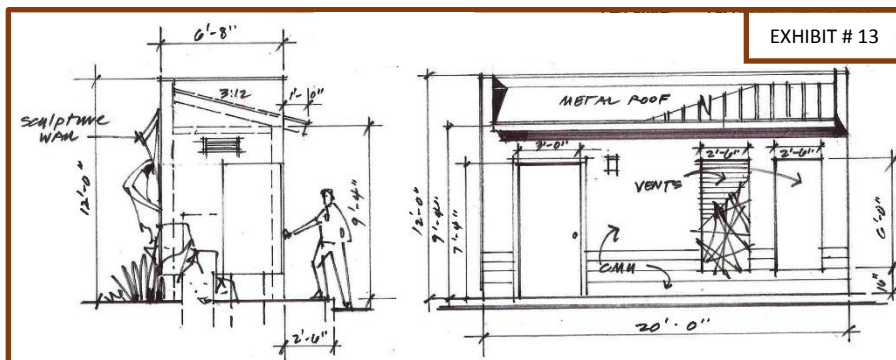
The size and elevation of the tank structure along Spokane Falls Boulevard provides a large space that provides great opportunities to accommodate public gatherings, interpretation of Spokane's History/Culture, Geography and CSO program, tell the story of the river, public art, excellent view overlooks of the Spokane River Gorge, Falls and Monroe Street Bridge etc. Proposed elevation changes on the top of the tank, along with roof garden landscape plantings provide a sense of place, interest and seating opportunities while maintaining views of the river from the Public Library. Maintenance access to the tank is provided via the shared recreational, multi-modal trail that is planned to eventually connect downtown to the Peaceful Valley Neighborhood and Glover Field. The trail also provides visual articulation on the north side of the tank that helps visually break up the vertical scale of the tank.

Site amenities such as site and accent lighting, a water feature to aid in the telling of the story of the river, redeveloped public art "That Place where the Ghosts of Salmon Jump", benches, solar powered trash receptacles, interpretive signage, overlook structure, shade structures, public art, accessible spaces, decorative pavements, etc., are all design elements incorporated to create that sense of place.

### CHANGES IN DESIGN SINCE DRB MEETING #1

As design for the plaza evolved, naturally many of the design elements previously shown have too evolved. One of the more significant changes is the project boundary, which has expanded to include improvements on the northern edge of the Spokane Public Library between the library and Spokane Falls Boulevard. These areas have been designated as the east and west library plazas and can be seen in the updated rendered site plan, Attachment #1. Design improvements for the library plazas are planned to include relocation of a refuse facility, library sign wall, new lighting, landscaping and decorative hardscape. You may also notice that the decorative intersection and crossings previously shown has been removed from the design per request of the City.

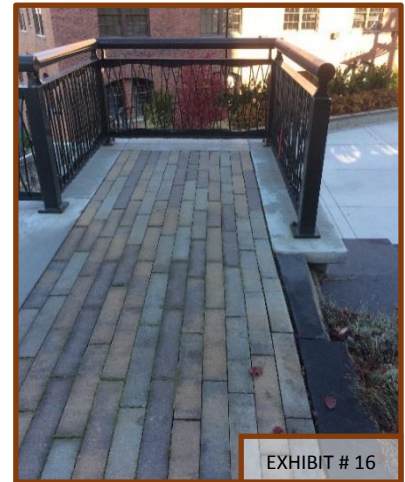
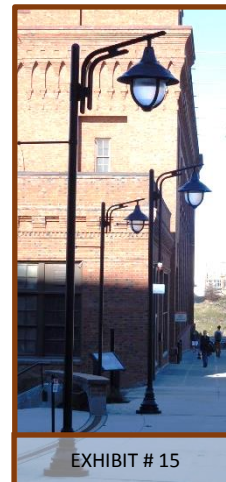
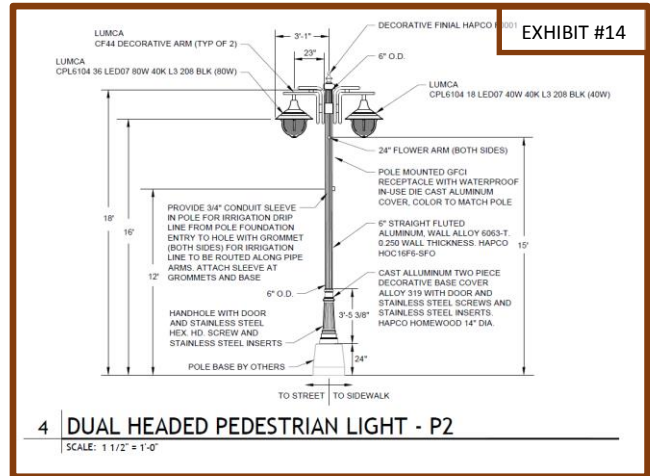
Utility locations and access requirements have played a significant role in the plaza's current configuration. With the requirement for a boom truck to access dry pits, a truck service back in area was introduced. Surface



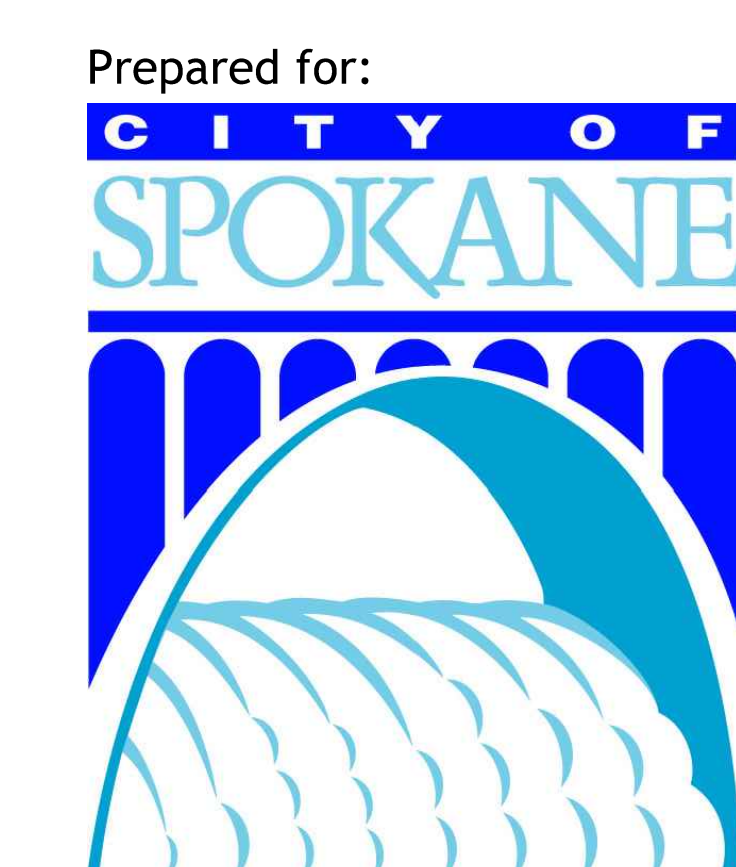
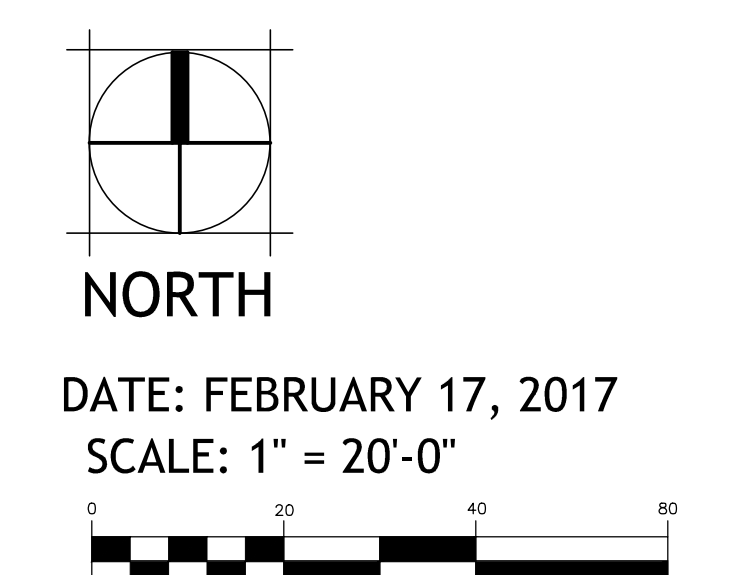
layout has continued to respond to various access hatch locations, working them into the adjacent hardscape score patterns so they appear to fit within the layout. A utility/mechanical building enclosure was introduced in the western end of the plaza, near the truck back in area, to house required equipment (Exhibit #11). Draft plans for the enclosure can be found in Attachment #7. It is the intent of the design team that eventually a portion of the building be wrapped in an artistic graphic or mural.

The design team has been working to refine material selections for the plaza and unify the space with that adjacent Huntington Park. Plank pavers like those used in the Gathering Place and similar decorative railings have been incorporated into the space, along with powder coat colors to match others used nearby. City standard lights like those found near the Gathering Place have been selected for use along the street and will be supplemented with accent lighting.

Plaza lighting design has developed significantly since the July meeting. As previously mentioned the standard city lights (Exhibit #14 and #15) will be located along the street. In addition, accent lighting will be placed throughout the plaza. Lighting models have been included for reference as Attachment #8 and show conceptually what the space will look like at night with this accent lighting. Accent lighting includes a series of tube or stick lights, low wall lights along the plaza's north edge and cube seat lights at gathering locations. Street lights are not included in the model attachment at this time.

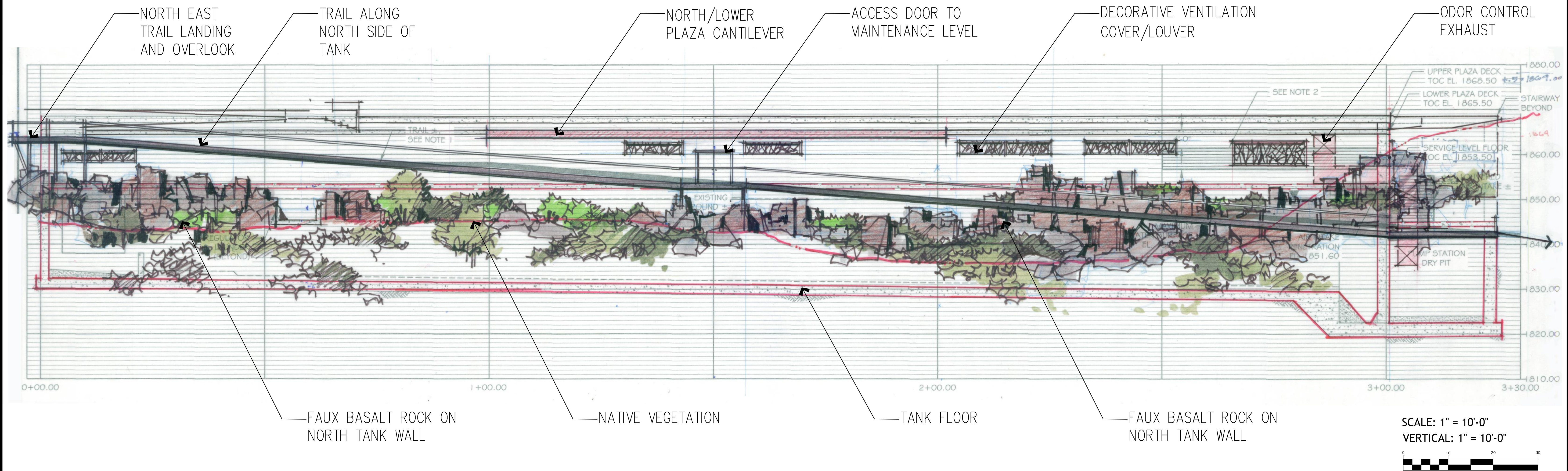






# SPOKANE FALLS CSO 26 CONTROL FACILITY





DATE: FEBRUARY 17, 2017

DESIGN BUILD TEAM:



**AECOM**

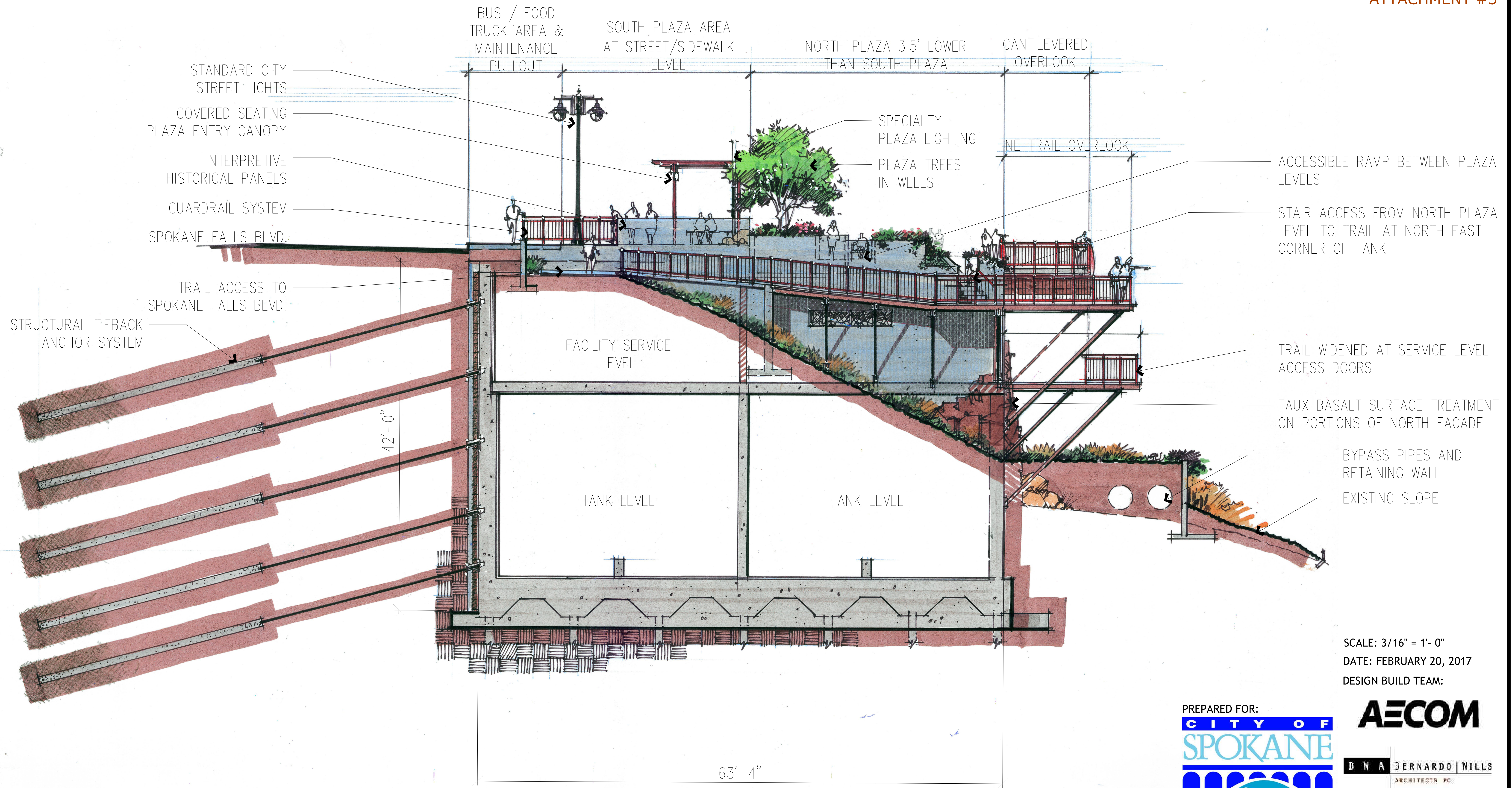
**B W A BERNARDO WILLS**  
ARCHITECTS PC

**GARCO**

NORTH ELEVATION CHARACTER

SPOKANE FALLS CSO 26 CONTROL FACILITY





## EAST SECTION/ELEVATION CHARACTER

# SPOKANE FALLS CSO 26 CONTROL FACILITY



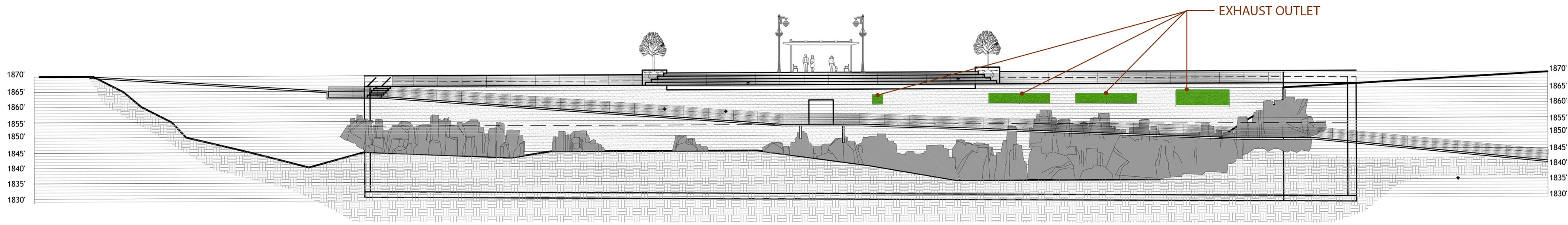
**AECOM**

**B W A BERNARDO WILLS**  
ARCHITECTS PC

**GARCO**

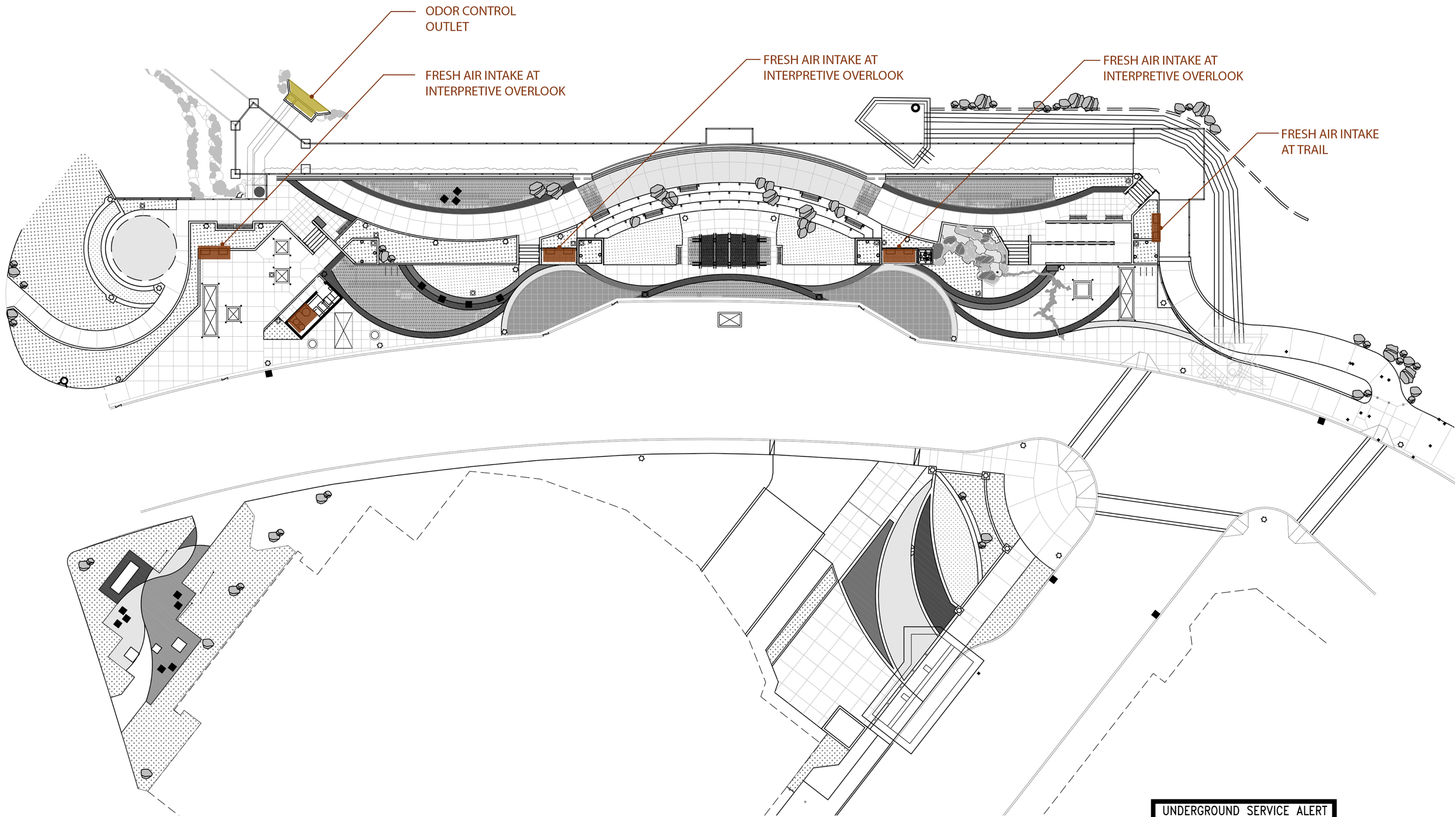
SCALE: 3/16" = 1'-0"  
DATE: FEBRUARY 20, 2017  
DESIGN BUILD TEAM:





1 NORTH WALL ELEVATION

SCALE: 1" = 20'-0"  
VERT: 1" = 20'-0"  
0 20 40 60



MATERIALS LEGEND

- CONCRETE FLATWORK  
COLOR: NATURAL GREY FINISH: MEDIUM BROOM
- CONCRETE FLATWORK - FULL DEPTH COLOR  
COLOR #1 EXPOSED AGGREGATE
- CONCRETE FLATWORK - FULL DEPTH COLOR  
COLOR #2 SALMON 10134: FINISH: MEDIUM BROOM
- CONCRETE FLATWORK - FULL DEPTH COLOR  
COLOR #3 PALOMINO 5447: FINISH: MEDIUM BROOM
- CONCRETE FLATWORK - FULL DEPTH COLOR  
COLOR #4 LAKESIDE BROWN 6804: FINISH: MEDIUM BROOM
- PLANK PAVERS
- SHURB BEDS
- LAWN AREAS
- SCORE JOINT - TOOLED OR SAW CUT WITH SPECIALTY BLADE.
- ISOLATION JOINT - DOWELED JOINT WITH EXPANSION MATERIAL. COLD JOINTS BETWEEN COLOR POURS SHALL BE DOWELED.
- BIKE RACKS
- BOLLARDS
- LIGHT TYPE I
- LIGHT TYPE II
- BENCHES
- LIGHT CUBE BENCHES - SEE ELECTRICAL PLANS
- WOOD BENCHES, SURFACE MOUNTED
- CARVED SHOTCRETE LANDSCAPE BOULDERS
- SOLAR TRASH COMPACTOR

MATERIAL NOTES

- CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO INITIATION OF ANY DEMOLITION OR CONSTRUCTION OPERATIONS. ANY DAMAGE TO EXISTING UTILITIES ON SITE OR ADJACENT PROPERTY SHALL BE CONTRACTOR'S RESPONSIBILITY TO CORRECT. COORDINATE ALL DEMOLITION AND CONSTRUCTION OPERATION WITH ARCHITECTURAL, CIVIL AND ELECTRICAL ENGINEERING SHEETS.
- COORDINATE INSTALLATION OF ELECTRICAL AND IRRIGATION CONDUIT AND SLEEVES WITH RESPECTIVE CONTRACTORS.
- PROVIDE TOOLED SCORE JOINTS AS SHOWN ON PLANS. SCORE JOINTS ARE AN INTEGRAL PART OF THE DESIGN AND SHALL NOT VARY FROM PATTERNS AND LOCATIONS SHOWN. CONTRACTOR SHALL REMOVE ANY FLATWORK THAT DOES NOT CONFORM TO DESIGN.
- CONTRACTOR SHALL REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- CONTRACTOR RESPONSIBLE FOR CORRECTING ANY DAMAGE TO NEW OR EXISTING CONCRETE FLATWORK, ASPHALT, TURF AREAS OR ANY OTHER EXISTING ELEMENTS AS A RESULT OF CONSTRUCTION ACTIVITIES.
- IN THE EVENT OF A DISCREPANCY, IMMEDIATELY NOTIFY LANDSCAPE ARCHITECT.



2 MATERAIL PLAN

SCALE: 1" = 20'-0"  
0 20 40 60



PRELIMINARY  
NOT FOR CONSTRUCTION



DELL R. HATCH  
CERTIFICATE NO. 1162

BWA BERNARD WILLS  
ARCHITECTS PC

L-1  
# OF #

NAVD88 = (OLD CBM ELEV.) - (13.13)		AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)	
BENCH MARK LOCATION: BEING A 3" BRASS CAP SET IN CONCRETE INSIDE OF A VISDOT MONUMENT CASE, MARKED VISDOT, DO NOT DISTURB. 1998 GPS/DOING 188 SURVEY MONUMENT LOCATED APPROXIMATELY AT 2ND AVENUE AND FISKE STREET, WITH A PUBLISHED ELEVATION OF 1926.46 FEET (NAVD88).		CURRENT C.O.S. DRAFTING STANDARDS ADOPTED FEB. 2007	
NAVD88 ELEV. 1926.46	NAVD88 DATUM	BY	DATES
CBM NO. 1926.46	CBM #	DRAWN	02/20/17
NAVD88 DATUM	NAVD88 DATUM	REVISED	02/20/17
NAVD88 DATUM	NAVD88 DATUM	DESIGN	02/20/17
NAVD88 DATUM	NAVD88 DATUM	APPROVED	02/20/17



CITY OF SPOKANE, WASHINGTON  
DEPARTMENT OF ENGINEERING SERVICES  
808 WEST SPOKANE FALLS BLVD.  
SPOKANE, WASHINGTON 99201-3343  
(509) 625-6700

PROJECT NAME:  
SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:  
MATERIAL PLAN

PROJECT LIMITS:

TYPE OF IMPROVEMENT:	
CITY PROJECT NUMBER	CITY PLAN NUMBER
2010088	SPOKA B(1)2
EPN	18-25-43

CALL BEFORE YOU DIG 1-800-424-5555

IN-PROGRESS (100%) DESIGN SUBMITTAL

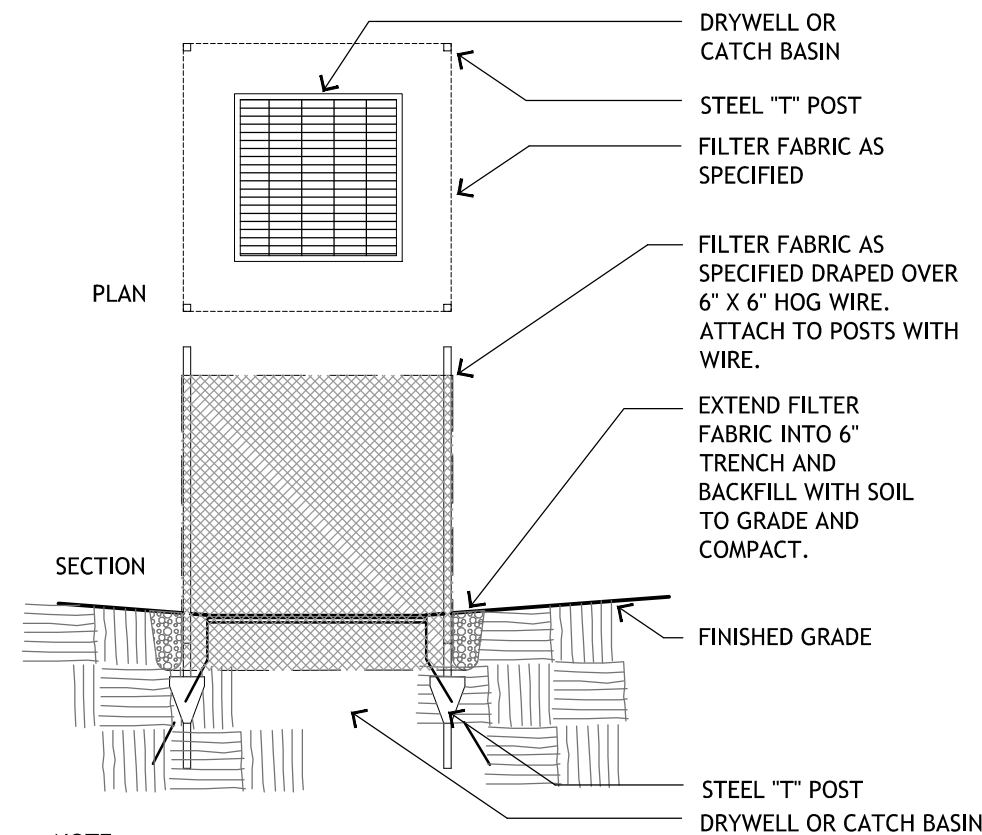


LEGEND

- BM #3 BENCH MARK ASSUMED ELEV 4477.60
- FG 75.22 PROPOSED FINISHED GRADE SPOT ELEVATION (18 PREFIX LEFT OFF)  
FG = FINISH GRADE
- 1905 PROPOSED 5' INDEX CONTOUR
- 1' CONTOUR
- FLOW DIRECTION
- 0.5% SLOPE DIRECTION
- DRAIN
- DRYWELL
- ACO K200 TRENCH DRAIN W/INLINE CATCH BASIN
- GRADE BREAK LINE
- HP HIGH POINT
- LP LOW POINT
- TC/BC TOP OF CURB/BOTTOM OF CURB
- TR/BR TOP OF RAMP/BOTTOM OF RAMP
- TS/BS TOP OF STAIR/BOTTOM OF STAIR
- TW/BW TOP OF WALL/BOTTOM OF WALL
- TSW/BSW TOP OF SEAT WALL/BOTTOM OF SEAT WALL
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- FG FINISHED GRADE
- FFE FINISHED FLOOR ELEVATION
- TD TRENCH DRAIN RIM ELEVATION
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GRADING NOTES

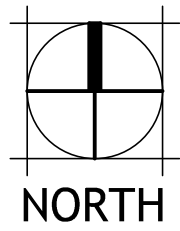
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- NOTE:
- INSPECT PERIODICALLY AND REPAIR OR REPLACE AS REQUIRED.
  - REMOVE SEDIMENT ACCUMULATIONS WHEN FILTER CAPACITY IS IMPAIRED.

2 INLET FILTER

SCALE: NTS



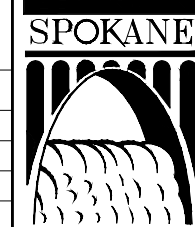
GRADING PLAN - WEST

SCALE: 1" = 10'-0"



DATE	BY	PROJ	DESCRIPTION	DATE	BY	PROJ	E.F.N. - U.S.N.	FROM	TO	COUNCIL	FROM	TO	ORD. NO.	DATE	FILE NO.
			REVISIONS							AS BUILT					

NAVDS88 - (OLD) CDM ELEV. - (1.3.1.3)	AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVDS88)
BENCHMARK LOCATION: BEING A 3" BRASS CAP SET IN CONCRETE INSIDE OF A WSDOT MONUMENT CASE, MARKED WSDOT, DO NOT DISTURB, 1998, GP32090-150, SURVEY MONUMENT, LOCATED APPROXIMATELY AT 2ND AVENUE AND FISKE STREET, WITH A PUBLISHED ELEVATION OF 1926.46 FEET (NAVDS88)	
NAVDS88 ELEV. 1926.46	
CDM NO. 1926.46	
CDM #	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALED ACCORDINGLY	
HORIZONTAL PLAN/PROFILE	
VERTICAL PROFILE ONLY	
SCALE	



CITY OF SPOKANE, WASHINGTON  
DEPARTMENT OF ENGINEERING SERVICES

808 WEST SPOKANE FALLS BLVD.  
SPOKANE, WASHINGTON 99201-3343  
(509) 625-6700



**PRELIMINARY**  
**NOT FOR CONSTRUCTION**

BWA BERNARDO WILLS  
ARCHITECTS PC



DELL R. HATCH  
CERTIFICATE NO. 1162

L-200  
# OF #

PROJECT NAME:  
SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:  
GRADING PLAN - WEST

PROJECT LIMITS:

TYPE OF IMPROVEMENT:	CITY PROJECT NUMBER	CITY PLAN NUMBER
	2010088	SPOKA B(1)2 18-25-43





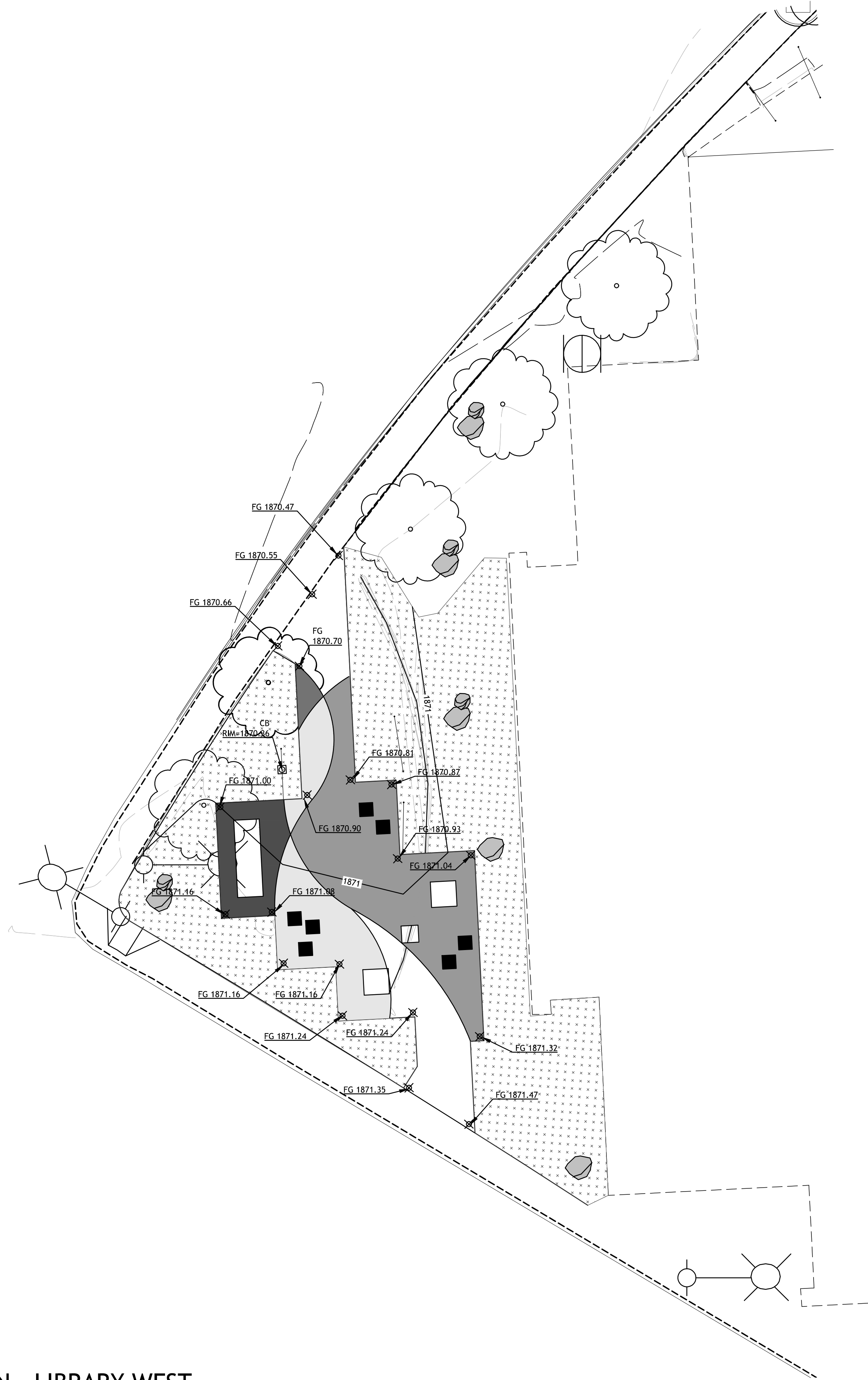
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CALL BEFORE YOU DIG 1-800-424-5555



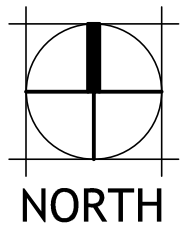


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GRADING PLAN - LIBRARY WEST

SCALE: 1" = 10'-0"



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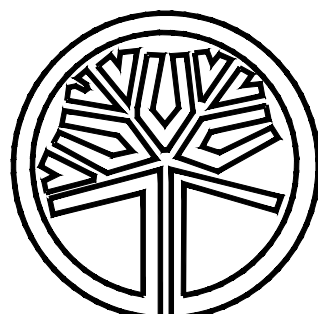
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IN-PROGRESS (100%) DESIGN SUBMITTAL



STATE OF  
WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT

DELL R. HATCH  
CERTIFICATE NO. 1162

PRELIMINARY  
NOT FOR CONSTRUCTION

B W A BERNARDO WILLS  
ARCHITECTS PC

L-203  
# OF #

PROJECT NAME:  
SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:  
GRADING PLAN - LIBRARY EAST

PROJECT LIMITS:

TYPE OF IMPROVEMENT:  
CITY PROJECT NUMBER  
2010088  
CITY PLAN NUMBER  
SPOKA B(1)2  
18-25-43

CALL BEFORE YOU DIG 1-800-424-5555



CITY OF SPOKANE, WASHINGTON  
DEPARTMENT OF ENGINEERING SERVICES  
808 WEST SPOKANE FALLS BLVD.  
SPOKANE, WASHINGTON 99201-3543  
(509) 625-6700



CURRENT C.O.S. DRAFTING  
STANDARDS ADOPTED FEB.2007

BY  
DRAWN:  
REVISED:  
DESIGN:  
APPROVED:

SCALE

HORIZONTAL  
PLAN/PROFILE  
VERTICAL  
PROFILE ONLY

IF NOT ONE INCH ON  
THIS SHEET, ADJUST  
SCALES ACCORDINGLY

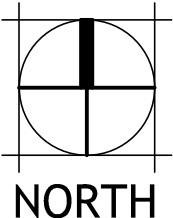
NAVDS88 = (OLD CBM ELEV.) - (13.13)  
AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVDS88)  
BENCHMARK LOCATION: BEING A 3" BRASS CAP SET IN CONCRETE INSIDE OF A WSDOT MONUMENT  
CASE, MARKED WSDOT, DO NOT DISTURB. 1998, SP33205-1 RG. SURVEY MONUMENT, LOCATED  
APPROXIMATELY AT 2ND AVENUE AND FISKE STREET, WITH A PUBLISHED ELEVATION OF 1926.46  
FEET (NAVDS88)  
NAVDS88 ELEV. 1926.46  
CBM NO. CBM #  
NAVDS88 DATUM

GRADE ORDINANCE LIST

AS BUILT

REVISIONS

DATE	BY	PROJ.	DESCRIPTION	DATE	BY	PROJ.	E.F.N. - U.S.N.	FROM	TO	COUNCIL ACCEPT DATE	FROM	TO	ORD. NO.	DATE	FILE NO.



GRADING PLAN - LIBRARY EAST

SCALE: 1" = 10'-0"









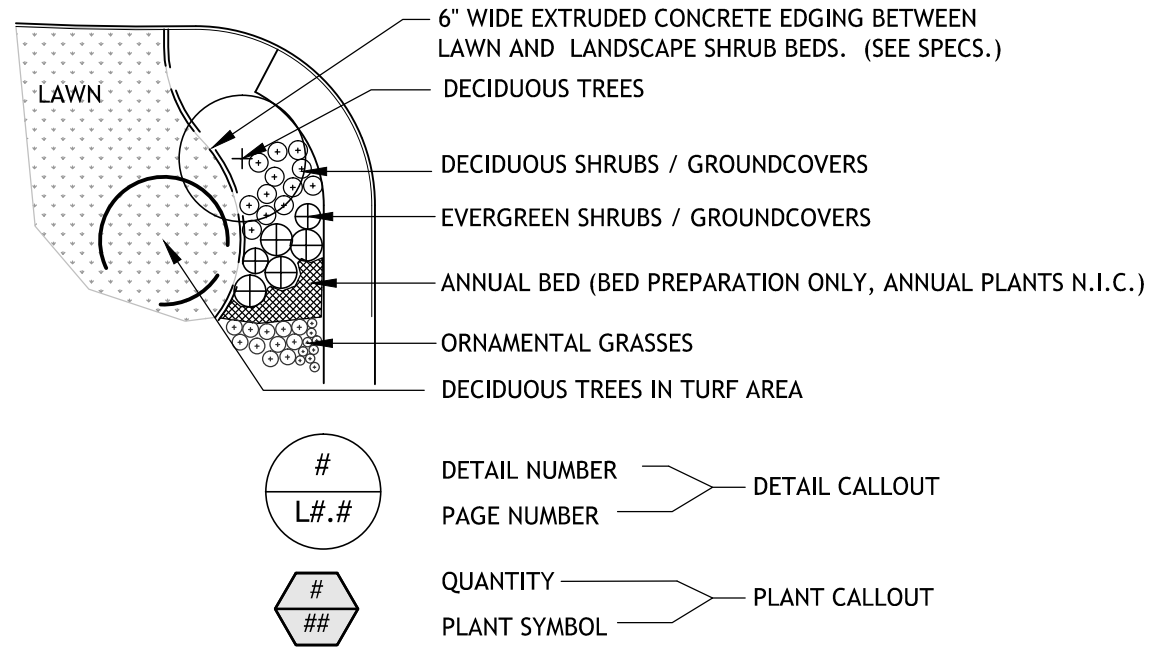
LANDSCAPE NOTES

1. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO INITIATION OF ANY DEMOLITION OR CONSTRUCTION OPERATIONS. ANY DAMAGE TO EXISTING UTILITIES ON SITE OR ADJACENT PROPERTY SHALL BE CONTRACTOR'S RESPONSIBILITY.
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6. ALL TURF AREAS SHALL RECEIVE (6") OF TOPSOIL. ALL PLANTING BEDS SHALL RECEIVE (18") OF TOPSOIL. TOPSOIL SHALL CONSIST OF 75% IMPORTED TOPSOIL AND 25% OF COMPOST THAT HAS BEEN THOROUGHLY MIXED.
7. ALL SHRUB BEDS SHALL BE LINED WITH WEED FABRIC (TYPAR 3201 OR EQUAL) AND HAVE A MIN. OF 3" TOPDRESSING INSTALLED OVER THE TOP OF THE FABRIC. TREAT TOPDRESSING WITH PRE EMERGENT PER DETAILS.
8. SHRUB BED EDGING SHALL BE CONCRETE MOWSTRIP. IT SHALL SEPARATE ALL GRASS AREAS FROM PLANTING BED LOCATIONS. SEE PLANS FOR SPECIFIC LOCATIONS.
9. FINISH GRADE OF SHRUB BEDS AFTER INSTALLATION OF MULCH SHALL BE WITHIN 1" OF TOP OF CURBS, SIDEWALKS AND SURROUNDING HARDSCAPE.
10. ALL PLANTS AND PLANT STAKES SHALL BE SET PLUMB UNLESS OTHERWISE NOTED.
11. ALL ROOT WRAPPING MATERIAL MADE OF SYNTHETICS SHALL BE REMOVED AT THE TIME OF PLANTING.
12. NO BARE ROOT STOCK SHALL BE USED UNLESS OTHERWISE NOTED IN CONTRACT DOCUMENTS.
13. CONTRACTOR IS RESPONSIBLE FOR LOCATING PROPERTY LINE AND WORKING WITHIN THE PROPERTY BOUNDARY.
14. 'BIOSTIMULANT FERTILIZER' SHALL BE : 'ROOTS' TRANSPLANT 1-STEP DRY AS MANUFACTURED BY ROOTS, INC., (203) 786-5295. APPLY AT MANUFACTURER'S RECOMMENDED RATES.
15. APPLY SCOTTS AGRIFORM 20-10-5-21 GRAM PLANTING TABLETS AT THE FOLLOWING RATES FOR ALL TREES AND SHRUBS/ORNAMENTAL GRASS: TREES - 4 TABS; SHRUBS/ORNAMENTALS - 182 GAL - 2 TABS, 5 GAL - 3 TABS. BACKFILL PLANTING HOLE HALFWAY THEN EQUALLY SPACE TABLETS AROUND ROOT BALL, APPROXIMATELY 1" AWAY FROM ROOTBALL. COMPLETE BACKFILL.
16. CONTRACTOR SHALL GUARANTEE ALL WORK, MATERIALS, AND PLANTS FOR A PERIOD OF ONE YEAR FROM THE DATE OF FINAL ACCEPTANCE.
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18. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY DAMAGE TO OR DEFACING OF NEW OR EXISTING CONCRETE FLATWORK, ASPHALT, TURF AREAS, AND ANY OTHER EXISTING OR NEW SITE ELEMENTS AS A RESULT OF CONSTRUCTION ACTIVITIES.

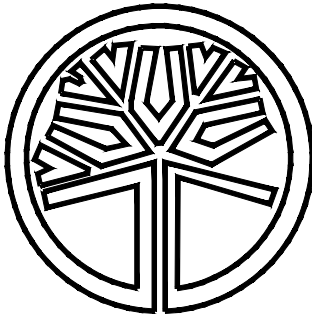
PLANT SCHEDULE

KEY	BOTANICAL NAME - COMMON NAME	QTY	SIZE	TYPE	REMARKS
PF	EVERGREEN TREES:				
	PINUS FLEXILIS 'VANDERWOLF PYRAMID' VANDERWOLF'S PYRAMID PINE		7' HIGH	B&B	MATCHED
AG	DECIDUOUS TREES:				
	ACER GINNALA 'MONDY' RED RHAPSODY AMUR MAPLE		2" CAL.	B&B	MATCHED
PC	PYRUS CALLERYANA 'CAPITAL' CAPITAL FLOWERING PEAR		2" CAL.	B&B	MATCHED
QR	QUERCUS ROBUR 'FASTIGIATA' COLUMNAR ENGLISH OAK		2" CAL.	B&B	MATCHED
AR	SHRUBS AND PERENNIALS:				
	AJUGA REPTANS 'CATLIN'S GIANT' CATLIN'S GIANT CARPET BUGLE	#1		CONT.	MATCHED
BT	BERBERIS THUNBERGII 'MARIA' SUNJOY GOLD PILLAR BARBERRY	#2		CONT.	MATCHED
BX	BUXUS X 'GREEN GEM' GREEN GEM BOXWOOD	#5		CONT.	MATCHED
CS	CORNUS SERICEA 'KELSEY' KELSEY RED DOGWOOD	#2		CONT.	MATCHED
DD	DIANTHUS 'DRAGON FRUIT' DRAGON FRUIT PINK	#1		CONT.	MATCHED
EF	EUONYMUS FORTUNEI 'EMERALD GAJETY' EMERALD GAJETY WINTERCREEPER	#1		CONT.	MATCHED
MR	MAHONIA REPENS CREEPING MAHONIA	#1		CONT.	MATCHED
PM	PHYSOCARPUS MALVACEUS MALLOW NINEBARK	#5		CONT.	MATCHED
PO	PHYSOCARPUS OPULIFOLIUS 'CENTER GLOW' CENTER GLOW NINEBARK	#5		CONT.	MATCHED
RF	RUDBECKIA FULGIDA 'GOLDSTRUM' GOLDSTRUM BLACK EYED SUSAN	#1		CONT.	MATCHED
RM	ROSA X 'MEIBOULKA' OSO EASY CHERRY PIE ROSE	#5		CONT.	MATCHED
RW	ROSA X WOODSII WOODS ROSE	#5		CONT.	MATCHED
SA	SYMPHORICARPOS ALBUS COMMON SNOWBERRY	#1		CONT.	MATCHED
CA	ORNAMENTAL GRASSES:				
	CALAMACROSTIS X ACUTIFLORA 'KARL FOERSTER' FOERSTER'S FEATHER REED GRASS	#2		CONT.	MATCHED
CE	CAREX ELATA 'BOWLES GOLDEN' BOWLES GOLDEN SEDGE	#1		CONT.	MATCHED
HS	HELICOTRICHON SEMPERVIRENS BLUE OAT GRASS	#1		CONT.	MATCHED
IC	IMPERATA CYLINDRICA 'RUBRA' JAPANESE BLOOD GRASS	#1		CONT.	MATCHED
PA	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY' LITTLE BUNNY DWARF FOUNTAIN GRASS	#1		CONT.	MATCHED

LANDSCAPE LEGEND



IN-PROGRESS (100%) DESIGN SUBMITTAL



STATE OF WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT

DELL R. HATCH  
CERTIFICATE NO. 1162

PRELIMINARY  
NOT FOR CONSTRUCTION

B N A BERNARDO HILLS  
ARCHITECTS PC

L-400  
# OF #

PROJECT NAME:  
SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:  
LANDSCAPE PLAN - WEST

PROJECT LIMITS:

TYPE OF IMPROVEMENT:

CITY PROJECT NUMBER:

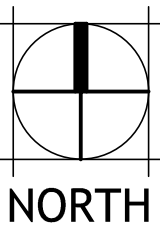
CITY PLAN NUMBER:

2010088

SPOKA B(1)2

18-25-43

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LANDSCAPE PLAN - WEST

SCALE: 1" = 10'-0"



DATE	BY	PROJ	DESCRIPTION	DATE	BY	PROJ	E.F.N. - U.S.N.	FROM	TO	COUNCIL ACCEPT DATE	FROM	TO	ORD. NO.	DATE	FILE NO.
REVISIONS															
AS BUILT															
GRADE ORDINANCE LIST															

NAVD88 = (OLD CBM ELEV.) - (1.3, 1.8)	AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)
BENCH MARK LOCATION: BONG A 3" BRASS CAP SET IN CONCRETE INSIDE OF A WISDOT MONUMENT CASE, MARKED WISDOT, DO NOT DISTURB, 1998, GP32090-166, SURVEY MONUMENT, LOCATED APPROXIMATELY AT 2ND AVENUE AND FISKE STREET, WITH A PUBLISHED ELEVATION OF 1926.46 FEET (NAVD88)	
NAVD88 ELEV. 1926.46	IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY
CBM NO.	CBM #
NAVD88 DATUM	SCALE

CURRENT C.O.S. DRAFTING  
STANDARDS ADOPTED FEB.2007

BY	DATES
DRAWN:	02/20/17
REVISED:	
DESIGN:	02/20/17
APPROVED:	



CITY OF SPOKANE, WASHINGTON  
DEPARTMENT OF ENGINEERING SERVICES

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SPOKANE, WASHINGTON 99201-3343  
(509) 625-6700

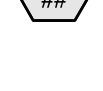




PLANT SCHEDULE					
KEY	BOTANICAL NAME - COMMON NAME	QTY	SIZE	TYPE	REMARKS
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PO	PHYSOCARPUS OPULIFOLIUS 'CENTER GLOW' CENTER GLOW NINEBARK		#5	CONT.	MATCHED
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RM	ROSA X 'MEIBOULKA' OSO EASY CHERRY PIE ROSE		#5	CONT.	MATCHED
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6" WIDE EXTRUDED CONCRETE EDGING BETWEEN  
LAWN AND LANDSCAPE SHRUB BEDS. (SEE SPECS.)  
DECIDUOUS TREES  
  
DECIDUOUS SHRUBS / GROUNDCOVERS  
  
EVERGREEN SHRUBS / GROUNDCOVERS  
  
ANNUAL BED (BED PREPARATION ONLY, ANNUAL PLANTS N.I.C.)  
  
ORNAMENTAL GRASSES  
  
DECIDUOUS TREES IN TURF AREA

DETAIL NUMBER ————  
PAGE NUMBER ————

QUANTITY ————  
PLANT SYMBOL ————

DETAIL CALLOUT  
PLANT CALLOUT



DELL R. HATCH  
CERTIFICATE NO. 1162

**PRELIMINARY  
NOT FOR CONSTRUCTION**

**BWA** BERNARDO | WILLS  
ARCHITECTS PC

L-401

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# OF #

PROJECT NAME: SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:

LANDSCAPE PLAN - EAST

PROJECT LIMITS:

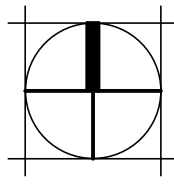
TYPE OF IMPROVEMENT:

<p> <math>\frac{1}{2} \times 100 = 50</math> </p>	<p> <math>\frac{1}{2} \times 100 = 50</math> </p>
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2010088	SPOKA B(1)
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## LANDSCAPE PLAN - EAST

SCALE: 1" = 10'-0"

[illegible]

NAD83 = (OIDI CBM ELEV.) - (1.13.1)		AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1986 (NAV86)	
BENCH MARK LOCATION: BING-A 3" BRASS CAP SET IN CONCRETE INSIDE OF A WISDOT MONUMENT CASE MARKED WISDOT, DO NOT DISTURB. 1958, G323020 - 166 SURVEY MONUMENT, LOCATED 100' NORTHEAST AT 2ND AVENUE AND FISHIE STREET, WITH A FINISHED ELEVATION OF 1936-46 FEET (NAV86).			
NAV86 ELEV.	1926.46		
CBM NO.	CBM #	BAR IS ONE INCH ON ORIGINAL DRAWING IF NOT ONE INCH ON THIS SHEET, ADJUST SIZES ACCORDINGLY	HORIZONTAL PLANIMETER PROFILE ONLY PROFILE ONLY
NAV86 DATUM		SCALE	BY: DATUM DRAWN: 02/20/01 REVISION: DESIGN: 02/20/01 APPROVED:



CITY OF SPOKANE, WASHINGTON  
DEPARTMENT OF ENGINEERING SERVICES

808 WEST SPOKANE FALLS BLVD.  
SPOKANE, WASHINGTON 99201-3343  
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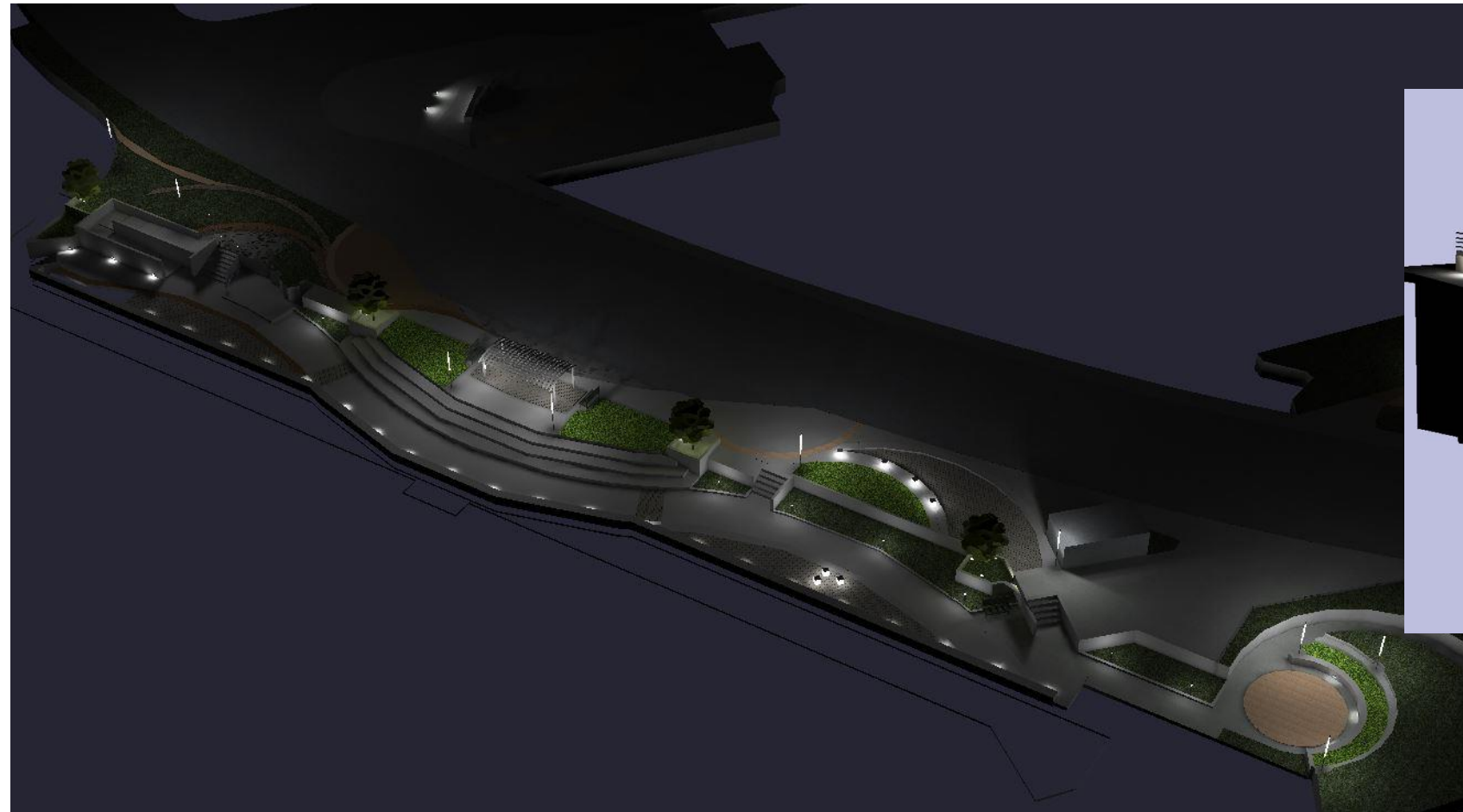






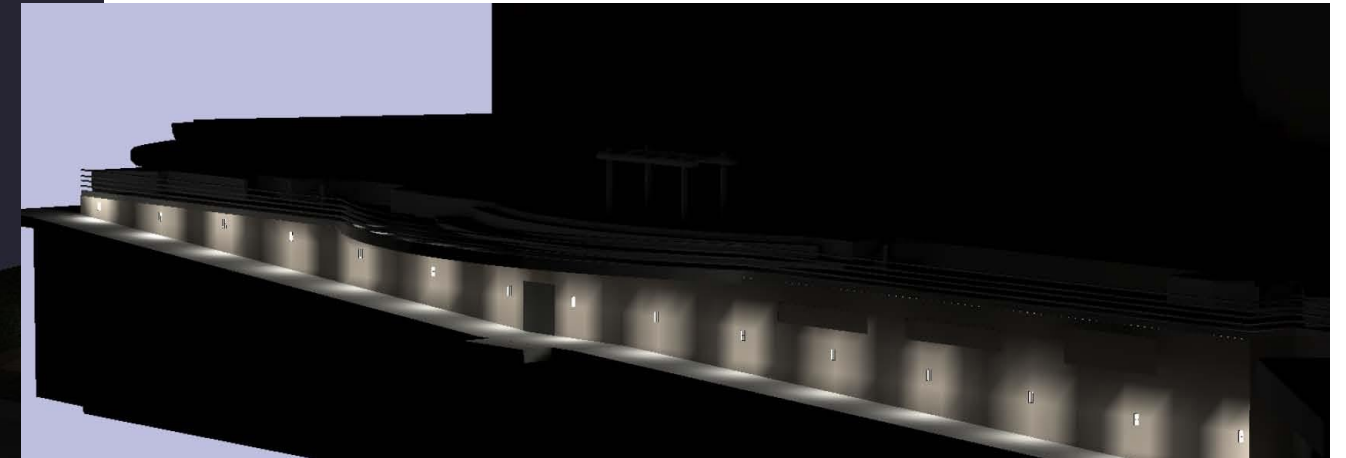




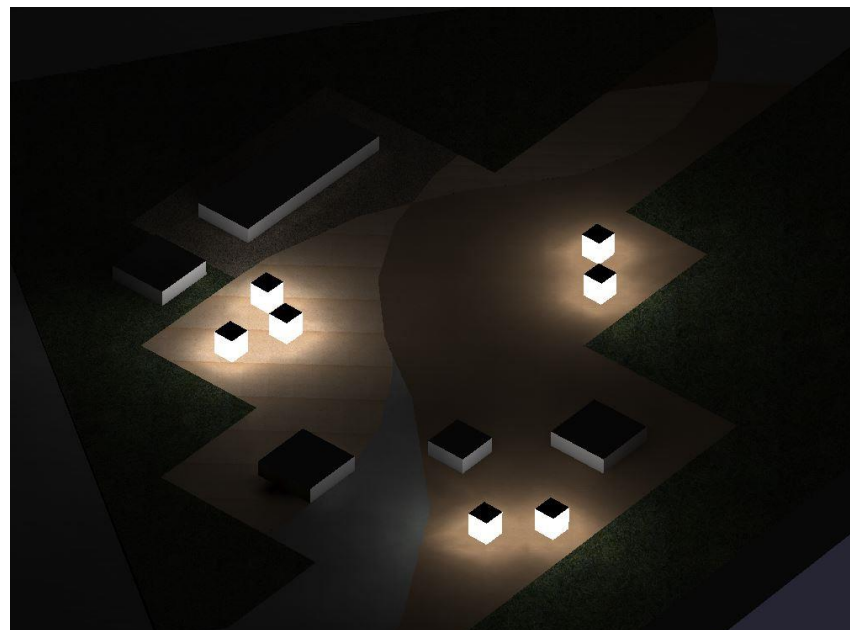


Birds Eye of Overall Plaza Accent Lighting Looking South, Southeast

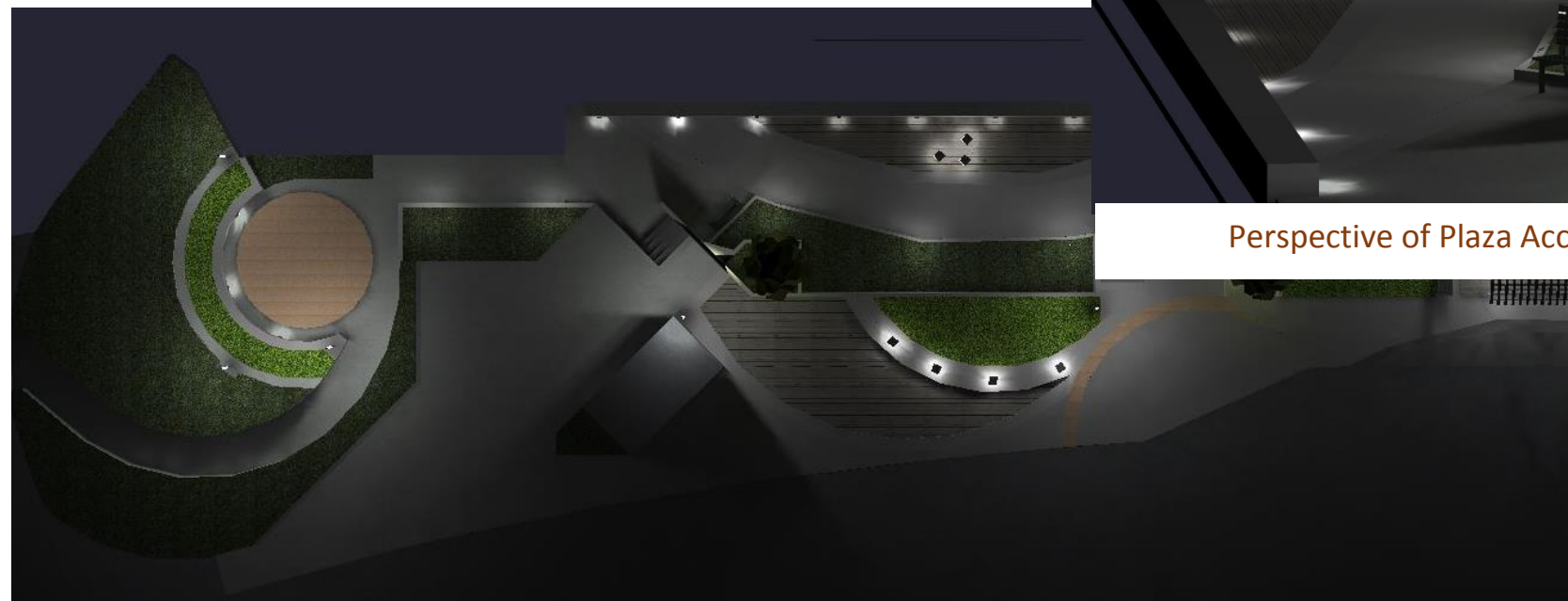
Perspective of Trail Lighting Looking Southeast



Perspective of Plaza Accent Lighting Looking East



Birds Eye of West Library Plaza Cube Seat Lights



Plan View of West Plaza Accent Lighting

**Project:** CSO 26 Plaza

**Regarding:** DRB Staff Comments

**To:** Omar Akkari

**Date:** March 9, 2017

1. Is the decorative railing design referenced from the Spokane Tribal Gathering Place considered a standard for parks along the river? If it is a parks standard, the railing should match more closely to the original design as shown in exhibit #16. If not, the reference of the wave element is a nice connection between the two parks.

**Response:** The railing is not considered a park's standard. Or at least not to BWA's knowledge. The intent was to echo the design but not make it an exact match as we are aware that the railing at the Gathering Place was very expensive and robust, for the purpose of stopping a vehicle. The design intent was to introduce the wave component as a nod to the nearby railing, tying the two spaces together.

2. The light box benches would be attractive targets for vandalism because of their white / semi-transparent surfaces. Under lit benches would be less attractive for tagging and still provide a similar aesthetic. (see attached image #2).

**Response:** We feel that this plaza offers the opportunity to do some unique lighting design in the city. The design team feels that the use of light cube benches within the design utilizes that opportunity. However, the concern over graffiti is valid. The design team is willing to consider alternative options to the cube seats if the city feels this is truly necessary. Flat concrete cube benches could also be targets for vandalism/graffiti. Any flat surface really. Bega lighting products are considered very durable and are appropriate for public application.

3. Would it be better to use abstracted geometric forms rather than trying to mimic the basalt rock if natural basalt rocks cannot be used? The concern being that the sculpted concrete will look fake and detract from the aesthetic quality of the plaza. If you have found a good sculptor this may not be an issue.

**Response:** Project specifications include provisions for a rock sculptor to prove experience and match a level of quality as shown in the images provided in the submittal package. The design team is confident that high quality basalt rocks can be replicated from shotcrete. Additionally, natural basalt is being mixed in planter beds and at the base of walls in an effort to tie the two materials together. Ultimately however, Garco is responsible for selecting the sculptor.

4. Will the 2" relief basalt form liner concrete be natural gray or a darker gray closer to basalt's natural color?

**Response:** At this time, the form liner wall is planned to be natural gray. The design team is considering a basalt colored stain but the cost impacts have not been fully vetted. The faux basalt shotcrete however, is planned to be stained to match real basalt.

5. The bikeway access route does not appear to meet ADA standards. Please provide applicable signage at the entrances to this area that states it is not an accessible route.

**Response:** Signage is easy include, however the city is planning to complete the signage and striping plan for the project. It is not in the consultant scope. The design team will work with the city staff developing this plan and offer the signage suggestion for inclusion.

6. The service access hatches could impact the flow of the hardscape patterns. Have you considered how these hatch lids can be visually minimized? I have seen hatch lids that integrate the paver and concrete patterns to minimize their visibility. Is this or another approach an option for some of the lids? I believe the technical term for this type of cover is a recessed tray. (see attached image #6a+b)

**Response:** We have considered the visual impact of the hatches and raised the concern with the city. At this time the utilities department is set on having diamond plated hatch covers. We realize that design wise, this is not the most attractive. The design team did offer hatch covers as a possible art opportunity to the short listed artists last week. This is also something that could be considered as an art competition held by the city or art's commission in the future if it is note included in the artists proposal. A competition could be similar to those held for the utility box wrappings in downtown.

7. Are their opportunities to have interactive seating or other interactive features? I am wondering what might be a beloved attraction in this plaza that would be simple and elegant, without detracting from the views of the falls. Bench swings might be and option if tucked away on the west side of the lower deck or under the shade structure. (see attached image #7a+b)

**Response:** Omar, bench swings are a fun idea! The design team has considered interactive components to the design. They have not yet been included but some ideas floated are outdoor drums, chimes and/or sculptural talk tubes. These items were also introduced to the artists as potential artistic opportunities at last weeks meeting. If the artists do not choose to include one of these components as part of their proposal, the design team will look at integration of one or more of the items as part of the contracted work.

8. Please share any additional information that is gathered about the proposed artwork and their locations on the site in your presentation. The art piece(s) could be very important to the activation of the site depending on its content and location.

**Response:** Omar, following the DRB submittal the design team met with the artists (Fri. March 3<sup>rd</sup>) on site and reviewed a drawings designating potential art locations. Attached is the drawing showing potential art locations and opportunities. Consider this exhibit Attachment #9 to the submittal package.

9. If information is available, please share what is being proposed for the reconstructed poem plaza. Are their opportunities for unique wayfinding to lead people to the somewhat hidden poem plaza that could be integrated with the proposed artwork(s) and/or site design?



**Response:** The reconstruction of the poem plaza is outside of the scope of our project and will be recreated with funds and efforts from the Spokane Art's Commission. Karen Mobley is the contact for this work.

10. How might the story told in the poem plaza be reflect in other areas of the park? The poem plaza is in a fairly hidden location. It might be interesting to pull elements of that story in to other areas of the park and have wayfinding cues leading back to the poem plaza. Perhaps one of the artist could create "Coyote" looking out over the falls or ghosts of salmon. (see attached image #10a+b)

**Response:** See response for question/comment #9. Karen Mobley is the contact for this work.

11. It appears the legend on attachment #4 is missing the "Light Type II" symbol. What is the difference between Light Type I and II?

**Response:** Following the submittal to the DRB the materials legend was revised on the landscape sheets to only include one symbol for lights. The legend then references the electrical plans which designates all of the different light types within the project. The electrical plans can be issued upon request.

12. Is there a habitat restoration plan available? If so, please submit it.

**Response:** Yes. Please See attached. Consider the restoration plan Attachment #10 to the DRB submittal set.

153 South Jefferson Street  
 Spokane, WA 99201  
 509 838.4511 | fax 509 838.4605  
 www.bernardowills.com

## Vegetation Restoration Plan Narrative

<b>To:</b> Dan Buller	<b>Date:</b> 8/4/16
<b>From:</b> Dell Hatch	<b>Project:</b> Spokane Falls CSO 26 Control Facility
<b>Subject:</b> Restoration Plan	<b>Project No:</b> 16-210

### Introduction

Installation of the Spokane Falls CSO 26 control facility and plaza improvements will impact and disturb approximate 47,455 sq. ft. of the hillside below (northeast) of Spokane Falls Boulevard, north of the downtown Spokane Library. Approximately 18,756 sq. ft. of this area is within the shoreline jurisdiction. Bernado|Wills Architects proposes the vegetation restoration plan, attached, to mitigate disturbance of vegetation and erosion on the hillside within the shoreline jurisdiction.

### Existing Conditions

The project site has steep slopes, approximately 1:1 and is largely comprised of fill material which was introduced during the construction of the Monroe Street Bridge. Basalt rock outcrops are visible in several locations. The majority of plant material on site is voluntary and not intentionally planted. A general inventory was taken for the site's existing vegetation, however due to the site's difficult terrain and steep slopes it was not feasible to do a comprehensive count of all plant material in order to meet the replacement ratios outlined in the Environmental Standards section 17E.060 of the Spokane Municipal Code. Evergreen trees over 6" caliper and deciduous trees over 6" were counted. Evergreen and deciduous trees under 6" caliper, shrubs, and groundcovers numbers are assumed estimates based on visual site inventory. It is important to note that a majority of the significant trees on site are not located within the shoreline jurisdiction. Because of this, they are neither shown on the attached plan or counted for replacement. Existing site vegetation includes, but is not limited to the following:

- Evergreen Trees – Ponderosa Pine (*Pinus ponderosa*), Scotch Pine (*Pinus sylvestris*)
- Deciduous Trees – Amur Maple (*Acer ginala*), Sumac (*Rhus Glabra*), Norway Maple (*Acer platanoides*), Mountain Ash (*Sorbus aucuparia*), Common Elderberry (*Sambucus nigra* sp. *Canadensis*), Black Cottonwood (*Populus trichocarpa*), Apple Trees (variety unknown)
- Shrubs and Grasses – Serviceberry (*Amelanchier alnifolia*), Oceanspray (*Holodiscus discolor*), Ninebark (*Physocarpus opulifolius*), Common Snowberry (*Symphoricarpos albus*), Woods Rose (*Rosa woodsii*), Rabbitsbrush (*Ericameria nauseosa*), Oregon Grape (*Mahonia aquifolium*), Idaho fescue (*Festuca idahoensis*)

### Vegetation Restoration Approach

The proposed CSO tank and pedestrian plaza improvements will cover approximately 7,091 sq. ft. of the 18,756 sq. ft. of the disturbed area within the shoreline jurisdiction, leaving approximately 11,665 sq. ft. for installation of restoration plantings. Vegetation used for restoration will mirror many of the desirable native varieties existing on site, with the addition of some additional regional natives. See the attached vegetation restoration plan for the full plant replacement schedule.

As previously mentioned, because of the site constraints and expanse of existing vegetation, only the evergreen trees over 6" caliper and deciduous trees over 6" were counted. Evergreen and deciduous trees under 6" caliper, shrubs, and groundcovers are assumed numbers based on visual site inventory. Because of the harsh site conditions, plant viability would likely be higher with smaller specimens. It is suggested that restoration tree and shrub plantings are installed at a 40 cubic inch plug or 4" pot sizes. Because it is suggested that vegetation is installed at smaller sizes than required by the municipal code we propose the ratio be adjusted to compensate for the variability in size. Where deeper soil conditions and more gentle slopes provide better planting conditions, larger specimens may be substituted. The below table explains the proposed ratio of removed plantings to new plug/pot sizes.

Vegetation Type	Required Replacement Ratio	Proposed Ratio
Native Deciduous Trees Less Than 6" Caliper	1:1 Replacement Ratio; Replacement tree(s) must be a minimum 2.5" caliper	Install 4 new native deciduous tree plugs for every one deciduous tree less than 6" caliper removed.  *Note: Where soil conditions allow, 1" caliper trees may replace 4 tree plugs.
Native Deciduous Trees Over 6" Caliper	2:1 Replacement Ratio; Replacement Tree(s) must be a minim 2.5" caliper	Install 8 new native deciduous tree plugs for every one deciduous tree over 6" caliper removed.  *Note: Where soil conditions allow, 1" caliper trees may replace 4 tree plugs.
Native Evergreen Trees Less Than 6" Caliper	1:1 Replacement Ratio; Replacement tree(s) must be a minimum 4" caliper	Install 4 new native evergreen tree plugs for every one evergreen tree less than 6" caliper removed.  *Note: Where soil conditions allow, 1" caliper trees may replace 4 tree plugs.

Native Evergreen Trees Over 6" Caliper	2:1 Replacement Ratio; Replacement tree(s) must be a minimum 4" caliper	Install 8 new native evergreen tree plugs for every one evergreen tree over 6" caliper removed.  *Note: Where soil conditions allow, 1" caliper trees may replace 4 tree plugs.
Native Shrubs	1:1 Replacement Ratio; Replacement shrub(s) must be at a minimum 12"-18 in diameter	Install 1 new native shrub every 50 sq. ft. of disturbed area within the shoreline jurisdiction.  *Note: Where soil conditions allow, 1 gallon plants may replace plugs and 4" pots.
Native Groundcover	1:1 Replacement Ratio; Replacement groundcover(s) must be at a minimum 4" in diameter	Seeding Full Extent of Disturbed Area



### **Irrigation Approach**

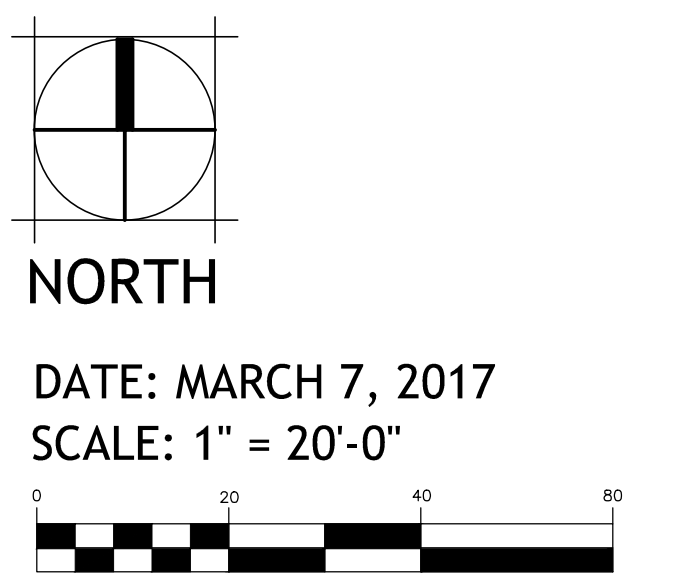
Temporary irrigation for a period of two years is suggested in order to establish restoration plantings. Irrigation should consist of Hunter MP Rotators or similar product at the top of the slope or at the interface between improvements and disturbed area. Irrigation shall be programed to a short, cycle-soak setting, in order to avoid rills and erosion due to over watering of the steep slopes. Site monitoring should be required as part of the project's erosion control plan.





POTENTIAL INTERACTIVE OPPORTUNITIES

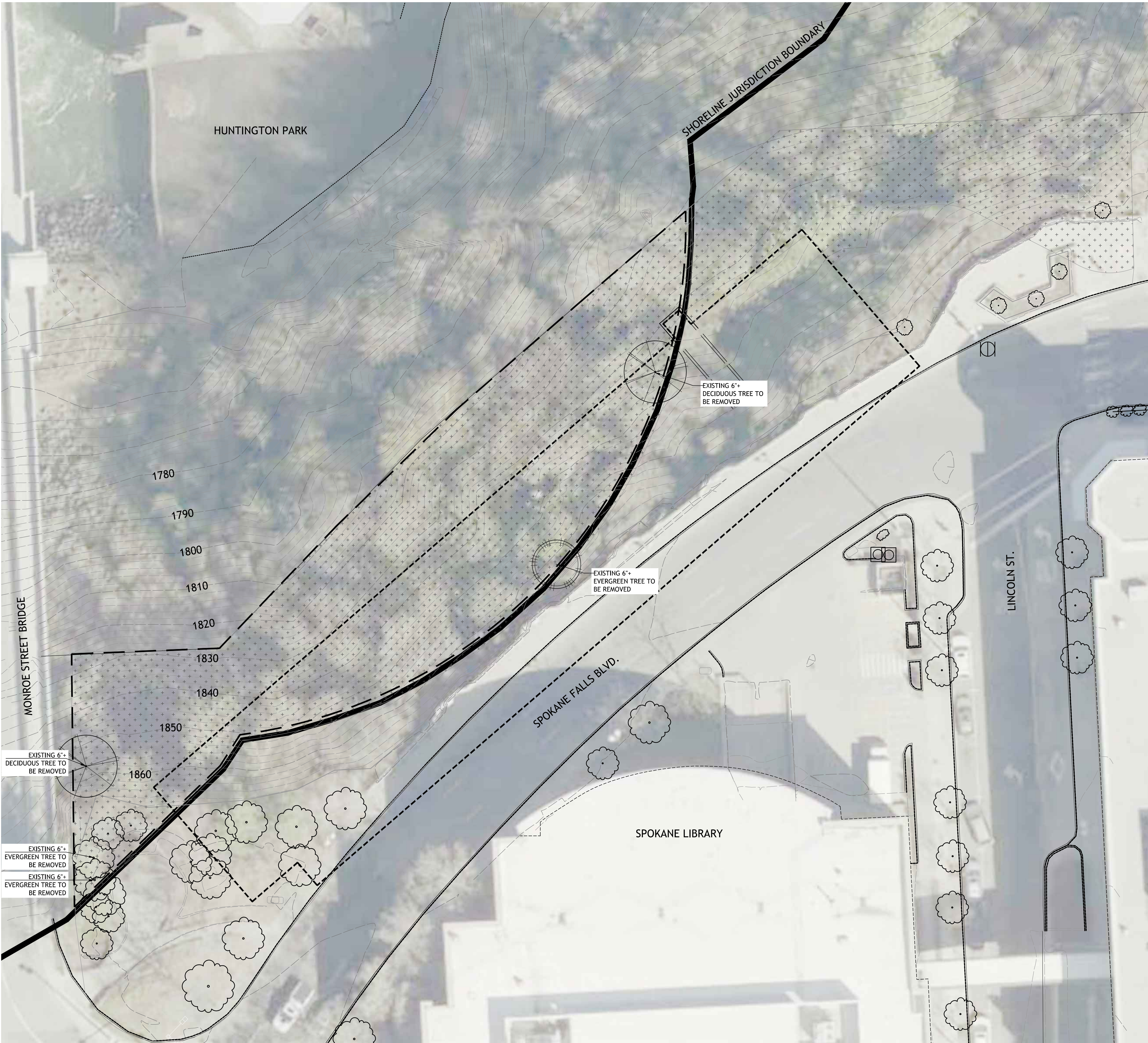
-  SCULPTURAL/INTERACTIVE TALK TUBES
-  MUSICAL DRUMS OR CHIMES



# POTENTIAL ART LOCATION EXHIBIT SPOKANE FALLS CSO 26 CONTROL FACILITY



8/5/2016 3:44 PM P:\2016\16-210 CSO Tank 26 Plaza\dwgs\16-8-4 CSO 26 Restoration Plan.dwg



NORTH

1

VEGETATION RESTORATION PLAN

SCALE: 1" = 20'-0"

0204060

										NAVDS8 = (OLD CBM ELEV) - (13.13)										AS OF JANUARY, 2000 USE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVDS8)									
										BENCH MARK LOCATION: BEING A 3" BRASS CAP SET IN CONCRETE INSIDE OF A WSDOT MONUMENT CASE, MARKED WSDOT, DO NOT DISTURB, 1990, GP32090-1-00. SURVEY MONUMENT, LOCATED APPROXIMATELY AT 2ND AVENUE AND FISKE STREET, WITH A PUBLISHED ELEVATION OF 1926.46 FEET (NAVDS8).										CURRENT C.O.S. DRAFTING STANDARDS ADOPTED FEB.2007									
										NAVDS8 ELEV: 1926.46										BY: JC DATES: 8/5/16									
										CBM NO. CBM #										DRAWN: JC REVISED: 8/5/16									
										BAR IS ONE INCH ON ORIGINAL DRAWING										DESIGN: APPROVED:									
										HORIZONTAL PLANS/PROFILE VERTICAL PROFILE ONLY										SCALE									
										IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY																			
										SPOKANE										CITY OF SPOKANE, WASHINGTON									
										DEPARTMENT OF ENGINEERING SERVICES										808 WEST SPOKANE FALLS BLVD. SPOKANE, WASHINGTON 99201-3343 (509) 625-6700									
										PROJECT NAME: SPOKANE FALLS CSO 26 CONTROL FACILITY										SEGMENT LIMITS:									
										PROJECT LIMITS:										TYPE OF IMPROVEMENT:									
										CITY PROJECT NUMBER: 2010088										CITY PLAN NUMBER: SPOKA B(1)2 18-25-43									
										EFN:																			

CALL BEFORE YOU DIG 1-800-424-5555

## RESTORATION PLAN

SHORELINE VEGETATION REPLACEMENT RATIOS				
VEGETATION REMOVED	REQUIRED REPLACEMENT RATIO	SITE VEGETATION TO BE REMOVED/DISTURBED	PROPOSED RATIO	QTY.
NATIVE DECIDUOUS TREES LESS THAN 6" CALIPER	1:1 REPLACEMENT RATIO; REPLACEMENT TREE(S) MUST BE A MINIMUM 2.5" CALIPER	NUMBER OF NATIVE DECIDUOUS TREES LESS THAN 6" CALIPER WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA IS ASSUMED AS (50) TREES	INSTALL 4 NEW NATIVE DECIDUOUS TREE PLUGS FOR EVERY ONE DECIDUOUS TREE LESS THAN 6" CALIPER REMOVED - (50) REMOVED = 200 NEW DECIDUOUS TREE PLUGS (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES) *NOTE: WHERE SOIL CONDITIONS ALLOW, 1" CALIPER TREES MAY REPLACE 4 TREE PLUGS.	200 PLUGS
NATIVE DECIDUOUS TREES OVER 6" CALIPER	2:1 REPLACEMENT RATIO; REPLACEMENT TREE(S) MUST BE A MINIMUM 2.5" CALIPER	(2) NATIVE DECIDUOUS TREES OVER 6" WERE INVENTORIED WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA	INSTALL 8 NEW NATIVE DECIDUOUS TREE PLUGS FOR EVERY ONE DECIDUOUS TREE OVER 6" CALIPER REMOVED - (2) REMOVED = 16 NEW DECIDUOUS TREE PLUGS (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES) *NOTE: WHERE SOIL CONDITIONS ALLOW, 1" CALIPER TREES MAY REPLACE 4 TREE PLUGS.	16 PLUGS
NATIVE EVERGREEN TREES LESS THAN 6" CALIPER	1:1 REPLACEMENT RATIO; REPLACEMENT TREES MUST BE A MINIMUM 4" CALIPER	NUMBER OF NATIVE EVERGREEN TREES LESS THAN 6" CALIPER WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA IS ASSUMED AS (0)	INSTALL 4 NEW NATIVE EVERGREEN TREE PLUGS FOR EVERY ONE EVERGREEN TREE LESS THAN 6" CALIPER REMOVED - (0) REMOVED = 0 NEW DECIDUOUS TREE PLUGS (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES) *NOTE: WHERE SOIL CONDITIONS ALLOW, 1" CALIPER TREES MAY REPLACE 4 TREE PLUGS.	0 PLUGS
NATIVE EVERGREEN TREES OVER 6" CALIPER	2:1 REPLACEMENT RATIO; REPLACEMENT TREES MUST BE A MINIMUM OF 4" CALIPER	(3) NATIVE EVERGREEN TREES OVER 6" WERE INVENTORIED WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA	INSTALL 8 NEW NATIVE EVERGREEN TREE PLUGS FOR EVERY ONE EVERGREEN TREE OVER 6" CALIPER REMOVED - (3) REMOVED = 24 NEW EVERGREEN TREE PLUGS (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES) *NOTE: WHERE SOIL CONDITIONS ALLOW, 1" CALIPER TREES MAY REPLACE 4 TREE PLUGS.	24 PLUGS
NATIVE SHRUBS	1:1 REPLACEMENT RATIO; REPLACEMENT SHRUB(S) MUST E AT A MINIMUM 12"-18" IN DIAMETER (AT HEAD)	NUMBER OF NATIVE SHRUBS WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA IS ASSUMED AS 1 SHRUB EVERY 50 SQ. FT.	INSTALL 1 NEW NATIVE SHRUB EVERY 50 SQ. FT. OF DISTURBED AREA WITHIN THE SHORELINE JURISDICTION - DISTURBED AREA IS 18,756 SQ. FT. = 375 NEW SHRUBS (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES) *NOTE: WHERE SOIL CONDITIONS ALLOW, 1 GALLON PLANTS MAY REPLACE PLUGS AND 4" POTS.	375 PLUGS OR 4" POTS
NATIVE GROUNDCOVER	1:1 REPLACEMENT RATIO; REPLACEMENT GROUNDCOVER(S) MUST BE AT A MINIMUM 4" IN DIAMETER (AT POT)	NATIVE GROUNDCOVER WITHIN THE SHORELINE JURISDICTION DISTURBANCE AREA IS PRIMARILY BUNCHGRASS	ENTIRE DISTURBED AREA WITHIN THE SHORELINE JURISDICTION WILL BE SEEDED WITH AN EROSION CONTROL MIX. (SEE PLANT SCHEDULE FOR TYPES AND QUANTITIES).	

## PLANT REPLACEMENT SCHEDULE

QTY.	DECIDUOUS TREES: TOTAL TREES 216		
125	AG	ACER GINNALA	AMUR MAPLE
91	RG	RHUS GLABRA	SUMAC
			40 CI PLUG OR 1" CAL.
			40 CI PLUG OR 1" CAL.
QTY.	EVERGREEN TREES: TOTAL TREES 24		
24	PP	PINUS PONDEROSA	PONDEROSA PINE
			40 CI PLUG
QTY.	SHRUBS: TOTAL SHRUBS 375		
60	AA	AMELANCHIER ALNIFOLIA	SASKATOON SERVICEBERRY
30	CR	CALAMAGROSTIS RUBESCENS	PINEGRASS
20	CS	CORNUS SERICEUS	REDOSEIR DOGWOOD
50	HD	HOLODISCUS DISCOLOR	OCEAN SPRAY
30	MR	MAHONIA REPENS	CREEPING MAHONIA
50	PL	PHILADELPHUS LEWISII	MOCK ORANGE
60	PM	PHYSOCARPUS OPULIFOLIUS	MALLOW NINEBARK
40	RW	ROSA WOODSII	WOODS ROSE
35	SA	SYMPHORICARPOS ALBUS	COMMON SNOWBERRY
			40 CI PLUG
QTY.	GROUNDCOVER		
	SEED MIX - EROSION CONTROL MIX - PLANTS OF THE WILD		

\*EXTENTS OF SEEDING APPROXIMATELY 26,341 SQ. FT. AT A RATE OF 2 POUNDS PER 1000 SQ. FT.

## LEGEND

	EXTENT OF DISTURBANCE WITHIN THE SHORELINE JURISDICTION (APPROXIMATELY 18,756 SQ. FT.)
	DESIGNATED VEGETATION RESTORATION AREA AND EXTENTS OF SEEDING
	SHORELINE JURISDICTION BOUNDARY
	LOCATION OF PROPOSED TANK
	EXISTING TOPOGRAPHY



L1.0

1 OF 1

PROJECT NAME: SPOKANE FALLS CSO 26 CONTROL FACILITY

SEGMENT LIMITS:		TYPE OF IMPROVEMENT:	
		CITY PROJECT NUMBER:	CITY PLAN NUMBER:
		2010088	SPOKA B(1)2 18-25-43
PROJECT LIMITS:		EFN:	



# Southgate KXLY

## Recommendation Meeting

### Staff Report

March 13, 2017



Julie Neff, Associate Planner  
Urban Design, Planning & Development

Tami Palmquist, Associate Planner  
Current Planning, Development Services Center

808 W. Spokane Falls Blvd.  
Spokane, WA 99201

#### **Applicant**

c/o Mike Stanicar  
Bernardo-Wills Architects  
153 South Jefferson  
Spokane, WA 99201

## Meeting Goals

At the March 22, 2017 Design Review Board (DRB) Recommendation Meeting, the DRB should:

- Determine how applicable criteria including the KXLY Development Agreement, 2009; the Integrated Site Plan, May 31, 2013; Initial Design Standards for Centers and Corridors, August 11, 2002; and the City of Spokane Comprehensive Plan affect or pertain to the proposed design; and
- Identify opportunities for design modifications as appropriate to maintain consistency with applicable criteria and respond to public comment.

## Background

The Design Review Board Program Review/Collaborative Workshop was held on January 25, 2017 and the Design Review Board Recommendation Meeting is scheduled for March 22, 2017.

The following materials are supplemental to this report:

- *Design Review Board Program Review/Collaborative Workshop Guidance*, January 25, 2017;
- *Design Review Staff Report (Program Review/Collaborative Workshop)*, January 12, 2016(17);

Additional information is on file and may be viewed at City Hall. Recommendations of the Design Review Board will be forwarded to the Planning Director.

## Regulatory Analysis

The site is zoned CC2 (Type 2 CC Zone) District Center, but is required to meet the CC1 (Type I CC Zone) Design Standards, per Section 7.4 of the Development Agreements.

Code requirements applicable to the project include the following (this list is not comprehensive – applicants should contact Current Planning Staff with any questions about code requirements).

**Section 17C.200.050 Street Tree Requirements.** Street trees are required along all city streets in commercial, center and corridor, industrial zones and along arterials in residential zones. Street trees shall be planted between the curb and the walking path of the sidewalk. Trees shall be spaced so the branches will touch after ten years of growth.

Staff note: This requirement will apply along Regal and will be verified at the time of Building Permit.

### **Staff assessment of consistency with CC Design Standards (2002 Initial Design Standards and Guidelines).**

1. Buildings Along the Street – More than 15% of the frontage has buildings up to the street. Windows and doors face the street.



2. Sidewalk Encroachments – None proposed at this time.
3. Lighting – Using the same pallet as identified in the ISP.
4. Screening and Noise Control of Service Areas – Addressed in the latest submittal on Page 25.
5. Ancillary Site Elements – Using the same pallet as identified in the ISP.
6. Curb Cut Limitations – The proposed driveways widths exceed the standard. It's anticipated that the applicants will request a design deviation from the City Traffic Engineer and will be required to provide justification for review. This includes fire access and "Auto Turn" layouts of the largest truck projected to use the approaches for turning radii and maneuvering onto the site.
7. Pedestrian Connections in Parking Lots - Addressed in the latest submittal on Page 6.
8. Drive-Through Lanes – Drive-through is internal to the site.
9. Transition between Commercial and Residential Development – Addressed in the latest submittal on Page 25.
10. Treatment of Blank Walls – Addressed in the latest submittal on Page 25.
11. Prominent Entrances – Addressed with renderings.
12. Façade Transparency – The applicant will be proving these calculations.
13. Massing - Addressed with renderings.
14. Roof Form - Addressed with renderings.
15. Historic Context Considerations – Using the same pallet as identified in the ISP.
16. Screening of Rooftop Equipment – Appears to have been addressed, but will be verified at time of Building Permit.

#### CC1 Design Standards

17. Pedestrian Oriented Signs – Specific signage will be reviewed subject to the CC1 Design Standards.
18. Integration with Architecture – Appears to be composed of similar materials and styles.
19. Creative Graphic Design – Specific signage will be reviewed subject to the CC1 Design Standards.
20. Unique Landmark Signs – None proposed, all signage will be reviewed subject to the CC1 Design Standards.
21. Ground Signs – The proposed monument and pylon signs exceed the standard. The applicants may decide to apply for a 'Bonus Allowance for Outstanding Design' ([Section 17C.240.290](#)) and follow the procedures for a type II land use application. This process will include department and agency review and public notice with a 14 day public comment period. At the conclusion of the comment period the decision will be issued by the Planning Director.

Staff note: With the exception of 6. 'Curb Cut Limitations' and 21. 'Ground Signs,' the standards appear to have been met or will be verified at the time of permitting.

## Topics for Discussion

Staff would suggest the following points for discussion.

1. Changes to the design since the Collaborative Workshop/Program Review, including any changes made in response to recommendations offered by the Design Review Board on January 25, 2017.
2. Any suggested design modifications to the site or building facades that may be necessary to address the ISP, Development Agreement, Centers & Corridors Design Guidelines or Comprehensive Plan.
3. Pedestrian Connections. Strategies to enhance the pedestrian environment including a mid-block crossing per the DRB's Jan. 25 recommendations.
  - a. Regal. Mid-block crossing and the pedestrian environment along the property and the Sports Complex.
  - b. Coordination with Parks Department. How will grade transitions to the park property be handled? What will be the impacts to the park property including trees?
  - c. South. Proposed connection to the residential area to the south.
  - d. Internal Site Circulation. Continuous, rather than meandering, walkways immediately adjacent to the parking lots along the center east-west walkway would allow people to access walkways without crossing planting areas.

Please see the following criteria:

- Centers and Corridors Zone [Section 17C.122.010](#) Intent encourages alternative modes of transportation with a safe pedestrian environment and quality design.
- City of Spokane Comprehensive Plan Land Use Policy 3.2 Centers and Corridors. *The circulation system is designed so pedestrian access between residential areas and the district center is provided. Frequent transit service, walkways, and bicycle paths link district centers and the downtown area.*
- Initial Design Standards and Guidelines for Centers and Corridors. "Pedestrian Connections in Parking Lots" page 10.
- Integrated Site Plan, May 31, 2013. Pages 7 & 9.
- 2009 Development Agreement 5.1 Pedestrian Connections: The Property shall contain dedicated pedestrian and bicycle connections ("path") which are designed to allow pedestrians and bicycles to access and move around and through the Integrated Properties with connection to the surrounding neighborhood. When feasible, such path shall connect to existing publicly accessible trails, sidewalks or other pathways that are adjacent and contiguous to the Integrated Properties.

4. Tree Preservation.

The development agreements recognize ponderosa pine as an important site characteristic. There are 13 existing ponderosas on the site, and the ISP requires saving 2. In response to the DRB's Jan. 25 recommendation, the applicants are proposing to plant 8 new ponderosa instead of the proposal to save 2 that are in poor condition.

- a. Because ponderosa tends to be difficult to establish, staff would suggest an establishment and replacement plan for new ponderosa plantings to help meet the requirements of the development agreement.
- b. To improve the potential for success, specify ponderosa that are container grown or containerized stock, meaning that the B&B trees are placed in a container to root-out over 6 to 12 months before sale. In addition, seed must be sourced from the Spokane area or high altitude eastern Oregon (Ponderosa installations from a Willamette Valley seed source will fail here). This information was shared by Angel Spell, Urban Forester.

Please see the following criteria:

- City of Spokane Comprehensive Plan Natural Environment Policy 15.5 Nature Themes – *Identify and use nature themes in large scale public and private landscape projects that reflect the natural character of the Spokane region.*
- Integrated Site Plan, May 31, 2013. Pages 7 & 11.
- 2009 Development Agreement 5.2 Tree Preservation: *Any plan for development of the Property shall provide for the preservation of trees, by leaving in place, a minimum of 10% of all Ponderosa Pine trees.*

5. Buildings.

6. Plazas. Programming or anticipated uses. How has the STA plaza been integrated per the DRB's Jan. 25 recommendations?

7. Site Details. Colors, patterns, lighting, plant palette.

For items 5-7, please see the following criteria:

- Initial Design Standards and Guidelines for Centers and Corridors.
- Integrated Site Plan, May 31, 2013. Part C Design Theme and Kit of Parts, Pages 12-16.
- 2009 Development Agreement 5.3 Design Theme: The Integrated Properties shall be developed with a consistent design theme utilizing, for example, similar or complementary construction materials, architectural characteristics, streetscapes, open spaces, fixtures, and landscaping. All buildings shall provide architectural treatment of interest on those facades visible from the street, such as color, texture, glazing, material differentiation or any other mechanism designed to lessen the impact of building mass when viewed from the street.

8. Screening. Protection of adjacent residences to the south from parking and service area impacts (applicant's submittal pgs. 25 and 31). Does the screening adequately address the DRB's Jan. 25 recommendation?

*Please see the following criteria:*

- Initial Design Standards and Guidelines for Centers and Corridors.  
Screening and Noise Control of Service Areas, page 7.  
Treatment of Blank Walls, page 13.

9. Low Impact Development (LID) Measures.

During the Jan. 25 Collaborative Workshop, the DRB requested innovative, alternative LID measures. The Development Agreement states *"Owners shall endeavor to implement Low Impact Development (LID) measures in any development of the property."*

The applicants indicated that they reviewed the [Eastern Washington Low Impact Development Guidance Manual](#). This manual describes a variety of alternatives for low impact development to reduce peak flows and notes that where infiltration-based practices are not possible, other alternatives may include reducing the amount of impervious surface, utilizing native vegetation and soils, permeable paving with an under-drain, rainwater collection systems, raised flow through planters to collect roof runoff, or bio-retention swales with an under-drain.

10. Long Term Development to an Urban District. Strategies for a successful transition to an Urban District.

*Please see the following criteria:*

- City of Spokane Comprehensive Plan, District Center: *As with a neighborhood center, buildings are oriented to the street and parking lots are located behind or on the side of buildings whenever possible.*
- Centers and Corridors Zone [Section 17C.122.010](#) Intent encourages a cohesive development pattern with screened parking areas behind buildings.
- "Narrative," Integrated Site Plan Pg. 22 (this list is not complete).
  - Development must encourage buildings built to the streets and should support future infill development as market demands.
  - Locate proposed driveways to align across Regal and Palouse to allow driveways to transition to future urban style intersections; locate appropriately to provide urban scale blocks.
  - Plan street improvements to allow future on-street parking while protecting urban-scale sidewalks, streetscape and storefronts.
- 2009 Development Agreement 5.6 Long-Term Development of Urban District: "The intent of the parties is to design and develop urban features that will facilitate integration of the Property (and surrounding area) into an urban district with a unified character that promotes pedestrian and vehicular circulation, without conflict, encourages opportunities for mixed use development and enhances the natural and built aesthetics in the area. In order to enhance connectivity and facilitate future urban development, driveways through the property shall be designed wherever possible, to facilitate connections to the properties identified in Recital D, above."

## Note

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of the Planning Dept.

## Policy Basis

Spokane Municipal Codes  
City of Spokane Comprehensive Plan

## DESIGN REVIEW BOARD

## Southgate KXLY Development

## 1 - Program Review/Collaborative Workshop

January 25, 2017



**Design Review Board**  
 Austin Dickey, DRB Chair  
 c/o Julie Neff, DRB Secretary  
 Planning & Development  
 808 W. Spokane Falls Blvd.  
 Spokane, WA 99201

**Based on review of the materials submitted by the applicant and discussion during the January 25, 2017 Collaborative Workshop, the Design Review Board recommends the following:**

**GENERAL**

- The board has no concerns with the proposed alternative zoning strategies.

**NEIGHBORHOOD**

- Applicant to provide additional detail on what is proposed for the pedestrian paths between the playfields and the development site.
- Applicant to pursue a mid-block, enhanced pedestrian crossing that strives to align with the proposed grid across Regal south of the Palouse Highway intersection in order for the proposal to evolve into an urban district per Section 5.6 of the Development Agreement.
- Work with STA to determine how to best incorporate the future High Performance Transit stop into the development.

**SITE**

- The applicant shall look into opportunities to provide either additional site trees and/or provide select trees of greater size in lieu of saving the two existing ponderosa pines which are in poor health and may be difficult to save.
- The applicant will provide a landscape separation at the north end of the "ribbon park" between the curb and sidewalk at Regal.
- The applicant is requested to provide innovative, alternative LID solutions.
- The applicant to provide additional detail on hardscape throughout the project on planned pedestrian connectivity within the parking lot and along Regal, include strategies that enhance the pedestrian environment.

**BUILDING**

- We request the applicant, before the next meeting, define whether Pad "E" will be in phase I or II.
- Applicant to provide better screening of Pad "A" along the south façade.

Austin Dickey, Chair, Design Review Board

Note: Supplementary information, audio tape and meeting summary are on file with City of Spokane Design Review Board.

# Southgate KXLY

Program Review/Collaborative Workshop

Staff Report

January 12, 2016



Julie Neff, Associate Planner  
Urban Design, Planning & Development

Tami Palmquist, Associate Planner  
Current Planning, Development Services Center

808 W. Spokane Falls Blvd.  
Spokane, WA 99201

## **Applicant**

c/o Mike Stanicar  
Bernardo-Wills Architects  
153 South Jefferson  
Spokane, WA 99201

## Meeting Goals

The Design Review Board will hold at least two meetings to review this proposal. The first meeting, the Program Review/Collaborative Workshop will be held on January 25, 2017 and must take place prior to submittal of a building permit application. It is an opportunity for project applicants to share preliminary information about the proposal early in the design process before any major decisions have been made. Citizens are invited to offer comments to help guide design decisions that will respect and build on the positive aspects of the neighborhood. During the meeting the Board may identify opportunities for design modifications as appropriate to respond to public comment and maintain consistency with criteria including the KXLY Development Agreement, 2009; the Integrated Site Plan, May 31, 2013; and the Initial Design Standards for Centers and Corridors, August 11, 2002.

The second meeting with the DRB, the Recommendation Meeting, may be scheduled after the proposed design has been refined to respond to criteria identified during the Collaborative Workshop as well as other applicable permitting requirements. Applicants are encouraged to proceed to the Recommendation Meeting prior to submitting the building permit application. The Recommendation Meeting is another opportunity for citizen comment. At the close of the Recommendation Meeting, the DRB will prepare a recommendation to the Planning Director.

## Authority

### **Chapter 04.13 Design Review Board**

A. Purpose. The design review board is hereby established to:

1. improve communication and participation among developers, neighbors and the City early in the design and siting of new development subject to design review under the Spokane Municipal Code;
2. ensure that projects subject to design review under the Spokane Municipal Code are consistent with adopted design guidelines and help implement the City's Comprehensive Plan.
3. advocate for the aesthetic quality of Spokane's public realm;
4. encourage design and site planning that responds to context, enhances pedestrian characteristics, considers sustainable design practices, and helps make Spokane a desirable place to live, work and visit.
5. provide flexibility in the application of development standards as allowed through development standard departures; and
6. ensure that public facilities and projects within the City's right of way:
  - a. wisely allocate the City's resources,
  - b. serve as models of design quality

This proposal is subject to design review under SMC **Section 17G.040.010 Design Review Board Authority**, F. A development proposal or planning study about which the plan commission, planning director, or hearing examiner requests to have the board's advice pertaining to any design elements.

Recommendations of the Design Review Board will be forwarded to the Planning Director.

## Background

In 2009 the City Council approved Development Agreements for three sites, KXLY, Joers (Richey), and Black. The agreements were prepared in response to the developers' request for a Comprehensive Plan amendment to change the land use map from single family and multifamily residential to CC Core (District Center).

- Section 4 "Stormwater Pilot Project" of the Development Agreements, states that "owners shall endeavor to implement Low Impact Development (LID) measures in any development of the property."
- Section 5 "Integrated Site Plan" of the Development Agreements requires an Integrated Site Plan for the three sites prior to submittal of any building permit application. Section 5 of the Development Agreements is copied at the end of this report.
- Section 6 "Review of Integrated Site Plan" requires the DRB to review the Integrated Site Plan and make a recommendation to the Planning Director as to whether the proposal is consistent with the criteria identified in Section 5.

*The DRB reviewed the proposed Integrated Site Plan on August 8, 2012 and January 9, 2013. The final Integrated Site Plan is dated May 31, 2013 and is included with the applicant's submittal package.*

- Section 7 "Building Permit Review" requires that all buildings proposed for development on the properties will be subject to City of Spokane Design Review. Text in the box below is copied directly from the KXLY Development Agreement.

*The DRB reviewed the proposed plans for the Black property on March 15, 2013 and April 24, 2013.*

**7. Building Permit Review:** All buildings proposed for development on the Property shall be subject to the City of Spokane Design Review process and shall comply with the following elements:

**7.1 Urban Design.** The building and improvements shall be consistent with the Integrated Site Plan.

**7.2 Building Treatment:** The allowed one large-format retail building on the Property shall orient the building with the narrow side toward the So. Palouse Highway or toward South Regal Street, whichever is closer to the proposed large format retail building and shall promote intervening retail between the large format store and the street. In addition, the siting and design of the large format retail building shall employ site planning strategies that enhance and enliven public spaces on the Property or in the vicinity. The preferred, but not required, configuration of any retail building is multi-story. All buildings shall provide architectural treatment of interest on those façades visible from the street, such as color, texture, glazing, material differentiation or any other mechanism designed to lessen the impact of building mass when viewed from the street.

**7.3 Square Footage Limitation:** Any plan for development of the Property shall include no more than one large format store up to a maximum of 105,000 square feet. No other single commercial building on the Property shall exceed 55,000 square feet.

**7.4 Design Standards:** Any plan for development of the Property shall comply with the design standards (but not including any square footage limitations) applicable to properties in the City's CC1 zone in effect on the date of this Agreement, which standards are dated August 11, 2002, and are titled "Initial Design Standards and Guidelines for Centers and Corridors," which are attached hereto as **Exhibit "B"** and incorporated herein by this reference.

Additional information is on file and may be viewed at City Hall.

## Location & Context

The site is located within the Southgate Neighborhood. It is separated from the other two sites included as part of the Integrated Site Plan by Regal and Palouse Highway. Both of these roads are designated as minor arterials. To the south and east are multi-family residences. West of the KXLY site is an area of single family residential. The property to the north is owned by the City of Spokane Parks Department. Several blocks to the north is the City of Spokane's Hazel's Creek property designated for stormwater runoff mitigation.

STA's High Performance Transit Network includes a line on Regal. The information below was copied off the agency's HPT Network page:

-To view a map of the HPT Network Plan, as adopted by the STA Board of Directors, click [here](#).

-To view the characteristics of the various types of service that may be available in a High Performance Transit Network, click [here](#).

## Regulatory Analysis - Zoning

The site is zoned CC2 (Type 2 CC Zone) District Center, but is required to meet the CC1 (Type I CC Zone) Design Standards, per Section 7.4 of the Development Agreements.

Code requirements applicable to the project include the following (this list is not comprehensive – applicants should contact Current Planning Staff with any questions about code requirements):

### **Chapter 17C.122 Center and Corridor Zones.**

#### **Initial Design Standards and Guidelines for Centers and Corridors, August 11, 2002.**

*The document titled "Initial Design Standards and Guidelines for Centers and Corridors" is adopted by reference as a part of the initial land use code for centers and corridors and incorporated as Attachment "A" to the initial land use code for centers and corridors. All projects must address these standards and guidelines. The applicant assumes the burden of proof to demonstrate how a proposed design addresses these standards and guidelines.*

Staff Note: The current revision of the design standards is dated Summer 2015. However, the 2002 version will apply to this proposal per the Development Agreements. In addition to standards required for all Centers and Corridors, the Design Standards pertaining to CC1 zones apply; see pages 26-30.

### **Section 17C.122.010 Intent**

*The intent of center and corridor regulations is to implement the goals and policies of the comprehensive plan for centers and corridors. These areas are intended to bring employment, shopping, and residential activities into shared locations and encourage, through new development and rehabilitation, new areas for economic activity.*

*New development and redevelopment is encouraged in these areas that promotes a relatively cohesive development pattern with a mix of uses, higher density housing, buildings oriented to the street, screened parking areas behind buildings, alternative modes of transportation with a safe pedestrian environment, quality design, smaller blocks and relatively narrow streets with on-street parking.*

### **Section 17C.122.020 Types of Centers/Corridors**

- A. *Type 1 (CC1): Pedestrian Emphasis/Auto Accommodating.*  
*The Type 1 center and corridor zone promotes the greatest pedestrian orientation of the center and corridor zones. To accomplish this, some limitations are placed on auto-oriented activities and some types and the allowable size of some uses are controlled.*
- B. *Type 2 (CC2): Pedestrian Enhanced/Auto Accommodating.*  
*The Type 2 center and corridor zone promotes new development and redevelopment that is pedestrian oriented while accommodating the automobile. Incentives allowing a higher floor area ratio in exchange for the provision of greater public amenities as land is developed and redeveloped are encouraged in these areas.*

**Section 17C.200.030 Landscape Types.** Description of L-1: Visual Screen, L-2: See-Through Buffer and L-3: Open Area landscaping.



#### **Section 17C.200.040 Site Planting Standards.**

**E. Parking Lot Landscape Design.** The visual impact of parking lots shall be reduced through landscape design. The design of the parking lot shall reinforce pedestrian circulation.

#### **F. Parking, Outdoor Sales, and Outdoor Display Areas.**

3. Provides options for meeting required trees in parking lots.

4. Where parking lots are located between the building and a street, the amount of required interior landscaped area or tree shade cover shall be increased.

5. Requires a planting strip with L1 visual screen landscaping or a 6'-0" site-obscuring fence where a single family residential zone would have views of parking or service areas.

Staff note: This condition occurs between the SE Sports Complex (Park property) and the parking lot. The applicants are requesting a visually permeable fence and sod in order to maintain open views. Staff recommends allowing the proposed visually permeable fencing (rather than 6' site obscuring fencing) in those locations. However, to address the SMC an L-2 "see through buffer," including a 3 to 4 foot evergreen screen will be required between parking lots and the sports complex to block headlights and screen automobiles.

**Section 17C.200.050 Street Tree Requirements.** Street trees are required along all city streets in commercial, center and corridor, industrial zones and along arterials in residential zones. Street trees shall be planted between the curb and the walking path of the sidewalk. Trees shall be spaced so the branches will touch after ten years of growth.

Staff note: This requirement will also apply to the east edge of the Sports Complex along Regal so the multi-modal pathway may need to be adjusted.

**Section 17C.200.110 Water Conservation Measures** encourages methods for conserving water.

**Section 17C.230.200 Bicycle Parking.** Bicycle parking is required for any new use that requires 20 or more automobile spaces.

## City of Spokane Comprehensive Plan

<https://my.spokanecity.org/business/planning/>

Policies that may be especially relevant to this project are listed below.

- **City of Spokane Comprehensive Plan Land Use Policy 3.2 Centers and Corridors**  
*Designate centers and corridors (neighborhood scale, community or district scale, and regional scale) on the land use plan map that encourage a mix of uses and activities around which growth is focused.*

#### **Discussion:**

Suggested centers are designated where the potential for center development exists. Final determination is subject to the neighborhood planning process.

#### **District Center**

District centers are designated on the land use plan map. They are similar to neighborhood centers, but the density of housing is greater (up to 44 dwelling units per acre in the core area of the center) and the size and scale of schools, parks, and shopping facilities are larger because they serve a larger portion of the city. As a general rule, the size of the district center, including the higher density housing surrounding the center, should be approximately 30 to 50 square blocks.

As with a neighborhood center, buildings are oriented to the street and parking lots are located behind or on the side of buildings whenever possible. A central gathering place, such as a civic green, square, or park is provided. To identify the district center as a major activity area, it is important to encourage buildings in the core area of the district center to be taller. Buildings up to five stories are encouraged in this area

The circulation system is designed so pedestrian access between residential areas and the district center is provided. Frequent transit service, walkways, and bicycle paths link district centers and the downtown area.

- **City of Spokane Comprehensive Plan Transportation Policy 1.1 Transportation Priorities** - Make transportation decisions based on prioritizing the needs of people as follows:
  - *Design transportation systems that protect and serve the pedestrian first.*
  - *Next, consider the needs of those who use public transportation and non-motorized transportation modes;*
  - *Then consider the needs of automobile users after the two groups above.*
- **City of Spokane Comprehensive Plan Transportation Policy 2.1 Physical Features** - *Incorporate site design and other physical features into developments that encourage alternatives to driving.*
- **City of Spokane Comprehensive Plan Transportation Policy 2.7 Safe Sidewalks** – *Provide for safe pedestrian circulation within the city; wherever possible, this should be in the form of sidewalks with a pedestrian buffer strip or other separation from the street.*
- **City of Spokane Comprehensive Plan Transportation Policy 2.13 Viable Bicycling** - *Promote and provide for bicycling as a viable alternative to driving.*
- **City of Spokane Comprehensive Plan Urban Design & Historic Preservation Goal 3 Function and Appearance** – *Use design to improve how development relates to and functions within its surrounding environment.*
- **City of Spokane Comprehensive Plan Urban Design & Historic Preservation Policy 3.2 Access to Alternative Modes of Transportation** – *Ensure that commercial and public building sites provide direct and convenient access for pedestrians, bicyclists, or persons utilizing alternative modes of transportation.*
- **City of Spokane Comprehensive Plan Urban Design & Historic Preservation 6.3 Transit and Pedestrian-Oriented Development** – *Encourage attractive transit and pedestrian-oriented development.*
- **City of Spokane Comprehensive Plan Natural Environment Policy 6.1 Native and Non-Native Adaptive Plants and Trees** – *Encourage the use of and develop standards for using native and non-native adaptive plants and trees in landscape designs for public and private projects.*
- **City of Spokane Comprehensive Plan Natural Environment Policy 15.5 Nature Themes** – *Identify and use nature themes in large scale public and private landscape projects that reflect the natural character of the Spokane region.*
- **City of Spokane Comprehensive Plan Natural Environment Policy 13.2 Walkway and Bicycle Path Design** – *Design walkways and bicycle paths based on qualities that make them safe, functional, and separated from automobile traffic where possible.*

## Topics for Discussion

Staff would suggest the following points for discussion and when identifying important points for the applicants to consider during the design process. Discussion should be focused on relationships between the various existing and proposed elements, consistency with the Integrated Site Plan (ISP), and expectations for Centers and Corridors.

- 1) **Pedestrian Connections.** Please see pages 7 and 9 of the Integrated Site Plan and Page 26 of the KXLY plan.
  - a. **Regal.** The ISP shows a mid-block crossing and pathway connection feature on the NE corner of the parcel opposite the center drive entrance to the Target site. Whereas, the KXLY plan shows “Airport Parking” and a planted median in this area.
  - b. **Parklet Walkway on Regal.** Minimizing the necessity for bicyclists and pedestrians to weave by straightening out the pathway and maintaining a consistent width for street trees, would improve the function as a multi-use path and meet code requirements for a pedestrian buffer strip planted with street trees.

- c. **Connections to Park Property.** A pedestrian connection between the soccer field to the west and the plaza between buildings “F” and “G” would improve circulation and be consistent with adopted policies and circulation identified in the ISP.
- d. **South.** Designing bicycle and pedestrian circulation to maintain opportunities for connections to housing areas south of the parcel, should they be desired by the adjacent property owners now or in the future, would be consistent with adopted policies and circulation identified in the ISP.

*During discussion, please consider additional adopted criteria below:*

**Centers and Corridors Zone [Section 17C.122.010](#) Intent** encourages alternative modes of transportation with a safe pedestrian environment and quality design.

**City of Spokane Comprehensive Plan Land Use Policy 3.2 Centers and Corridors**

*The circulation system is designed so pedestrian access between residential areas and the district center is provided. Frequent transit service, walkways, and bicycle paths link district centers and the downtown area.*

**Initial Design Standards and Guidelines for Centers and Corridors.**

- **Pedestrian Connections in Parking Lots** page 10.

- 2) **Tree Preservation.** Please see pages 7 and 11 of the Integrated Site Plan and Page 27 of the KXLY plan. Per the Development Agreement, “Any plan for development of the Property shall provide for the preservation of trees, by leaving in place, a minimum of 10% of all Ponderosa Pine trees.” On the KXLY site this would require preservation of at least two trees.

- a. **Tree Selection and Protection.** In 2012, a tree inventory was performed during development of the Integrated Site Plan. The ISP (pg. 11) indicates that two trees along the north property line, selected from #1-6 would be saved. Instead, the KXLY submittal (pg. 7) proposes preserving trees # 8 and #11.

The development agreement recognizes ponderosa pine as an important site characteristic. Designing the site to showcase and protect healthy ponderosas (for example, selecting good specimens, providing adequate space, and protecting their root zones from likely pedestrian traffic between parking lots and walkways) would be consistent with the ISP.

TREE INVENTORY						
NO.	SPECIES	HEIGHT	DBH	CALIPER	CONDITION	COMMENTS
1	PONDEROSA PINE	73'-0"	32"		GOOD	PERIMETER TREE / BUFFER
2	PONDEROSA PINE	73'-0"	29"		GOOD	PERIMETER TREE / BUFFER
3	PONDEROSA PINE	70'-0"	30"		GOOD	PERIMETER TREE / BUFFER
4	PONDEROSA PINE	70'-0"	19"		GOOD	PERIMETER TREE / BUFFER
5	PONDEROSA PINE	70'-0"	19"		GOOD	PERIMETER TREE / BUFFER
6	PONDEROSA PINE	70'-0"	25" & 20"		GOOD	DOUBLE TRUNK / BUFFER
7	PONDEROSA PINE	44'-0"	16"		FAIR	
8	PONDEROSA PINE	22'-0"	11"		POOR	SUPPRESSED
9	PONDEROSA PINE	45' & 60'	15" & 22"		POOR	LEANING AND EXPOSED ROOTS
10	PONDEROSA PINE	35'-0"	11"		FAIR	POOR SHAPE AND CANOPY
11	PONDEROSA PINE	17'-0"	14"		POOR	NO LEADER
12	PONDEROSA PINE	52'-0"	19"		GOOD	
13	PONDEROSA PINE	47'-0"	23"		FAIR	EXPOSED ROOTS
14	BLACK LOCUST	15'-0"	17"		POOR	SCARRED TRUNK
15	BLACK LOCUST	15'-0"	10"		POOR	SCARRED TRUNK

*During discussion, please consider additional adopted criteria below:*

**City of Spokane Comprehensive Plan Natural Environment Policy 15.5 Nature Themes –**  
*Identify and use nature themes in large scale public and private landscape projects that reflect the natural character of the Spokane region.*

- 3) **Design Theme.** Please see pages 7 and 12-16 of the Integrated Site Plan and Pages 19 and 28-44 of the KXLY plan. It's anticipated that additional information on how the elements outlined in the Kit of Parts, including tree and plant selection, will be presented at the Recommendation Meeting. In addition, the applicants are encouraged to continue working with the Parks Department to successfully integrate the site with surrounding park properties.
- 4) **Viewscape.** Please see pages 8 and 18 of the Integrated Site Plan and Page 46 of the KXLY plan. Are there opportunities for the design to respond to, and frame, desirable views?
- 5) **Long Term Development to an Urban District.** Please see pages 8 and 22 of the Integrated Site Plan and Page 47 of the KXLY Plan.
  - a. **How does the proposal address the "Narrative" on page 22 of the ISP?** Including strategies for a successful transition to an Urban District.
  - b. **Proposed Simulated Streetscape, KXLY Plan, Page 23.** The applicants are requesting a recommendation in favor of this proposal.

*During discussion, please consider additional adopted criteria below:*

**City of Spokane Comprehensive Plan, District Center:** *As with a neighborhood center, buildings are oriented to the street and parking lots are located behind or on the side of buildings whenever possible.*

**Centers and Corridors Zone [Section 17C.122.010](#) Intent** encourages a cohesive development pattern with screened parking areas behind buildings.

**"Narrative," Integrated Site Plan Pg. 22 (this list is not complete).**

- Development must encourage buildings built to the streets and should support future infill development as market demands.
- Locate proposed driveways to align across Regal and Palouse to allow driveways to transition to future urban style intersections; locate appropriately to provide urban scale blocks.
- Plan street improvements to allow future on-street parking while protecting urban-scale sidewalks, streetscape and storefronts.

*Staff notes:*

- Urban blocks in downtown Spokane range from 250' to 350'.
- The urban grid shown on page 47 of the KXLY submittal is generally consistent with these dimensions.
- The distance between buildings at "Pad B" and "Pad C" may preclude evolution to an urban grid.
- Some lifestyle centers in other cities successfully simulate traditional streetscapes, including on-street parking, within the development.

6) **Low Impact Development (LID) Measures.** What LID measures will be included on the site? The Development Agreement states "Owners shall endeavor to implement Low Impact Development (LID) measures in any development of the property."

7) **Screening.** Please address how adjacent residences to the south will be protected from service area impacts.

*During discussion, please consider additional adopted criteria below:*

**Initial Design Standards and Guidelines for Centers and Corridors.**

- **Screening and Noise Control of Service Areas,** page 7.

## Note

The recommendation of the Design Review Board does not alleviate any requirements that may be imposed on this project by other City Departments including the Current Planning Section of the Planning Dept.

## Policy Basis

Spokane Municipal Codes  
City of Spokane Comprehensive Plan

# 2009 Development Agreement

**Section 5. Integrated Site Plan:** An integrated site plan, containing the elements in this subsection, shall be completed prior to the issuance of a building permit. The following elements shall apply to the Integrated Site Plan which shall include the Property and the properties included in Ordinance C3426 ("Richey") and Ordinances C34256 ("Black") and C34257 ("KXLY"), hereinafter the "Integrated Properties."

**5.1 Pedestrian Connections:** The Property shall contain dedicated pedestrian and bicycle connections ("path") which are designed to allow pedestrians and bicycles to access and move around and through the Integrated Properties with connection to the surrounding neighborhood. When feasible, such path shall connect to existing publicly accessible trails, sidewalks or other pathways that are adjacent and contiguous to the Integrated Properties.

**5.2 Tree Preservation:** Any plan for development of the Property shall provide for the preservation of trees, by leaving in place, a minimum of 10% of all Ponderosa Pine trees.

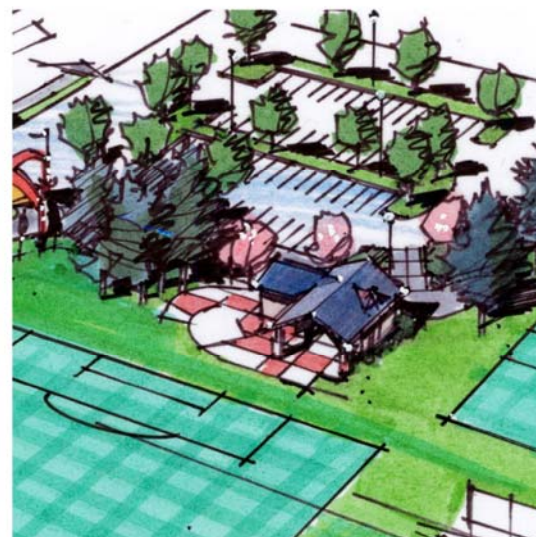
**5.3 Design Theme:** The Integrated Properties shall be developed with a consistent design theme utilizing, for example, similar or complementary construction materials, architectural characteristics, streetscapes, open spaces, fixtures, and landscaping. All buildings shall provide architectural treatment of interest on those facades visible from the street, such as color, texture, glazing, material differentiation or any other mechanism designed to lessen the impact of building mass when viewed from the street.

**5.4 Community Plaza:** A community plaza shall be designated that serves as a central gathering place on one of the Integrated Properties and, if not located on the Property, Owners shall provide satisfactory evidence of the Owners' contractual and financial commitment to participate in the development of the community plaza.

**5.5 Viewscape:** The Owners shall determine and map view corridors that allow persons on the property from common or public areas to view Mt. Spokane and Browne's Mountain. Owners shall consult with the City's Planning Service staff and designated representative of the Southgate Neighborhood Council in scoping and determining view corridors. The identified views corridors shall be protected by site and architectural design strategies, if necessary, such as, co-location of important view corridors with public spaces between buildings and with public gathering spaces(s). In the event of a conflict between this element and elements 5.2, 5.4, 5.6, 7.2 or 7.4, this provision shall yield to those elements(s).

**5.6 Long-Term Development of Urban District:** "The intent of the parties is to design and develop urban features that will facilitate integration of the Property (and surrounding area) into an urban district with a unified character that promotes pedestrian and vehicular circulation, without conflict, encourages opportunities for mixed use development and enhances the natural and built aesthetics in the area. In order to enhance connectivity and facilitate future urban development, driveways through the property shall be designed wherever possible, to facilitate connections to the properties identified in Recital D, above."





# Southgate KXLY Development

SWC Regal Street & Palouse Highway

Design Review Board – Recommendations  
March 01, 2017

## Narrative & Summary

*January 25, 2017, the applicant presented and discussed the project with the Design Review Board during the Collaborative Workshop. Following the workshop, the Board prepared a guidance memo outlining how elements of the design could better implement the guidelines and requirements found in the Development Agreement in addition to additional detail of internal pedestrian systems and circulation.*

*In response to the guidance memo, we have added detail, met with City agencies, and made revisions where suggested by the Board. Changes to the project since the Collaborative Workshop include:*

- *Minor changes to curb layout around the north side of Retail A to improve pedestrian circulation from the Retail A plaza*
  - *A result of the revised curb lines and pedestrian features is the reduction of supporting parking from 849 to 842; reducing the parking ration to 3.39 stalls per 1,000 SF.*
- *Changes to the curb alignment along the south property line to allow for increased vegetative screening, including use of Ponderosa Pines*
- *Changes to the Ribbon Park alignment and separation from Regal St.*
- *STA HPT integration*
- *Concept detail at plaza spaces*
- *Concept detail at pedestrian connections to the park*
- *Revisions to the south elevation to better break up the mass of the building in addition to enhanced screening of the loading area*

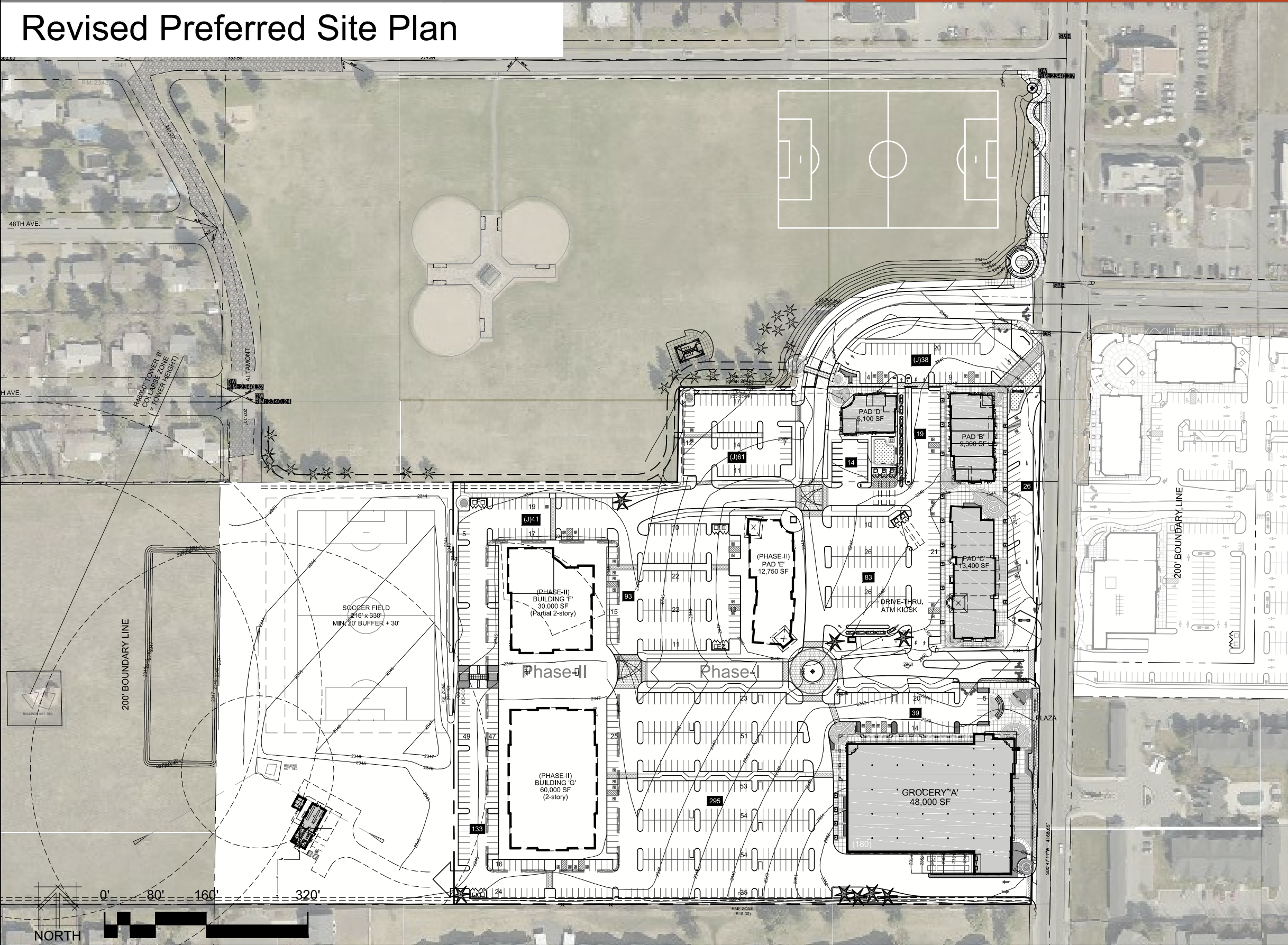
*Since the Collaborative Workshop, the Development team the has met with STA concerning integration of a Higher Performance Transit stop into the development, started an on-going dialog regarding a potential mid-block crossing and made evaluations of potential “innovative, alternative” LID opportunities.*

*In response to the DRB Guidance, that Retail-E is not a part of the application.*

*The following pages detail how the project addresses remaining guidance items given by the DRB.*



# Revised Preferred Site Plan



## A. SUBJECT PROPERTY(S) DATA

PARCEL ADDRESS	PARCEL NUMBER	ZONING & LAND USE	PARCEL AREA
5222 S. REGAL ST.	34041.9077	CC-2 DC (CC-1) CC CORE	609,840 SF
2651 E. 49th AVENUE	34041.0038	RSF OPEN SPACE	85,378 SF
ASSEMBLED PROPERTY AREA (ASSESSED):			695,218 SF
			15.96 ACRES

## B. Required BLDG Setbacks & Landscaping

YARD / SITE LOCATION	REQ'D MIN. BUILDING SETBACK	SETBACK ADJACENT TO R-ZONE	REQ'D MIN. LANDSCAPING
FRONT	0-FEET	10-FEET	5-FEET
SIDES	0-FEET	10-FEET	5-FEET
REAR	0-FEET	10-FEET	5-FEET
STREETS	12-FEET BACK OF CURB	12-FEET BACK OF CURB	6-FEET
USE BUFFER	-	-	6-FEET + FENCE
INTERIOR	-	-	10% PARKING AREA

## C. BUILDING(S) AREA & REQUIRED PARKING

BUILDING INFORMATION		REQUIRED PARKING	
BLDG NAME	BLDG AREA	MINIMUM (1/1000 SF)	MAXIMUM (1/250 SF)
GROCERY 'A'	48,000 SF	48.0 STALLS	192.0 STALLS
PAD 'B'	9,300 SF	9.3 STALLS	37.2 STALLS
PAD 'C'	13,400 SF	13.4 STALLS	53.6 STALLS
PAD 'D'	5,100 SF	5.1 STALLS	20.4 STALLS
PAD 'E'	12,750 SF	12.8 STALLS	51.0 STALLS
BUILDING 'F' (PARTIAL 2-STORY)	30,000 SF	30.0 STALLS	120.0 STALLS
BUILDING 'G' (2-STORY)	60,000 SF	60.0 STALLS	240.0 STALLS
TOTALS:	178,550 SF	179 Stalls	714 Stalls
PROVIDED PARKING:			842 Stalls
LESS 140 SOCCER COMPLEX PARKING STALLS:			702 Stalls
			3.93 STALLS / 1,000 SF
DEVELOPMENT PARKING RATIO:			
REQUIRED ADA PARKING (5%):			42 ADA Stalls
PARKING LOT TREES (1 INTERIOR TREE PER 6 STALLS PER 'SMC' 17C.200.040(F)a):			140 TREES

## D. PROJECT DATA

DATA	EXISTING	PROPOSED
OCCUPANCY GROUP	-	M, B, A2 & A3
CONSTRUCTION TYPE	-	TYPE-II & V-N
MAX. BLDG HEIGHT	-	55-FEET
MAX. FLOOR AREA RATIO (F.A.R.)	-	0.5
BLDG(S) SPRINKLED	-	YES
SEPA REQUIRED	-	YES
ESTIMATED PROJECT VALUE	-	\$0
ESTIMATED START DATE	-	TBD

## DRB Guidance

During the January 25, 2017 DRB Collaborative Workshop, the DRB set forth the following guidance for revisions and action to be taken by the Development:

### NEIGHBORHOOD

- Applicant to provide additional detail on what is proposed for the pedestrian paths between the playfields and the development site.
- Applicant to pursue a mid-block, enhanced pedestrian crossing that strives to align with the proposed grid across Regal south of the Palouse Highway intersection in order for the proposal to evolve into an urban district per Section 5.6 of the Development Agreement.
- Work with STA to determine how to best incorporate the future High Performance Transit stop into the development.

### SITE

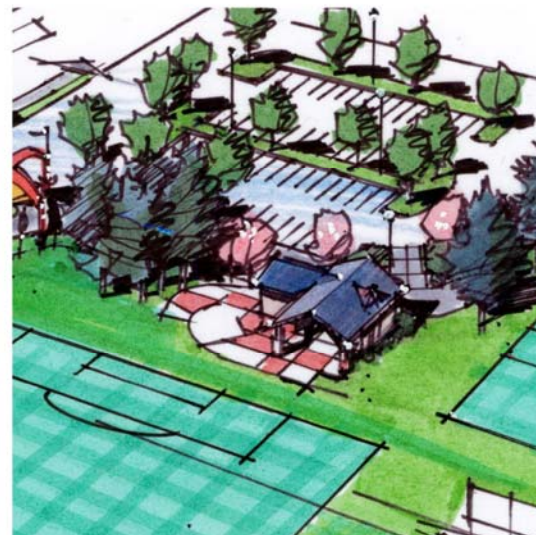
- The applicant shall look into opportunities to provide either additional site trees and/or provide select trees of greater size in lieu of saving the two existing ponderosa pines which are in poor health and may be difficult to save.
- The applicant will provide a landscape separation at the north end of the “ribbon park” between the curb and sidewalk at Regal.

- The applicant is requested to provide innovative, alternative LID solutions.
- The applicant to provide additional detail on hardscape throughout the project on planned pedestrian connectivity within the parking lot and along Regal. Include strategies that enhance the pedestrian environment.

### BUILDING

- We request the applicant, before the next meeting, define whether Pad “E” will be in phase I or II.
- Applicant to provide better screening of Pad “A” along the south façade.





## DRB GUIDANCE NEIGHBORHOOD

- *“Applicant to provide additional detail on what is proposed for the pedestrian paths between the playfields and the development site.”*
- *“Applicant to pursue a mid-block, enhanced pedestrian crossing that strives to align with the proposed grid across Regal south of the Palouse Highway intersection in order for the proposal to evolve into an urban district per Section 5.6 of the Development Agreement.”*
- *“Work with STA to determine how to best incorporate the future High Performance Transit stop into the development.”*



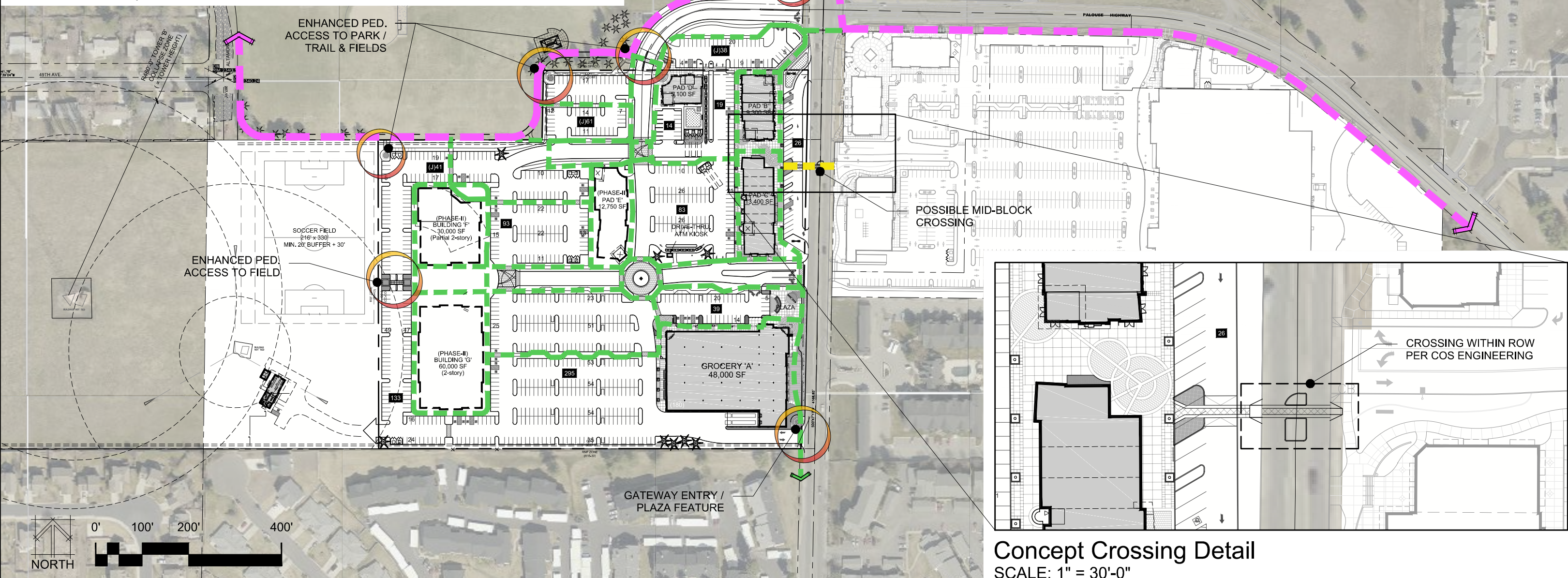
# Enhanced Compliance

## DA Criterion 5.1: Pedestrian Connections

"The Property shall contain dedicated pedestrian and bicycle connections ("path") which are designed to allow pedestrians and bicycles to access and move around and through the Integrated Properties with connection to the surrounding neighborhood. When feasible, such path shall connect to existing publicly accessible trails, sidewalks or other pathways that are adjacent and contiguous to the Integrated Properties."

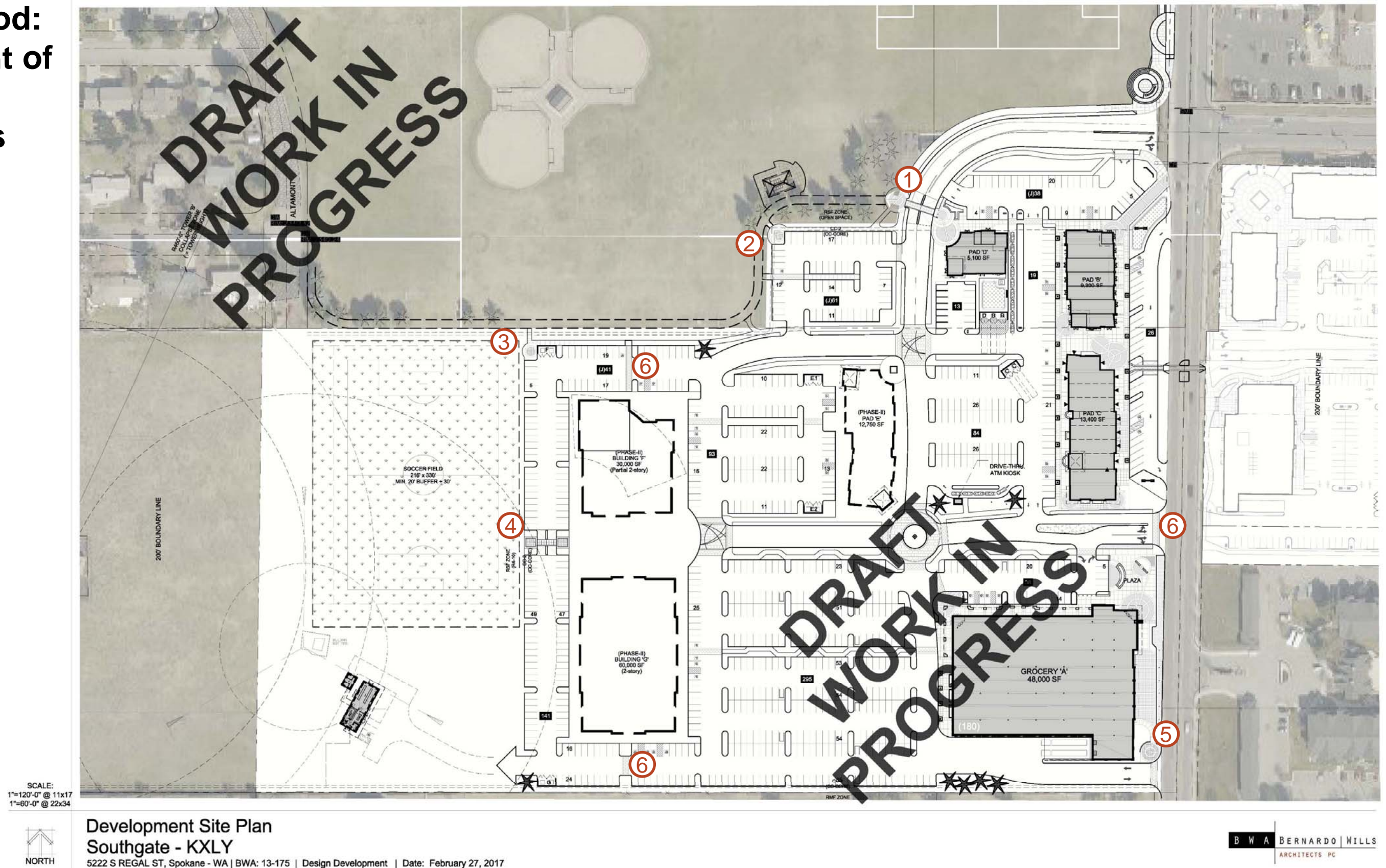
**DRB Guidance:**  
Applicant to provide additional detail on what is proposed for the pedestrian paths between the playfields and the development site.

- PEDESTRIAN CONNECTION TO PARK, BETWEEN BUILDINGS AND WITHIN PROJECT
- CONTINUATION OF MULTIMODAL PATHWAY
- POSSIBLE MID-BLOCK CROSSING
- INDICATES MAJOR PATHWAY CONNECTION OR FEATURE

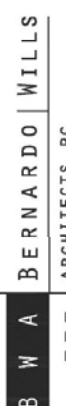
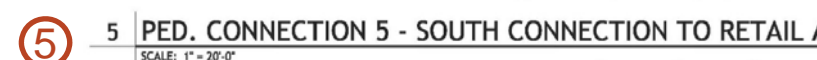




## Neighborhood: Development of Pedestrian Connections







**Southgate / KXLY**  
5222 S. Regal St.  
Spokane, Washington 99336

Project No: 13-175  
 Drawn by: JLC  
 Date: 2/23/17  
**EX 1**



# ① Neighborhood: Ped. Connection 1 Retail-D to Park



Pedestrian Access to Park: Looking NNE to Retail-D & Access Road



Pedestrian Access to Park: Looking ESE to Retail-D Plaza



Ped. Access to Park: Looking SE at Park Entry Feature



Ped. Access: Looking SW from Access Road



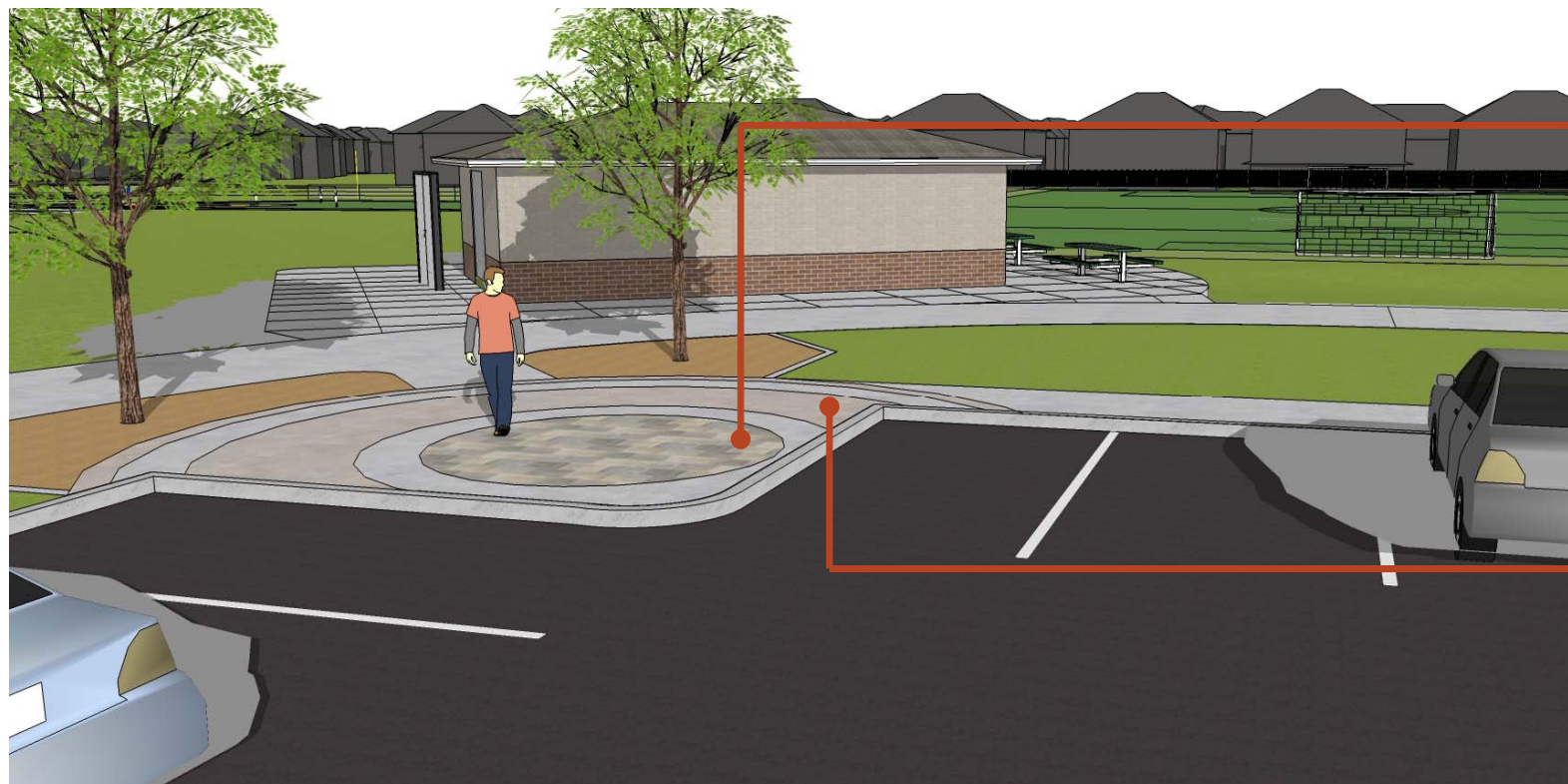
Ped. Access: Birds Eye Looking W



## ② Neighborhood: Pedestrian Connection 2 From North Central Parking to Park



### ② Pedestrian Access to Park: Looking East From Park/Trail



### ② Pedestrian Access to Park: Looking North to Park & Trail Access



## 4 Neighborhood: Pedestrian Connection 4 - Bldg-F/G Plaza to Park



Pedestrian Access to Park: Looking West From Plaza Between Bldg. F/G to New Soccer Field



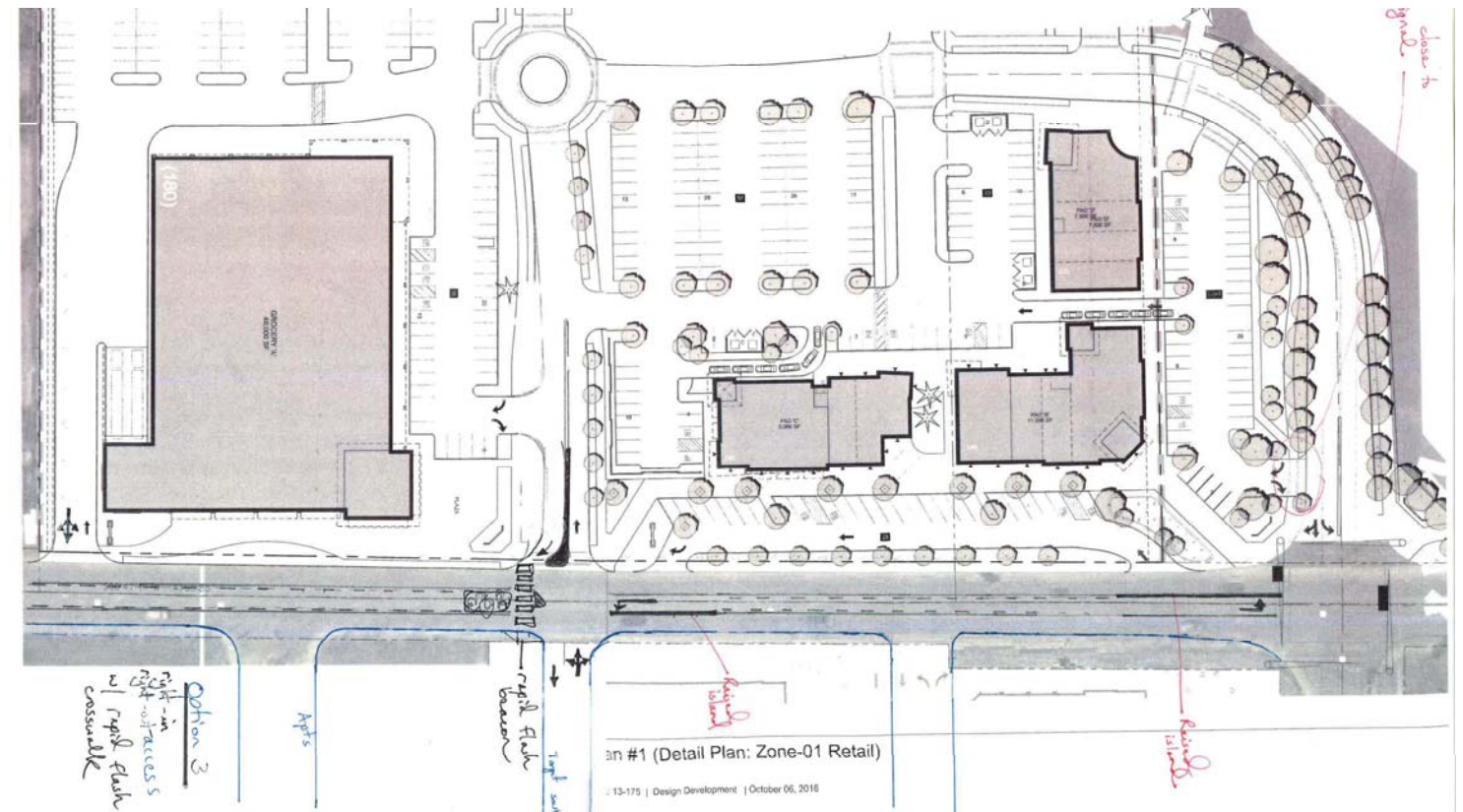
## Neighborhood: Mid-Block Crossing

DRB Guidance: *“Applicant to pursue a mid-block, enhanced pedestrian crossing that strives to align with the proposed grid across Regal south of the Palouse Highway intersection in order for the proposal to evolve into an urban district per Section 5.6 of the Development Agreement.”*

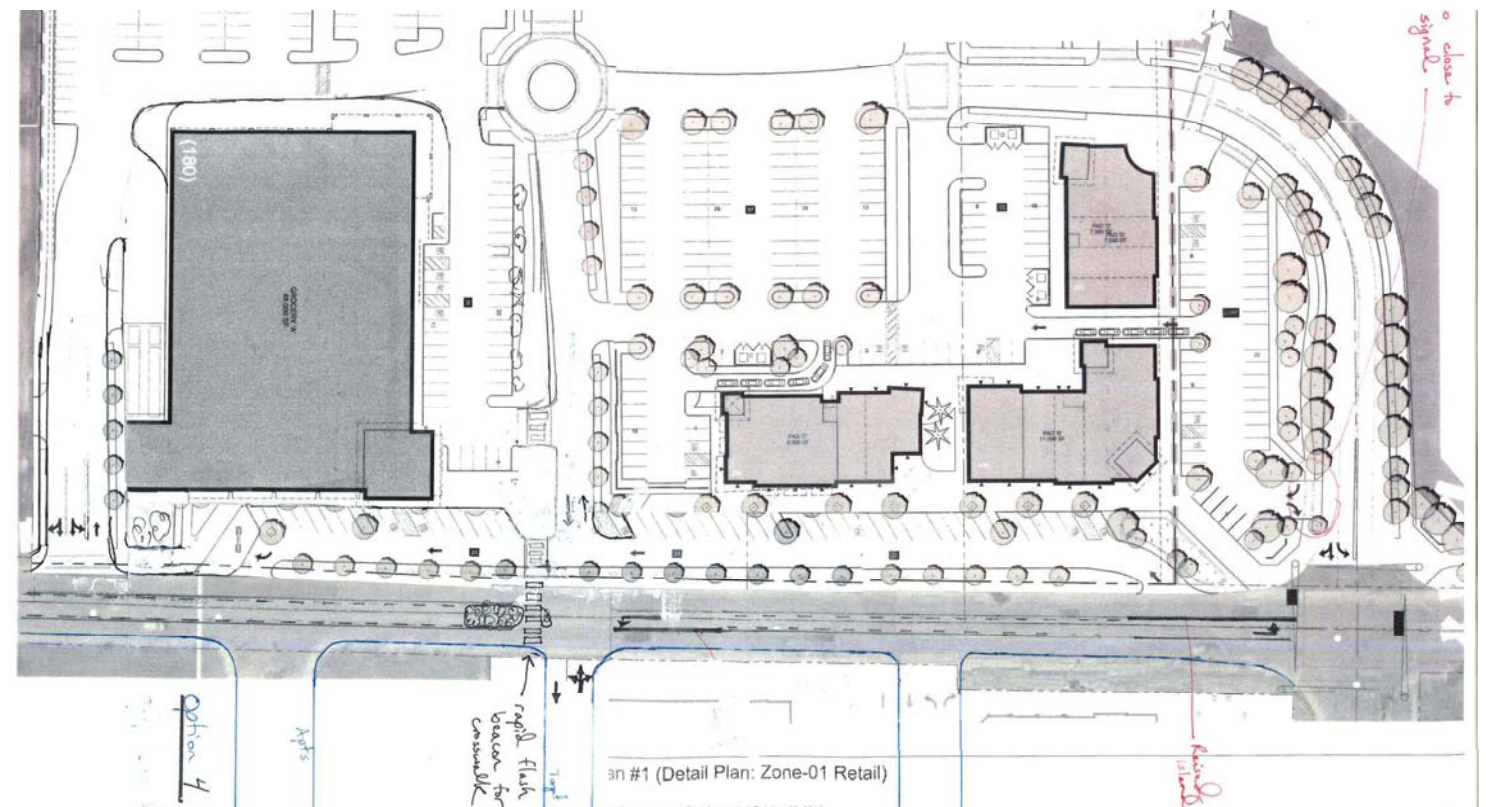
The development team engaged the City Planning and Engineering departments in discussions concerning a crossing warrant and location of a mid-block crossing if found to be warranted. The City of Spokane Engineering department was helpful in identifying a preferred location and configuration options from an engineering perspective - shown on this page as Options 3 & 4.

The DRB guidance charged the development to “pursue” a mid-block crossing. Discussions surrounding the feasibility and location of a mid-block crossing are proceeding ahead with City departments in good faith. For the purpose of the DRB’s role in this matter, the development team considers to have fulfilled this requirement.

A conceptual detail showing the potential Regal St. mid-block pedestrian crossing is shown on Page 06 .



City proposed mid-block crossing – Option 3



City proposed mid-block crossing – Option 4

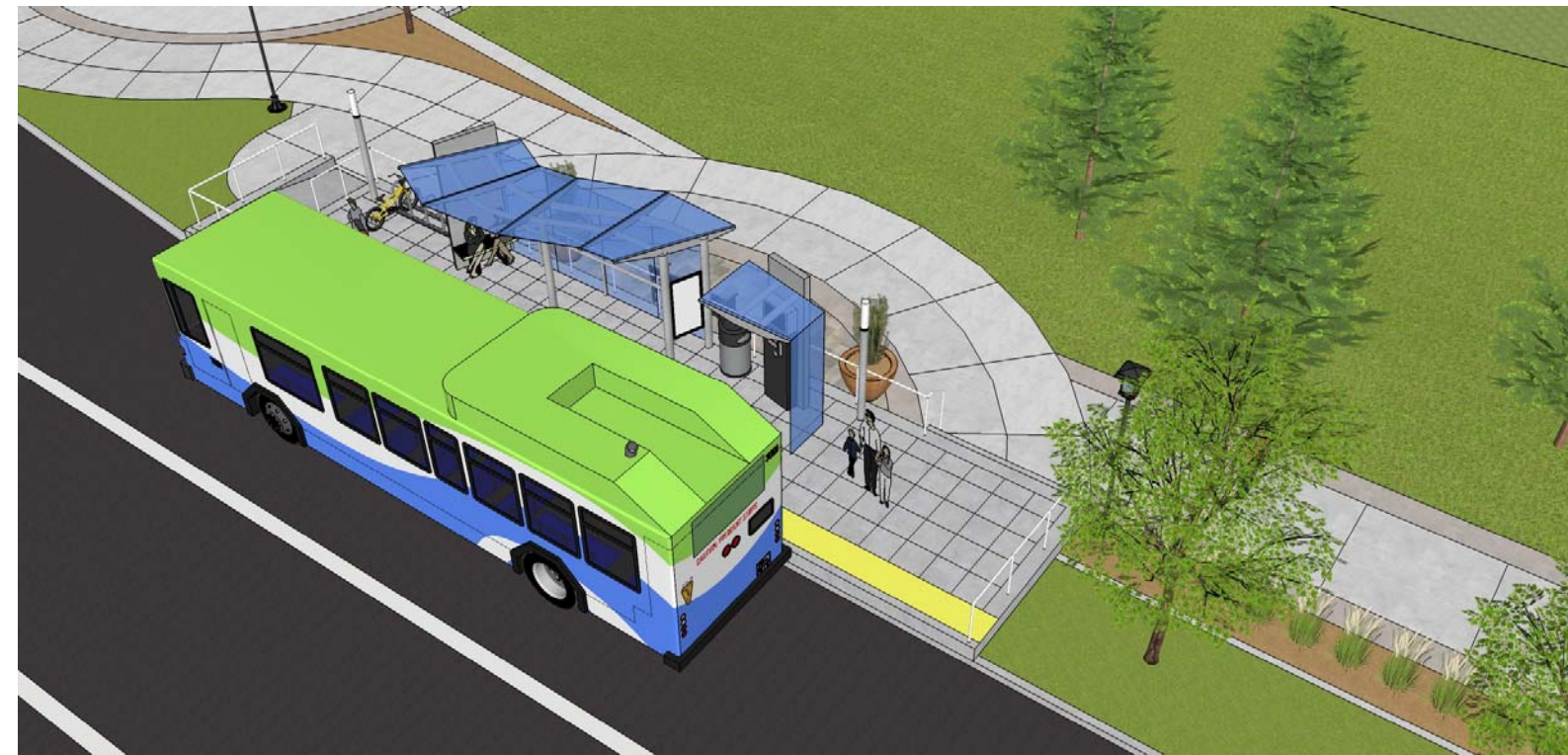


## Neighborhood: STA HPA

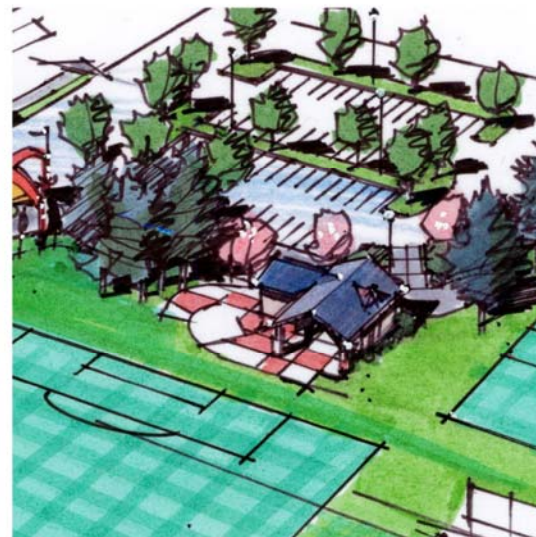
DRB Guidance: *“Work with STA to determine how to best incorporate the future High Performance Transit stop into the development.”*

The development team met with representatives from the Spokane Transit Authority to discuss integrating a future HPT stop into the project. Several locations were discussed. In the end it was decided that a location nearest the northwest corner of the Regal and Palouse intersection would centrally service both the shopping center and sports complex in addition to offering the most direct, convenient and safest access to the crossing of the controlled intersection.

STA requested a 12-foot x 60-foot long reserved area adjacent to the curb on Regal for their future improvements. It was also requested that no permanent improvements be placed within the reservation area. The adjacent concept renderings show how STA's future HPT can be integrated into the project.







## DRB GUIDANCE

### SITE

- *“The applicant shall look into opportunities to provide either additional site trees and/or provide select trees of greater size in lieu of saving the two existing ponderosa pines which are in poor health and may be difficult to save.”*
- *“The applicant will provide a landscape separation at the north end of the “ribbon park” between the curb and sidewalk at Regal.”*
- *“The applicant is requested to provide innovative, alternative LID solutions.”*
- *“The applicant to provide additional detail on hardscape throughout the project on planned pedestrian connectivity within the parking lot and along Regal, include strategies that enhance the pedestrian environment.”*



# Enhance Compliance DA Criterion 5.2: Tree Preservation

*"Any plan for development of the Property shall provide for the preservation of trees, by leaving in place, a minimum of 10% of all Ponderosa Pine trees."*

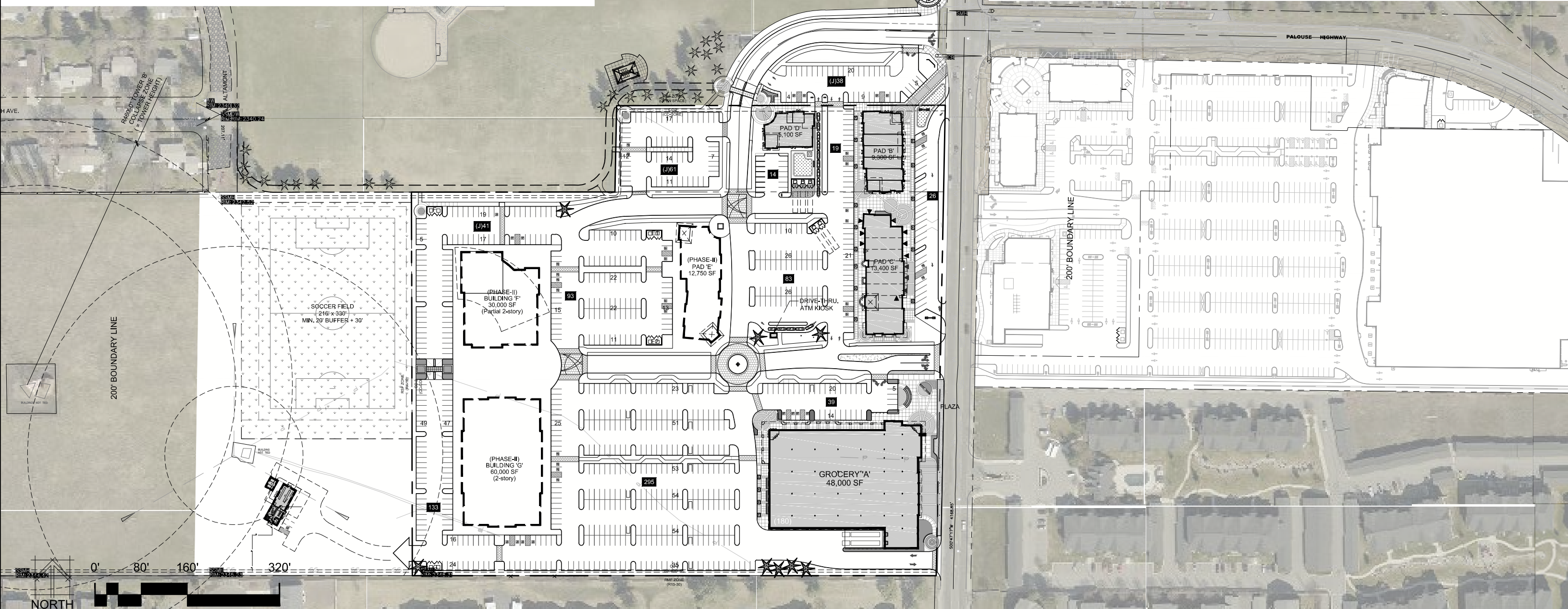
## DRB Guidance:

*"The applicant shall look into opportunities to provide either additional site trees and/or provide select trees of greater size in lieu of saving the two existing ponderosa pines which are in poor health and may be difficult to save."*

## Applicant Response:

As discussed during the Collaborative Workshop, the probability of a high mortality rate of existing Ponderosa Pines is exceedingly high. As alternative compliance the DA 5.2, and in keeping with the guidance of the DRB, new Ponderosa Pines are proposed at a rate of 4 to 1 above the DA mandate for preserved trees. A minimum of eight Ponderosa Pines will be planted on site. These pines are also be incorporated in the enhanced landscape buffer to provide year-round screening of the back of Retail-A.

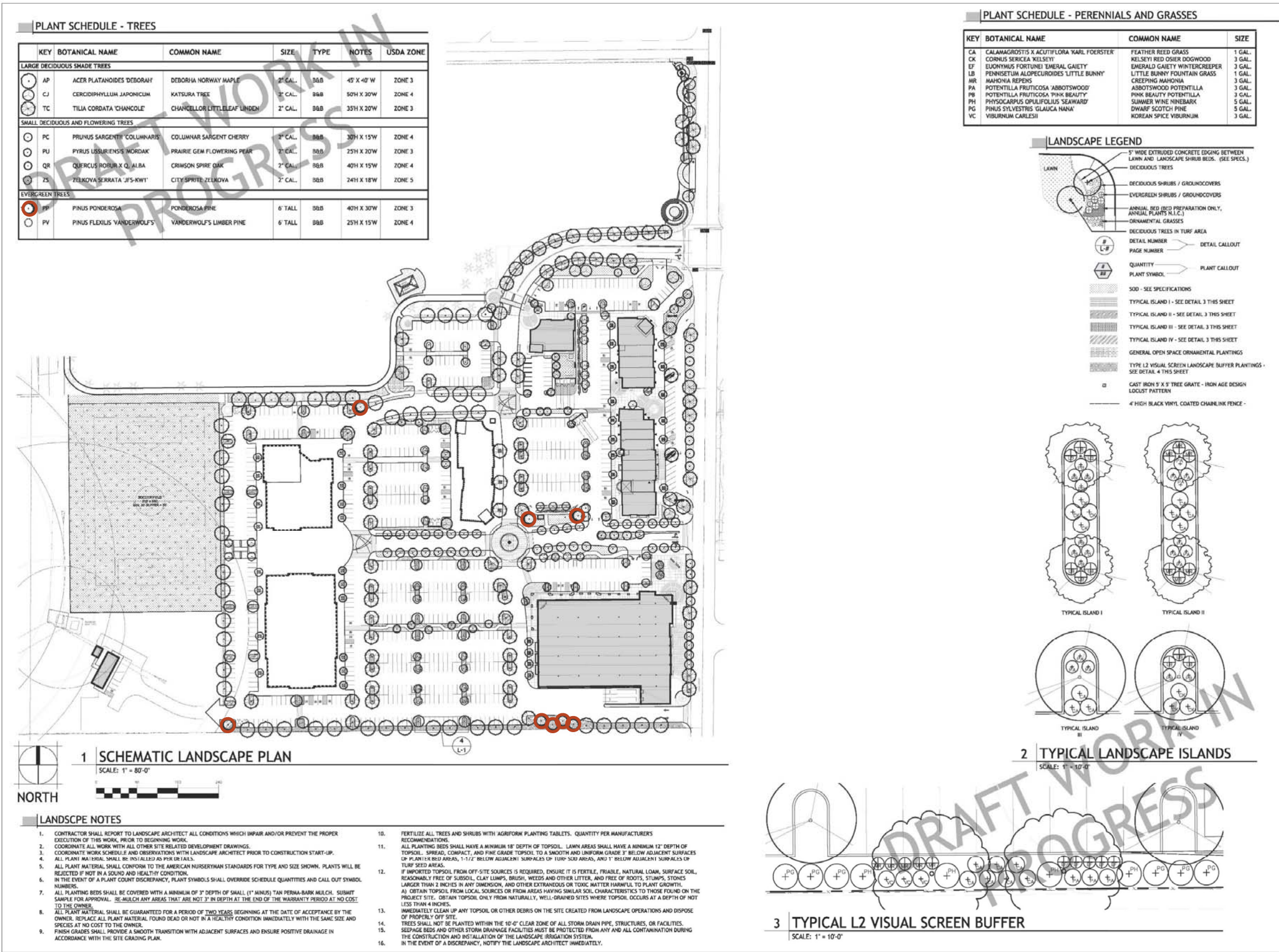
Replacement Ponderosa Pines are marked on this plan by the symbol: ★





Site: Trees

DRB Guidance: “The applicant shall look into opportunities to provide either additional site trees and/or provide select trees of greater size in lieu of saving the two existing ponderosa pines which are in poor health and may be difficult to save.”



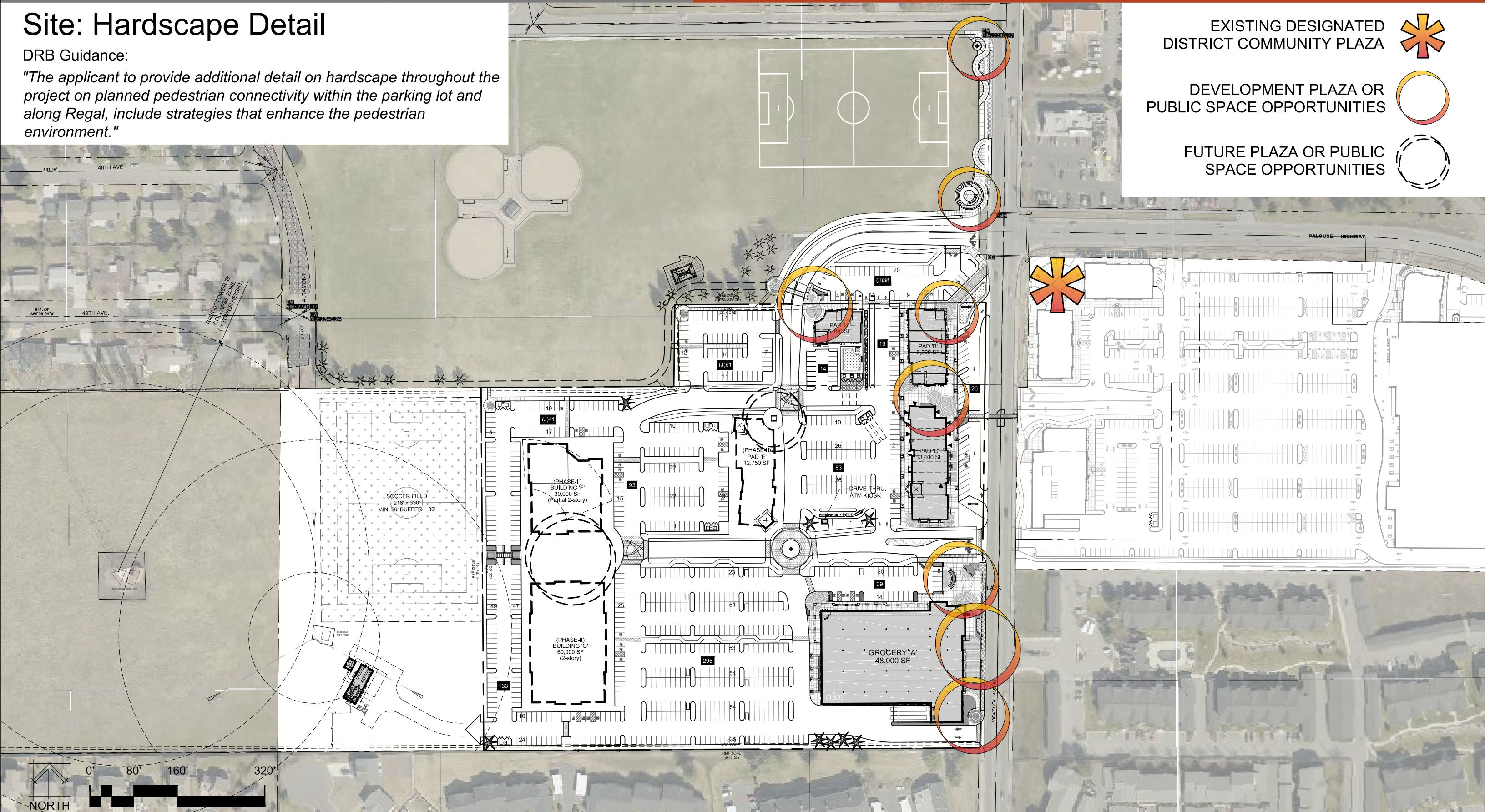


# Site: Hardscape Detail

DRB Guidance:

"The applicant to provide additional detail on hardscape throughout the project on planned pedestrian connectivity within the parking lot and along Regal, include strategies that enhance the pedestrian environment."

- EXISTING DESIGNATED DISTRICT COMMUNITY PLAZA 
- DEVELOPMENT PLAZA OR PUBLIC SPACE OPPORTUNITIES 
- FUTURE PLAZA OR PUBLIC SPACE OPPORTUNITIES 





## Site: Ribbon Park Separation

DRB Guidance: *“The applicant will provide a landscape separation at the north end of the “ribbon park” between the curb and sidewalk at Regal.”*





## Site: LID

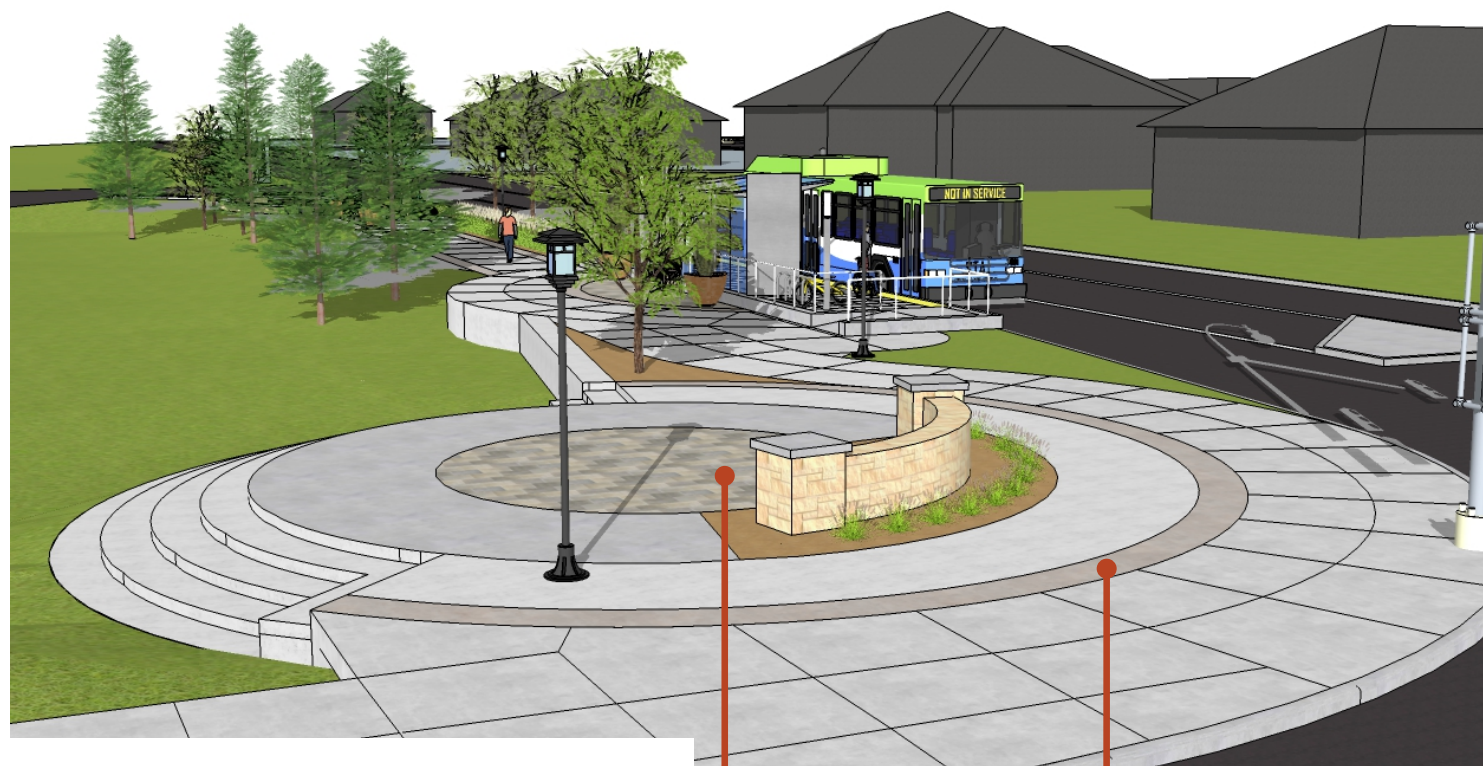
DRB Guidance: *“The applicant is requested to provide innovative, alternative LID solutions.”*

### *Applicant Response:*

The development team reviewed opportunities found within the DOE Eastern Washington Low Impact Development Guidance Manual; which we understand is the guiding document for LID development adopted by the City of Spokane. Most LID opportunities concern alternative strategies for the infiltration of storm water. As discussed during the Collaborative Workshop, due to subsurface geologic and hydrologic conditions, infiltration of development storm water is not feasible. Other opportunities, such as vegetative roofs, were considered and were found to be cost prohibitive.

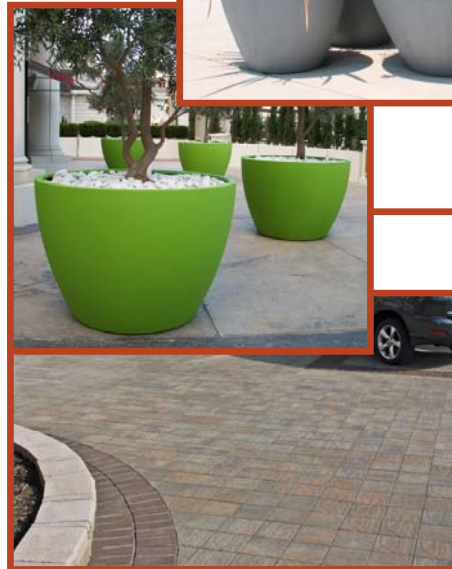
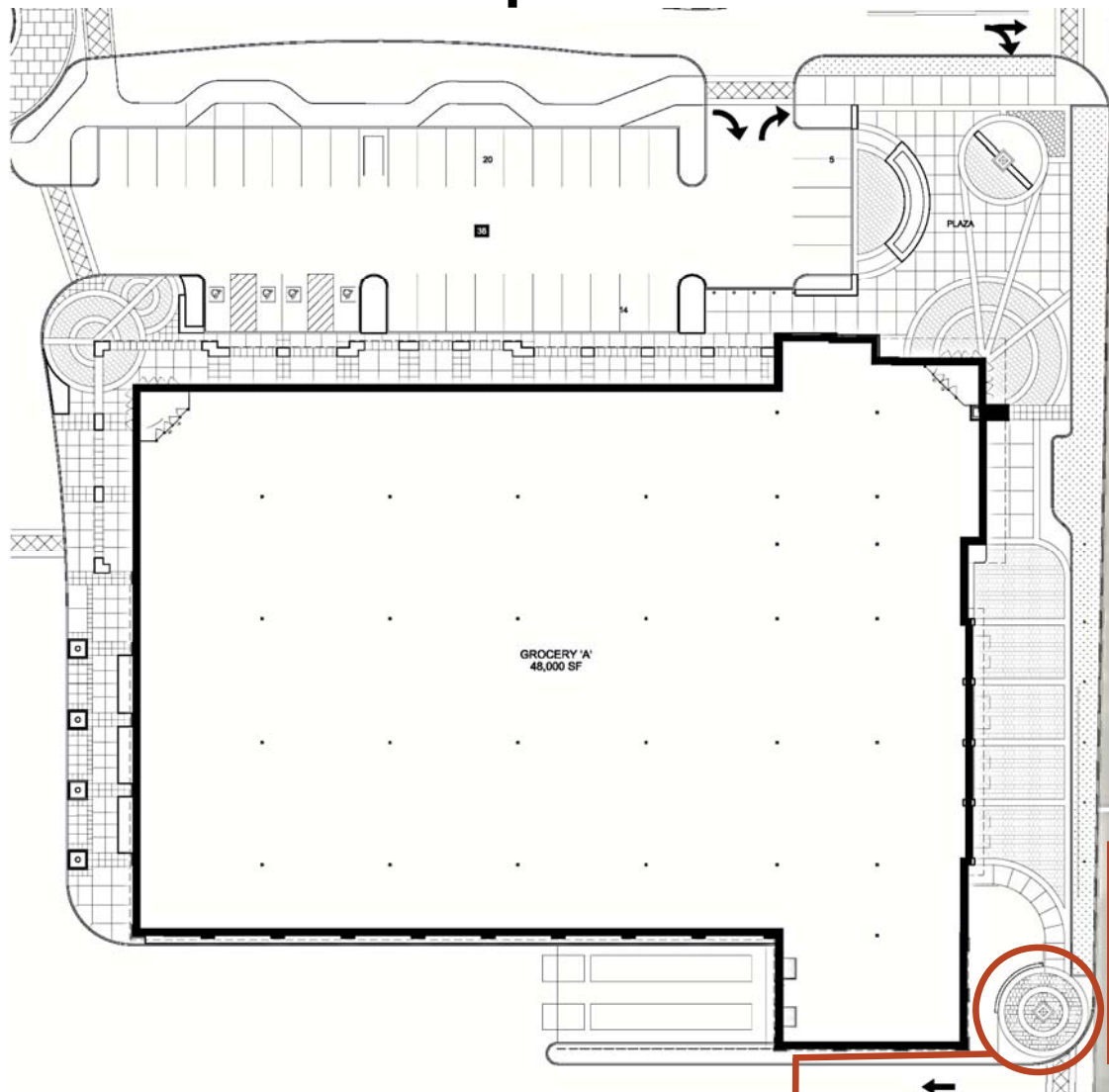


## Site: Hardscape Detail – Ribbon Park



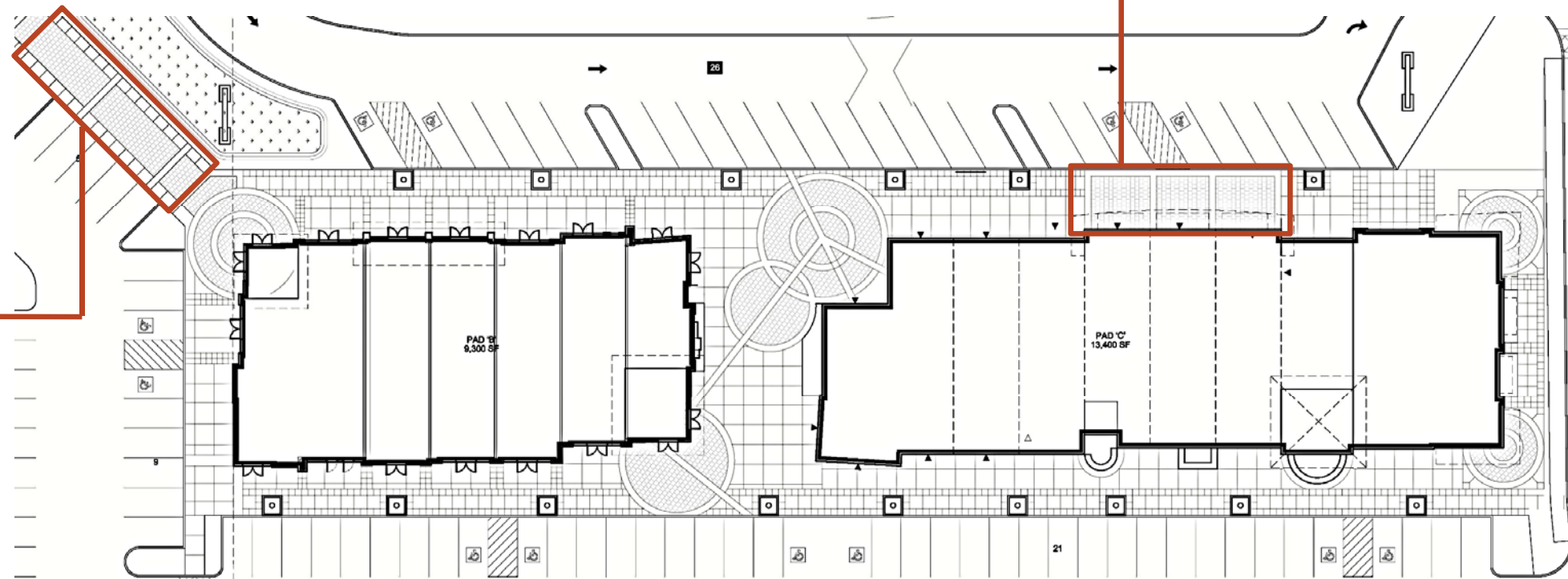


## Site: Hardscape Detail Retail-A



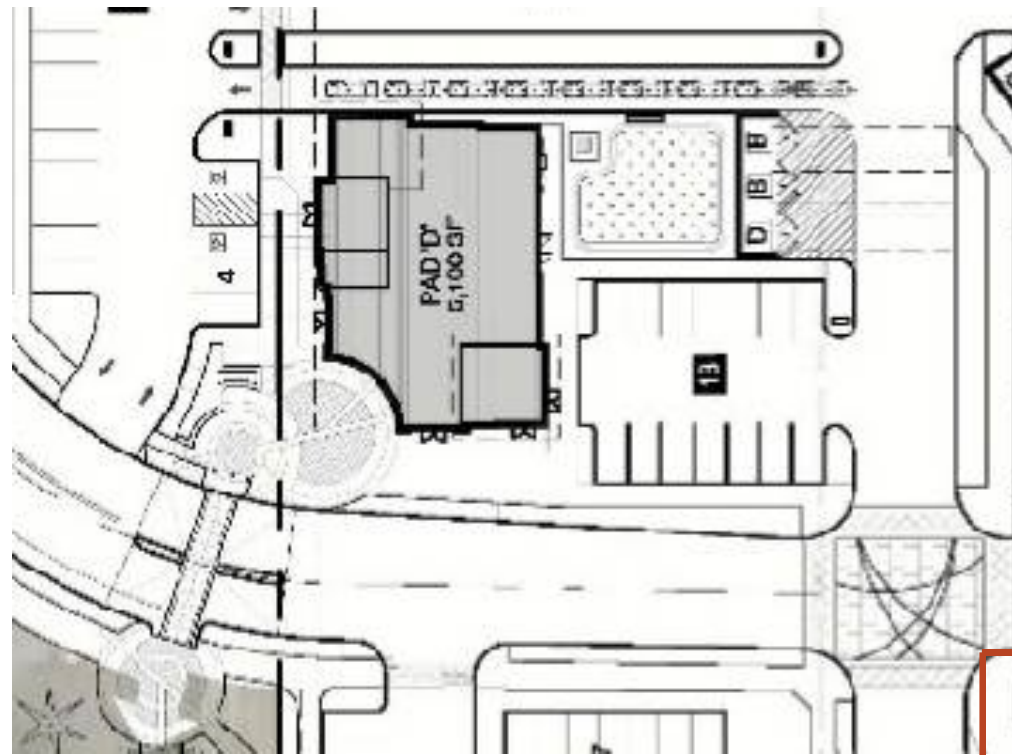


## Site: Hardscape Detail Retails - B & C

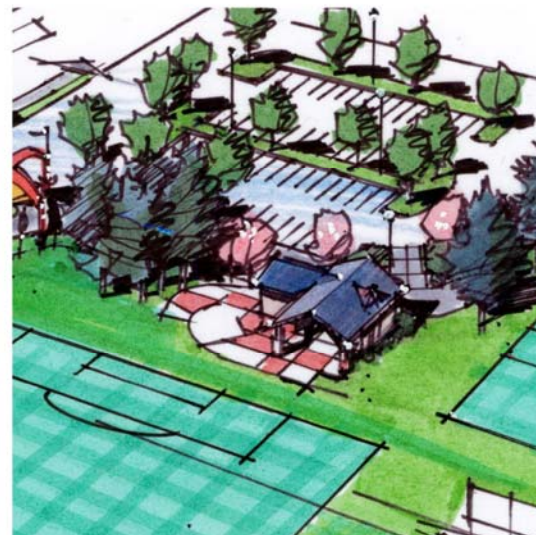




## Site: Hardscape Detail Retails - D







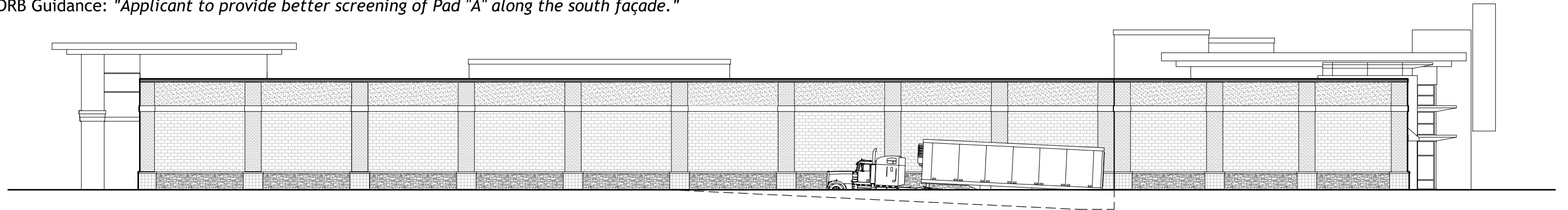
## DRB GUIDANCE BUILDING

- *“We request the applicant, before the next meeting, define whether Pad “E” will be in phase I or II.”*
- *“Applicant to provide better screening of Pad “A” along the south façade.”*



# Building: Retail-A

DRB Guidance: "Applicant to provide better screening of Pad "A" along the south façade."



## 1 Retail-A South Elevation (Presented at Collaborative Workshop) SCALE: 1" = 40'-0"

### Applicant Response:

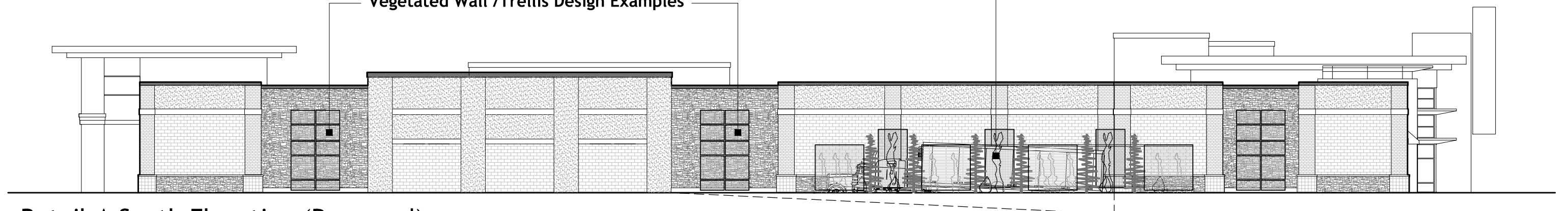
In addition to incorporating Ponderosa Pine into select areas of the landscape buffer screen, the south facade' has been enhance with vegetative screen walls located between the Retail-A service area and the property line. Parapet articulation and changes in materials and textures are also used to reduce mass and visual impact.



Vegetated Wall /Trellis Design Examples

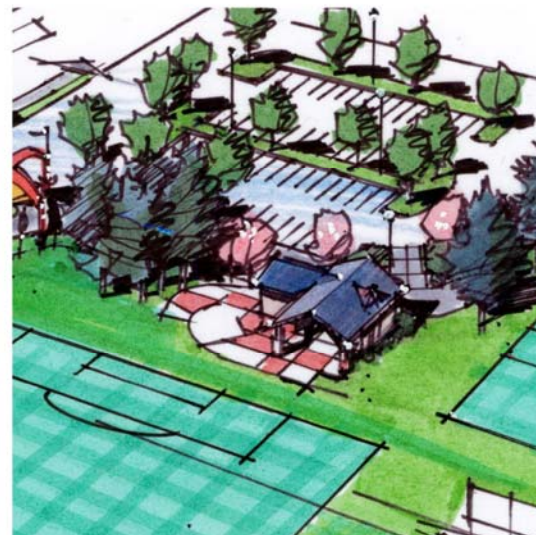


Screen Wall Trellis Design Examples



## 2 Retail-A South Elevation (Proposed) SCALE: 1" = 40'-0"





# Site Signs



1 SOUTHGATE KXLY DEVELOPMENT - TWO D/S ILLUMINATED PYLON SIGNS  
SCALE: 1/4" = 1' - 0"

TWO SIGNS - 1A 1B 112 SQ. FT. PER DISPLAY



ENDICOTT BRICK  
MEDIUM IRONSPOT 77  
27

1 of 4

Client:  
**Southgate KXLY Development**  
SWC Regal St. / Palouse Hwy  
Spokane, WA

Date:  
**14 Feb. 2017**

File Name:  
**RTV 1 R1**

Project Manager:  
**John L. Johnston**  
john@signcorpokane.com

Drawn by:  
**TEA**  
design@signcorpokane.com

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approximate. All rights to its  
use are reserved.

Revision:  
**R1 - 18>22ft 2/14**

[ ] Approved  
[ ] Approved With Changes Noted

Customer Signature

Date

Landlord Signature

Date



Inland Sign & Lighting  
www.SignCorpSpokane.com

Spokane Office  
131 N. Altamont  
Spokane, Wa. 99202

509-535-2913  
FAX 509-535-6522

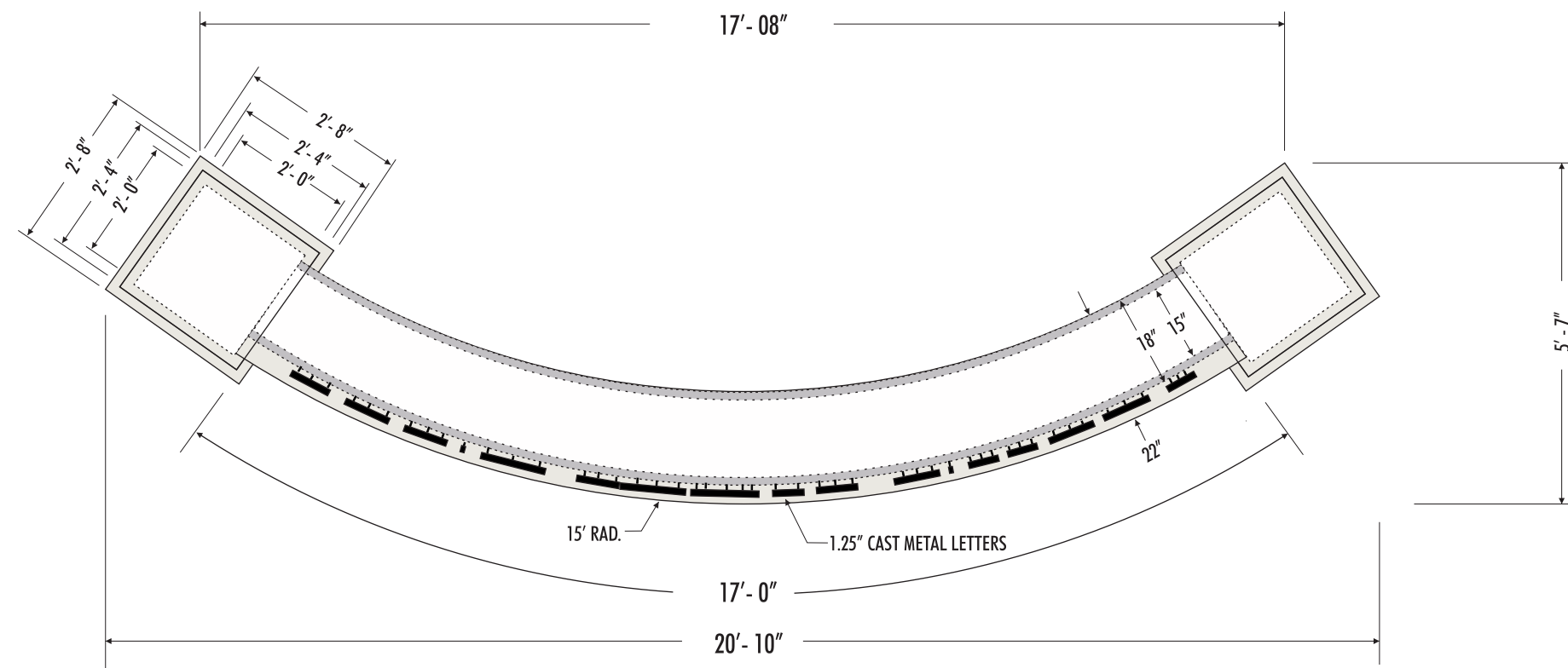


**SUITABLE FOR WET LOCATIONS**  
This sign is intended to be installed  
in accordance with the requirements of  
Article 600 of the National Electrical Code  
and/or other applicable local codes.  
This includes proper grounding and  
bonding of the sign.



2

12" CAST METAL LETTERS  
PAINTED ALUMINUM 1.25" DEEP  
FONT: RIBBON ,  
COLOR: DURANODIC BROZE (SATIN)



ENDICOTT BRICK  
MEDIUM IRONSPOT 77

Client:

**Southgate KXLY Development**  
**SWC Regal St. / Palouse Hwy**  
**Spokane, WA**

File Name:  
**RTV 1 R1**

Project Manager:  
**John L. Johnston**  
john@signcorpokane.com

Drawn by:  
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Revision:  
**R1 - 18>22ft 2/14**

☐ Approved

☐ Approved With Changes Noted

Customer Signature \_\_\_\_\_

Date \_\_\_\_\_

Landlord Signature \_\_\_\_\_

Date \_\_\_\_\_



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**SUITABLE FOR WET LOCATIONS**

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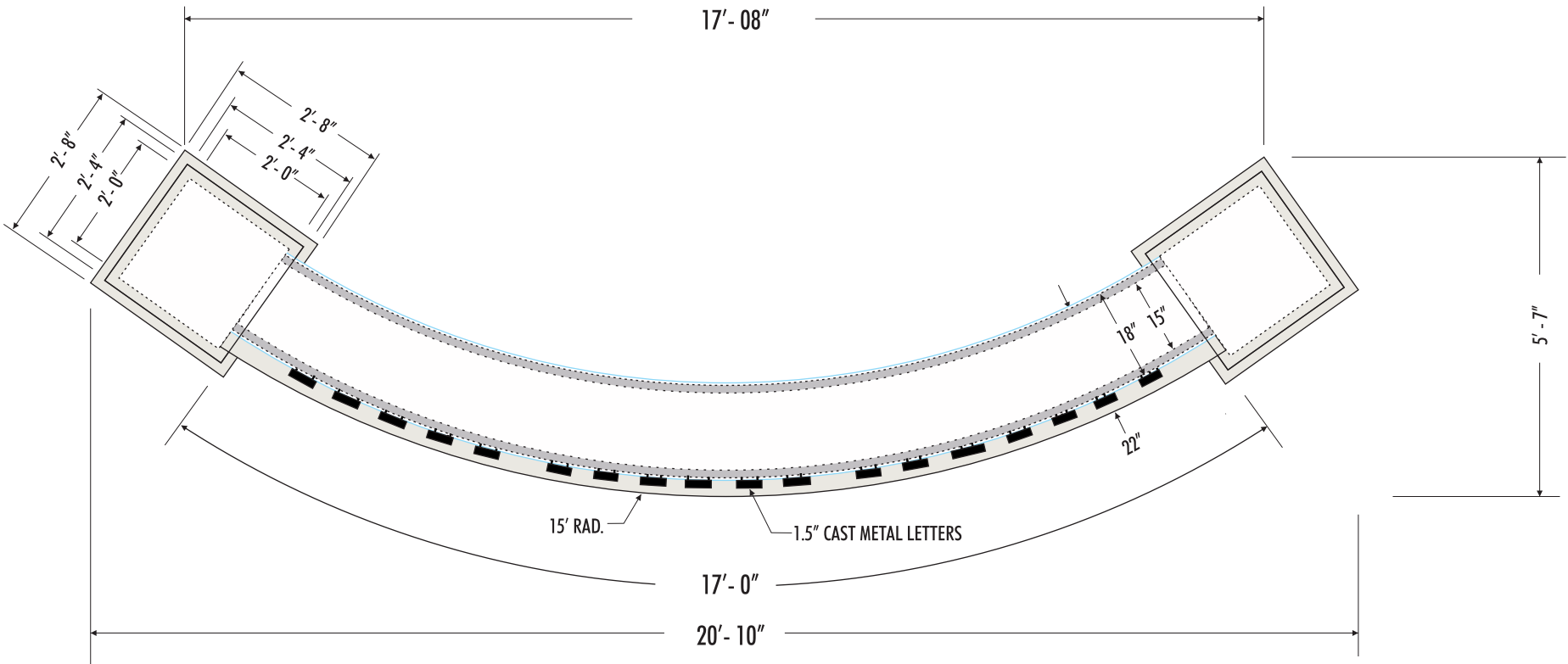


3 SOUTHGATE KXLY DEVELOPMENT - MONUMENT W/ CAST METAL LETTERS

SCALE: 3/8" = 1' - 0"

68 SQ. FT.

15" CAST METAL LETTERS  
PAINTED ALUMINUM 1.5" DEEP  
FONT: RIBBON CONDENSED,  
COLOR: DURANODIC BROZE (SATIN)



ENDICOTT BRICK  
MEDIUM IRONSPOT 77

3 of 4

Client:  
Southgate KXLY Development  
SWC Regal St. / Palouse Hwy  
Spokane, WA

Date:  
14 Feb. 2017

File Name:  
RTV 1 R1

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John L. Johnston  
john@signcorpokane.com

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Revision:  
R1 - 18>22ft 2/14

[ ] Approved  
[ ] Approved With Changes Noted

Customer Signature

Date

Landlord Signature

Date



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Spokane Office  
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FAX 509-535-6522



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and/or other applicable local codes.  
This includes proper grounding and  
bonding of the sign.



**4 SOUTHGATE KXLY DEVELOPMENT - MONUMENT W/ CAST METAL LETTERS**  
SCALE: MTS



Client:  
**Southgate KXLY Development**  
SWC Regal St. / Palouse Hwy  
Spokane, WA

Date:  
**14 Feb. 2017**

File Name:  
**RTV 1 R1**

Project Manager:  
**John L. Johnston**  
john@signcorpokane.com

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Revision:  
**R1 - 18>22ft 2/14**

[ ] Approved  
[ ] Approved With Changes Noted

Customer Signature \_\_\_\_\_

Date \_\_\_\_\_

Landlord Signature \_\_\_\_\_

Date \_\_\_\_\_



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[www.SignCorpSpokane.com](http://www.SignCorpSpokane.com)

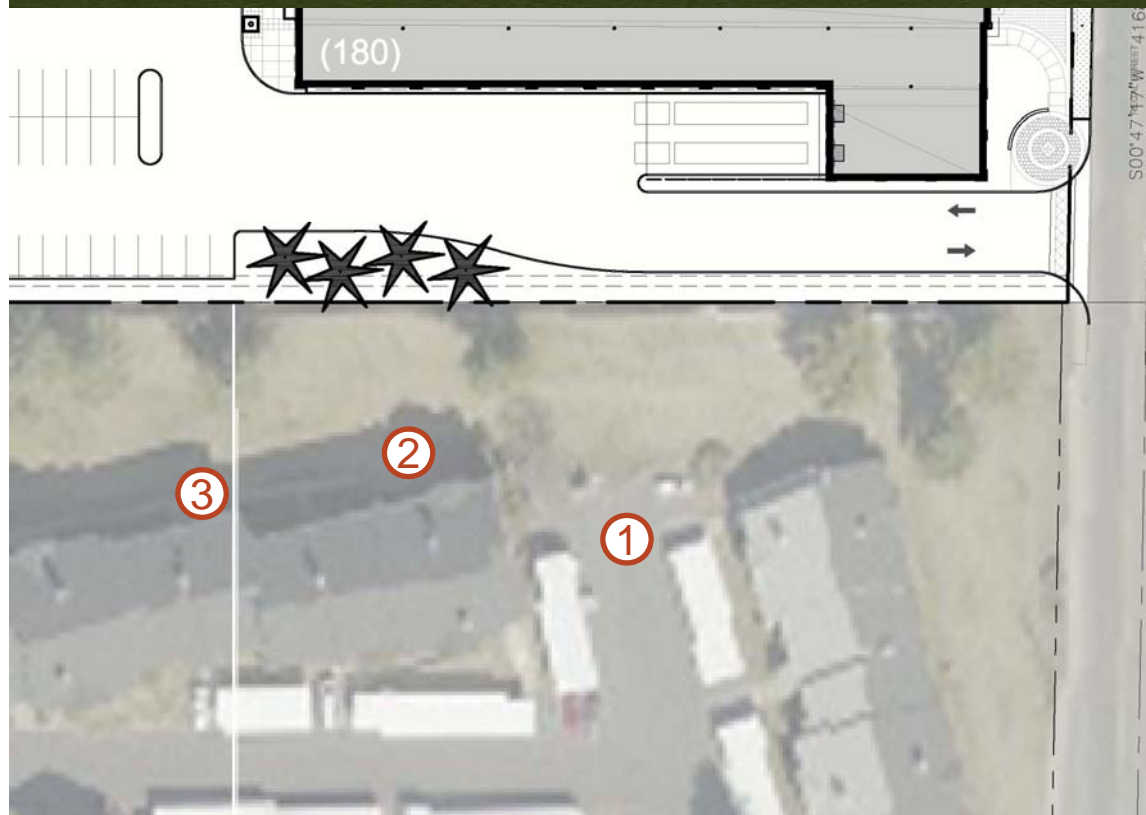
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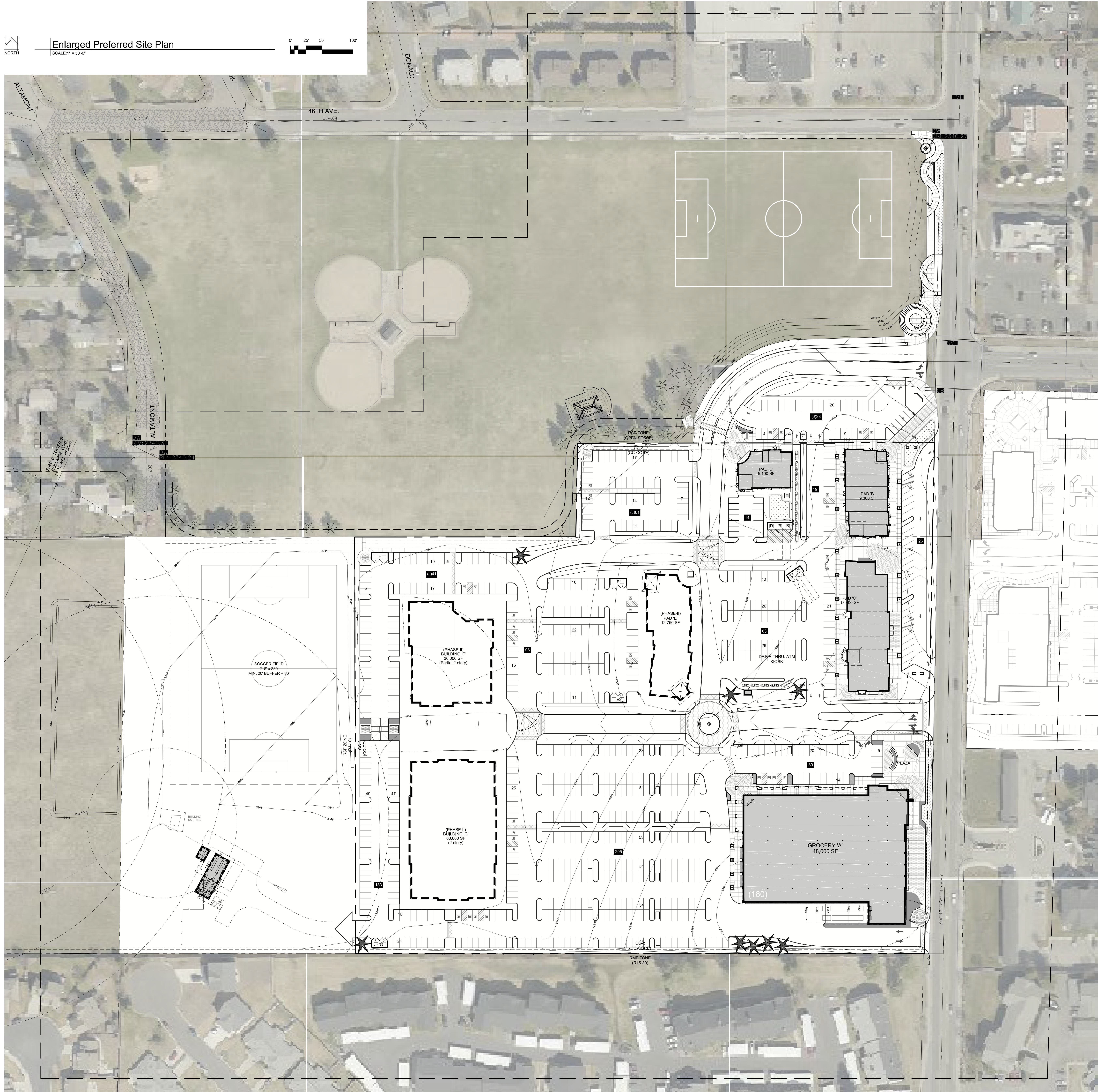
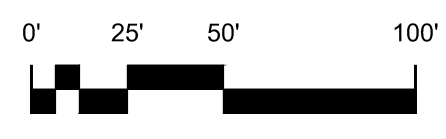
## Building: Retail-A (Views from Adjacent Multi-Family)







Enlarged Preferred Site Plan  
SCALE: 1" = 50'-0"



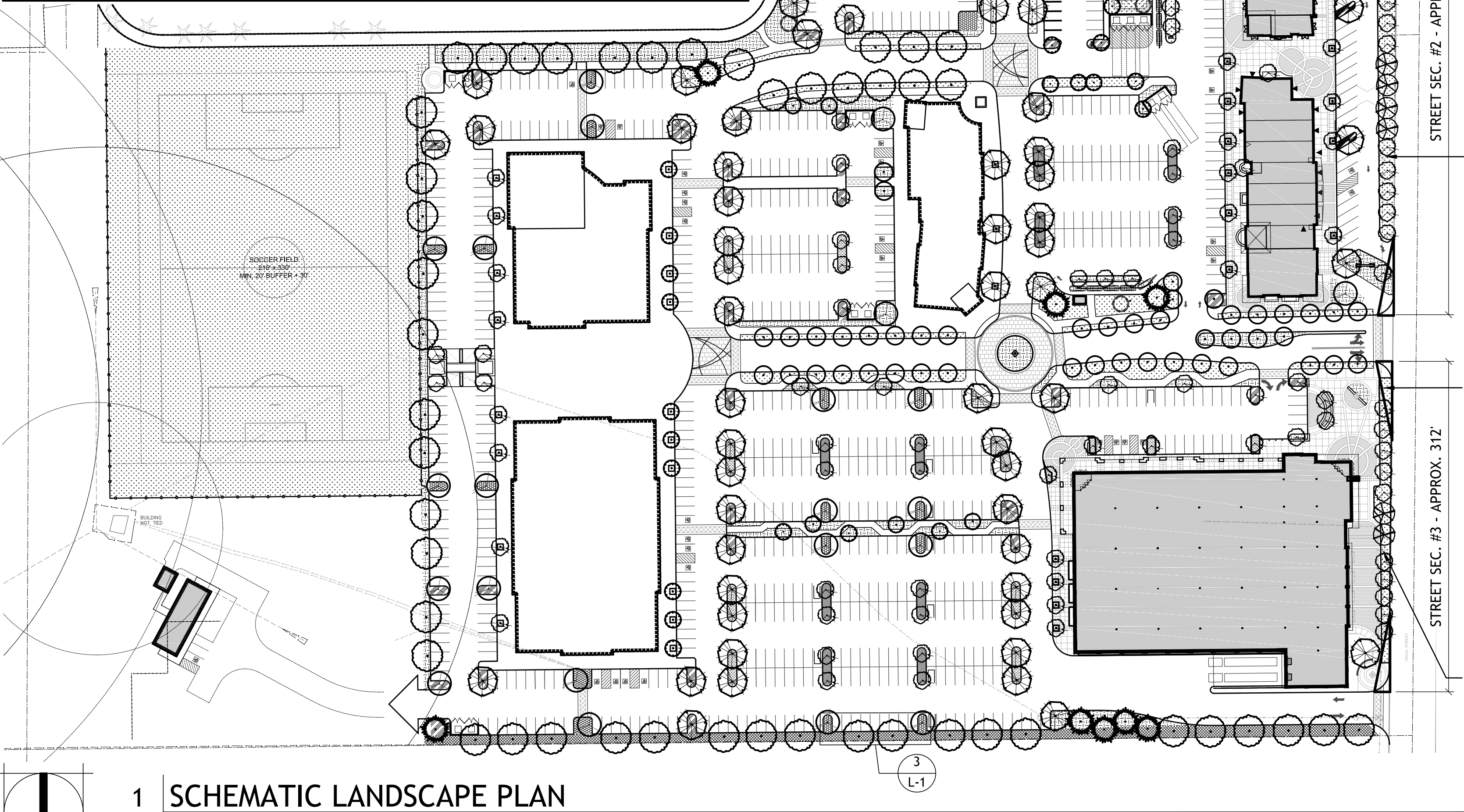


PLANT SCHEDULE - TREES

KEY	BOTANICAL NAME	COMMON NAME	SIZE	TYPE	NOTES	USDA ZONE
LARGE DECIDUOUS SHADE TREES						
AP	ACER PLATANOIDES 'DEBORAH'	DEBORHA NORWAY MAPLE	2" CAL.	B&B	45' X 40' W	ZONE 3
CJ	CERCIDIPHYLLUM JAPONICUM	KATSURA TREE	2" CAL.	B&B	50'H X 30'W	ZONE 4
TC	TILIA CORDATA 'CHANCOLE'	CHANCELLOR LITTLELEAF LINDEN	2" CAL.	B&B	35'H X 20'W	ZONE 3
SMALL DECIDUOUS AND FLOWERING TREES						
MA	MAACKIA AMURENSIS	AMUR MAACKIA	2" CAL.	B&B	25'H X 20'W	ZONE 4
PC	PRUNUS SARGENTII 'COLUMNARIS'	COLUMNAR SARGENT CHERRY	2" CAL.	B&B	30'H X 15'W	ZONE 4
PU	PYRUS USSURIENSIS 'MORDAK'	PRAIRIE GEM FLOWERING PEAR	2" CAL.	B&B	25'H X 20'W	ZONE 3
QR	QUERCUS ROBUR X Q. ALBA	CRIMSON SPIRE OAK	2" CAL.	B&B	40'H X 15'W	ZONE 4
ZS	ZELKOVA SERRATA 'JFS-KW1'	CITY SPRITE ZELKOVA	2" CAL.	B&B	24'H X 18'W	ZONE 5
EVERGREEN TREES						
PP	PINUS PONDEROSA	PONDEROSA PINE	6' TALL	B&B	40'H X 30'W	ZONE 3
PV	PINUS FLEXILIS 'VANDERWOLFS'	VANDERWOLF'S LIMBER PINE	6' TALL	B&B	25'H X 15'W	ZONE 4

STREET TREE CALCULATIONS

STREET SECTION	LINEAR FEET/LENGTH OF STREET SECTION	REQ. TREE SPACING	# OF TREES REQ.	TREES PROVIDED
STREET SECTION 1	LENGTH: 342' - 66' BUS STOP PLACE HOLDER = 276'	DIVIDED BY 25' OC.	11 TREES (MIN)	9 TREES DUE TO POWER POLES AND LIGHTS
STREET SECTION 2	LENGTH: 519' - 84' (DRIVEWAYS) = 435'	DIVIDED BY 25' OC.	17.4 TREES (MIN)	18 TREES
STREET SECTION 3	LENGTH: 312'	DIVIDED BY 25' OC.	12.4 TREES (MIN)	12 TREES DUE TO POWER POLES



1 SCHEMATIC LANDSCAPE PLAN

SCALE: 1" = 80'-0"

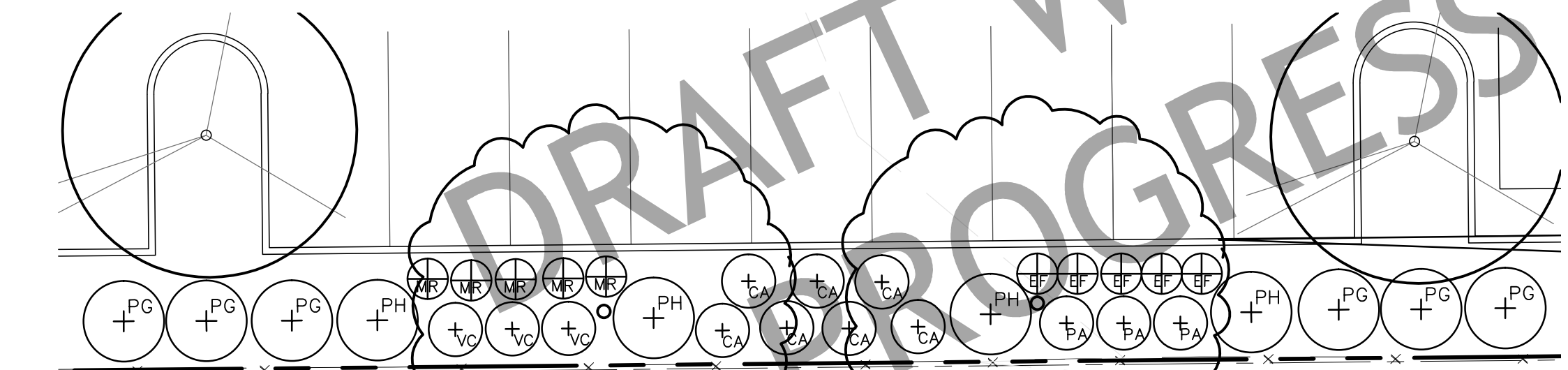


LANDSCAPE NOTES

- CONTRACTOR SHALL REPORT TO LANDSCAPE ARCHITECT ALL CONDITIONS WHICH IMPAIR AND/OR PREVENT THE PROPER EXECUTION OF THIS WORK, PRIOR TO BEGINNING WORK.
- COORDINATE ALL WORK WITH ALL OTHER SITE RELATED DEVELOPMENT DRAWINGS.
- COORDINATE WORK SCHEDULE AND OBSERVATIONS WITH LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION START-UP.
- ALL PLANT MATERIAL SHALL BE INSTALLED AS PER DETAILS.
- ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN NURSERYMAN STANDARDS FOR TYPE AND SIZE SHOWN. PLANTS WILL BE REJECTED IF NOT IN A SOUND AND HEALTHY CONDITION.
- IN THE EVENT OF A PLANT COUNT DISCREPANCY, PLANT SYMBOLS SHALL OVERRIDE SCHEDULE QUANTITIES AND CALL OUT SYMBOL NUMBERS.
- ALL PLANTING BEDS SHALL BE COVERED WITH A MINIMUM OF 3" DEPTH OF SMALL (1" MINUS) TAN PERMA-BARK MULCH. SUBMIT SAMPLE FOR APPROVAL. RE-MULCH ANY AREAS THAT ARE NOT 3" IN DEPTH AT THE END OF THE WARRANTY PERIOD AT NO COST TO THE OWNER.
- ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS BEGINNING AT THE DATE OF ACCEPTANCE BY THE OWNER. REPLACE ALL PLANT MATERIAL FOUND DEAD OR NOT IN A HEALTHY CONDITION IMMEDIATELY WITH THE SAME SIZE AND SPECIES AT NO COST TO THE OWNER.
- FINISH GRADES SHALL PROVIDE A SMOOTH TRANSITION WITH ADJACENT SURFACES AND ENSURE POSITIVE DRAINAGE IN ACCORDANCE WITH THE SITE GRADING PLAN.
- FERTILIZE ALL TREES AND SHRUBS WITH 'AGRIFORM' PLANTING TABLETS. QUANTITY PER MANUFACTURER'S RECOMMENDATIONS.
- ALL PLANTING BEDS SHALL HAVE A MINIMUM 18" DEPTH OF TOPSOIL. LAWN AREAS SHALL HAVE A MINIMUM 12" DEPTH OF TOPSOIL. SPREAD, COMPACT, AND FINE GRADE TOPSOIL TO A SMOOTH AND UNIFORM GRADE 3" BELOW ADJACENT SURFACES OF PLANTER BED AREAS, 1-1/2" BELOW ADJACENT SURFACES OF TURF SOD AREAS, AND 1" BELOW ADJACENT SURFACES OF TURF SEED AREAS.
- IF IMPORTED TOPSOIL FROM OFF-SITE SOURCES IS REQUIRED, ENSURE IT IS FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH, WEEDS AND OTHER LITTER, AND FREE OF ROOTS, STUMPS, STONES LARGER THAN 2 INCHES IN ANY DIMENSION, AND OTHER EXTRANEANEOUS OR TOXIC MATTER HARMFUL TO PLANT GROWTH. A) OBTAIN TOPSOIL FROM LOCAL SOURCES OR FROM AREAS HAVING SIMILAR SOIL CHARACTERISTICS TO THOSE FOUND ON THE PROJECT SITE. OBTAIN TOPSOIL ONLY FROM NATURALLY, WELL-DRAINED SITES WHERE TOPSOIL OCCURS AT A DEPTH OF NOT LESS THAN 4 INCHES.
- IMMEDIATELY CLEAN UP ANY TOPSOIL OR OTHER DEBRIS ON THE SITE CREATED FROM LANDSCAPE OPERATIONS AND DISPOSE OF PROPERLY OFF SITE.
- TREES SHALL NOT BE PLANTED WITHIN THE 10'-0" CLEAR ZONE OF ALL STORM DRAIN PIPE, STRUCTURES, OR FACILITIES. SEEPAGE BEDS AND OTHER STORM DRAINAGE FACILITIES MUST BE PROTECTED FROM ANY AND ALL CONTAMINATION DURING THE CONSTRUCTION AND INSTALLATION OF THE LANDSCAPE IRRIGATION SYSTEM.
- IN THE EVENT OF A DISCREPANCY, NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY.

4 REGAL PLAZA ENLARGEMENT

SCALE: 1" = 30'-0"



3 TYPICAL L2 VISUAL SCREEN BUFFER

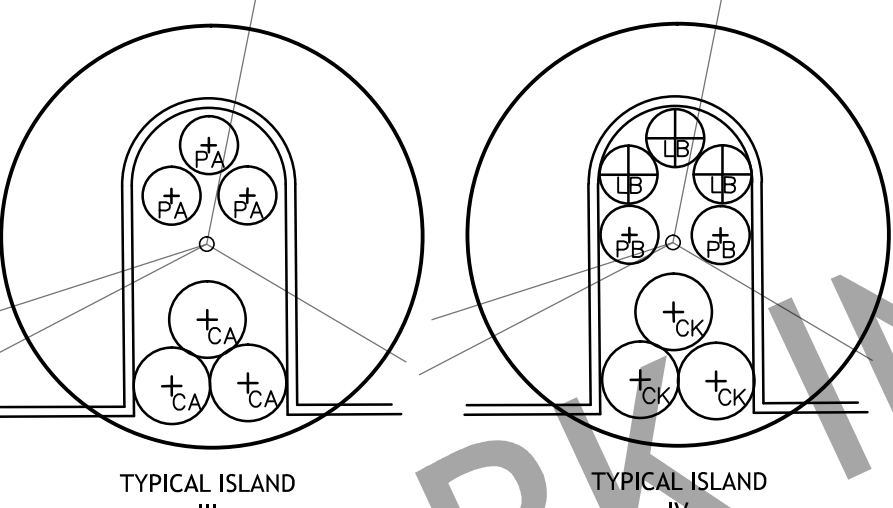
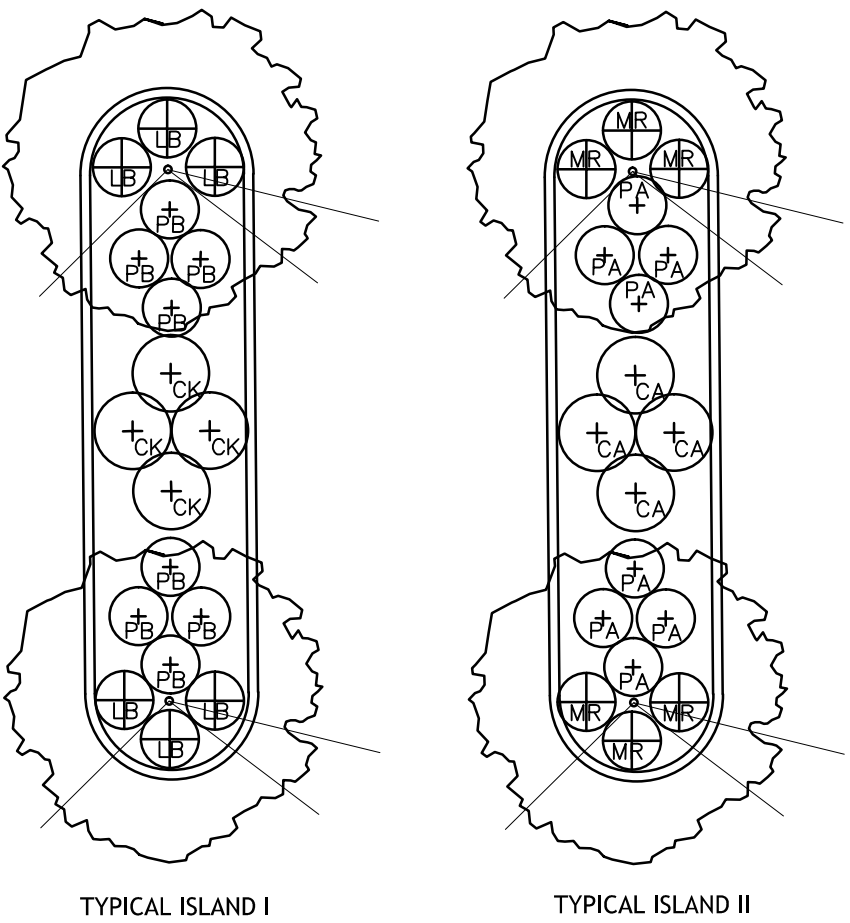
SCALE: 1" = 10'-0"

PLANT SCHEDULE - PERENNIALS AND GRASSES

KEY	BOTANICAL NAME	COMMON NAME	SIZE
CA	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	1 GAL.
CK	CORNUS SERICEA 'KELSEY'	KELSEY RED OSIER DOGWOOD	3 GAL.
EF	EUONYMUS FORTUNEI 'EMERALD GAIETY'	EMERALD GAIETY WINTERCREEPER	3 GAL.
LB	PENNISSETUM ALOPECUROIDES 'LITTLE BUNNY'	LITTLE BUNNY FOUNTAIN GRASS	1 GAL.
MR	MAHONIA REPENS	CREeping MAHONIA	3 GAL.
PA	POTENTILLA FRUTICOSA 'ABBOTSWOOD'	ABBOTSWOOD POTENTILLA	3 GAL.
PB	POTENTILLA FRUTICOSA 'PINK BEAUTY'	PINK BEAUTY POTENTILLA	3 GAL.
PH	PHYSCARPUS OPULIFOLIUS 'SEAWARD'	SUMMER WINE NINEBARK	5 GAL.
PG	PINUS SYLVESTRIS 'GLAUCNA NANA'	DWARF SCOTCH PINE	5 GAL.
VC	VIBURNUM CARLESII	KOREAN SPICE VIBURNUM	3 GAL.

LANDSCAPE LEGEND

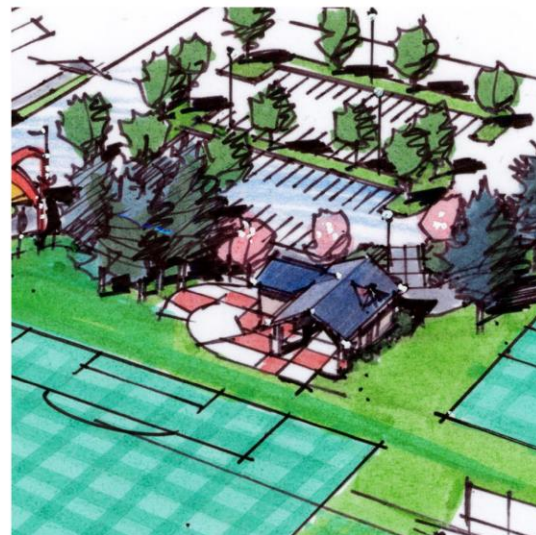
- 5" WIDE EXTRUDED CONCRETE EDGING BETWEEN LAWN AND LANDSCAPE SHRUB BEDS. (SEE SPECS.)
- DECIDUOUS TREES
- DECIDUOUS SHRUBS / GROUNDCOVERS
- EVERGREEN SHRUBS / GROUNDCOVERS
- ANNUAL BED (BED PREPARATION ONLY, ANNUAL PLANTS N.I.C.)
- ORNAMENTAL GRASSES
- DECIDUOUS TREES IN TURF AREA
- DETAIL NUMBER
- PAGE NUMBER
- QUANTITY
- PLANT SYMBOL
- SOD - SEE SPECIFICATIONS
- TYPICAL ISLAND I - SEE DETAIL 2 THIS SHEET
- TYPICAL ISLAND II - SEE DETAIL 2 THIS SHEET
- TYPICAL ISLAND III - SEE DETAIL 2 THIS SHEET
- TYPICAL ISLAND IV - SEE DETAIL 2 THIS SHEET
- GENERAL OPEN SPACE ORNAMENTAL PLANTINGS
- TYPE L2 VISUAL SCREEN LANDSCAPE BUFFER PLANTINGS - SEE DETAIL 3 THIS SHEET
- CAST IRON 5' X 5' TREE GRATE - IRON AGE DESIGN LOCUST PATTERN
- 4' HIGH BLACK VINYL COATED CHAIN LINK FENCE
- CLEAR VIEW TRIANGLE



2 TYPICAL LANDSCAPE ISLANDS

SCALE: 1" = 10'-0"





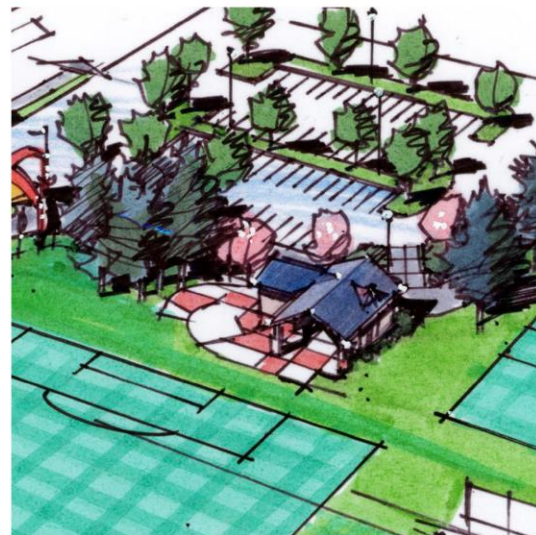
# Southgate KXLY Development

SWC Regal Street & Palouse Highway

Design Review Board – Supplemental #01  
December 30, 2016



# Background & Context





# Introduction

## Narrative & Summary

### *Background:*

In 2009, as part of Comprehensive Plan map amendments, the City and Owners of the three subject properties, entered into an agreement that would provide for commercial and retail development subject to certain conditions. The guiding precept of the resulting Development Agreement (DA) is establishment of a unified commercial district, spanning each of the three properties; the epicenter of which is the intersection of Regal and Palouse. The DA required that the Developers' prepare an Integrated Site Plan (ISP) showing implementation of six planning principles which establish the unified commercial district. In addition, the ISP provides a unified theme of similar architectural styles, colors, materials, site amenities. The unified theme is embodied in the ISP "Kit of Parts." The Integrated Site Plan and Kit of Parts were processed through the Design Review Board with collaborative input from City staff and Southgate Neighborhood Council representatives. The ISP and Kit of Parts were approved by the Planning Director with conditions in May 2013.

The Black/Target Development was the first project to move forward under the ISP and Development Agreement. The "Black Specific Plan" was processed through the Design Review Board to verify consistency with the intent of the ISP, Kit of Parts, and Development Agreement and was approved by the Director in April of 2013.

### *Project Description:*

The KXLY development is the second development to proceed under the DA. The project area is +/- 14 acres and is contemplated to be anchored by a large regional grocery store, four buildings to accommodate small and medium-sized retailers, and two large future-phase buildings thought to be offices or similar uses. Future-phase buildings are not part of this application.

The KXLY Site Specific Plan incorporates the intent of the ISP's six guiding design principles and includes such features as: pedestrian and bicycle connectivity, preservation of selected Ponderosa Pine trees, unified design theme, community plaza, preservation of selected viewscales and design strategies to allow the project to evolve into an urban center over time as market forces dictate; in addition to compliance with the City's strict development standards for projects like this including landscaping, site lighting, building façade treatment, setbacks, parking standards, etc. Additionally, the project will incorporate selected features and finishes from the ISP Kit of Parts.

### *Submittal Intent*

The intent of this Design Review Submittal is to present the design work prepared to date of both a general and specific nature, discuss applicable elements of the proposal with the DRB and demonstrate proposal's consistency with the intent of the ISP, Kit of Parts, and Development Agreement. The design effort presented herein is the culmination of years of collaboration with area stakeholders, most notably the Southgate Neighborhood Council, SNG Land Use Committee, City of Spokane, and City of Spokane Parks and Recreation Department.



## Context

### Integrated Sites & City Property





## Context

Property Purchased from City of Spokane



### *PSA City Parcel*

In addition to property already owned, KXLY has entered into agreement with the City of Spokane to purchase the property immediately adjacent to the north, approximately 2.7 acres, bringing the total project ownership to +/- 16.7 acres.

The purchase and sale agreement with the City sets forth requirements of both the buyer and seller. The purchased property is to be treated as an extension of the principal KXLY property and subject to the same design and development requirements established in the DA and ISP and make development subject to the implementation memo. In addition, the City will set aside \$300k from the property sale price for trail development within the greater Southgate Neighborhood (at the time of this narrative, we understand the SGN has pledged \$150k of the \$300k to develop trails associated with this project). The PSA is contingent upon several factors including a successful map amendment to the Comprehensive Plan of the purchase property to CC2-DC and securing an easement across City Park property to access the development from the fourth leg of the signalized intersection at Regal and Palouse.

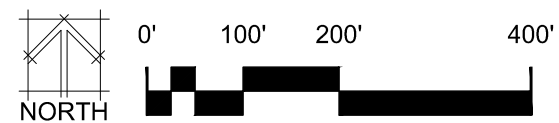
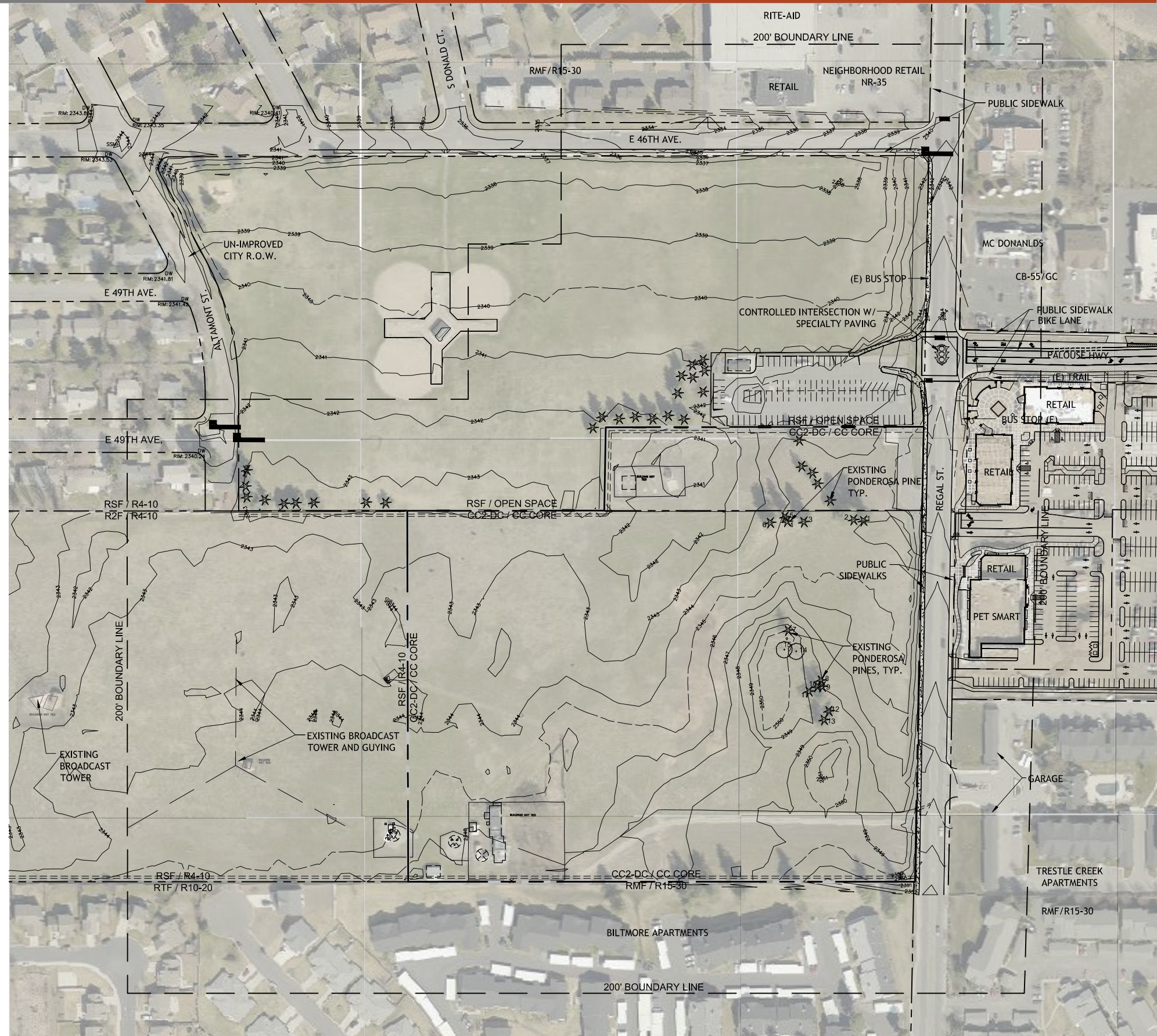


## Existing Site Conditions and Context





# Context: Existing Conditions





# Context: Existing Street Views



1



2



3



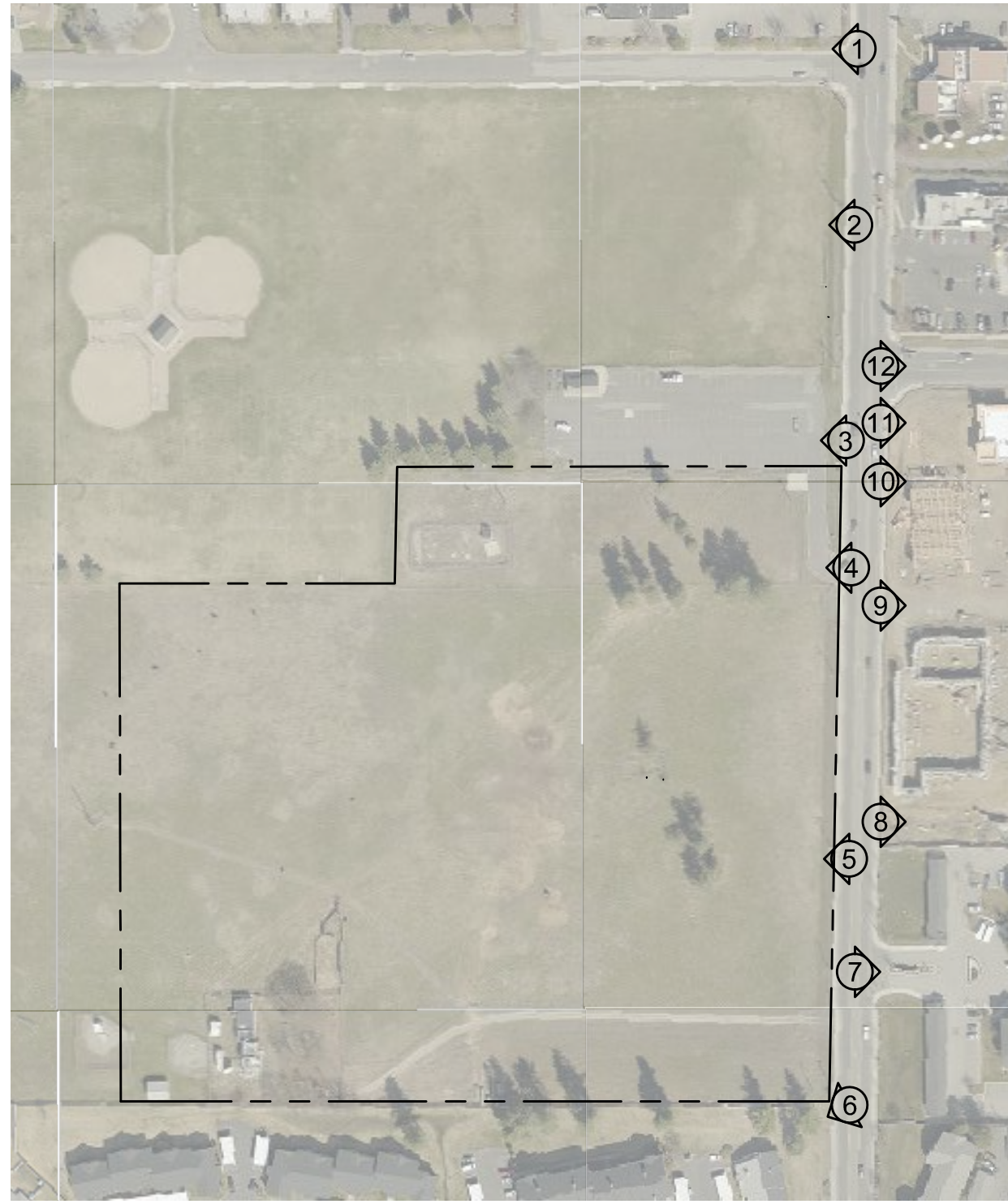
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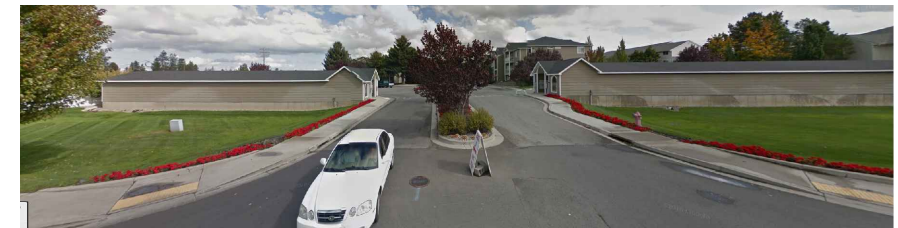
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Existing Aerial View  
SCALE: 1" = 200'-0"



7



8



9



10



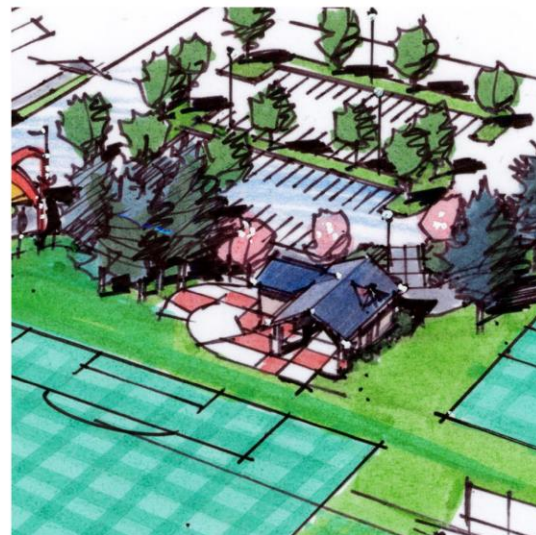
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12







# City Parks Board Collaboration

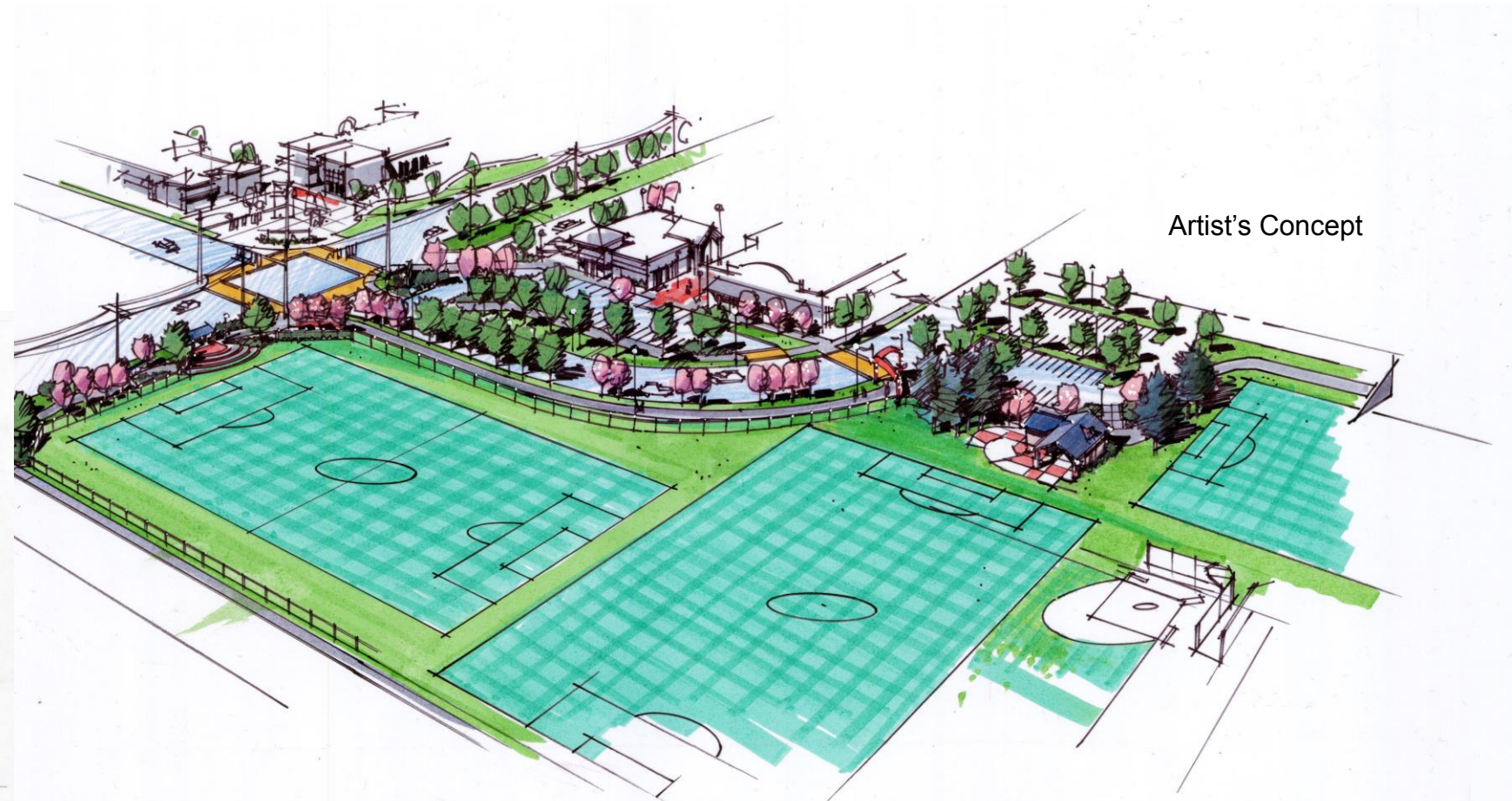


## City Parks Board Collaboration Southside Sports Complex



Artist's Concept

Key to the success of the KXLY development is access to the fourth-leg of the intersection at Regal and Palouse as contemplated in the ISP. The access is routed across City Park property. The effect of the access conflicts with existing park improvements including the Park's primary parking lot, two play fields and restroom kiosk.



Artist's Concept

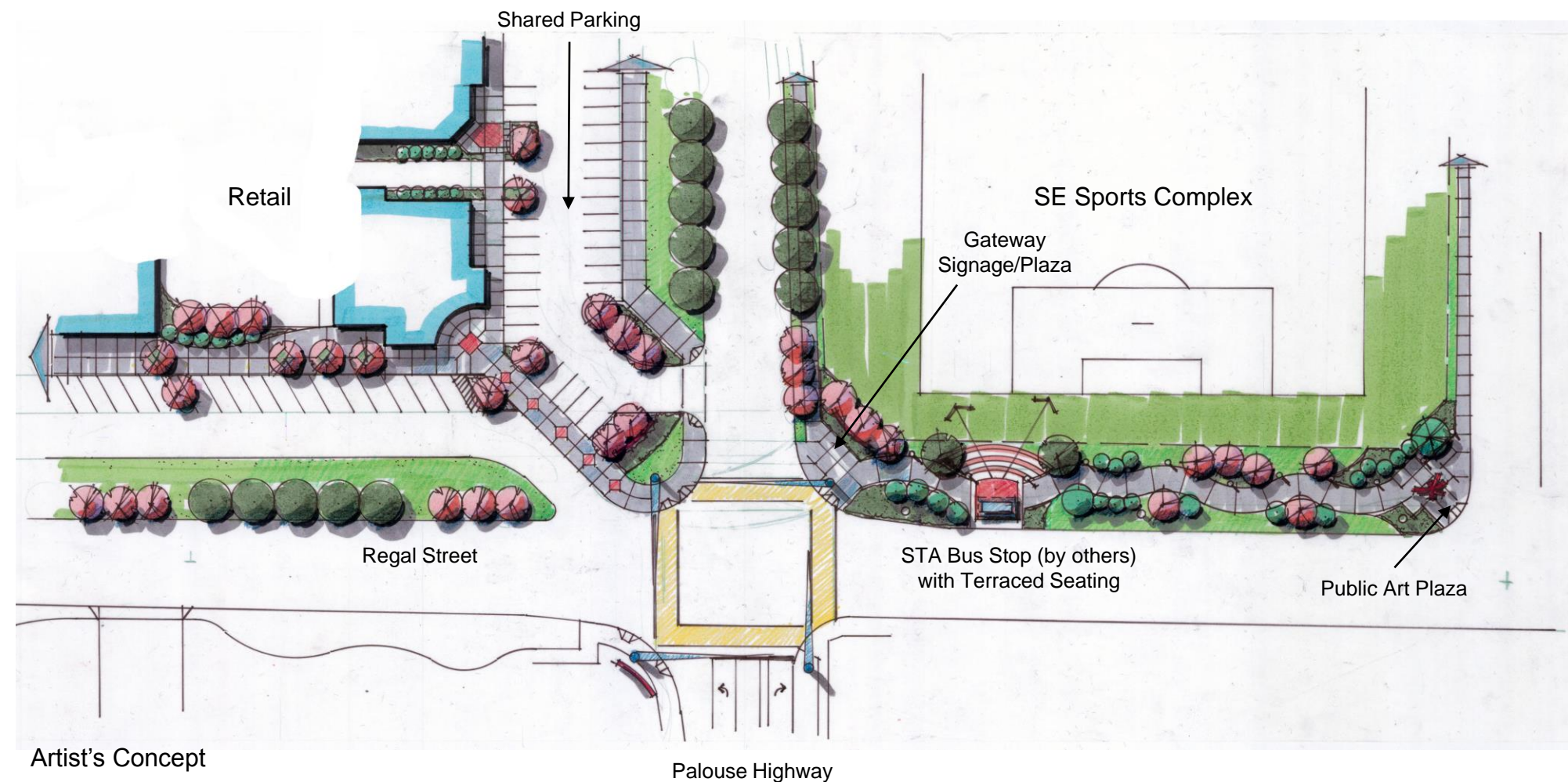


## City Parks Board Collaboration

To mitigate the impacts to the park and public facilities, the City Parks Department and KXLY have entered into agreement to secure an access easement to the intersection through a development license agreement. In exchange for rights of access, KXLY, at their sole expense, will make the City and Public whole by:

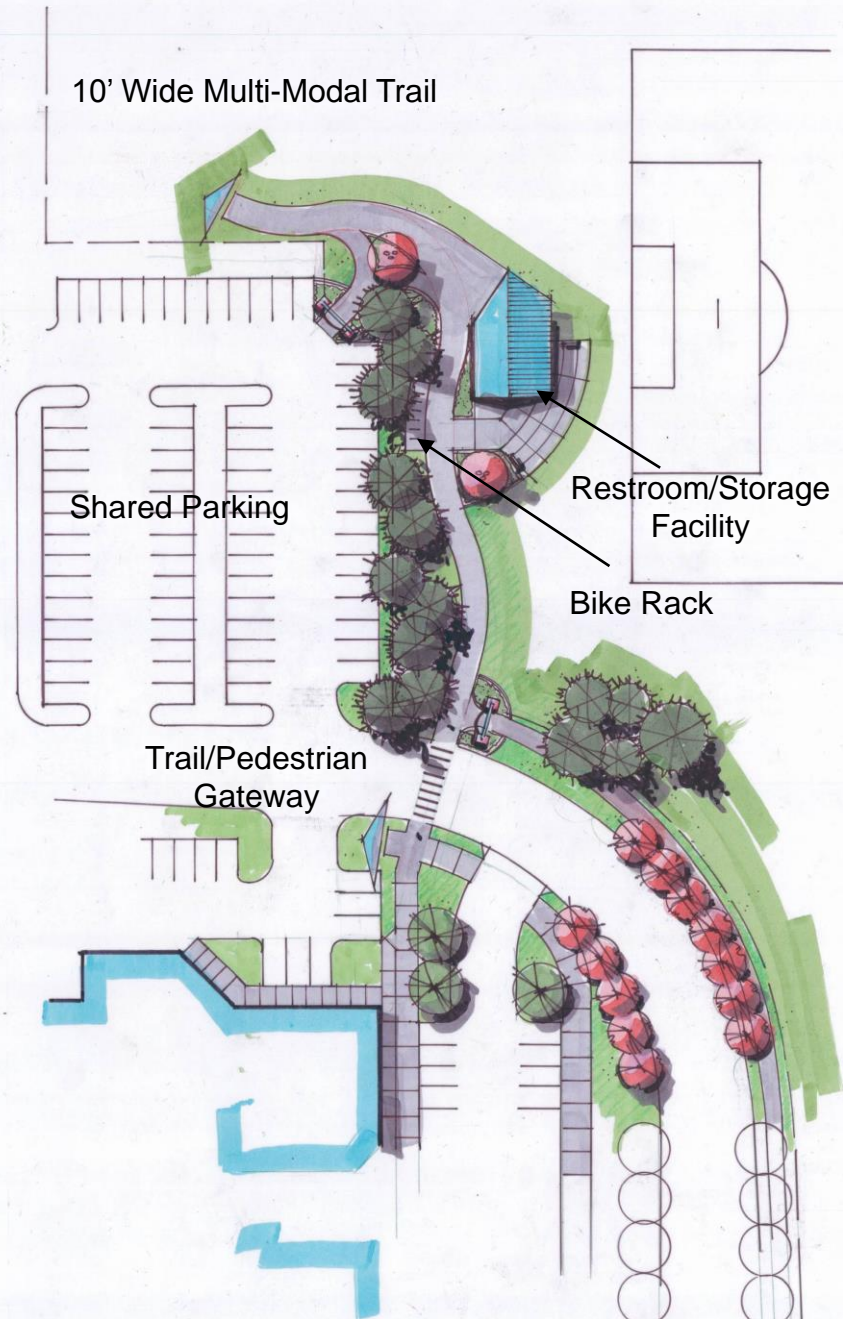
- Reconfiguring the impacted soccer field in the NEC of the park (46<sup>th</sup> & Regal)
- Constructing a Ribbon Park along Parks Department property fronting Regal St.
- Constructing designated non-exclusive supporting parking for the Park including accessible parking spaces and barrier-free access from designated parking lots to the Park.
- Constructing a new replacement regulation-size turf soccer field on KXLY property directly adjacent to the park.
- Replacing existing restroom and storage kiosk.

In addition to constructing the improvements, KXLY will maintain the parking lots, ribbon park and other improvements within the described easement area as set forth in the development license agreement. This allows the City to retain the same number of public recreational facilities while relieving the City Parks Department of the burdensome maintenance of parking lots, sidewalks and other non-recreational related facilities. The proposed development license agreement easement has been approved by the Park Board and final details are being worked out with the KXLY design team and Parks Department representatives.

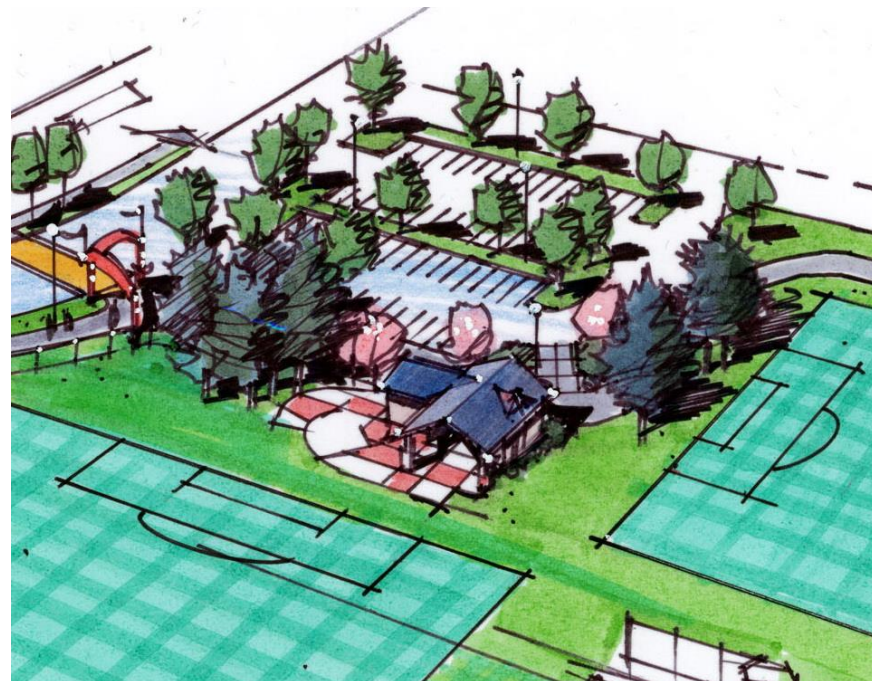
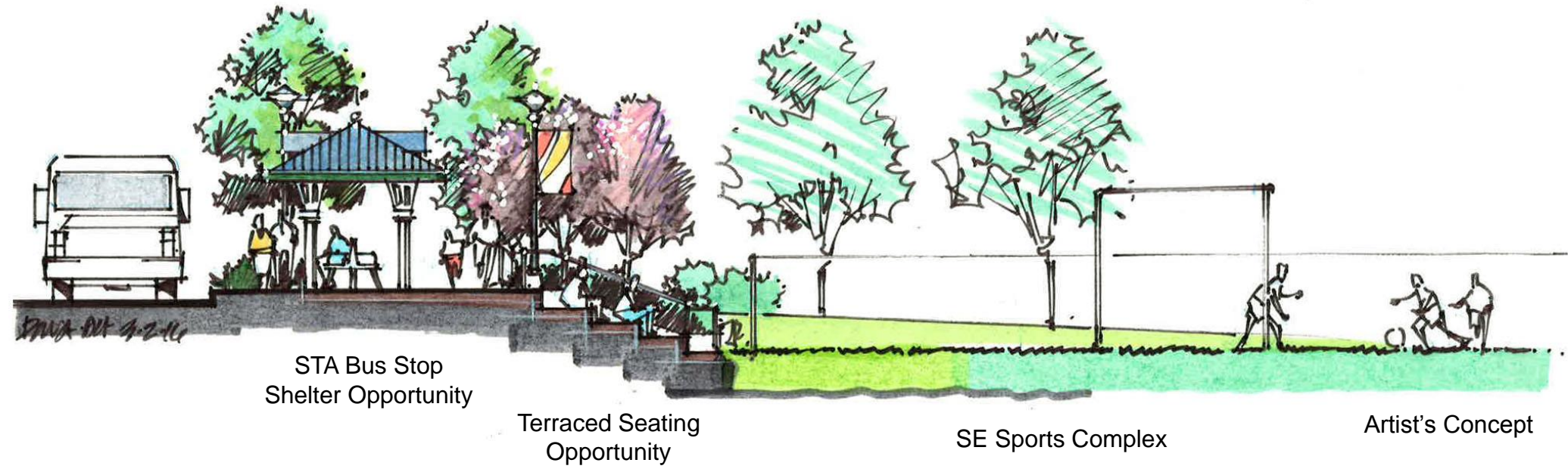




## City Parks Board Collaboration



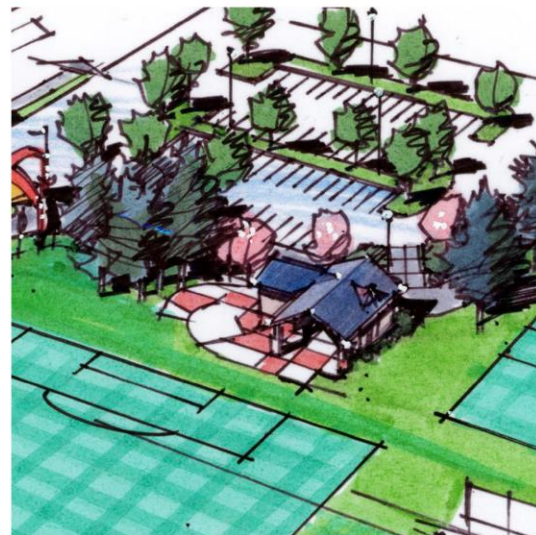
Artist's Concept



Artist's Concept



# Site Plan



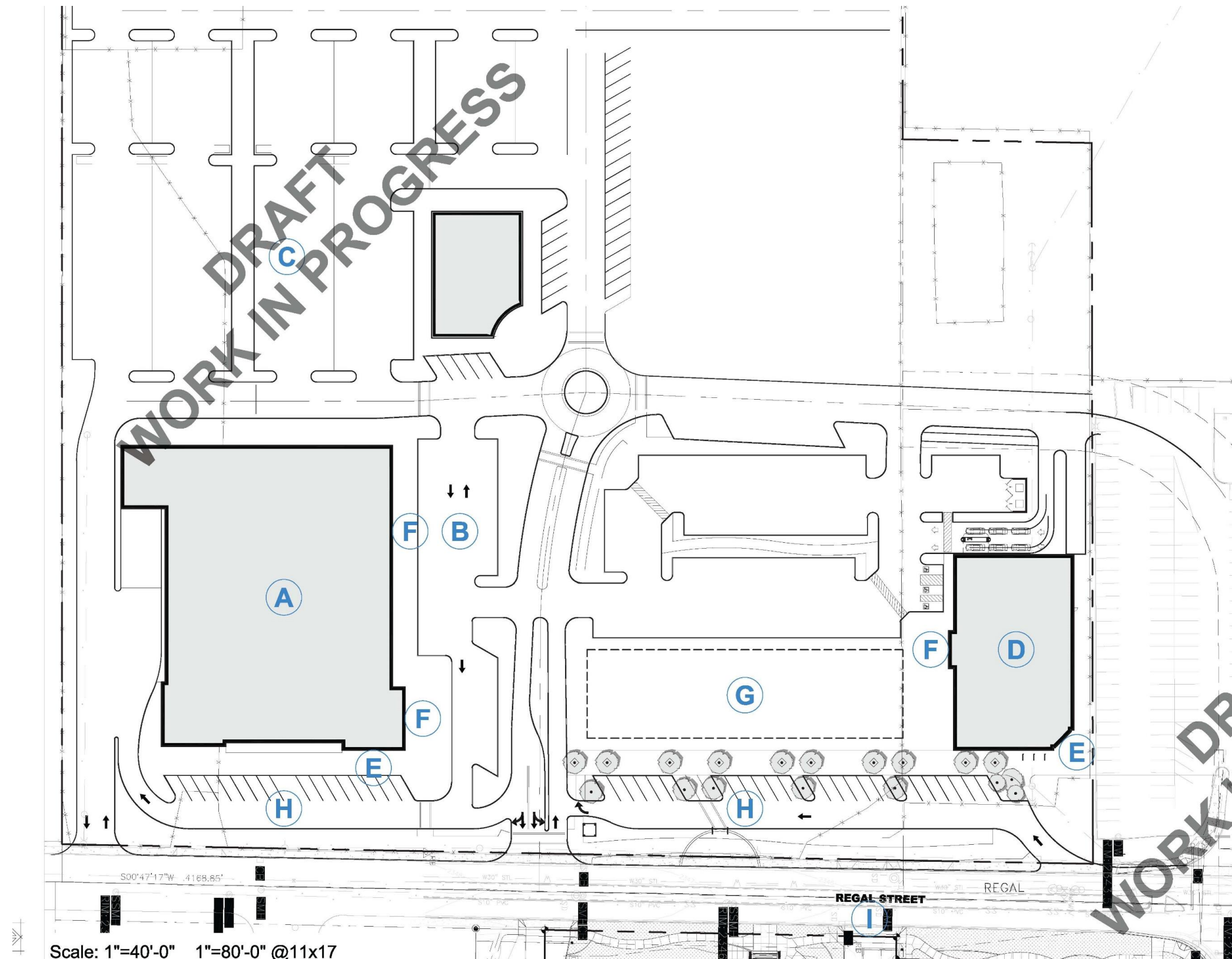


## Site Evolution: 01



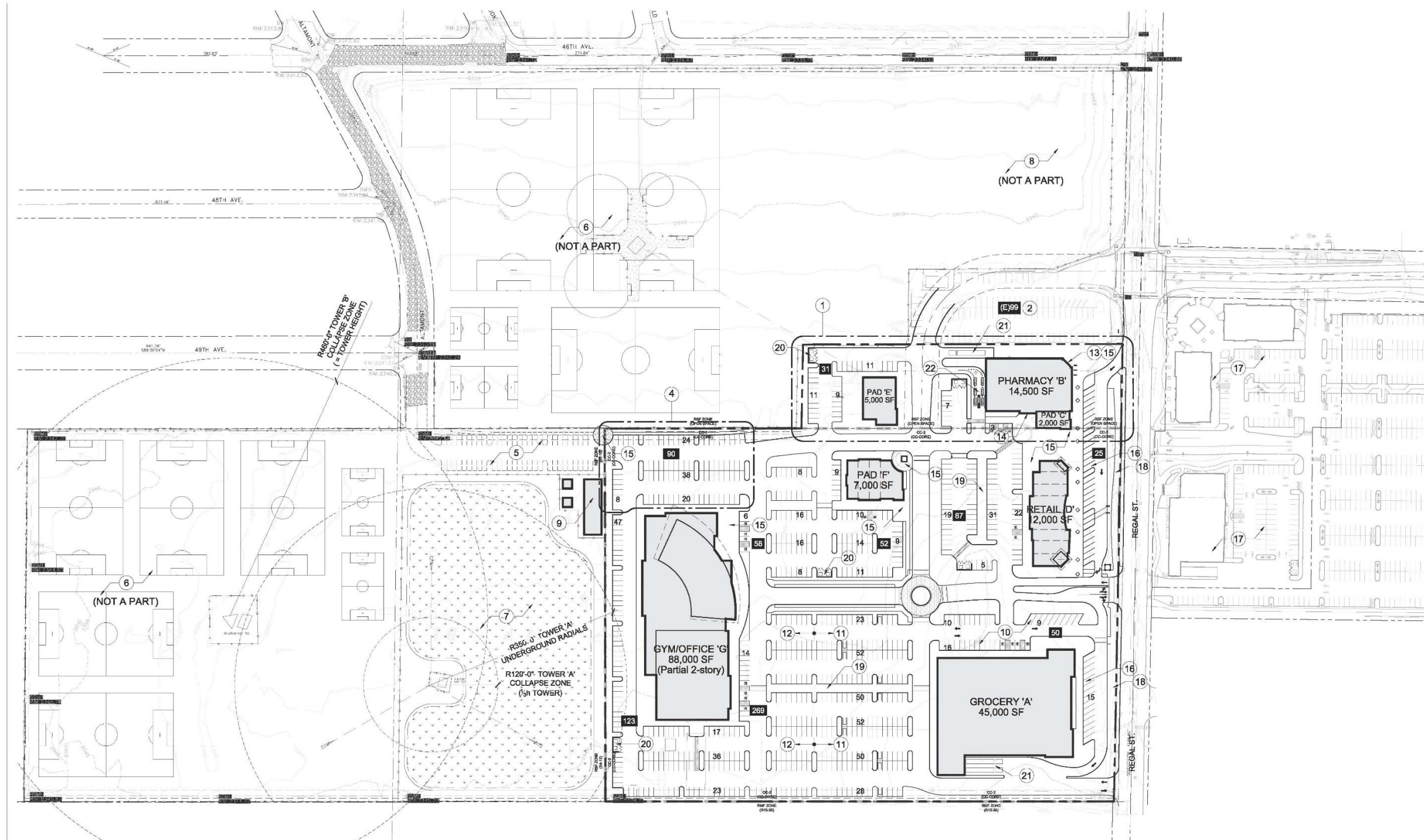


## Site Evolution: 02





## Site Evolution: 03



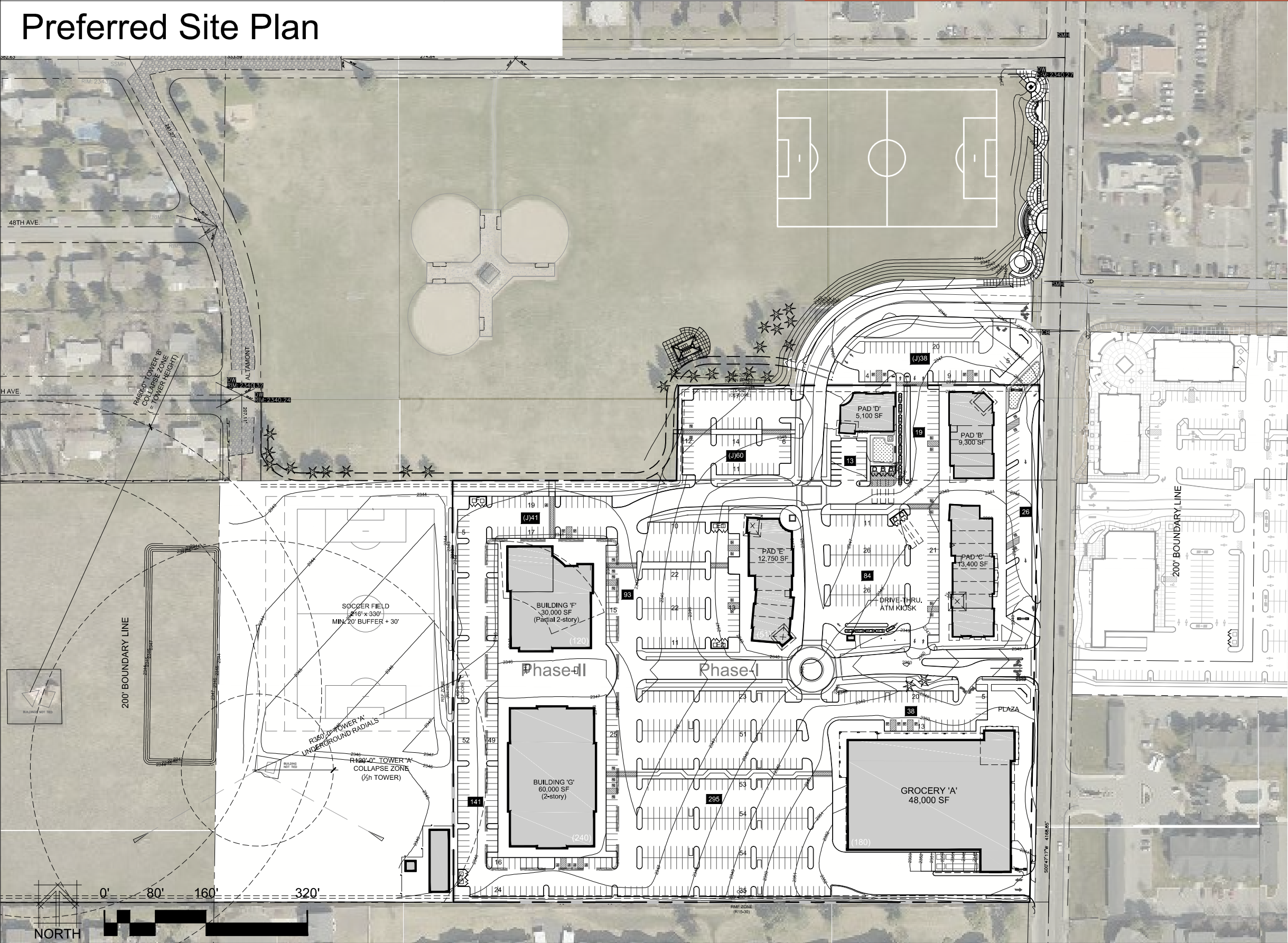


## Site Evolution: Early Massing





# Preferred Site Plan



### A. SUBJECT PROPERTY(S) DATA

PARCEL ADDRESS	PARCEL NUMBER	ZONING & LAND USE	PARCEL AREA
5222 S. REGAL ST.	34041.9077	CC-2 DC (CC-1) CC CORE	609,840 SF
2651 E. 49th AVENUE	34041.0038	RSF OPEN SPACE	85,378 SF
ASSEMBLED PROPERTY AREA (ASSESSED):			695,218 SF
			15.96 ACRES

### B. Required BLDG Setbacks & Landscaping

YARD / SITE LOCATION	REQ'D MIN. BUILDING SETBACK	SETBACK ADJACENT TO R-ZONE	REQ'D MIN. LANDSCAPING
FRONT	0-FEET	10-FEET	5-FEET
SIDES	0-FEET	10-FEET	5-FEET
REAR	0-FEET	10-FEET	5-FEET
STREETS	12-FEET BACK OF CURB	12-FEET BACK OF CURB	6-FEET
USE BUFFER	-	-	6-FEET + FENCE
INTERIOR	-	-	10% PARKING AREA

### C. BUILDING(S) AREA & REQUIRED PARKING

BUILDING INFORMATION		REQUIRED PARKING	
BLDG NAME	BLDG AREA	MINIMUM (1/1000 SF)	MAXIMUM (1/250 SF)
GROCERY 'A'	48,000 SF	48.0 STALLS	192.0 STALLS
PAD 'B'	9,300 SF	9.3 STALLS	37.2 STALLS
PAD-C	13,400 SF	13.4 STALLS	53.6 STALLS
PAD-'D'	5,100 SF	5.1 STALLS	20.4 STALLS
PAD-'E'	12,750 SF	12.8 STALLS	51.0 STALLS
BUILDING -'F' (PARTIAL 2-STORY)	30,000 SF	30.0 STALLS	120.0 STALLS
BUILDING - 'G' (2-STORY)	60,000 SF	60.0 STALLS	240.0 STALLS
TOTALS:	178,550 SF	179 Stalls	714 Stalls
PROVIDED PARKING:		849 Stalls	
LESS 139 SOCCER COMPLEX PARKING STALLS:		710 Stalls	
DEVELOPMENT PARKING RATIO:		3.98 STALLS / 1,000 SF	
REQUIRED ADA PARKING (5%):		42 ADA Stalls	
PARKING LOT TREES (1 INTERIOR TREE PER 6 STALLS PER 'SMC' 17C.200.040(F)a):		142 TREES	

### D. PROJECT DATA

DATA	EXISTING	PROPOSED
OCCUPANCY GROUP	-	M, B, A2 & A3
CONSTRUCTION TYPE	-	TYPE-II & V-N
MAX. BLDG HEIGHT	-	55-FEET
MAX. FLOOR AREA RATIO (F.A.R.)	-	0.5
BLDG(S) SPRINKLED	-	YES
SEPA REQUIRED	-	YES
ESTIMATED PROJECT VALUE	-	\$0
ESTIMATED START DATE	-	TBD



# Concept Landscape Plan



1 SCHEMATIC LANDSCAPE PLAN

SCALE: 1" = 80'-0"

LANDSCAPE NOTES

1. CONTRACTOR SHALL REPORT TO LANDSCAPE ARCHITECT ALL CONDITIONS WHICH IMPAIR AND/OR PREVENT THE PROPER EXECUTION OF THIS WORK, PRIOR TO BEGINNING WORK.

2. COORDINATE ALL WORK WITH ALL OTHER SITE RELATED DEVELOPMENT DRAWINGS.

3. COORDINATE WORK SCHEDULE AND OBSERVATIONS WITH LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION START-UP.

4. ALL PLANT MATERIAL SHALL BE INSTALLED AS PER DETAILS.

5. ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN NURSERYMAN STANDARDS FOR TYPE AND SIZE SHOWN. PLANTS WILL BE REJECTED IF NOT IN A SOUND AND HEALTHY CONDITION.

6. IN THE EVENT OF A PLANT COUNT DISCREPANCY, PLANT SYMBOLS SHALL OVERRIDE SCHEDULE QUANTITIES AND CALL OUT SYMBOL NUMBERS.

7. ALL PLANTING BEDS SHALL BE COVERED WITH A MINIMUM OF 3" DEPTH OF SMALL (1" MINUS) TAN PERMA-BARK MULCH. SUBMIT SAMPLE FOR APPROVAL. RE-MULCH ANY AREAS THAT ARE NOT 3" IN DEPTH AT THE END OF THE WARRANTY PERIOD AT NO COST TO THE OWNER.

8. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS BEGINNING AT THE DATE OF ACCEPTANCE BY THE OWNER. REPLACE ALL PLANT MATERIAL FOUND DEAD OR NOT IN A HEALTHY CONDITION IMMEDIATELY WITH THE SAME SIZE AND SPECIES AT NO COST TO THE OWNER.

9. FINISH GRADES SHALL PROVIDE A SMOOTH TRANSITION WITH ADJACENT SURFACES AND ENSURE POSITIVE DRAINAGE IN ACCORDANCE WITH THE SITE GRADING PLAN.
10. FERTILIZE ALL TREES AND SHRUBS WITH 'AGRIFORM' PLANTING TABLETS. QUANTITY PER MANUFACTURERS RECOMMENDATIONS.

11. ALL PLANTING BEDS SHALL HAVE A MINIMUM 18" DEPTH OF TOPSOIL. LAWN AREAS SHALL HAVE A MINIMUM 12" DEPTH OF TOPSOIL. SPREAD, COMPACT, AND FINE GRADE TOPSOIL TO A SMOOTH AND UNIFORM GRADE 3" BELOW ADJACENT SURFACES OF PLANTER BED AREAS, 1-1/2" BELOW ADJACENT SURFACES OF TURF SOD AREAS, AND 1" BELOW ADJACENT SURFACES OF TURF SEED AREAS.

12. IF IMPORTED TOPSOIL FROM OFF-SITE SOURCES IS REQUIRED, ENSURE IT IS FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH, WEEDS AND OTHER LITTER, AND FREE OF ROOTS, STUMPS, STONES LARGER THAN 2 INCHES IN ANY DIMENSION, AND OTHER EXTRANEOUS OR TOXIC MATTER HARMFUL TO PLANT GROWTH. A) OBTAIN TOPSOIL FROM LOCAL SOURCES OR FROM AREAS HAVING SIMILAR SOIL CHARACTERISTICS TO THOSE FOUND ON THE PROJECT SITE. B) OBTAIN TOPSOIL ONLY FROM NATURALLY, WELL-DRAINED SITES WHERE TOPSOIL OCCURS AT A DEPTH OF NOT LESS THAN 4 INCHES.

13. IMMEDIATELY CLEAN UP ANY TOPSOIL OR OTHER DEBRIS ON THE SITE CREATED FROM LANDSCAPE OPERATIONS AND DISPOSE OF PROPERLY OFF SITE.

14. TREES SHALL NOT BE PLANTED WITHIN THE 10'-0" CLEAR ZONE OF ALL STORM DRAIN PIPE, STRUCTURES, OR FACILITIES.

15. SEEPAGE BEDS AND OTHER STORM DRAINAGE FACILITIES MUST BE PROTECTED FROM ANY AND ALL CONTAMINATION DURING THE CONSTRUCTION AND INSTALLATION OF THE LANDSCAPE IRRIGATION SYSTEM.

16. IN THE EVENT OF A DISCREPANCY, NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY.

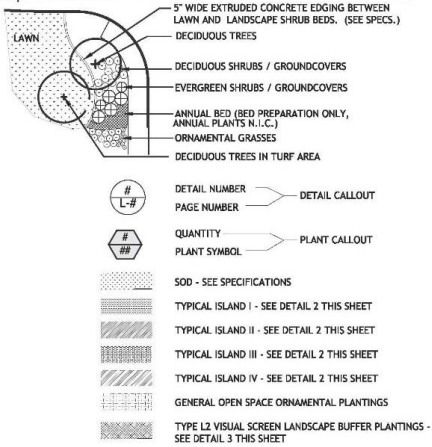
PLANT SCHEDULE - TREES

KEY	BOTANICAL NAME	COMMON NAME	SIZE	TYPE	NOTES	USDA ZONE
LARGE DECIDUOUS SHADE TREES						
AP	ACER PLATANOIDES 'DEBORAH'	DEBORAH NORWAY MAPLE	2" CAL.	B&B	45' X 40' W	ZONE 3
CJ	CERCIDIPHYLLUM JAPONICUM	KATSURA TREE	2" CAL.	B&B	50H X 30W	ZONE 4
TC	TILIA CORDATA 'CHANCOLE'	CHANCELLOR LITTLELEAF LINDEN	2" CAL.	B&B	35H X 20W	ZONE 3
SMALL DECIDUOUS AND FLOWERING TREES						
PC	PRUNUS SARGENTII 'COLUMNARIS'	COLUMNAR SARGENT CHERRY	2" CAL.	B&B	30H X 15W	ZONE 4
PU	PYRUS USSURIENSIS 'MORDAK'	PRAIRIE GEM FLOWERING PEAR	2" CAL.	B&B	25H X 20W	ZONE 3
QR	QUERCUS ROBUR X Q. ALBA	CRIMSON SPIRE OAK	2" CAL.	B&B	40H X 15W	ZONE 4
ZS	ZELKOVA SERRATA 'JFS-KW1'	CITY SPRITE ZELKOVA	2" CAL.	B&B	24H X 18W	ZONE 5
EVERGREEN TREES						
PS	PINUS SYLVESTRIS	SCOTCH PINE	6' TALL	B&B	40H X 30W	ZONE 3
PV	PINUS FLEXILIS 'VANDERWOLFS'	VANDERWOLF'S LIMBER PINE	6' TALL	B&B	25H X 15W	ZONE 4

PLANT SCHEDULE - PERENNIALS AND GRASSES

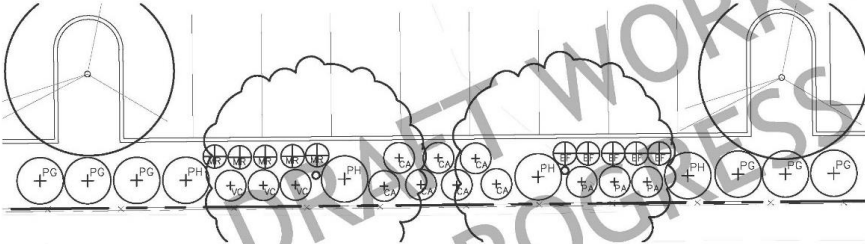
KEY	BOTANICAL NAME	COMMON NAME	SIZE
CA	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	1 GAL.
CK	CORNUS SERICEA 'KELSEY'	KELSEY RED OSIER DOGWOOD	3 GAL.
EF	EUONYMUS FORTUNEI 'EMERALD GALETY'	EMERALD GALETY WINTERCREEPER	3 GAL.
LB	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	LITTLE BUNNY FOUNTAIN GRASS	1 GAL.
MR	MAHONIA REPENS	CREEPING MAHONIA	3 GAL.
PA	POTENTILLA FRUTICOSA 'ABBOTSWOOD'	ABBOTSWOOD POTENTILLA	3 GAL.
PB	POTENTILLA FRUTICOSA 'PINK BEAUTY'	PINK BEAUTY POTENTILLA	3 GAL.
PH	PHYSCARPUS OPULEFOLIUS 'SEAWARD'	SUMMER WINE NINEBARK	5 GAL.
PG	PINUS SYLVESTRIS 'GLAUKA NANA'	DWARF SCOTCH PINE	5 GAL.
VC	VIBURNUM CARLESII	KOREAN SPICE VIBURNUM	3 GAL.

LANDSCAPE LEGEND



2 TYPICAL LANDSCAPE ISLANDS

SCALE: 1" = 10'-0"



3 TYPICAL L2 VISUAL SCREEN BUFFER

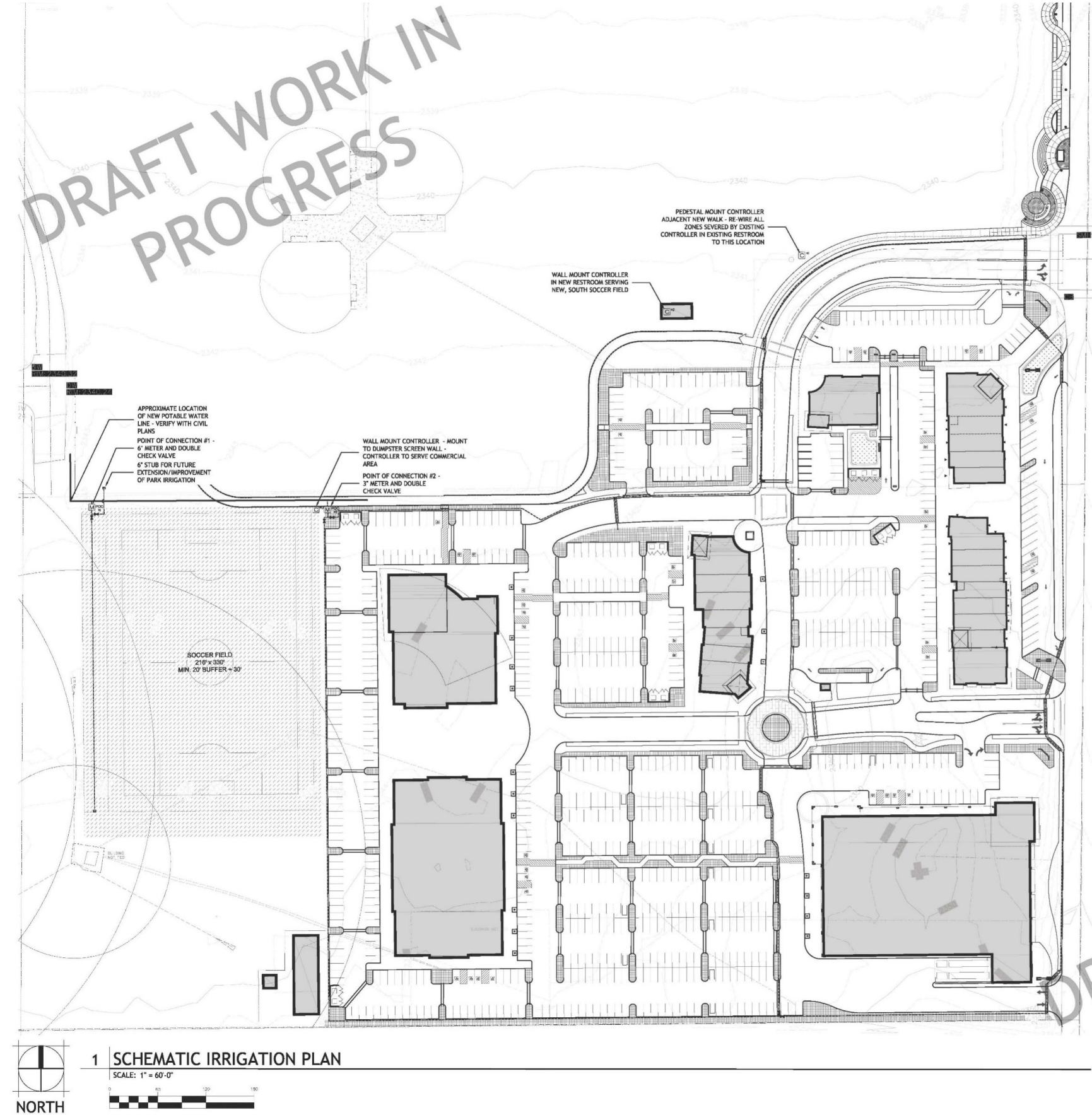
SCALE: 1" = 10'-0"

UTILITY STATEMENT  
LOCATION OF EXISTING UNDERGROUND UTILITIES  
HAVE BEEN TAKEN FROM DRAWINGS AND FIELD  
LOCATES SUPPLIED BY THE APPROPRIATE  
UTILITY COMPANIES. UTILITY LOCATIONS  
SHOWN ON THIS DRAWING ARE APPROXIMATE  
ONLY. PRIOR TO BEGINNING ANY  
CONSTRUCTION, THE CONTRACTOR SHALL  
VERIFY THE EXACT LOCATION OF EACH UTILITY.

UNDERGROUND SERVICE ALERT  
ONE-CALL NUMBER  
811  
CALL TWO BUSINESS DAYS  
BEFORE YOU DIG



# Concept Irrigation Plan



IRRIGATION LEGEND				
IRRIGATION TYPE				
	DRIP LINE AREA AREA - SHRUB BED AREAS: TO BE LAID OUT ACCORDING TO MANUFACTURER'S RECOMMENDATIONS 'RAIN BIRD' XFS SUB-SURFACE DRIPLINE WITH COPPER SHIELD, XFS-06-18 (18" LINE SPACING AND 18" EMITTER SPACING) (0.6 GPH EMITTERS)			
	TURF SPRAY AREA - NARROW TURF AREAS: SHORT RADIUS RAINBIRD (NPR OR VAN SERIES) SPRAYS - TO BE LAID OUT ACCORDING TO HEAD LAYOUT SHOWN ON IRRIGATION PLANS (TBD).			
	ROTOR AREA - SPORTS FIELD TURF AREAS: LARGE RADIUS HUNTER ROTORS - TO BE LAID OUT ACCORDING TO HEAD LAYOUT SHOWN ON IRRIGATION PLANS (TBD).			
EQUIPMENT				
SYMBOL	MFG	DESCRIPTION	MODEL NO.	DETAIL NO.
	P.O.C. #1	POINT OF CONNECTION TO POTABLE WATER LINE - 6' METER		
	P.O.C. #2	POINT OF CONNECTION TO POTABLE WATER LINE - 3' METER		
	RAINBIRD	CONTROLLER #1	ESP-LXME/F	
	RAINBIRD	CONTROLLER #2	ESP-LXME/F	
	RAINBIRD	CONTROLLER #3	ESP-LXME/F	
	RAINBIRD	RAIN SENSOR - WIRELESS	WR2-RFC	
	RAINBIRD	CONTROL VALVE	PEB	
	RAINBIRD	MODULAR PRESSURE REGULATOR	PRS-DIAL	
	WATTS	ISOLATION VALVE	WGV-X	
	RAINBIRD	(SAME SIZE AS MAINLINE) QUICK COUPLER	44NP	
PHASE I - 3" SCH. 40 PVC MAINLINE, UNLESS OTHERWISE NOTED				
PHASE II - 3" SCH. 40 PVC MAINLINE, UNLESS OTHERWISE NOTED				
6" SCH. 40 PVC SLEEVING - UNLESS NOTED IN SPECS.				
*THE NUMBER IN FRONT OF THE SV INDICATES THE QUANTITY OF SLEEVES IN THAT AREA				

- IRRIGATION NOTES**
- ALL WORK SHALL BE PERFORMED BY PERSONS FAMILIAR WITH THIS TYPE OF WORK AND UNDER THE SUPERVISION OF A QUALIFIED FOREMAN. THE FOREMAN SHALL REMAIN ON-SITE DURING ALL LANDSCAPE SOIL PREPARATION, IRRIGATION AND PLANTING INSTALLATIONS.
  - CONTRACTOR SHALL COMPLY WITH ALL LOCAL CODES AND ORDINANCES.
  - CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO INITIATION OF ANY DEMOLITION OR CONSTRUCTION OPERATIONS. ANY DAMAGE TO EXISTING UTILITIES ON SITE OR ADJACENT PROPERTY SHALL BE CONTRACTORS RESPONSIBILITY TO REPAIR.
  - COORDINATE ALL IRRIGATION INSTALLATION OPERATIONS WITH CIVIL, AND ELECTRICAL ENGINEERING SHEETS.
  - IN THE EVENT OF A DISCREPANCY, IMMEDIATELY NOTIFY THE LANDSCAPE ARCHITECT.
  - CONTRACTOR SHALL REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
  - CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO NEW OR EXISTING CONCRETE FLATWORK, ASPHALT, OR LANDSCAPE AND TURF AREAS AS A RESULT OF CONSTRUCTION ACTIVITIES.
  - CONTRACTOR SHALL BE RESPONSIBLE FOR INITIAL ANNUAL IRRIGATION SCHEDULE AND SHALL AFFIX THE SCHEDULE INSIDE THE CONTROLLER CABINET DOOR.
  - LOCATIONS OF EQUIPMENT SHALL BE APPROVED IN THE FIELD WITH OWNER AND LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
  - CONTRACTOR SHALL SUPPLY AS-BUILT DRAWINGS TO THE OWNER PRIOR TO FINAL ACCEPTANCE. AS BUILTS SHALL BE IN BOTH PAPER FORM AND AUTOCAD DRAWING FORMAT SUBMITTED ON A CD.
  - CONTRACTOR SHALL FURNISH ALL LABOR, TOOLS, EQUIPMENT AND HARDWARE NECESSARY FOR PRODUCTION, EXECUTION AND COMPLETION OF ALL IRRIGATION WORK.
  - CONTRACTOR TO OBTAIN AND PAY FOR ALL PERMITS AND FEES REQUIRED FOR THIS WORK.
  - CONTRACTOR SHALL VERIFY WORKING PSI AT MAXIMUM OPERATING FLOW PRIOR TO CONSTRUCTION OF SYSTEM AND NOTIFY OWNERS REPRESENTATIVE OF ANY FINDINGS. (+/- 77 HIGH, 50 LOW PSI REPORTED)
  - CONTRACTOR SHALL PERFORM WATER COVERAGE TEST PRIOR TO SEEDING/SODDING TURF. IF NECESSARY ADJUST IRRIGATION SO 100% COVERAGE IS MET.
  - ALL DISTURBED AREAS SHALL BE SODDED AND RECEIVE TEMPORARILY IRRIGATION THROUGHOUT THE WARRANTY PERIOD UNLESS OTHERWISE INDICATED.
  - ALL EXISTING GRASS REMOVED FOR TRENCHING SHALL BE SAVED FOR REPLACEMENT. IF, AT THE DISCRETION OF THE OWNERS REPRESENTATIVE, THE SAVED GRASS DOES NOT SEEM ALIVE THEN NEW SOD SHALL BE INSTALLED IN ITS PLACE AT NO ADDITIONAL COST TO THE OWNER.
  - CONTRACTOR IS RESPONSIBLE FOR KEEPING EXISTING LAWNS AND LANDSCAPE ALIVE AND THRIVING DURING ALL PHASES OF CONSTRUCTION. ANY LANDSCAPE AND TURF GRASS AREAS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO OWNERS SATISFACTION AT NO ADDITIONAL COST.
  - CONTRACTOR SHALL PATCH AND REPAIR EXISTING IRRIGATION SYSTEM AS REQUIRED FROM DAMAGE CAUSED BY CONSTRUCTION ACTIVITIES.
  - USE KING BROTHERS (KB) VALVE OR IN HEAD DRAIN CHECK VALVES TO PREVENT LOW HEAD DRAINAGE WHERE IT OCCURS.
  - CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL ROCK AND DEBRIS BROUGHT TO THE SURFACE AS A RESULT OF TRENCHING OPERATIONS.
  - IRRIGATION CONTROL WIRE SHALL BE 12 GAUGE INSULATED DIRECT BURIAL SINGLE CONDUCTOR PE IRRIGATION WIRE. 2 SPARE WIRES SHALL BE INSTALLED THROUGHOUT THE ENTIRE IRRIGATION SYSTEM AND BE OF A UNIQUE COLOR (ORANGE).
  - ALL 24V. WIRING UNDER PATHWAYS, PAVING AND ROADWAYS SHALL BE IN A SEPARATE 2" SLEEVE, UNLESS OTHERWISE NOTED ON PLAN, AT DEPTH & TYPE PER SPECIFICATIONS.
  - ALL WIRE SPLICES TO BE DONE WITH SPECIFIED DOR-6 WIRE SPLICE SNAP CONNECTOR AND SEALER OR APPROVED EQUAL.
  - CONTROL WIRES NOT FOLLOWING MAINLINE TRENCH SHALL BE IN SCH. 40 PVC CONDUIT. SIZE TO FIT.
  - TERMINATION OF UNUSED CONTROL WIRE SHALL BE COVERED WITH A WIRE NUT AND PLACED WITHIN SPECIFIED DOR-6 WIRE SPLICE SNAP CONNECTOR AND SEALER, AND PLACED IN A CONTROL VALVE BOX.
  - ALL TRENCHING REQUIRED WITHIN PAVED AREAS SHALL BE BACKFILLED IN 4" LIFTS AND COMPACTED TO 95% RELATIVE COMPACTION PER ASTM-D1557. JETTING IS NOT ALLOWED. THE MOISTURE CONTENT SHALL RANGE FROM OPTIMUM TO 3% ABOVE OPTIMUM MOISTURE CONTENT AT TIME OF PLACEMENT. ALL COMPACTION SHALL BE COMPLETED WITH SPECIFIC EQUIPMENT THAT CAN COMPACT NARROW TRENCHES. THIS COMPACTION REQUIREMENT SHALL EXTEND INSIDE THE LANDSCAPED AREAS BEHIND BACK OF CURB.
  - ALL PIPE LINES SHALL BE PLACED SO THAT DESCRIPTIONS FACE UP.
  - PLACE NON-POTABLE PURPLE LOCATOR RIBBON 3" ABOVE PROTECTIVE LAYER OF SOIL ON MAINLINE ONLY.
  - CONTRACTOR SHALL LOCATE CONTROL VALVES AND ISOLATION VALVES WITHIN 1' OF SIDEWALKS OR CURB EDGES IN A CLEAN ORDERLY MANNER WHENEVER POSSIBLE.
  - ALL VALVE BOXES (VB) LOCATED ON A SLOPE SHALL BE PLACED SO THAT ALL 4 CORNERS OF THE VALVE BOX ARE FLUSH WITH THE SLOPE.
  - CONTRACTOR SHALL COORDINATE AND VERIFY THE INSTALLATION OF SLEEVES, WITH RESPECTIVE CONTRACTORS, IN ALL NECESSARY LOCATIONS FOR THE PROPOSED IRRIGATION DESIGN, PRIOR TO HARDSCAPE BEING INSTALLED THAT WOULD PREVENT IRRIGATION ACCESS TO ADJACENT LANDSCAPE AREAS. FAILURE TO VERIFY ALL NECESSARY IRRIGATION IS INSTALLED TO LANDSCAPE AREAS PRIOR TO HARDSCAPE BEING INSTALLED SHALL RESULT IN THE CONTRACTOR BEING RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH EXTENDING THE IRRIGATION AND ANY HARDSCAPE/SCULPTURE REMOVAL AND REPAIR/REPLACEMENT.
  - INSTALL THE PRESSURE REGULATOR WHERE INDICATED WITH AN (\*) NEXT TO THE CONTROL VALVE SYMBOL AND SET SO THAT ALL IRRIGATION HEADS ON CONTROL VALVE RECEIVE STATED PSI PER LEGEND.
  - IRRIGATION SHALL BE ADJUSTED SO THAT NO OVERSPRAY HITS THE BUILDING, AND MINIMUM TO NO OVERSPRAY HITS SIDEWALKS, AND PAVEMENT.
  - POWER SHALL BE SUPPLIED TO CONTROLLER LOCATION BY GENERAL CONTRACTOR. LANDSCAPE CONTRACTOR SHALL INFORM GENERAL OF THE NEED FOR 110V POWER AT THE REQUIRED LOCATION.
  - NO CONTROL WIRES TO BE PLACED IN THRUST BLOCKS. LOCATE WIRES ON OPPOSITE SIDE OF THRUST BLOCK. CONTROL WIRES NOT FOLLOWING MAINLINE TRENCH SHALL BE IN CONDUIT.
  - RAIN SENSOR SHALL BE LOCATED IN THE BEST POSITION TO RECEIVE RAINFALL. SUBMIT LOCATION OF RAIN SENSOR FOR APPROVAL PRIOR TO INSTALLATION.
  - CONTRACTOR SHALL COORDINATE WINTERIZATION OF THE SYSTEM WITH THE MAINTENANCE STAFF. THE CONTRACTOR SHALL INSPECT, ADJUST & MAKE ANY NECESSARY REPAIRS TO THE SYSTEM THE SPRING FOLLOWING INSTALLATION.

UTILITY STATEMENT

LOCATION OF EXISTING UNDERGROUND UTILITIES HAVE BEEN TAKEN FROM DRAWINGS AND FIELD LOCATES SUPPLIED BY THE APPROPRIATE UTILITY COMPANIES. UTILITY LOCATIONS SHOWN ON THIS DRAWING ARE APPROXIMATE ONLY. PRIOR TO BEGINNING ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF EACH UTILITY.

UNDERGROUND SERVICE ALERT

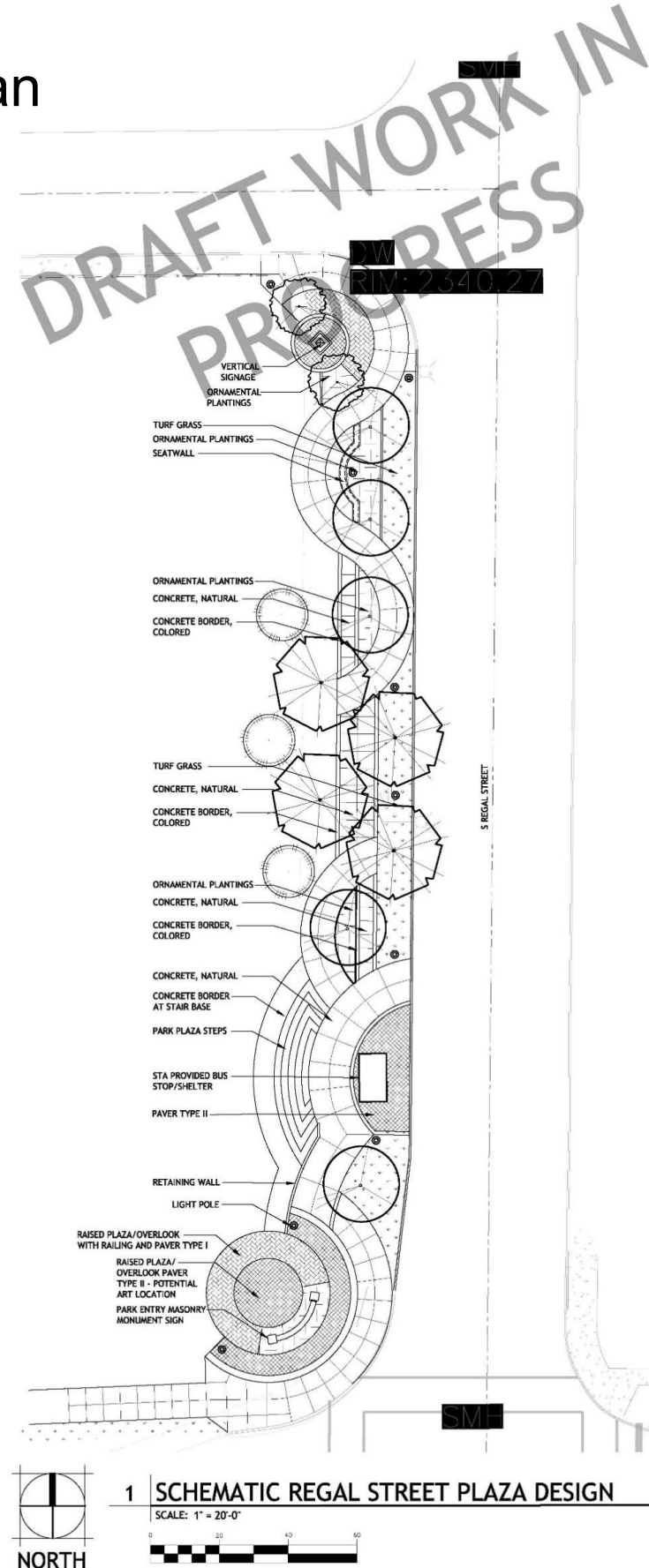
ONE-CALL NUMBER

811

CALL TWO BUSINESS DAYS BEFORE YOU DIG



# Concept 'Ribbon Park' Plan



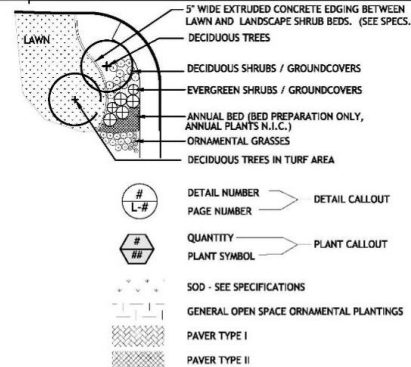
## PLAZA MODEL IMAGES



## PLAZA MATERIAL CHARACTER IMAGES

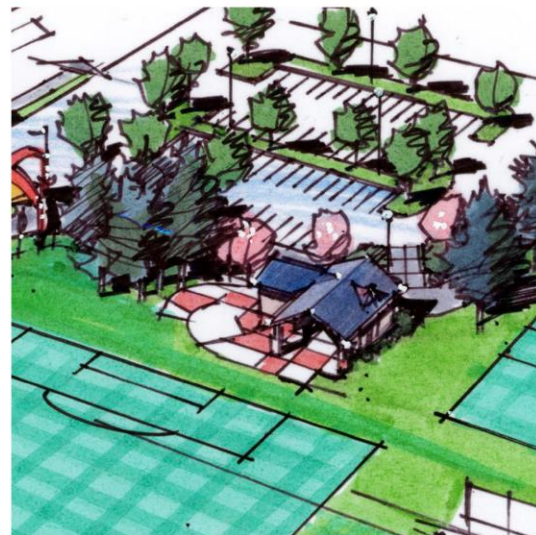


## LANDSCAPE LEGEND





# Design Deviations



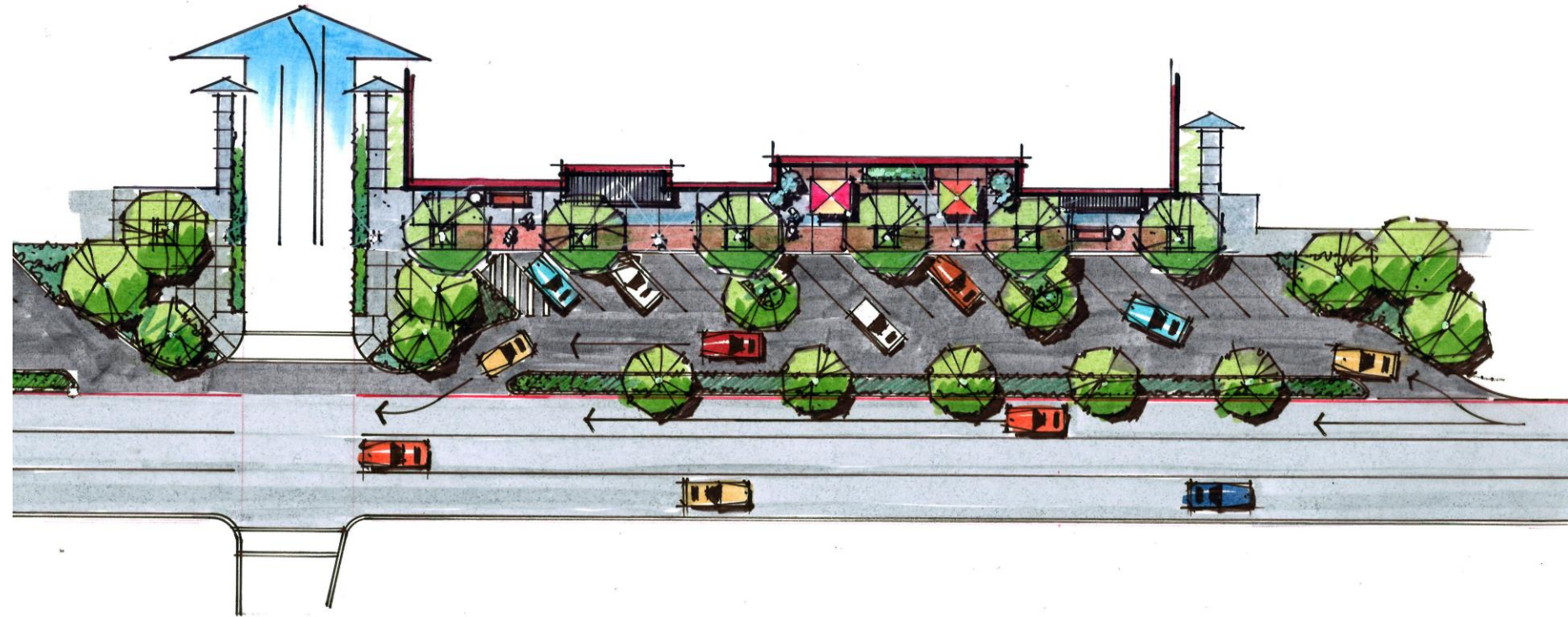


## Design Deviation 1: Simulated Streetscape

The ISP, implementation memo, and Southgate Neighborhood aspire to create a walkable urban streetscape. A key component of the urban streetscape desired by the Neighborhood is on-street parking. The implementation memo obligates the City to provide on-street parking along Regal and Palouse. It is understandable that the idea of street parking would exacerbate an already difficult traffic issue along the south Regal corridor.

In an effort to find a middle ground, the KXLY Development team visited with the Southgate Neighborhood to collaborate on unique solutions to bridge the gap between the need to keep traffic moving on Regal and provide a comfortable walkable urban streetscape. The unanimous solution was to separate the buildings from the street; allowing street traffic to flow unhindered and provide a simulated streetscape with on-street style parking along the storefronts. A +/- 15 to 20-foot wide, treed sidewalk is proposed allowing activities commonly found along urban streetscapes (such as sidewalk cafés) while still allowing pedestrian movements along and through streetscape elements. The KXLY development team refers to this concept as “Airport Parking” as it is similar to metered short-term parking found at airport drop-off and pick-up locations. After the initial collaborative meetings with the Neighborhood, it was presented to multiple City departments and met with a surprising show of support for the concept.

The applicant requests that the DRB recommend to the Director a design deviation allowing the “Airport Parking” concept along Regal Street.



Artist's Concept



Artist's Concept



## Design Deviation 2: Fencing and Screening

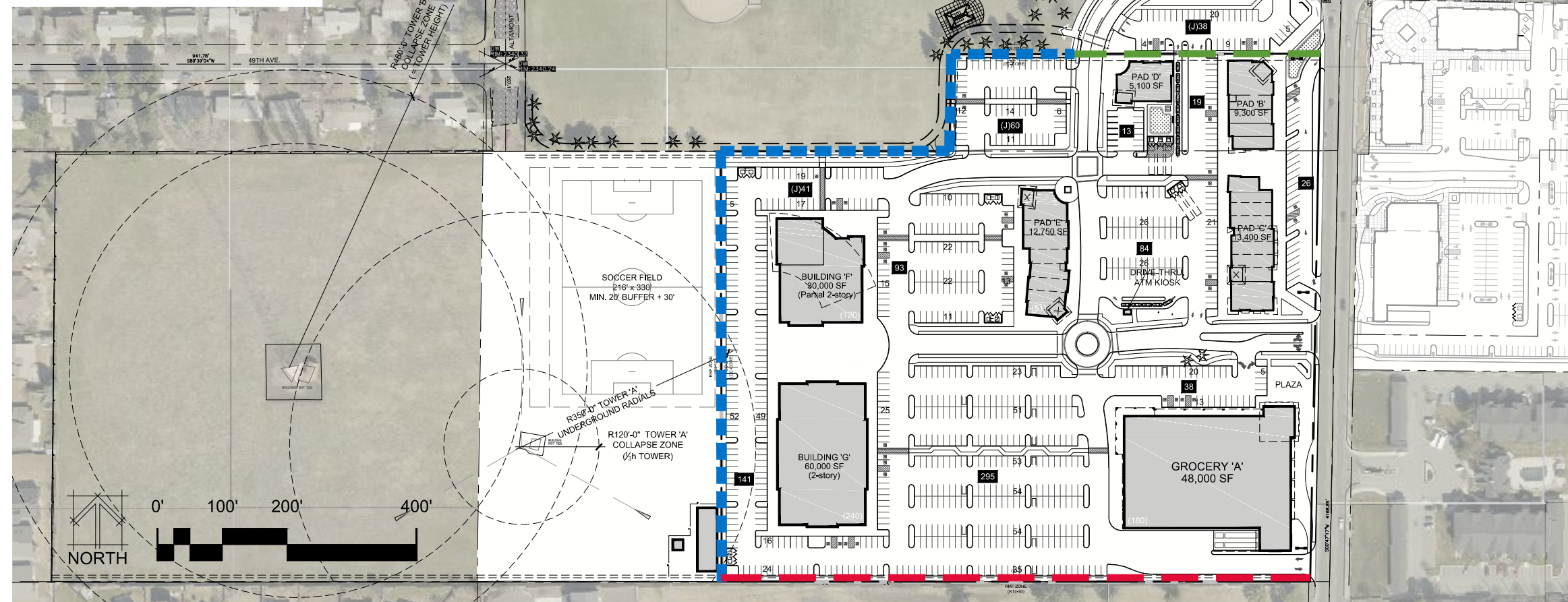
Due to the close interaction of the development with the adjacent park, the applicant requests that the DRB recommend to the Director a design deviation to omit sight-obscuring fencing and buffer landscaping along property lines adjacent the park and park facilities. This design deviation will allow access and visual connection from parking areas designated for the park to the park and other recreational facilities. A 36 to 42-inch high visually open fence is proposed to be installed along the perimeter of park property and facilities with off-set style gates typically found at parks and other sports fields.

The deviation will encourage the free interaction between public recreation and open spaces and the greater Southgate District Center promoting pedestrian and multimodal movements and visually connectivity with area recreational opportunities.

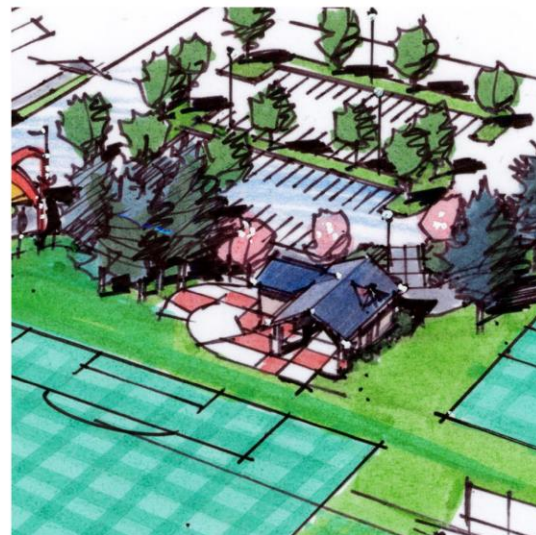
Match existing sports complex, substitute 6-foot site obscuring fencing with 36" to 42" visually open fencing. Utilize Type-III landscaping and/or sod in lieu of Type-I buffer landscaping.

Omit fencing and landscaping at joint use parking areas

Required fencing and zoning buffer landscaping per SMC







# Development Agreement

The ISP sets forth six guiding design principles to achieve a vibrant unified district center. The design principles are enumerated in DA Sections 5.1 through DA 5.6 as follows:

- DA 5.1: Pedestrian Connections
- DA 5.2: Trees Preservation
- DA 5.3: Design Theme
- DA 5.4: Community Plaza
- DA 5.5: Viewscales
- DA 5.6: Long-Term Development of Urban District

The ISP offers guidance as the intent of the guiding design principles. The following pages demonstrate the Proposal's compliance with the DA and ISP guidelines.



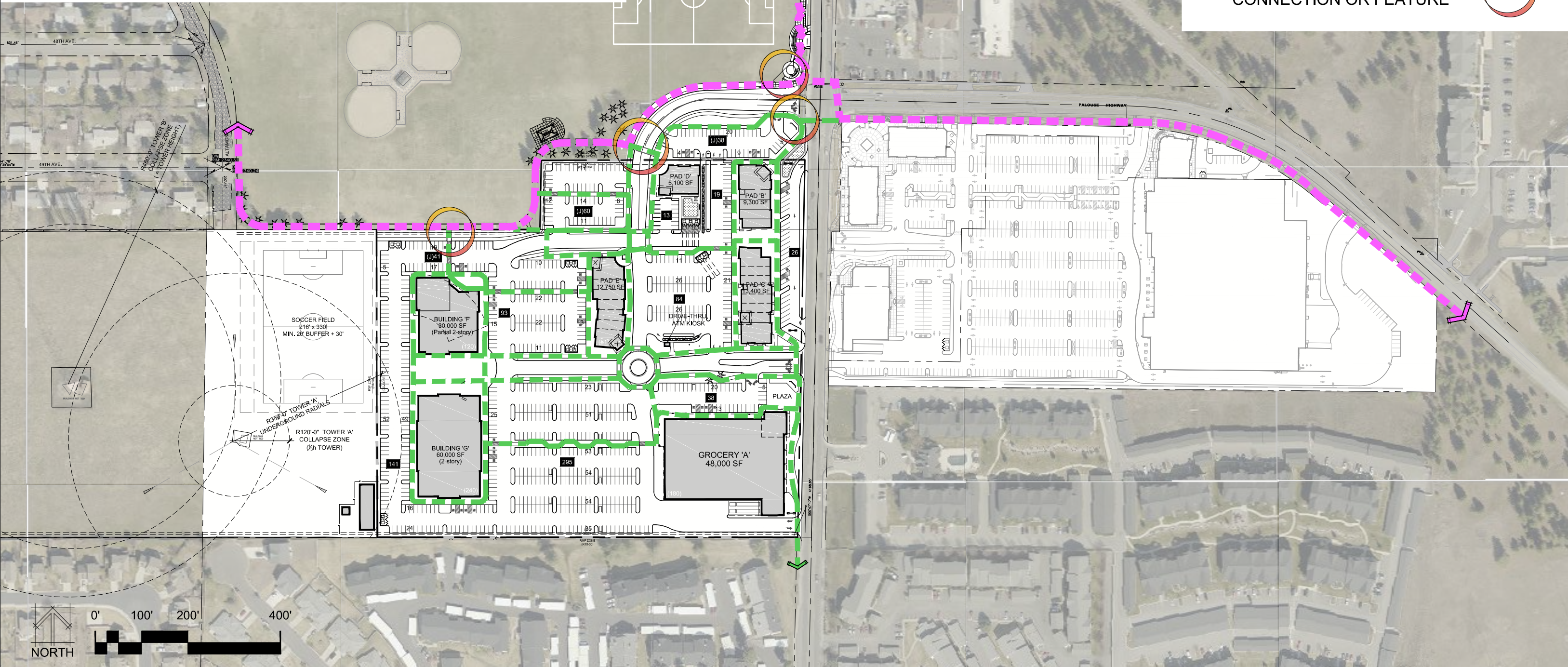
# DA Criterion 5.1: Pedestrian Connections

"The Property shall contain dedicated pedestrian and bicycle connections ("path") which are designed to allow pedestrians and bicycles to access and move around and through the Integrated Properties with connection to the surrounding neighborhood. When feasible, such path shall connect to existing publicly accessible trails, sidewalks or other pathways that are adjacent and contiguous to the Integrated Properties."

PEDESTRIAN CONNECTION TO  
PARK, BETWEEN BUILDINGS  
AND WITHIN PROJECT

CONTINUATION OF  
MULTIMODAL PATHWAY

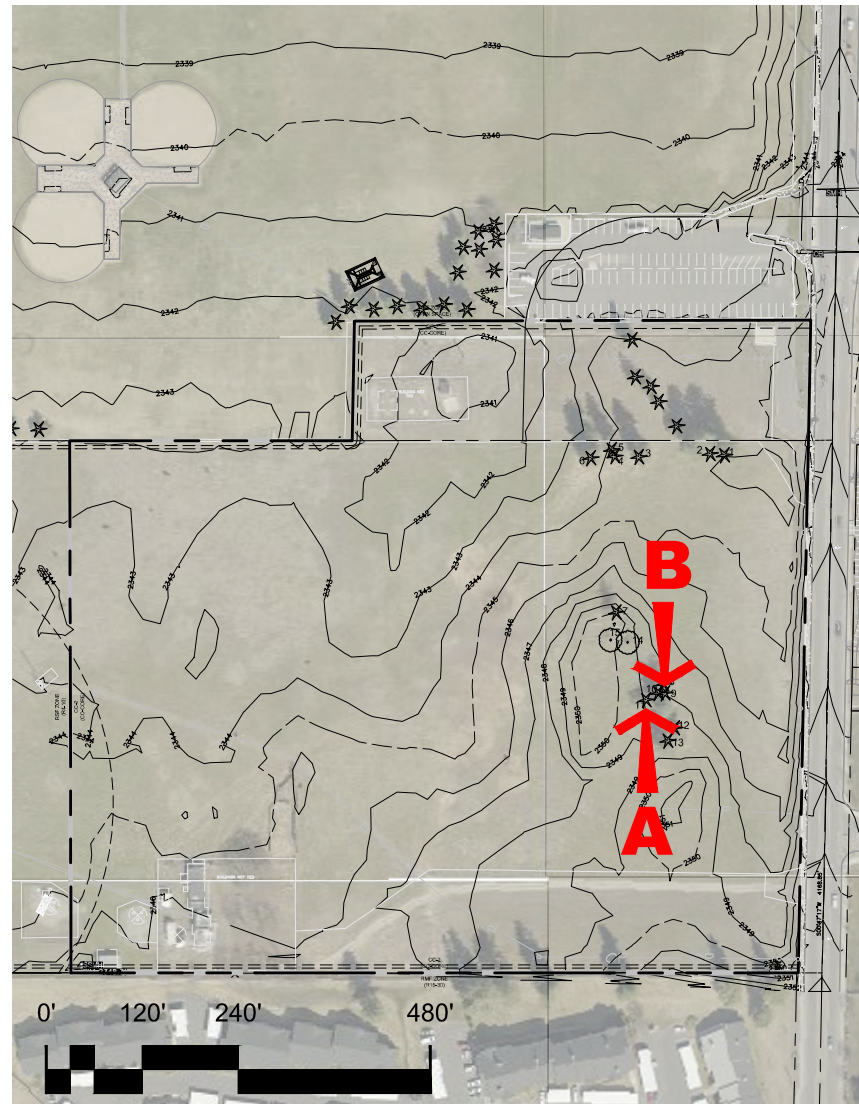
INDICATES MAJOR PATHWAY  
CONNECTION OR FEATURE





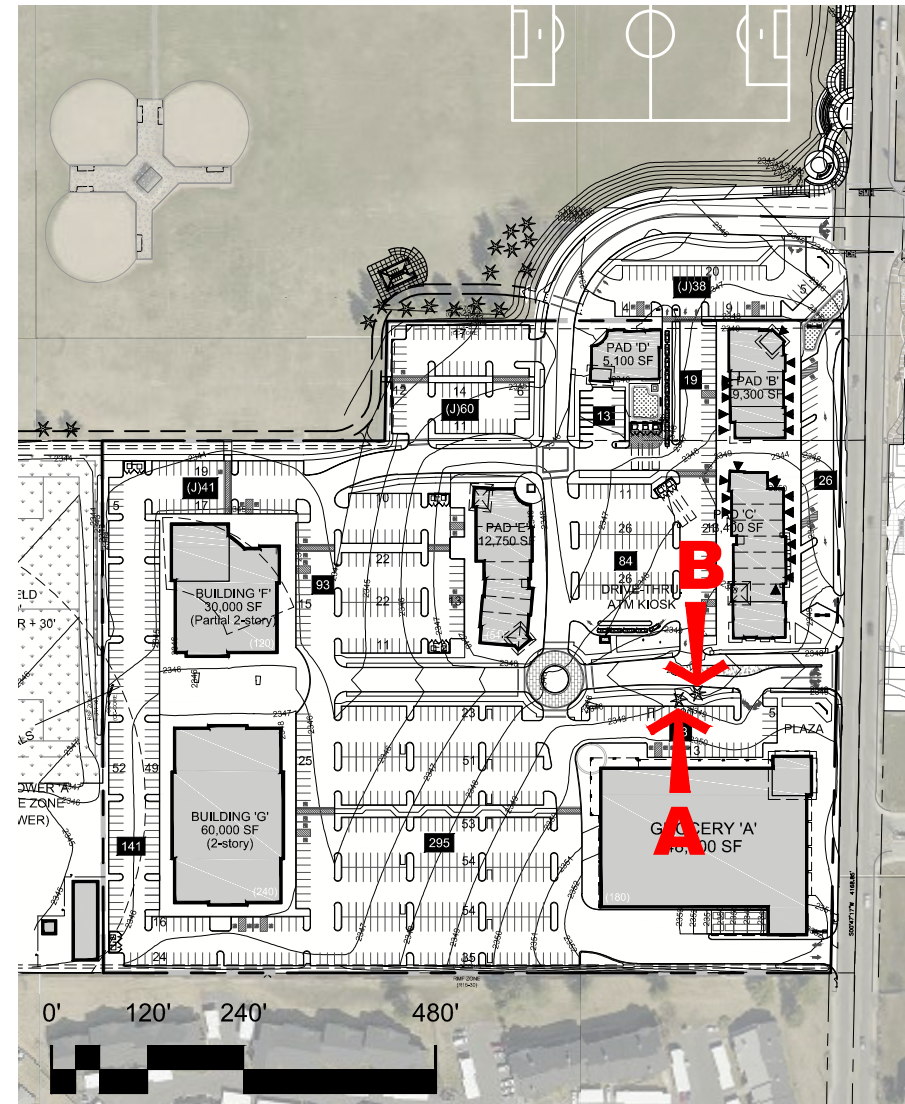
## DA Criterion 5.2: Tree Preservation

"Any plan for development of the Property shall provide for the preservation of trees, by leaving in place, a minimum of 10% of all Ponderosa Pine trees."



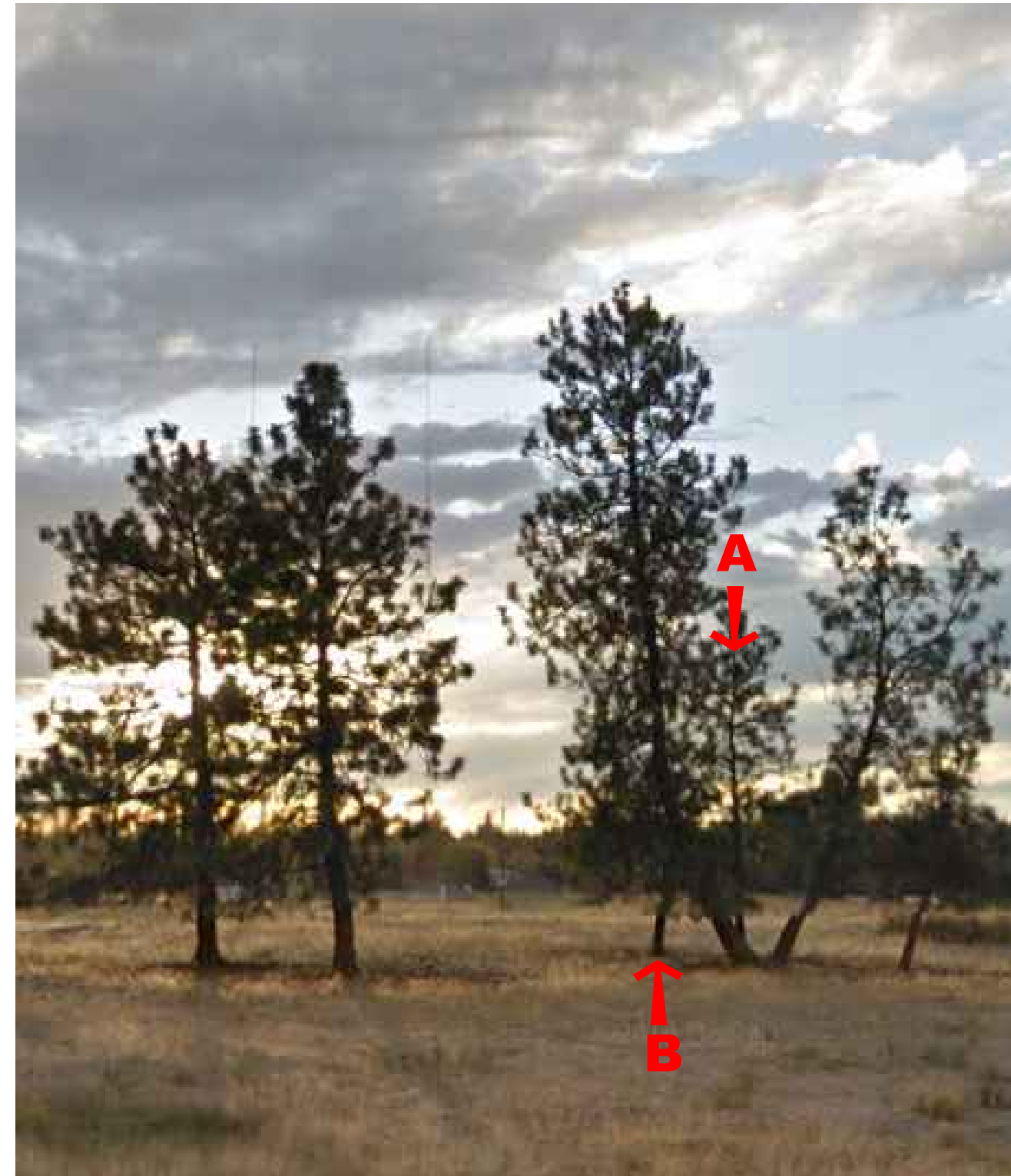
Existing Tree Plan

18 ON-SITE PONDEROSA PINES



Preserved Tree Plan

2 PRESERVED = 11% OF EXISTING





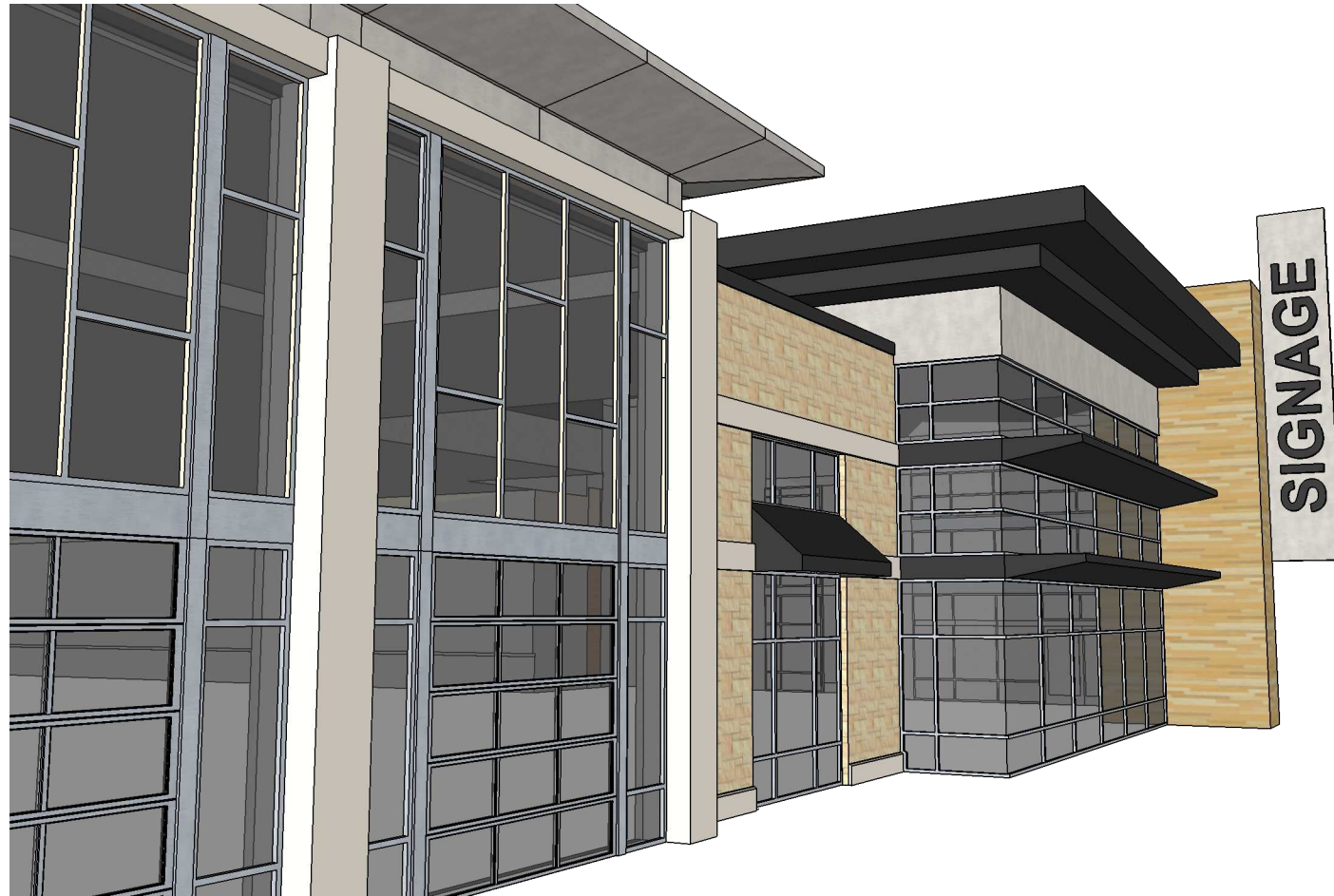
## DA Criterion 5.3: Design Theme

*“The Integrated Properties shall be developed with a consistent design theme utilizing, for example, similar or complementary construction materials, architectural characteristics, streetscapes, open spaces, fixtures, and landscaping. All buildings shall provide architectural treatment of interest on those facades visible from the street, such as color, texture, glazing, material differentiation or any other mechanism designed to lessen the impact of building mass when viewed from the street.”*



# RETAIL-A

## ANCHOR GROCERY



Regal Street - Perspective View



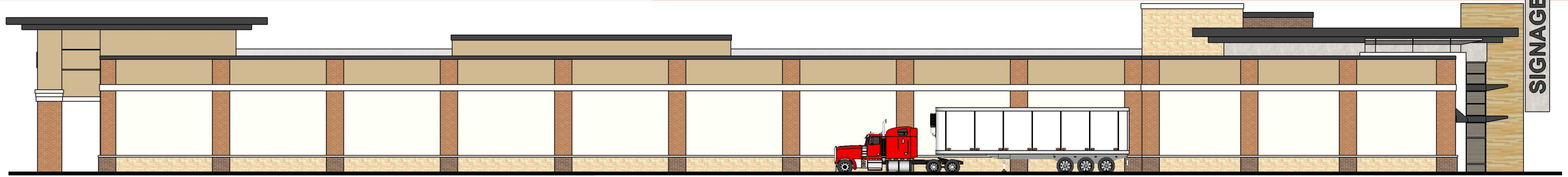
Regal Street - Perspective View - NEC







SIGNAGE



Overall Elevation - South



T.O. Wall  
22'-8"

B.O. Soffit  
14'-0"

Finish Floor  
0'-0"

Partial Elevation - South SCALE: 1" = 10'-0"

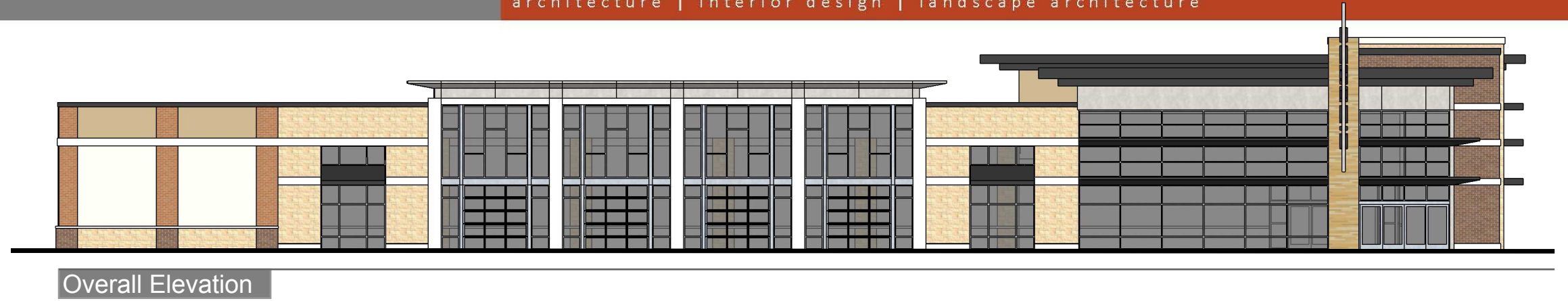
T.O. Wall  
22'-8"

Finish Floor  
0'-0"

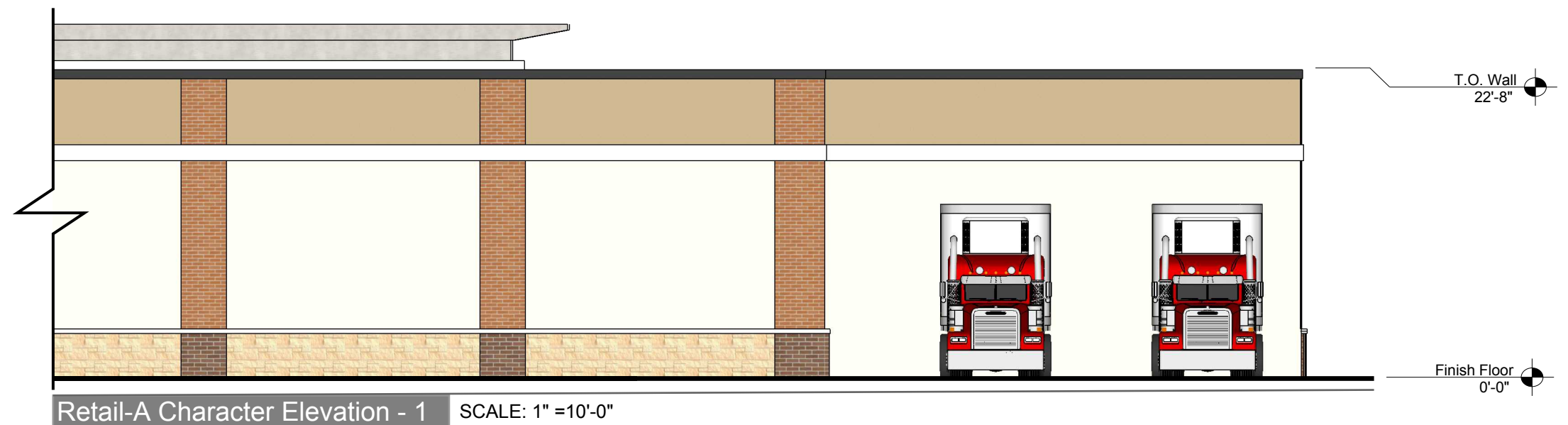
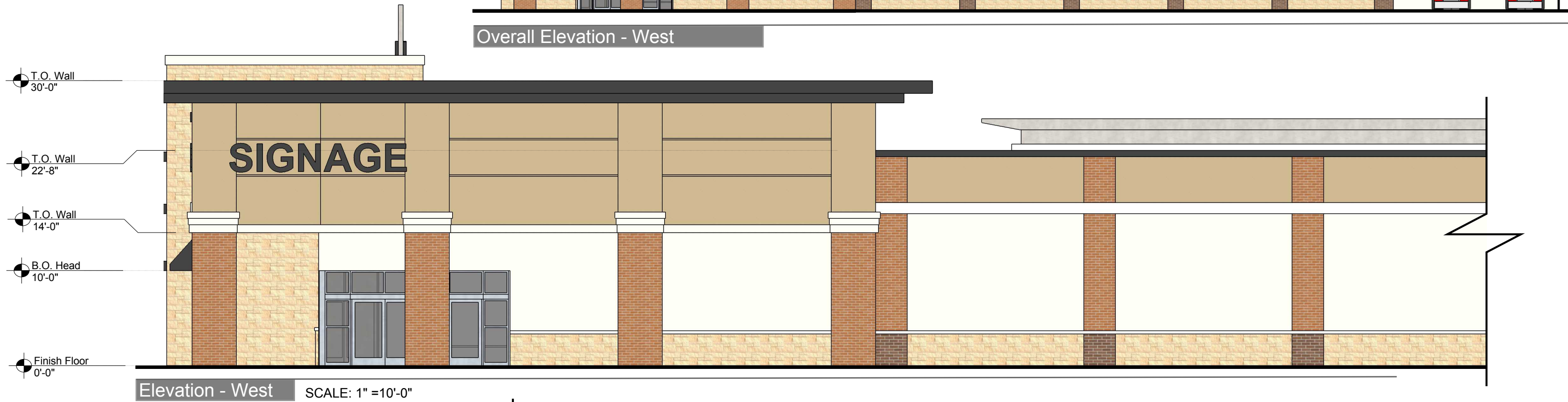
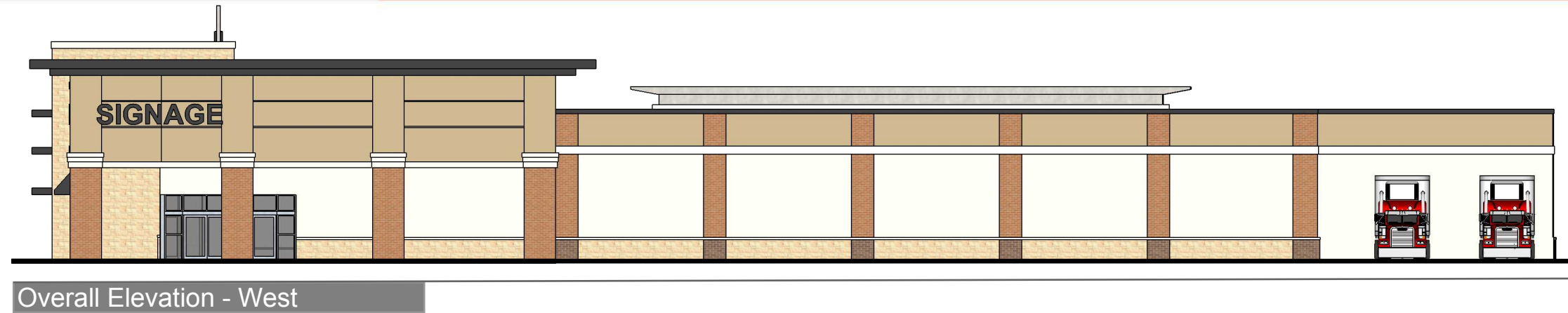


Partial Elevation - South SCALE: 1" = 10'-0"











# RETAIL-B

## SMALL / MEDIUM RETAIL



Concept Massing - NWC

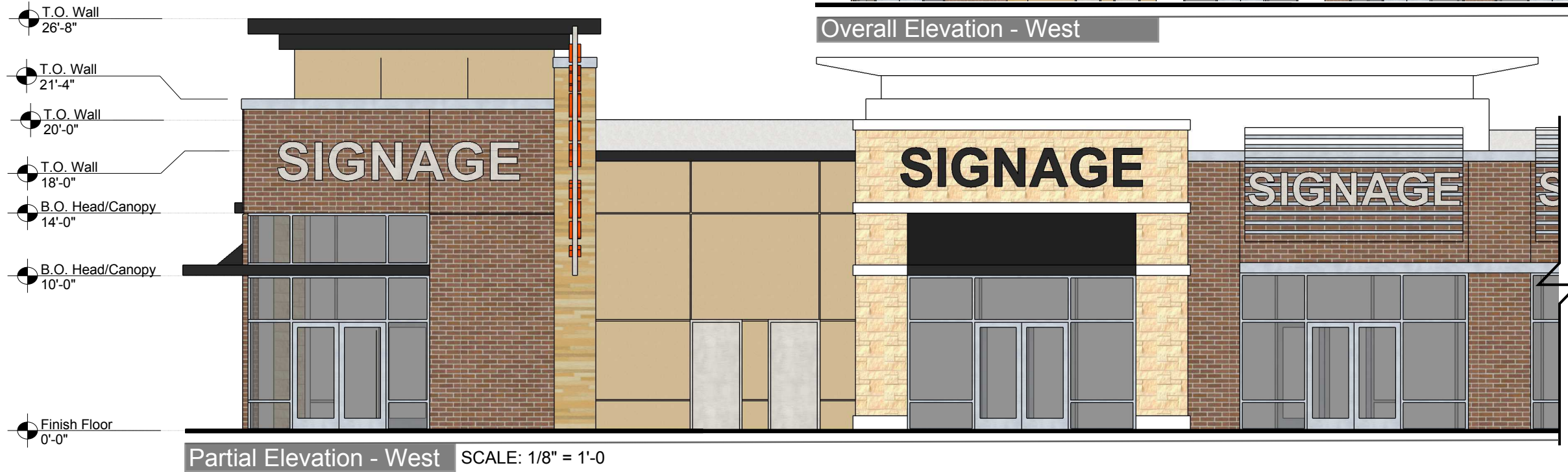


Concept Massing - SEC

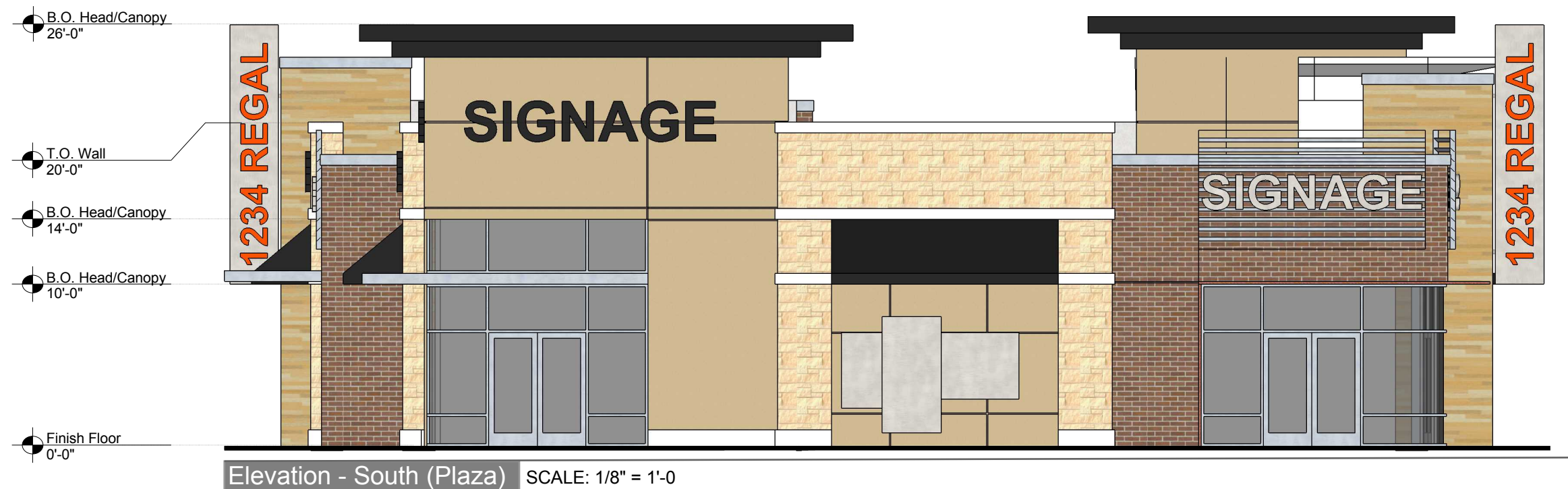


Concept Massing - NEC















# RETAIL-C

## SMALL / MEDIUM RETAIL



Detail - West



NEC (Regal) - Perspective View



Detail - NWC







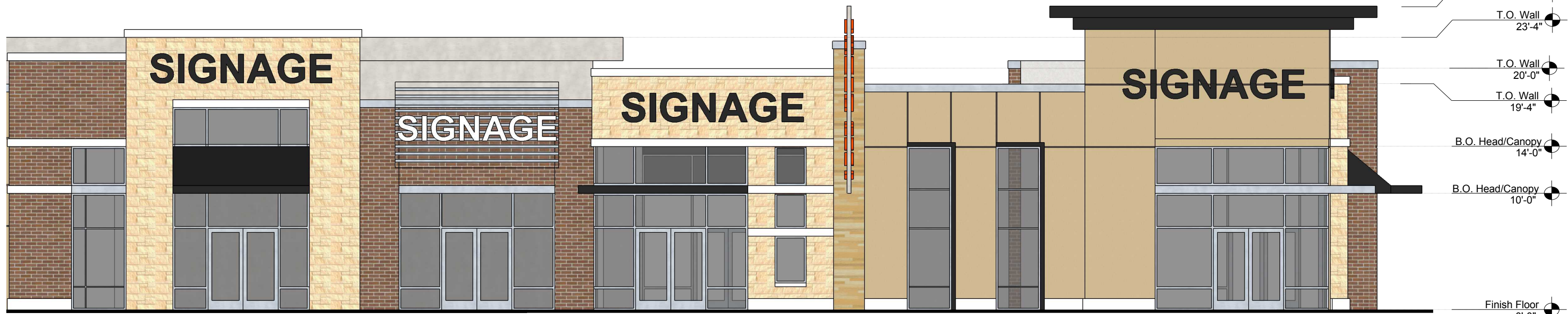


Overall Elevation - West

- T.O. Wall 24'-0"
- T.O. Wall 21'-4"
- T.O. Wall 20'-8"
- T.O. Wall 18'-0"
- B.O. Head/Canopy 14'-0"
- B.O. Head/Canopy 10'-0"
- Finish Floor 0'-0"



Partial Elevation - West SCALE: 1/8" = 1'-0



Partial Elevation - West SCALE: 1/8" = 1'-0

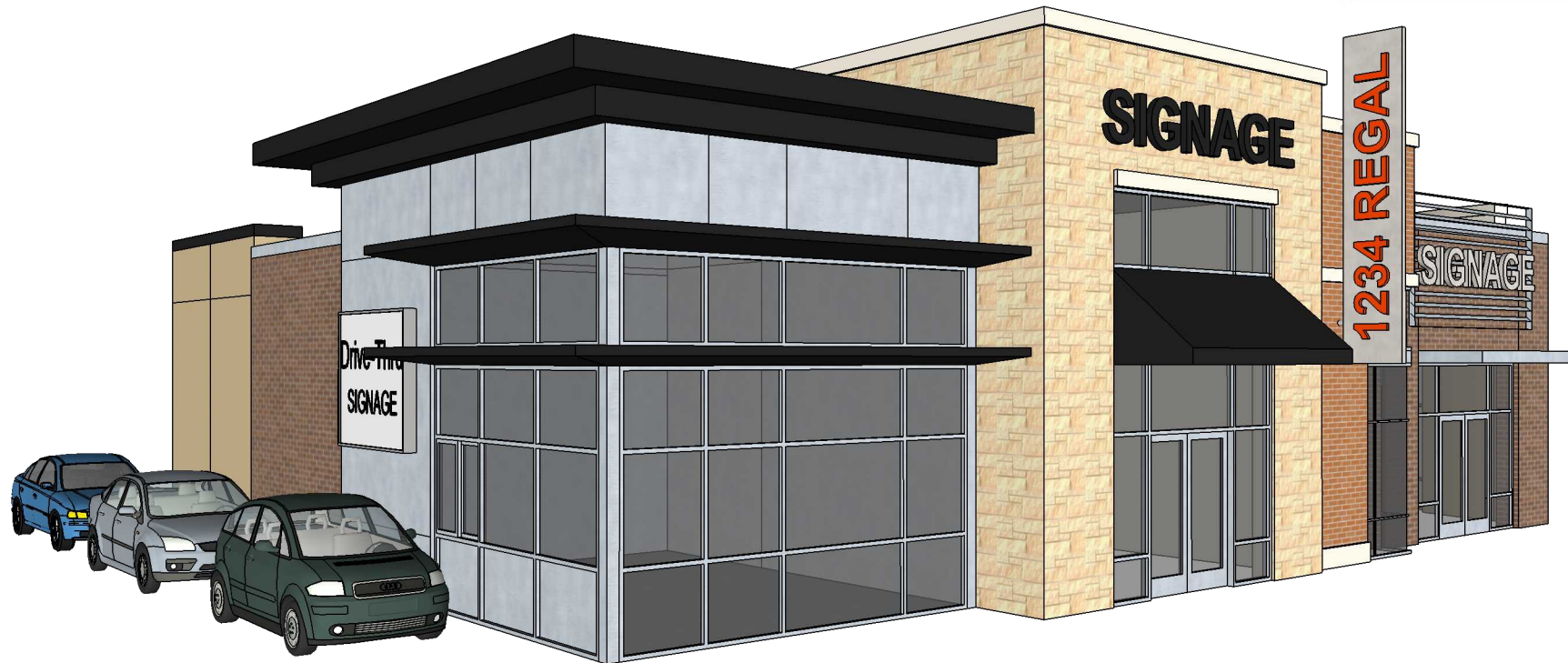






# RETAIL-D

SMALL / MEDIUM RETAIL



NEC - Perspective View

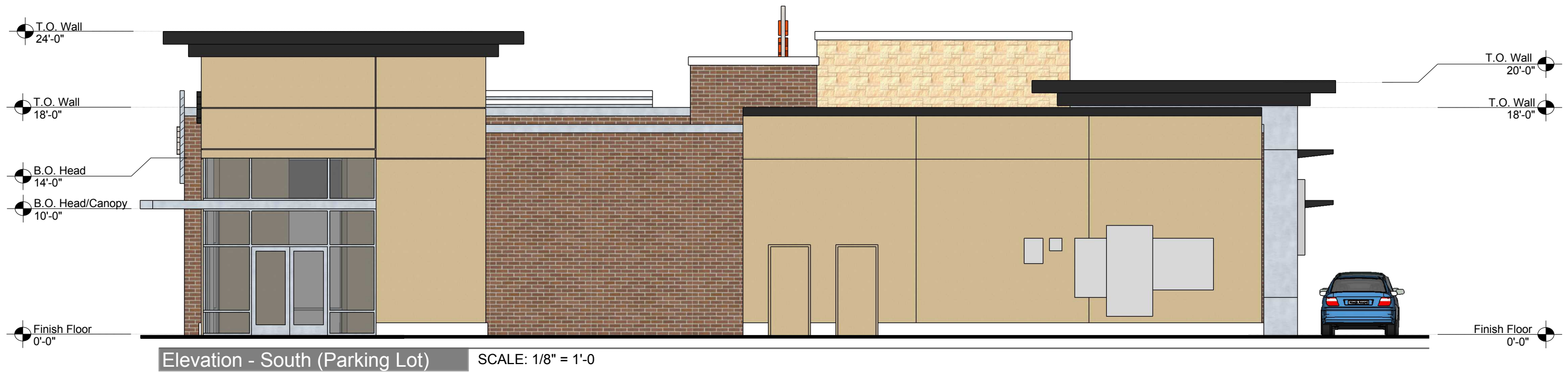


NWC - Perspective View



NWC - Perspective View











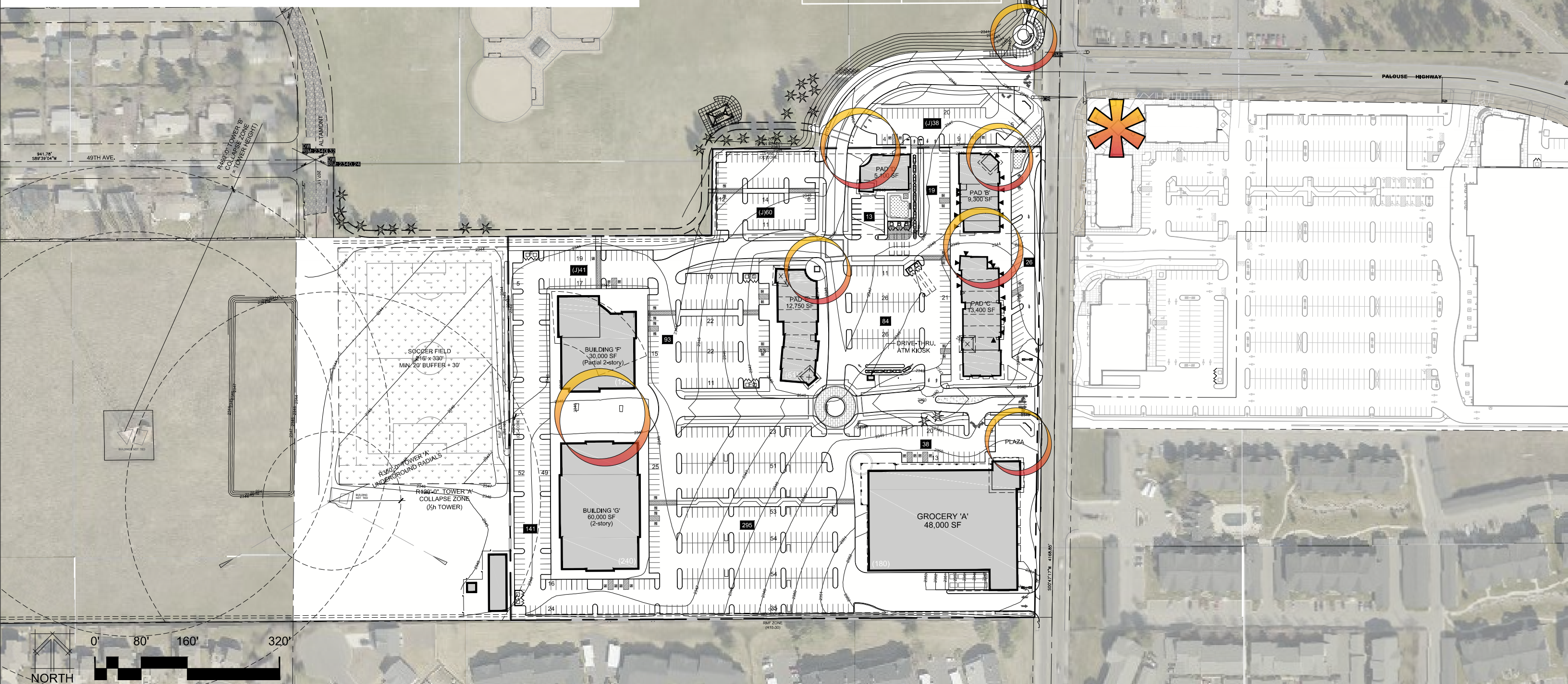
## DA Criterion 5.4: Community Plaza

*"A community plaza shall be designated that serves as a central gathering place on one of the Integrated Properties and, if not located on the Property, Owners shall provide satisfactory evidence of the Owners' contractual and financial commitment to participate in the development of the community plaza."*

EXISTING DESIGNATED  
DISTRICT COMMUNITY PLAZA



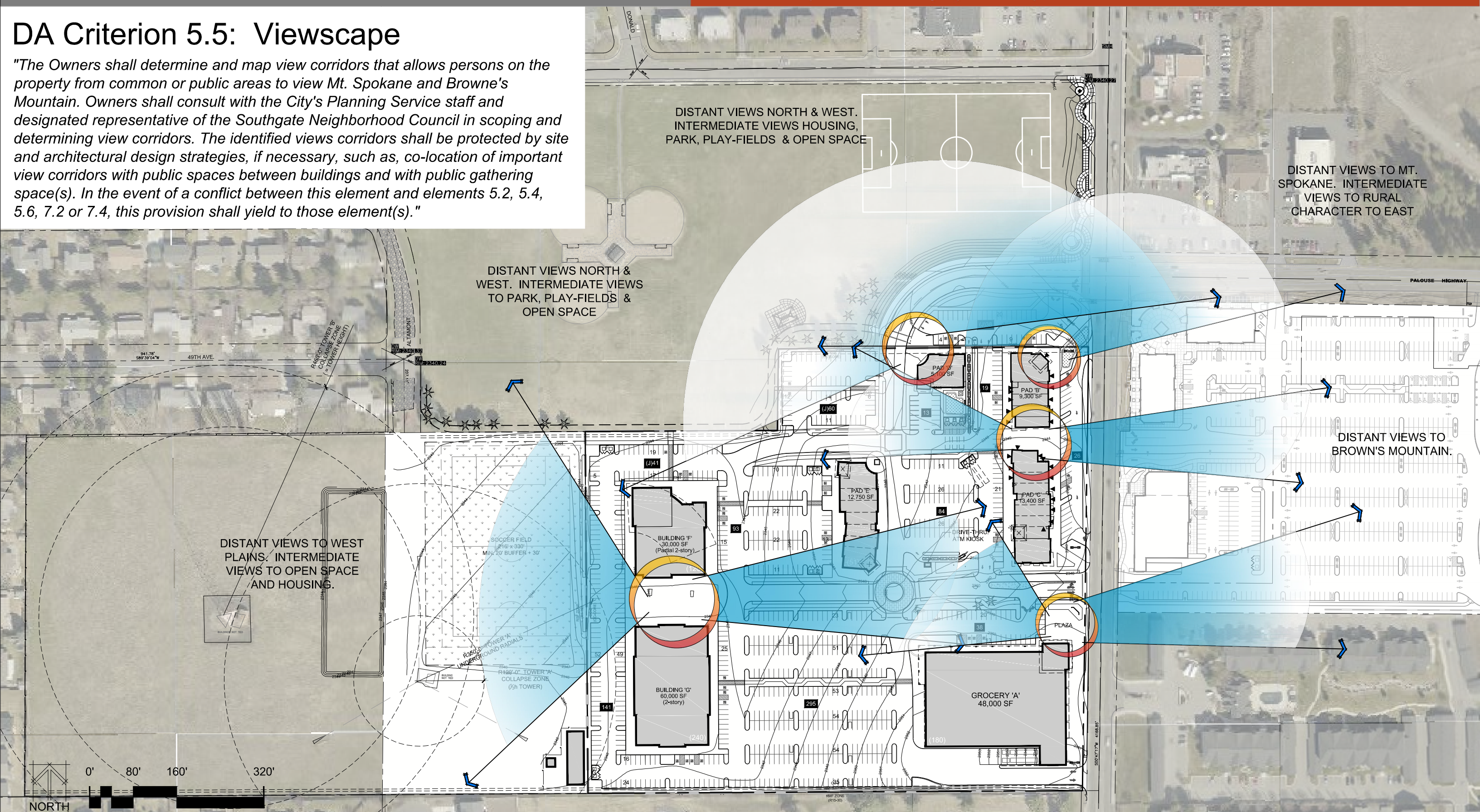
## SECONDARY PLAZA OR PUBLIC SPACE OPPORTUNITIES





## DA Criterion 5.5: Viewscape

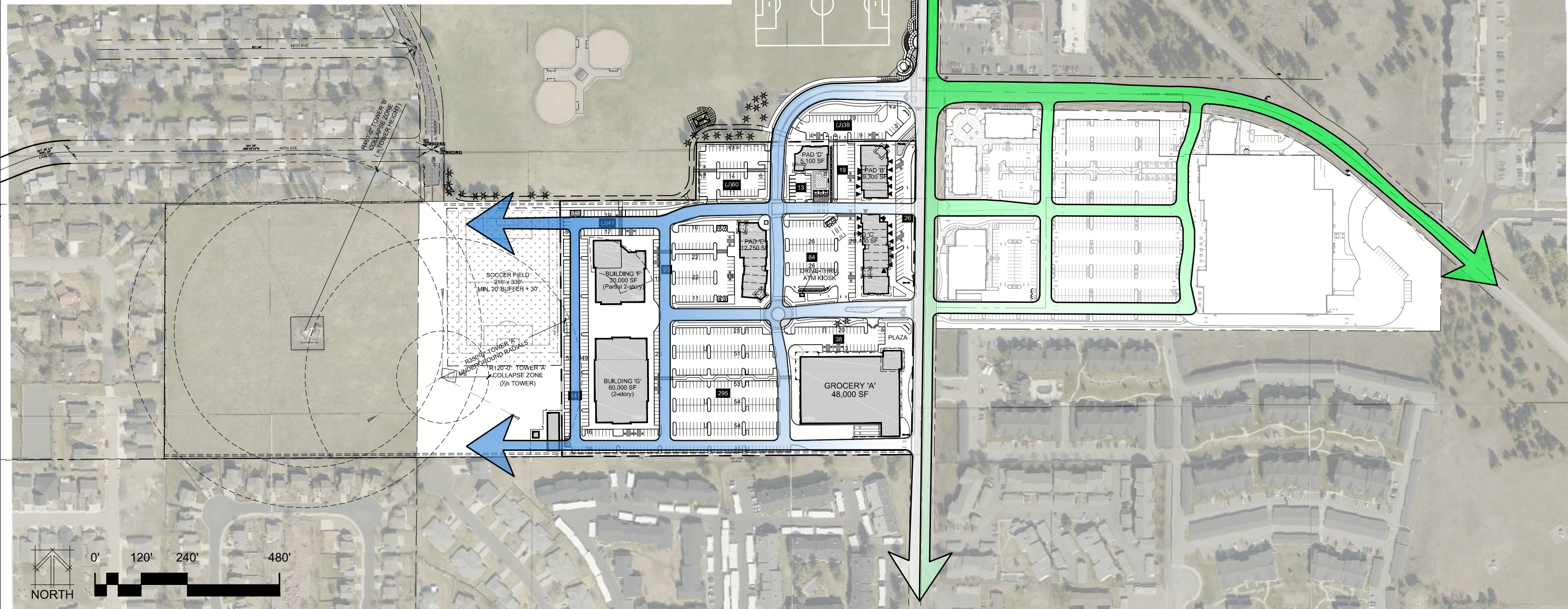
*"The Owners shall determine and map view corridors that allows persons on the property from common or public areas to view Mt. Spokane and Browne's Mountain. Owners shall consult with the City's Planning Service staff and designated representative of the Southgate Neighborhood Council in scoping and determining view corridors. The identified views corridors shall be protected by site and architectural design strategies, if necessary, such as, co-location of important view corridors with public spaces between buildings and with public gathering space(s). In the event of a conflict between this element and elements 5.2, 5.4, 5.6, 7.2 or 7.4, this provision shall yield to those element(s)."*





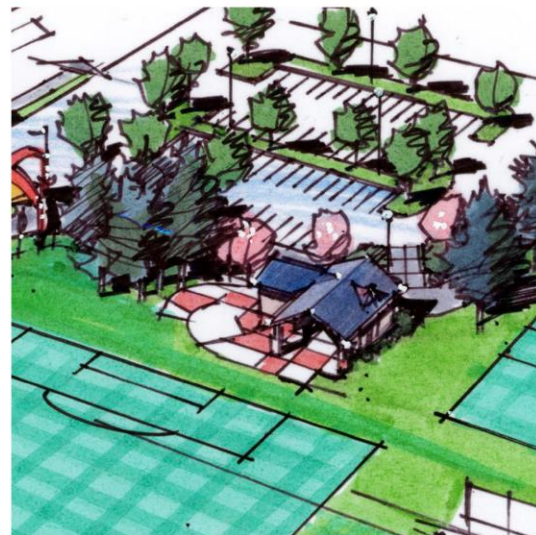
# DA Criterion 5.6: Long-Term Urban Development

"The intent of the parties is to design and develop urban features that will facilitate integration of the Property (and surrounding area) into an urban district with a unified character that promotes pedestrian and vehicular circulation, without conflict, encourages opportunities for mixed use development and enhances the natural and built aesthetics in the area. In order to enhance connectivity and facilitate future urban development, driveways though the property shall be designed where ever possible, to facilitate connections to the propeties identified in Recital D, above. Curbing shall be used to define the parking lot area, such as perimeter curbing and main drive aisles. Driveway entrance(s) and interior landscaping features will also be curbed."





# Site Massing







View From Regal & Palouse Looking West



View From Regal St. looking South





View From Target Center Main Driveway Looking West



View From Target Center 2<sup>nd</sup> Driveway into Proposal's Main Regal St. Access



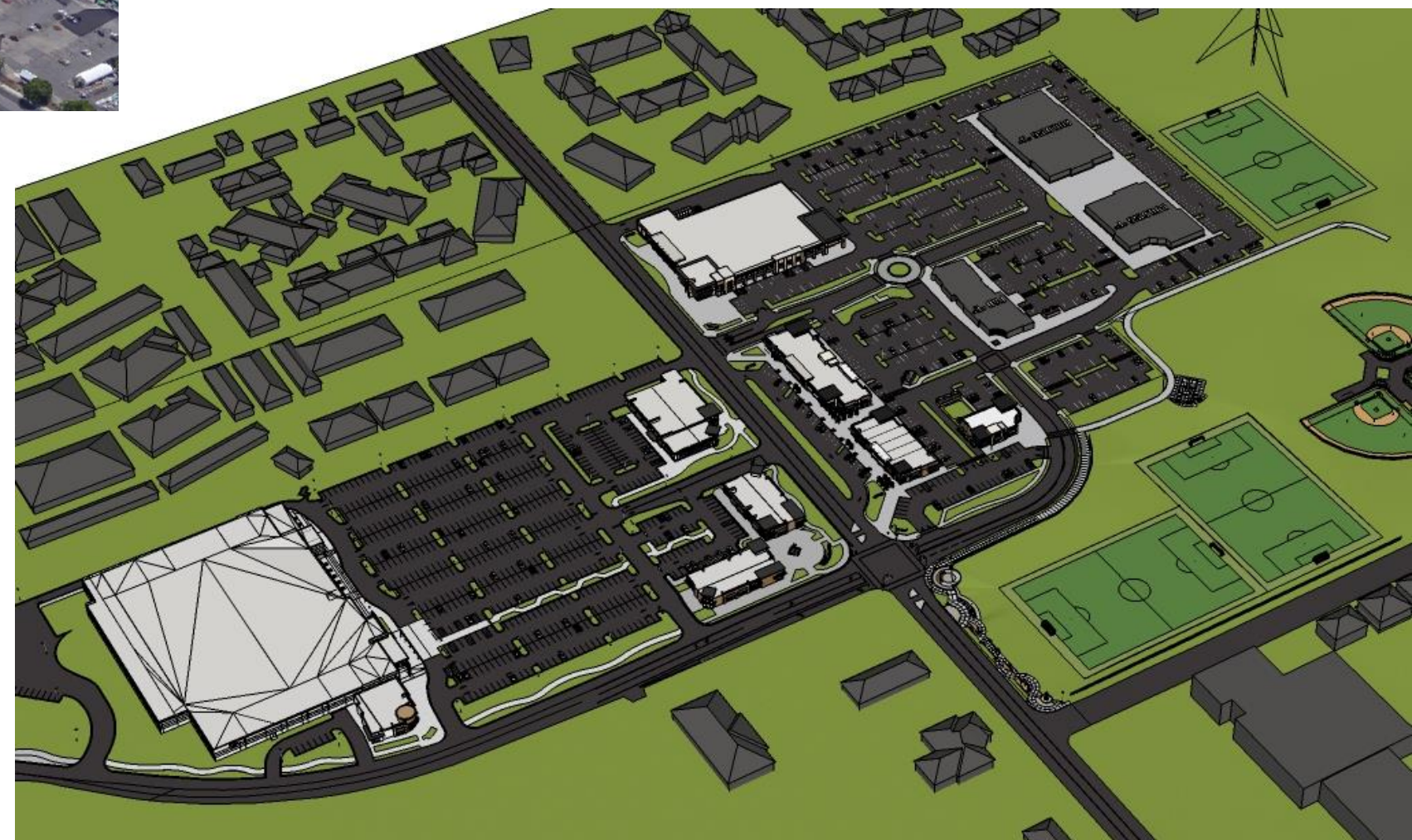


View From Regal St. Looking NW TO 'AirPort Parking'



View From Proposal Looking East Through Plaza

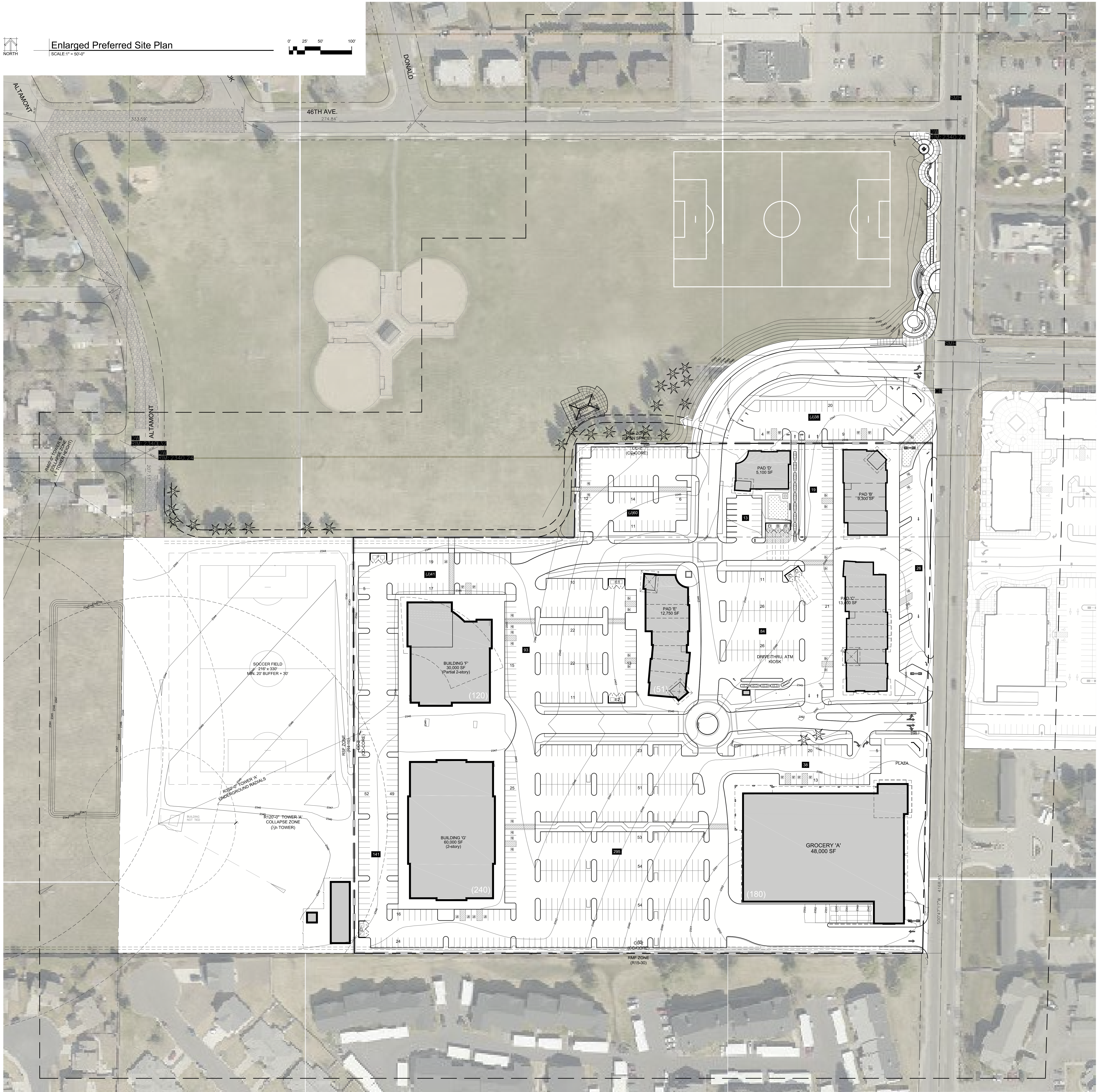
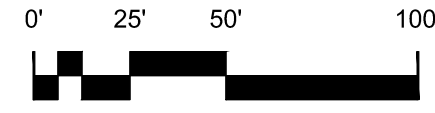








Enlarged Preferred Site Plan  
SCALE: 1" = 50'-0"











# Southgate Integrated Site Plan

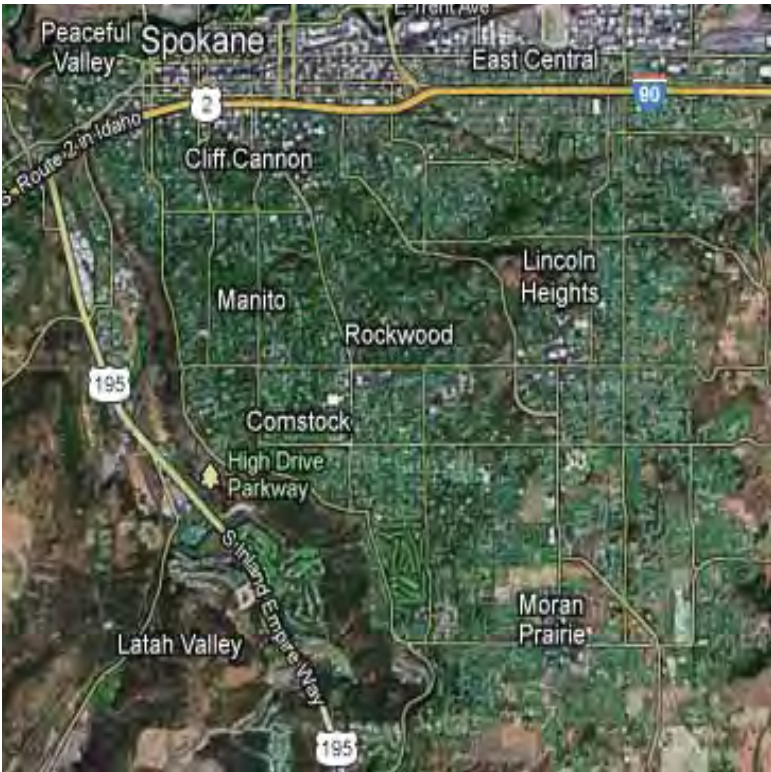
## Regal Street & Palouse Highway

Revised Final Submittal  
May 31, 2013

NOTE: This submittal revises the Revised Final Submittal dated 04/01/2013 and includes comments from Scott Chesney approval memo dated 03/29/2013 and an expanded Kit-of-Parts.



# Part A/Introduction



Aerial View | City Scale

## Background:

In 2005 Black Properties and KXLY submitted applications for Comprehensive Plan Map Amendments for their respective properties at the Palouse/Regal intersection.

In 2006 Home Depot assembled +/- 15 acres near the same intersection and likewise made application for a Comprehensive Plan Map Amendment.

The three applications made their way through the normal Amendment process of notifications, public hearings, neighborhood meetings, requisite studies and supplemental supporting information.

The three applications were eventually effectively combined into a single process and the City Council took up the matter in 2008. The Council approved the three Comprehensive Plan Map Amendment applications and entered into detailed Development Agreements in 2009 with each of the three Applicants.

The agreements balanced accepted City-wide and Neighborhood planning principles, Comprehensive Plan goals and policies, traffic and infrastructure concerns, market demand and preferences, and the applicants' desires and rights to develop their properties.

## The Development Agreement:

The City entered into separate Development Agreements with each of the three Applicants that take into account minor differences in the parcels and their respective development opportunities. They are, however, essentially identical in intent and include the following keypoints:

- Comprehensive Plan designation of CC Core/District Center and corresponding Zoning Map designation of CC2-DC.
- To provide both the Neighborhood and Applicants with a reliable level of development predictability, established the CC2-DC General Development Standards as the applicable development standards for the duration of the Development Agreement.
- Requirement that the three Applicants jointly prepare an overall Integrated Site Plan covering the +/- 45-acres included in the Development Agreements prior to issuance of any building permits. The Integrated Site Plan is required to include the following components (Reference Development Agreement Paragraph 5):
  - ♦ **Pedestrian Connections:** Provide pedestrian and bicycle connectivity to, through, and between the three developments and the Neighborhood.
  - ♦ **Tree Preservation:** Preserve selected existing Ponderosa Pines on the development parcels.
  - ♦ **Design Theme:** Develop a consistent design theme for the three developments utilizing common or complementary architectural and landscape/open space features and materials.
  - ♦ **Community Plaza:** Designate a central gathering place on one of the development parcels.
  - ♦ **Viewscape:** Protect selected views to Mt. Spokane and Browne's Mountain.
  - ♦ **Urban District:** Make provisions in site plan design and infrastructure to allow evolution into a mixed use urban center as market conditions warrant and such developments become feasible.



# Part A/Introduction

## The Development Agreement (cont’d.):

The Development Agreements also address transportation mitigation and fees, building design, and square footage limitations for each parcel.

A key provision of the Development Agreements provides for the review and comment of the Integrated Site Plan by the City Design Review Committee, including input from a Southgate Neighborhood resident appointed by the Mayor, and a recommendation to the Planning Director who will make the final decision regarding the Integrated Site Plan’s compliance with the intent of the Development Agreements.

## Design Review Board Collaborative Workshop

On August 8, 2012, the Applicants collectively presented the Southgate Integrated Site Plan to the Design Review Board including background, a summary of the Development Agreement key points, and a graphic presentation of the Applicants’ response to the Development Agreements key design criteria. As required by the Development Agreement, a Southgate Neighborhood representative was included as a non-voting member of the Board.

The Design Review Board offered the following recommendations corresponding to the six key Development Agreement and Integrated Site Plan issues:

- Pedestrian Connections:
  - ♦ Show pedestrian connection west to the sports complex.
  - ♦ Better pedestrian connections between the three properties.
  - ♦ Show bike lane and pedestrian corridor character consistent with Southgate Connectivity Plan.
- Tree Preservation:
  - ♦ Inventory trees and identify quality.
- Design Theme:
  - ♦ Use Community Plaza as design theme and emulate that character throughout streetscape and open spaces.
  - ♦ Use “Kit-of-Parts” including color palette and material choices.
  - ♦ Show fixtures and landscape as they relate to open space.
- Community Plaza:
  - ♦ Identify 2-3 potential locations based on site analysis.
- Viewscape:
  - ♦ Integrate view corridors with Plaza locations and coordinate with Neighborhood.
- Long-Term Development:
  - ♦ Future DRB’s must consider how future building permit applications will facilitate transition to an urban district.

## The Solution:

The Development Agreements recognize that large scale developments like this take place in phases over a number of years and that a preferred site plan in today’s marketplace may not be the preferred site plan in a changing retail marketplace in a few years.

Accordingly, the Development Agreements do not require specific site plans illustrating building footprints, landscaping, and parking as a typical site plan often does. It relies instead on the City’s established development standards for parking ratios, setbacks, landscaping, building design, lot coverage, etc., to produce neighborhood and street appropriate projects consistent with the City’s development goals and policies.

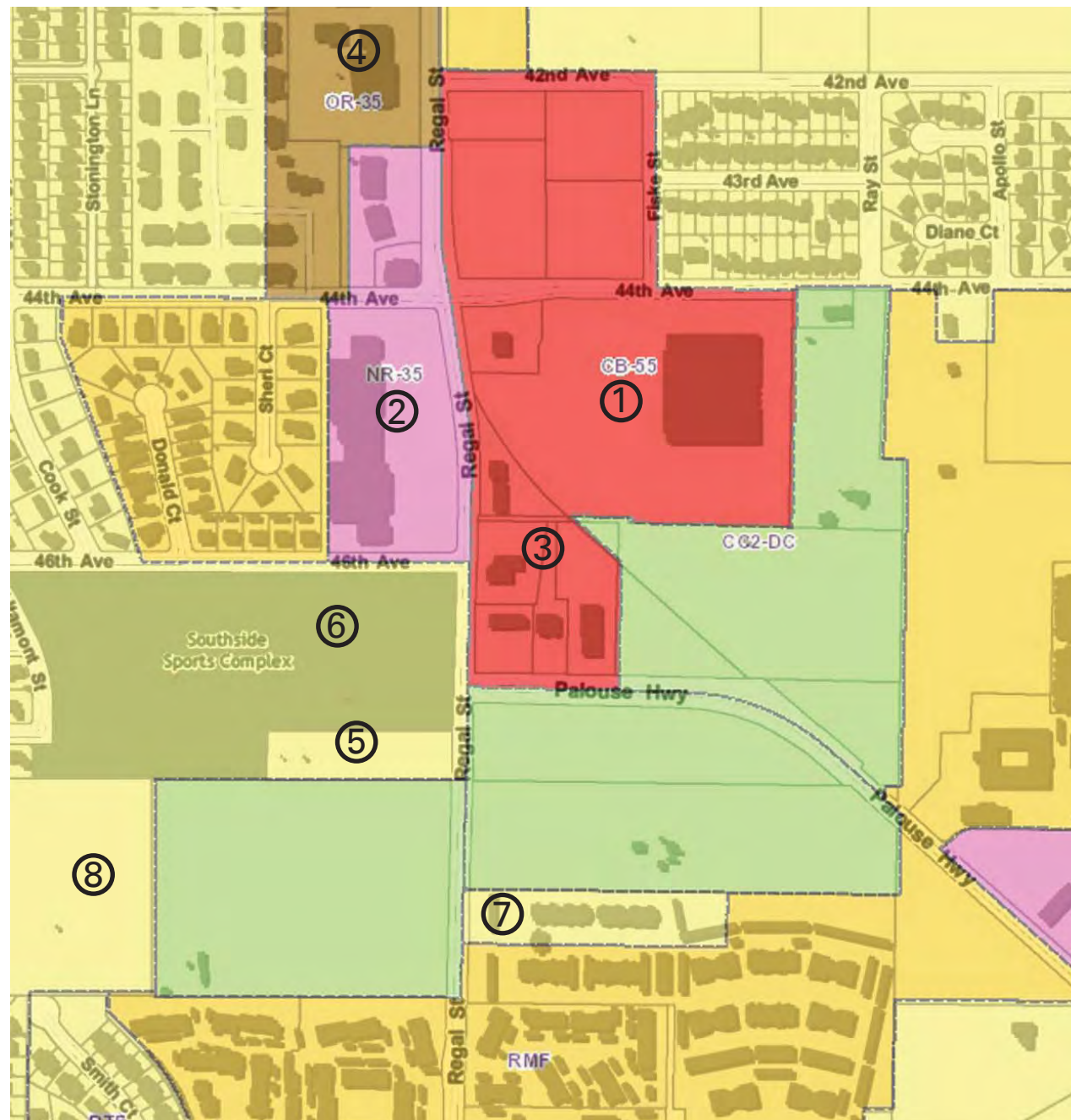
The proposed Integrated Site Plan is intended to provide a framework and general strategy for developing future project specific site plans and acknowledgement of the Development Agreement’s requirements and graphically represent them in a manner that future site plans can be tested against administratively.

Additionally, the Development Agreement requires each future project, including a very specific site plan, be reviewed by the DRB prior to issuance of any building permits.

This submittal is intended to respond to the Design Review Board recommendations with a design solution described herein that revised, expands, and clarifies the solution originally presented to the Design Review Board.









## Part B/Context: Zoning Map



### Context

- 1 Shopko
- 2 Albertson's Center
- 3 McDonald's | HiCo | Auto Repair
- 4 Regal Village
- 5 School District Property
- 6 Parks Department Sports Complex
- 7 Apartment Complex
- 8 KXLY Radio Tower

### Zoning Legend

	OR-35	Office retail
	CB-55	Community Business
	NR-35	Neighborhood Retail
	CC2-DC	Center & Corridor
	RSF	Residential Single-Family
	RMF	Residential Multi-Family



## Part B/Context: Existing Street Views



 Looking West along S. Regal



 **B** Looking West along S. Regal



 Looking West along S. Regal



 Looking West along S. Regal



 Looking West along S. Regal



### Vicinity Satellite View



 Looking West along S. Regal



 Looking East along S. Regal



## Part B/Context: Existing Street Views




 Looking East along S. Regal



Looking East along S. Regal




 Looking East along S. Regal



 Looking East along S. Regal




 Looking North along Palouse Highway




Vicinity Satellite View



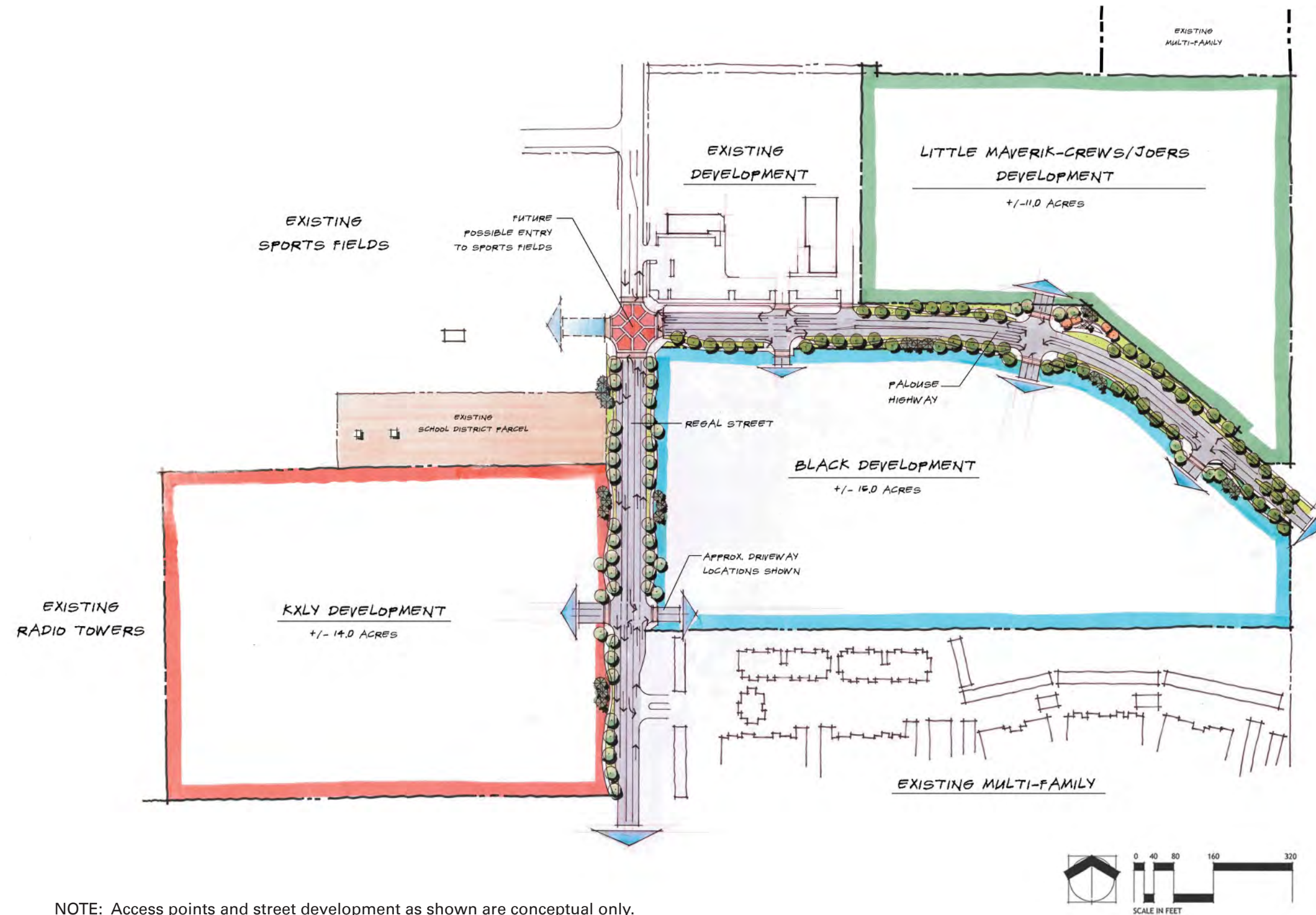
 Looking East along S. Regal



 Looking North along Palouse Highway



# Part C/Solution: Overall Site Plan





# Part C/Solution: Development Agreement Requirements & DRB Recommendations

## 5.1 Pedestrian Connections

### Development Agreement Requirements:

The properties shall contain dedicated pedestrian and bicycle connections (“paths”) which are designed to allow pedestrians and bicycles to access and move around and through the Integrated Properties with connection to the surrounding neighborhood. When feasible, such path shall connect to existing publicly accessible trails, sidewalks or other pathways that are adjacent and contiguous to the Integrated Properties.

### Design Review Board Initial Recommendations:

- Show a pedestrian connection west to the sports complex.
- Better pedestrian connections between the three properties-continue to work with City staff.
- Show bike lane and pedestrian corridor character consistent with Southgate Connectivity Plan.

### Applicants’ Response & Proposed Solution:

The Southgate Neighborhood Connectivity Plan (SNCP) provides a context in which to apply the planning principles of the Development Agreement as well as the recommendations of the DRB. Key applicable elements of the SNCP include the proposed “Green Ring” that generally circumvents the Southgate Neighborhood and the proposed non-motorized “Ferris-Adams Student Trail” connecting Ferris and Adams schools with areas west of Regal Street through the Project.

The Applicants’ revised proposal supports and implements the Southgate Neighborhood Connectivity Plan in very significant ways by extending the “Ferris-Adams Student Trail” through the District Center to portions of the Neighborhood lying west of Regal Street and provides a connection to the “Green Ring” from the District Center. Additionally, the Applicants propose to utilize the “Typical Street/Arterial Character” character designs for the Non-Motorized Trail, Collector Arterial (Palouse Highway), and Minor Arterial (Regal Street) included in the SNCP. The Applicants acknowledge that final design of the connections and streets will require engineering input from City Staff and further traffic and safety analysis.

## 5.2 Tree Preservation

### Development Agreement Requirements:

Any plan for development of the Property shall provide for the preservation of trees, by leaving in place a minimum of 10% of all Ponderosa Pine trees.

### Design Review Board Initial Recommendations:

- Inventory the trees and identify quality.

### Applicants’ Response & Proposed Solution:

The Applicants revised proposal includes a detailed tree inventory report that locates trees generally above 1-1/2-inch caliper and evaluates them for health and overall condition. This work was completed by a registered Landscape Architect on our staff with experience doing this kind of work. Based on the location and health of the existing tree inventory, appropriate numbers of healthy trees will be selected to be preserved and integrated into the Project as appropriate.

## 5.3 Design Theme

### Development Agreement Requirements:

The Integrated Properties shall be developed with a consistent design theme utilizing, for example, similar or complementary construction materials, architectural characteristics, streetscapes, open spaces, and landscaping. All buildings shall provide architectural treatment of interest on those facades visible from the street, such as color, texture, glazing, material differentiation or other mechanism designed to lessen the impact of building mass when viewed from the street.

### Design Review Board Initial Recommendations:

- Plaza could define theme and that character could be emulated throughout the streetscapes and open spaces of the three sites.
- Put together a palette or “kit of parts” including color palette and material choices.
  - Include fixtures and landscape as it relates to the open spaces.

### Applicants’ Response & Proposed Solution:

The Applicants revised proposal includes an expanded “kit of parts” that includes lighting (parking area, pedestrian, and accent types), street furniture (benches, trash receptacles, bollards, kiosks, way finding signage), materials and textures (paving patterns and colors), and a landscape material palette to be used throughout the three projects. This strategy allows appropriate variation in the building architecture based on building height, footprint and retail tenant brand and provides consistency at the pedestrian and streetscape level where it provides the most impact. The mitigation of larger buildings is established in the City-Wide Development Standards which address massing, height, blank walls, parapet features, etc.

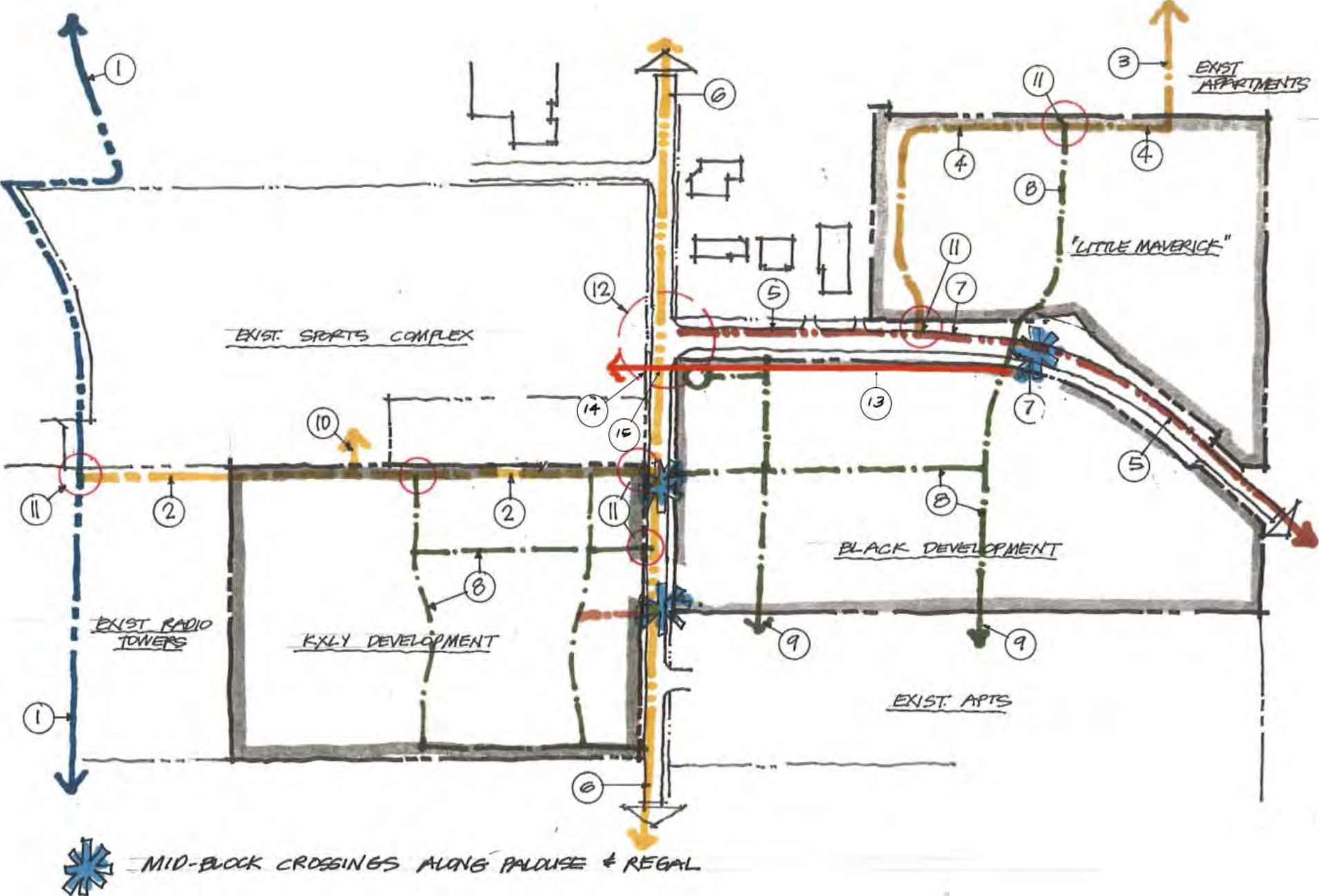


# Part C/Solution: Development Agreement Requirements & DRB Recommendations

<p>5.4 Community Plaza</p> <p><b>Development Agreement Requirements:</b></p> <p>A community plaza shall be designated that serves as a central gathering place on one of the Integrated Properties and, if not located on the Property, Owners shall provide satisfactory evidence of the Owners’ contractual and financial</p> <p>Commitment to participate in the development of the community plaza.</p>	<p><b>Design Review Board Initial Recommendations:</b></p> <ul style="list-style-type: none"><li>• Identify 2 - 3 potential locations based on site analysis.</li></ul>	<p><b>Applicants’ Response &amp; Proposed Solution:</b></p> <p>The Applicants’ revised proposal includes identifying three potential locations (one on each of the three parcels) and evaluating each of them taking into consideration views, tree preservation, cost and practical timing of parcel development, integration into the streetscape, and potential benefits from and for a retail environment. After due consideration, the preferred Community Plaza location is at the southeast corner of the Regal Street and Palouse Highway intersection as a part of the out-parcel development of the Black Development project.</p>
<p>5.5 Viewscapes</p> <p><b>Development Agreement Requirements:</b></p> <p>The Owners shall determine and map view corridors that allows persons on the property from common or public areas to view Mt. Spokane and Browne’s Mountain. Owners shall consult with the City’s Planning Service staff and designated representative of the Southgate Neighborhood Council in scoping and determining view corridors. The identified view corridors shall be protected by site and architectural design strategies, if necessary, such as co-location of important view corridors with public spaces between buildings and with public spaces between buildings and with public gathering spaces. In the event of a conflict between this element and elements 5.2, 5.4, 5.6, 7.2, or 7.4, this provision shall yield to those elements.</p>	<p><b>Design Review Board Initial Recommendations:</b></p> <ul style="list-style-type: none"><li>• View corridors should be integrated with the plaza locations as identified in 5.4 and as coordinated with the neighborhood.</li></ul>	<p><b>Applicants’ Response &amp; Proposed Solution:</b></p> <p>The Applicant met with City Staff and Neighborhood representatives on site on August 14, 2012 to review significant viewscapes and strategies for preserving them in the context of the other factors that will impact the site layout. In most cases, the quality of views to Mt. Spokane and Browne’s Mountain were already compromised by existing tall trees or existing buildings in close proximity to the potential community plaza site. In the final analysis, the Applicants consider all three locations under considering for the Community Plaza to provide different, but essentially equal, viewscapes..</p>
<p>5.6 Long-Term Development of the Urban District</p> <p><b>Development Agreement Requirements:</b></p> <p>The intent of the parties is to design and develop urban features that will facilitate integration of the Property (and surrounding area) into an urban district with a unified character that promotes pedestrian and vehicular circulation, without conflict, encourages opportunities for mixed use development and enhances the natural and built aesthetics in the area. In order to enhance connectivity and facilitate future urban development, driveways through the properties shall be designed, wherever possible, to facilitate connections to the properties identified in Recital D, above. Curbing shall be used to define the parking lot area, such as perimeter perimeter curbing and main drive aisles. Driveway entrances and interior landscaping features will also be curbed.</p>	<p><b>Design Review Board Initial Recommendations:</b></p> <ul style="list-style-type: none"><li>• Future DRB’s must consider how future building permit applications, with regard to siting and design, will facilitate evolution of the sites into an urban district.</li></ul>	<p><b>Applicants’ Response &amp; Proposed Solution:</b></p> <p>Project specific site plans for each of the three development projects will be designed to allow a transition to a more urban type district by identifying future building sites and access points that will support the characteristics typically found in an Urban District. Additionally, infrastructure such as utilities or similar required improvements will be located such that they will not interfere with future building locations.</p>



# Part C/Solution: Pedestrian Connections



## Narrative:

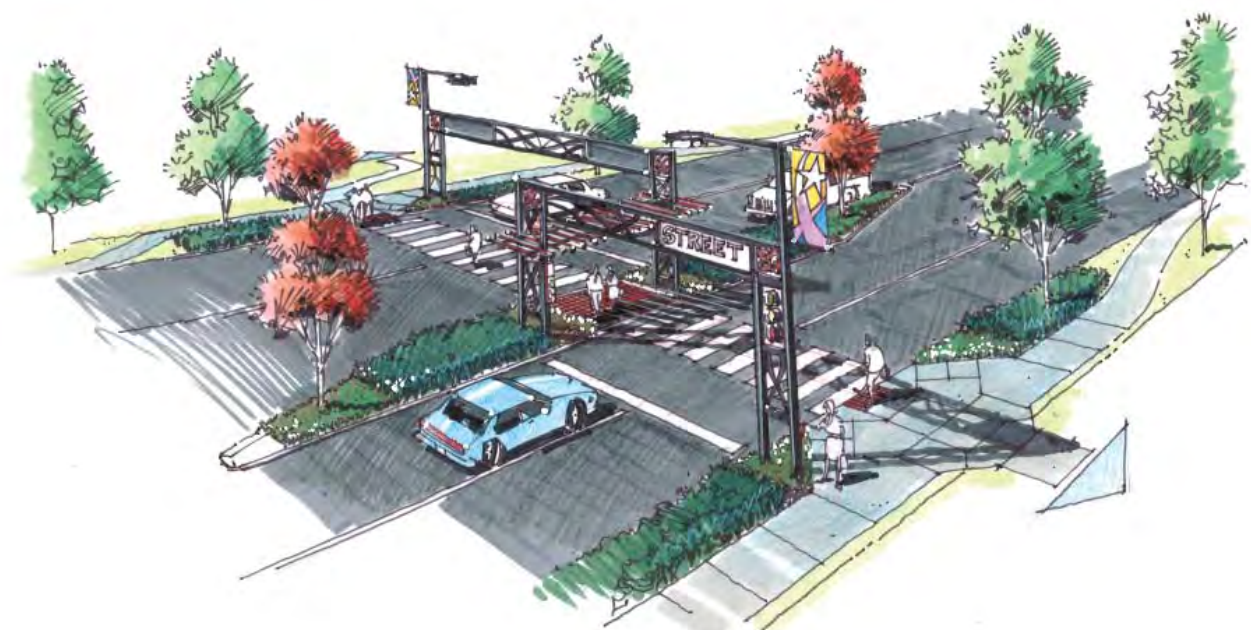
The proposed Connectivity Plan is a combination of Applicant-provided on-site trails and pathways and public right-of-way improvements proposed by the Southgate Neighborhood Connectivity Plan and City Public Works Standards for street development.

## Legend:

- 1. Proposed "Green Ring" - see SNCP.
- 2. Proposed Non-Motorized Trail - See SNCP.
- 3. Proposed Ferris-Adams Student Trail - See SNCP.
- 4. Extend Ferris-Adams Student Trail as shown.
- 5. Collector Arterial: Palouse Highway with Bicycle Lanes and sidewalks
- 6. Minor Arterial: Regal Street with Bicycle Lanes and Sidewalks.
- 7. Mid-Block Pedestrian and Bicycle Crossing
- 8. Pedestrian Connection through project site/parking lot. Provides connectivity through the site and between buildings within the project.
- 9. Connection to adjacent apartment project.
- 10. Connection to existing sports complex.
- 11. Pathway connection feature.
- 12. Signalized intersection with enhanced pedestrian features.
- 13. Dedicated 12'-16' bicycle and pedestrian path.
- 14. Connectivity to the west via controlled intersection at Regal Street.
- 15. Enhanced crosswalk on southern boundary of four-way intersection.



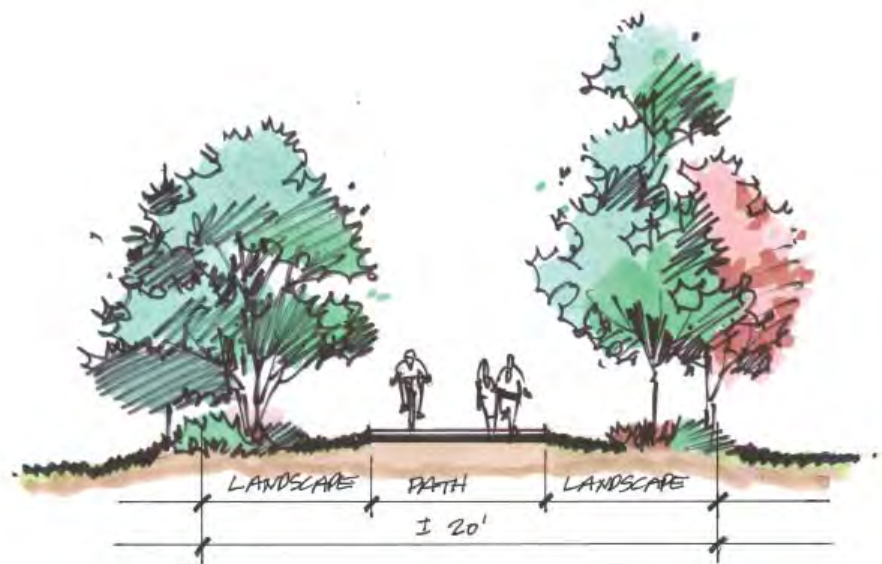
# Part C/Solution: Pedestrian Connection Character



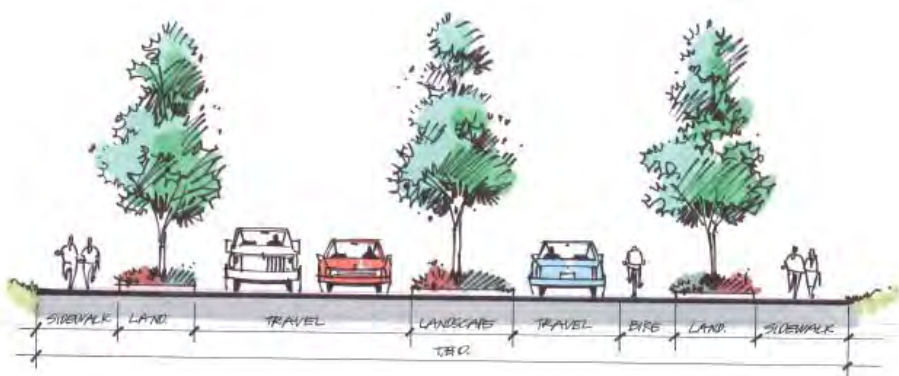
Proposed Palouse Highway Mid-Block Crossing Feature and Future “Urban District” Intersection



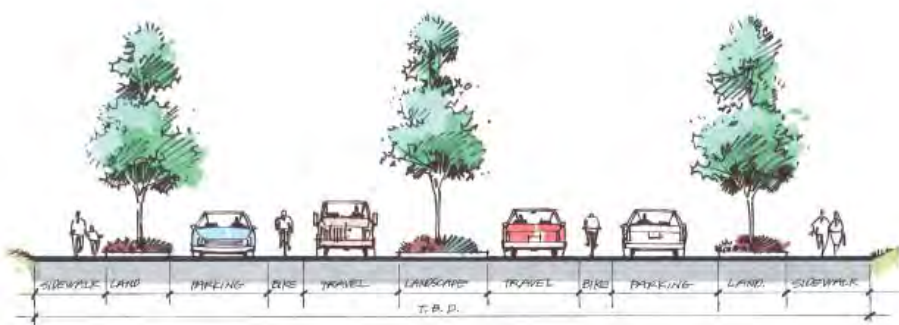
Protected Pedestrian Connection Between Buildings and Through Parking Areas Within Project Sites



Non-Motorized Trail Cross Section



Minor Arterial/Regal Street Cross Section



Collector Arterial/Palouse Highway Cross Section

NOTE: Street cross sections and mid-block crossing subject to revisions based on further Traffic Engineer studies and coordination with City Public Works/Traffic Department.



## Part C/Solution: Tree Preservation

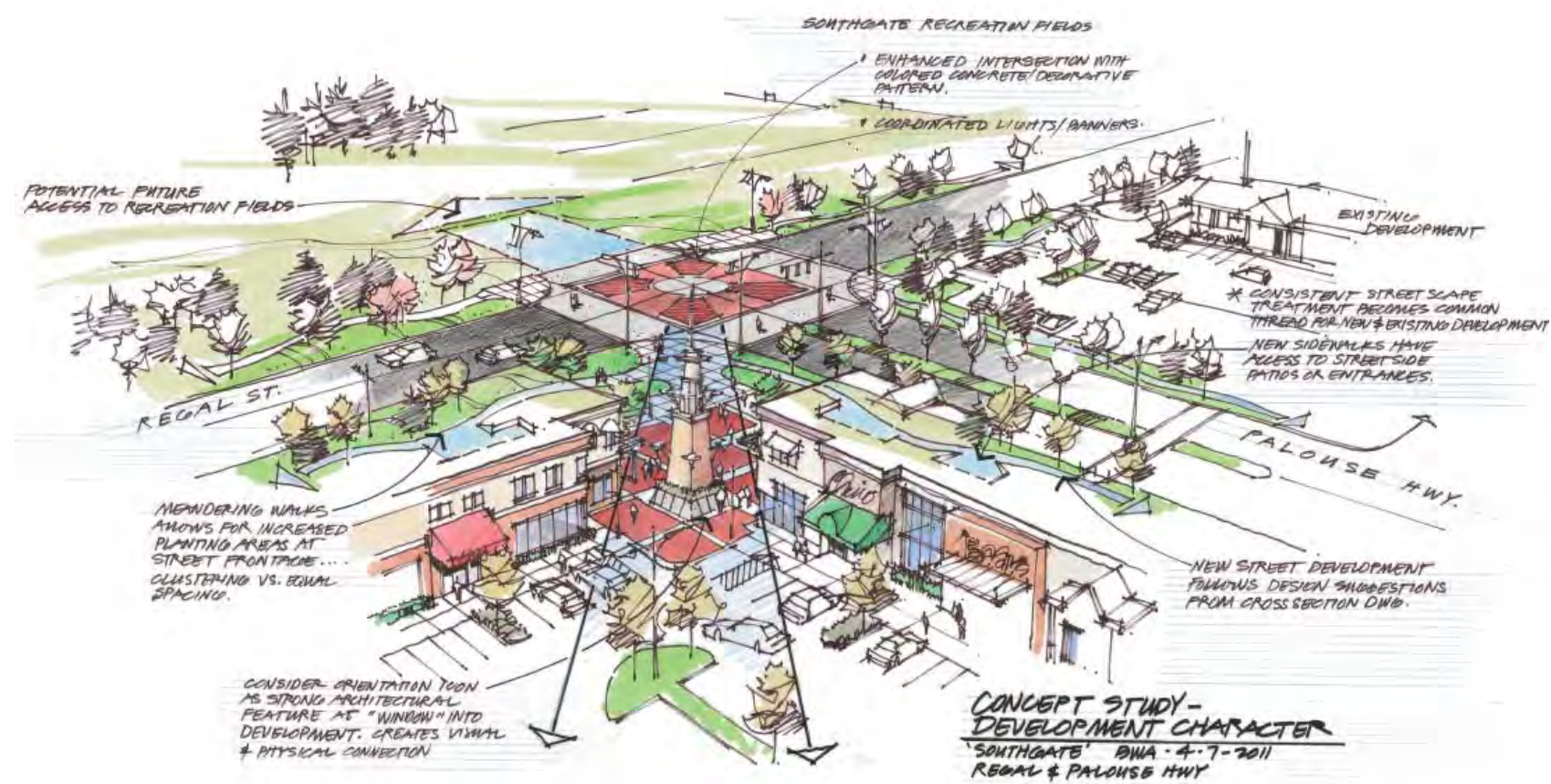


### Narrative:

The Applicants revised proposal includes a detailed tree inventory report that locates trees generally above 1-1/2-inch caliper and evaluates them for health and overall condition. This work was completed by a registered Landscape Architect on our staff with experience doing this kind of work. Based on the location and health of the existing tree inventory, appropriate numbers of healthy trees will be selected to be preserved and integrated into the Project as appropriate.



# Part C/Solution: Design Theme & Kit of Parts



## Narrative:

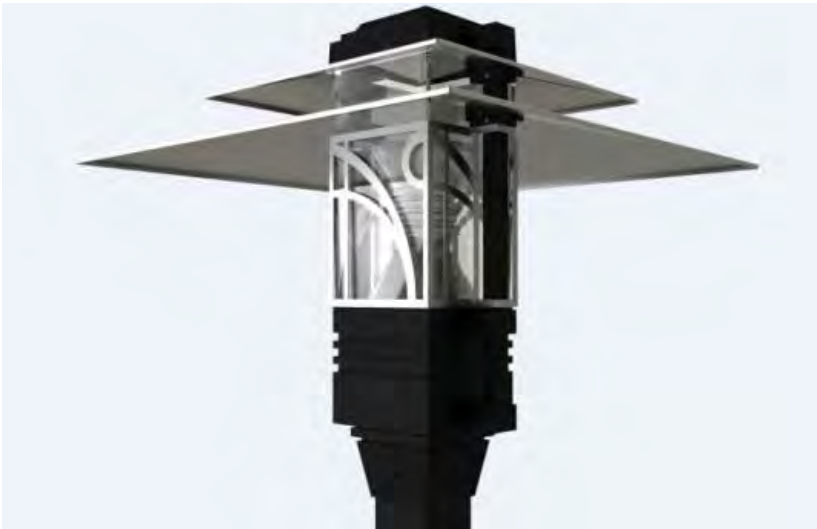
The Applicants revised proposal includes an expanded “kit of parts” that includes lighting (parking area, pedestrian, and accent types), street furniture (benches, trash receptacles, bollards, kiosks, way finding signage), materials and textures (paving patterns and colors), and a landscape material palette to be used throughout the three projects. This strategy allows appropriate variation in the building architecture based on building height, footprint and retail tenant brand and provides consistency at the pedestrian and streetscape level where it provides the most impact. The mitigation of larger buildings is established in the City-Wide Development Standards which address massing, height, blank walls, parapet features, etc.

Additionally, the Applicants acknowledge the benefits of incorporating local artwork where practical.

Prior to issuance of a building permit, an amended Development Agreement shall be executed stating that all future development subject to the ISP agrees to use the Kit of Parts approved for the Black property. The Black property Kit of Parts will be submitted to the Planning Director for review and approval prior to issuance of a building permit.



Part C/Solution: Kit of Parts -Themed



Site furnishings with natural, whimsical character representative of grassland and Palouse.



Craftsman style lighting provides historical significance and reflects the South Hill architectural character.







TallGrass Pattern Seating

Products

Product Name  
TallGrass Pattern Seating - Bronze Color

Manufacturer  
SiteScapes Inc. or equal  
P.O. Box 22326  
Lincoln, NE 68542  
Phone: 888.331.9464  
Fax: 402.421.9479  
[www.sitescapesonline.com](http://www.sitescapesonline.com)  
[info@sitescapesonline.com](mailto:info@sitescapesonline.com)

Materials  
Frame - 1 1/2" Sq. x 11 Ga. Wall Steel Tubing  
  
Panels - 7 Ga. Steel Sheet Metal

Foot Plates: Foot plates are 5/16" x 1 1/2" stainless steel. Each plate has a 9/16" hole for mounting.

Grass Pattern Receptacles

Products

Product Name  
TallGrass Grass Pattern Receptacles - Bronze Color

Manufacturer  
SiteScapes Inc. or equal  
P.O. Box 22326  
Lincoln, NE 68542  
Phone: 888.331.9464  
Fax: 402.421.9479  
[www.sitescapesonline.com](http://www.sitescapesonline.com)  
[info@sitescapesonline.com](mailto:info@sitescapesonline.com)

Materials  
Frame : 7 Gauge Steel Shell/Stainless Steel  
  
Lid: Removable Dome lid  
  
Foot Plates: Foot plates are 5/16" x 1 1/2" stainless steel. Each plate has a 9/16" hole for mounting.  
  
Ash Inlay: 13ga. Stainless steel ash pan  
  
Liner: High Density Polyethylene liner with handles



TallGrass  
TallGrass Pattern Seating

TallGrass Seating  
A variation of the JordanCreek, the TallGrass Collection utilizes a rigid square tubing framework while presenting an elegant silhouette accent. The TallGrass products accentuate their natural surroundings while blending into a variety of urban settings.

Seating Mounting  
Options include stainless steel mounting plates for permanent mounting to a concrete pad or heavy duty stainless steel leveling feet suitable for indoor or outdoor use.

Materials  
Frame - 1 1/2" Sq. x 11 Ga. Wall Steel Tubing  
Panels - 7 Ga. Steel Sheet Metal  
Foot Plates - 1/4" x 1 1/2" Stainless Steel with 9/16" Mounting Holes  
Mounted w/ four 1/2" x 4-5" Stainless Steel Anchor Bolts (Customer Supplied)

Sustainability and LEED  
TallGrass benches have a recycled material content of 84% of which 73% is post consumer content. This content may vary based on the product design, product material type, and interchangeable piece parts. All styles are 100% recyclable. For more information about SiteScapes sustainable products and policies, please refer to our environmental statement.

Choices  
Choose six or eight foot length; backed or backless bench; zero or one center armrest; powdercoat or Duracoat finish; and color.

TallGrass seating options



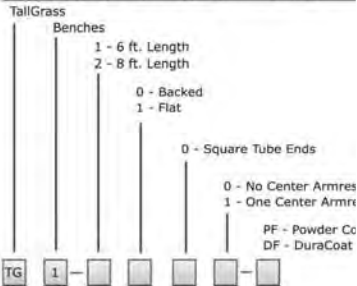
888.331.9464 402.421.9479 fax  
P.O. Box 22326 Lincoln, NE 68542  
[www.sitescapesonline.com](http://www.sitescapesonline.com)  
[info@sitescapesonline.com](mailto:info@sitescapesonline.com)

Pictured this page  
left: TallGrass backed TallGrass pattern bench TG1-1000 (Sterling). top right: TallGrass backed TallGrass pattern bench TG1-1000 (Bronze). bottom right: TallGrass backed TallGrass pattern bench TG1-1000 (Onyx).

Finishes  
SiteScapes offers product in both standard powdercoat and its own proprietary Duracoat finish. Both finishes are designed to weather elements and are guaranteed not to rust, chip, peel, or fade. Call for standard color chart.

[www.sitescapesonline.com](http://www.sitescapesonline.com)  
For more information visit our website where you can download product drawings, photos, product brochure, CSI specs, and other technical data.

To Specify TallGrass Benches





Kit of Parts—Themed Pedestrian Lighting  
Southgate Integrated Site Plan



Parkway Square Pedes-  
trian Light

Products

**Product Name**  
Parkway Square PKWM or equal.  
Dark Bronze Color  
Decorative Screen—DSWH Wheat  
Hood Finish—STS Stainless Steel

**Manufacturer**  
ARCHITECTURAL AREA LIGHTING  
16555 East Gale Ave. | City of Industry |  
CA 91745  
P 626.968.5666 | F 626.369.2695 |  
www.aal.net

PKWM

Parkway Square™ Pedestrian (Medium) Scale

NEW



wiHUBB

ida

DESIGNER  
SSL

PERIOD

FEATURES

- Four optical systems: horizontal, LED, vertical, and indirect
- Three hood styles and six decorative screen options
- Features highly efficient, exclusive MicroEmitter™ technology
  - Wireless control system for 0-10VDC full range dimming control
  - Programmable autonomous operation
- Features exclusive wiHUBB technology
  - Wireless control system for 0-10VDC full range dimming control
  - Programmable autonomous operation

- Custom mounting options
- Cast aluminum struts
- Full cutoff options
- IP65 rated for horizontal configurations, and IP66 rated for vertical configurations
- Powder coat finish in 13 standard colors with a polymer primer sealer

ORDERING INFORMATION

PKWM

MODEL

Slips over 4" open top square pole or 2 3/8" tenon.

PKWM Parkway Square Medium Scale

HOODS

ANG

Angled hood

STR

Straight hood

IND

Indirect hood

DBL

Double hood

OPTICS

Horizontal LED - Available with ANG hood and LEDs only. Flat glass lens.

T2

IES Type 1 distribution

T3

IES Type 3 distribution

T4

IES Type 4 distribution

T5

IES Type 5 distribution

Vertical LED

VL3

Asymmetric distribution

VL5

Symmetric distribution

Horizontal Optical System

Available with ANG hood only. Not available with LEDs. Flat glass lens.

H2

IES Type 2 distribution

H3

IES Type 3 distribution

H4

IES Type 4 distribution

H5

IES Type 5 distribution

Vertical Optical System - Not available with IND or LEDs. Four sided clear acrylic lens.

GLA

Frosted glass diffuser

GR3

IES Type 3 glass refractor

GR5

IES Type 5 glass refractor

Indirect Optical System- Available with IND hood only. Not available with LEDs. Four sided clear acrylic lens. Underside of hood is painted in high reflectance white.

-3

IES Type 3 distribution

-5

IES Type 5 distribution

Bare Lamp - Available with IL system only. Lightly diffused four sided acrylic lens.

BLO

For IL only

LAMP/BALLAST

55LED-BW\*

42 light emitting diode array. Bright white (5100K). VL3 & VL5 only.

60LED-WW

MicroEmitter™ 60 light emitting diode array. Warm white (3500K). Class 1, 120 thru 277 volt. Horizontal LED only.

60LED-BW

MicroEmitter™ 60 light emitting diode array. Bright white (5100K). Class 1, 120 thru 277 volt. Horizontal LED only.

70MH

70 watt metal halide 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

70MHT6

70 watt metal halide 120/277/347 volt ballast. Use G12 base, T6 ceramic lamp.

70MHT6EB

70 watt electronic metal halide 120 thru 277 volt ballast. Use G12 base, T6 ceramic lamp.

100MH

100 watt metal halide 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

100MHEB

100 watt electronic metal halide 120 thru 277 volt ballast. Use medium base, ED-17 lamp.

150PSMH

Pulse start 150 watt metal halide 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

150PSMHT6

Pulse start 150 watt metal halide 120/277 volt ballast. Use G12 base, T6 ceramic lamp.

150MHEB

150 watt electronic metal halide 120 or 277 volt ballast. Use medium base, ED-17 lamp.

150MHT6EB

150 watt electronic metal halide 120 or 277 volt ballast. Use G12 base, T6 ceramic lamp.

70HPS

70 watt high pressure sodium 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

100HPS

100 watt high pressure sodium 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

150HPS

150 watt high pressure sodium 120/208/240/277 volt ballast. Use medium base, ED-17 lamp.

IL85

85 watt induction lamp system. 120, 208, 240 or 277 volt. -25°C min. start temp. Available in bare lamp configuration (BLO) with LDL lens only.

CF\*\*

Electronic 120 thru 277 volt ballast. Use GX24q 4 pin base, 26, 32 or 42 watt lamp. -18°C min start temp. Specify wattage.

140CO\*\*

140 watt electronic CosmoPolis™ 120 or 208 thru 277 volt ballast. Use PG212 base, CosmoPolis™ lamp.

DECORATIVE SCREENS (OPTIONAL)

WIH-IM

In fixture wireless control module, PS68 pg381. For LED only.

ULS

Upper lens shield above the hood. Reduces light above hood.

TEN

2-3/8" O.D. x 4" long tenon. Required for all AAL poles, except for PS4.

TA23

Tenon adaptor slips over a 2-3/8" O.D. x 4" long tenon. Required for poles by others or used in conjunction with the TEN pole option by AAL.

LDL

Lightly diffused lens

347

120/240/347 volt ballast for HID only, except 50MH and 50HPS.

COP

Brushed copper hood

STS

Stainless steel hood

SSP

Screen in brushed stainless steel

CPR

Screen in brushed copper

COLORS

AWT

Arctic White

BLK

Black

MTB

Matte Black

DGN

Dark Green

DBZ

Dark Bronze

WRZ

Weathered Bronze

BRM

Metallic Bronze

VBL

Verde Blue

CRT

Corten

MAL

Matte Aluminum

MDG

Medium Grey

ATG

Antique Green

LGY

Light Grey

RAL/ PREMIUM COLOR

Provide a RAL 4 digit color number

CUSTOM COLOR

Please provide a color chip for matching

OPTIONS

WIH-IM

In fixture wireless control module, PS68 pg381. For LED only.

ULS

Upper lens shield above the hood. Reduces light above hood.

TEN

2-3/8" O.D. x 4" long tenon. Required for all AAL poles, except for PS4.

TA23

Tenon adaptor slips over a 2-3/8" O.D. x 4" long tenon. Required for poles by others or used in conjunction with the TEN pole option by AAL.

LDL

Lightly diffused lens

347

120/240/347 volt ballast for HID only, except 50MH and 50HPS.

COP

Brushed copper hood

STS

Stainless steel hood

SSP

Screen in brushed stainless steel

CPR

Screen in brushed copper

MOUNTING

Wall Mount - Mounting plate included. Hardware by others.

PSWM-TU

Fixture above with arm mounted up.

PSWM-TD

Fixture above with arm mounted down.

PSWM-BU

Fixture below with arm mounted up. Not available with indirect.

PSWM-BD

Fixture below with arm mounted down. Not available with indirect.

Pole Mount - Arm mounts to side of a 4" O.D. pole.

PSA-TU

Fixture above with arm mounted up.

PSA-TD

Fixture above with arm mounted down.

PSA-BU

Fixture below with arm mounted up. Not available with indirect.

PSA-BD

Fixture below with arm mounted down. Not available with indirect.

DIMENSIONS

MEDIUM

29.75" SQ x 30.5" H - Vertical

23" SQ x 30.5" H - Horizontal

Southgate Integrated Site Plan

Regal Street & Palouse Highway | Revised Final Submittal | May 31, 2013

BWA

BERNARDO | WILLS

ARCHITECTS PC

13.2



Kit of Parts—Parking Lot Area Lighting  
Southgate Integrated Site Plan



American—Metal Halide Area Light

Products


Product Name



American Shoe Box Cube—Bronze  
Metal Halide Lamp

Manufacturer

[Visionaire Lighting, LLC](#) | Tel. (877) 977-LITE(5483)  
19645 Rancho Way, Rancho Dominguez, CA 90220

American HID



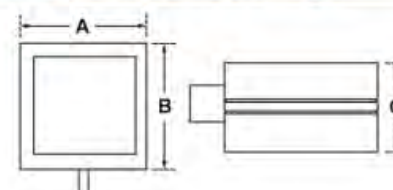


Project Name:

Catalog Number:

Type:

Dimensional Drawings








Fixture	A	B	C	Stripe	Max. Watts	Lbs
AME-1	14"	14"	10"	2"	175 W	35
AME-2	19"	19"	12"	2"	400 W	50
AME-3	23"	23"	13"	2"	1000 W	70
AME-4	23"	23"	15"	2"	1000 W	75

The American series features the most advanced lighting reflector system available today. Vision™ is a patented, revolutionary reflector system unlike any other. The flat lens, vertical lamp, IES full cutoff luminaire is Dark-Sky certified to restrict light trespass, glare and light pollution for neighborhood-friendly outdoor lighting. Convex glass lens is available when required.

The clean, compact housing style features a decorative reveal available in complementary or contrasting colors. Computerized precision machinery and quality materials ensure manufacturing to the highest industry standards.


American is offered in four enclosure sizes and five distribution patterns, including a special forward throw T4A reflector for auto dealership front line lighting. Vision™ Reflector System allows the use of fewer fixtures and poles with wider spacings, providing substantial equipment, installation and energy cost savings.

American is a proven performer for auto dealerships, shopping centers, parking lots and general area lighting. A wide selection of light sources from 100 through 1000 watts are offered in Metal Halide, High Pressure Sodium and Pulse Start Metal Halide lamp which provides excellent efficiency, lumen maintenance and color.

Model	Optics	Wattage	Source	Voltage	Mounting	Finish/Stripe	Options
AME-1 Flat Glass Only	Type II (T2) 	100 (100) 150 (150) 175 (175)	PS HPS PS HPS PS (P) (S)	120 (1) 208 (2)	Bolt-On Arm 6" (BOA6)	Bronze (BZ)	Photocell & Receptacle *Specify voltage (PCR120) (PCR208) (PCR240) (PCR277) (PCR480) Photo Receptacle (PER) *With shorting cap Button Type Photocell *Specify voltage (PC120) (PC208) (PC240) (PC277)
AME-2 Flat Glass (Convex Glass Optional)	Type III (T3)  Type IV (T4) 	200 (200) 250 (250) 320 (320) 400 (400) *Reduced envelope lamp on 400 W	PS PS HPS PS PS HPS (P) (S)	240 (3) 277 (4)	Bolt-On Arm 10" *Supplied for all AME sizes with D92, T30 & Quad Mounting (BOA10) Spider Mount *Fits 2 1/2" O.D. (SM)	Black (BK) White (WH) Forest Green (FG) Grey (GY)	Quartz Restrike (QR) Fusing *Specify voltage Single in-line fuse (SF120) (SF277) Double in-line fuse (DF208) (DF240) (DF480) House Side Light Shield (LS)
AME-3 Convex Glass Only	Type IV-A (T4A)  *Not available for AME-1 & 2	400 (400) 750 (750) 1000 (1000) *Reduced envelope lamp on 1000 W	PS HPS PS HPS MH PS HPS (M) (P) (S)	480 (5) M-Tap *Multi-Tap ballast wired at 277 V unless specified (6)	Wall Mount *Cast Wall Plate (BAWP) is to be ordered separately. Standard length for WM bolt-on arm is 6" (WM)	Silver Metallic (SL) Custom Color (CC)	Internal Light Shield *AME-2 & AME-4 only (ILS) Convex Glass Lens *AME-2 only (VLCG) Round Pole Plate Adaptor (RPP) Cast Wall Plate (BAWP)
AME-4 Flat Glass Only	Type V (T5) 	575 (575) 750 (750) 775 (775) 1000 (1000) *Reduced envelope lamp on 1000 W	PS HPS PS HPS PS HPS MH PS HPS (M) (P) (S)	347 (8)	AME-1 uses 4" tall arm. Round Pole Plate Adaptors (RPP) are to be ordered separately.	Stripe Color Blue Metallic (94) Red Opaque (188) Silver Metallic (58) Gold Metallic (41) White Opaque (50) Black Metallic (60) Charcoal Metallic (20)	

MH - Metal Halide  
PS - Pulse Start Metal Halide  
HPS - High Pressure Sodium

For more detailed information on mounting, wiring or installation instructions, please consult factory. If poles are not listed with fixtures, please specify mounting requirements. This document contains proprietary information of Visionaire Lighting, LLC. Any use of this information requires the written approval of Visionaire Lighting, LLC. In keeping with our TCM policy of continuous improvement, Visionaire reserves the right to change any specifications contained herein without prior notice.

**VISIONAIRE LIGHTING**  
Performance In A Whole New Light™



Bike Garden Bike Rack

Products

Product Name

Bike Garden - Dark Bronze  
Surface Mount

Manufacturer

FORMS+SURFACES 800.451.0410 <http://www.forms-surfaces.com/> or equal



BIKE GARDEN™ BIKE RACK



PRODUCT DATA

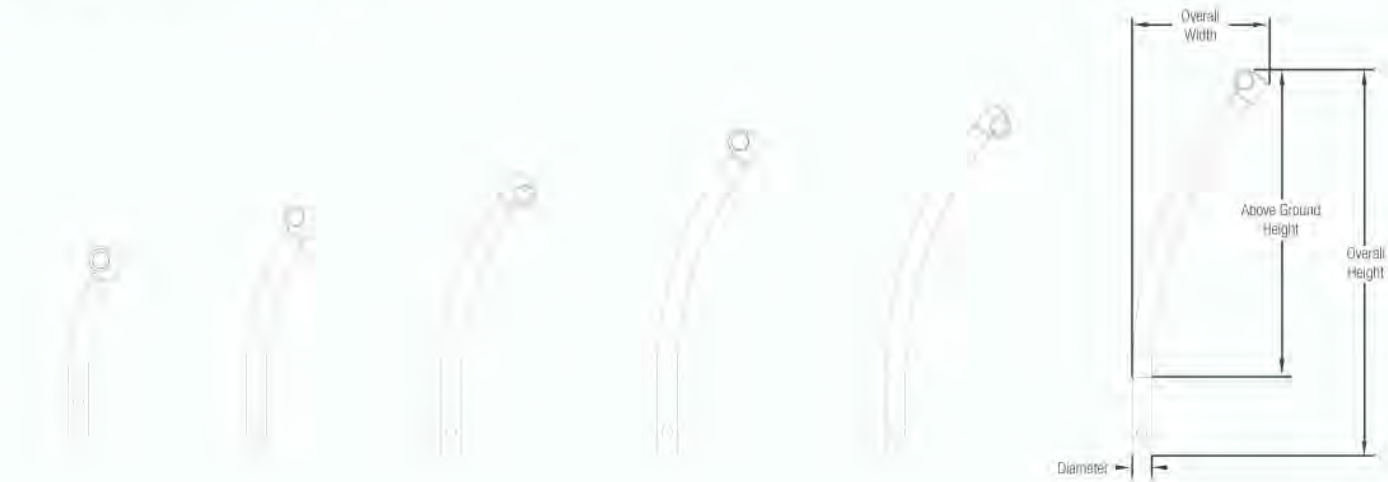
Inspired by organic forms, the **Bike Garden** provides a truly unique solution to the increasing challenges of bike parking and security. Its “stems” can be arranged in a wide variety of configurations to creatively accommodate almost any setting and provide riders with the added assurance of multiple locking points to secure the frame and wheels. Constructed entirely of rugged, corrosion-resistant stainless steel, Bike Garden’s stems can be surface mounted or cast-in-place and may be purchased individually for maximum arrangement flexibility or in pre-configured layouts.

MATERIALS & FINISHES

MATERIALS	FINISH	INSTALLATION	MAINTENANCE
<ul style="list-style-type: none"><li>Constructed entirely of corrosion-resistant stainless steel.</li><li>Head and optional surface mount foot are cast stainless steel; body is stainless steel tubing.</li></ul>	<ul style="list-style-type: none"><li>Available in stainless steel with a radial Satin finish or powdercoated.</li><li>Standard powdercoat colors are Aluminum Texture and Slate Texture; optional colors from the F+S color chart and custom RAL colors are available for an upcharge.</li><li>Due to the inherent nature of metal castings, gloss powdercoats are not offered for cast components.</li></ul>	<ul style="list-style-type: none"><li>Bike Garden can be cast-in-place or surface mounted. Anchors and stainless steel screws are included for surface mount.</li></ul>	<ul style="list-style-type: none"><li>Metal surfaces can be cleaned as needed using a soft cloth or brush with warm water and a mild detergent. Avoid abrasive cleaners.</li></ul>

INSTALLATION & MAINTENANCE

NOMINAL DIMENSIONS (Cast-in-place)



MODEL	ABOVE GROUND HEIGHT	OVERALL HEIGHT	OVERALL WIDTH	DIAMETER	WEIGHT
SKGAR-162-CIP	16.2" (411 mm)	26.2" (665 mm)	7.1" (180 mm)	2.5" (63.5 mm)	8.6 lbs (3.9 kg)
SKGAR-208-CIP	20.8" (528 mm)	30.8" (782 mm)	9.2" (234 mm)	2.5" (63.5 mm)	9.8 lbs (4.4 kg)
SKGAR-254-CIP	25.3" (643 mm)	35.3" (897 mm)	11.8" (300 mm)	2.5" (63.5 mm)	11.3 lbs (5.1 kg)
SKGAR-300-CIP	29.9" (759 mm)	39.9" (1,013 mm)	13.3" (338 mm)	2.5" (63.5 mm)	12.7 lbs (5.8 kg)
SKGAR-344-CIP	34.4" (874 mm)	44.4" (1,128 mm)	15.8" (401 mm)	2.5" (63.5 mm)	14.1 lbs (6.4 kg)
SKGAR-391-CIP	39.1" (993 mm)	49.1" (1,247 mm)	17.3" (439 mm)	2.5" (63.5 mm)	15.4 lbs (7.0 kg)



Kit of Parts—Hardscape Treatment—Paver Accents  
Southgate Integrated Site Plan



Pacific Slate Finish-  
Classic Standard Series  
Products

**Standard**  
Length: 8-7/8" (225mm)  
Width: 4-7/16" (112.5mm)  
Area: 3.7 stones /ft2  
40 stones /m2  
Thickness: 2-3/8" (60mm)



**Double Standard**  
Length: 8-7/8" (225mm)  
Width: 8-7/8" (225mm)  
Area: 1.8 stones /ft2  
20 stones /m2  
Thickness: 2-3/8" (60mm)



**Half Standard**  
Length: 4-7/16" (112.5mm)  
Width: 4-7/16" (112.5mm)  
Area: 7.3 stones /ft2  
80 stones /m2Thickness:



Concrete Pavers  
Products

**Product Name**  
Pacific Slate  
Color : Red, Desert Sand and Sand/Brown  
Blend or other as appropriate

**Compressive strength:** 8000 PSI (55 MPA)  
**Absorption:** shall not exceed 5%  
**Weight:** 1- 5/8" (40mm): approx. 20lbs/ft2 (98kg/m2)  
2" (50mm): approx. 25lbs/ft2 (122kg/m2)  
2- 3/8" (60mm): approx. 28lbs/ft2 (140kg/m2)  
3- 1/8" (80mm): approx. 37lbs/ft2 (188kg/m2)

**Manufacturer**  
ABBOTSFORD CONCRETE PRODUCTS or equal  
Pavers are manufactured to ASTM C 936-09 and  
CSA A231.2-06 specifications,





Kit of Parts—Hardscape Treatment—Colored Concrete Accents  
Southgate Integrated Site Plan

Colored Concrete - Textured/  
Stamped  
Products

**Product Name**  
Integral Color Concrete  
Color : Scofield Colored Products, Roman Clay, Pueblo Brown  
and Schooner Beige or other as appropriate

**CHROMIX® L Admixtures for Color-Conditioned® Concrete**  
CHROMIX L Admixtures for Color-Conditioned® Concrete are formu-  
lated for use with the CHROMIX®-It Liquid Color Dispensers. High-  
quality CHROMIX L Admixtures are mixed liquid dispersions developed  
exclusively for use in concrete and provide consistent, permanent, fade-  
resistant and streak-free integral color conditioning for all types of concrete  
projects.

Accent concrete shall be textured by sandblasting or stamped  
texture where occurring

**Integral Color SG Standard Grade:**  
**Manufacturer**  
L. M. Scofield Company or equal  
6533 Bandini Blvd.  
Los Angeles, CA 90040  
(800) 800-9900





## Part C/Solution: Kit of Parts - Landscaping



Sustainable landscaping used to accentuate trail/walk crossings through parking lot.



Use of plant color to create consistency and uniformity.



Below: Example of plaza/seawall used with drought-resistant plantings and natural materials.



Native, drought-resistant plantings used to enhance nodes along multi-modal trail.



Sustainable Landscapes: Combination of native and adaptive shrubs used with ornamental grasses.



# Part C/Solution: Kit of Parts - Landscape Tree Plantings



## Narrative:

The landscape plantings have been selected to create ‘areas of enhancement’ that will utilize a variety of color, size and texture in the plant selections. These areas are located at selected site entrances, common areas, landscape buffer strips as well as key areas in the parking lots and along pedestrian routes. Medium to large canopy, native and non-native adapted shade trees will buffer prevailing winds, define special enclosure, provide seasonal interest and summer shade.

A small group of Ponderosa Pines will be preserved at the east edge of the property and additional Ponderosa Pines will be added at the property perimeter where the formal on-site landscaping transitions to more natural and existing off-site vegetation and taller, more intense buffering is appropriate .

Shrub plantings will also consist of native and adapted plant selections with an emphasis on native and ornamental grasses. The tall, native grasses are an important part of theming within the site design and are used to emulate the grassy landscape of the Palouse. Additionally, the landscape design will be closely coordinated with the architectural features and site design to enhance the overall integrated aesthetics. Lawn areas will be seeded or sodded and a mix utilizing native fescue grasses to provide a sustainable drought resistant turf.

The landscape and irrigation design uses the principles of xeriscape with the intent of reducing maintenance and water use. These landscape areas are intended to be mostly self-sustaining, utilizing drip emitter irrigation within the shrub areas with smart control technology and flow meter options to allow the water savings benefit of the equipment to be optimized.

## Legend:

- 1. Autumn Blaze Maple
- 2. Ponderosa Pine
- 3. Pyrus “Chanticleer”
- 4. Japanese Lilac
- 5. Patmore Green Ash
- 6. Black Pine
- 7. Prunus Sargentii
- 8. Amur Maackie
- 9. Northern Red Oak
- 10. Moonglow Juniper



## Part C/Solution: Kit of Parts - Landscape Plantings



### Legend:

1. Abbotswood Potentilla
2. Northern Lights Azalea
3. Blue Shage Eastern White Pine
4. HamIn Fountain Grass
5. Arctic Fire Red Osier Dogwood
6. Creeping Mahonia and Oat Grass
7. Flame Grass
8. Rose Nutkana
9. Mugo Pine
10. Spiraea Japonica Shirobana
11. Karl Foerster Grass
12. Pioneer Rhododendron



# Part C/Solution: Community Plaza Location Options

## Narrative:

Three locations within the District were considered as potential locations for the Community Plaza:

- 1

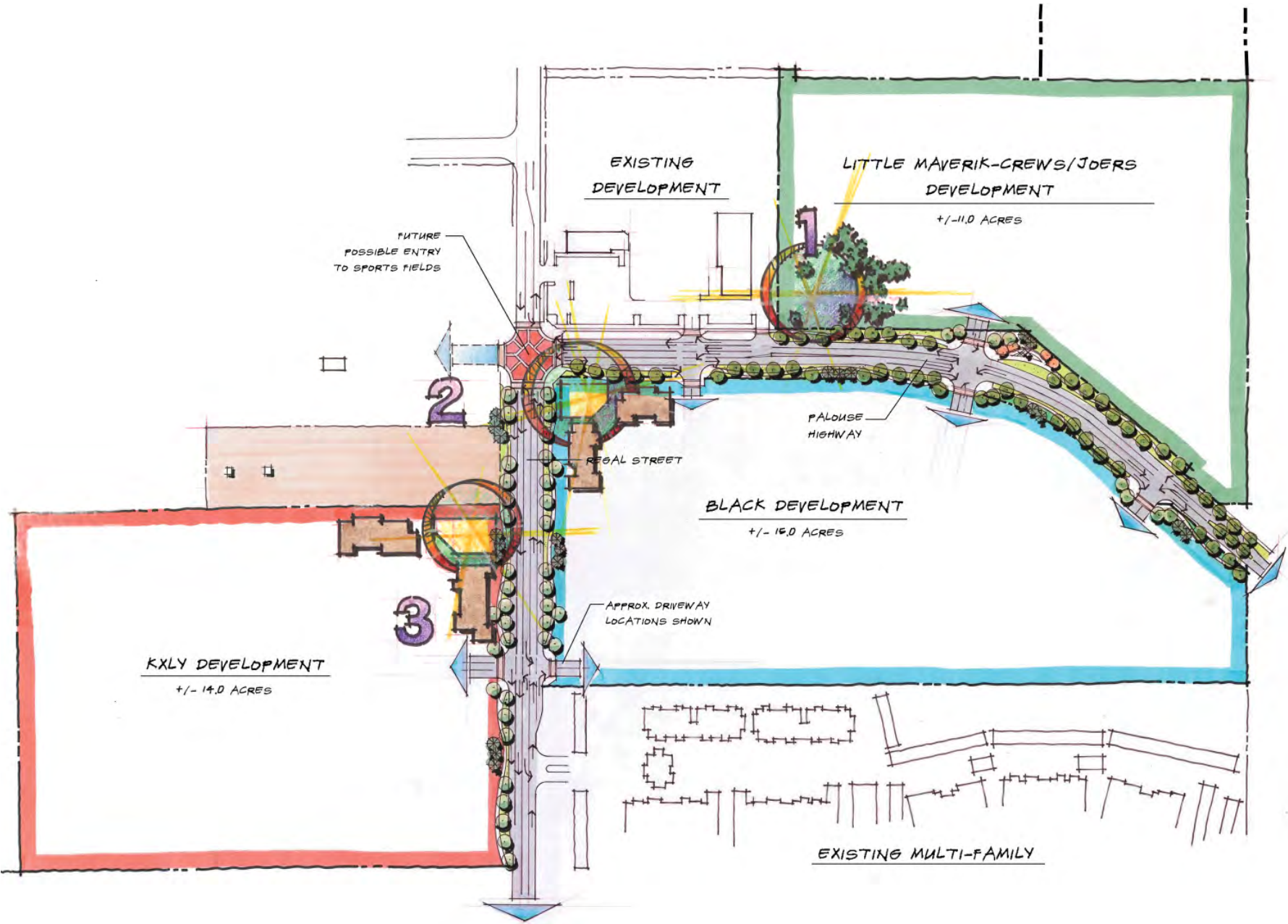
Park-Like Plaza Surrounded by Existing Trees: This location would provide for a more passive/rural setting for a community gathering area at about the scale of a small neighborhood park. Good views to distant landmarks to the north and south, however the Plaza would be more quiet and passive than other locations.
- 2

Urban Plaza in Conjunction With Retail: This location would be developed as more of an urban plaza or public square. It would transition well to project elements, possibly having similar furniture and hardscape treatments. This would be a busier space more affected by traffic noise and movement. Provides adequate views to distant landmarks in all directions.
- 3

Urban Plaza in Conjunction With Retail: This location has the potential to include both rural and urban character. Its adjacency to playfields and open space at north and west would allow for activities associated with active sports. Its proximity to project development character would allow for similar materials, furniture, and character of streetscape to be used for continuity. Good to adequate views of distant landmarks.

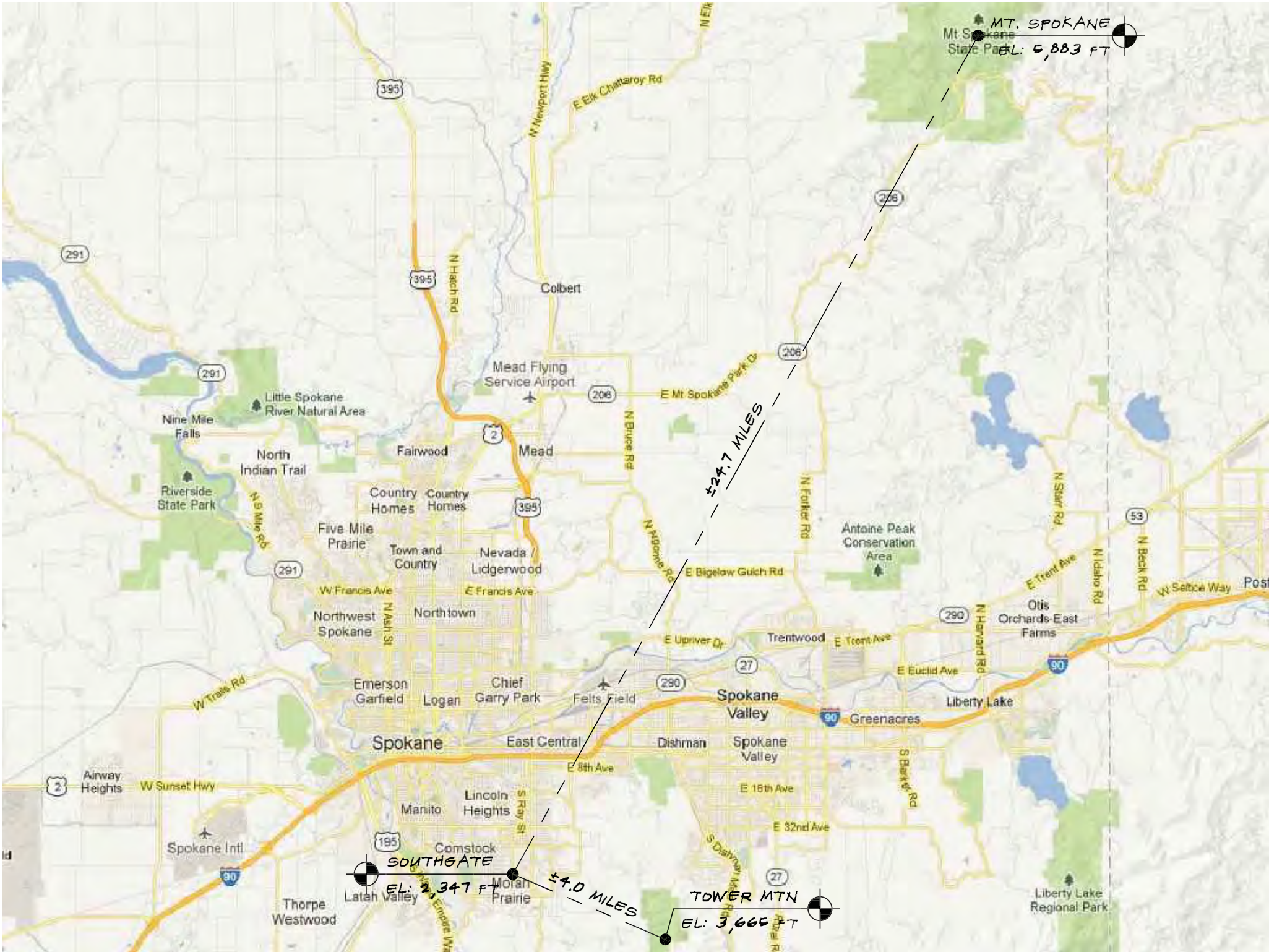
## Summary:

- The Applicant's preferred location is Site 2 for the following reasons:
- Situated on parcel likely to be developed first.
  - Provides amenity for the project as a "draw" and feature as well as having the retail uses "feed" the plaza and provide necessary activity and life to an urban plaza.
  - Visibility from key intersection provides security, sense of place, and an iconic element for the District.
  - Most easily funded as part of first development project in District.





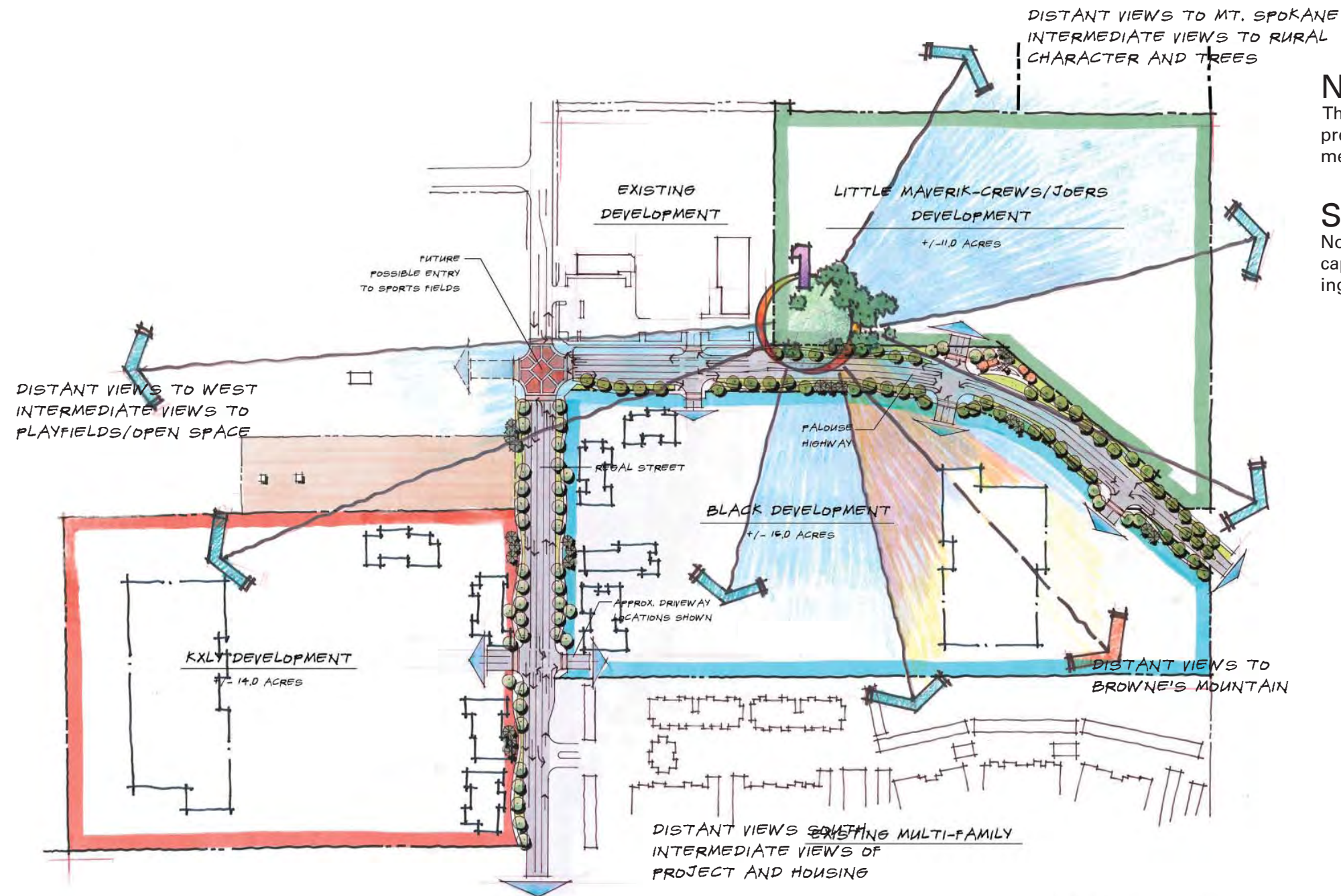
Part C/Solution: Viewscape



**Narrative:**  
This map illustrates the relationship between the identified viewscape points of Mt. Spokane and Browne's/Tower Mountain both in distance and elevation.



## Part C/Solution: Viewscape - Plaza Location Option 1



### Narrative:

The three potential Community Plaza locations were evaluated for view preservation and each site offered advantages and disadvantages when measured against each other.

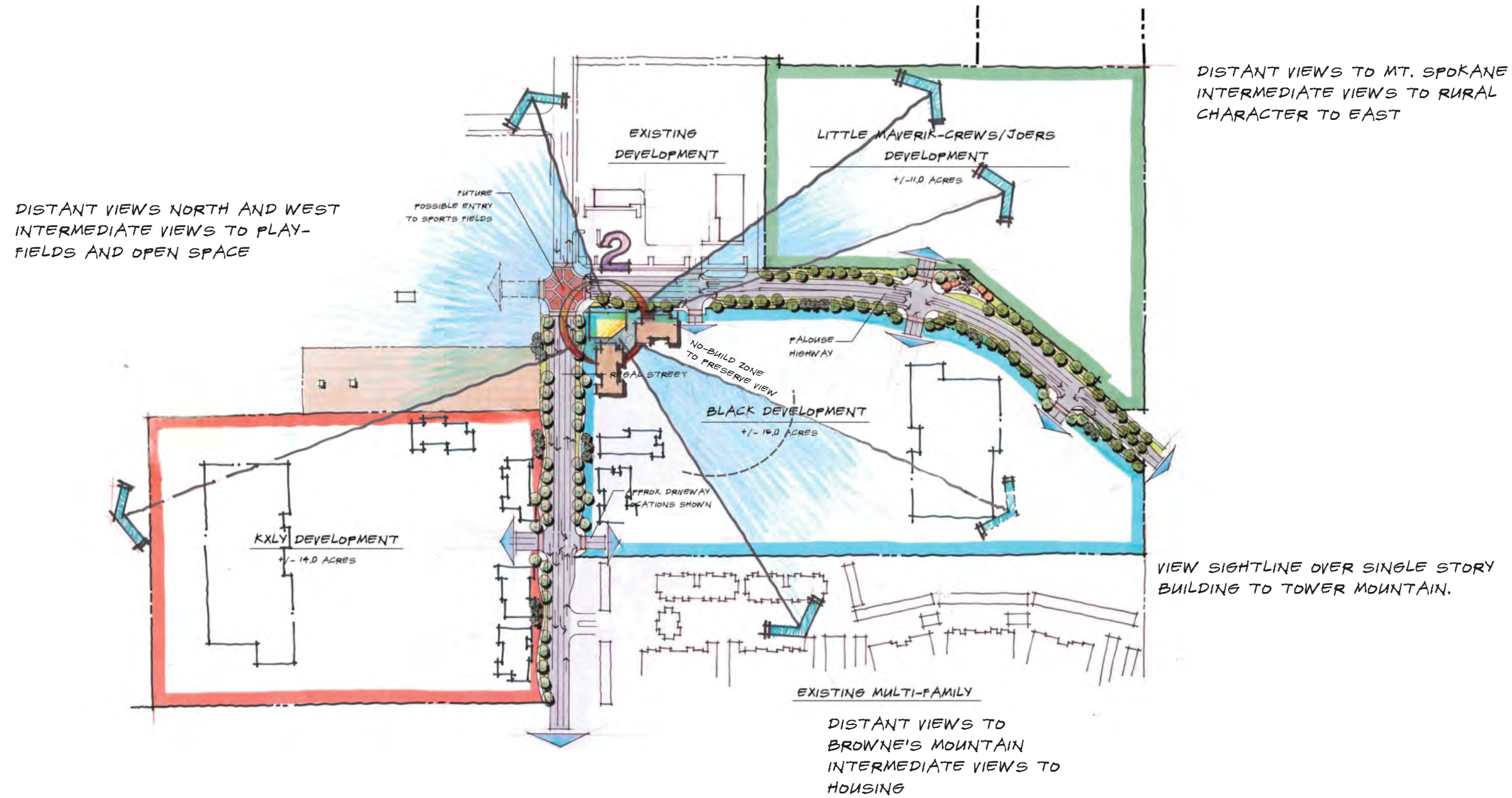
### Summary:

None of the three potential locations had a significantly better viewscape than the others and the viewscape criterion was neutral in selecting the preferred Community Plaza location.

NOTE: Access points and street development as shown are conceptual only.



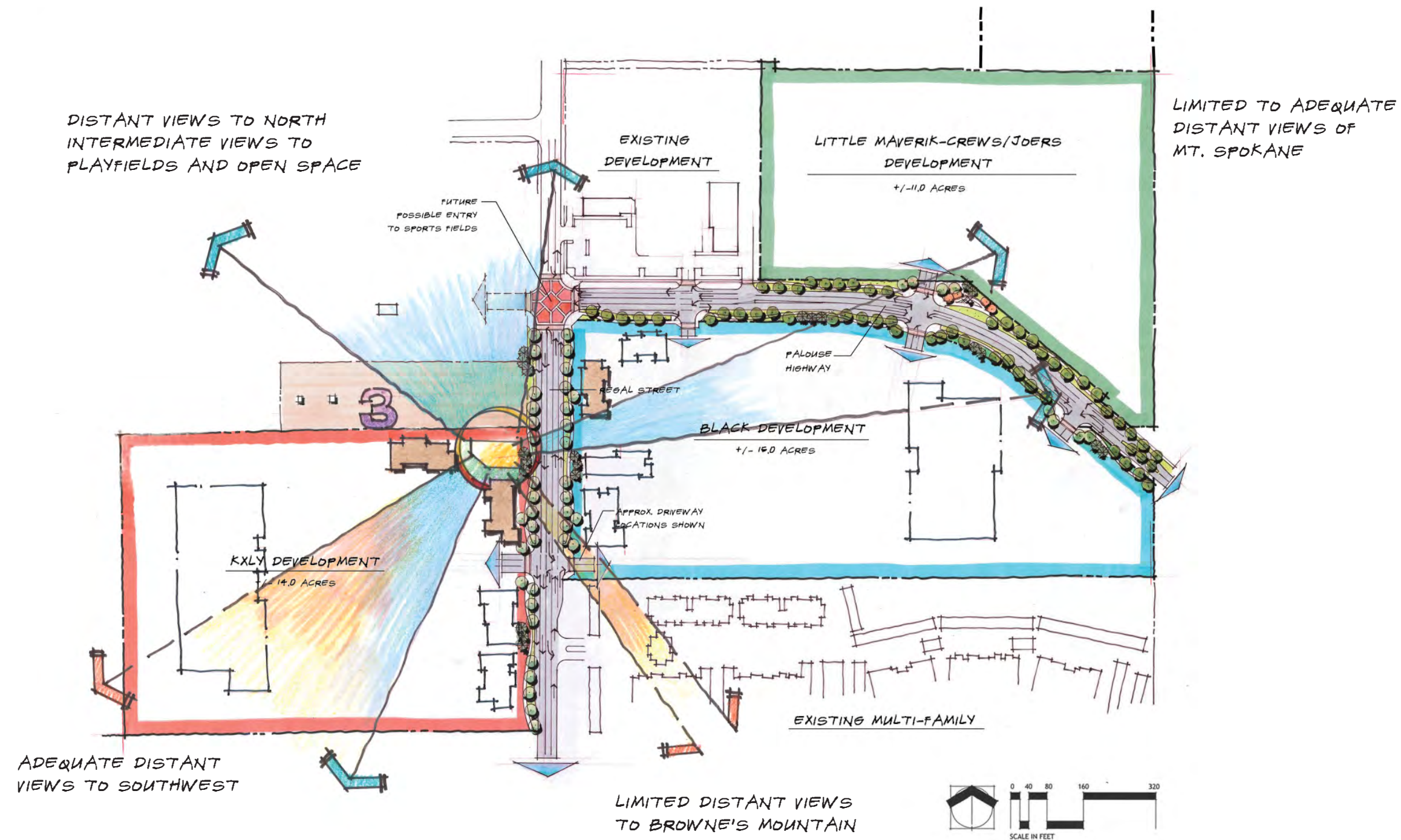
Part C/Solution: Viewscape - Plaza Location Option 2



NOTE: Access points and street development as shown are conceptual only.



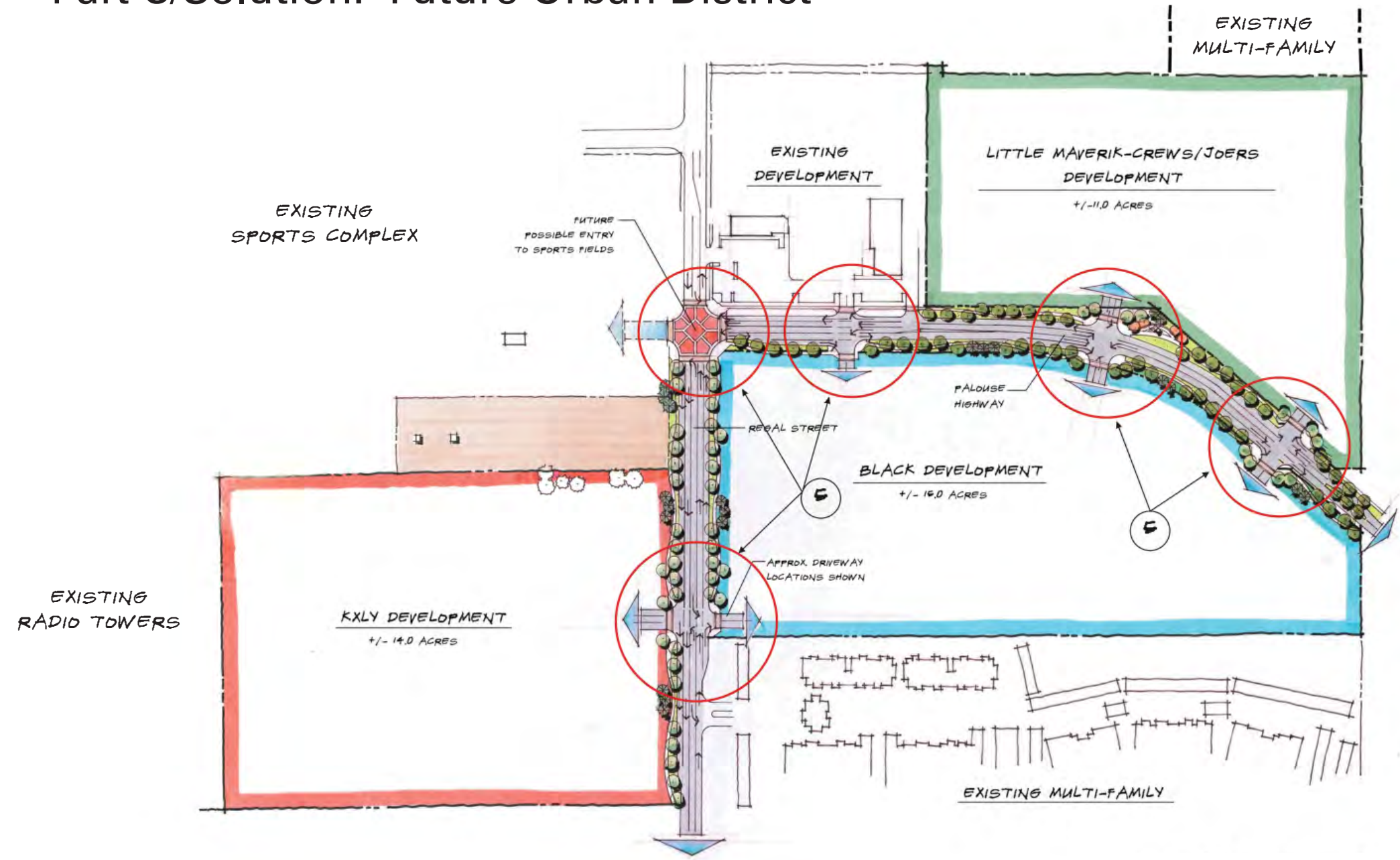
## Part C/Solution: Viewscape - Plaza Location Option 3



NOTE: Access points and street development as shown are conceptual only.



# Part C/Solution: Future Urban District



## DEVELOPMENT AGREEMENT REQUIREMENT:

5.6 Long-Term Development of Urban District. The intent of the parties is to design and develop urban features that will facilitate integration of the Property (and surrounding area) into an urban district with a unified character that promotes pedestrian and vehicular circulation, without conflict, encourages opportunities for mixed-use development and enhances the

natural and built aesthetics in the area. In order to enhance connectivity and facilitate future urban development, driveways through the property shall be designed, wherever possible, to facilitate connections to the properties identified in Recital D, above. Curbing shall be used to define the parking lot area, such as perimeter curbing and main drive aisles. Driveway entrance(s) and interior landscaping features will also be curbed.

## Narrative:

Transition to an Urban District will likely occur as population density increases, land costs go up, the availability of commercial property diminishes, and a cultural transition occurs that makes projects combining retail, office and/or housing viable in the Spokane Marketplace. All of these necessary precedents are beyond the Applicant's control.

Both the Neighborhood and Applicants acknowledge that as a District Center this location is well-suited when the right combination of social and economic circumstances make it attractive.

To preserve the opportunity for a successful transition to an Urban District, the following strategies are a part of the planning process:

- Development must encourage buildings built to the streets and should support future infill development as market demands.
- Where practical, locate buildings towards the streetscape and provide storefront orientation to both the interior parking area and the streetscape.
- Quality "four-sided" architecture of all elevations and a preference for multi-story and mixed use buildings shall be implemented where practical, considering the need for utilitarian equipment such as meters, loading docks, etc.
- Preserve future building pad locations along the street and do not locate necessary infrastructure in these areas.
- Pedestrian circulation within the site shall be high quality and include urban amenities whenever practical.
- Urban plaza(s) shall provide for a quality urban experience. A transit-oriented stop is envisioned immediately south of the mid-parcel entrance to the Black property. Design of the transit stop shall include architecture components complementary with the adjacent building and may be incorporated in the building's envelope if desired. All plaza(s) will connect to the pedestrian systems along Regal and be designed for integration with the site development.
- Locate proposed driveways to align across Regal and Palouse to allow driveways to transition to future urban-style intersections; locate appropriately to provide urban-scale blocks.
- Plan street improvements to allow future on-street parking while protecting urban-scale sidewalks, streetscape and storefronts.
- Keyed Note 5 shows preferred locations for four-way driveway intersections to simulate urban scale blocks.